

<b>Location:</b>	<b>Oak Knoll Development – Parcel 12; 8750 Mountain Boulevard</b>
<b>Assessor’s Parcel Number:</b>	<b>043A467500321</b>
<b>Proposal:</b>	Oak Knoll Final Development Permit (FDP) for construction of 38 residential townhouse units on Parcel 12
<b>Applicant:</b>	Marc Magstadt, SunCal
<b>Phone Number:</b>	Jeff Stevens, Danielian Associates/(949) 474.6030
<b>Owner:</b>	Oak Knoll Venture Acquisitions LLC
<b>Case File Number:</b>	PLN15378-PUDF04
<b>Planning Permits Required:</b>	FDP, compliance with CEQA
<b>General Plan:</b>	Mixed Housing Type Residential
<b>Zoning:</b>	D-OK-3 Oak Knoll District Residential Zone - 3
<b>Environmental Determination:</b>	Final Supplemental EIR certified on Nov. 7, 2017
<b>Historic Status:</b>	Non-Historic Property
<b>City Council District:</b>	7- Treva Reid
<b>Status:</b>	Under Review
<b>Staff Recommendation:</b>	Approve Final Development Permit
<b>Finality of Decision:</b>	Appealable to City Council
<b>For Further Information:</b>	Contact case planner <b>Michele T. Morris</b> at (510) 238-2235 or <a href="mailto:mmorris2@oaklandca.gov">mmorris2@oaklandca.gov</a>

**SUMMARY**

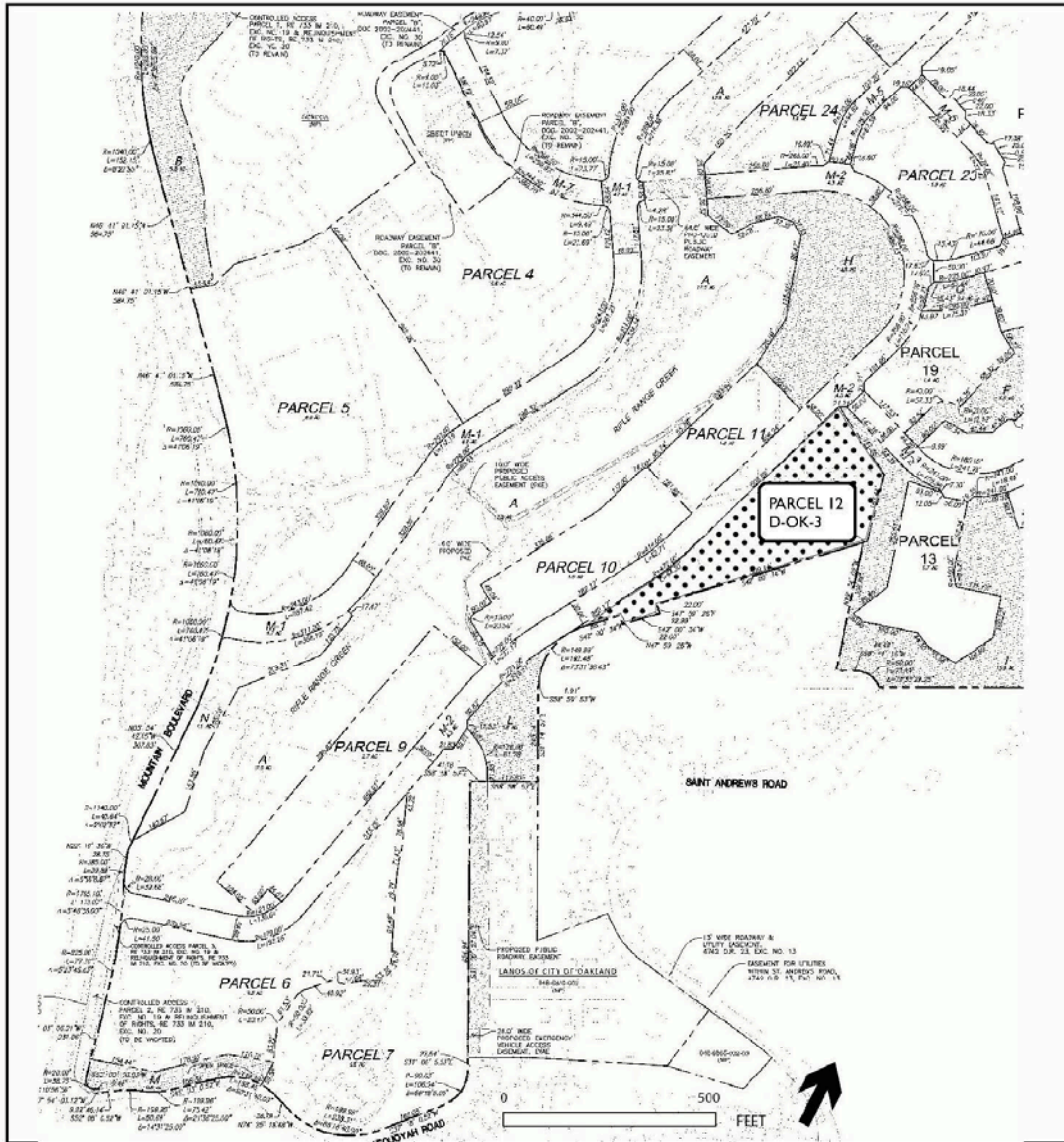
The proposed project is a Phase 1 Final Development Permit (FDP) for construction of 38 residential units (townhomes) on Parcel 12 in the Oak Knoll Planned Unit Development (PUD). Parcel 12 is a northern uplands site along the new Creekside Loop which is currently only accessible from Mountain Boulevard.

As detailed below, staff finds that the project meets all the required Findings. Therefore, staff recommends approval of the project subject to the Oak Knoll PUD Conditions of Approval (Attachment C).

**PROPERTY AND NEIGHBORHOOD DESCRIPTION**

Oak Knoll Development encompasses an 84.7-acre site east of Interstate 580 (I-580) and is located approximately 9 miles southeast of downtown Oakland. Mountain Boulevard and the I-580 freeway are to the west; Keller Avenue to the north and east; and Sequoyah Road, a City-owned property, and residential neighborhoods are located to the south. Parcel 12, the project site, which is near the center of the Oak Knoll Development site and is currently not accessible. Parcel 12 will be accessible from a new road, Creekside Loop, and bordered on its east by Street “C.”

# CITY OF OAKLAND PLANNING COMMISSION



Case File: PLN15378-PUDF04  
Applicant: David Soyka and Marc Magstadt - SunCal  
Address: Oak Knoll FDP Parcel 12  
Zone: D-OK-3



**BACKGROUND***Planned Unit Development History*

In 1996, the Naval Medical Center Oakland property was subject to a Final Reuse Plan that presented five land use alternatives for the reuse of the property. The Maximum Capacity Alternative within the Final Reuse Plan included: a) 584 residential units; b) 400,000 sq. ft. of commercial space; and c) 32 acres of open space. The Maximum Capacity Alternative was approved by the Oakland City Council as the preferred alternative.

In 2005, SunCal Oak Knoll, LLC proposed the “former Oak Knoll Project” which included 960 residential units, 82,000 sq. ft. of commercial space and 53 acres of open space. The “former Oak Knoll Project” was not approved.

*Approved Oak Knoll Land Use Entitlements*

The Oak Knoll Development was submitted in 2015 and approved in November 2017. The approval included General Plan Amendments, Rezoning, Planned Unit Development/Preliminary Development Plan, Final Development Plan for Master Developer Site Improvements, Final Development Permit (FDP) or Relocation and Rehabilitation of Club Knoll, Design Review, Vesting Tentative Tract Map, and a Creek Permit. The 2017 approved project is referred to as “Oak Knoll.”

Oak Knoll includes:

- 918 residential units of varying types;
- 72,000 sq. ft. of neighborhood serving commercial in the Village Center;
- 14,000 square feet of civic/commercial use, including relocation of the historic Club Knoll to the center of the Project site with 4,000 sq. ft. of community space and 10,000 sq. ft. of commercial space;
- Approximately 67.6 acres of open space and recreation areas, including four new public parks, a system of trails, bikeways, and walkways;
- Restoration and enhancement of the Rifle Range Creek, Powerhouse Creek and Hospital Creek corridors (16.7 acres);
- Three phases of development; and
- Street network designed as "complete streets" for the safe and comfortable travel of all transportation modes.

The following provides a summary of the current status of the Oak Knoll Development:

- Land Use Entitlement: The Oak Knoll Project Supplemental Environmental Impact Report (SEIR) was certified and the General Plan Amendment, Rezoning, Vesting Tentative Tract Map, Creek Permit, and the Oak Knoll PUD was approved on November 7, 2017.

- Construction-Related Permits:
  - Grading Permit: The applicant has received a Grading permit for Phase 1 of the development, which includes Parcel 6 and Parcel 12.
  - Bridge Permits: The applicant has received construction related permits for the pedestrian and vehicular bridge located in Phase 1.
  - Public Improvements: The applicant has applied for and received the Public Infrastructure on Private Property (PX) permit for the public improvements in Phase 1, including the streets and utilities.
  - Club Knoll: The historic Club Knoll has a series of Building Permits associated with it, including demolition, alteration, and reconstruction.
- Compliance with Conditions of Approval: The relocation and restoration of Club Knoll is underway. Public improvement permits, various alternate method construction permits and Private infrastructure permits for on-site improvements are under review. The City and the applicant are actively working on formation of the Community Facilities District (CFD), Geologic Hazard Abatement District (GHAD) and Subdivision Agreement.
- Tree Permit Amendment: An amendment to the approved Tree Removal Permit was received on May 3, 2021. The amendment proposes to remove 394 additional trees and requires compliance with California Environmental Quality Act (CEQA). The CEQA consultant has not yet finalized the scope of work in order to proceed with analysis.
- Final Development Permits:
  - FDP for Club Knoll was approved with the PUD on November 7, 2017;
  - FDP for Phase 1 Master Developer Site Improvements was approved with the PUD on November 7, 2017;
  - FDPs for Phase 1 Residential Development Parcels. The Master Developer has submitted eight FDPs for Phase 1, which are in various stages of City review:
    - Parcel 6: Townhomes. Deemed complete and under consideration by Planning Commission;
    - Parcel 12: Townhomes. Deemed complete and under consideration by the Planning Commission at this meeting (and the subject of this report);
    - Parcel 11: Alley homes. Deemed complete and under review;
    - Parcel 19: Alley homes. Deemed complete and under review;
    - Parcel 23: Alley homes. Deemed complete and under review;
    - Parcel 24: Alley homes. Deemed complete and under review;
    - Parcel 9: Court homes. Deemed complete and under review;
    - Parcel 10: Court homes. Deemed complete and under review.

### *Public Review:*

The proposed FDP for Parcel 12 was initially presented to the Design Review Committee (DRC) on June 23, 2021 and again to DRC meeting of October 27, 2021. At the first DRC public hearing, the applicant was asked to revise the plans in order to: be more responsive to site conditions (such as slope and size of parcel); differentiate the buildings and groups of buildings from one another, break the massing of the buildings and articulate the facades to provide visual

interest, and improve the street interface and enhance the connection to the street. The DRC reviewed the applicant's resubmittal at the second DRC meeting and found it to be substantially improved and responsive to the comments received. The DRC voted to move the application forward for Planning Commission consideration.

## **PROJECT DESCRIPTION**

The proposed Parcel 12 project includes 38 residential units. Plans, elevations, and illustrations are provided in **Attachment A** to this report. In general, the proposed plans include the following characteristics:

- **Style:** The proposed residential development includes stylistic references to common and vernacular California architectural styles, including Craftsman and Mission architectural styles.
- **Site Planning:** The proposed FDP includes 11 buildings including duplex, triplex, 4-plex and 5-plex building arrangements.
- **Unit Types:** Parcel 12 proposes three-story, three-bedroom townhomes grouped into multifamily buildings and would consist of two duplex, five triplex, one 4-plex, and three 5-plex buildings. These may be units for rent, or condominium units in the future.
- **Parking:** Each unit has a two-car attached garage, for a total of 76 off-street parking spaces.
- **Open Space:** The FDP includes a combination of group open space, private balconies and ground floor porches.

## **GENERAL PLAN ANALYSIS**

The Parcel 12 project site is in the Mixed Housing Type Residential General Plan land use designation. The intent of the Mixed Housing Type Residential land use designation is "to create, maintain, and enhance residential areas typically located near the City's major arterials and characterized by a mix of single-family homes, townhouses, small multi-unit buildings, and neighborhood businesses where appropriate." However, the Land Use Element further describes the Desired Character and Use in this designation to involve future development "remain[s] residential in character." The master-planned Oak Knoll PUD allows for development of up to 918 residential units.

The required Findings for the FDP are attached and included in staff's evaluation as part of this report. The following is an analysis of how the proposed project meets applicable General Plan objectives (staff analysis in indented, italicized text below each objective):

- **Objective N3:** Encourage the construction, conservation, and enhancement of housing resources to meet the current and future needs of the Oakland community.
  - *Policy N3.9 – Facilitating Housing Construction. Orienting Residential Development. Residential developments should be encouraged to face the street*



and to orient their units to desirable sunlight and views, while avoiding unreasonably blocking sunlight and views for neighboring buildings, respecting the privacy needs of residents of the development and surrounding properties, providing for sufficient conveniently located on-site open space, and avoiding undue noise exposure.

- *The proposal will deliver market-rate housing that will intensify and support new uses in the South Hills area of Oakland. Front entry porches and rear-facing porches are designed to create a “sense of address” and providing gates, yards and access to public streets and paseos and/or pathways.*
- Objective N6: Encourage a mix of housing costs, unit sizes, types, and ownership structures.
  - *The proposed project will include townhomes consisting of duplexes, triplexes, four-plex and five-plex buildings which will create more home ownership opportunities.*

**ZONING ANALYSIS**

Parcel 12 is located within the South Hills area of the Oakland hills in the D-OK-3 Oak Knoll District Residential Zone - 3 (D-OK-3). The intent of the D-OK-3 Zone is to create, maintain, and enhance areas suitable for medium-density residential units, such as townhomes. The zoning district provides medium density housing development. The following discussion outlines the purpose of the D-OK-3 regulations, with staff analysis provided below in indented, italicized text:

- Create, maintain, and enhance areas suitable for medium-density residential units, such as townhomes.
  - *The proposed project is a market-rate housing project that will diversify living and home ownership opportunities in the Oak Knoll Development.*

*Zoning Analysis*

<b>Criteria</b>	<b>OK-3</b>	<b>Proposed</b>	<b>Analysis</b>
<b>Land Use</b>			
Permanent Residential	P	P	Allowed
Multi-family Dwelling Facility	P	P	Allowed
Density	1 unit per 1600 sf lot area on lots 5000 sf or greater	174,240 sq ft, 38 units*1600 = 60,800	Complies
Maximum Lot Coverage	55%	36.1%	Complies

<b>Criteria</b>	<b>OK-3</b>	<b>Proposed</b>	<b>Analysis</b>
<b>Land Use</b>			
Maximum wall height primary building	35 ft/ 3 stories	3 stories/ approx. 30 ft	Complies
Maximum pitched roof height	40 ft	40 ft	Complies
Open Space – Group Residential	170 sf per unit (can be replaced by 70 sf of dedicated Private Open Space per unit).	2 <sup>nd</sup> floor decks between 128 sf or 144 sf.	Complies
Parking	1 space per dwelling unit = 38 spaces	Individual two-car garages per unit	Complies
Retaining Walls	Multiple retaining walls shall be separated by a distance of at least four (4) feet between the exposed faces of each wall.	4 ft. minimum	Complies

*Compliance with Oak Knoll Design Guidelines*

The DRC and Planning staff believe that the FDP for Parcel 12 is in compliance with the Oak Knoll Design Guidelines which is provided as a part of **Attachment C** to this report.

**ZONING AND RELATED ISSUES**

*Design*

Staff has worked with the applicant and architect to refine the proposed design for the Parcel 12 site. The project complies with the underlying zoning regulations. The applicant team has worked to improve the overall site plan of the project to provide activation on Mountain Boulevard and to limit the ‘back of house’ impacts. Staff reviewed the proposed project against the Oak Knoll Design Guidelines (see Attachment C). The project meets the following key guidelines:

<b>Design Guideline</b>	<b>Compliance Analysis</b>
<i>2.4 Townhomes</i>	
Create a 'sense of address' and a front door for each unit by providing 'door yards,'	Complies

gates, and access to public streets and paseos.	
<i>3.5 High Visibility Façades</i>	
Additive Façade Elements	Complies
Successful execution of second façade	Complies

The design has been revised to refine wayfinding, enhance a sense of address, and add visual impact for the townhomes. The corner lot façades and additive façade elements were enhanced with revised patio railings, added articulation at the ground floor by slimming porch columns, and rearranging gabled roof lines.

*Issues*

In general, the revised project plans are more responsive to the Oak Knoll Design Guidelines. at the second DRC meeting, staff identified the following design guideline issue related to the current Parcel 12 plans:

*Design Guideline 2.4 Townhomes - Stepping between units*  
 Stepping between units is encouraged to provide private balconies and a varied building frontage as viewed from the street. (Design Guidelines Compliance Matrix, Attachment D)

In general, staff acknowledges that the project has improved since receiving feedback from the Design Review Committee. The proposal has been revised to better comply with this design guideline by making improvements to the site plan and building façades. Although the proposed site plan exhibits minimal vertical stepping between the unit buildings, the varied mix of unit plans and façades combined with the design and building placement reduces the visual impact and breaks up the massing of the townhomes facing Creekside Loop.

**CALIFORNIA ENVIRONMENTAL QUALITY ACT**

The City Council certified a Supplemental Environmental Impact Report (SEIR) for the existing project approvals on November 7, 2017. The Oak Knoll Mixed Use Community Plan Project Supplemental Environmental Impact Report is available to the public on the web at: <https://oaklandca.s3.us-west-1.amazonaws.com/government/o/PBN/OurServices/Application/oak060436.pdf>. Staff has determined that no new information about the site, changes to the project, or circumstances under which the project would be undertaken have occurred that would require subsequent or supplemental environmental review for the proposed Parcel 12 FDP. In accordance with CEQA, the City reviewed and analyzed the proposed project changes and other relevant information to determine whether circumstances requiring the preparation of a subsequent or supplemental EIR exist. Based upon available information, the City has determined that none of those circumstances are present. Because the FDP is a refinement of,



and not a substantive change to, the approved project, no further environmental review is required. None of the circumstances that require a supplemental or subsequent EIR pursuant to CEQA Guidelines Section 15162 have occurred. And, specifically:

- There are no substantial changes proposed in the project which would result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- There are no substantial changes with respect to project circumstances which would result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- There is no new information of substantial importance which would result in new significant environmental effects, a substantial increase in the severity of previously identified significant effects, previously infeasible mitigation measures or alternatives now found to be feasible, or new mitigation measures or alternatives which are considerably different from previous ones that would substantially reduce environmental effects.

Here, based upon available information, the City believes that none of the circumstances described above have occurred since 2017 and, therefore, no subsequent or supplemental environmental review is required under CEQA.

**RECOMMENDATIONS:**

The proposed Oak Knoll Parcel 12 Final Development Permit is consistent with and constitutes a design evolution and refinement of the previously approved Preliminary Development Plan. Staff finds the proposed project to be well-designed, responsive to DRC comment, and recommends approval. Staff specifically recommends that the Planning Commission:

1. Rely on the Oak Knoll Mixed Use Community Plan Project Supplemental EIR as adequate under CEQA for analysis of the Oak Knoll Parcel 12 Final Development Permit pursuant to CEQA Guidelines Section 15162, and based on the attached findings (and incorporated herein by reference);
2. Approve the Oak Knoll Parcel 12 Final Development

Permit, subject to the attached findings.

Prepared by:



Michele T. Morris, Planner III

Reviewed by:



Catherine Payne  
Development Planning Manager

Approved for forwarding to the  
City of Oakland Planning Commission:



Edward Manasse, Deputy Director  
Bureau of Planning

**ATTACHMENTS:**

- A. Proposed Oak Knoll Parcel 12 Plans, dated Oct. 11, 2021
- B. Oak Knoll Mixed Use Community Plan Project Supplemental EIR provided online at <https://oaklandca.s3.us-west-1.amazonaws.com/government/o/PBN/OurServices/Application/oak060436.pdf>;
- C. Background Documents:
  - 1. D-OK-3 Oak Knoll District Residential Zone - 3 Zoning District Regulations
  - 2. Oak Knoll Preliminary Development Plan, Nov. 7, 2017; and
  - 3. Oak Knoll Design Guidelines, Nov. 7, 2017
  - 4. Oak Knoll Conditions of Approval, Nov. 7, 2017
- D. Design Guidelines Compliance Matrix for Parcel 12

**REQUIRED FINDINGS:**  
**OAK KNOLL DEVELOPMENT PARCEL 6**  
**FINAL DEVELOPMENT PERMIT**

Required findings include:

- California Environmental Quality Act
- D-OK-3 Oak Knoll District Residential Zone – 3 (D-OK-3) Findings for FDP
- Regular Design Review: Planning Code Section 17.136.050



**California Environmental Quality Act**

The City Council certified a Supplemental Environmental Impact Report (SEIR) for the existing project approvals on November 7, 2017. The Oak Knoll Mixed Use Community Plan Project Supplemental Environmental Impact Report is available to the public on the web at: <https://oaklandca.s3.us-west-1.amazonaws.com/government/o/PBN/OurServices/Application/oak060436.pdf>. Staff has determined that no new information about the site, changes to the project, or circumstances under which the project would be undertaken have occurred that would require subsequent or supplemental environmental review for the proposed Parcel 12 FDP. In accordance with CEQA, the City reviewed and analyzed the proposed project changes and other relevant information to determine whether circumstances requiring the preparation of a subsequent or supplemental EIR exist. Based upon available information, the City has determined that none of those circumstances are present. Because the FDP is a refinement of, and not a substantive change to, the approved project, no further environmental review is required. None of the circumstances that require a supplemental or subsequent EIR pursuant to CEQA Guidelines Section 15162 have occurred. And, specifically:

- There are no substantial changes proposed in the project which would result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- There are no substantial changes with respect to project circumstances which would result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- There is no new information of substantial importance which would result in new significant environmental effects, a substantial increase in the severity of previously identified significant effects, previously infeasible mitigation measures or alternatives now found to be feasible, or new mitigation measures or alternatives which are considerably different from previous ones that would substantially reduce environmental effects.

Here, based upon available information, the City believes that none of the circumstances described above have occurred since 2017 and, therefore, no subsequent or supplemental environmental review is required under CEQA.

**Oak Knoll Zoning District (OK-3)  
Findings for FDP**

**“The Planning Commission shall approve the Final Development Plan if it makes written findings that the Final Development Plan is in substantial conformance with the Preliminary Development Plan; Oak Knoll Design Guidelines, Oak Knoll Zoning Regulations (D-OK-3 Zone), Vesting Tentative Tract Map No. 8320, Conditions of Approval, and the Mitigation Monitoring Reporting Program ...”**

As demonstrated throughout this staff report, the Oak Knoll Development Parcel 12 Final Development Permit is consistent with the Preliminary Development Plan, the intent of the Oak Knoll Design Guidelines, the D-OK-3 District Residential zoning regulations, the Conditions of Approval, and the MMRP. As noted in this report, the FDP is a refinement of the PDP and includes only non-substantive changes intended to carry out the Oak Knoll Development project approvals and refine the design of Parcel 12 development.

**City of Oakland Design Review Findings**

The proposed Oak Knoll Development Parcel 12 design is subject to Planning Code Section 17.136.050 - Regular design review criteria. Accordingly, regular design review approval may be granted only if the proposal conforms to all of the following general design review criteria, as well as to any and all other applicable design review criteria:

**17.136.050 Regular design review criteria.**

Regular design review approval may be granted only if the proposal conforms to all of the following general design review criteria, as well as to any and all other applicable design review criteria:

A. For Residential Facilities.

1. That the proposed design will create a building or set of buildings that are well related to the surrounding area in their setting, scale, bulk, height, materials, and textures:

*The proposed Parcel 12 project is comprised of 38 townhouse residences that are designed to comply with the applicable design regulations for the site. The project will complement the surrounding area in scale, bulk, materials and textures.*

2. That the proposed design will protect, preserve, or enhance desirable neighborhood characteristics;

*The proposed Oak Knoll Parcel 12 is the first parcel of Phase 1 of the Oak Knoll Development. The project complies with the intent of the Oak Knoll Design Guidelines and provides massing and architectural style that enhances the visual appearance of the neighboring vicinity.*

3. That the proposed design will be sensitive to the topography and landscape.

*The Oak Knoll Parcel 12 project has been revised to be sensitive to the hillside topography and carefully designed to suit the hillside terrain.*

4. That, if situated on a hill, the design and massing of the proposed building relates to the grade of the hill;

*The proposal has been designed to complement the natural setting of the building site and complements the grade of the hill.*

5. That the proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district



plan, or development control map which have been adopted by the Planning Commission or City Council.

*As noted throughout this staff report, the Parcel 12 Final Development Permit is an evolution and refinement of the approved Planned Unit Development, and complies with the underlying regulations controlling development of the site, and with the intent of the Oak Knoll Design Guidelines.*



# OAK KNOLL

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FINAL  
DEVELOPMENT  
PLAN  
PARCEL 12

02.03.20

Revision 7: 10.11.21

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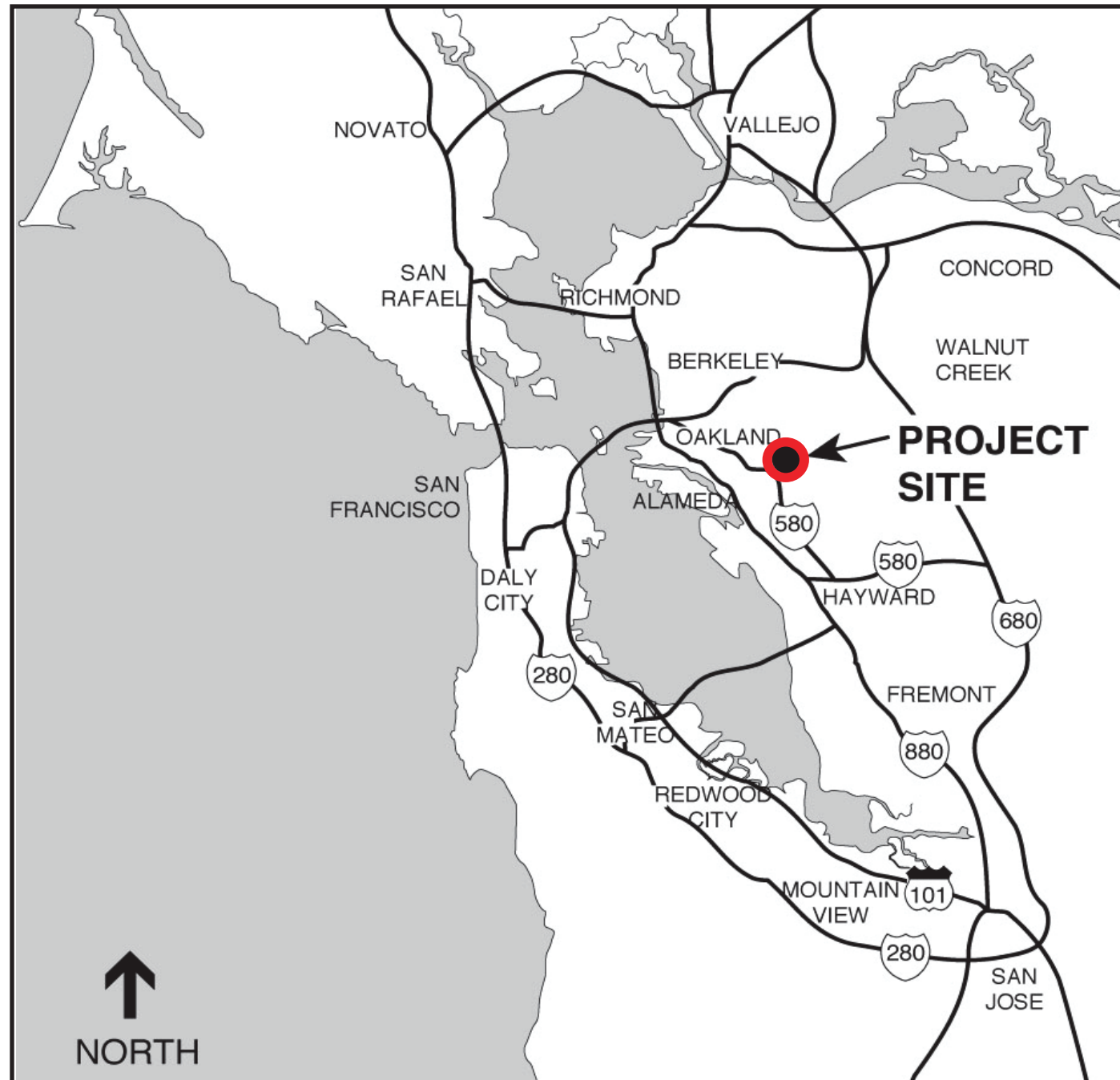
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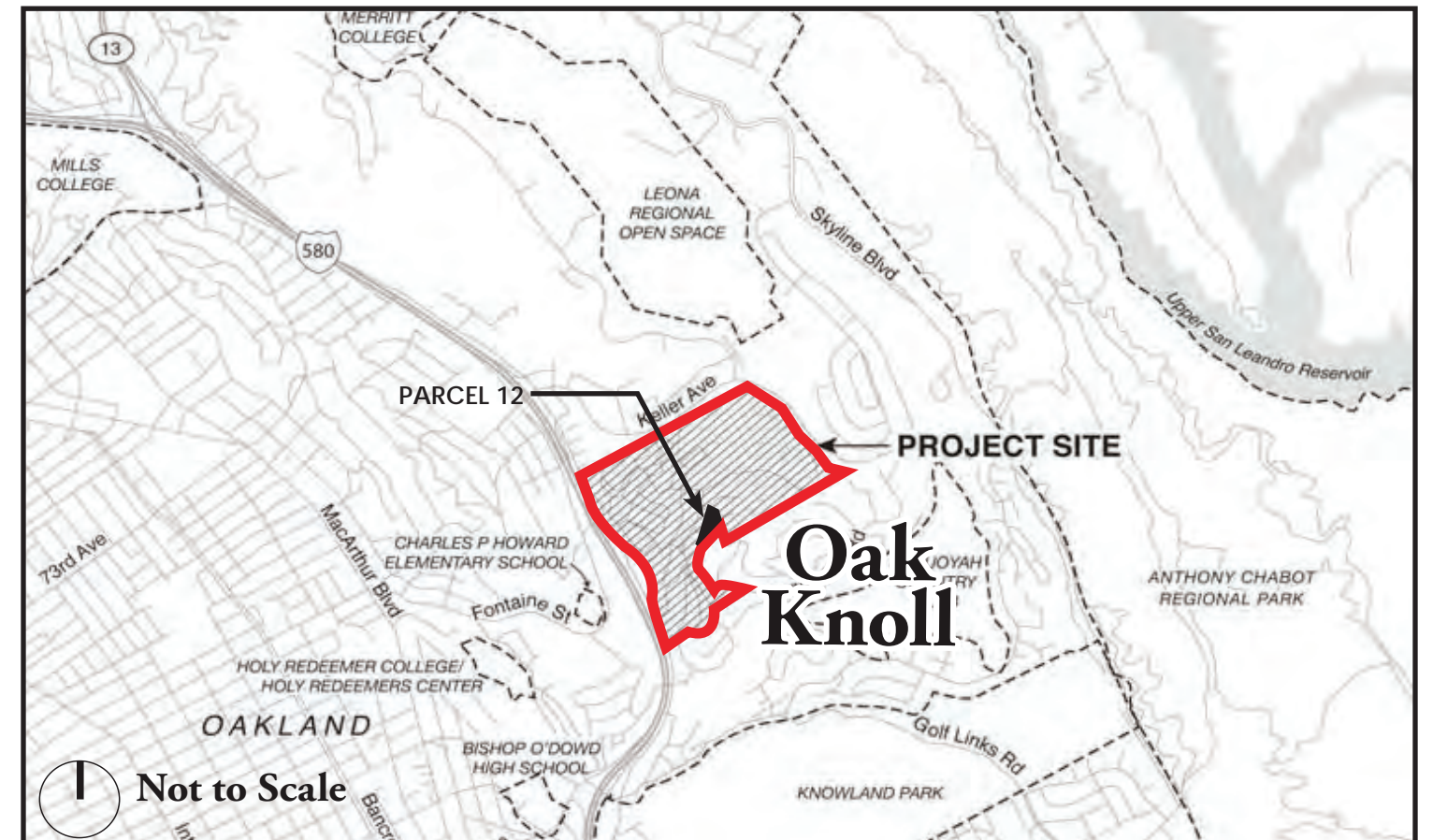


An aerial photograph of a city and its surrounding landscape. The city is densely packed with buildings and roads, extending towards a large body of water in the distance. The foreground shows a mix of green fields, forests, and some industrial or commercial structures. A semi-transparent blue box is overlaid on the center of the image, containing the word "INTRODUCTION" in a blue, serif font.

# INTRODUCTION



LOCATION



VICINITY

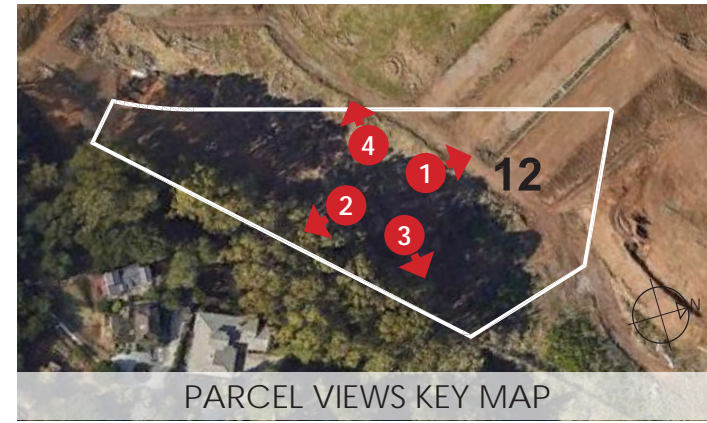
# OAK KNOLL

LOCATION & VICINITY MAP  
FINAL DEVELOPMENT PLAN - PARCEL 12





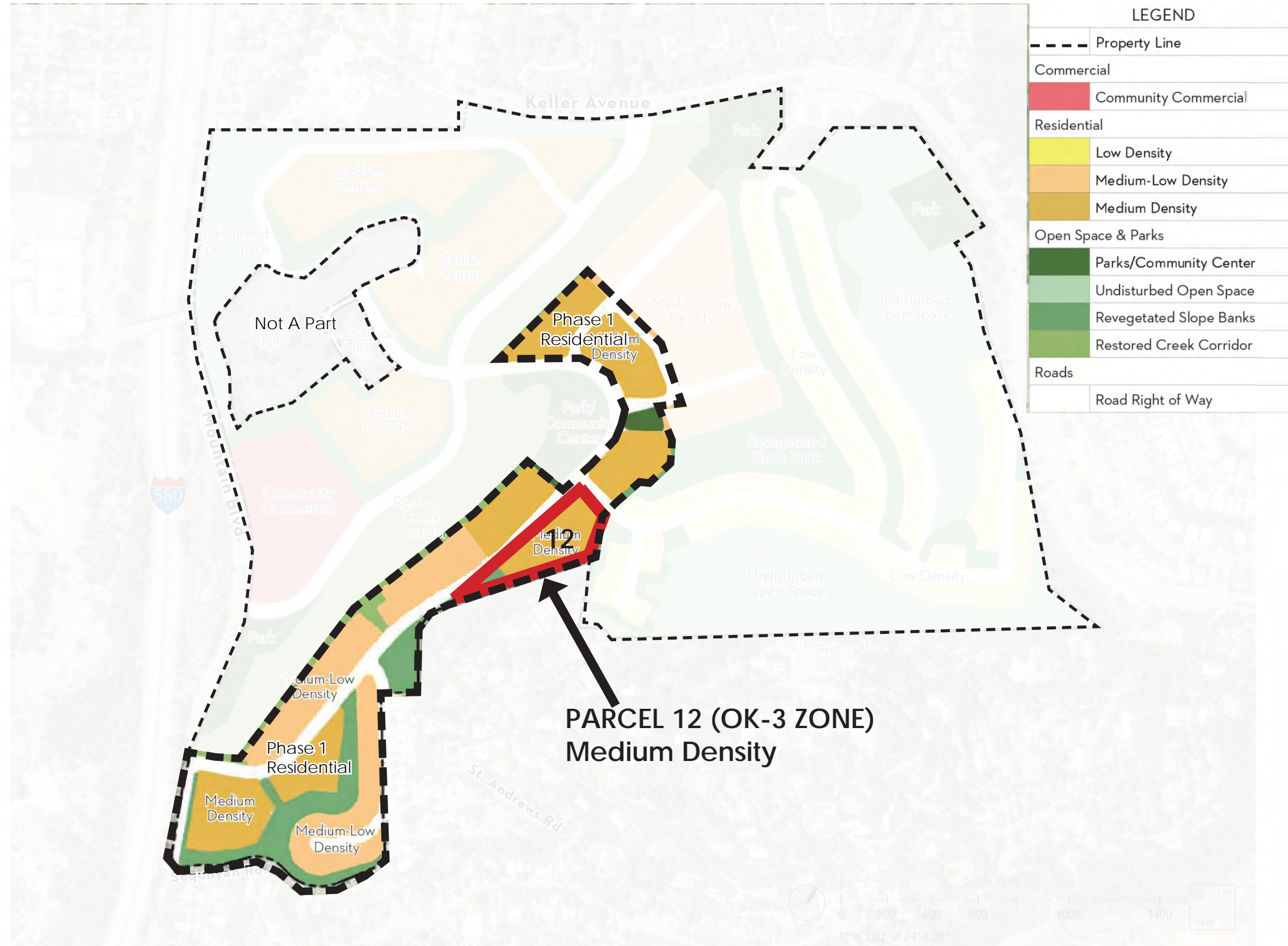




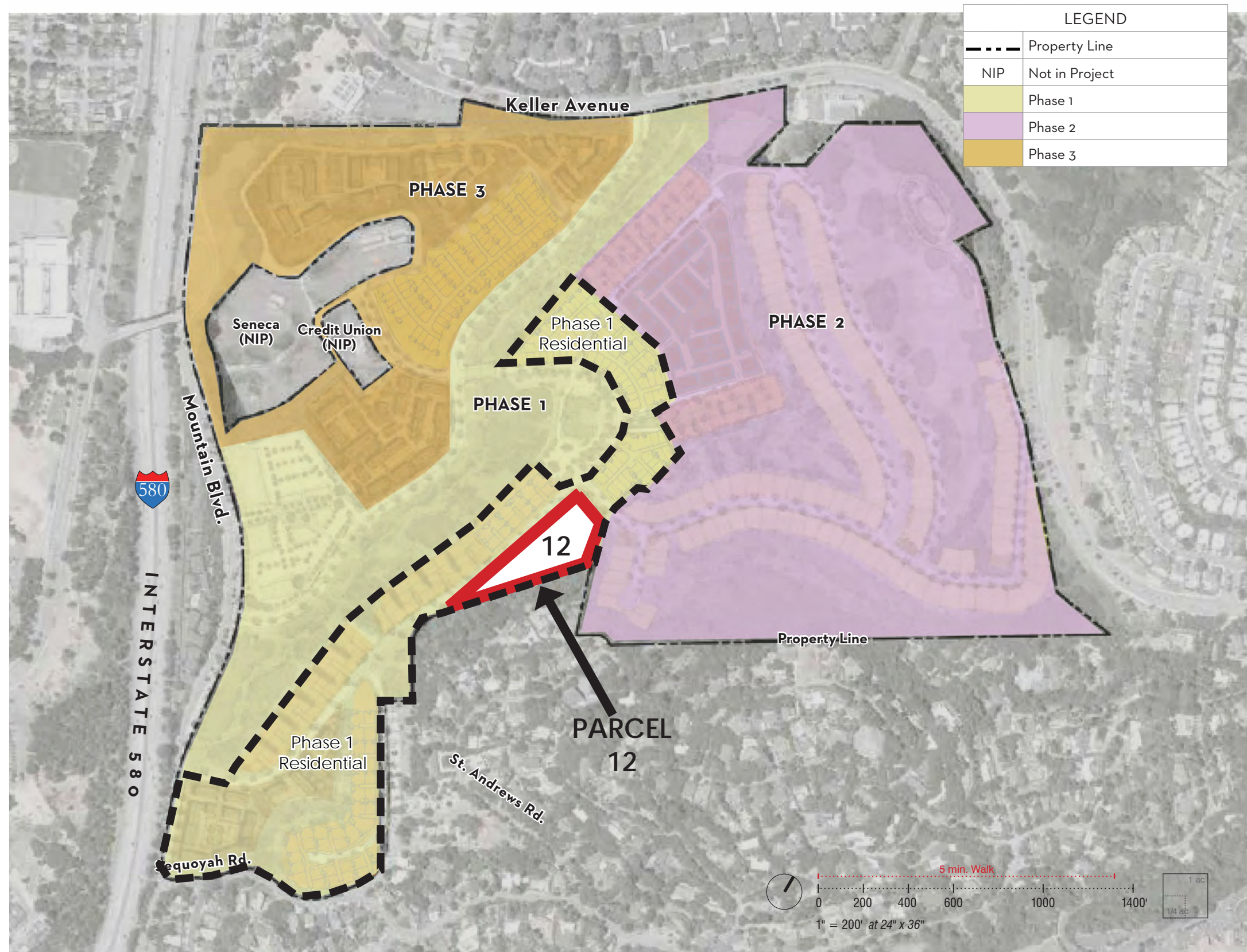
# OAK KNOLL

CONTEXT PHOTOS  
FINAL DEVELOPMENT PLAN - PARCEL 12







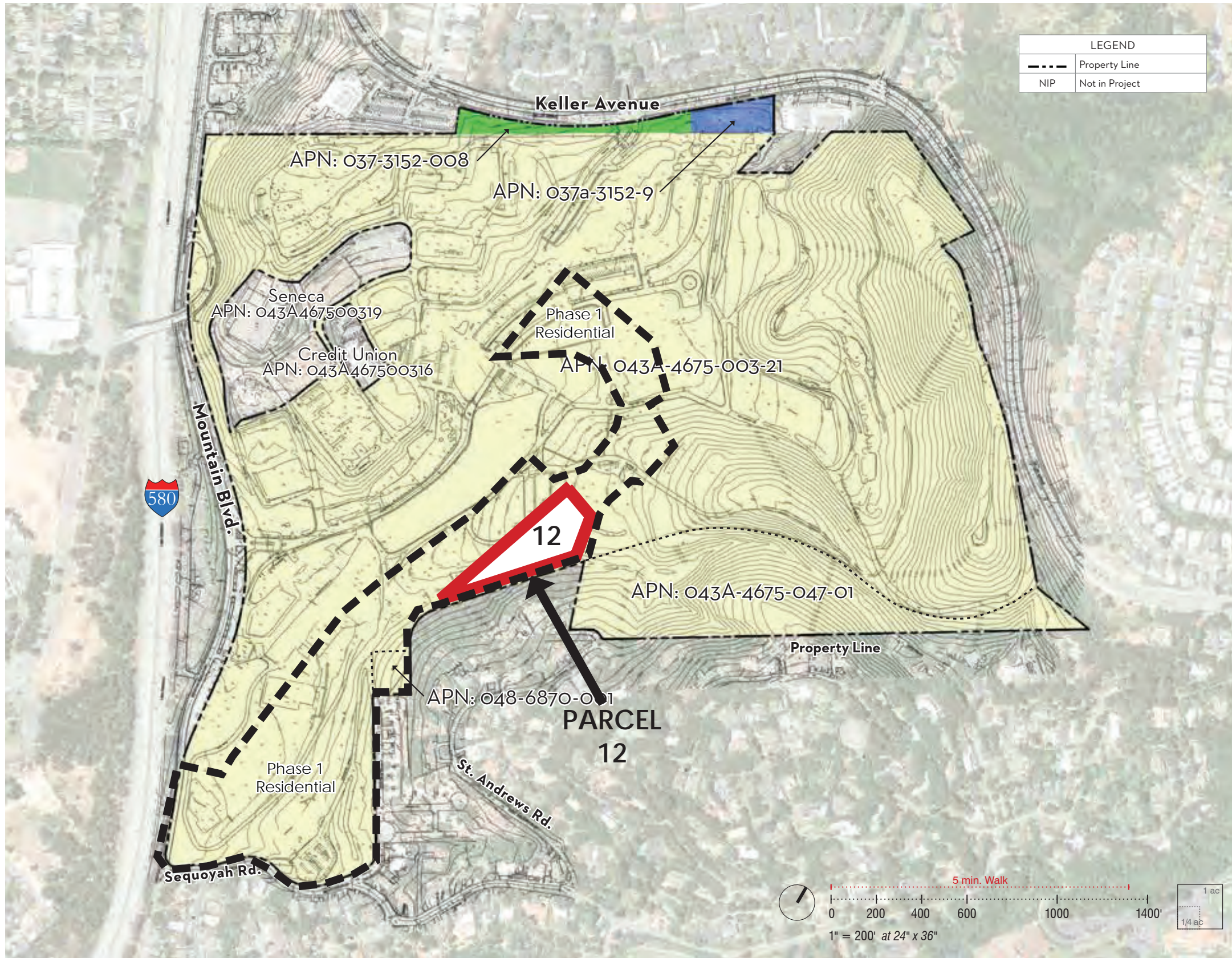


# OAK KNOLL

## PHASING & PHASE 1 RESIDENTIAL

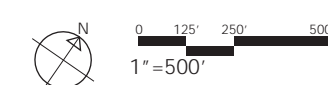
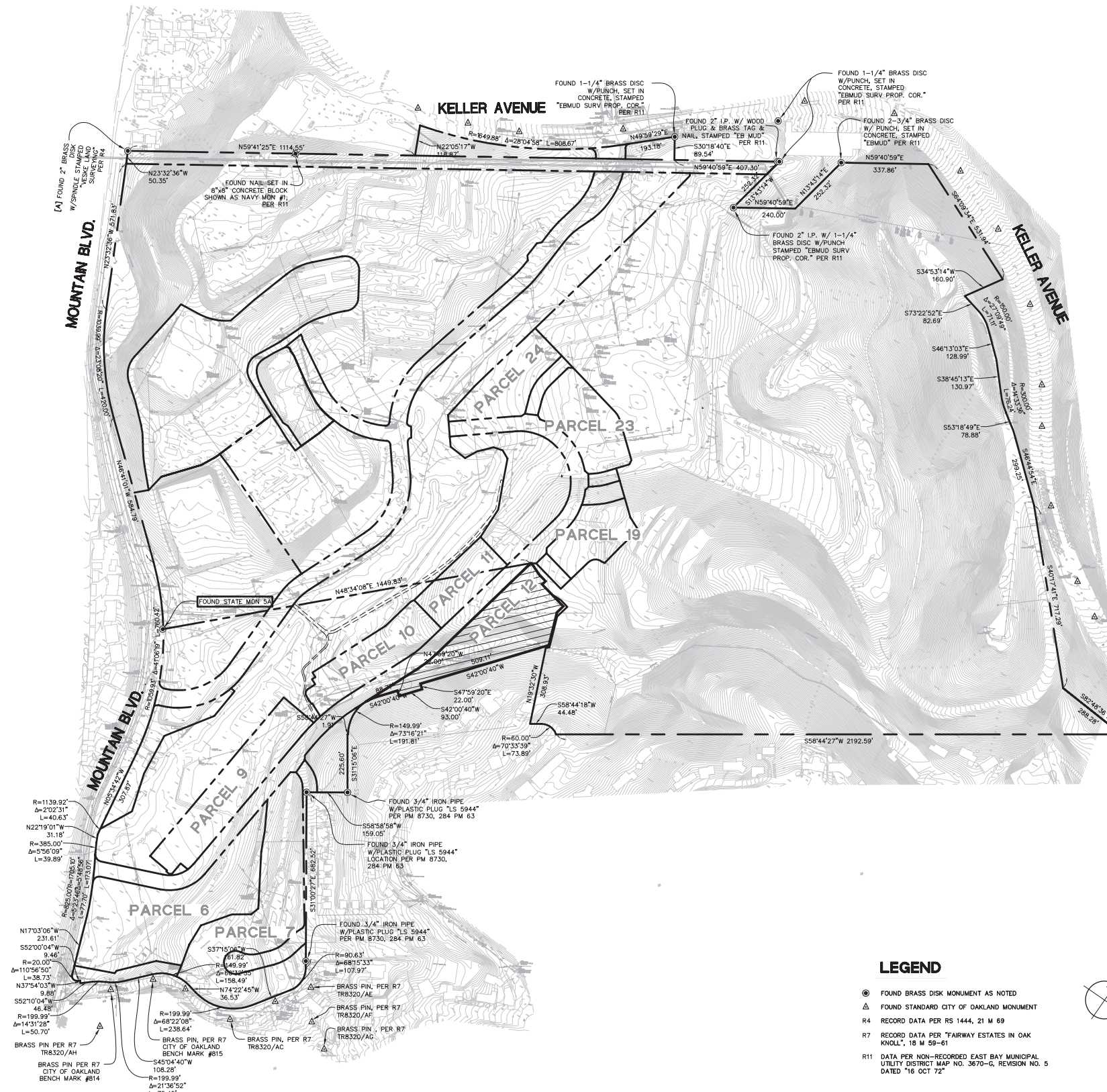
### FINAL DEVELOPMENT PLAN - PARCEL 12







**NOTES:**  
 DATE OF AERIAL TOPOGRAPHIC SURVEY:  
 09-24-13 BY PHOTO SCIENCE, INC.



# OAK KNOLL

## OVERALL PROPERTY BOUNDARY & TOPOGRAPHY

### FINAL DEVELOPMENT PLAN - PARCEL 12

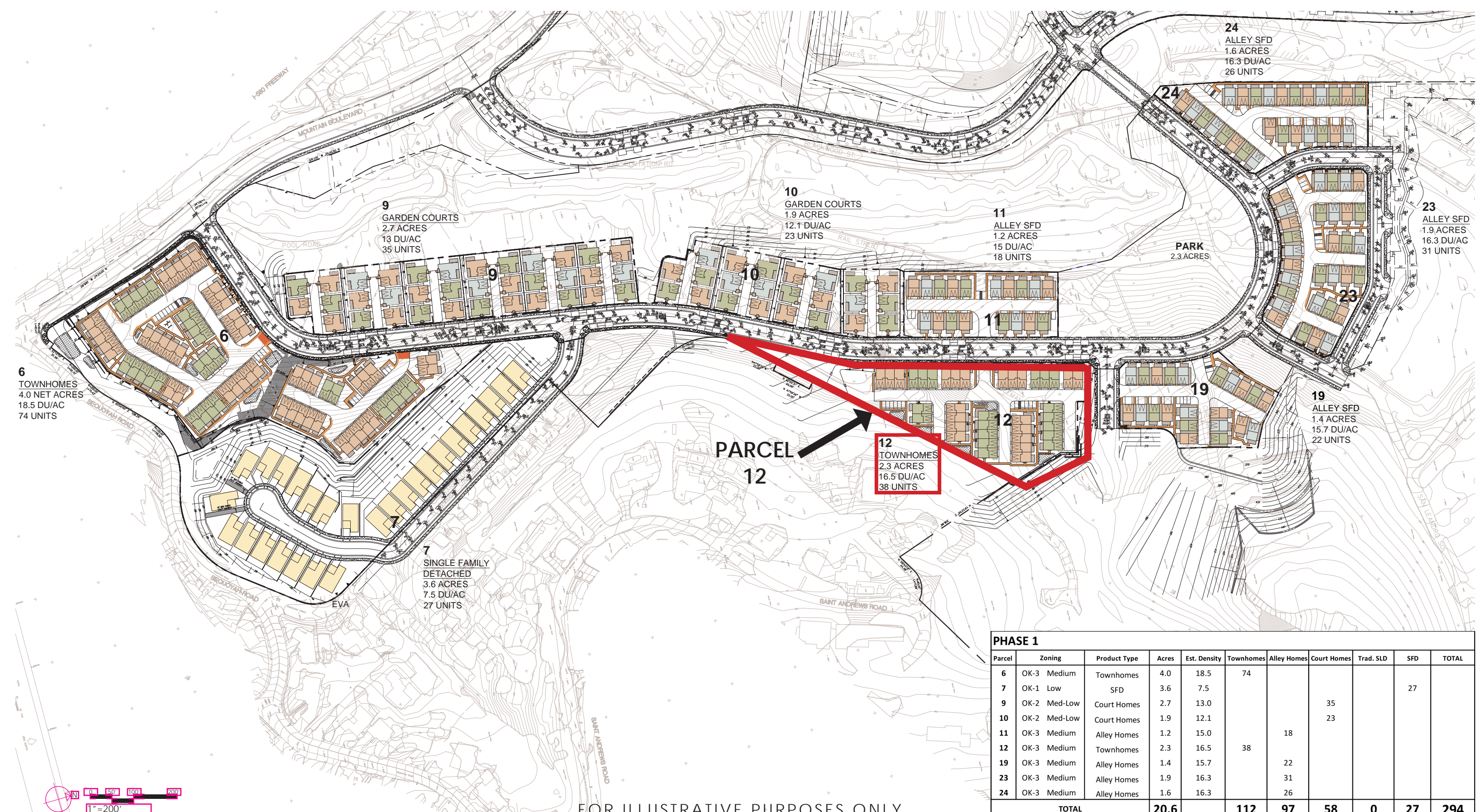






# THE PLAN





**6**  
TOWNHOMES  
4.0 NET ACRES  
18.5 DU/AC  
74 UNITS

**9**  
GARDEN COURTS  
2.7 ACRES  
13 DU/AC  
35 UNITS

**10**  
GARDEN COURTS  
1.9 ACRES  
12.1 DU/AC  
23 UNITS

**11**  
ALLEY SFD  
1.2 ACRES  
15 DU/AC  
18 UNITS

**24**  
ALLEY SFD  
1.6 ACRES  
16.3 DU/AC  
26 UNITS

**23**  
ALLEY SFD  
1.9 ACRES  
16.3 DU/AC  
31 UNITS

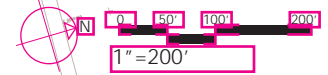
**19**  
ALLEY SFD  
1.4 ACRES  
15.7 DU/AC  
22 UNITS

**12**  
TOWNHOMES  
2.3 ACRES  
16.5 DU/AC  
38 UNITS

**7**  
SINGLE FAMILY  
DETACHED  
3.6 ACRES  
7.5 DU/AC  
27 UNITS

**PARK**  
2.3 ACRES

**PARCEL**  
**12**



**PHASE 1**

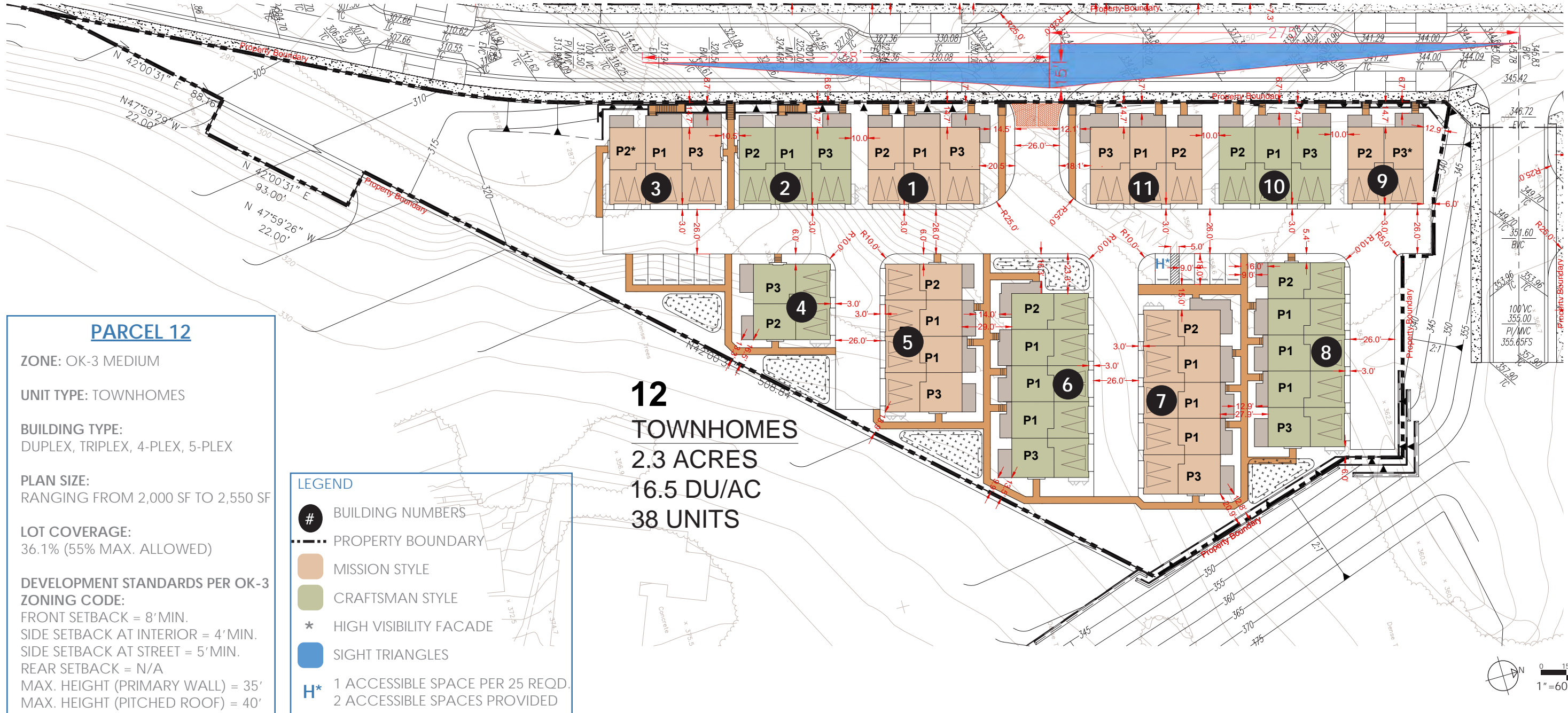
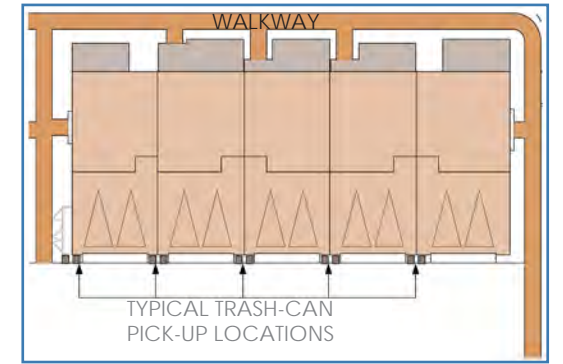
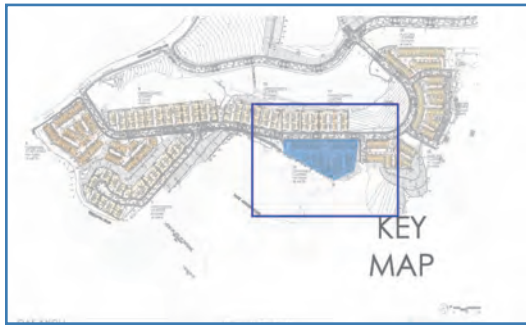
Parcel	Zoning	Product Type	Acres	Est. Density	Townhomes	Alley Homes	Court Homes	Trad. SLD	SFD	TOTAL
6	OK-3 Medium	Townhomes	4.0	18.5	74					
7	OK-1 Low	SFD	3.6	7.5					27	
9	OK-2 Med-Low	Court Homes	2.7	13.0			35			
10	OK-2 Med-Low	Court Homes	1.9	12.1			23			
11	OK-3 Medium	Alley Homes	1.2	15.0		18				
12	OK-3 Medium	Townhomes	2.3	16.5	38					
19	OK-3 Medium	Alley Homes	1.4	15.7		22				
23	OK-3 Medium	Alley Homes	1.9	16.3		31				
24	OK-3 Medium	Alley Homes	1.6	16.3		26				
<b>TOTAL</b>			<b>20.6</b>		<b>112</b>	<b>97</b>	<b>58</b>	<b>0</b>	<b>27</b>	<b>294</b>

FOR ILLUSTRATIVE PURPOSES ONLY

**OAK KNOLL**  
PHASE 1 SITE PLAN  
FINAL DEVELOPMENT PLAN - PARCEL 12







**PARCEL 12**

ZONE: OK-3 MEDIUM

UNIT TYPE: TOWNHOMES

BUILDING TYPE:  
DUPLEX, TRIPLEX, 4-PLEX, 5-PLEX

PLAN SIZE:  
RANGING FROM 2,000 SF TO 2,550 SF

LOT COVERAGE:  
36.1% (55% MAX. ALLOWED)

DEVELOPMENT STANDARDS PER OK-3 ZONING CODE:  
FRONT SETBACK = 8' MIN.  
SIDE SETBACK AT INTERIOR = 4' MIN.  
SIDE SETBACK AT STREET = 5' MIN.  
REAR SETBACK = N/A  
MAX. HEIGHT (PRIMARY WALL) = 35'  
MAX. HEIGHT (PITCHED ROOF) = 40'

**LEGEND**

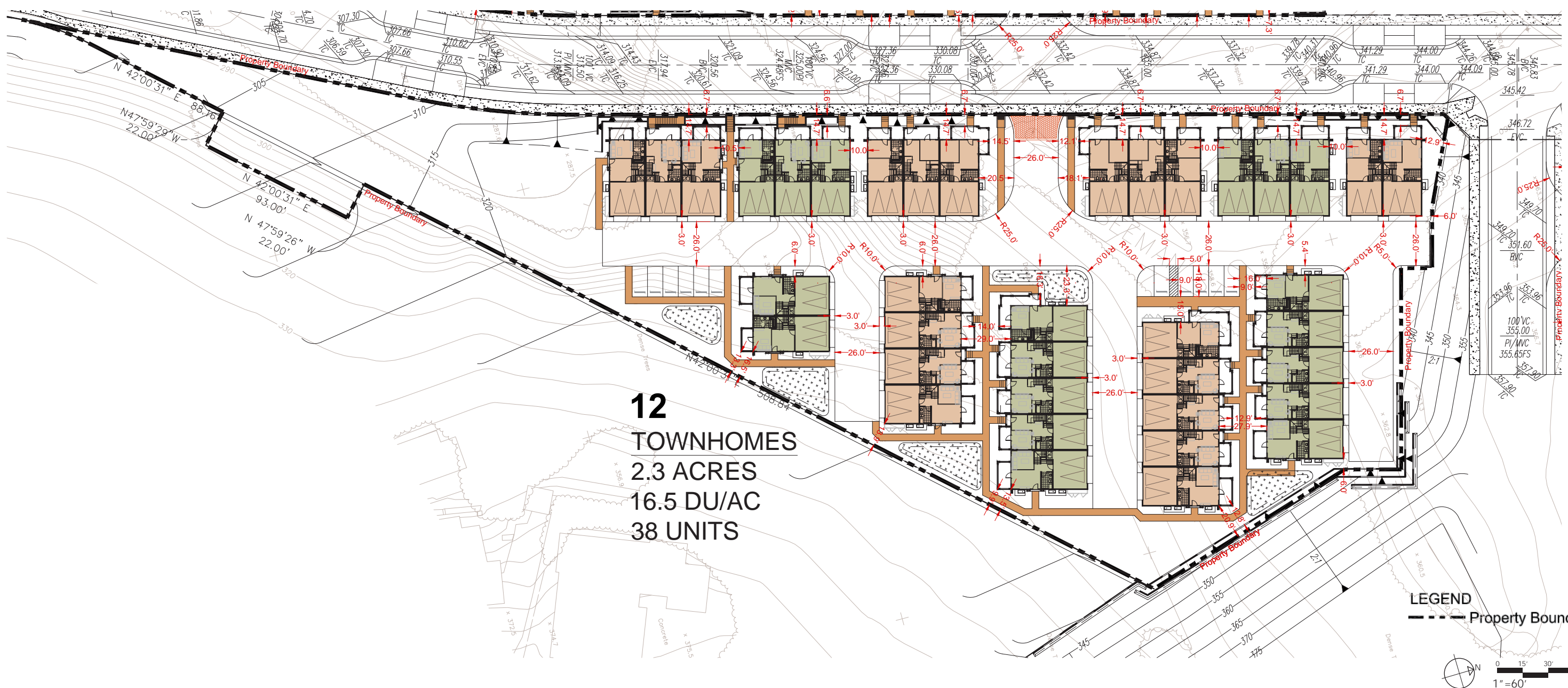
- # BUILDING NUMBERS
- PROPERTY BOUNDARY
- MISSION STYLE
- CRAFTSMAN STYLE
- \* HIGH VISIBILITY FACADE
- SIGHT TRIANGLES
- H\* 1 ACCESSIBLE SPACE PER 25 REQD.  
2 ACCESSIBLE SPACES PROVIDED

**12**  
**TOWNHOMES**  
**2.3 ACRES**  
**16.5 DU/AC**  
**38 UNITS**



**OAK KNOLL**  
PARCEL SITE PLAN  
FINAL DEVELOPMENT PLAN - PARCEL 12

Notes:  
Refer to engineer's drawings for details regarding retaining walls, precise location of boundaries, grading and slopes.  
For details of the floorplans, please see the floorplans in the Architecture section of this document.  
For landscaping and fence details refer to landscape plans of this document.



**12**  
**TOWNHOMES**  
 2.3 ACRES  
 16.5 DU/AC  
 38 UNITS

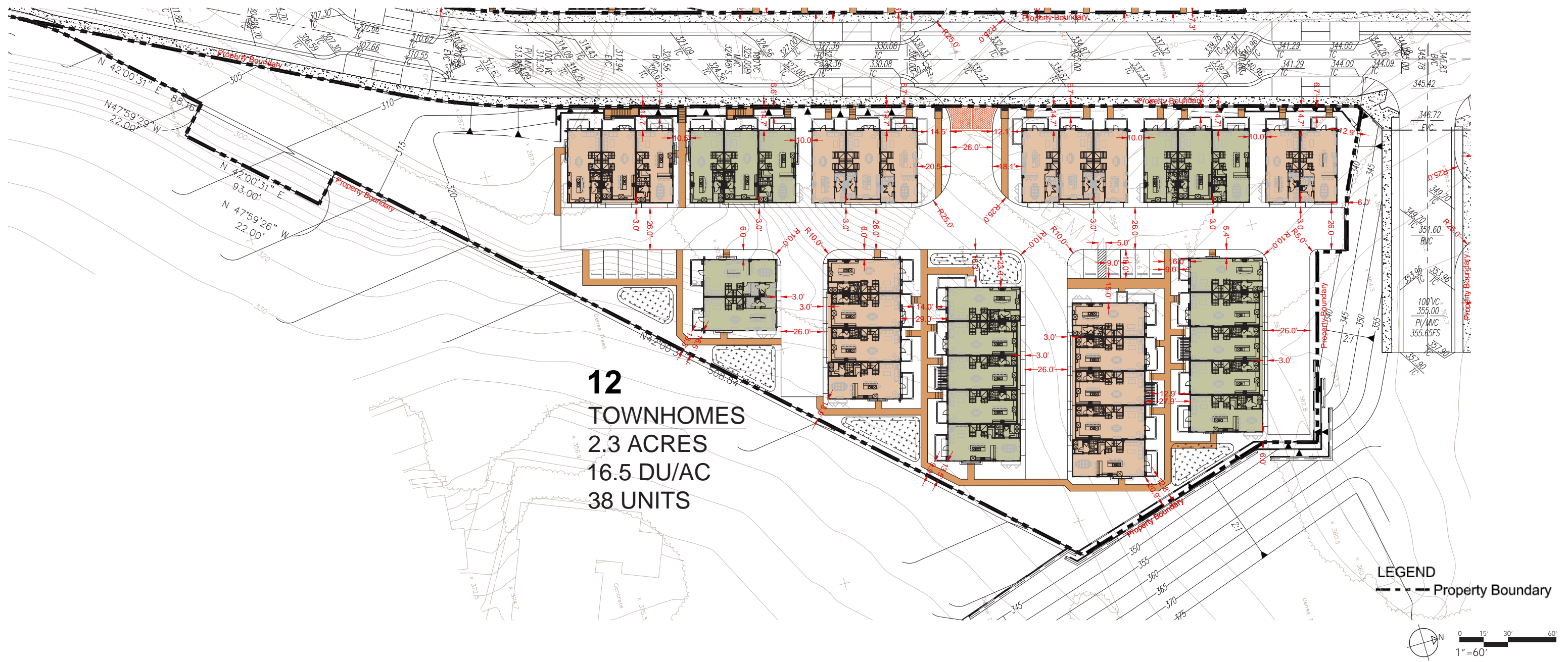
**LEGEND**  
 - - - Property Boundary

Notes:  
 Refer to engineer's drawings for details regarding retaining walls, precise location of boundaries, grading and slopes.  
 For details of the floorplans, please see the floorplans in the Architecture section of this document.  
 For landscaping and fence details refer to landscape plans of this document.

**OAK KNOLL**  
 FIRST FLOOR SITE PLAN  
 FINAL DEVELOPMENT PLAN - PARCEL 12







**12**  
**TOWNHOMES**  
 2.3 ACRES  
 16.5 DU/AC  
 38 UNITS

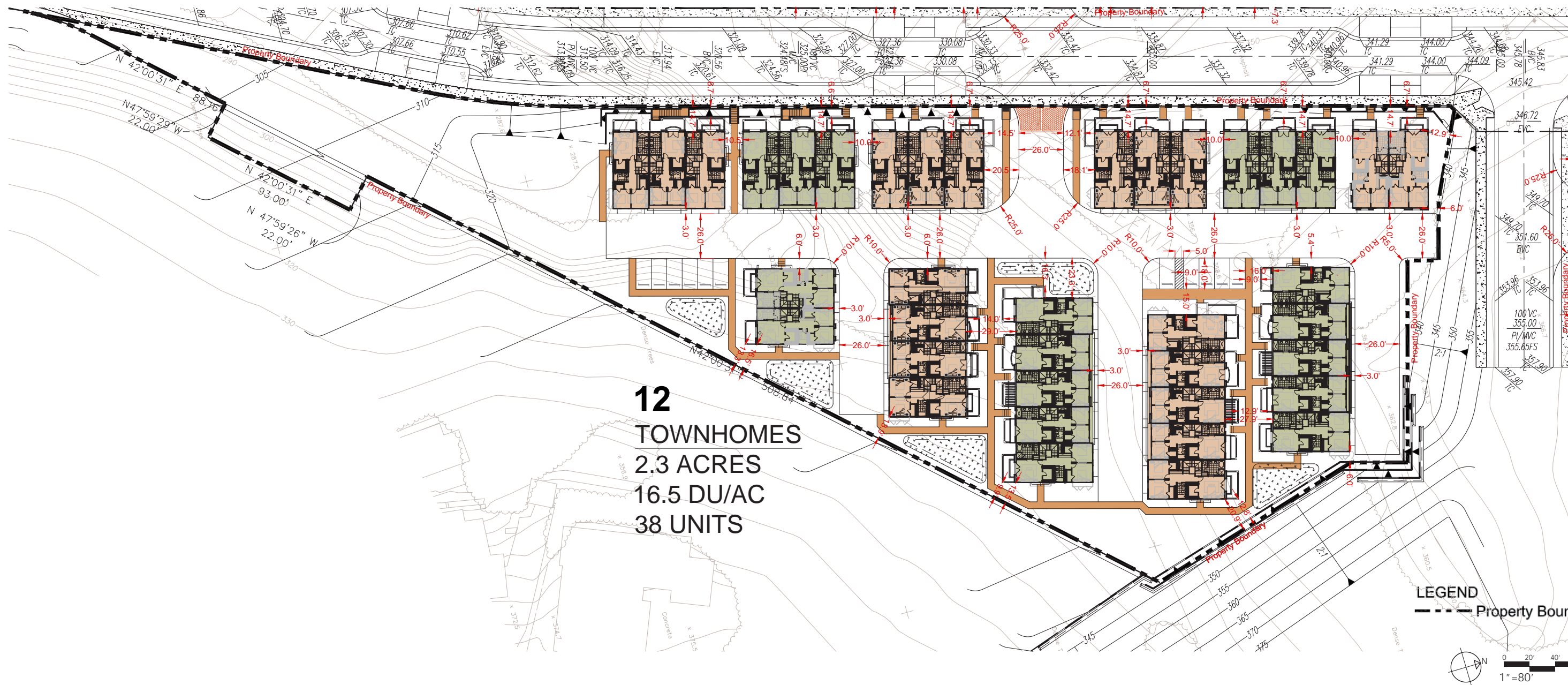
**LEGEND**  
 - - - - Property Boundary

0 15' 30' 60'  
 1" = 60'



**OAK KNOLL**  
 SECOND FLOOR SITE PLAN  
 FINAL DEVELOPMENT PLAN - PARCEL 12

Notes:  
 For details of the floorplans, please see the floorplans in the Architecture section of this document.



Notes:  
For details of the floorplans, please see the floorplans in the Architecture section of this document.

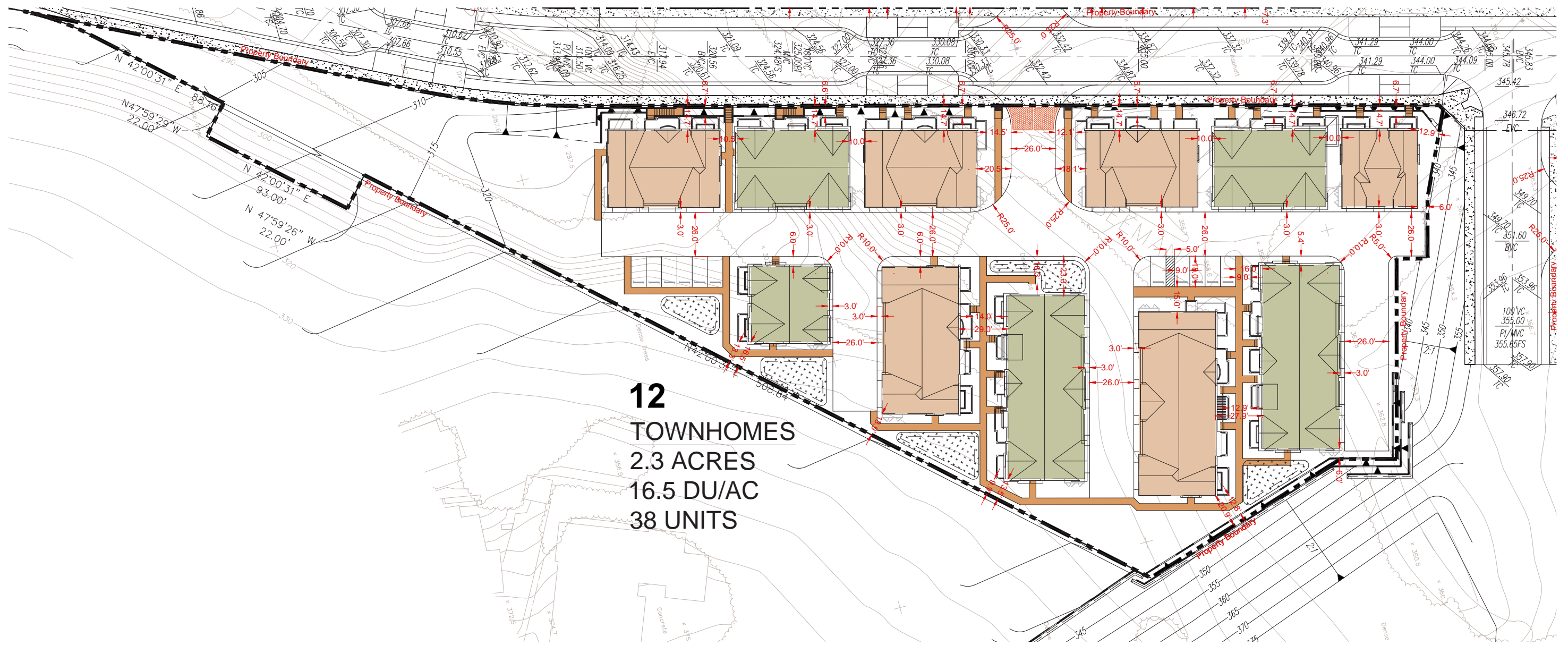
# OAK KNOLL

## THIRD FLOOR SITE PLAN

### FINAL DEVELOPMENT PLAN - PARCEL 12

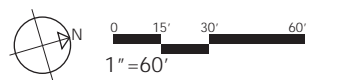






**12**  
**TOWNHOMES**  
 2.3 ACRES  
 16.5 DU/AC  
 38 UNITS

**LEGEND**  
 - - - - Property Boundary



**OAK KNOLL**  
 ROOF SITE PLAN  
 FINAL DEVELOPMENT PLAN - PARCEL 12

Notes:  
 For details of the floorplans, please see the floorplans in the Architecture section of this document.

# PARCEL 12

## OPEN SPACE SUMMARY

### TOTAL USABLE GROUP OPEN SPACE

REQUIRED 170 SF PER UNIT (74 UNITS) = 12,580 SF  
PROVIDED = 20,460 SF



**12**  
**TOWNHOMES**  
2.3 ACRES  
16.5 DU/AC  
38 UNITS

# OAK KNOLL

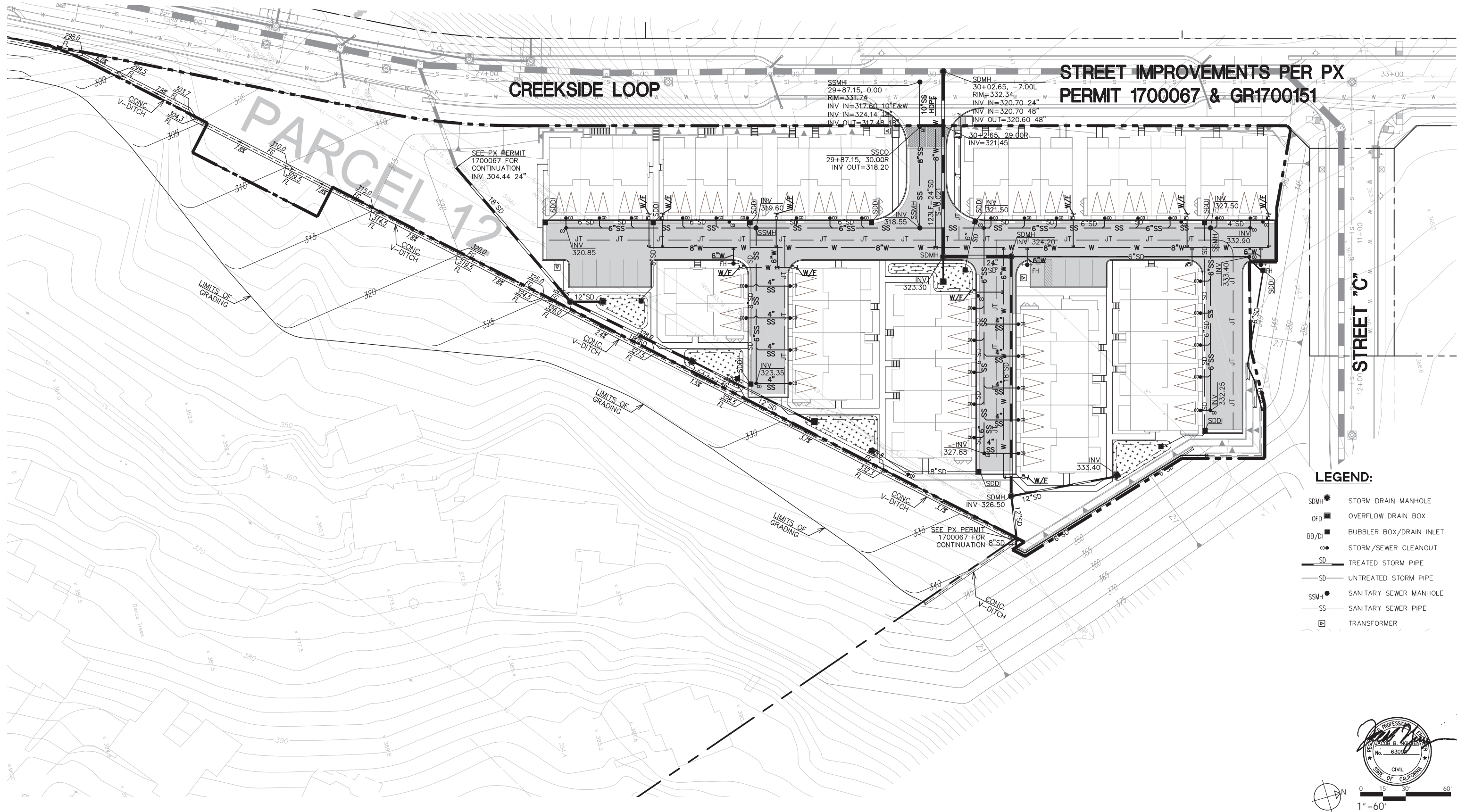
## OPEN SPACE SUMMARY

FINAL DEVELOPMENT PLAN - PARCEL 12









**STREET IMPROVEMENTS PER PX PERMIT 1700067 & GR1700151**

**CREEKSIDE LOOP**

**PARCEL 12**

**LEGEND:**

- SDMH ● STORM DRAIN MANHOLE
- OFD ■ OVERFLOW DRAIN BOX
- BB/DI ■ BUBBLER BOX/DRAIN INLET
- ∞ ● STORM/SEWER CLEANOUT
- SD — TREATED STORM PIPE
- SD — UNTREATED STORM PIPE
- SSMH ● SANITARY SEWER MANHOLE
- SS — SANITARY SEWER PIPE
- ⊠ TRANSFORMER

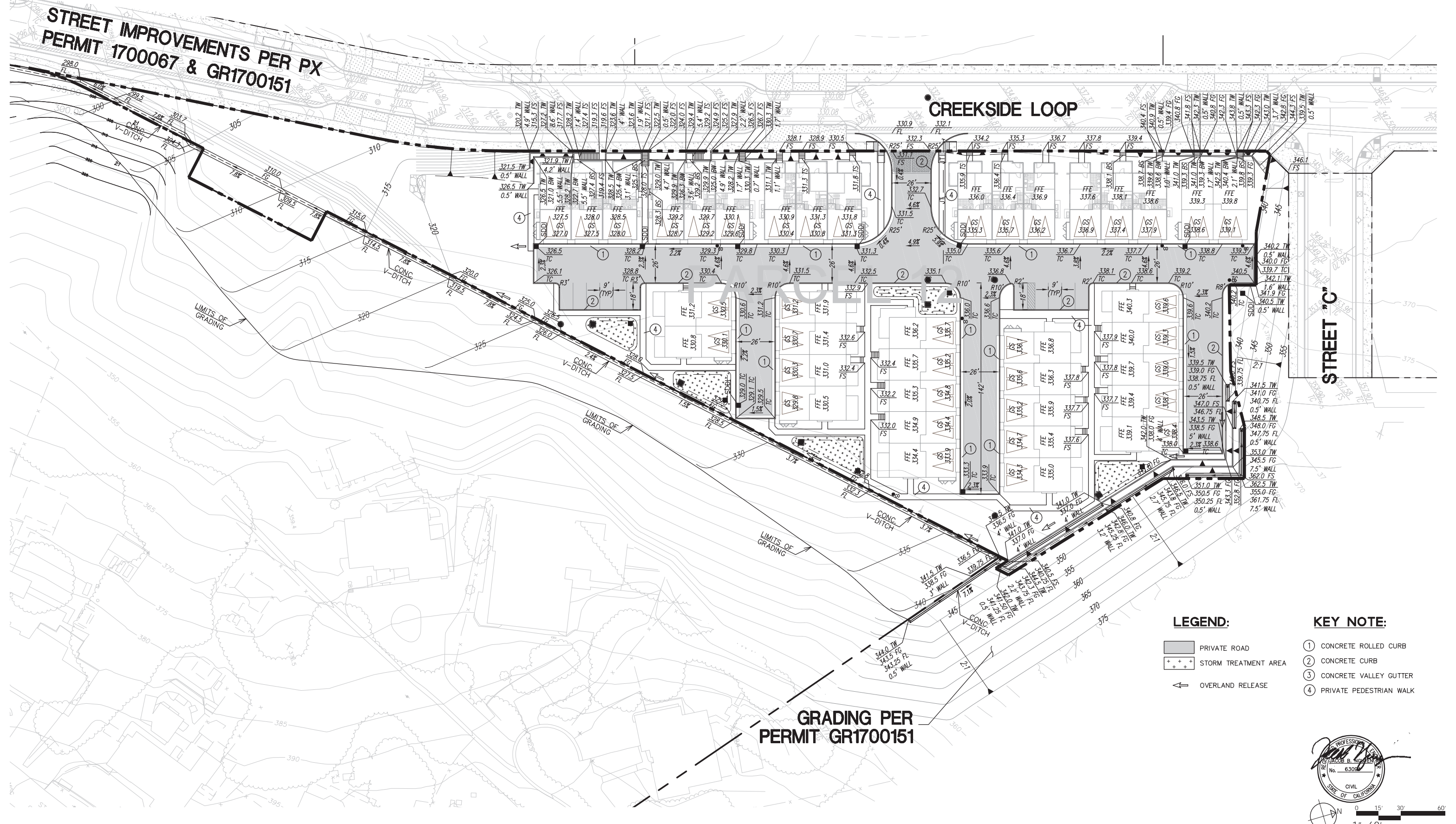
**OAK KNOLL**  
UTILITY PLAN

FINAL DEVELOPMENT PLAN - PARCEL 12





**STREET IMPROVEMENTS PER PX  
PERMIT 1700067 & GR1700151**



**CREEKSIDE LOOP**

**STREET "C"**

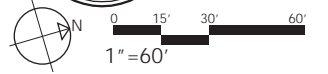
**GRADING PER  
PERMIT GR1700151**

**LEGEND:**

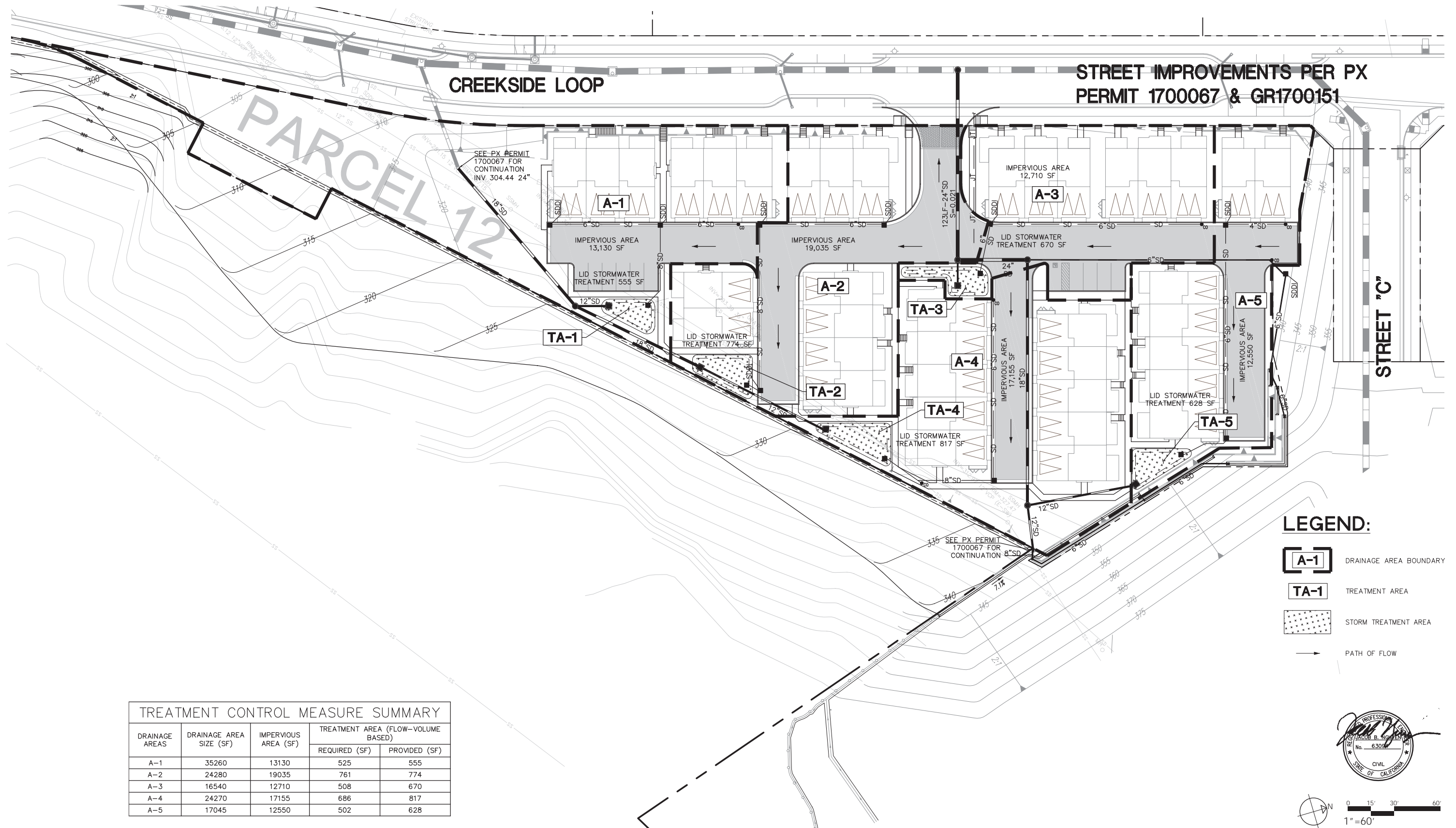
- PRIVATE ROAD
- STORM TREATMENT AREA
- OVERLAND RELEASE

**KEY NOTE:**

- ① CONCRETE ROLLED CURB
- ② CONCRETE CURB
- ③ CONCRETE VALLEY GUTTER
- ④ PRIVATE PEDESTRIAN WALK



**OAK KNOLL**  
GRADING & DRAINAGE PLAN  
FINAL DEVELOPMENT PLAN - PARCEL 12



**STREET IMPROVEMENTS PER PX PERMIT 1700067 & GR1700151**

**CREEKSIDE LOOP**

**STREET "C"**

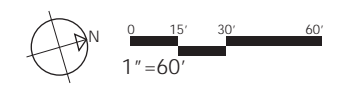
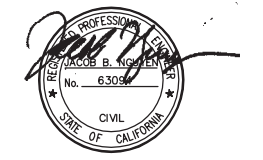
**PARCEL 12**

SEE PX PERMIT 1700067 FOR CONTINUATION INV. 304.44 24"

SEE PX PERMIT 1700067 FOR CONTINUATION 8"SD

TREATMENT CONTROL MEASURE SUMMARY				
DRAINAGE AREAS	DRAINAGE AREA SIZE (SF)	IMPERVIOUS AREA (SF)	TREATMENT AREA (FLOW-VOLUME BASED)	
			REQUIRED (SF)	PROVIDED (SF)
A-1	35260	13130	525	555
A-2	24280	19035	761	774
A-3	16540	12710	508	670
A-4	24270	17155	686	817
A-5	17045	12550	502	628

- LEGEND:**
- A-1 DRAINAGE AREA BOUNDARY
  - TA-1 TREATMENT AREA
  - STORM TREATMENT AREA
  - PATH OF FLOW



# OAK KNOLL

## STORMWATER TREATMENT PLAN

### FINAL DEVELOPMENT PLAN - PARCEL 12







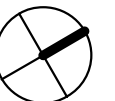
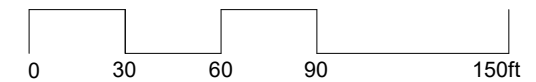
**PARCEL 12**

SHORT-TERM BICYCLE PARKING SUMMARY

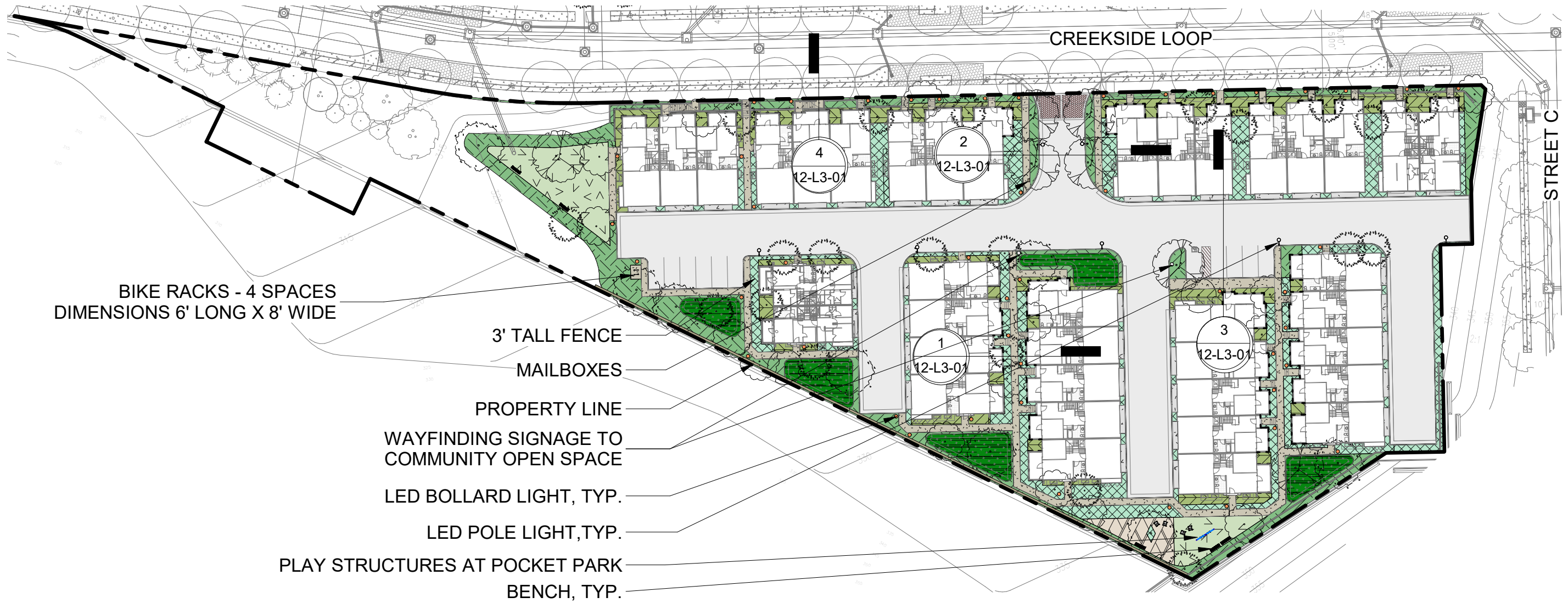
1 PER 20 MULTIFAMILY UNITS REQUIRED (38 UNITS)

REQUIRED:  
38/20 = 1.9 SPACES

PROVIDED:  
4 SPACES







BIKE RACKS - 4 SPACES  
DIMENSIONS 6' LONG X 8' WIDE

3' TALL FENCE

MAILBOXES

PROPERTY LINE

WAYFINDING SIGNAGE TO  
COMMUNITY OPEN SPACE

LED BOLLARD LIGHT, TYP.

LED POLE LIGHT, TYP.

PLAY STRUCTURES AT POCKET PARK

BENCH, TYP.

**HARDSCAPE LEGEND**

- PROPERTY LINE
- ▭ ENTRY DRIVEWAY, SEE CIVIL PLAN
- ▭ PRIVATE DRIVEWAY
- ▭ PEDESTRIAN PAVING, SEE LANDSCAPE MATERIALS
- LED BOLLARD LIGHT, TYP.
- ⊕ LED POLE LIGHT, TYP.
- - - 3' TALL FENCE, STEEL PICKET
- - - 6' TALL FENCE, WOOD
- BENCH

**PLANTING LEGEND**

- LARGE CANOPY TREE**  
SEE PLANT LIST FOR SPECIES
- ORNAMENTAL TREE**  
SEE PLANT LIST FOR SPECIES
- PRIVACY SCREENING PLANTING**  
HIGH SHRUBS, LOW SHRUBS, GROUNDCOVER & GRASSES
- ORNAMENTAL PLANTING**  
LOW SHRUBS, GROUNDCOVER & GRASSES
- STORMWATER TREATMENT PLANTING**  
SEE PLANT LIST FOR SPECIES
- NO MOW TURF**  
SEE PLANT LIST FOR SPECIES, HYDROSEED ON SLOPES  
GREATER THAN 3:1
- RECREATIONAL TURF**

**IRRIGATION DESIGN INTENT & PERFORMANCE STANDARDS**

1. ALL SHRUBS AND GROUNDCOVER AREAS (NON-TURF AREAS) TO BE IRRIGATED WITH DRIP IRRIGATION. ALL TURF AREAS IRRIGATED WITH HIGH EFFICIENCY SPRINKLERS.
2. LANDSCAPING TO BE DESIGNED TO BE IRRIGATED AT NO MORE THAN 55% OF THE REFERENCE EVAPOTRANSPIRATION FOR THE IRRIGATED AREA.
3. NO ORNAMENTAL TURF HAS BEEN SPECIFIED. ALL TURF IS FOR RECREATIONAL USE AND WILL NOT COVER MORE THAN 25% OF TOTAL IRRIGATED AREA.
4. TURF IS NOT ALLOWED IN AREAS LESS THAN 10' WIDE.
5. AUTOMATIC, SELF-ADJUSTING IRRIGATION CONTROLLERS ARE TO BE SPECIFIED ON ALL IRRIGATION SYSTEMS AND WILL AUTOMATICALLY ACTIVATE AND DEACTIVATE THE IRRIGATION SYSTEM BASED ON CHANGES IN THE WEATHER. ALL AUTOMATIC IRRIGATION SYSTEMS ARE EQUIPPED WITH RAIN SENSORS.
6. OVERHEAD SPRINKLER IRRIGATION FOR TURF AREAS ONLY. NO SPRINKLERS OR SPRAY HEADS IN AREAS LESS THAN 10' WIDE. LANDSCAPE DESIGN BEST PRACTICES WILL INCLUDE DISTRIBUTION UNIFORMITY, HEAD TO HEAD SPACING AND SETBACKS FROM WALKWAYS AND PAVEMENT.
7. HOMEOWNER AND DEVELOPER TO CONFORM TO EBMUD SECTION 31 WATER EFFICIENCY REQUIREMENTS FOR LANDSCAPE. PLANS PROVIDED INCLUDE SUGGESTED PLANT PALETTE, AND IRRIGATION DESIGN/BUILD SPECIFICATION TO CONFORM TO SECTION 31. HOMEOWNER TO REFER TO EBMUD BOOK "PLANTS AND LANDSCAPES FOR SUMMER-DRY CLIMATES OF THE SAN FRANCISCO BAY REGION" FOR FURTHER INFORMATION AND PLANT SELECTION. WWW.STOPWASTE.ORG WEB SITE PROVIDES ADDITIONAL INFORMATION REGARDING BAY FRIENDLY PLANTS AND PRACTICES FOR LANDSCAPING.
8. VALVES AND CIRCUITS TO BE SEPARATED (INDIVIDUAL HYDROZONES) BASED ON PLANT MATERIAL AND WATER USE.
9. STATIC PRESSURE AT POINT OF CONNECTION TO BE 60 PSI OR HIGHER. IRRIGATION DEMAND NOT TO EXCEED 20 GPM AT 60 PSI STATIC PRESSURE.
10. PROVIDE AUTOMATIC IRRIGATION SYSTEM THAT PROVIDES 100% UNIFORM COVERAGE AND MEETS CURRENT WATER EFFICIENCY STANDARDS FOR LANDSCAPE AREAS.
11. IRRIGATION BACKFLOW PREVENTION DEVICE TO BE LOCATED CLOSE TO STRUCTURE AWAY FROM EDGE OF ROAD OR PAVEMENT ON A CONCRETE PAD. A POLAR BLANKET AND STEEL CAGING TO BE PROVIDED FOR EACH BACKFLOW PREVENTER.
12. WATER USE ACCORDING TO "WUCOLS: WATER USE CLASSIFICATION OF LANDSCAPE SPECIES"

**PLANTING DESIGN INTENT & NOTES**

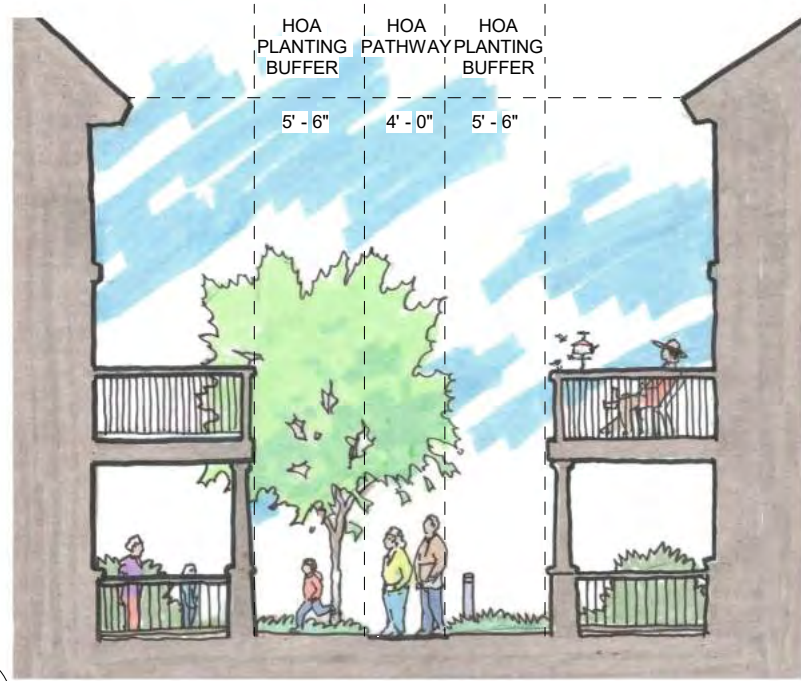
1. ALL LANDSCAPE AREAS ARE TO BE MAINTAINED BY HOME OWNERS' ASSOCIATION. PRIVATE PATIOS AND PRIVATE YARDS WILL BE MAINTAINED BY INDIVIDUAL HOME OWNERS.
2. PLANT LISTS ARE SUGGESTED PALETTE, PLANTS MAY BE SUBSTITUTED AT OWNER'S DISCRETION SO LONG AS THEY ARE CLIMATE ADAPTED, AND MEET WATER REQUIREMENTS.
3. PLANT ALL TREES A MINIMUM OF 5 FEET AWAY FROM ANY UNDERGROUND UTILITIES, A MINIMUM OF 15 FEET FROM A LIGHT POLE, AND A MINIMUM OF 30 FEET FROM THE FACE OF A TRAFFIC SIGNAL, OR AS OTHERWISE SPECIFIED BY THE CITY.
4. PROVIDE ROOT BARRIER FOR ALL TREES LOCATED WITHIN 7 FEET OF PAVED EDGES OR STRUCTURE. ROOT BARRIER IS 24 INCH DEEP BY APPROXIMATELY 6 FT LONG PANEL BARRIER, DEEP ROOT SM 24, AVAILABLE FROM VILLA LANDSCAPE PRODUCTS, INC. (714) 630-3181; ROOT SOLUTIONS (800)554-0914 OR APPROVED EQUIVALENT. INSTALL 12' LENGTH ALONG EDGE OF PAVEMENT CENTERED ON EACH TREE.
5. ALL SHRUBS, GROUNDCOVERS, TREES AND VINES SELECTED FOR PLANTING ARE CLIMATE ADAPTED AND DROUGHT TOLERANT. ALL SHRUBS AND GROUNDCOVER AREAS (NON-TURF AREAS) TO BE IRRIGATED WITH DRIP IRRIGATION. ALL TURF AREAS IRRIGATED WITH HIGH-EFFICIENCY SPRAY.
6. NON-TURF AREAS: AT LEAST 80% OF PLANTS SELECTED ARE CLIMATE APPROPRIATE LOW WATER USE SPECIES AND REQUIRE MINIMAL WATER ONCE ESTABLISHED. UP TO 20% OF THE PLANTS MAY BE NON-DROUGHT TOLERANT VARIETY AS LONG AS THEY ARE APPROPRIATELY GROUPED TOGETHER AND IRRIGATED SEPARATELY AND EFFICIENTLY.
7. WATER USE ACCORDING TO "WUCOLS: WATER USE CLASSIFICATION OF LANDSCAPE SPECIES"



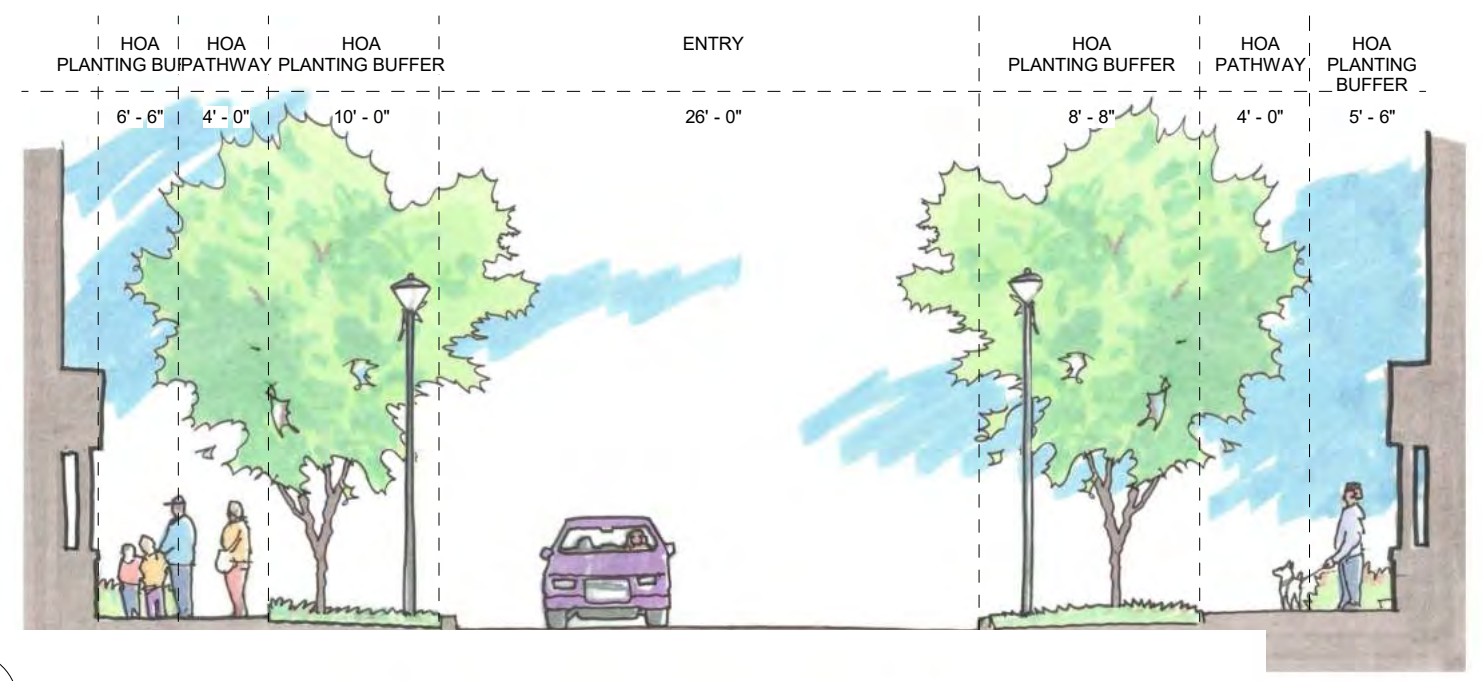
**OAK KNOLL**  
LANDSCAPE CONCEPT  
FINAL DEVELOPMENT PLAN - PARCEL 12





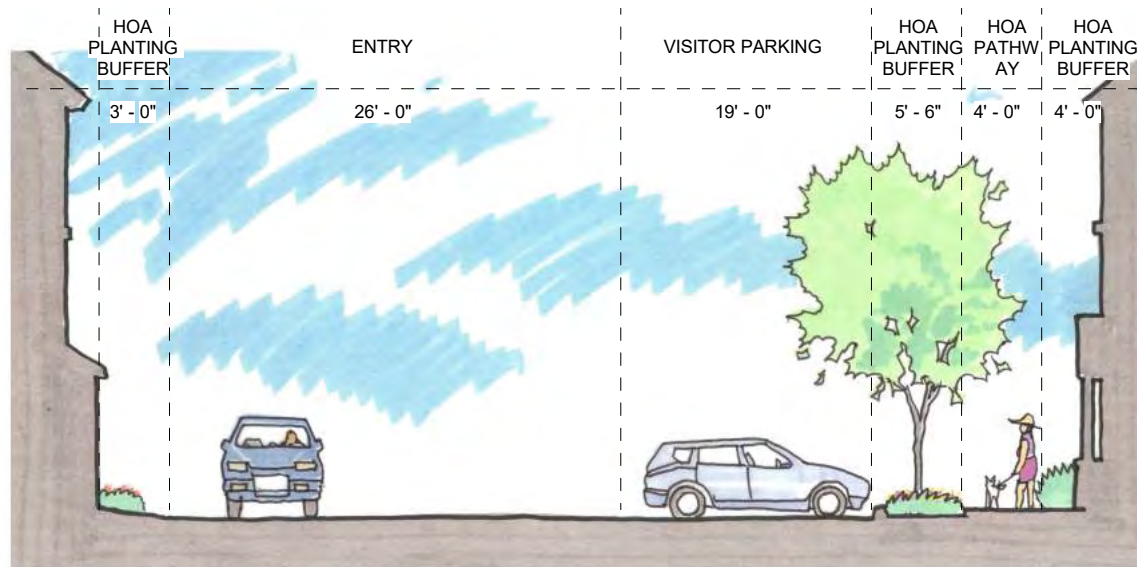


1



2

SEE PARCEL 12 LANDSCAPE CONCEPT FOR SECTION LOCATIONS











3



4

12D



TREE LIST				
SYMBOL	BOTANICAL NAME	COMMON NAME	CONTAINER SIZE	WATER USE
TREE				
	AESCLUSUS CALIFORNICA	CALIFORNIA BUCKEYE	24" BOX	VERY LOW
	ALBIZIA JULIBRISSIN	SILK TREE	24" BOX	LOW
	ARBUTUS UNEDO MULTI STEM	COMPACT STRAWBERRY TREE	24" BOX	LOW
	CEANOTHUS 'RAY HARTMAN'	RAY HARTMAN WILD LILAC	15 GAL	LOW
	JACARANDA MIMOSIFOLIA	JACARANDA	24" BOX	MODERATE
	LAGERSTROEMIA INDICA	GRAPE MYRTLE	24" BOX	LOW
	PLATANUS X ACERIFOLIA 'COLUMBIA'	LONDON PLANE TREE	24" BOX	MODERATE
	QUERCUS AGRIFOLIA	COAST LIVE OAK	24" BOX	VERY LOW

SHRUBS, GROUNDCOVERS & GRASSES				
Type	COMMON NAME	CONTAINER SIZE	SPACING	WATER USE
GRASS				
CAREX DIVULSA	BERKELEY SEDGE	1 GAL	2'-6"	LOW
FESTUCA 'SISKIYOU BLUE'	SISKIYOU BLUE FESCUE	1 GAL	18"	MODERATE
FESTUCA CALIFORNICA	CALIFORNIA FESCUE	1 GAL	2'-6"	LOW
FESTUCA RUBRA 'PT. MOLATE'	MOLATE FESCUE	1 GAL	1'-6"	LOW
JUNCUS PATENS 'ELK BLUE'	ELK BLUE CALIFORNIA GRAY RUSH	1 GAL	2'-0"	LOW
MUHLENBERGIA RIGENS	DEERGRASS	1 GAL	3'-0"	LOW
SESLERIA AUTUMNALIS	AUTUMN MOOR GRASS	1 GAL	1'-0"	MODERATE
GROUNDCOVER				
ACHILLEA MILLEFOLIUM	YARROW	1 GAL	1'-6"	LOW
ARCTOSTAPHYLOS 'PACIFIC MIST'	PACIFIC MIST MANZANITA	15 GAL	8'-0"	LOW
ARCTOTIS STOECHADIFOLIA	AFRICAN DAISY	1 GAL	1'-6"	LOW
BERBERIS REPENS	CREEPING BARBERRY	5 GAL	1'-6"	LOW
CISTUS CORBARIENSIS	ROCKROSE	5 GAL	6'-0"	LOW
ERIGERON GLAUCUS	SEASIDE DAISY	5 GAL	2'-0"	LOW
MYOPORUM PARVIFOLIUM 'PUTAH CREEK'	CREEPING MYOPORUM	1 GAL	1'-0"	LOW
ROSMARINUS 'HUNTINGTON CARPET'	HUNTINGTON CARPET ROSEMARY	5 GAL	8'-0"	LOW
SALVIA SPATHACEA	HUMMINGBIRD SAGE	1 GAL	4'-0"	LOW
SENECIO MANDRALISCAE	BLUE CHALKSTICKS	5 GAL	2'-0"	LOW
STACHYS BYZANTINA 'SILVER CARPET'	LAMB'S EARS	1 GAL	3'-0"	LOW
ZAUSCHNERIA CALIFORNICA 'ROUTE 66'	ROUTE 66 CALIFORNIA FUCHSIA	1 GAL	3'-0"	LOW
HIGH SHRUB				
ARCTOSTAPHYLOS DENSIFLORA 'HOWARD MCMINN'	HOWARD MCMINN MANZANITA	24" BOX	5'-0"	LOW
ARCTOSTAPHYLOS DENSIFLORA 'LUTSKO'S PINK'	MANZANITA	1 GAL	6'-0"	LOW
CARPENTERIA CALIFORNICA 'ELIZABETH'	BUSH ANEMONE	1 GAL	4'-0"	MODERATE
CEANOTHUS 'CONCHA'	CALIFORNIA LILAC	1 GAL	9'-0"	LOW
CEANOTHUS 'FROSTY BLUE'	CALIFORNIA LILAC	15 GAL	10'-0"	LOW
CEANOTHUS GLORIOSUS VAR. EXALTATUS 'EMILY BROWN'	NAVARRO CEANOTHUS	1 GAL	8'-0"	LOW
HETEROMELES ARBUTIFOLIA	TOYON	15 GAL	6'-0"	LOW
OLEA EUROPAEA 'MONTRA'	LITTLE OLIVE	15 GAL	4'-0"	VERY LOW
PHORMIUM 'BRONZE BABY'	NEW ZEALAND FLAX	5 GAL	3'-0"	LOW
PHORMIUM 'DARK DELIGHT'	NEW ZEALAND FLAX	5 GAL	4'-0"	LOW
RIBES SANGUINEUM 'CLAREMONT'	FLOWERING CURRANT	5 GAL	6'-0"	LOW
RIBES VIBURNIFOLIUM	CATALINA PERFUME	1 GAL	5'-0"	LOW
ROSA CALIFORNICA	CALIFORNIA WILD ROSE	5 GAL	3'-0"	LOW
SALVIA LEUCANTHA	MEXICAN BUSH SAGE	5 GAL	5'-0"	LOW
SENECIO LEUCOSTACHYS	WHITE GROUNDSEL	5 GAL	4'-0"	LOW
WESTRINGIA FRUTICOSA 'MORNING LIGHT'	COAST ROSEMARY	5 GAL	3'-0"	LOW
LOW SHRUB				
ANIGOZANTHOS 'BUSH LANTERN'	DWARF YELLOW KANGAROO PAW	1 GAL	2'-0"	LOW
ANIGOZANTHOS 'HARMONY'	KANGAROO PAW	5 GAL	2'-6"	LOW
ASCLEPIAS FASCICULARIS	NARROWLEAF MILKWEED	1 GAL	3'-0"	LOW
ASCLEPIAS SPECIOSA 'DAVIS'	SHOWY MILKWEED	1 GAL	3'-0"	LOW
ERYSIMUM LINIFOLIUM 'BOWLES' MAUVE'	WALLFLOWER	1 GAL	1'-6"	LOW
GALVEZIA SPECIOSA 'FIRECRACKER'	FIRECRACKER ISLAND BUSH SNAPDRAGON	1 GAL	4'-0"	LOW
IRIS DOUGLASIANA 'CANYON SNOW'	PACIFIC COAST HYBRID IRIS	1 GAL	1'-6"	LOW
LAVANDULA ANGUSTIFOLIA 'HIDCOTE BLUE'	HIDCOTE BLUE ENGLISH LAVENDER	5 GAL	3'-0"	LOW
PHORMIUM 'CREAM DELIGHT'	NEW ZEALAND FLAX	5 GAL	2'-0"	LOW
PHORMIUM 'JACK SPRATT'	NEW ZEALAND FLAX	5 GAL	1' 0"	LOW
POLYSTICHUM MUNITUM	WESTERN SWORD FERN	1 GAL	3'-0"	MODERATE
RHAMNUS CALIFORNICA 'MOUND SAN BRUNO'	COFFEEBERRY	24" BOX	6'-0"	LOW
ROSMARINUS OFFICINALIS 'COLLINGWOOD INGRAM'	DWARF ROSEMARY	1 GAL	4'-0"	LOW
SALVIA MICROPHYLLA 'BERZERKELEY'	BERZERKELEY SALVIA	1 GAL	2'-0"	LOW
TEUCRIUM CHAMAEDRYIS	WALL GERMANDER	1 GAL	2'-0"	LOW
TREE				
AESCLUSUS CALIFORNICA	CALIFORNIA BUCKEYE	24" BOX	25'-0"	VERY LOW
ARBUTUS UNEDO MULTI STEM	COMPACT STRAWBERRY TREE	24" BOX	8'-0"	LOW
CEANOTHUS 'RAY HARTMAN'	RAY HARTMAN WILD LILAC	15 GAL	10'-0"	LOW
JACARANDA MIMOSIFOLIA	JACARANDA	24" BOX	30'-0"	MODERATE
LAGERSTROEMIA INDICA	GRAPE MYRTLE	24" BOX	20'-0"	LOW
PLATANUS X ACERIFOLIA 'COLUMBIA'	LONDON PLANE TREE	24" BOX	30'-0"	MODERATE
VINE				
SOLANUM JASMINOIDES	POTATO VINE	1 GAL	15'-0"	MODERATE
VITIS 'ROGER'S RED'	ROGER'S CALIFORNIA GRAPE	5 GAL	15'-0"	LOW

# OAK KNOLL

## PLANT LIST

FINAL DEVELOPMENT PLAN - PARCEL 12

**IRRIGATION DESIGN INTENT & PERFORMANCE STANDARDS**

1. ALL SHRUBS, GROUNDCOVERS, TREES AND VINES SELECTED FOR PLANTING ARE CLIMATE ADAPTED AND DROUGHT TOLERANT. ALL SHRUBS AND GROUNDCOVER AREAS (NON-TURF AREAS) TO BE IRRIGATED WITH DRIP IRRIGATION. ALL TURF AREAS IRRIGATED WITH HIGH EFFICIENCY SPRINKLERS.
2. LANDSCAPING TO BE DESIGNED TO BE IRRIGATED AT NO MORE THAN 70% OF THE REFERENCE EVAPOTRANSPIRATION FOR THE IRRIGATED AREA.
3. NO ORNAMENTAL TURF HAS BEEN SPECIFIED. ALL TURF IS FOR RECREATIONAL USE AND WILL NOT COVER MORE THAN 25% OF TOTAL IRRIGATED AREA.
4. TURF IS NOT ALLOWED IN AREAS LESS THAN 10' WIDE.
5. AUTOMATIC, SELF-ADJUSTING IRRIGATION CONTROLLERS ARE TO BE SPECIFIED ON ALL IRRIGATION SYSTEMS AND WILL AUTOMATICALLY ACTIVATE AND DEACTIVATE THE IRRIGATION SYSTEM BASED ON CHANGES IN THE WEATHER. ALL AUTOMATIC IRRIGATION SYSTEMS ARE EQUIPPED WITH RAIN SENSORS.
6. OVERHEAD SPRINKLER IRRIGATION FOR TURF AREAS ONLY, NO SPRINKLERS OR SPRAY HEADS IN AREAS LESS THAN 10' WIDE. LANDSCAPE DESIGN BEST PRACTICES WILL INCLUDE DISTRIBUTION UNIFORMITY, HEAD TO HEAD SPACING AND SETBACKS FROM WALKWAYS AND PAVEMENT.
7. HOMEOWNER AND DEVELOPER TO CONFORM TO EBMUD SECTION 31 WATER EFFICIENCY REQUIREMENTS FOR LANDSCAPE. PLANS PROVIDED INCLUDE SUGGESTED PLANT PALETTE, AND IRRIGATION DESIGN/BUILD SPECIFICATION TO CONFORM TO SECTION 31. HOMEOWNER TO REFER TO EBMUD BOOK "PLANTS AND LANDSCAPES FOR SUMMER-DRY CLIMATES OF THE SAN FRANCISCO BAY REGION" FOR FURTHER INFORMATION AND PLANT SELECTION. WWW.STOPWASTE.ORG WEB SITE PROVIDES ADDITIONAL INFORMATION REGARDING BAY FRIENDLY PLANTS AND PRACTICES FOR LANDSCAPING.
8. VALVES AND CIRCUITS TO BE SEPARATED (INDIVIDUAL HYDROZONES) BASED ON PLANT MATERIAL AND WATER USE.
9. STATIC PRESSURE AT POINT OF CONNECTION TO BE 60 PSI OR HIGHER. IRRIGATION DEMAND NOT TO EXCEED 20 GPM AT 60 PSI STATIC PRESSURE.
10. PROVIDE AUTOMATIC IRRIGATION SYSTEM THAT PROVIDES 100% UNIFORM COVERAGE AND MEETS CURRENT WATER EFFICIENCY STANDARDS FOR LANDSCAPE AREAS.
11. IRRIGATION BACKFLOW PREVENTION DEVICE TO BE LOCATED CLOSE TO STRUCTURE AWAY FROM EDGE OF ROAD OR PAVEMENT ON A CONCRETE PAD. A POLAR BLANKET AND STEEL CAGING TO BE PROVIDED FOR EACH BACKFLOW PREVENTER.
12. WATER USE ACCORDING TO "WUCOLS: WATER USE CLASSIFICATION OF LANDSCAPE SPECIES"

**PLANTING DESIGN INTENT & NOTES**

1. ALL LANDSCAPE AREAS ARE TO BE MAINTAINED BY HOME OWNERS' ASSOCIATION. PRIVATE PATIOS AND PRIVATE YARDS WILL BE MAINTAINED BY INDIVIDUAL HOME OWNERS.
2. PLANT LISTS ARE SUGGESTED PALETTE, PLANTS MAY BE SUBSTITUTED AT OWNER'S DISCRETION SO LONG AS THEY ARE CLIMATE ADAPTED, AND MEET WATER REQUIREMENTS.
3. PLANT ALL TREES A MINIMUM OF 5 FEET AWAY FROM ANY UNDERGROUND UTILITIES, A MINIMUM OF 15 FEET FROM A LIGHT POLE, AND A MINIMUM OF 30 FEET FROM THE FACE OF A TRAFFIC SIGNAL, OR AS OTHERWISE SPECIFIED BY THE CITY.
4. PROVIDE ROOT BARRIER FOR ALL TREES LOCATED WITHIN 7 FEET OF PAVED EDGES OR STRUCTURE. ROOT BARRIER IS 24 INCH DEEP BY APPROXIMATELY 6 FT LONG PANEL BARRIER, DEEP ROOT SM 24, AVAILABLE FROM VILLA LANDSCAPE PRODUCTS, INC. (714) 630-3181; ROOT SOLUTIONS (800)554-0914 OR APPROVED EQUIVALENT. INSTALL 12' LENGTH ALONG EDGE OF PAVEMENT CENTERED ON EACH TREE.
5. ALL SHRUBS, GROUNDCOVERS, TREES AND VINES SELECTED FOR PLANTING ARE CLIMATE ADAPTED AND DROUGHT TOLERANT. ALL SHRUBS AND GROUNDCOVER AREAS (NON-TURF AREAS) TO BE IRRIGATED WITH DRIP IRRIGATION. ALL TURF AREAS IRRIGATED WITH HIGH-EFFICIENCY SPRAY.
6. NON-TURF AREAS: AT LEAST 80% OF PLANTS SELECTED ARE CLIMATE APPROPRIATE LOW WATER USE SPECIES AND REQUIRE MINIMAL WATER ONCE ESTABLISHED. UP TO 20% OF THE PLANTS MAY BE NON-DROUGHT TOLERANT VARIETY AS LONG AS THEY ARE APPROPRIATELY GROUPED TOGETHER AND IRRIGATED SEPARATELY AND EFFICIENTLY.
7. WATER USE ACCORDING TO "WUCOLS: WATER USE CLASSIFICATION OF LANDSCAPE SPECIES"

STORMWATER TREATMENT PLANTING				
BOTANICAL NAME	COMMON NAME	CONTAINER SIZE	SPACING	WATER USE
GRASS				
CAREX DIVULSA	BERKELEY SEDGE	1 GAL	2'-6"	LOW
FESTUCA CALIFORNICA	CALIFORNIA FESCUE	1 GAL	2'-6"	LOW
JUNCUS PATENS 'ELK BLUE'	ELK BLUE CALIFORNIA GRAY RUSH	1 GAL	2'-0"	LOW
MUHLENBERGIA RIGENS	DEERGRASS	1 GAL	3'-0"	LOW
CHONDROPETALUM TECTORUM	CAPE RUSH	5 GAL	4'-0"	LOW
GROUNDCOVER				
ERIGERON GLAUCUS	SEASIDE DAISY	5 GAL	2'-0"	LOW
ACHILLEA MILLEFOLIUM	YARROW	1 GAL	1'-6"	LOW
BERBERIS REPENS	CREEPING BARBERRY	5 GAL	1'-6"	LOW
SALVIA SPATHACEA	HUMMINGBIRD SAGE	1 GAL	4'-0"	LOW
HIGH SHRUB				
RIBES SANGUINEUM 'CLAREMONT'	FLOWERING CURRANT	5 GAL	6'-0"	LOW
ROSA CALIFORNICA	CALIFORNIA WILD ROSE	5 GAL	3'-0"	LOW
LOW SHRUB				
ASCLEPIAS FASCICULARIS	NARROWLEAF MILKWEED	1 GAL	3'-0"	LOW
ASCLEPIAS SPECIOSA 'DAVIS'	SHOWY MILKWEED	1 GAL	3'-0"	LOW
GRINDELIA HIRSUTULA	HAIRY GUMPLANT	1 GAL	3'-0"	LOW
IRIS DOUGLASIANA 'CANYON SNOW'	PACIFIC COAST HYBRID IRIS	1 GAL	1'-6"	LOW

NO-MOW TURF PLANTING			
BOTANICAL NAME	COMMON NAME	CONTAINER SIZE	WATER USE
GRASS			
FESTUCA RUBRA	MOLATE FESCUE	PART OF SOD MIX	LOW
STIPA PULCHRA	PURPLE NEEDLEGRASS	PART OF SOD MIX	LOW
STIPA CERNUA	NODDING NEEDLEGRASS	PART OF SOD MIX	LOW
KOELERIA MACRANTHA	PRARIE JUNEGRASS	PART OF SOD MIX	LOW



trees



*Albizia julibrissin* | Silk Tree



*Ceanothus 'Ray Hartman'* | Ray Hartman Wild Lilac



*Platanus 'Columbia'* | London Plane Tree



*Aesculus californica* | California Buckeye



*Jacaranda mimosifolia* | Jacaranda



*Quercus agrifolia* | Coast Live Oak



*Arbutus undedo* | Strawberry Tree



*Lagerstoemia indica* | Crape Myrtle



grasses



*Carex divulsa* | Berkeley Sedge



*Juncus 'Elk Blue'* | Elk Blue Juncus



*Festuca 'Siskiyou Blue'* | Siskiyou Blue Fescue



*Muhlenbergia rigens* | Deer Grass



*Festuca californica* | California Fescue



*Festuca rubra 'Pt Molate'* | Molate Fescue



*Sesleria autumnalis* | Autumn Moor Grass

groundcover



*Achillea millefolium* | Yarrow



*Berberis repens* | Creeping Barberry



*Senecio mandraliscae* | Blue Chalksticks



*Arctostaphylos 'Pacific Mist'* | Pacific Mist Manzanita



*Cistus corbariensis* | Rockrose



*Salvia spathacea* | Hummingbird Sage



*Arctotis stoechadifolia* | African Daisy



*Erigeron glaucus* | Seaside Daisy



*Myoporum parvifolium* | Creeping Myoporum



shrubs



*Anigozanthos, sp.* | Kangaroo Paw



*Ceanothus, sp.* | Wild Lilac



*Olea 'Little Ollie'* | Little Ollie Olive



*Salvia leucantha* | Mexican Bush Sage



*Arctostaphylos 'Howard McMinn'* | Howard McMinn Manzanita



*Aesclepias, sp.* | Milkweed



*Lavandula angustifolia 'Hidcote Blue'* | English Lavender



*Phormium, sp.* | New Zealand Flax



*Teucrium chamaedrys* | Wall Germander



*Carpenteria californica* | Bush Anemone



*Galvezia speciosa 'Firecracker'* | Island Bush Snapdragon



*Iris douglasiana 'Island Snow'* | Pacific Coast Iris



*Ribes sanguineum* | Red Flowering Currant



*Westringia 'Morning Light'* | Morning Light Coast Rosemary



*Rosmarinus officinalis 'Collingwood Ingram'* | Dwarf Rosemary

OAK KNOLL

PLANT IMAGES

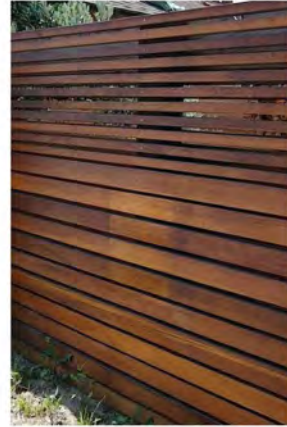
FINAL DEVELOPMENT PLAN - PARCEL 12



fencing



STEEL PICKET FENCE - 3' HEIGHT



WOOD FENCE - HORIZONTAL BOARD, 6' HEIGHT. MATERIALS PER OAK KNOLL DESIGN GUIDELINES



WOOD FENCE - BOARD-ON-BATTEN, 6' HEIGHT. MATERIALS PER OAK KNOLL DESIGN GUIDELINES



COMMUNITY WALL, CONCRETE PANEL, 6' TALL

paving



PRIVATE DRIVEWAY - INTEGRAL COLOR CONCRETE WITH SAWCUT JOINTS



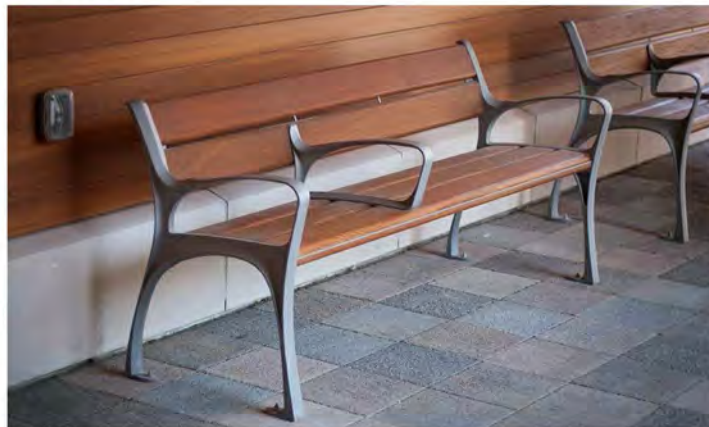
PEDESTRIAN PAVING - INTEGRAL COLOR CONCRETE WITH ROCK SALT FINISH, SAWCUT JOINTS

retaining wall



ANCHOR HIGHLAND STONE RETAINING WALL, SIZES 6X6X12, 6X12X12, 6X18X12, WITH HIGHLAND CAP. COLOR: MONTECITO. AS AVAILABLE FROM BELGARD, WEB SITE: WWW.BELGARD.COM

bench



MAGLIN MLB870-W SERIES BENCH AS AVAILABLE FROM MAGLIN, WEB SITE: WWW.MAGLIN.COM

play structure



FREESTANDING SWINGING AND SPINNING ELEMENTS



entry sign





**UDU-10176**  
Duomo 1 Bollard

7144 NE Progress Ct | T:503.645.0500  
Hillsboro, Oregon 97124 | F:503.645.8100  
www.ligmanlightingusa.com



Diameter - 6.3" | Height - 29.7"/28.9" | Weight 15.8 lbs  
IP55 - Suitable For Wet Locations  
IK04 - Impact Resistant (Vandal Resistant)

**Construction**

**Aluminum**  
Less than 0.1% copper content - Marine Grade 6060 extruded & LM6 Aluminum High Pressure die casting provides excellent mechanical strength, clean detailed product lines and excellent heat dissipation.

**Pre paint**  
8 step degrease and phosphate process that includes deoxidizing and etching as well as a zinc and nickel phosphate process before product painting.

**Memory Retentive - Silicon Gasket**  
Provided with special injection molded "fit for purpose" long life high temperature memory retentive silicon gaskets. Maintains the gaskets exact profile and seal over years of use and compression.

**Thermal management**  
LM6 Aluminum is used for its excellent mechanical strength and thermal dissipation properties in low and high ambient temperatures. The superior thermal heat sink design by Ligman used in conjunction with the driver, controls thermals below critical temperature range to ensure maximum luminous flux output, as well as providing long LED service life and ensuring less than 10% lumen depreciation at 50,000 hours.

**Surge Suppression**  
Standard 10kv surge suppressor provided with all fixtures.

**BVG Rating**  
B0 - U3 - G1

**Finishing**  
All Ligman products go through an extensive finishing process that includes fettling to improve paint adherence.

**Paint**  
UV Stabilized 4.9Mil thick powder coat paint and baked at 200 Deg C. This process ensures that Ligman products can withstand harsh environments. Rated for use in natatoriums.

**Hardware**  
Provided Hardware is Marine grade 316 Stainless steel.

**Anti-Seize Screw Holes**  
Tapped holes are infused with a special anti-seize compound designed to prevent seizure of threaded connections, due to electrolysis from heat, corrosive atmospheres and moisture.

**Opal Borosilicate Glass Lens**  
Provided with opal borosilicate impact resistant glass.

**Optics & LED**  
Precise optic design provides exceptional light control and precise distribution of light. LED CRI > 80

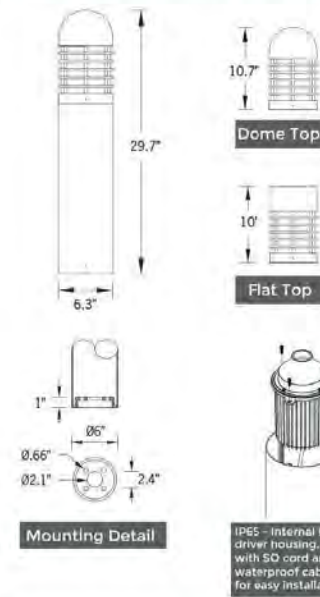
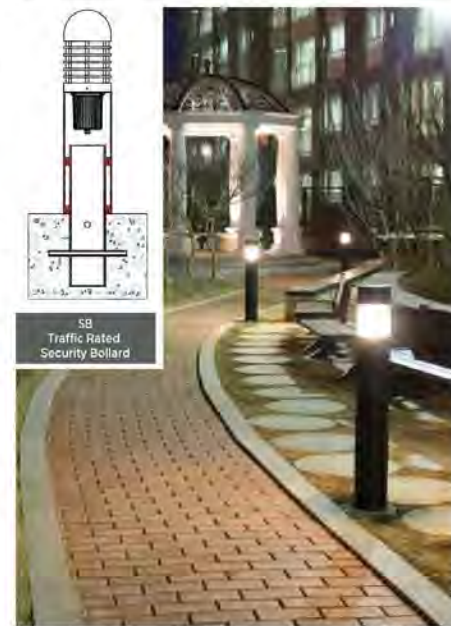
**Lumen - Maintenance Life**  
L80 /B10 at 50,000 hours (This means that at least 90% of the LED still achieve 80% of their original flux)

**Compact, screened bollard fixtures. Residential-scale, providing soft downward and vertical illumination.**

Duomo is a decorative bollard that is suitable for both modern and classic architecture. Ideal for creating visual guidance with exceptional visual comfort. This product was developed to complement the Duomo range of pillar lights, wall sconces and post tops. This sleek shape provides distinctive lighting effects by night and decorative urban effect during the day. Suitable for pedestrian precincts, building surrounds, shopping centers, squares and parks. The luminaire is provided with a opal borosilicate high impact glass lens that providing low glare vertical and horizontal illumination. The Duomo Bollard comes standard with a unique waterproof internal driver housing compartment that is situated at the top of the pole to stop water and dust from entering the electrical components. This fixture is supplied completely wired with powercord and waterproof gland from the driver enclosure to the base of the bollard ensuring quick trouble-free installation. Custom bollard heights are available, please specify. Color temperature 2700K, 3000K and 4000K. Custom wattages can be provided to suit customer and Title 24 requirements. (Specify total watts per fixture)

**Security Bollard:**  
The Duomo Bollard is available as a traffic rated security bollard. This optional design includes a 1/4" wall thickness galvanized steel security pole with 2 solid 1" galvanized steel cross support rods that are embedded into concrete. This security bollard provides restraint of vehicular traffic in unauthorized areas. Impact studies shows this bollard will stop a 5,500lb vehicle, travelling at 30mph. For additional strength, the galvanized pole can be filled with concrete up to the waterproof driver housing to provide a solid concrete barrier.

**Additional Options (Consult Factory For Pricing)**



**Duomo Product Family**



Ligman Lighting USA reserves the right to change specifications without prior notice. Please contact factory for latest information. Due to the continual improvements in LED technology and components may change without notice.

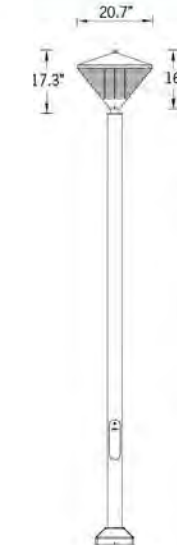
**UQB-20941**  
QBA Post Top

7144 NE Progress Ct | T:503.645.0500  
Hillsboro, Oregon 97124 | F:503.645.8100  
www.ligmanlightingusa.com



Length - 20.7"  
Height - 17.3"  
Weight 39.6 lbs  
IP54  
Suitable for wet locations  
IK07  
Impact Resistant (Vandal Resistant)  
EPA - 1.33

POLE NOT INCLUDED



**Construction**

**Aluminum**  
Less than 0.1% copper content - Marine Grade 6060 extruded & LM6 Aluminum High Pressure die casting provides excellent mechanical strength, clean detailed product lines and excellent heat dissipation.

**Pre paint**  
8 step degrease and phosphate process that includes deoxidizing and etching as well as a zinc and nickel phosphate process before product painting.

**Memory Retentive - Silicon Gasket**  
Provided with special injection molded "fit for purpose" long life high temperature memory retentive silicon gaskets. Maintains the gaskets exact profile and seal over years of use and compression.

**Thermal management**  
LM6 Aluminum is used for its excellent mechanical strength and thermal dissipation properties in low and high ambient temperatures. The superior thermal heat sink design by Ligman used in conjunction with the driver, controls thermals below critical temperature range to ensure maximum luminous flux output, as well as providing long LED service life and ensuring less than 10% lumen depreciation at 50,000 hours.

**Surge Suppression**  
Standard 10kv surge suppressor provided with all fixtures.

**BVG Rating**  
B1 - U3 - G1

**Finishing**  
All Ligman products go through an extensive finishing process that includes fettling to improve paint adherence.

**Paint**  
UV Stabilized 4.9Mil thick powder coat paint and baked at 200 Deg C. This process ensures that Ligman products can withstand harsh environments. Rated for use in natatoriums.

**Hardware**  
Provided Hardware is Marine grade 316 Stainless steel.

**Anti-Seize Screw Holes**  
Tapped holes are infused with a special anti-seize compound designed to prevent seizure of threaded connections, due to electrolysis from heat, corrosive atmospheres and moisture.

**High Impact Acrylic Lens**  
Manufactured with Ultra High Impact, Naturally UV Stabilized Injection Molded Acrylic.

**Optics & LED**  
Precise optic design provides exceptional light control and precise distribution of light. LED CRI > 80

**Lumen - Maintenance Life**  
L80 /B10 at 50,000 hours (This means that at least 90% of the LED still achieve 80% of their original flux)

**Sealed-optic urban post top. Traditional urban realm lighting post top, with external diffuser cover and clean lines**

A modern post top luminaire with excellent downward symmetrical light distribution and visual appeal. The precision optical system gives very low glare rating, while reducing light pollution. Designed for lighting entrances, footpaths and car parks.

Color temperature 2700K, 3000K, 3500K and 4000K, LED CRI >80 and life time 50,000 Hours. Low copper content die-cast aluminium housing with high corrosion resistance. Stainless steel fasteners in grade 316. Durable silicone memory retentive gasket and clear prismatic UV stabilized acrylic lens. Housing is treated with a nickel and zinc phosphate protection before powder coating, ensuring high corrosion resistance.

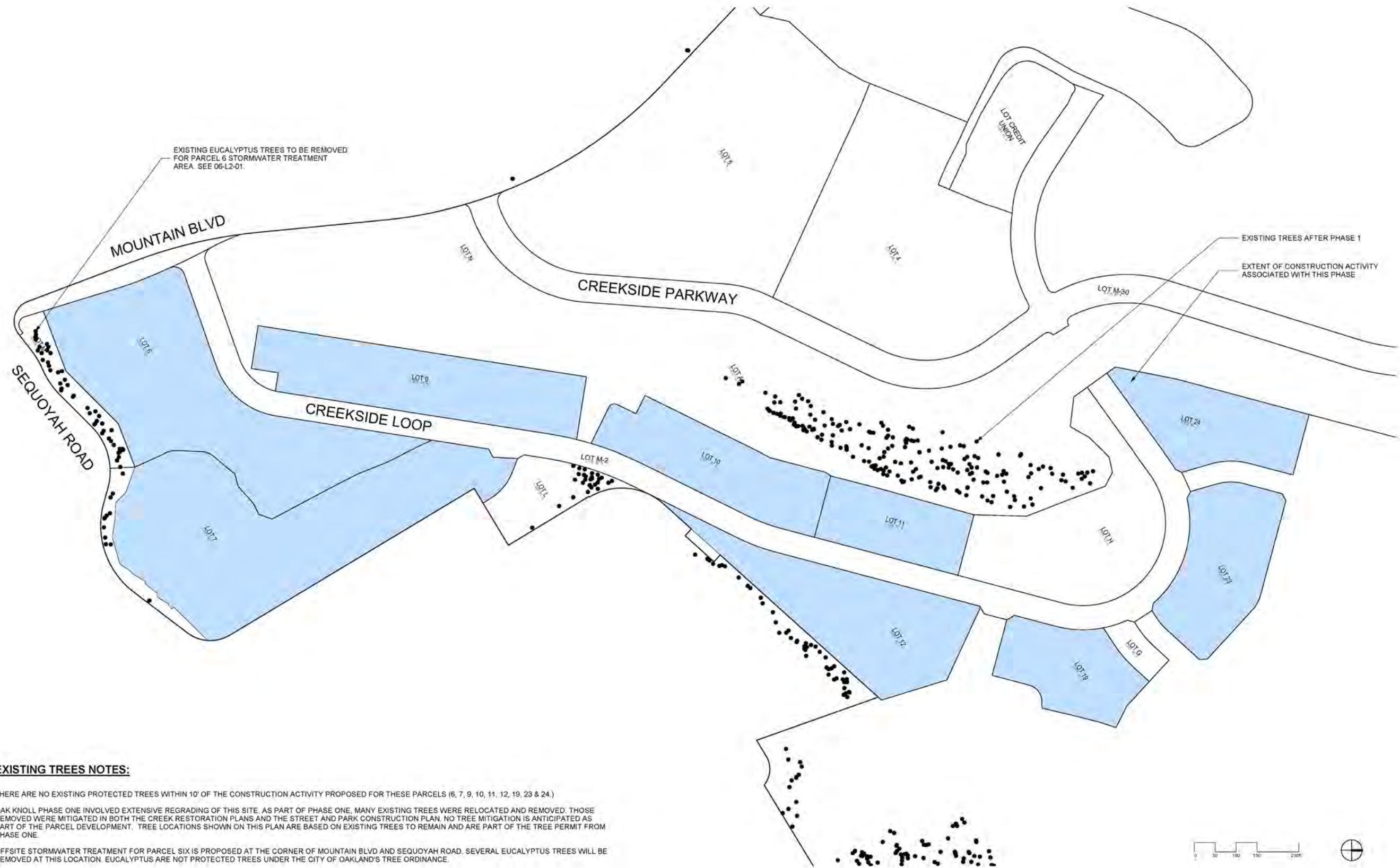
High performance COB LED light engine. White coating aluminium reflector on the top of luminaire. This luminaire is provided prewired with power cord to the handhole to simplify installation

**Additional Options (Consult Factory For Pricing)**



Ligman Lighting USA reserves the right to change specifications without prior notice. Please contact factory for latest information. Due to the continual improvements in LED technology and components may change without notice.





**EXISTING TREES NOTES:**

THERE ARE NO EXISTING PROTECTED TREES WITHIN 10' OF THE CONSTRUCTION ACTIVITY PROPOSED FOR THESE PARCELS (6, 7, 9, 10, 11, 12, 19, 23 & 24.)

OAK KNOLL PHASE ONE INVOLVED EXTENSIVE REGRADING OF THIS SITE. AS PART OF PHASE ONE, MANY EXISTING TREES WERE RELOCATED AND REMOVED. THOSE REMOVED WERE MITIGATED IN BOTH THE CREEK RESTORATION PLANS AND THE STREET AND PARK CONSTRUCTION PLAN. NO TREE MITIGATION IS ANTICIPATED AS PART OF THE PARCEL DEVELOPMENT. TREE LOCATIONS SHOWN ON THIS PLAN ARE BASED ON EXISTING TREES TO REMAIN AND ARE PART OF THE TREE PERMIT FROM PHASE ONE.

OFFSITE STORMWATER TREATMENT FOR PARCEL SIX IS PROPOSED AT THE CORNER OF MOUNTAIN BLVD AND SEQUOYAH ROAD. SEVERAL EUCALYPTUS TREES WILL BE REMOVED AT THIS LOCATION. EUCALYPTUS ARE NOT PROTECTED TREES UNDER THE CITY OF OAKLAND'S TREE ORDINANCE.



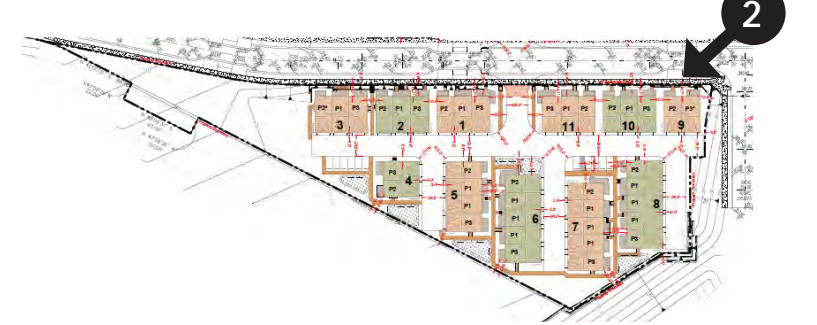
An architectural rendering of a modern townhome complex. The buildings are two stories high with light-colored siding and dark window frames. Each unit has a balcony with a dark railing. The complex is set on a grassy hillside with several trees. In the foreground, a paved path runs across the grass, with a person walking and another person crouching. A car is partially visible in the bottom right corner. The sky is blue with some clouds. A large, semi-transparent text box is centered over the image, containing the word "ARCHITECTURE" in a blue, serif font.

# ARCHITECTURE









OAK KNOLL  
TOWNHOME RENDERING  
FINAL DEVELOPMENT PLAN - PARCEL 12





**TOWNHOMES DUPLEX**  
 MISSION  
 SHOWN HERE ALSO WITH  
 CRAFTSMAN STYLE  
**DUPLEX BUILDING AREA**



**TOWNHOMES TRIPLEX**  
 MISSION  
 SHOWN HERE ALSO WITH  
 CRAFTSMAN STYLE  
**TRIPLEX BUILDING AREA**

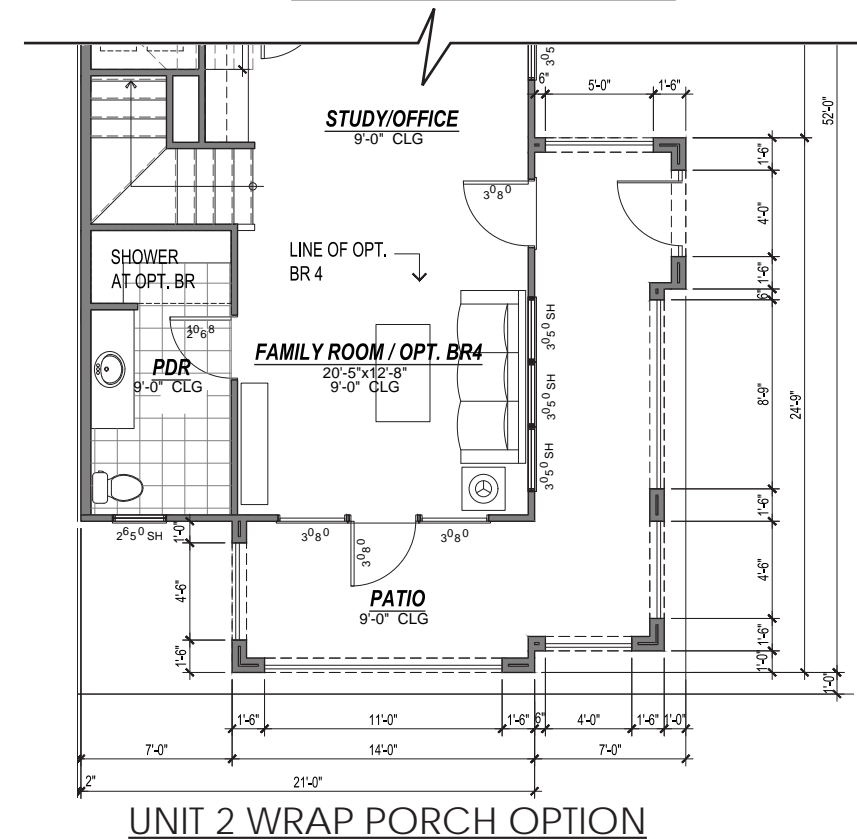
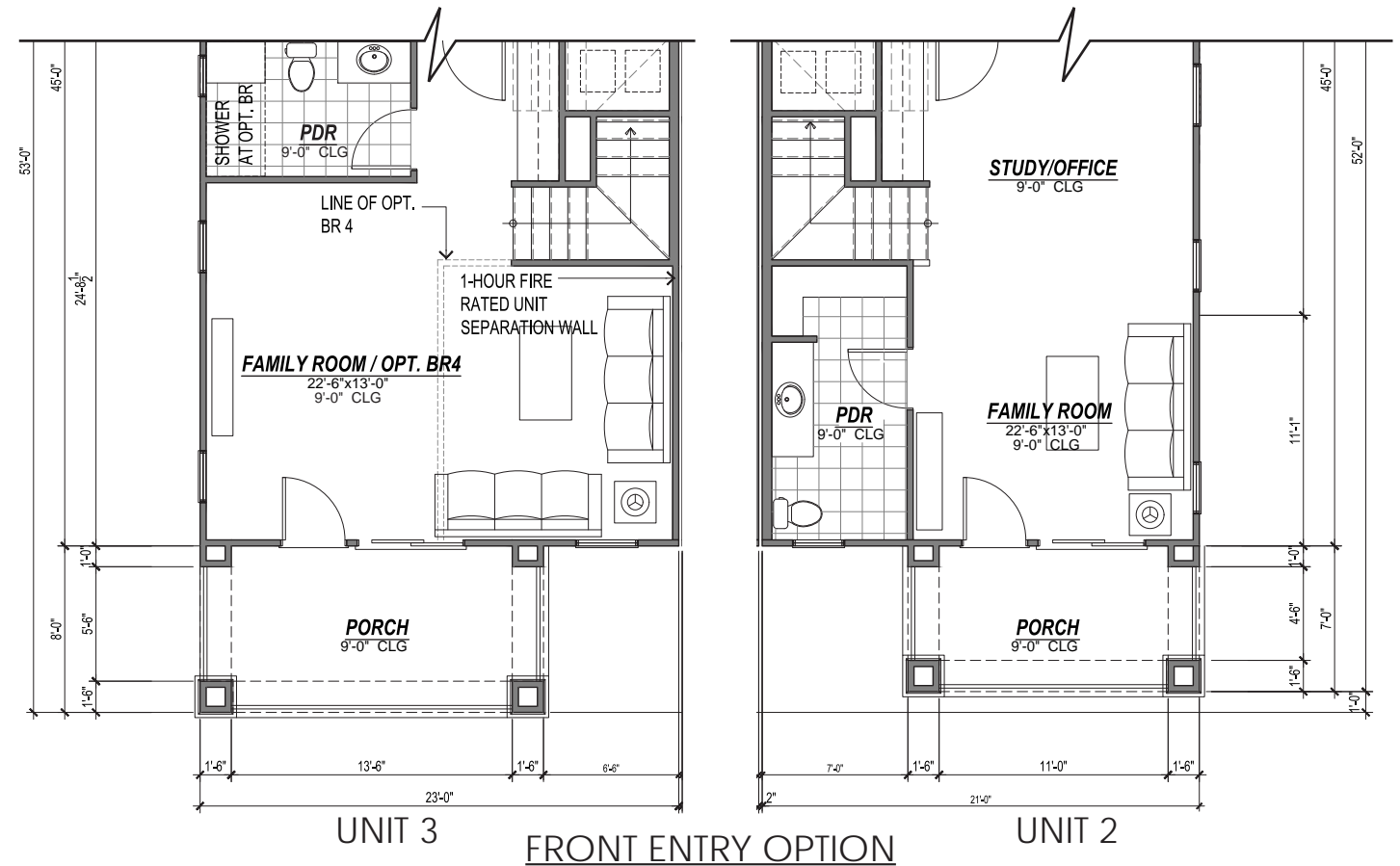
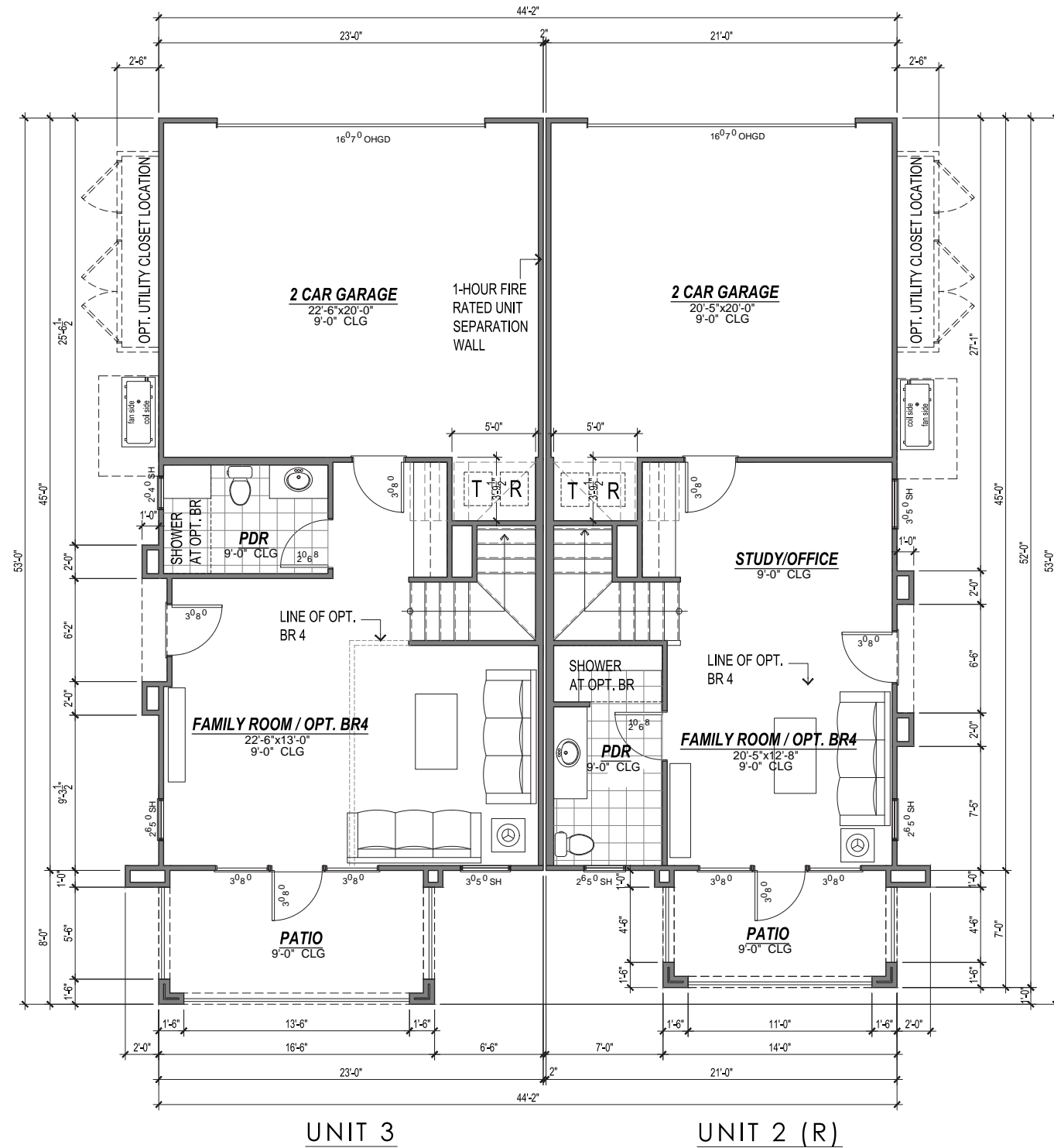


**TOWNHOMES 4-PLEX**  
 CRAFTSMAN  
 SHOWN HERE ALSO WITH  
 MISSION STYLE

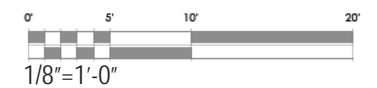


**TOWNHOMES 5-PLEX**  
 CRAFTSMAN  
 SHOWN HERE ALSO WITH  
 MISSION STYLE





- NOTES:  
 1. FLOOR PLAN REPRESENTS MISSION ELEVATION STYLE.  
 2. UTILITY CLOSET LOCATION TO BE DETERMINED IN COORDINATION WITH UTILITY PROVIDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

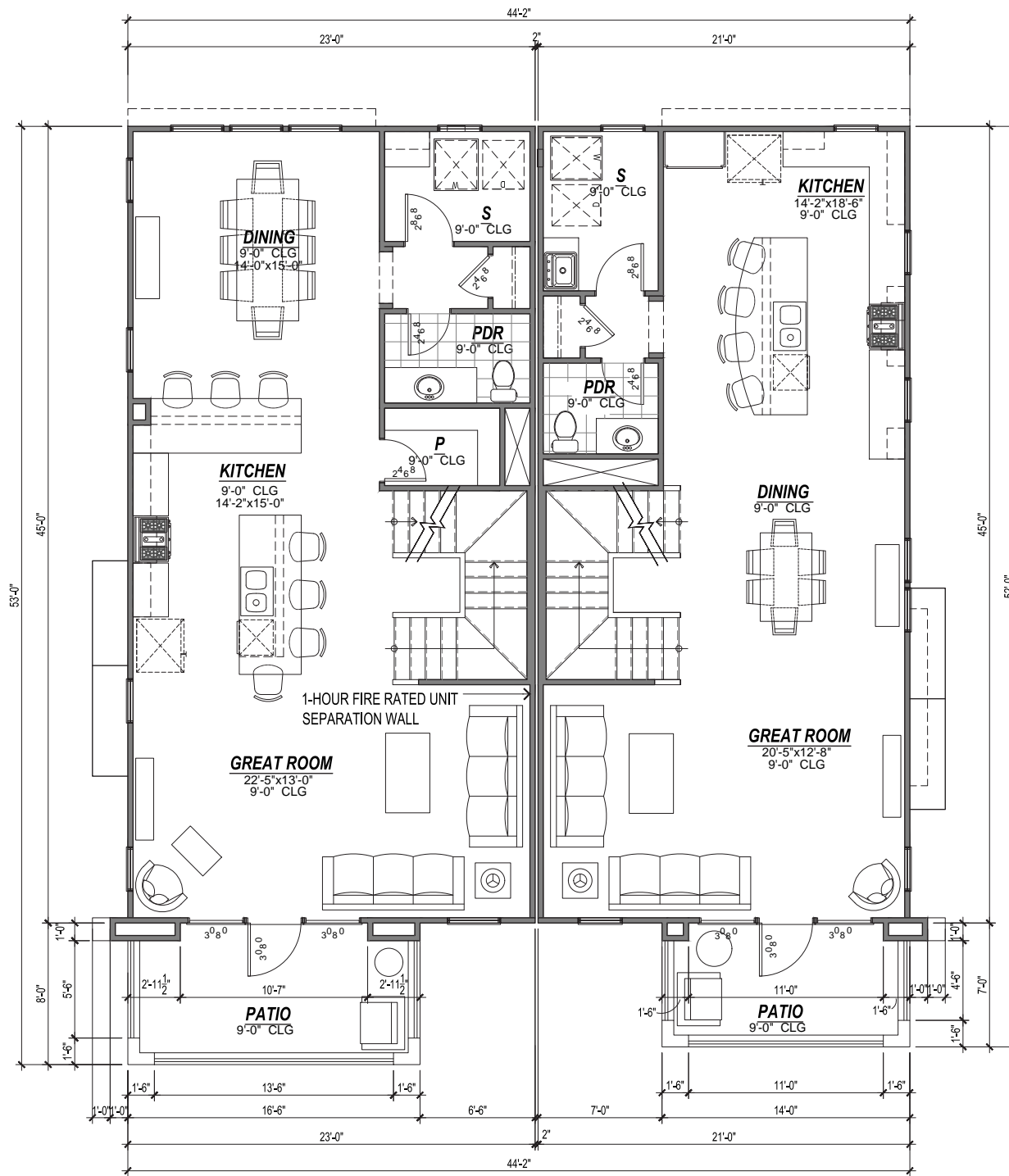
# OAK KNOLL

BUILDING COMPOSITES - TYPICAL DUPLEX TOWNHOMES FIRST FLOOR PLAN

FINAL DEVELOPMENT PLAN - PARCEL 12



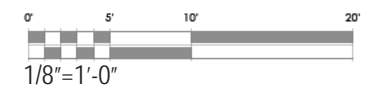




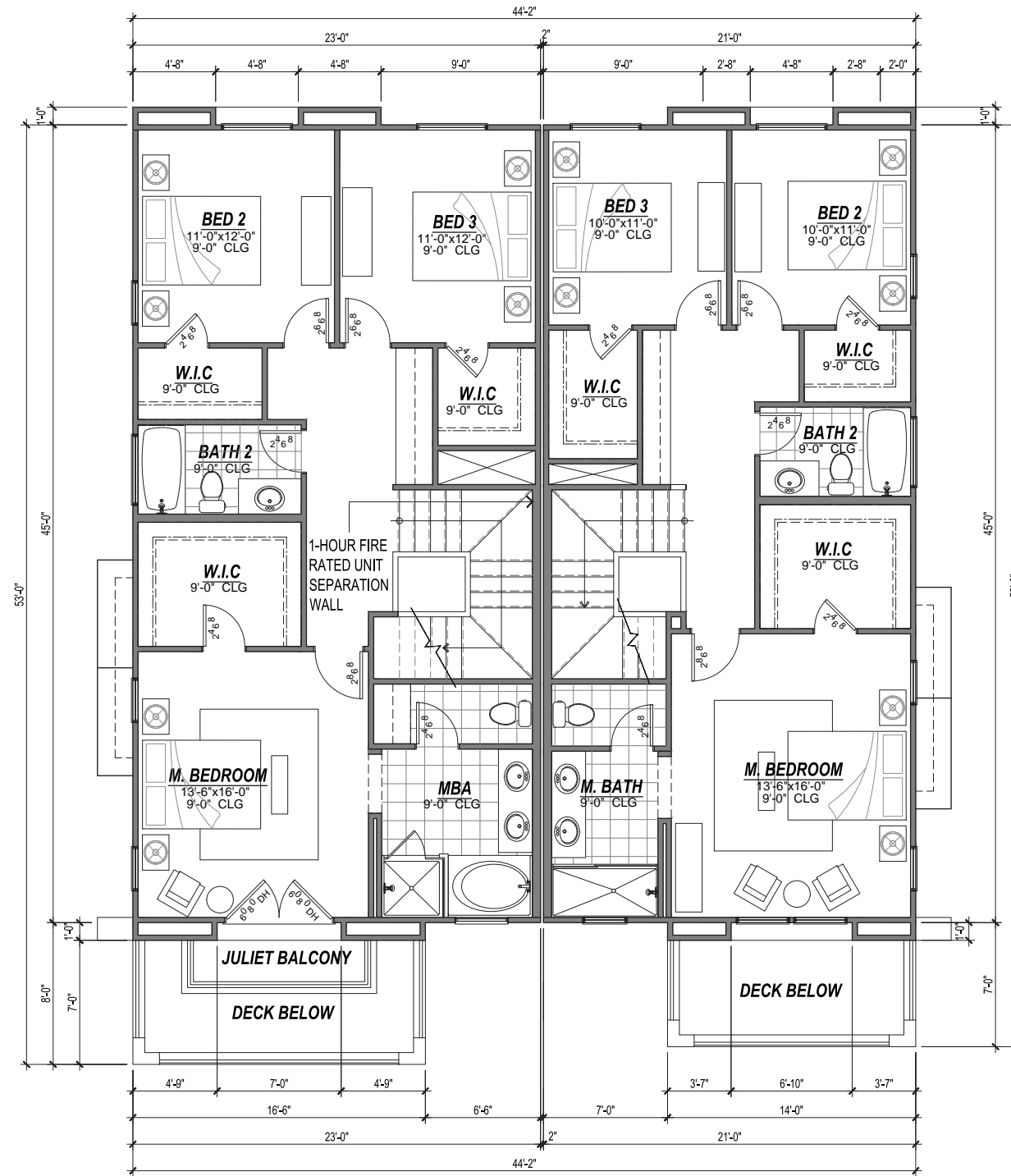
UNIT 3

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



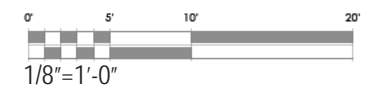




UNIT 3

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

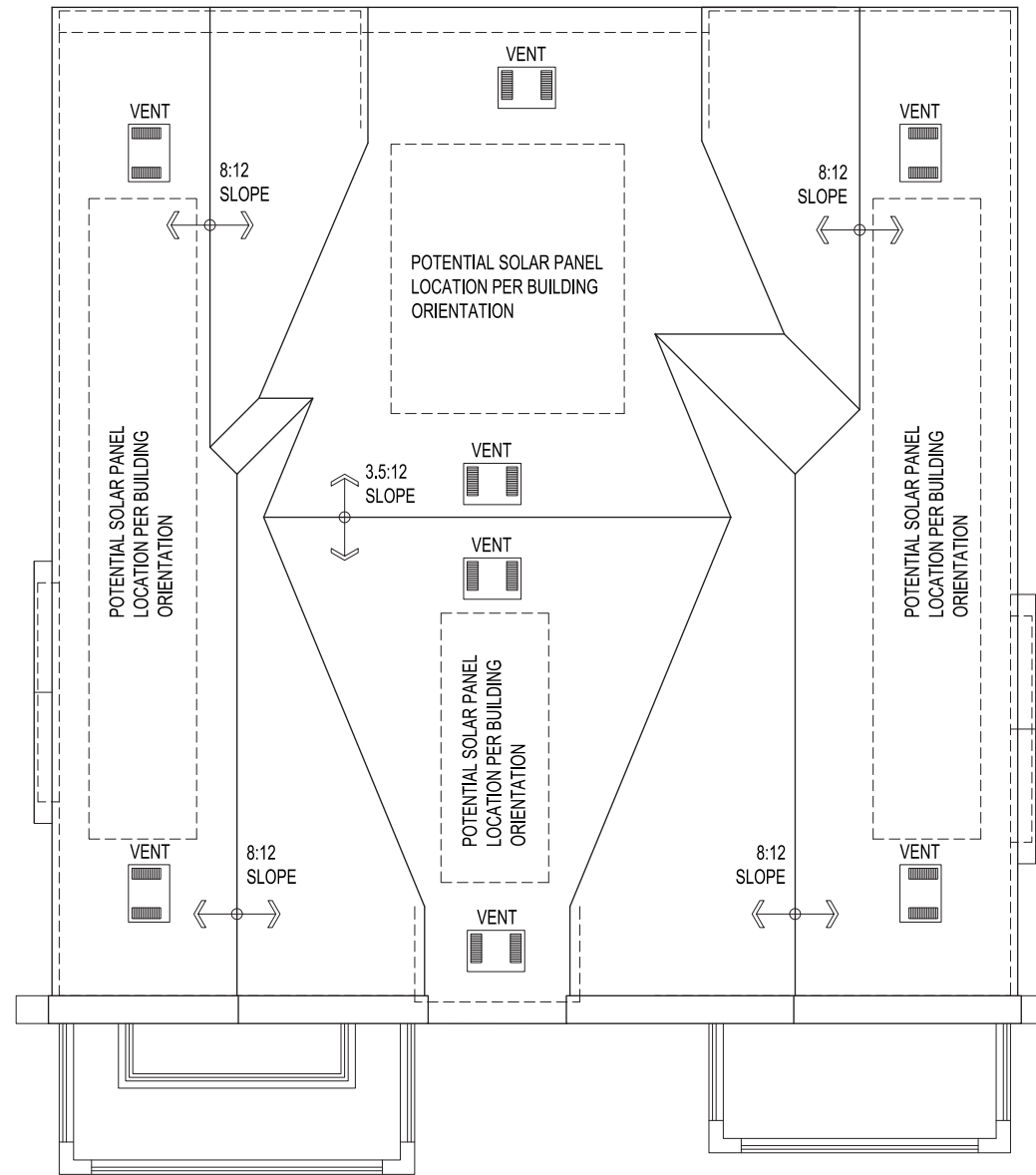
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL DUPLEX TOWNHOMES THIRD FLOOR PLAN

### FINAL DEVELOPMENT PLAN - PARCEL 12



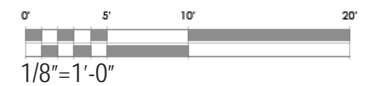




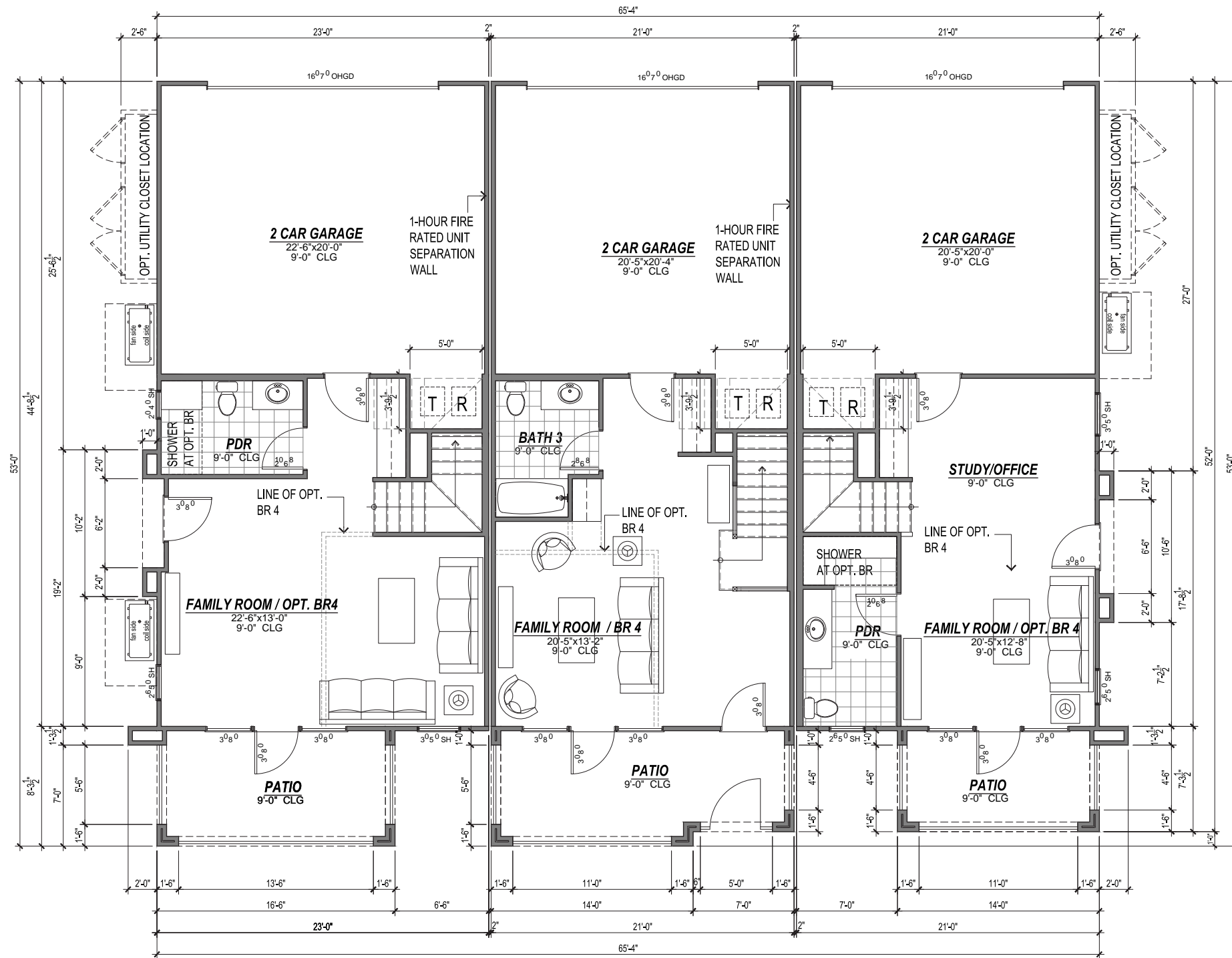
UNIT 3

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.





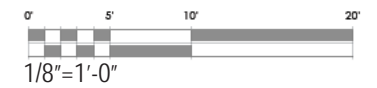


**UNIT 3**  
(SEE PAGE 36 FOR OPTIONAL UNIT 3 PLANS)

**UNIT 1**

**UNIT 2 (R)**  
(SEE PAGE 36 FOR OPTIONAL UNIT 2 PLANS)

- NOTES:
1. FLOOR PLAN REPRESENTS MISSION ELEVATION STYLE.
  2. UTILITY CLOSET LOCATION TO BE DETERMINED IN COORDINATION WITH UTILITY PROVIDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

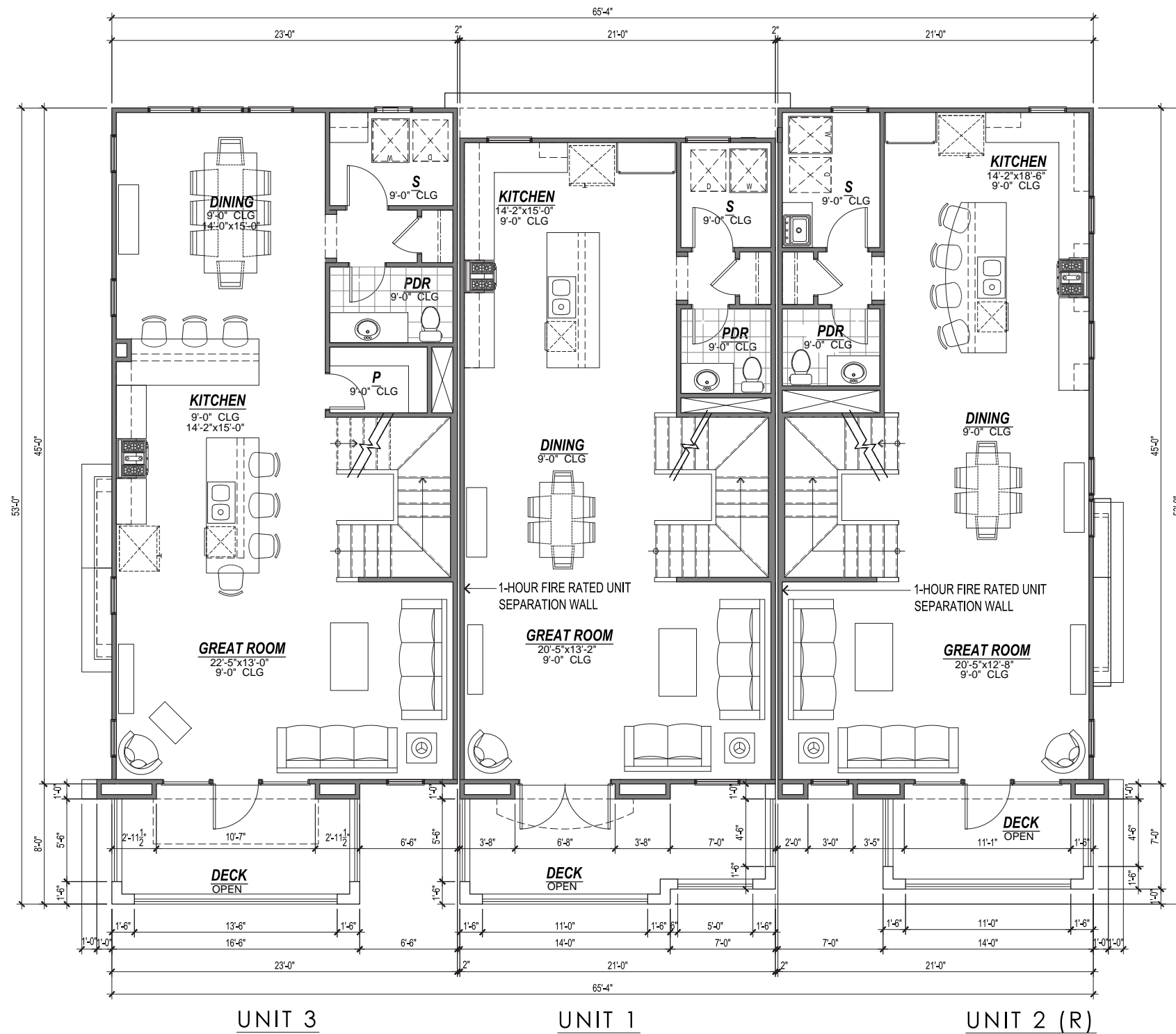
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL TRIPLEX TOWNHOMES FIRST FLOOR PLAN

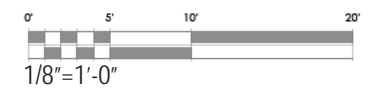
### FINAL DEVELOPMENT PLAN - PARCEL 12







NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



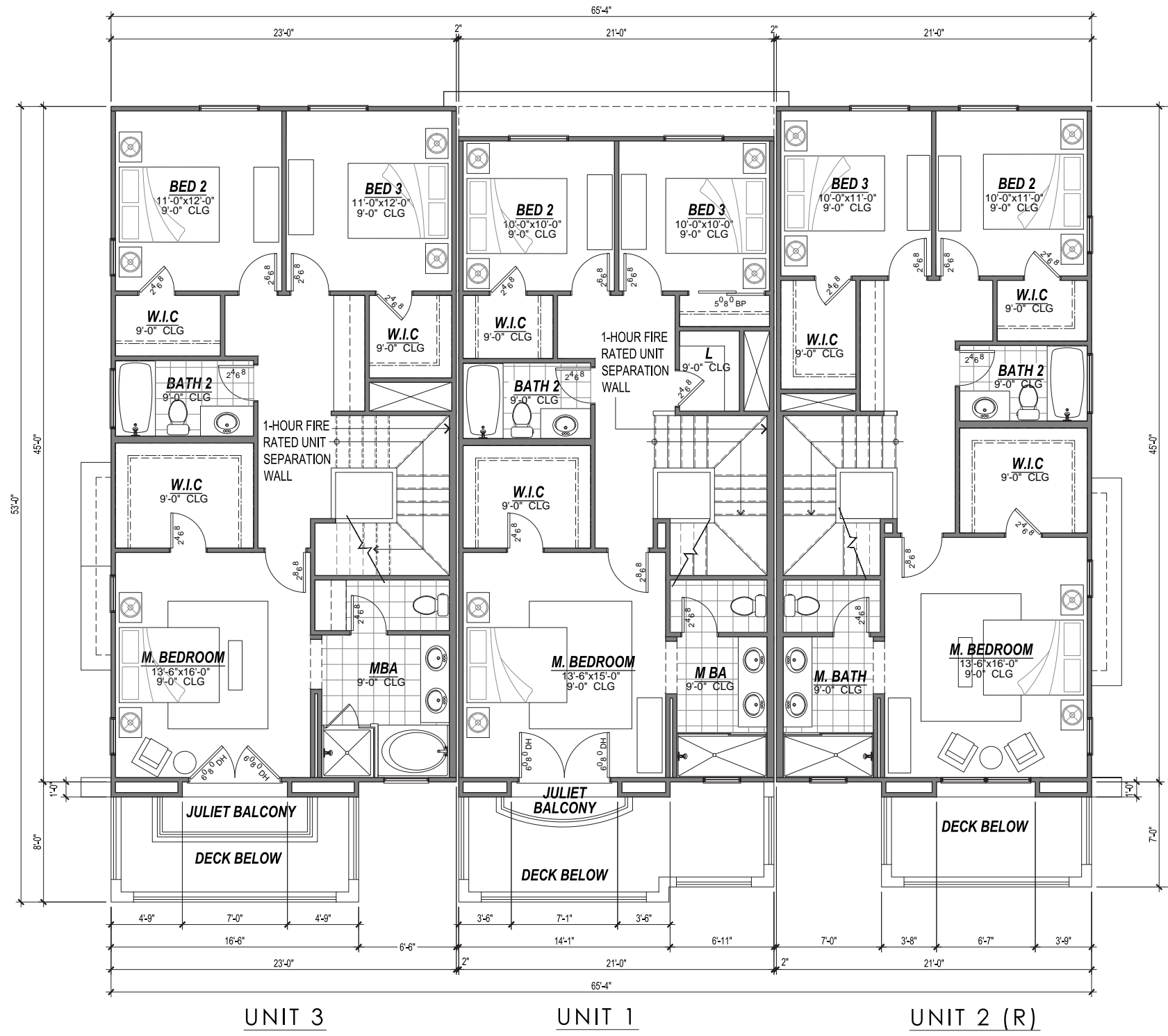
# OAK KNOLL

BUILDING COMPOSITES - TYPICAL TRIPLEX TOWNHOMES SECOND FLOOR PLAN

FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.





NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

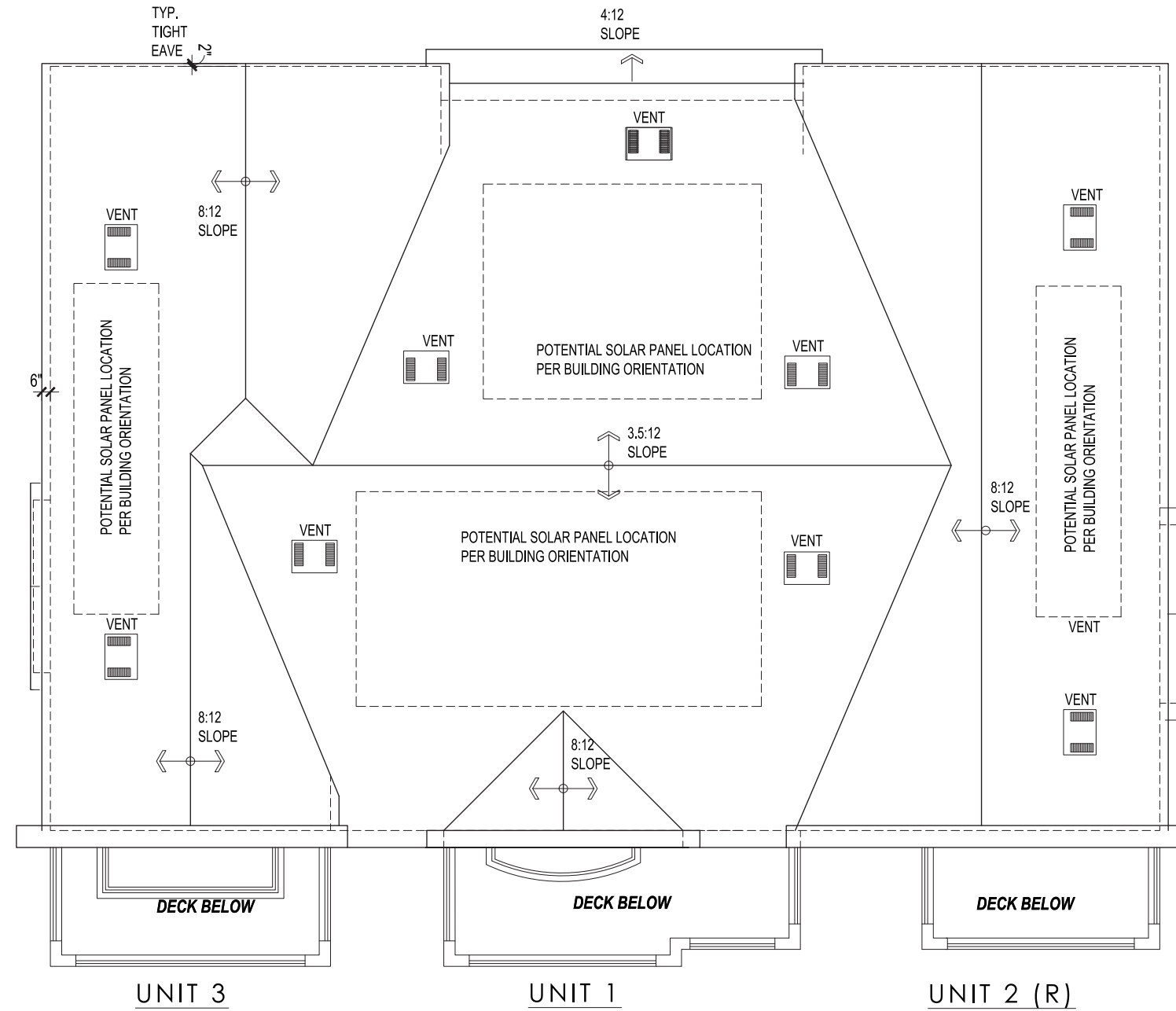
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL TRIPLEX TOWNHOMES THIRD FLOOR PLAN

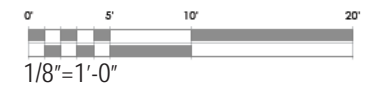
### FINAL DEVELOPMENT PLAN - PARCEL 12



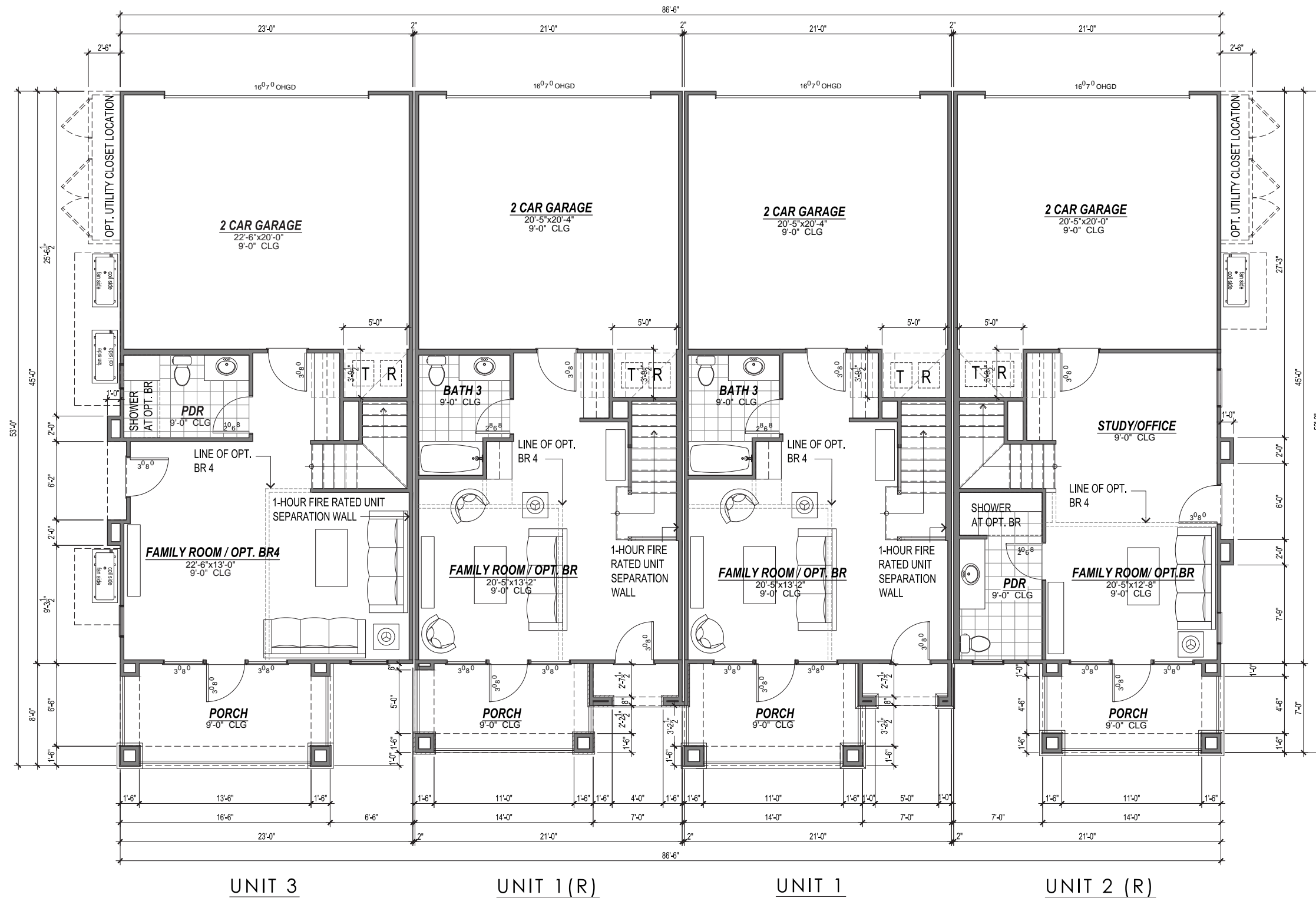




NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.

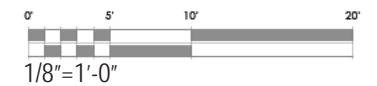






AS REQUIRED PER CBC 1102A.03  
 MULTI-STORY DWELLINGS, 10%  
 OF THE UNITS WILL BE PROVIDED,  
 IDENTIFIED AND THEIR LOCATION  
 BE DETERMINED AT THE TIME OF THE  
 FINAL PRECISE GRADING PLAN.

- NOTES:
1. FLOOR PLAN REPRESENTS MISSION ELEVATION STYLE.
  2. UTILITY CLOSET LOCATION TO BE DETERMINED IN COORDINATION WITH UTILITY PROVIDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

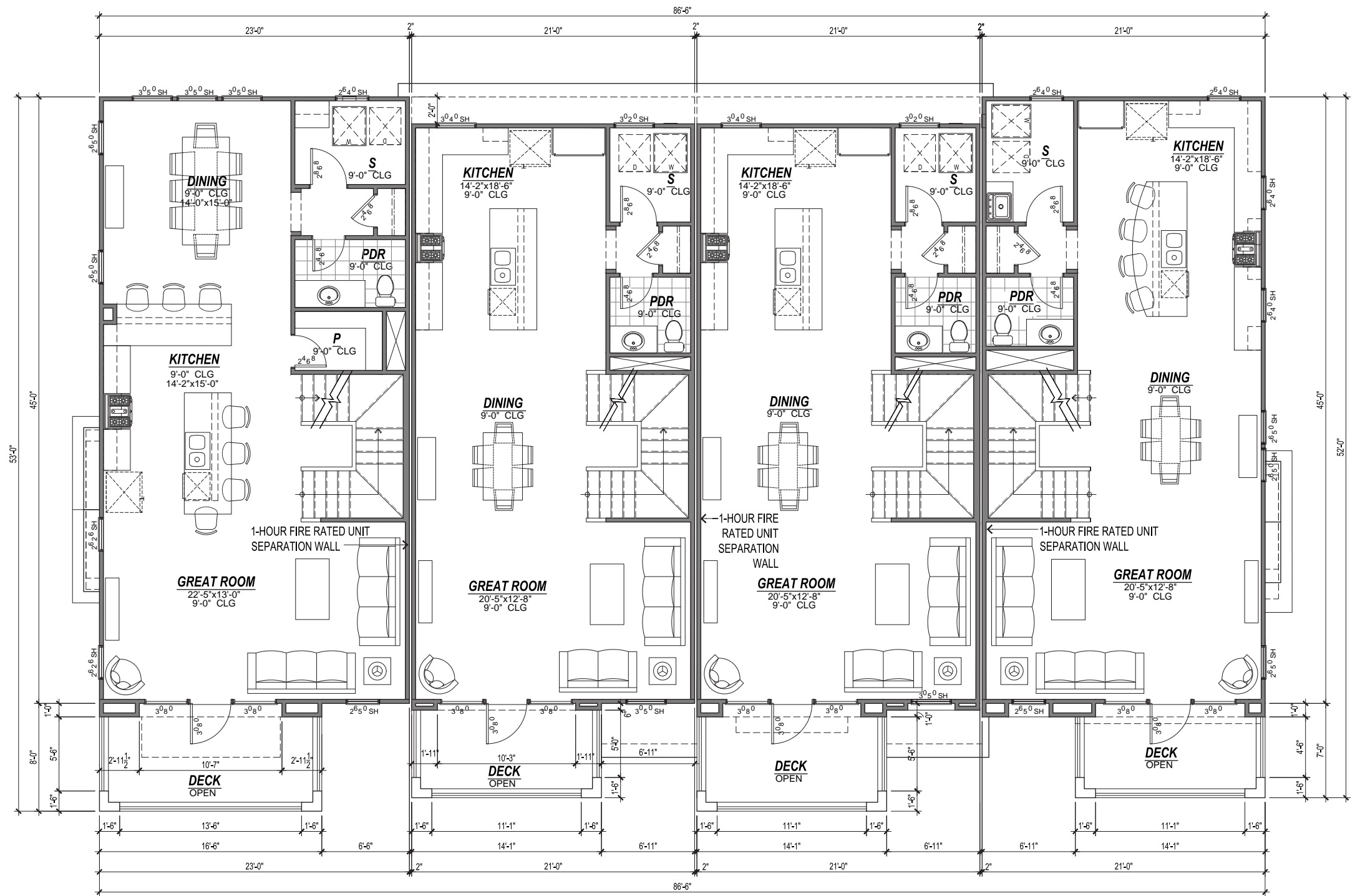
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL 4-PLEX TOWNHOMES FIRST FLOOR PLAN

### FINAL DEVELOPMENT PLAN - PARCEL 12







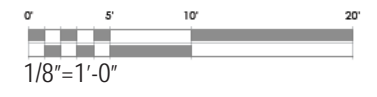
UNIT 3

UNIT 1 (R)

UNIT 1

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.

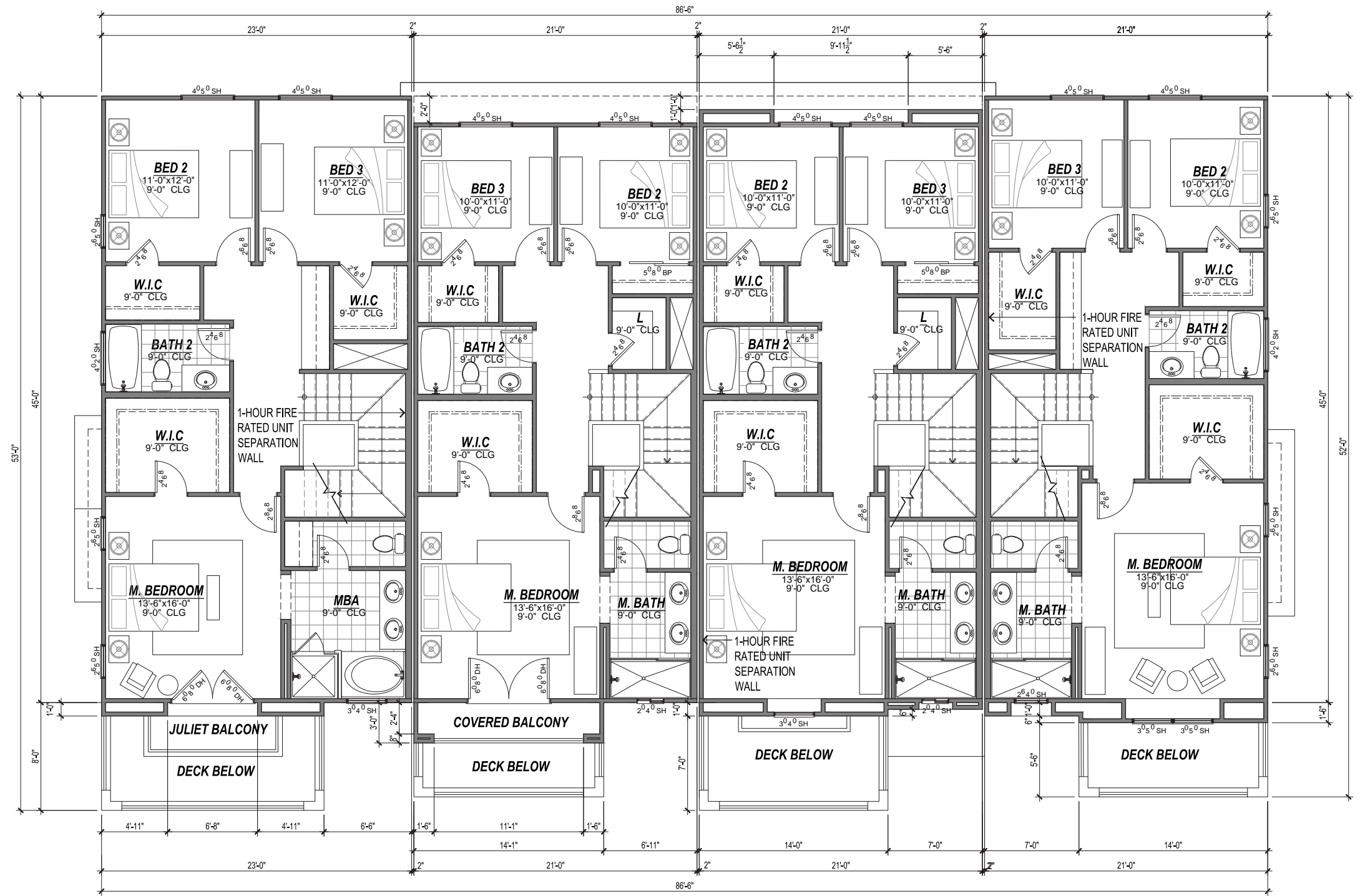


# OAK KNOLL

BUILDING COMPOSITES - TYPICAL 4-PLEX TOWNHOMES SECOND FLOOR PLAN

FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



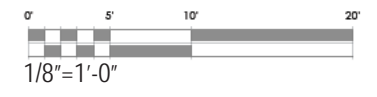
UNIT 3

UNIT 1 (R)

UNIT 1

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

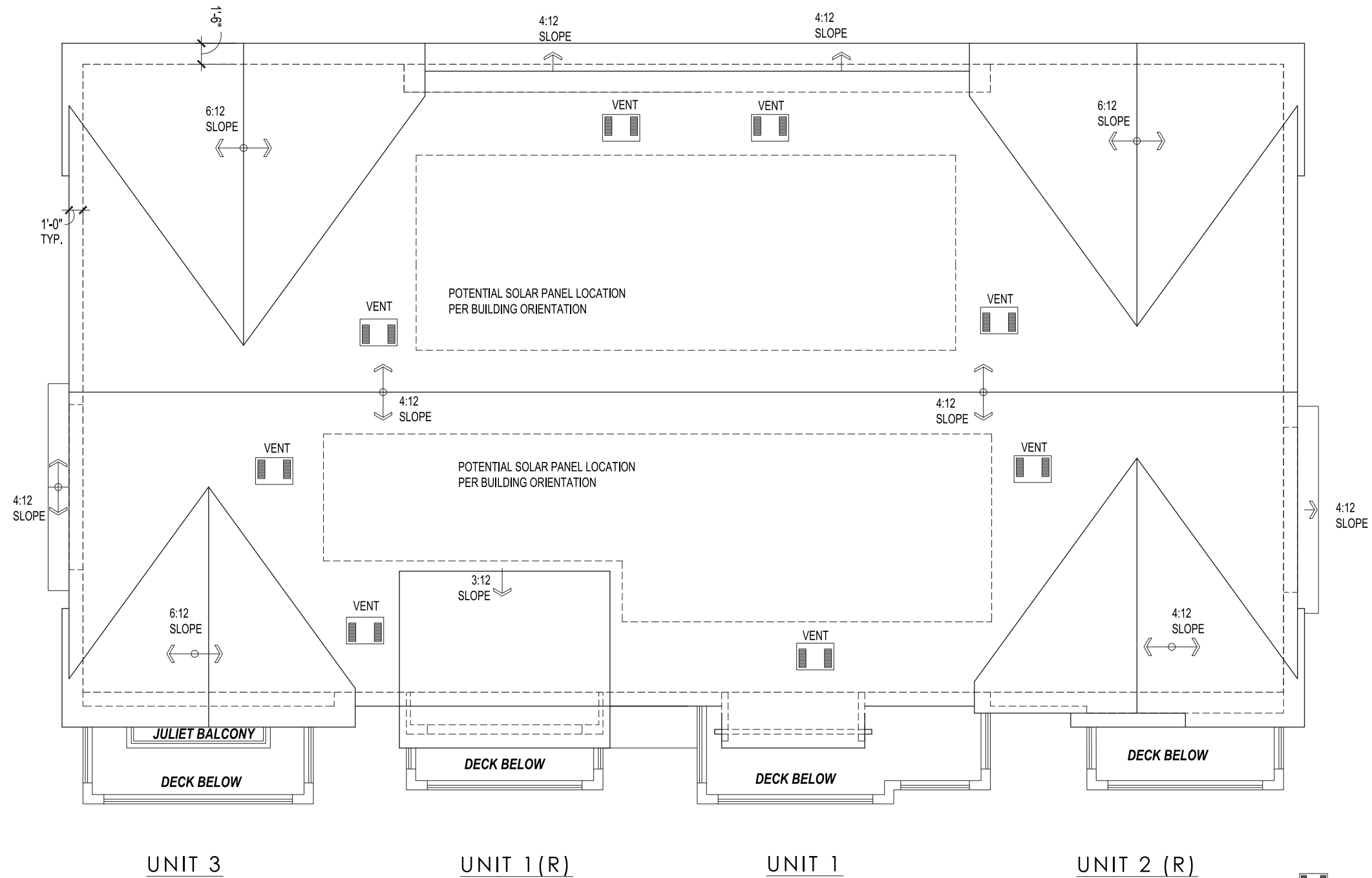
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL 4-PLEX TOWNHOMES THIRD FLOOR PLAN

### FINAL DEVELOPMENT PLAN - PARCEL 12

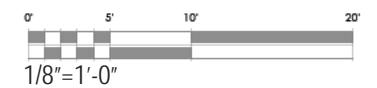


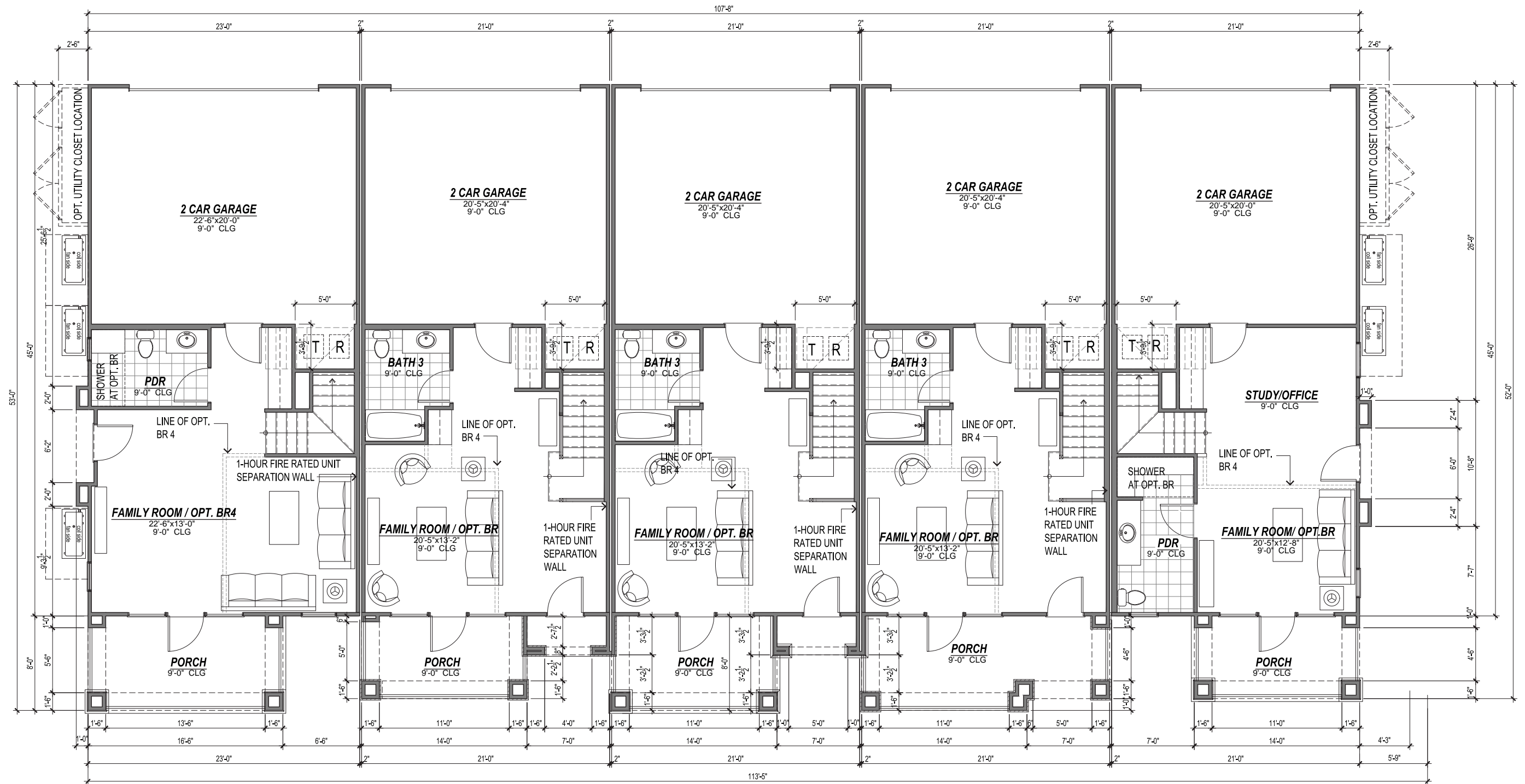




 ROOF VENT  
 (FINAL ATTIC VENT COUNT AND LOCATION TO BE DETERMINED AT PRODUCTION)

NOTE:  
 FLOOR PLAN REPRESENTS MISSION  
 ELEVATION STYLE.





**UNIT 3**

(SEE PAGE 36 FOR OPTIONAL UNIT 3 PLANS)

**UNIT 1 (R)**

**UNIT 1**

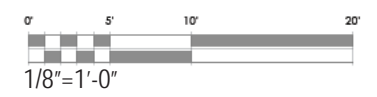
**UNIT 1 (R)**

**UNIT 2 (R)**

(SEE PAGE 36 FOR OPTIONAL UNIT 2 PLANS)

AS REQUIRED PER CBC 1102A.03 MULTI-STORY DWELLINGS, 10% OF THE UNITS WILL BE PROVIDED, IDENTIFIED AND THEIR LOCATION BE DETERMINED AT THE TIME OF THE FINAL PRECISE GRADING PLAN.

- NOTES:
1. FLOOR PLAN REPRESENTS MISSION ELEVATION STYLE.
  2. UTILITY CLOSET LOCATION TO BE DETERMINED IN COORDINATION WITH UTILITY PROVIDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

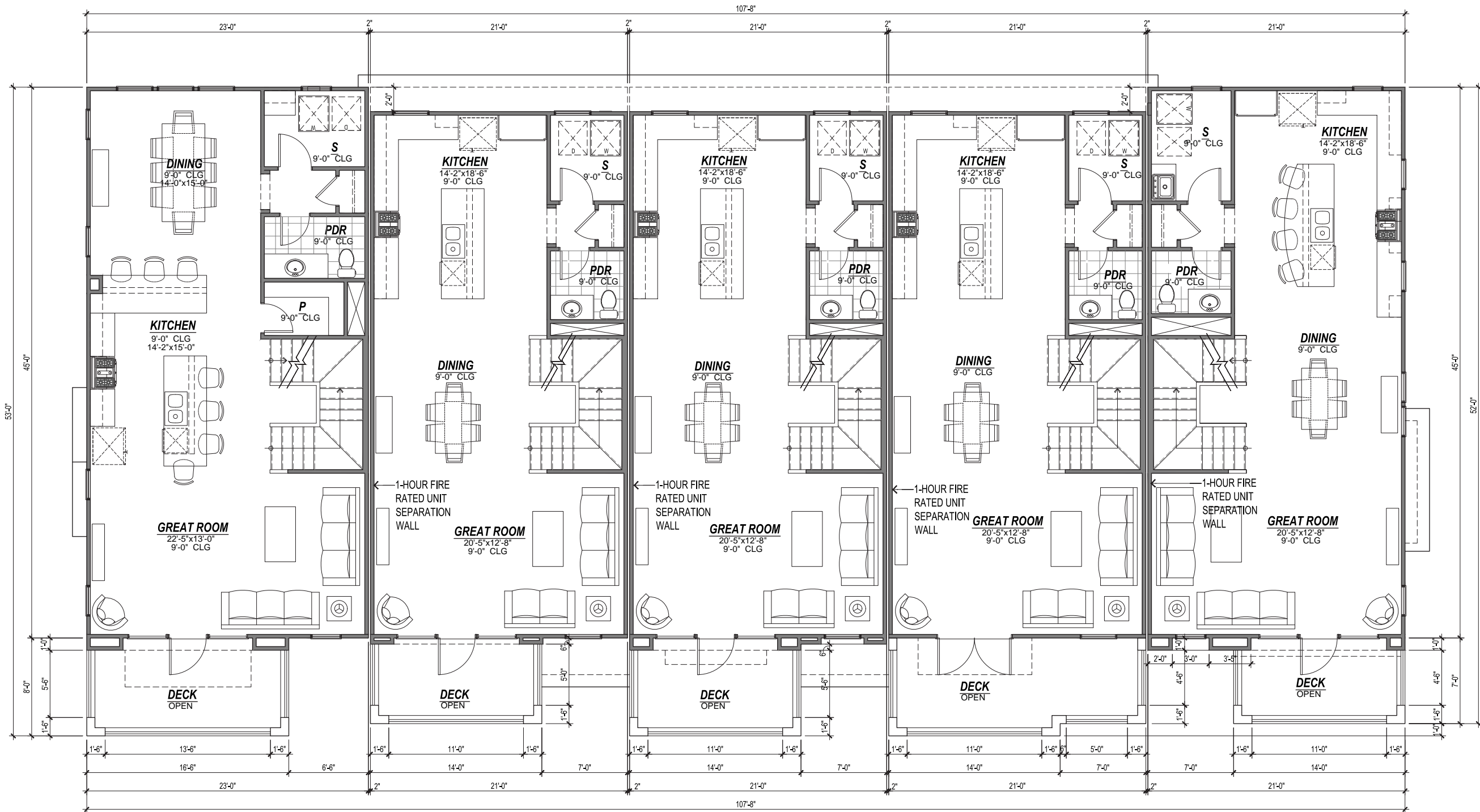
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL 5-PLEX TOWNHOMES FIRST FLOOR PLAN

FINAL DEVELOPMENT PLAN - PARCEL 12







UNIT 3

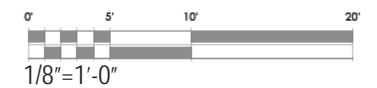
UNIT 1 (R)

UNIT 1

UNIT 1 (R)

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



# OAK KNOLL

BUILDING COMPOSITES - TYPICAL 5-PLEX TOWNHOMES SECOND FLOOR PLAN

FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



UNIT 3

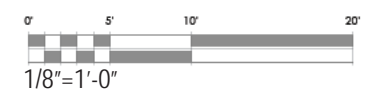
UNIT 1 (R)

UNIT 1

UNIT 1 (R)

UNIT 2 (R)

NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

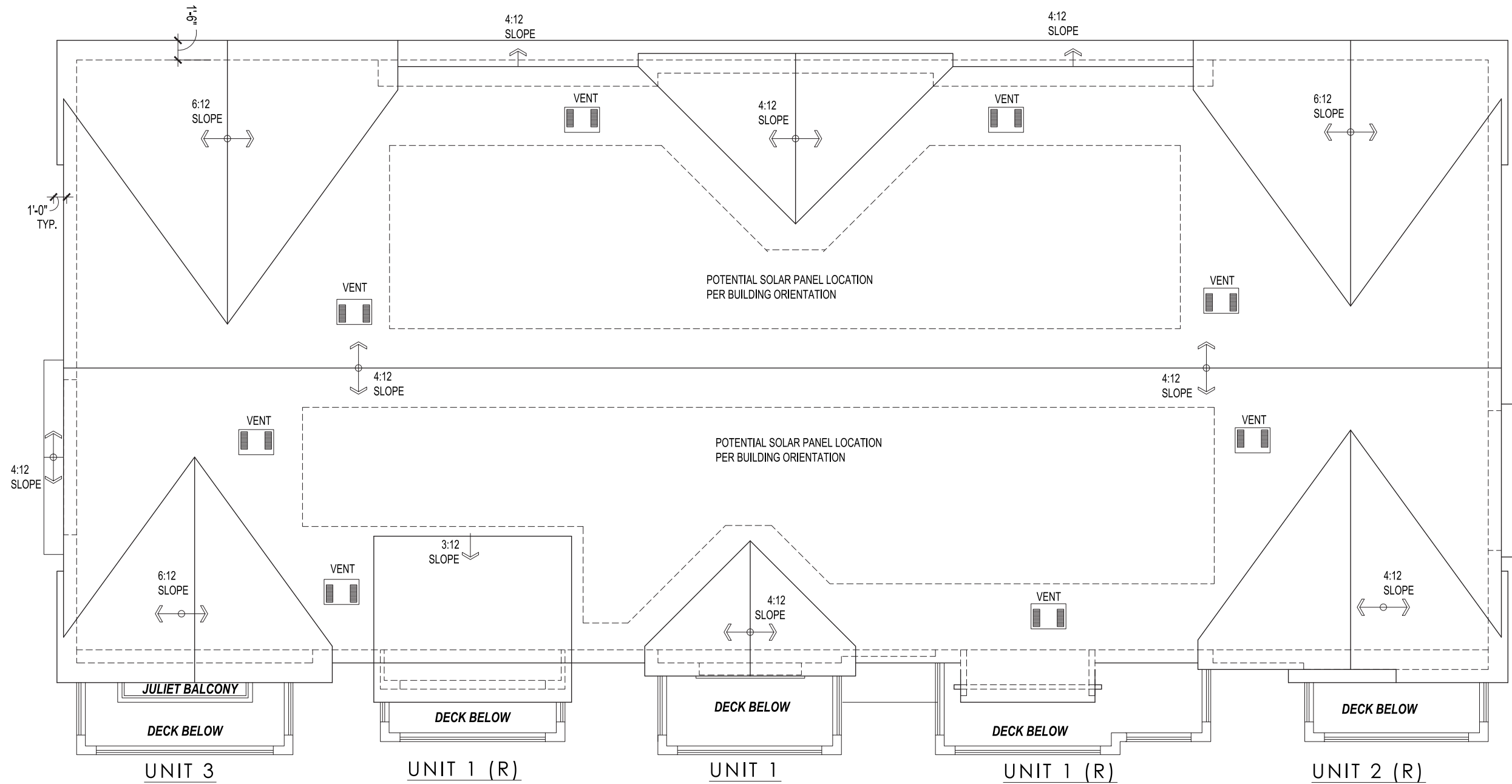
# OAK KNOLL

## BUILDING COMPOSITES - TYPICAL 5-PLEX TOWNHOMES THIRD FLOOR PLAN

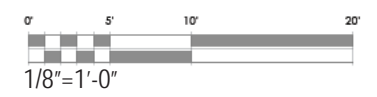
### FINAL DEVELOPMENT PLAN - PARCEL 12







NOTE:  
FLOOR PLAN REPRESENTS MISSION  
ELEVATION STYLE.



# OAK KNOLL

BUILDING COMPOSITES - TYPICAL 5-PLEX TOWNHOMES ROOF PLAN  
FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



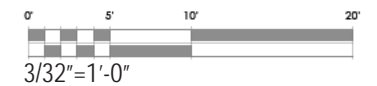
1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

**ELEVATION KEY NOTES**



NOTES:

1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

# OAK KNOLL

## BUILDING 1 ELEVATIONS - TRIPLEX MISSION WRAPPED PORCH

### FINAL DEVELOPMENT PLAN - PARCEL 12







UNIT 3  
LEFT ELEVATION



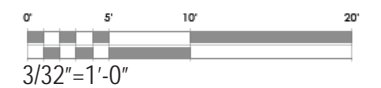
UNIT 2 (R)  
RIGHT ELEVATION

- 1 STUCCO
- 2 HORIZONTAL SIDING
- 3 BOARD & BATT SIDING
- 4 SHINGLE SIDING
- 5 VINYL WINDOW
- 6 SHUTTERS AT ENHANCED ELEVATION
- 7 WROUGHT IRON RAILING
- 8 STANDING SEAM METAL ROOF
- 9 FLAT CONCRETE TILE ROOF
- 10 S-TILE ROOF
- 11 GARAGE DOOR
- 12 STANDING SEAM METAL CANOPY
- 13 N/A
- 14 WINDOW WOOD TRIM
- 15 STONE VENEER
- 16 EXTERIOR LIGHTING
- 17 UTILITY LOCATION / ROOM TO BE DETERMINED
- 18 A/C LOCATION
- 19 PRIVACY FENCE AT END UNIT
- 20 TILE ACCENT
- 21 DECORATIVE WOOD RAIL

**ELEVATION KEY NOTES**



- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P.) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.





FRONT ELEVATION



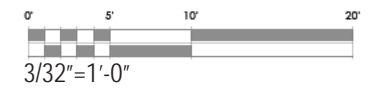
REAR ELEVATION

- |    |  |
|----|--|
| 1  | STUCCO                                   |
| 2  | HORIZONTAL SIDING                        |
| 3  | BOARD & BATT SIDING                      |
| 4  | SHINGLE SIDING                           |
| 5  | VINYL WINDOW                             |
| 6  | SHUTTERS AT ENHANCED ELEVATION           |
| 7  | WROUGHT IRON RAILING                     |
| 8  | STANDING SEAM METAL ROOF                 |
| 9  | FLAT CONCRETE TILE ROOF                  |
| 10 | S-TILE ROOF                              |
| 11 | GARAGE DOOR                              |
| 12 | STANDING SEAM METAL CANOPY               |
| 13 | N/A                                      |
| 14 | WINDOW WOOD TRIM                         |
| 15 | STONE VENEER                             |
| 16 | EXTERIOR LIGHTING                        |
| 17 | UTILITY LOCATION / ROOM TO BE DETERMINED |
| 18 | A/C LOCATION                             |
| 19 | PRIVACY FENCE AT END UNIT                |
| 20 | TILE ACCENT                              |
| 21 | DECORATIVE WOOD RAIL                     |

ELEVATION KEY NOTES



- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



Alvarez

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

# OAK KNOLL

## BUILDING 2 ELEVATIONS - TRIPLEX CRAFTSMAN

### FINAL DEVELOPMENT PLAN - PARCEL 12







UNIT 3  
LEFT ELEVATION



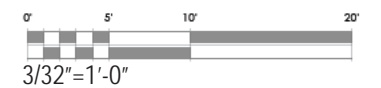
UNIT 2 (R)  
RIGHT ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

ELEVATION KEY NOTES



- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



# OAK KNOLL

## BUILDING 2 ELEVATIONS - TRIPLEX CRAFTSMAN

### FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



UNIT 3                      UNIT 1                      UNIT 2 (R)  
**FRONT ELEVATION**

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

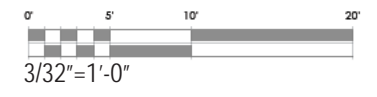
**ELEVATION KEY NOTES**



UNIT 2 (R)                      UNIT 1                      UNIT 3  
**REAR ELEVATION**

- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.

NC  
1  
2  
3



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

# OAK KNOLL

## BUILDING 3 ELEVATIONS - TRIPLEX MISSION ENHANCED SIDE

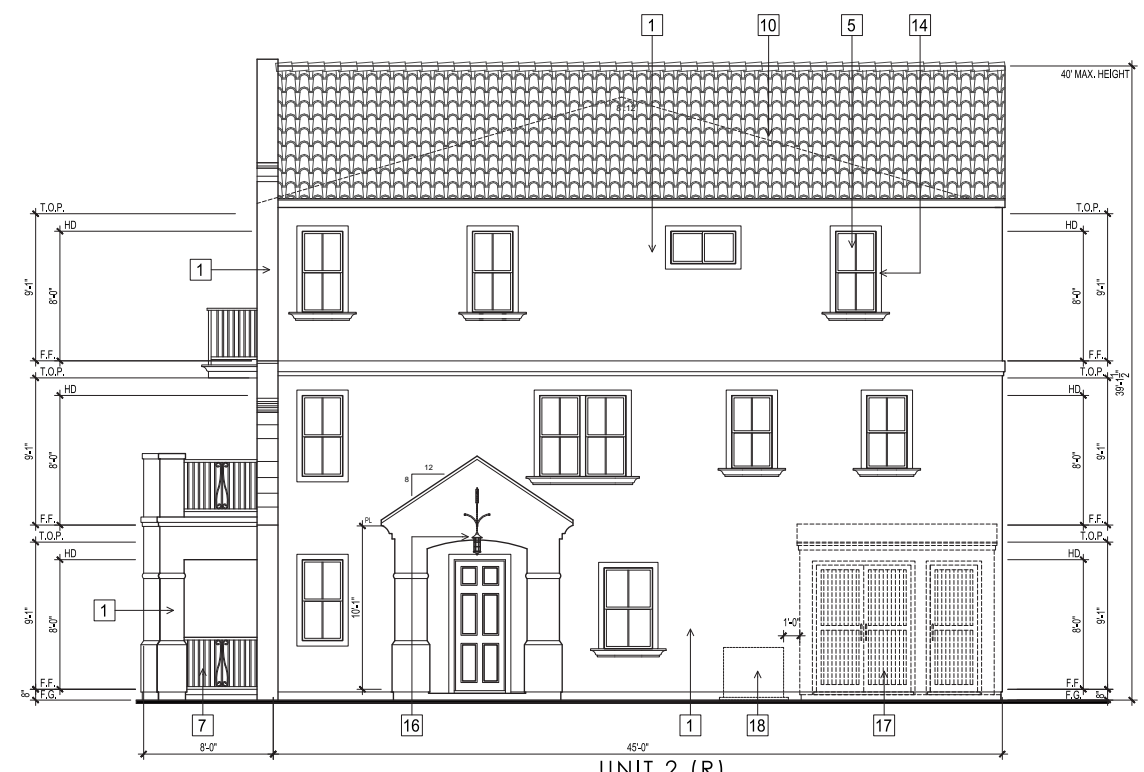
### FINAL DEVELOPMENT PLAN - PARCEL 12







UNIT 3  
LEFT ENHANCED ELEVATION



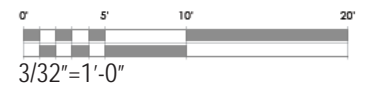
UNIT 2 (R)  
RIGHT ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

**ELEVATION KEY NOTES**



NOTES:  
 1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.  
 2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



# OAK KNOLL

BUILDING 3 ELEVATIONS - TRIPLEX MISSION ENHANCED SIDE  
 FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.





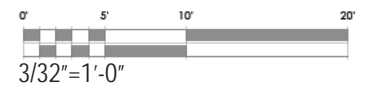


1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

**ELEVATION KEY NOTES**



- NOTES:
- HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
  - WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



# OAK KNOLL

## BUILDING 5 ELEVATIONS - 4-PLEX MISSION

### FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



UNIT 3  
RIGHT ELEVATION



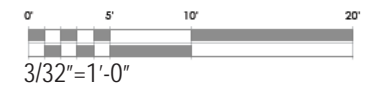
UNIT 2 (R)  
LEFT ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

**ELEVATION KEY NOTES**



- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

# OAK KNOLL

## BUILDING 5 ELEVATIONS - 4-PLEX MISSION

### FINAL DEVELOPMENT PLAN - PARCEL 12







1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

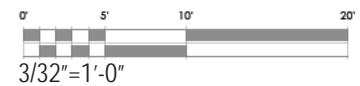
**ELEVATION KEY NOTES**



FRONT ELEVATION



- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P.) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



# OAK KNOLL

BUILDING 6 ELEVATIONS - 5-PLEX CRAFTSMAN WRAPPED PORCH  
FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



UNIT 3  
LEFT ELEVATION



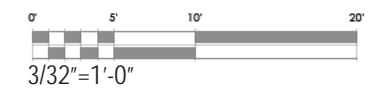
UNIT 2 (R)  
RIGHT ELEVATION

- 1 STUCCO
- 2 HORIZONTAL SIDING
- 3 BOARD & BATT SIDING
- 4 SHINGLE SIDING
- 5 VINYL WINDOW
- 6 SHUTTERS AT ENHANCED ELEVATION
- 7 WROUGHT IRON RAILING
- 8 STANDING SEAM METAL ROOF
- 9 FLAT CONCRETE TILE ROOF
- 10 S-TILE ROOF
- 11 GARAGE DOOR
- 12 STANDING SEAM METAL CANOPY
- 13 N/A
- 14 WINDOW WOOD TRIM
- 15 STONE VENEER
- 16 EXTERIOR LIGHTING
- 17 UTILITY LOCATION / ROOM TO BE DETERMINED
- 18 A/C LOCATION
- 19 PRIVACY FENCE AT END UNIT
- 20 TILE ACCENT
- 21 DECORATIVE WOOD RAIL

ELEVATION KEY NOTES



- NOTES:
1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.
  2. WINDOWS WILL MAINTAIN A STANDARD SIZE AS REQUIRED BY CODE BUT MUST RESPECT THE DESIGN PROPORTIONS AND MADE OUT OF VINYL. THE WINDOW MANUFACTURER SHALL BE SELECTED BY THE BUILDER.



Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

# OAK KNOLL

## BUILDING 6 ELEVATIONS - 5-PLEX CRAFTSMAN WRAPPED PORCH

### FINAL DEVELOPMENT PLAN - PARCEL 12







1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
10	S-TILE ROOF
11	GARAGE DOOR
12	STANDING SEAM METAL CANOPY
13	N/A
14	WINDOW WOOD TRIM
15	STONE VENEER
16	EXTERIOR LIGHTING
17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

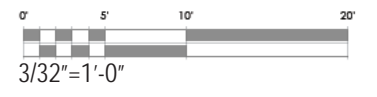
**ELEVATION KEY NOTES**

UNIT 3                      UNIT 1                      UNIT 2 (R)  
**FRONT ELEVATION**



UNIT 2 (R)                      UNIT 1                      UNIT 3  
**REAR ELEVATION**

NOTES:  
 1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.  
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# OAK KNOLL

BUILDING 7 ELEVATIONS - 5-PLEX MISSION WRAPPED PORCH  
 FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



UNIT 3  
LEFT ELEVATION



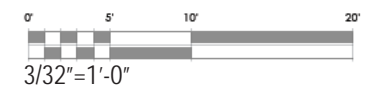
UNIT 2 (R)  
RIGHT ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
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**ELEVATION KEY NOTES**



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# OAK KNOLL

## BUILDING 7 ELEVATIONS - 5-PLEX MISSION WRAPPED PORCH

### FINAL DEVELOPMENT PLAN - PARCEL 12

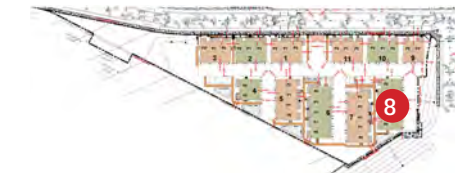




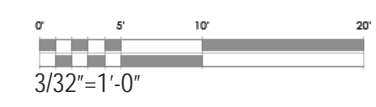


1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
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**ELEVATION KEY NOTES**



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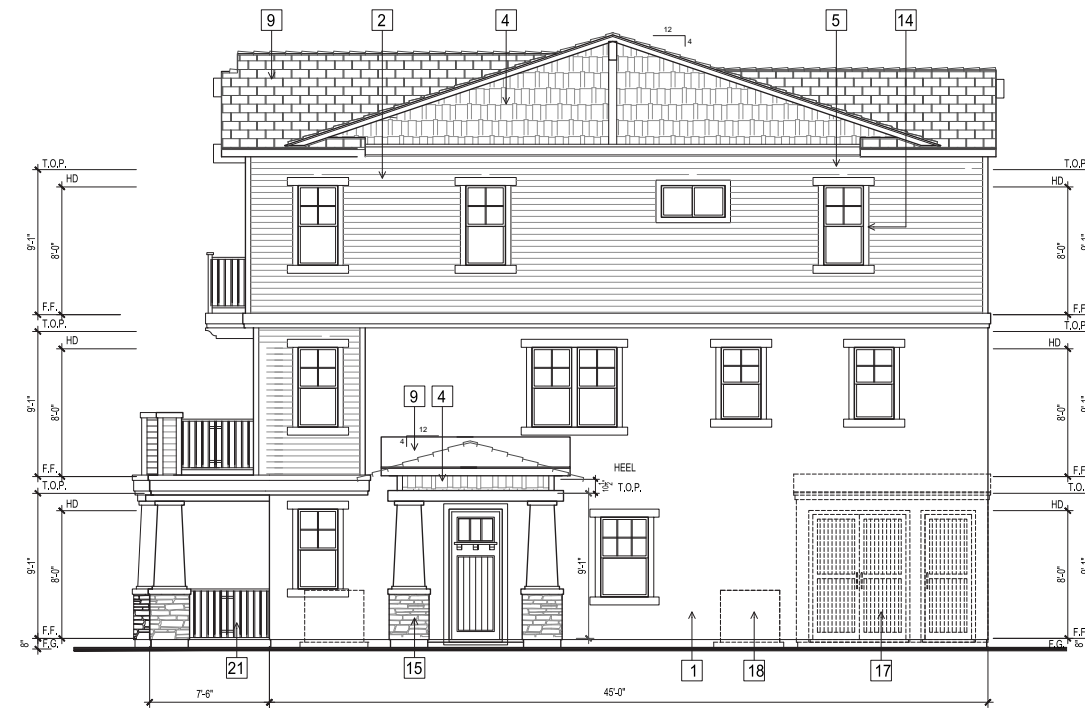


# OAK KNOLL

## BUILDING 8 ELEVATIONS - 5-PLEX CRAFTSMAN

### FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



UNIT 2 (R)  
RIGHT ELEVATION



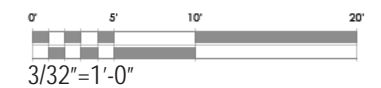
UNIT 3  
LEFT ELEVATION

- 1 STUCCO
- 2 HORIZONTAL SIDING
- 3 BOARD & BATT SIDING
- 4 SHINGLE SIDING
- 5 VINYL WINDOW
- 6 SHUTTERS AT ENHANCED ELEVATION
- 7 WROUGHT IRON RAILING
- 8 STANDING SEAM METAL ROOF
- 9 FLAT CONCRETE TILE ROOF
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ELEVATION KEY NOTES



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# OAK KNOLL

## BUILDING 8 ELEVATIONS - 5-PLEX CRAFTSMAN

### FINAL DEVELOPMENT PLAN - PARCEL 12







UNIT 3  
LEFT ELEVATION



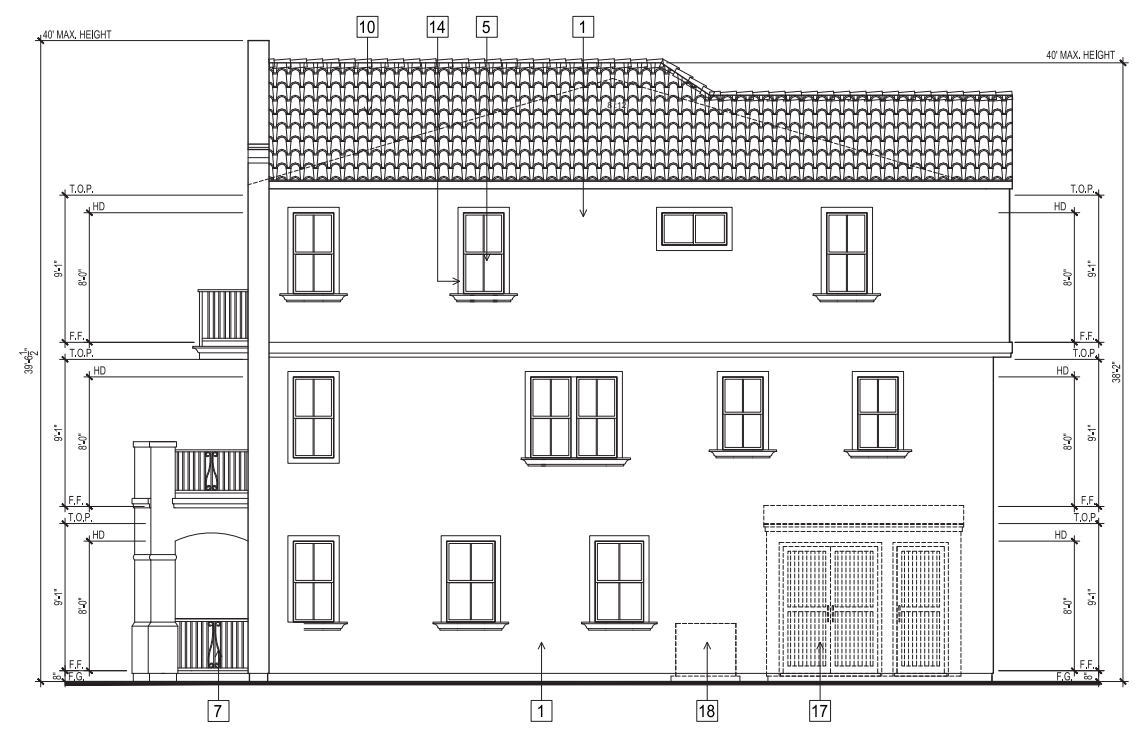
UNIT 3  
UNIT 2 (R)  
FRONT ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
8	STANDING SEAM METAL ROOF
9	FLAT CONCRETE TILE ROOF
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17	UTILITY LOCATION / ROOM TO BE DETERMINED
18	A/C LOCATION
19	PRIVACY FENCE AT END UNIT
20	TILE ACCENT
21	DECORATIVE WOOD RAIL

**ELEVATION KEY NOTES**

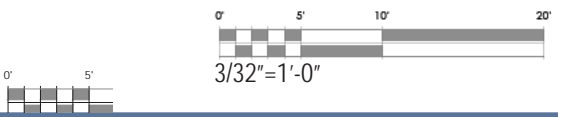


UNIT 2 (R)  
UNIT 3  
REAR ELEVATION



UNIT 2 (R)  
RIGHT ELEVATION

NOTES:  
 1. HABITABLE BUILDING HEIGHT / TOP OF PLATE (T.O.P) NOT TO EXCEED 30 FEET.  
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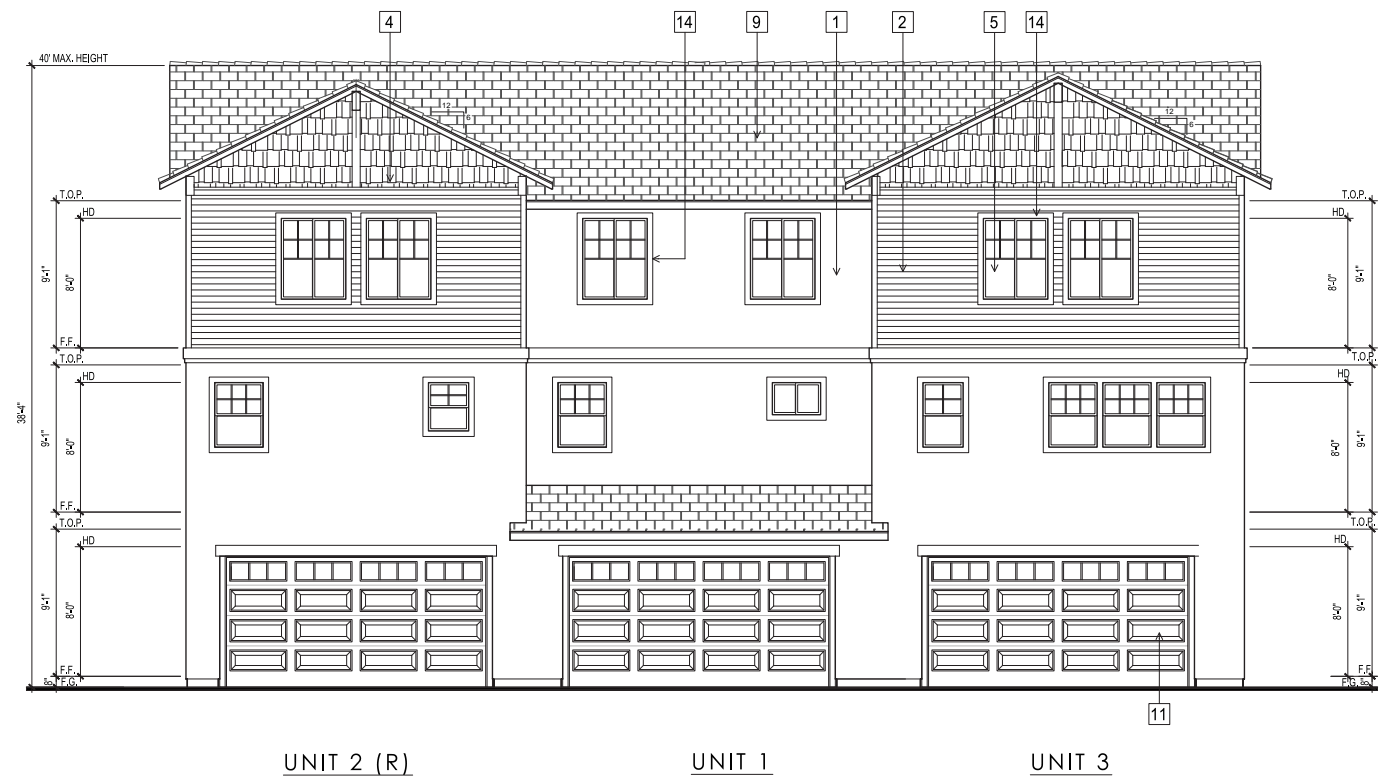
# OAK KNOLL

BUILDING 9 ELEVATIONS - DUPLEX MISSION ENHANCED SIDE  
 FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.



FRONT ELEVATION



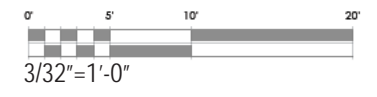
REAR ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
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ELEVATION KEY NOTES



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# OAK KNOLL

## BUILDING 10 ELEVATIONS - TRIPLEX CRAFTSMAN

### FINAL DEVELOPMENT PLAN - PARCEL 12







UNIT 3  
LEFT ELEVATION



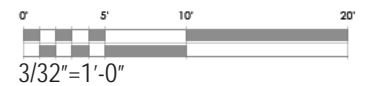
UNIT 2 (R)  
RIGHT ELEVATION

1	STUCCO
2	HORIZONTAL SIDING
3	BOARD & BATT SIDING
4	SHINGLE SIDING
5	VINYL WINDOW
6	SHUTTERS AT ENHANCED ELEVATION
7	WROUGHT IRON RAILING
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ELEVATION KEY NOTES



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# OAK KNOLL

BUILDING 10 ELEVATIONS - TRIPLEX CRAFTSMAN  
FINAL DEVELOPMENT PLAN - PARCEL 12

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.







UNIT 3  
LEFT ELEVATION



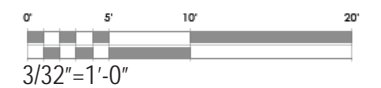
UNIT 2 (R)  
RIGHT ELEVATION

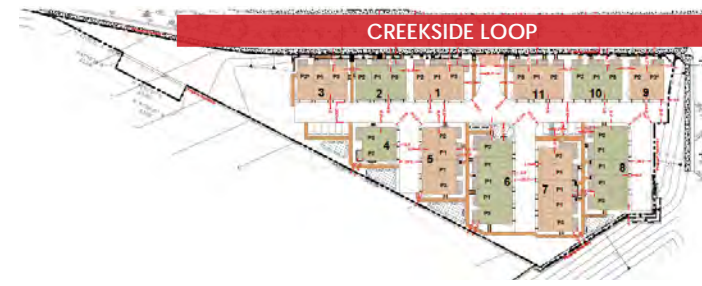
- 1 STUCCO
- 2 HORIZONTAL SIDING
- 3 BOARD & BATT SIDING
- 4 SHINGLE SIDING
- 5 VINYL WINDOW
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ELEVATION KEY NOTES



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Note:  
For more detailed information on retaining wall heights and locations please see the grading and drainage Plan on page 18.

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

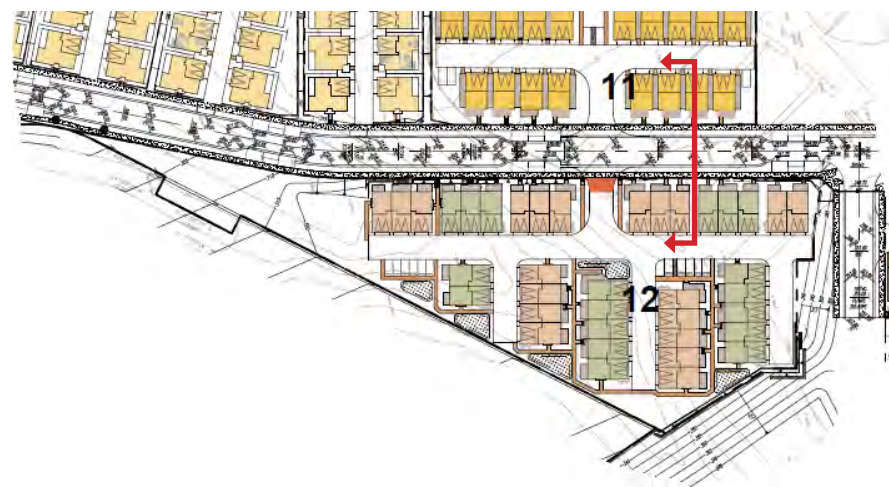
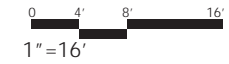
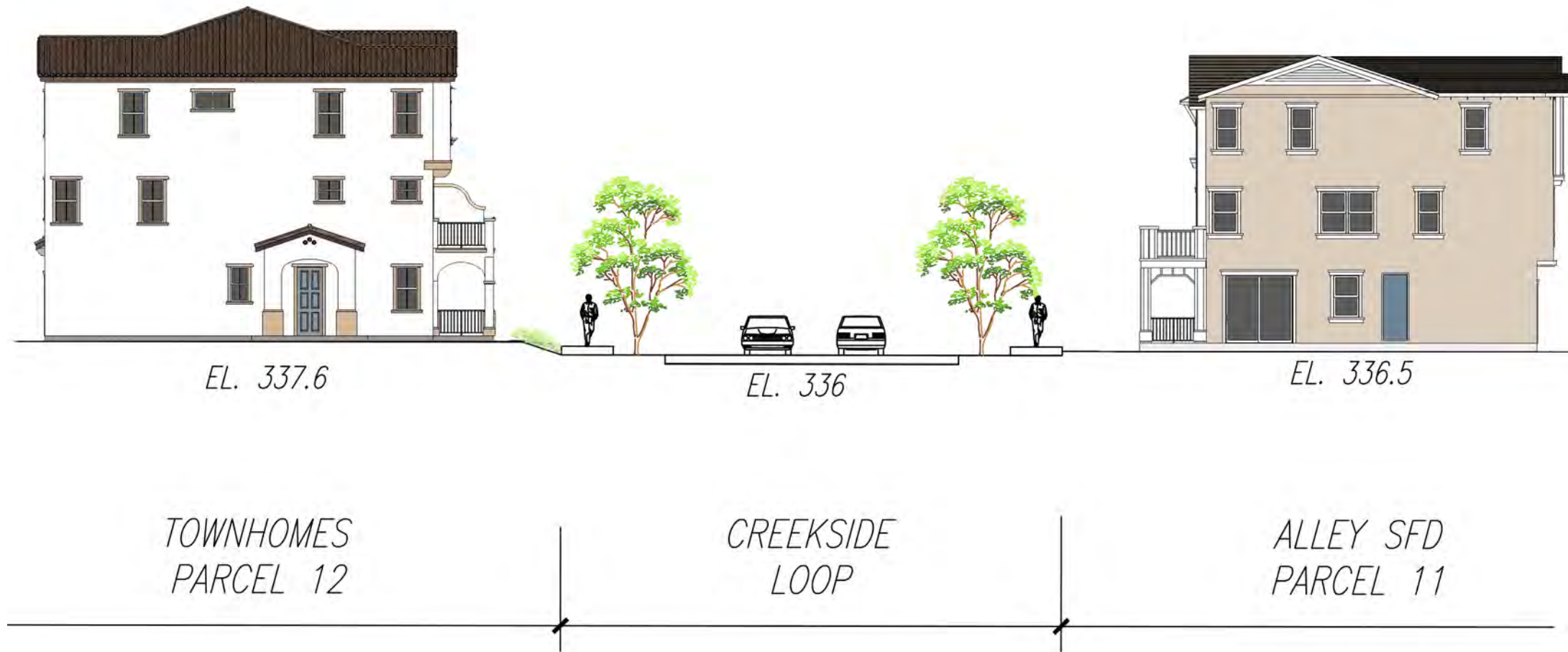
# OAK KNOLL

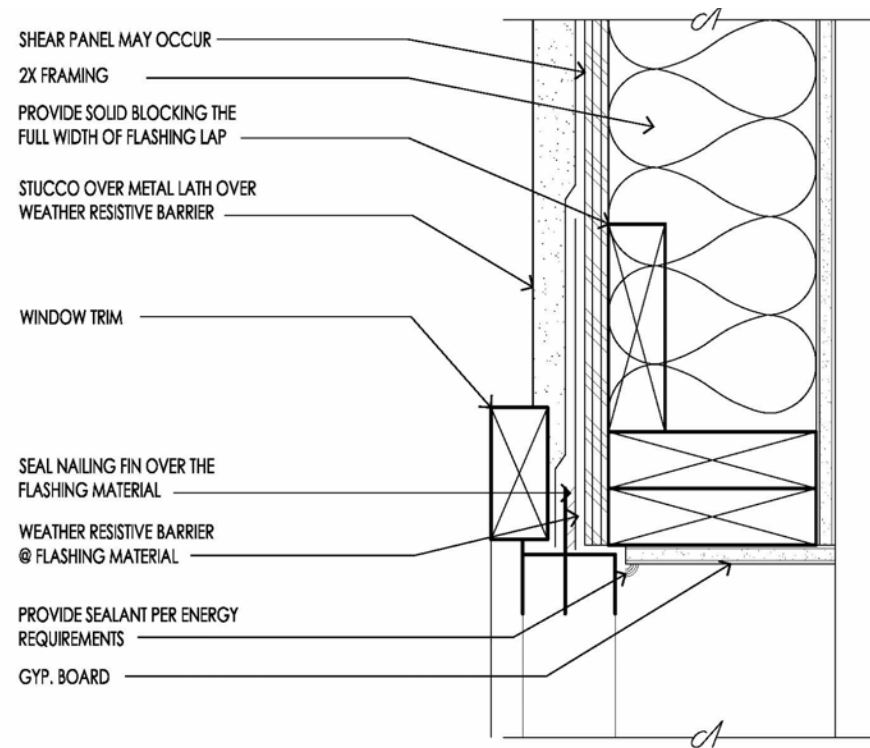
## CREEKSIDE LOOP & STREET "C" STREET SCENE ELEVATION

### FINAL DEVELOPMENT PLAN - PARCEL 12

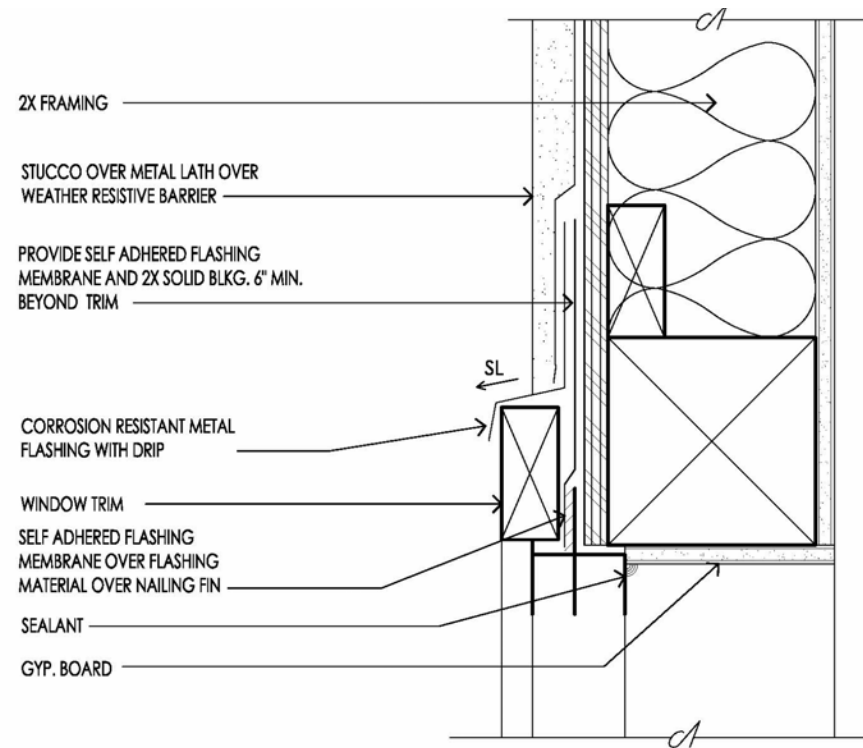




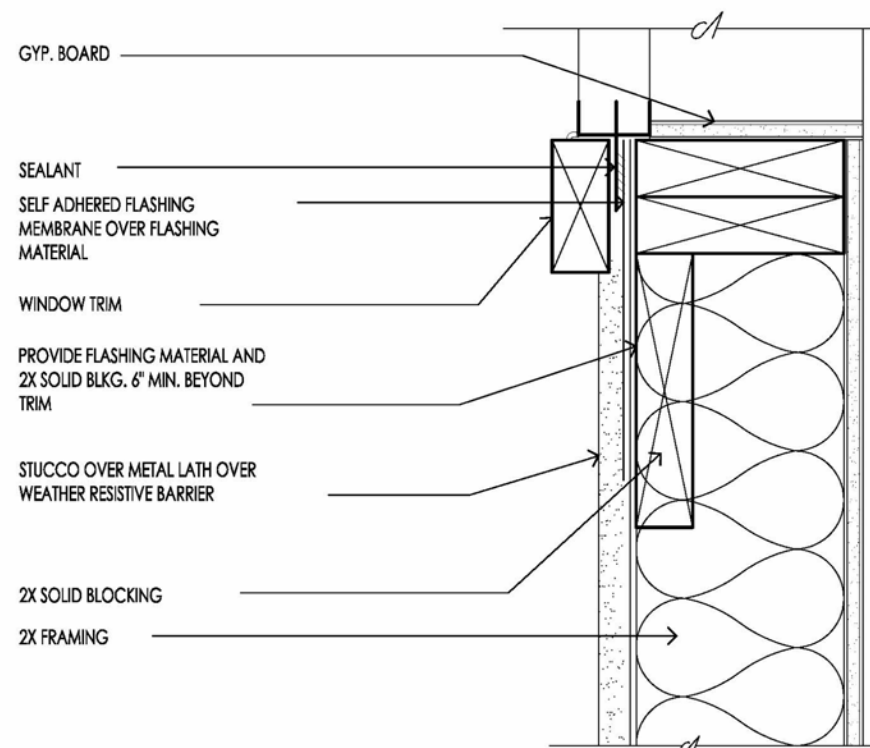




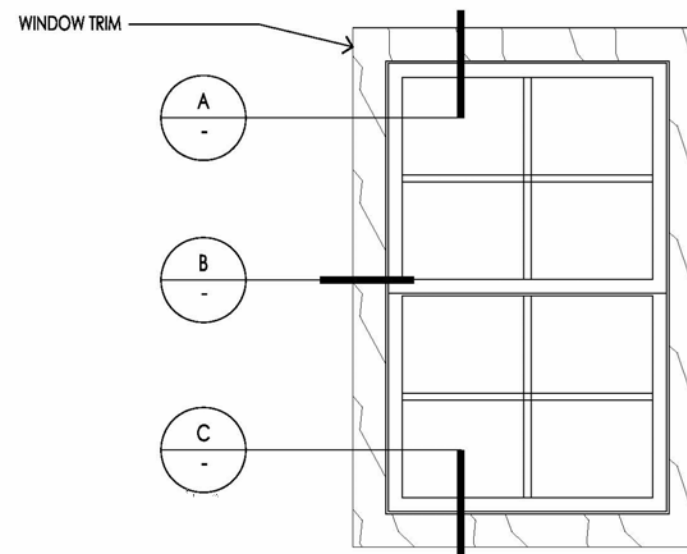
**B** WINDOW JAMB



**A** WINDOW HEADER



**C** WINDOW SILL



**D** WINDOW WITH TRIM  
3 COAT STUCCO

N.T.S.  
DA © 2017 WDW1-VI-F-04

THE FINAL WINDOW MANUFACTURER CONTRACTED BY THE BUILDER AT THE TIME OF CONSTRUCTION PERMITS MUST ADHERE TO THE FOLLOWING REQUIREMENTS BASED ON TITLE 24, LOCAL CODE REQUIREMENTS, CITY REQUIREMENTS AND DESIGN INTENT OF THE ELEVATION STYLE AS FOLLOWS:

WINDOWS MAY BE MULLED TOGETHER TO ACHIEVE WIDER EXPANSES OF GLASS, BUT SHALL NOT EXCEED 12' IN TOTAL WIDTH. WINDOWS MAY HAVE DIVIDED LITES, A 2 OVER 2, 4 OVER 1, 4 OVER 4, 6 OVER 1, OR 6 OVER 6 MUNTIN PATTERN. TRUE DIVIDED LITES ARE PREFERRED, SIMULATED DIVIDED LITES, BETWEEN THE GLASS, ARE ACCEPTABLE, AND REMOVABLE DIVIDED LITES, ON TOP OF THE GLASS, ARE PROHIBITED. WOOD AND COMPOSITE TRIM MATERIALS ARE PERMITTED. FOAM TRIM IS NOT ALLOWED.

Imagery shown is to indicate design intent. Actual floorplans, colors or materials may vary slightly.

# OAK KNOLL

## MATERIALS AND COLORS BOARDS - WINDOWS EXHIBIT

### FINAL DEVELOPMENT PLAN - PARCEL 12







- |    |  |
|----|--|
| 1  | STUCCO                                   |
| 2  | HORIZONTAL SIDING                        |
| 3  | BOARD & BATT SIDING                      |
| 4  | SHINGLE SIDING                           |
| 5  | VINYL WINDOW                             |
| 6  | SHUTTERS AT ENHANCED ELEVATION           |
| 7  | WROUGHT IRON RAILING                     |
| 8  | STANDING SEAM METAL ROOF                 |
| 9  | FLAT CONCRETE TILE ROOF                  |
| 10 | S-TILE ROOF                              |
| 11 | GARAGE DOOR                              |
| 12 | STANDING SEAM METAL CANOPY               |
| 13 | N/A                                      |
| 14 | WINDOW WOOD TRIM                         |
| 15 | STONE VENEER                             |
| 16 | EXTERIOR LIGHTING                        |
| 17 | UTILITY LOCATION / ROOM TO BE DETERMINED |
| 18 | A/C LOCATION                             |
| 19 | PRIVACY FENCE AT END UNIT                |
| 20 | TILE ACCENT                              |
| 21 | DECORATIVE WOOD RAIL                     |
- ELEVATION KEY NOTES**





- 1 STUCCO
  - 2 HORIZONTAL SIDING
  - 3 BOARD & BATT SIDING
  - 4 SHINGLE SIDING
  - 5 VINYL WINDOW
  - 6 SHUTTERS AT ENHANCED ELEVATION
  - 7 WROUGHT IRON RAILING
  - 8 STANDING SEAM METAL ROOF
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  - 21 DECORATIVE WOOD RAIL
- ELEVATION KEY NOTES**

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# OAK KNOLL

## MATERIALS AND COLORS BOARDS

FINAL DEVELOPMENT PLAN - PARCEL 12







- 1 STUCCO
  - 2 HORIZONTAL SIDING
  - 3 BOARD & BATT SIDING
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- ELEVATION KEY NOTES**





- 1 STUCCO
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  - 3 BOARD & BATT SIDING
  - 4 SHINGLE SIDING
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- ELEVATION KEY NOTES**

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# OAK KNOLL

## MATERIALS AND COLORS BOARDS

FINAL DEVELOPMENT PLAN - PARCEL 12



OAK KNOLL  **SunCal**

**Chapter 17.101J - D-OK OAK KNOLL DISTRICT ZONES REGULATIONS****Sections:**

- 17.101J.010 - Title, intent, and description.
- 17.101J.020 - Required design review process, including preliminary and final development plan approval and Oak Knoll Design Guidelines.
- 17.101J.030 - Permitted and conditionally permitted activities.
- 17.101J.040 - Permitted and conditionally permitted facilities.
- 17.101J.050 - Property development standards.
- 17.101J.060 - Subdivision.
- 17.101J.070 - Conditional use permit criteria.
- 17.101J.080 - Other zoning provisions.
- 17.101J.090 – Definitions

**17.101J.010 – Title, intent, and description.**

- A. Title and Intent. The provisions of this Chapter shall be known as the D-OK Oak Knoll District Zones Regulations. The intent of the Oak Knoll District (D-OK) Zones is to implement the Oak Knoll District Planned Unit Development (OKPUD) permit. The OKPUD intends to establish and maintain a pedestrian-oriented neighborhood that contains a mix of housing types, community amenities, a retail area, passive and active recreational opportunities, and open spaces. These regulations shall apply to the six (6) zoning districts (the D-OK zones) in the OKPUD area.
- B. Description of Zones. This Chapter establishes land use regulations for the following six zones:
  1. **D-OK-1 Oak Knoll District Residential Zone - 1.** The D-OK-1 Zone is intended to create, maintain, and enhance areas suitable for low-density single-family home development that responds to the site's topography and includes appropriate landscaping.
  2. **D-OK-2 Oak Knoll District Residential Zone - 2.** The D-OK-2 Zone is intended to create, maintain, and enhance areas suitable for medium-low density single-family homes.
  3. **D-OK-3 Oak Knoll District Residential Zone - 3.** The D-OK-3 Zone is intended to create, maintain, and enhance areas suitable for medium-density residential units, such as townhomes.
  4. **D-OK-4 Oak Knoll District Commercial Zone - 4.** The D-OK-4 Zone is intended to create, maintain, and enhance areas that provide neighborhood-serving retail, such as supermarkets, banks, cafes, and dry-cleaners. Ground floor commercial uses and upper story office uses are encouraged in this zone.
  5. **D-OK-5 Oak Knoll District Amenity Community Commercial Zone - 5.** The D-OK-5 Zone is intended to create, maintain, and enhance areas for community activities and



commercial uses that provide a community amenity. Although this area is intended primarily to serve the community, spaces may be rented for non-community functions, including weddings and other organized events.

6. **D-OK-6 Oak Knoll District Active Open Space Zone – 6.** The D-OK-6 Zone is intended to create, maintain and enhance open space areas that provide opportunities for informal active recreation and park use. The programming of each individual open space will respond to its location and the needs of surrounding residents. This zone is appropriate for lawn and landscaped areas, tot lots, and street furniture, such as benches, tables, and ornamental fixtures.
7. **D-OK-7 Oak Knoll District Passive Open Space Zone - 7.** The D-OK-7 Zone is intended to create, maintain, and enhance open space areas that preserve natural features of the OKPUD area and provide opportunities for passive recreation and maintenance of visual buffers. The programming of each individual open space will respond to its location, natural resources, and topography. This zone is appropriate for management of vegetation and water features, hiking and walking trails, and enhancement of wildlife.

**17.101J.020 – Required Design Review Process, Including Preliminary and Final Development Plan Approval and Oak Knoll Design Guidelines.**

- A. All development within the Oak Knoll District is subject to an approved Planned Unit Development (PUD) permit pursuant to Chapters 17.140 and 17.142.
- B. Development within the District shall be consistent with the Oak Knoll Preliminary Development Plan (as it may be amended), in particular with the Oak Knoll Design Guidelines, which were adopted as part of the Preliminary Development Plan. Conformance to the Oak Knoll Design Guidelines is required for any change to the exterior of a building that requires a building permit in the Oak Knoll District.
- C. In addition, as applicable, development is subject to the following design review requirements:
  1. Original development of more than two lots at the same time shall require approval of, and be consistent with, a Final Development Plan in accordance with the procedures set forth in Chapter 17.140. For development requiring a Final Development Plan, a determination of consistency with the Final Development Plan shall take the place of, and be deemed equivalent to, the design review procedures in Chapter 17.136.
  2. Original development of two or fewer lots at the same time shall be subject to the design review procedures set forth in under 17.136.
  3. Subsequent approvals to existing development shall be subject to design review under Chapter 17.136, except where such projects are exempt from design review pursuant to Section 17.136.025. Where there is a conflict between the design review criteria contained in Section 17.136.070 and the design objectives contained in the Oak Knoll Design Guidelines, the design objectives in the Oak Knoll Design Guidelines shall prevail.
  4. Telecommunications facilities shall be subject to design review in accordance with Chapter 17.128 unless they have already been approved pursuant to a Final Development Plan.
  5. Signs other than those covered by a Final Development Plan shall be subject to design review in accordance with Chapter 17.104.

**17.101J.030 – Permitted and conditionally permitted activities.**

Table 17.101J.01 lists the permitted, conditionally permitted, and prohibited activities in the D-OK Zones. The descriptions of these activities are contained in Chapter 17.10. Section 17.10.040 contains permitted accessory activities.

"P" designates permitted activities in the corresponding zone.

"C" designates activities that are permitted only upon the granting of a Conditional Use permit (CUP) in the corresponding zone (see Chapter 17.134 and Section 17.101J.070 for the CUP procedure and criteria).

"L" designates activities subject to certain limitations or notes listed at the bottom of the table.

"—" designates activities that are prohibited except as accessory activities according to the regulations contained in Section 17.010.040.

**Table 17.101J.01: Permitted and Conditionally Permitted Activities**

Activities	Primary Zones							Additional Regulations
	D-OK-1	D-OK-2	D-OK-3	D-OK-4	D-OK-5	D-OK-6	D-OK-7	
<b>Residential Activities</b>								
Permanent	P	P	P	—	—	—	—	
Residential Care	P(L1)	P(L1)	P(L1)	—	—	—	—	17.103.010
Supportive Housing	P	P	P	—	—	—	—	
Transitional Housing	P	P	P	—	—	—	—	
Emergency Shelter	—	—	—	—	C(L1)(L2)	—	—	17.103.010
Semi-Transient	—	—	—	—	—	—	—	
Bed and Breakfast	—	—	—	—	—	—	—	
<b>Civic Activities</b>								
Essential Service	P	P	P	P	P	P	P	
Limited Child-Care Activities	P	P	P	P	P	—	—	
Community Assembly	C	C	C	C	P	P(L3)	—	
Recreational Assembly	C	C	C	P	C	P(L4)	—	
Community Education	C	C	C	C	C	—	—	
Nonassembly Cultural	C	C	C	P	P	—	—	



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Administrative	—	—	—	P	P	—	—	
Health Care	—	—	—	C(L5)	P(L6)	—	—	
Special Health Care	—	—	—	—	—	—	—	17.103.020
Utility and Vehicular	C	C	C	C	—	—	—	
Extensive Impact	—	—	—	—	—	—	—	
<b>Commercial Activities</b>								
General Food Sales	—	—	—	P	P(L7)	—	—	
Full Service Restaurants	—	—	—	P	C	—	—	
Limited Service Restaurant and Cafe	—	—	—	P	P(L7)	—	—	
Fast-Food Restaurant	—	—	—	—	—	—	—	17.103.030 and 8.09
Convenience Market	—	—	—	C	—	—	—	17.103.030
Alcoholic Beverage Sales	—	—	—	C	C	—	—	17.103.030, and 17.114.030,
Mechanical or Electronic Games	—	—	—	--	—	—	—	
Medical Service	—	—	—	P(L7)	—	—	—	
General Retail Sales	—	—	—	P	P(L7)	—	—	
Large-Scale Combined Retail and Grocery Sales	—	—	—	—	—	—	—	
Consumer Service	—	—	—	P(L8)(L9)	—	—	—	
Consultative and Financial Service	—	—	—	P	—	—	—	
Check Cashier and Check Cashing	—	—	—	—	—	—	—	
Consumer Cleaning and Repair Service	—	—	—	P(L9)	—	—	—	

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Consumer Dry Cleaning Plant	—	—	—	—	—	—	—	
Group Assembly	—	—	—	C	P	—	—	
Personal Instruction and Improvement Services	—	—	—	P	P	—	—	
Administrative	—	—	—	P(L10)	P	—	—	
Business, Communication, and Media Services	—	—	—	P(L11)	—	—	—	
Broadcasting and Recording Services	—	—	—	—	—	—	—	
Research Service	—	—	—	C(L11)	—	—	—	
General Wholesale Sales	—	—	—	—	—	—	—	
Transient Habitation	—	—	—	—	—	—	—	17.103.050
Building Material Sales	—	—	—	—	—	—	—	
Automobile and Other Light Vehicle Sales and Rental	—	—	—	—	—	—	—	
Automobile and Other Light Vehicle Gas Station and Servicing	—	—	—	—	—	—	—	
Automobile and Other Light Vehicle Repair and Cleaning	—	—	—	—	—	—	—	
Taxi and Light Fleet-Based Services	—	—	—	—	—	—	—	
Automotive Fee Parking	—	—	—	—	—	—	—	
Animal Boarding	—	—	—	C	—	—	—	
Animal Care	—	—	—	P	—	—	—	



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Undertaking Service	—	—	—	—	—	—	—	
<b>Industrial Activities</b>								
Custom Manufacturing	—	—	—	P	P(L12)	—	—	17.120
Light Manufacturing	—	—	—	—	—	—	—	
General Manufacturing	—	—	—	—	—	—	—	
Heavy/High Impact	—	—	—	—	—	—	—	
Research and Development	—	—	—	—	—	—	—	
Construction Operations	—	—	—	—	—	—	—	
<b>Warehousing, Storage, and Distribution-Related</b>								
Warehousing, Storage, and Distribution-Related (all categories)	—	—	—	—	—	—	—	
<b>Recycling and Waste-Related</b>								
Recycling and Waste-Related (all categories)	—	—	—	—	—	—	—	
<b>Agriculture and Extractive Activities</b>								
Limited Agriculture	—	—	P(L13)(L14)	P(L13)(L14)	P(L13)(L14)	P(L13)(L14)	P(L13)(L14)	
Extensive Agriculture	—	—	—	—	—	—	—	
Plant Nursery	—	—	—	—	—	—	—	
Mining and Quarrying	—	—	—	—	—	—	—	

**Limitations on Table 17.101J.01:**

**L1.** Residential Care is only permitted in a One-Family Dwelling Residential Facility. No Residential Care or Emergency Shelter Residential Activity shall be located closer than three hundred (300) feet from any other such Activity or Facility. See Section 17.103.010 for other regulations regarding Residential Care and Emergency Shelter Residential Activities.

**L2.** An Emergency Shelter Residential Activity is limited to the temporary housing of displaced people after a catastrophe, including earthquake, fire, flood, or other similar act of God, that makes residences uninhabitable. All other types of Emergency Shelter Residential Activities are prohibited.

**L3.** Community Assembly Civic Activities are allowed only in accordance with the requirements in Section 17.11.060 for parks designated Neighborhood Park (NP). Where Section 17.11.060 is inconsistent with Note L4, below, Note L4 controls.

**L4.** The following Recreational Assembly Civic Activities are permitted: playgrounds and playing fields; basketball courts, tennis courts, handball courts, lawn bowling, leisure areas, and similar outdoor park and recreational facilities; picnic areas; community gardens; dog run areas; and uses accessory to these permitted uses. The following Recreational Assembly Civic Activities are conditionally permitted: food service and other concessions; temporary nonprofit festivals; community outdoor swimming and wading pools, and other water play features; and permanent bleacher seating and outdoor field lighting.

**L5.** In addition to the Conditional Use Permit (CUP) criteria required under Section 17.134.050, as modified by Section 17.101J.070, the following additional criteria also must be met:

1. That the proposal will not interfere with the operations of the surrounding uses;
2. That the proposal will not exceed 5,000 square feet on the ground floor; and
3. That the proposal will not interfere with the movement of people along the sidewalk or pedestrian walkways.

**L6.** Temporary health clinics, including clinics for flu shots, vaccinations, eye health screening, and other similar activities, and temporary blood drives, are permitted. As used in Note L6, “temporary” means an event that lasts no longer than seven days. All other Health Care Civic Activities are prohibited.

**L7.** The total floor area devoted to these activities is limited to 5,000 square feet.

**L8.** See Section 17.102.170 for special regulations relating to massage services. Also, no new or expanded laundromat shall be located closer than five hundred (500) feet from any existing laundromat. See Section 17.102.450 for further regulations regarding laundromats.

**L9.** The total floor area devoted to these activities on the ground floor by any single establishment may only exceed five thousand (5,000) square feet upon the granting of a Conditional Use Permit (see Chapter 17.134 and Section 17.101J.070 for the CUP procedure and criteria).

**L10.** The total floor area devoted to Administrative Activities is limited to 5,000 square feet if located on the ground floor. There is no size limitation for such uses if not located on the ground floor.

**L11.** Except for media services, these activities are not permitted if located on the ground floor of a building.

**L12.** In areas with existing kitchen facilities, Custom Manufacturing Activities that require a kitchen, such as beverage and food production, are permitted. Temporary demonstrations, educational programs, and workshops about any custom manufacturing activity are permitted.



The permanent establishment of Custom Manufacturing Activities that do not require kitchens or in any area that does not contain existing kitchen facilities requires a Major Conditional Use Permit.

**L13.** Other than Community Gardens (see Note L14), Limited Agriculture is only permitted upon the granting of a Conditional Use Permit (see Chapter 17.134 and Section 17.101J.070 for the CUP procedure and criteria). In addition to the CUP criteria contained in Section 17.134.050 and Section 17.101J.070, this activity must meet the following use permit criteria:

1. The proposal will not adversely affect the livability or appropriate development of abutting properties and the surrounding neighborhood in terms of noise, water and pesticide runoff, farming equipment operation, hours of operation, odor, security, and vehicular traffic;
2. Agricultural chemicals or pesticides will not impact abutting properties or the surrounding neighborhood; and
3. The soil used in growing does not contain any harmful contaminants and the activity will not create contaminated soil.

**L14.** Community Gardens are permitted by right if they do not include the cultivation of animals, animal products, and/or livestock production, except for bee keeping involving no more than three (3) hives. The cultivation of animals, animal products and/or livestock production, except for bee keeping involving no more than three (3) hives, is only permitted upon the granting of a Conditional Use Permit (see Chapter 17.134 and Section 17.101J.070 for the CUP procedure and criteria).

**17.101J.040 – Permitted and conditionally permitted facilities.**

Table 17.101J.02 lists the permitted, conditionally permitted, and prohibited facilities in the D-OK Zones. The descriptions of these facilities are contained in Chapter 17.10.

"P" designates permitted facilities in the corresponding zone.

"C" designates facilities that are permitted only upon the granting of a Conditional Use Permit (CUP) in the corresponding zone (see Chapter 17.134 and Section 17.101J.070 for the CUP procedure and criteria).

"L" designates facilities subject to certain limitations listed at the bottom of the Table.

"—" designates facilities that are prohibited.

**Table 17.101J.02: Permitted and Conditionally Permitted Facilities**

Facilities	Zones							Additional Regulations
	D-OK-1	D-OK-2	D-OK-3	D-OK-4	D-OK-5	D-OK-6	D-OK-7	
<b>Residential Facilities</b>								
One-Family Dwelling	P	P	C	—	—	—	—	
One-Family Dwelling with Secondary Unit	P	P	C	—	—	—	—	17.103.080

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Two-Family Dwelling	—	—	P	—	—	—	—	
Multifamily Dwelling	—	—	P	—	—	—	—	
Rooming House	—	—	—	—	—	—	—	
Mobile Home	—	—	—	—	—	—	—	
<b>Nonresidential Facilities</b>								
Enclosed Nonresidential	—	—	—	P	P	C	—	
Open Nonresidential	P	P	P	C	C	P	P	
Sidewalk Cafe	—	—	—	P(L1)	—	—	—	17.103.090
Drive-In	—	—	—	—	—	—	—	
Drive-Through	—	—	—	P(L2)	—	—	—	17.103.100
<b>Telecommunications Facilities</b>								
Micro Tele-communications	C	C	C	C	C	C	C	17.128
Mini Tele-communications	C	C	C	C	C	C	C	17.128
Macro Tele-communications	C	C	C	C	C	C	C	17.128
Monopole Tele-communications	C	C	C	C	C	C	C	17.128
Tower Tele-communications	C	C	C	C	C	C	C	17.128
<b>Sign Facilities</b>								
Residential Signs	P(L3)	P(L3)	P(L3)	—	—	—	—	
Special Signs	P(L3)	P(L3)	P(L3)	P(L4)	P(L3)	P(L3)	—	
Development Signs	P(L3)	P(L3)	P(L3)	P(L4)	P(L3)	—	—	
Realty Signs	P(L3)	P(L3)	P(L3)	P(L4)	P(L3)	—	—	
Civic Signs	P(L3)	P(L3)	P(L3)	P(L4)	P(L3)	C(L3)	—	
Business Signs	—	—	—	P(L4)	P(L3)	—	—	
Advertising Signs	—	—	—	—	—	—	—	



**L1.** Sidewalk cafes are allowed only as an accessory facility to an approved Full Service Restaurant or Limited Service Restaurant and Cafe. The sidewalk cafe may only operate within the hours of 7:00 a.m. to 10:30 p.m. See Section 17.103.090 for other regulations regarding Sidewalk Cafes; however, the regulations in this Section supersede any contradicting regulations in Section 17.103.090.

**L2.** Drive-through facilities are permitted for pharmacy and retail banking uses only. Drive-through facilities are prohibited for all other uses.

**L3.** All signs other than monument signs, which are defined in the OKPUD, shall comply with the regulations in Section 17.104.010. Monument signs shall comply with the regulations in Section 17.101J.020 and the OKPUD.

**L4.** Signs shall comply with the regulations in Section 17.101J.020, 17.104.020, or 17.104.070, as applicable.

**17.101J.050 – Property development standards.**

**A. Zone Specific Standards.** Table 17.101J.03 below prescribes development standards specific to individual zones. The number designations in the "Additional Regulations" column refer to the regulations listed at the end of the Table. "—" indicates that a standard is not required in the specified zone.

**Table 17.101J.03: Property Development Standards**

Development Standards	Zones							Additional Regulations
	D-OK-1	D-OK-2	D-OK-3	D-OK-4	D-OK-5	D-OK-6	D-OK-7	
<b>Minimum Lot Dimensions</b>								
Width mean	42 ft.	40 ft.	20 ft.	25 ft.	25 ft.	20 ft.	N/A	1
Frontage	25 ft.	25 ft.	20 ft.	25 ft.	25 ft.	20 ft.	N/A	2
Lot area	3,750 sf.	2,000 sf.	5,000 sf.	4,000 sf.	4,000 sf.	2,000 sf.	N/A	
<b>Minimum/Maximum Setbacks</b>								
Minimum Front	15 ft./5 ft.	8 ft.	8 ft.	0 ft.	20 ft.	8 ft.	N/A	3, 4, 5, 6, 10
Minimum Interior Side	4 ft./5 ft.	3 ft. per side or a total of 5 ft.	4 ft.	0 ft./5 ft.	20 ft.	4 ft.	N/A	7, 8, 10
Minimum Street Side	5 ft.	5 ft.	5 ft.	0 ft.	20 ft.	0 ft.	N/A	10
Rear	15 ft.	12 ft.	N/A	0 ft./5 ft.	20 ft.	0 ft.	N/A	9, 10
<b>Maximum Density</b>								
Permitted Density	1 primary	1 primary	1 unit per	0.50 FAR	0.50 FAR	0.15 FAR	N/A	11

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	unit per lot	unit per lot	1,600 sf. of lot area on lots 5,000 sf. or greater					
Maximum FAR for Lots with a Footprint Slope >20%	0.65	N/A	N/A	N/A	N/A	N/A	N/A	11
Maximum Lot Coverage (%)	50%	55%	55%	N/A	N/A	N/A	N/A	11
<b>Heights</b>								
Maximum wall height primary building	30 ft. and 2 stories	35 ft. and 3 stories	35 ft.	30 ft.	42 ft.	15 ft.	N/A	12
Maximum pitched roof height primary building	32 ft. and 2 stories	35 ft. and 3 stories	40 ft.	30 ft.	46 ft.	20 ft.	N/A	12
Maximum height for accessory structures	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.	15 ft.	12
Height Regulations for all Lots with a Footprint Slope of > 20% or that are terraced or split	See Table 17.101J.04	N/A	N/A	N/A	N/A	N/A	N/A	
<b>Minimum Open Space</b>								
Group Open Space	N/A	N/A	170 sf. per unit	N/A	N/A	N/A	N/A	13
Courtyard Regulations	N/A	N/A	See Section 17.108.120	N/A	N/A	N/A	N/A	
<b>Other Development Standards</b>								



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Parking and driveway location requirements	Yes	Yes	Yes	Yes	No	No	N/A	14
Ground floor active space requirement	N/A	N/A	N/A	Yes	N/A	N/A	N/A	15
Minimum ground floor façade glazing	N/A	N/A	N/A	40 to 70%	N/A	N/A	N/A	16
Minimum height of ground floor facilities	N/A	N/A	N/A	12 ft.	N/A	N/A	N/A	17
Minimum Required Parking	Yes	Yes	Yes	Yes	No	No	N/A	18

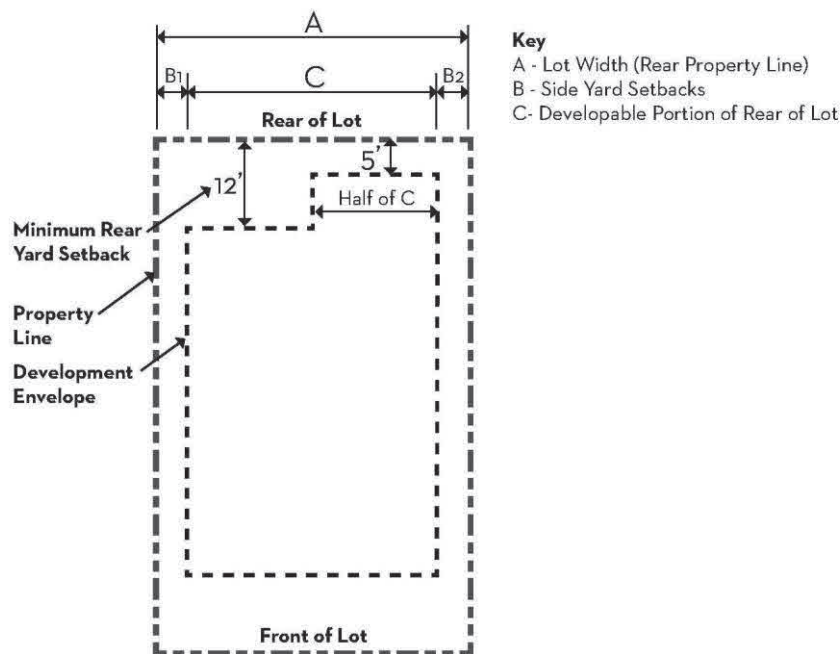
**Additional Regulations for Table 17.101J.03:**

1. No additional subdivisions other than what was approved in the OKPUD are permitted unless a new planned unit development (PUD) application is submitted pursuant to the procedures in Chapter 17.140.
2. The minimum frontage requirement does not apply to flag lots created as part of the original OKPUD.
3. For lots in the D-OK-1 Zone with a footprint slope of less than twenty (20) percent, the minimum front setback is fifteen (15) feet. For lots in the D-OK-1 Zone with a footprint slope equal to or greater than twenty (20) percent, the minimum front setback is five (5) feet.
4. For residences in the D-OK-2 Zone located off of a shared access facility, the minimum front setback is measured from the edge of the pavement of the common access drive.
5. The minimum front setback for a garage is eighteen (18) feet for lots in the D-OK-1 Zone with a footprint slope equal to or less than twenty (20) percent, and lots in the D-OK-3 Zone. In the D-OK-2 Zone, the minimum front setback for a garage is fifteen (15) feet. The minimum front setback for a garage is five (5) feet for lots in the D-OK-1 Zone with a footprint slope greater than twenty (20) percent.
6. In the D-OK-1, D-OK-2, and D-OK-3 Zones, covered porches are permitted in the front setback; in these zones, covered porches not exceeding one story and structures under 30 inches in height shall have a minimum front setback of three (3) feet. Stairs in these zones are permitted up to the front property line.

7. In the D-OK-2 Zone, the minimum interior side yard is three (3) feet except, for a single-family dwelling unit on a lot with two interior side yards, one of the two interior side yards can be reduced to zero (0) feet without a variance if the other interior side yard is at least five (5) feet. On the ground-floor, only a garage of a maximum length of twenty-four (24) feet is allowed on the interior side without a side yard. Habitable rooms are permitted over such a garage. On the interior side without a side yard, no windows, doors or upper floor balconies or decks are permitted less than three (3) feet of the property line.

8. In the D-OK-1 Zone, the minimum interior side yard is four (4) feet on lots less than six thousand (6,000) square feet. The minimum interior side yard is five (5) feet on all other lots in the D-OK-1 Zone. In the D-OK-4 Zone, there is no minimum interior side yard setback unless the interior side lot line is adjacent to a Residential Zone, in which case the minimum side yard setback is five (5) feet.

9. In the D-OK-2 Zone, the minimum rear setback can be reduced to five (5) feet over a maximum of one-half (1/2) the width of the developable portion of the lot (i.e., the width not including the applicable side yard setbacks), as shown in the following figure. In the D-OK-4 Zone, there is no minimum rear setback unless the rear lot line is adjacent to a Residential Zone, in which case the rear setback is five (5) feet.



10. If there is any ambiguity regarding the required setbacks, the setbacks in the Oak Knoll Design Guidelines shall control.

11. Where a Secondary Unit is permitted in the D-OK Zones (i.e., on a residential lot with only one (1) single-family dwelling unit), the requirements in Section 17.103.080 shall apply.

12. See Section 17.108.030 for allowed projections above height limits and Section 17.108.020 for increased height limits for civic buildings. See Table 17.101J.04 for height regulations for all lots with a footprint slope exceeding twenty (20) percent, or that are terraced or split. In the D-OK-5 Zone, the maximum height is measured from the ground level of the building, and the tower of Club Knoll is an allowed projection above the height limit. Also in the



D-OK-5 Zone, for any building other than Club Knoll or a replica of Club Knoll, the height limit is the same as in the D-OK-4 Zone.

**13.** The per unit Group Open Space requirement can be replaced by 70 square feet of dedicated Private Open Space per unit. All usable Group Open Space shall meet the requirements in Sections 17.126.030.A through 17.126.030.E. Usable Private Open Space shall meet the following requirements:

- a. **Usability.** A surface shall be provided which prevents dust and allows convenient use for outdoor activities. Such surface shall be any practicable combination of lawn, garden, flagstone, wood planking, concrete, asphalt, or other serviceable, dust-free surfacing. Slope shall not exceed ten percent (10%). Off-street parking and loading areas, driveways, and service areas shall not be counted as usable open space. Adequate safety railings or other protective devices shall be erected wherever necessary for space on a roof or balcony, but shall not be more than the minimum height required by the Oakland Building Code.
- b. **Location.** The space may be located anywhere on the lot. Above-ground-level space may be counted even though it projects beyond a street line. All spaces shall be adjacent to, and not more than four (4) feet above or below the floor level of, the living unit served.
- c. **Size and Shape.** An area of contiguous ground-level space shall be of such size and shape that a rectangle inscribed within it shall have no dimension less than ten (10) feet. An area of above-ground-level space shall be of such size and shape that a rectangle inscribed within it shall have no dimension less than five (5) feet. When space is located on a roof, the area occupied by vents or other structures which do not enhance usability of the space shall not be counted toward the above dimension.
- d. **Accessibility.** The space shall be accessible to only one living unit by a doorway to a habitable room or hallway.
- e. **Openness.** There shall be no obstructions over ground-level space except for devices to enhance its usability and except that not more than fifty percent (50%) of the space may be covered by a private balcony projecting from a higher story. Above-ground-level space shall have at least one exterior side open and unobstructed, except for incidental railings or balustrades, for eight (8) feet above its floor level.

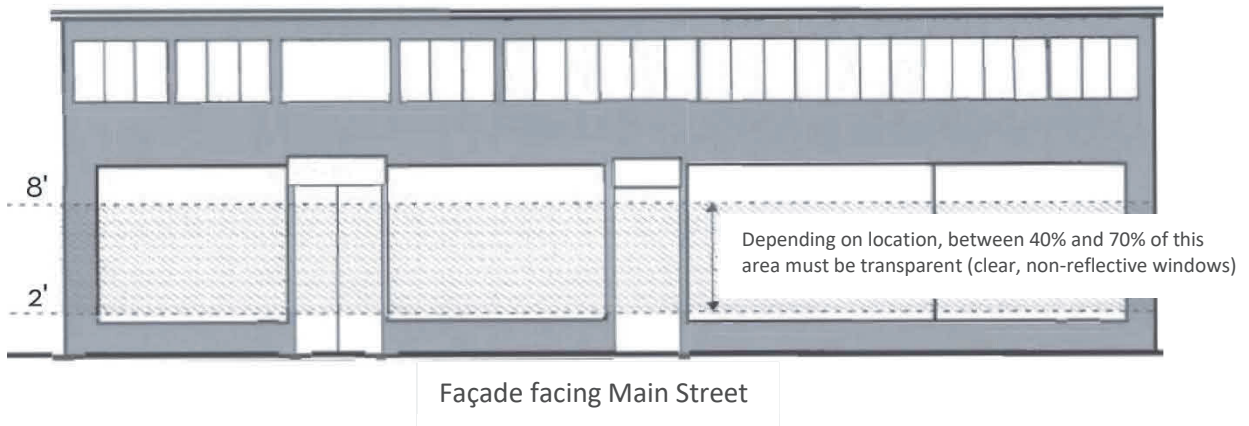
Enclosures and landscaping for both usable Group and Private Open Spaces must be consistent with the requirements in the Oak Knoll Design Guidelines.

**14.** In the D-OK District Residential Zones, Section 2.6 of the Oak Knoll Design Guidelines applies. Parking and loading requirements in the D-OK-4 Zone are the same as in the CN-4 Zone, subject to the exceptions stated in Note 15 below.

**15.** Locker areas, storage areas, mechanical rooms, and other non-active spaces shall not be located within thirty (30) feet from the front of the principal building except for incidental entrances to such activities elsewhere in the building. Exceptions to this regulation may be permitted by the Planning Director for utilities and trash enclosures that cannot be feasibly placed in other locations of the building. Driveways, garage entrances, or other access to parking and loading facilities may be located in front of the principal building, as regulated by Note 12 to Table 17.33.03 in Planning Code Section 17.33.050.A. "Street" parking is allowed along either the principal street or principal private driveway in front of the principal building.

(An example of “street” parking is shown in Figure 1.5 in Chapter 2.3 of the Oak Knoll Design Guidelines.) Parking lots are not allowed in the front of the principal building.

16. The minimum percent transparency for the band between two (2) feet and eight (8) feet in height of any side of a commercial building facing a Principal Drive is forty (40) percent. The minimum percent transparency for the band between two (2) feet and eight (8) feet in height of any side of a commercial building facing a retail plaza is seventy (70) percent. An example of glazing treatment that meets these requirements is illustrated in the figure below.



17. The minimum height requirement applies only for new principal buildings and the height is measured from the sidewalk grade to the ground floor ceiling.

18. In the D-OK-1, D-OK-2, and D-OK-3 Zones, the regulations in Sections 17.116.050, 17.116.060 and 17.116.070 for the RH and/or RD Zones apply. In the D-OK-4 Zone, the regulations in Sections 17.116.050, 17.116.070, 17.116.080, 17.116.090, and 17.116.140 for the CN Zone apply. In the D-OK-5 Zone, the regulations in Sections 17.116.050, 17.116.070, 17.116.080, and 17.116.090, and 17.116.130 for “any other zone” apply. In the D-OK-6 and D-OK-7 Zones, the regulations in Section 17.116.050 through 17.116.100 for the OS Zone apply.

B. **Height Standards for Sloped, Terraced, or Split Lots.** Table 17.101J.04 below prescribes the height standards associated with different sloped, terraced, or split lots. The numbers in the "Additional Regulations" column refer to the regulations listed at the end of the Table.

**Table 17.101J.04 Height Regulations for all Lots with a Footprint Slope of >20%, Terraced, or Split**

Regulation	Downslope Lot Height Regulations With a Footprint Slope of:			Upslope Lot Height Regulations With a Footprint Slope of:	Additional Regulations
	> 20% and < 40%, and all terraced and split regardless of slope	> 40% and < 60%	> 60%	> 20%, terraced and split	



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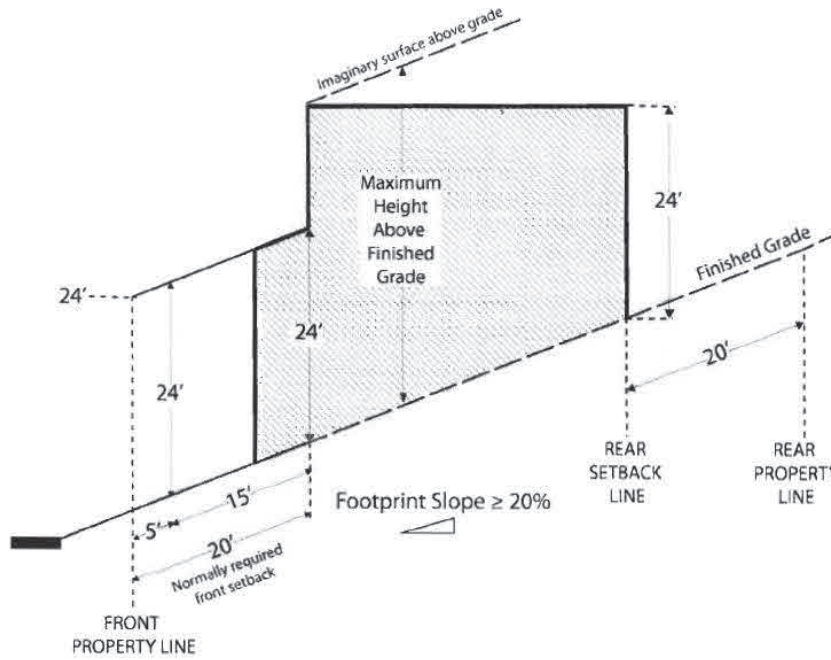
Maximum Height for Detached Accessory Structures	15 ft.	15 ft.	15 ft.	15 ft.	<b>1</b>
Maximum Wall Height Primary Building	32 ft.	34 ft.	36 ft.	32 ft.	<b>1, 2</b>
Maximum Wall Height Primary Building with a CUP	36 ft.	38 ft.	40 ft.	35 ft.	<b>1</b>
Maximum Pitched Roof Height Primary Building	36 ft.	38 ft.	40 ft.	35 ft.	<b>1, 2</b>
Maximum Height Above Edge of Pavement	18 ft.	18 ft.	18 ft.	N/A	<b>1</b>
Maximum Height Above the Ground Elevation at the Rear Setback Line	N/A	N/A	N/A	24 ft.	<b>1</b>
Maximum Height from Finished or Existing Grade (whichever is lower) Within 20' of the Front Property Line	N/A	N/A	N/A	24 ft.	<b>1, 3</b>

**Additional Regulations for Table 17.101J.04:**

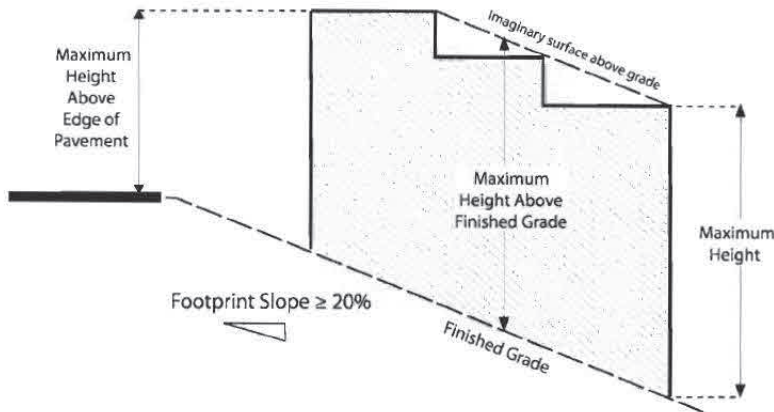
1. See Section 17.108.030 for allowed projections above height limits and Section 17.108.020 for increased height limits for civic buildings.
2. On a downslope lot greater than seventeen percent (20%) footprint slope, or that is terraced or split, the rear wall of an attached garage or carport may exceed the wall height and roof height by five (5) feet, but may not exceed eighteen (18) feet above ground elevation at edge of pavement, if the garage or carport conforms with all of the following criteria:
  - a. Maximum width is twenty-two (22) feet and maximum depth is twenty (20) feet; and
  - b. Garage or carport floor is at the same level as the edge of the street pavement resulting from the project at the center point of the driveway entrance or is at a lower level; and
  - c. Maximum height above the garage or carport floor is ten (10) feet for walls to the top of the plate or flat roof, and twelve (12) feet for pitched roofs (see Illustration for Table 17.17.06 [Additional Regulation 2], below).
3. The building height is measured from finished or existing grade, whichever is lower.

Illustrations for Table 17.101J.04  
 \*for illustration purposes only

Upslope



Downslope



C. **Additional Development Regulations in the D-OK-1 and D-OK-2 Zones.** Table 17.101J.05 below prescribes the standards for garage doors, retaining walls, stoops, and porches in the D-OK-1 and D-OK-2 Zones. The numbers in the "Additional Regulations" column refer to the regulations listed at the end of the Table.



**Table 17.101J.05 Additional Development Regulations  
in the D-OK-1 and D-OK-2 Zones**

Facility	Zones		Additional Regulations
	D-OK-1	D-OK-2	
<b>Garages</b>			
Maximum Garage Door Width	16 ft.	16 ft.	
Maximum Garage Door Height	8 ft.	8 ft.	
Minimum Garage Door Recess	6 in.	6 in.	
<b>Retaining Walls</b>			
Maximum Front Retaining Wall Height	30 in.	30 in.	1
Maximum Side and Rear Retaining Wall Height	--	--	
<b>Stoops</b>			
Minimum Landing Depth	5 ft.	5 ft.	
Minimum Landing Width	6 ft.	6 ft.	
<b>Porches</b>			
Minimum covered area	30 sq. ft.	30 sq. ft.	
Minimum elevation above grade	8 in.	8 in.	

**Additional Regulations for Table 17.101J.05:**

1. Retaining walls, if used in the front, must be set back a minimum of two (2) feet from the sidewalk.

**17.101J.060 – Subdivision**

The OKPUD is approved for 935 residential dwelling units and 82,000 sf. of non-residential space. No additional residential subdivisions are permitted.

**17.101J.070 – Conditional use permit criteria.**

- A. The procedures in Chapter 17.134 apply in the D-OK Zones except for the criterion required by Section 17.134.050.D.
- B. In the D-OK Zones, the following criterion replaces the criterion found in Section 17.134.050.D: That the proposal conforms to all applicable design review criteria set forth in the design review procedure at Section 17.101J.020.C and the Oak Knoll Design Guidelines.

**17.101J.080 – Other zoning provisions.**

- A. Home Occupations. Home occupations shall be subject to the applicable provisions of the home occupation regulations in Chapter 17.112.
- B. Nonconforming Uses. Nonconforming uses and changes therein shall be subject to the nonconforming use regulations in Chapter 17.114.
- C. General Provisions. The general exceptions and other regulations set forth in Chapters 17.102, 17.104, 17.106, and 17.108 shall apply in the D-OK Zones to the extent not contrary to the standards specifically set forth in this Chapter and the Oak Knoll Design Guidelines.
- D. Recycling Space Allocation Requirements. The regulations set forth in Chapter 17.118 shall apply in the D-OK Zones.
- E. Landscaping and Screening Standards. The regulations set forth in Chapter 17.124 shall apply in the D-OK Zones to the extent not contrary to the standards specifically set forth in the Oak Knoll Design Guidelines.
- F. Buffering. All uses shall be subject to the applicable requirements of the buffering regulations in Chapter 17.110 with respect to screening or location of parking, loading, storage areas, control of artificial illumination, and other matters specified therein to the extent not contrary to the standards specifically set forth in the Oak Knoll Design Guidelines.
- G. Bicycle Parking. The bicycle parking requirements in Chapter 17.117 apply in the D-OK Zones.
- H. Special Regulations and Findings for Certain Use Classifications. The regulations set forth in Chapter 17.103 shall apply in the D-OK Zones to the extent noted in Table 17.101J.01.
- I. Performance Standards. The regulations set forth in Chapter 17.120 shall apply in the D-OK Zones.
- J. Hillside Subdivisions. The regulations set forth in Chapter 16.28 shall not apply in the D-OK Zones.

**17.101J.090 – Definitions.**

As used in Chapter 17.101J, the following words have the meanings defined below:

- A. **Developable Area.** The developable area is the lot area excluding required setbacks.
- B. **Footprint Slope.** Footprint slope means the grade across a lot from the front to back (or back to front) of the developable area.
- C. **Principal Drive.** Principal Drive is a private driveway in a commercial area that connects to secondary drives.
- D. **Secondary Drive.** Secondary Drive is a drive aisle located in a surface or structured parking lot.
- E. **Split Lot.** A split lot is a lot that was sloped and has been graded to have two different levels.



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- F. **Terraced Lot.** A terraced lot is a lot that was sloped and has been graded to have more than two levels.



# OAK KNOLL PRELIMINARY DEVELOPMENT PLAN

*September 2017*

Submitted by: Oak Knoll Venture Acquisitions, LLC





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# PREFACE

This document constitutes the Preliminary Development Plan for the Oak Knoll Mixed Use Community Plan. It includes:

- Discussion of General Plan Compliance and proposed Oak Knoll Zoning District
- Preliminary Development Plan
- Design Guidelines

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## OAK KNOLL DESIGN GUIDELINES

Separate companion document

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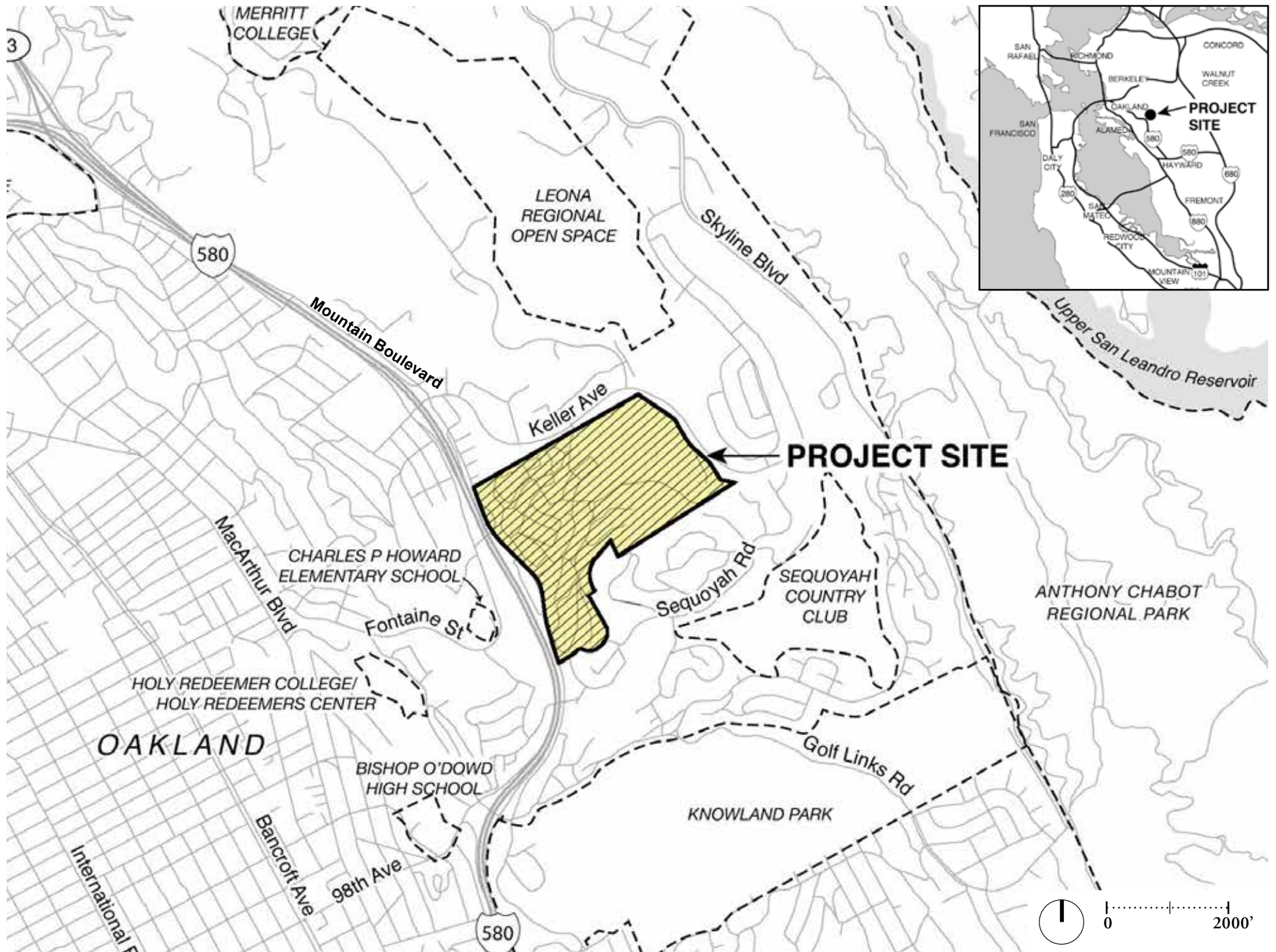


Figure 1 - Project Location and Access

# 1.0 INTRODUCTION

This document constitutes the Preliminary Development Plan (“PDP”) for the Planned Unit Development Permit (“PUD”) for the redevelopment and reuse of the Oak Knoll Naval Medical Center. This document includes the supplemental submittal requirements including a Preliminary Development Plan (“PDP”) required by the Oakland Planning Code (“OPC”) Section 17.140.020. Included as part of this document are the following:

- Discussion of General Plan Compliance
- Discussion of Proposed Oak Knoll Zoning District
- Oak Knoll Design Guidelines (separate document)

The applicant, Oak Knoll Venture Acquisitions, LLC (OKVA), is submitting additional applications to the City of Oakland related to the Oak Knoll PUD application including:

- Application for New Oak Knoll Zoning District
- Tree Removal and Preservation Permit Application
- Creek Protection Permit Application
- Vesting Tentative Map Application
- Development Agreement Application
- Final Development Plan (“FDP”) for Master Development Improvements
- FDP for Club Knoll Relocation and Rehabilitation

According to OPC 17.140.020 the PUD application is to be accompanied by the following:

- PDP showing the character of the entire development (see Section 3.0)
- Land use area tabulations and residential densities (see Table 2)
- Staged development plan/phasing (see Section 3.0)
- Schedule for submission of Final Development Plans (see Section 3.0)

Future development at Oak Knoll will be required to be consistent with the PDP. Design Guidelines have been prepared to accompany the PDP to provide planning, architectural, and landscape design guidance for the overall character and intent of development at Oak Knoll. Design Guidelines are intended to be the roadmap for future designers and builders to follow when submitting Final Development Plans (FDPs) to the City. The exact location, size, configuration and slopes of streets and lots will be controlled by the Vesting Tentative Map (VTM).

## PROJECT LOCATION AND ACCESS

The Oak Knoll Project site is located in the South Hills area of the City of Oakland, California (see Figure 1). The site is located approximately 9 miles southeast of downtown Oakland and 12 miles from the Oakland-San Francisco Bay Bridge. Interstate-580 (I-580) is the nearest regional highway and varies from approximately 100 feet to 600 feet away from the Project site’s curved western boundary (Mountain Boulevard) that parallels the Highway. Highway access to the site is via the Keller Avenue off-ramp and Mountain Boulevard on-and off-ramps to I-580. Access to State Route 13 (SR-13) is located approximately 2 miles north of the site. The Oakland International Airport is located approximately 5.5 miles west of the site.

The Project site is currently accessible via Mountain Boulevard only. Other potential access points from the east-west segment of Keller Avenue and from Sequoyah Road/Barcelona Street to the south do not currently permit access.



## PROJECT CONTEXT AND SETTING

The Project site is located in a largely residential setting in the southeast Oakland hills east of I-580 (See Figure 1). The Project site is bounded by Mountain Boulevard and Interstate 580 (I-580) to the west, Keller Avenue to the north and east, and Sequoyah Road, a City-owned property, and residential neighborhoods to the south. West of Mountain Boulevard is I-580 and a multi-family residential complex of 2 to 3 stories. To the north and east are upland hillside housing composed of both multi-family and single family homes up to two stories. Bordering the site in the northwest corner is Sequoia Community Church which occupies a relatively modern 3-story structure. Near the northeast corner of the site lies a small commercial center, Ridgemont Plaza, which borders the site. Access to Leona Regional Open Space lies approximately 1,000 feet north of Ridgemont Plaza. Neighborhoods to the south are larger lot single family one and two story homes in a wooded setting. An aerial photograph of the Project site and the surrounding vicinity is shown in Figure 2.

Existing site character is described in Section 3.0.

## PROJECT SUMMARY

The Oak Knoll Project site consists of approximately 165 acres of the 183 acre former Oak Knoll Naval Medical Center Oakland (Navy) property, approximately 16 acres of an adjacent property (known as the “Hardenstine parcel”), approximately 1.6 acres of City-owned property, and 1.4 acres of East Bay Municipal Utilities District (EBMUD), for a site with a total size of approximately 186 acres. As currently proposed and described in this document, the Project is an integrated community plan of 918 residential units of varying types, 72,000 square feet of neighborhood commercial development, a 14,000 square foot community center/accessory commercial use building, approximately 85 acres of open space, parks and trails including the restoration of Rifle Range creek, and commensurate roads, infrastructure, and landscaping (See Table 1). The project is described more fully in Section 3.0 Preliminary Development Plan.

**TABLE 1  
KEY OAK KNOLL PROJECT CHARACTERISTICS**

<b>Land Use/Etc.</b>	<b>Proposed Project</b>
Residential	918 dwelling units
Commercial	72,000 sf (square feet)
Open Space	84.7 acres
Club Knoll Relocation/Restoration	14,000 sf Civic/Commercial
Total Site Acreage	185.5
Creek Crossings (auto/pedestrian)	1/1
Trails	3.5 miles
Creek Restoration	17.8 acres



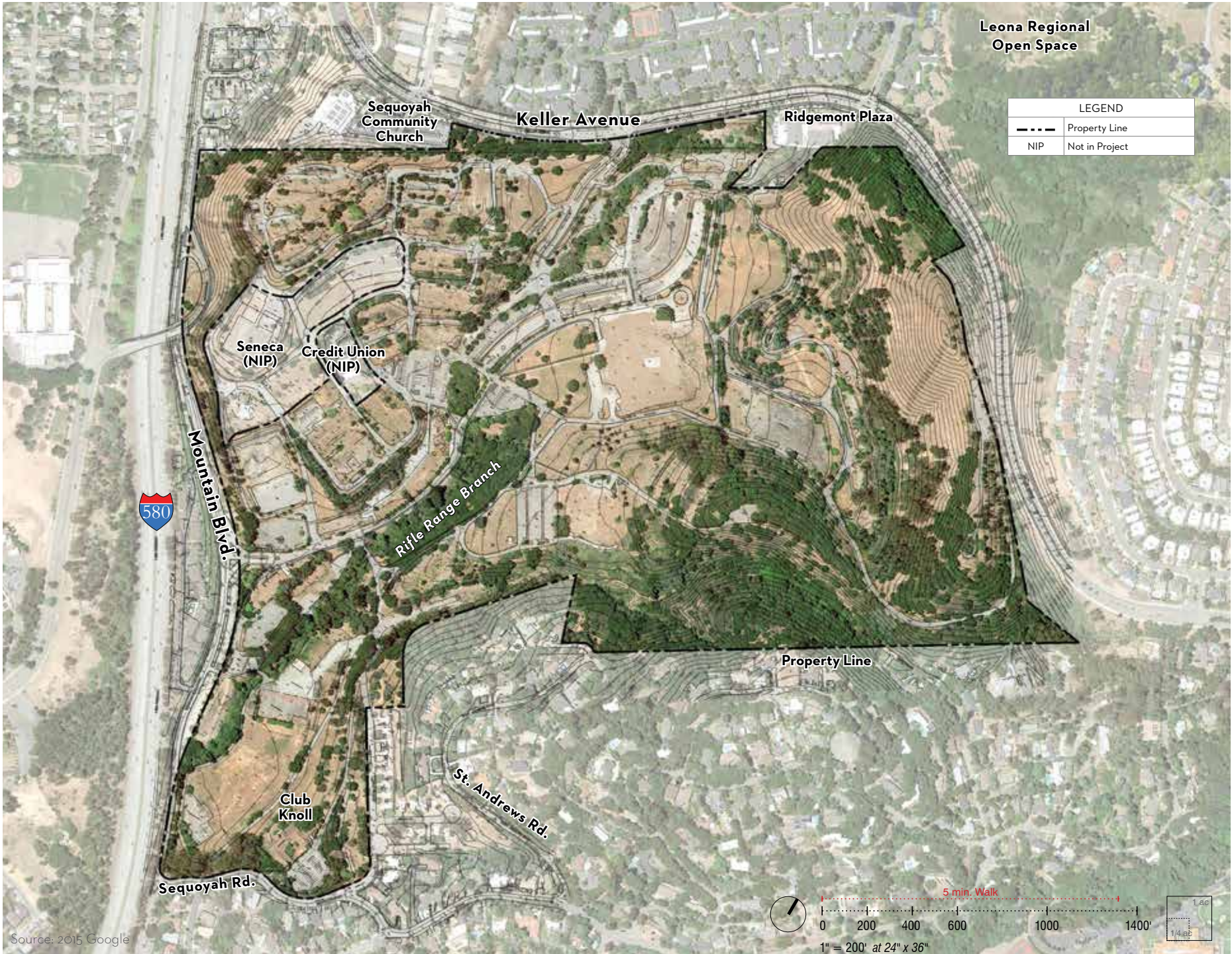


Figure 2 - Existing Conditions



## PROJECT OWNERSHIP

The Project site consists of the following Alameda County assessor parcel numbers (See Figure 3), with current ownership indicated in parentheses:

- 043A-4675-003-21 (OKVA)
- 043A-4675-047-01 (OKVA, Hardenstine Property)
- 048-6870-001 (OKVA)
- 037-3152-008 (City of Oakland)
- 037A-3152-9 (EBMUD)

Two parcels are surrounded by the Oak Knoll project site. These include:

- 043A-4675-003-19 (Seneca Family of Agencies)
- 043A-4675-003-16 (Sea West Coast Guard Federal Credit Union)

These two parcels are not part of the project.

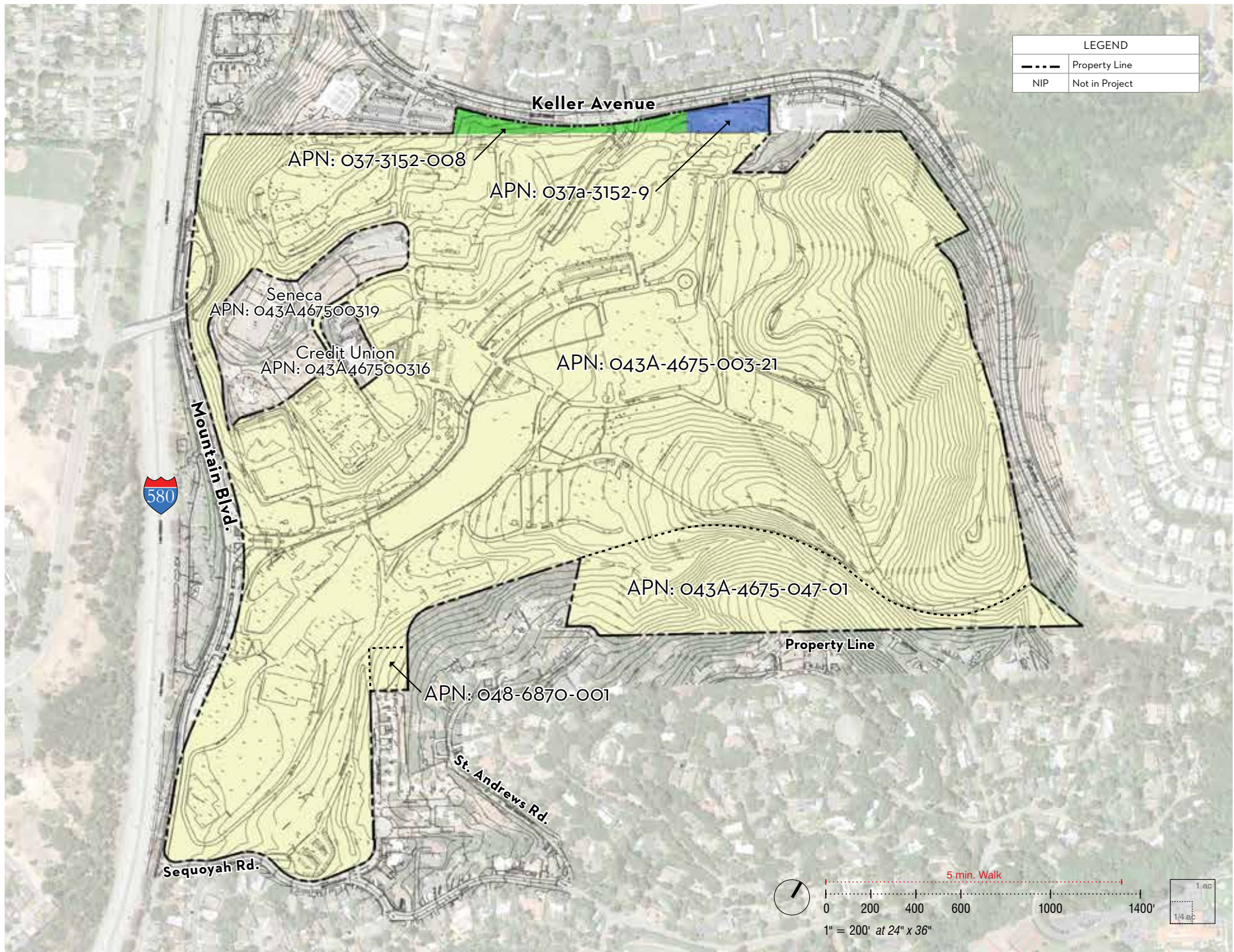


Figure 3 - Ownership



## 2.0 GENERAL PLAN AND ZONING

### PRIOR 2006 GENERAL PLAN CONFORMITY DETERMINATION

In May of 2006, the City of Oakland reviewed the Oak Knoll Mixed Use Community Plan (i.e., the 2006 Oak Knoll Plan) as was then proposed, and determined that the 2006 Oak Knoll Plan was in substantial conformance with the Oakland General Plan land use designations.

#### Uses and Facility Types

The uses and facility types of the 2006 Oak Knoll Plan was found to conform to the Land Use Transportation Element (LUTE) land use plan and diagram. The 2006 Oak Knoll Plan proposed a land use development program that did not change or introduce new or different land use designations for the property other than those shown on the LUTE Land Use Diagram. Boundaries of those land use designations were adjusted in part to reflect site conditions and environmental resources. The LUTE Land Use Diagram is intended to be general and broadly applied to areas without parcel-by-parcel specificity and illustrate the goals and policies of the General Plan. Existing site conditions were found to be in close correspondence with the boundaries proposed under the 2006 Oak Knoll Plan and the 2006 Oak Knoll Plan was found generally consistent with the adopted LUTE Land Use Diagram.

#### Density of Development

The density/intensity of the 2006 Oak Knoll Plan was found to be within the maximum limits set forth in the General Plan. The overall density of proposed residential development under the 2006 Oak Knoll Plan was found to be equal to or less than the density that would be realized under the LUTE land use designations.

- The LUTE Land Use Diagram designates approximately 91 acres of the Oak Knoll property as Hillside Residential, which has a maximum allowable density of 5 units per gross acre, enabling development of approximately 455 residential units. The 2006 Oak Knoll Plan proposed 416 residential units on properties designated Hillside Residential at a gross density of 4.57 dwelling units per acre, below the maximum gross density permitted under the General Plan.
- The LUTE Land Use Diagram also designates approximately 36.4 acres of the Oak Knoll property as Community Commercial, which has a maximum allowable density of as much as 125 units per gross acre, or a calculated density of approximately 167 units per net acre (using an average net-to-gross ratio of 75%). The 2006 Oak Knoll Plan proposed 544 residential units on properties designated Community Commercial at a net density of only 19.9 dwelling units per acre, well below the maximum residential density limits under the Community Commercial General Plan land use designation.

The total of 960 units as was proposed under the 2006 Oak Knoll Plan was found to be within the maximum density limits set forth in the General Plan.

#### General Plan Conformity Guidelines

The 2006 Oak Knoll Plan was found consistent with key General Plan policies cited in City-adopted guidelines, which were used at that time for determining General Plan conformity. The Oakland General Plan is comprised of numerous elements, including but not limited to, the Land Use and Transportation Element (LUTE), the Open Space Conservation and Recreation Element (OSCAR), the Housing Element, Safety Element and the Historic Preservation Element. Both the City's General Plan and case law interpreting general plan requirements recognize that the General Plan is a collection of competing goals and policies, which must be read together as a whole, and not in isolation. In reviewing a project for conformity with the General Plan, the City is required to balance these competing goals and policies. Case law has determined that a project "need not be in perfect conformity with each and every policy" and that "no project could completely

satisfy every policy stated in the General Plan, and state law does not impose such a requirement” (Sequoyah Hills Homeowners Association vs. City of Oakland - 1993).

After reviewing and weighing all pertinent goals and policies of the Oakland General Plan, the City issued a written determination concluding that the 2006 Oak Knoll Plan was in substantial compliance with the General Plan. <sup>(i)</sup>

### 2016 Project Conformity with the General Plan Land Use Diagram

The current 2016 development plan for Oak Knoll differs slightly from the 2006 Oak Knoll Plan, but remains in substantial conformance with the prior 2006 Plan, thereby also meeting the same criteria for General Plan land use consistency.

The density of the current 2016 Oak Knoll Project is within the maximum limits set forth in the General Plan. The total residential development proposed under the current 2016 Oak Knoll Project is 918 residential units, less than the 960 units that were previously proposed in the 2006 Oak Knoll Plan and that were found to be within the maximum limits set forth in the General Plan.

Nonetheless, planning staff and the applicant believe it prudent to modify the General Plan Land Use to provide better guidance for future planning efforts. The proposed amendments to the boundaries of the existing classifications and the addition of two new land use classifications reflect three main changes to the Plan since 1998 and 2006: the addition of the 15-acre Hardenstine parcel to the project area, the relocation of the Club Knoll to the center of the site, and the desire by City staff to re-designate areas shown as Community Commercial on the 1998 diagram where townhomes and garden court homes are proposed under the 2017 Plan. These changes are briefly discussed below:

- The addition of the 15-acre Hardenstine parcel to the project site will allow a significant increase in the number of acres to be permanently preserved as open space and allows the adjustment of the site’s Urban Open Space designation to include the Hardenstine parcel, which had previously been designated as Hillside Development. This modification is consistent with OSCAR Policy OS-1.3, which states that “on large sites with subdivision potential, generally conserve ridges, knolls and other visually prominent features as open space.

Maintain development regulations which consider environmental and open space factors such as soil stability, plant and animal resources, earthquake and fire hazards, and visual impacts, in the determination of allowable density.” The Hardenstine parcel is a visually prominent, wooded, undisturbed area located along one of the project area’s lower ridgelines. It contains a stand of oak woodlands, and provides habitat to common wildlife species. The preservation of these 15 acres as open space better reflects the goals of OSCAR Policy OS-1.3 than providing fewer acres of open space near the corner of Mountain Blvd. and Sequoia Drive. Designating the Hardenstine parcel as open space and redesignating the area near the corner of Mountain and Sequoia for housing also reflects the current project’s proposed relocation of Club Knoll, discussed below, from this corner to the center of the site.

- The 2017 project proposes the relocation of Club Knoll from the southwestern corner of the project to a more central location in the development to better serve the new community, to provide an important focal point and visual amenity along the creek and to reduce potential land use conflicts with adjacent neighboring residential uses. This modification is also consistent with the General Plan Policy LU Policy N. 7 .1, Ensuring Compatible Development and LU Policy N5.2, Buffering Residential Areas.
  - As did the 2006 project, the 2017 project proposes townhomes in the area designated Community Commercial on the 1998 Land Use Diagram. Even though townhomes are allowed in areas designated Community Commercial, planning staff believes it prudent to redesignate these areas with a land use classification that more closely matches the proposed residential zoning for these areas. When Final Development Plans are submitted for these areas in the future, it will be clear that the City’s vision for these areas is not commercial but is residential.
- (i) Due to the economic downturn, the project sponsor withdrew the 2006 Oak Knoll Plan from further review in 2007. In 2011, at its own initiative, the City included the project site its City-wide zoning update effort but expressly recognized that the zoning districts it approved in 2011 (RH-4, with some pockets of RH-3) were intended to be “place-holder” zoning until there was a specific development plan to be implemented. See Draft SEIR at pp. 4.9-16-17.



## PROPOSED ZONING DISTRICT

The Project proposes rezoning the Project site to better match the broad mix of land use types allowed under the General Plan Land Use Diagram, to accurately reflect the Oak Knoll PDP's proposed Land Use Plan, and to codify the specific development standards for new development under the Oak Knoll PUD permit.

### New D-OK District Sub-Zones

The proposed rezoning would create site-specific D-OK Oak Knoll District Zone with a series of sub-zones. The intent of the regulations associated with the new Oak Knoll District Sub-zones are to:

- create, maintain, and enhance residential areas characterized by single family homes and townhouses at a mix of densities,
- allow different types and character of development where appropriate on the site,
- provide for appropriately scaled retail and commercial uses in a village center setting,
- allow neighborhood and community-serving assembly uses in the relocated and rehabilitated Club Knoll building, and
- identify areas best suited for recreation and conservation and permanently protect these areas as parks and open space.

The provisions of the new D-OK zoning district are similar to the City's existing RH-4, RM-4, CN-4, and OS zoning districts but with development standards that better accommodate the mix of development proposed by the Project. For example, the new D-OK zoning district will enable the Project to include a variety of residential development types. In addition, one or more of the D-OK sub-zones would allow commercial uses such as the neighborhood retail envisioned in the Village Center and any accessory commercial uses in the relocated and rehabilitated Club Knoll building. At least one of the D-OK sub-zones would result in generally smaller lots with smaller setbacks, and enable zero-lot line development. The proposed zoning district sub-zones are shown in Figure 4.

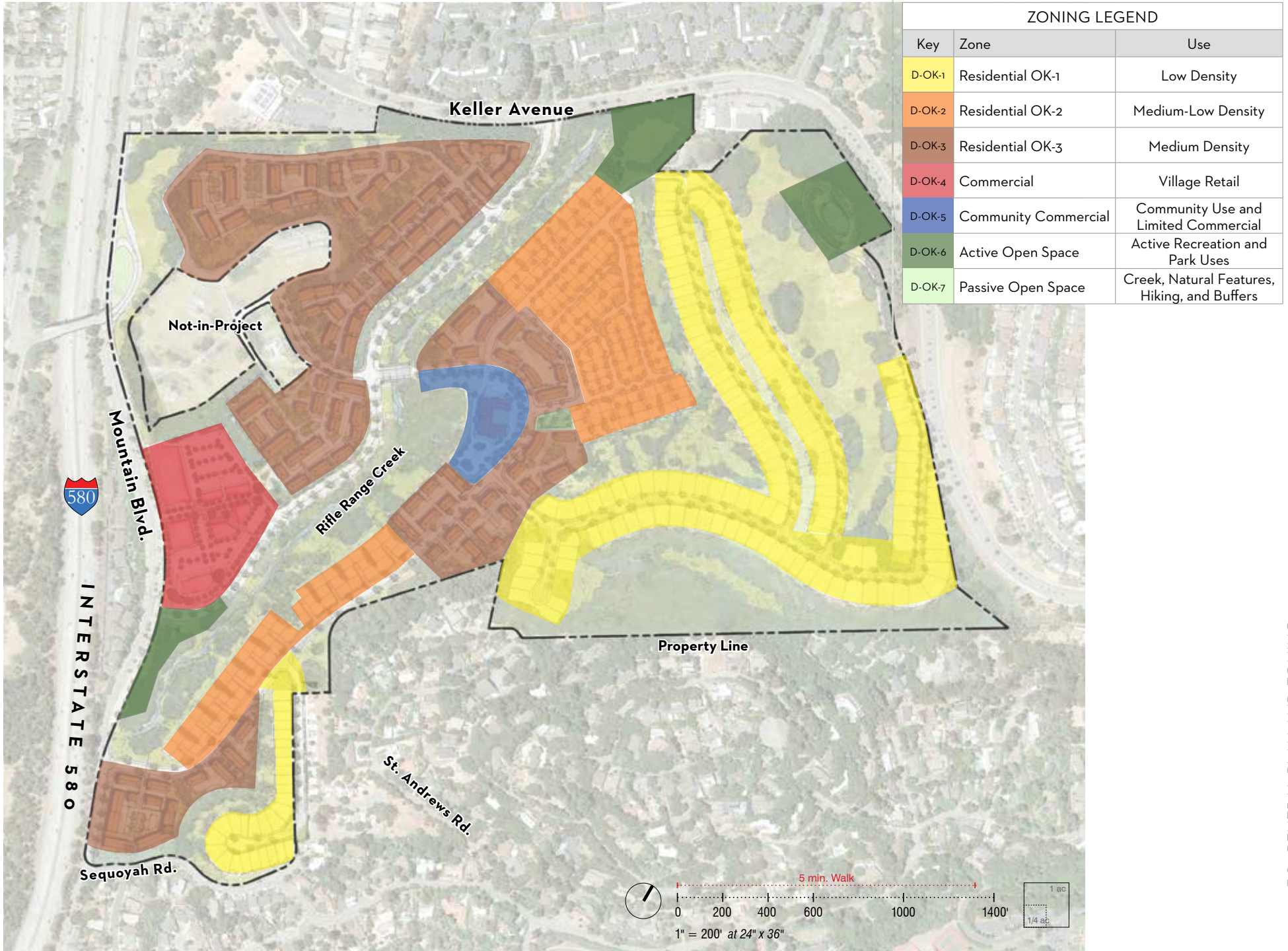


Figure 4 - Proposed Oak Knoll Zoning District



## 3.0 PRELIMINARY DEVELOPMENT PLAN

### EXISTING SITE CHARACTER

The 190.9 acre Oak Knoll site has varied terrain and features including remnants of past Navy activity. Rifle Range Creek, a tributary of Arroyo Viejo Creek, flows from north to south through the central portion of the property, and is one of the most prominent natural features of the Project site. Through the north-central portion of the site, the Creek runs through an approximately 900-foot-long pipe beneath roadway and other paved areas. Two secondary drainages, Powerhouse Creek and Hospital Creek, join Rifle Range Creek in the east portion of the site.

Much of the Project site consists of hilly terrain with oak, eucalyptus, Monterey pine, and riparian and annual grassland habitats. Three ridge and hillside areas distinguish the site: 1) a broad ridge situated between Mountain Boulevard and Rifle Range Creek (which includes a prominent knoll at the northwest corner of the site); 2) a narrow ridge situated near the southern property line (which includes a second prominent knoll); and 3) a prominent ridge near the Project site's eastern property line.

In general, topography on the Project site is downsloping toward the west, from the Eastern Ridge. Elevations onsite range from a low of approximately 222 feet where Rifle Range Creek discharges from the site at Mountain Boulevard, to a high of about 665 feet on the Eastern Ridge. Most of the topography on the site has been altered by previous grading and slopes as steep as 1:1 (horizontal:vertical) have been created.

Although most of the Navy facilities on the site have been demolished and removed some features remain including:

- Club Knoll clubhouse
- Parking lots, pathways, and a pedestrian bridge over Rifle Range Creek
- Dormant underground utilities and piping

- Facilities associated with the Seneca and Navy Credit Union parcel which are not included in the project area.

The existing site character is depicted in photographs shown in Figure 5.

### PROJECT VISION

The community at Oak Knoll is planned as a walkable collection of neighborhoods anchored by a village commercial center, neighborhood parks, a relocated and rehabilitated Club Knoll building, and natural accessible open space. The primary landscape features around which the land plan is shaped is the restored branch of Rifle Range Creek and the surrounding upland areas to the north, south and east of the Creek. Neighborhoods are woven together through an extensive system of trails and carefully designed streetscapes and "Complete Streets." (See Circulation and Complete Streets).

### COMMUNITY MASTER PLAN

The overall community plan is organized to preserve major open space features such as the Oak Knoll on the eastern edge of the site and open spaces areas that flank the southeastern and northwestern corners of the site. An illustrative rendition of the master plan is shown in Figure 6. The project site is moderately sloped in the southern and western portions. These areas are most suitable for the village commercial center adjacent to the project entry along Mountain Boulevard. Higher density townhomes and small lot single family development are located within walking distance to the Village commercial center, restored Creek trails and open space and the Project's community center/clubhouse. See proposed project renderings at the end of Section 3.0

The existing Club Knoll building will be relocated to the center of the site and act as a focal point for the community. The building will be rehabilitated for community, HOA, and community commercial uses.

Per the City of Oakland's Public Art Ordinance the project will also include a public art component. This is discussed later in this Section.



Figure 5 - Existing Site Photos





Figure 6 - Illustrative Master Plan



## PROJECT LAND USE AND PROGRAM

The project program includes 918 residential units and 72,000 square feet of commercial uses. An additional 10,000 square feet of community related commercial and 4,000 of community/HOA space may occupy the relocated Club Knoll building. Table 2 contains the Land Use and Density tabulations for the proposed project. Figure 8 shows the distribution of land uses on the site and proposed residential densities.

Land uses are organized such that the village commercial area and community center area (Club Knoll) are centrally located in the lower areas of the site. Both areas are accessible to Mountain Boulevard and Keller Avenue via collector streets. Adjacent to the Village Center and Community Center are medium density townhome developments and small lot single family neighborhoods to the east. Lower density single family residential areas occupy the upland areas of the site to the east and south. Protected open space, trails, and parks are woven throughout the site and are discussed later in this section.

Using a factor of 2.77 people per household based on Alameda County's average persons per household (U.S. Census, 2010), the site would have an approximate build-out population of 2,590 persons.

### Village Center Commercial Areas

The Village Center commercial area represents the focal point of the Oak Knoll community and provides retail, services, a plaza, and space for community events (see Figure 7). It is connected to both transit, trails, and bike trails. The retail program may include a grocer, convenience retail, small office medical, restaurant(s), bank, and office uses. The Village Center will provide parking for commercial and public use. 72,000 square feet of gross leasable space is planned for the Village Center.

The character and organization of the Village Center is discussed in more detail in the Design Guidelines.



Figure 7 - Illustrative Rendering of Village Center



**TABLE 2**  
**LAND USE AND DENSITY**

Land Use	Density	Gross Area (Acres)	Net Area (Acres)	Units	Net Density <sup>(1)</sup> (du/ac)	Floor Area (SF) (approx.)
<b>RESIDENTIAL</b>						
<b>Typical Single Family Detached Lots</b> 42' x 90', 45' x 100', 55' x 90', 60' x 100'	Low	30.7	22.2	170	7.7	475,000
<b>Small Lot Single Family Detached</b> 42' x 50', 50' x 60'	Medium-Low	17.1	12.5	179	14.3	400,000
<b>Townhomes</b> 18'-25' Wide	Medium	33.3	33.3	569	17.1	1,150,000
<b>subtotal</b>		<b>81.1</b>	<b>68.0</b>	<b>918</b>	<b>13.5</b>	<b>2,025,000</b>
<b>COMMERCIAL/COMMUNITY FACILITIES</b>						
<b>Village Center</b>			8.4			72,000
<b>Community Center</b>			2.8			14,000
<b>subtotal</b>			<b>11.2</b>			<b>86,000</b>
<b>OPEN SPACE</b>						
<b>Public Parks</b>			7.6			
<b>Undisturbed Open Space</b>			39.0			
<b>Revegetated Slope Banks</b>			20.3			
<b>Restored Creek Corridor</b>			17.8			
<b>subtotal</b>			<b>84.7</b>			
<b>STREETS</b>						
<b>Collector Streets</b>			10.1			
<b>Neighborhood Streets</b>			11.5			
<b>subtotal</b>			<b>21.6</b>			
<b>SITE TOTAL</b>			<b>185.5</b>			<b>2,111,000</b>

Note (1) Net density is defined in Section 17.142.030 of the OPC as "land area within the development, excluding publicly dedicated streets, freeways, alleys and paths... land which is specifically devoted to non-residential facilities." Open space, parks and commercial areas are also excluded.

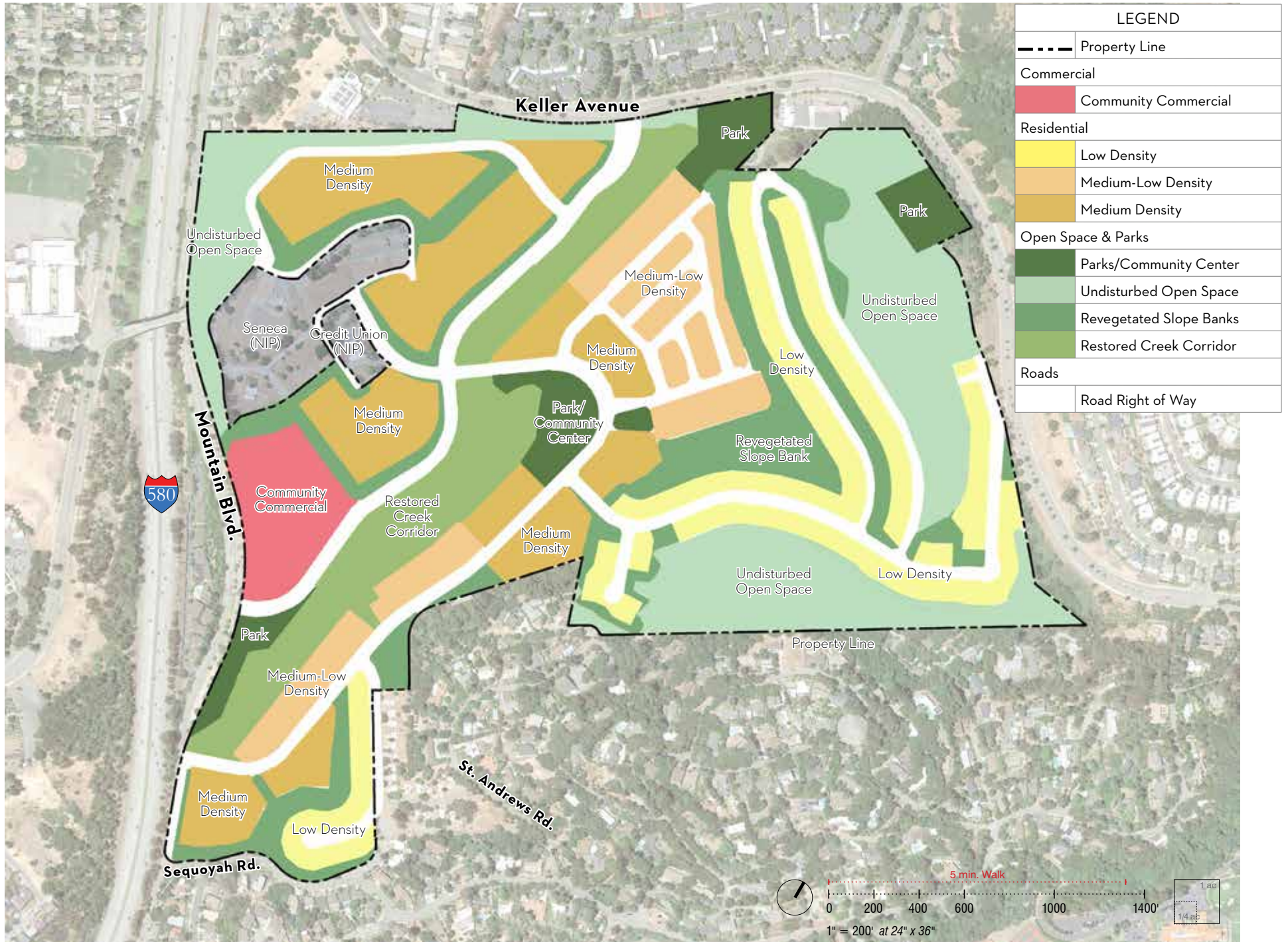


Figure 8 - Proposed Land Use



### Relocation and Rehabilitation of Club Knoll

The Project proposes to relocate and rehabilitate the key historical portions of Club Knoll, a locally-designated historic resource constructed in 1924 as a clubhouse to the Oak Knoll Golf and Country Club and then used as an Officer's Club when the site was used as naval hospital. Club Knoll is located in the southwestern part of the Project site near Sequoyah Road (the site's southern boundary) and is currently in disrepair, having been vacant for many years. See Figure 9 for the existing location of Club Knoll and proposed relocation site. Also shown is a concept site plan and aerial illustrations of the future rehabilitated building.

The proposed Project would relocate and restore the key historic portions of Club Knoll in a central and publicly prominent portion of the site and reuse it as a community center with accessory commercial uses. The portions of the building to be relocated include the main hall, dining hall, lobby/mezzanine areas, building wings, courtyard and cupola. The components of the building proposed for demolition include the basement, and the additional third wing used for administrative/office purposes. The garage, which is not considered to be a historic resource, would also be demolished.

The Club Knoll building may provide space for community use, HOA offices and activities, and accessory commercial uses. Commercial uses may include a health club/fitness center, cafe or coffee/juice bar. The large rooms provided in the rehabilitated Club Knoll building may provide space for programmable activities which may include:

- Resident activities such as meetings, programmed events, and social activities
- Space available on rental basis for both residents and non-residents for events such as weddings and Bar Mitzvahs.

Outdoor activities and uses at the community center may include:

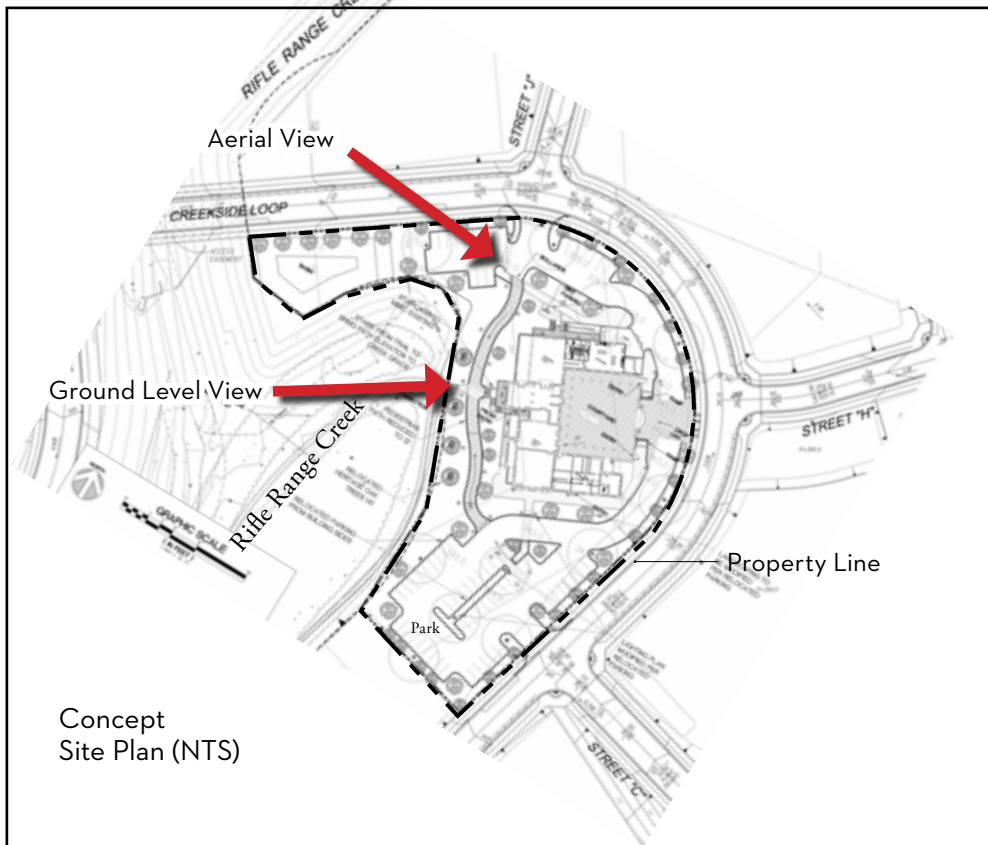
- Events and dining in a picnic/patio space
- Picnic and seating areas
- Potential public art location(s)
- Stormwater management



Aerial View from Northwest



Ground Level View from West



Concept Site Plan (NTS)



Location of Existing Building and Relocation Site (NTS)

Figure 9 - Club Knoll Relocation and Rehabilitation



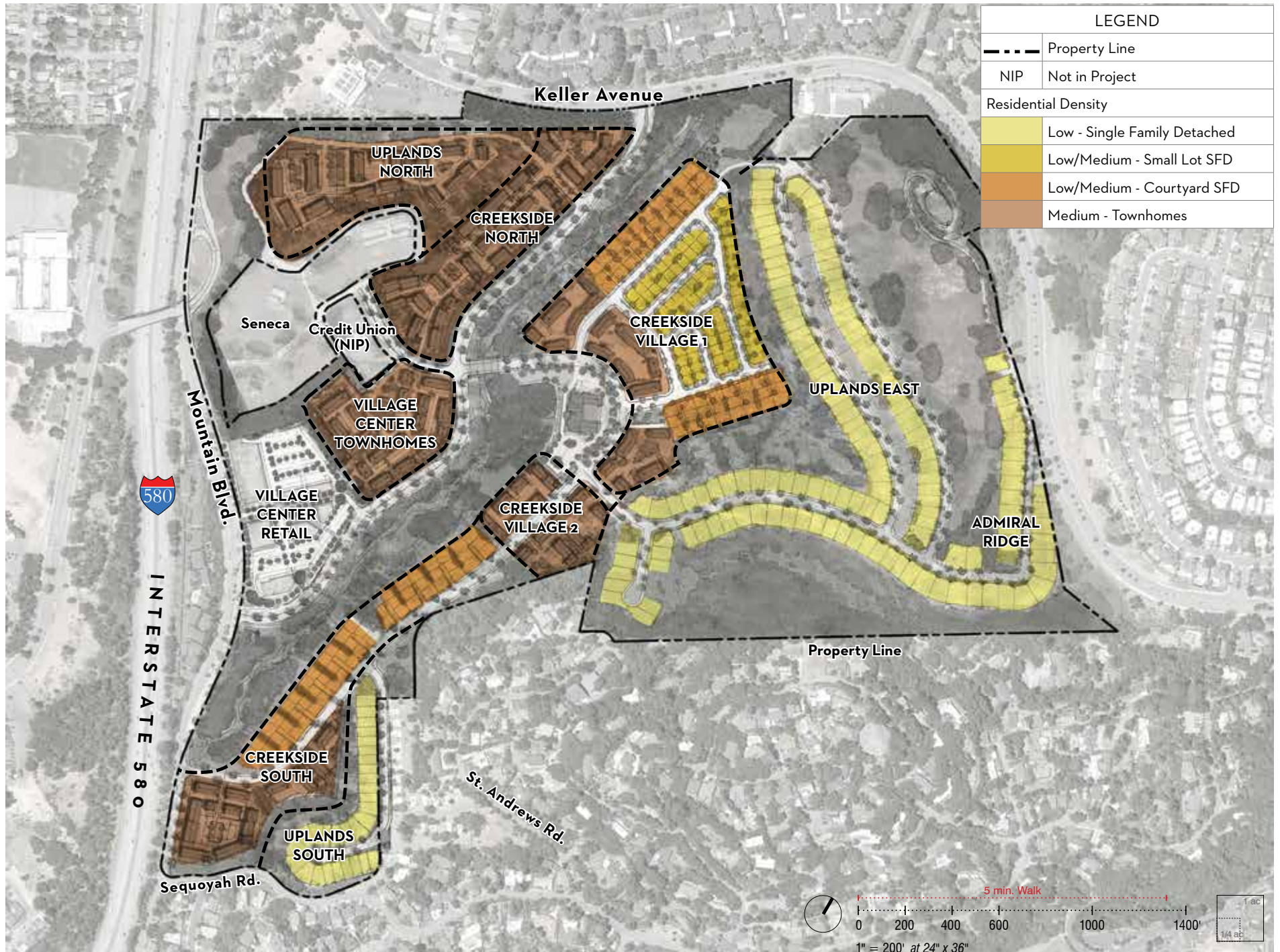


Figure 10 - Residential Neighborhoods

## RESIDENTIAL NEIGHBORHOODS

To provide a diverse range of housing and residential opportunities for new residents there are three general housing types proposed at Oak Knoll; townhome, small lot single family detached (SFD) homes, and more conventional SFD homes. The three housing types are briefly described below. See Design Guidelines for additional information on building type, arrangement, massing, character and other features. For development standards that apply to these housing types, the Oak Knoll District Zoning should be consulted.

**Townhomes** may be two or three stories. Townhomes should be organized in attached buildings of no more than 10 units. Townhome units will range from 1600 to 2400 square feet and each have garage space for two vehicles accessible from an alley or internal driveway. The front of the townhome units may face an internal driveway, paseo, or edge of parcel condition which may include open space or neighborhood street. Each townhome neighborhood has shared group open space and guest parking. Townhome neighborhoods are within the D-OK-3 Sub-zone of the Oak Knoll (OK) Zoning District (see Figure 4).

**Small Lot Single Family Detached Homes** are organized into coherent small scaled neighborhoods similar to those found in the lowland areas of Oakland and Berkeley. Lot sizes range between 2000 and 3500 square feet. These small homes are 2 or 3 stories. Floorplans range from 1600 to 2400 square feet and each unit has garage space for two vehicles accessible from the neighborhood street or a courtyard drive. The courtyard homes are arranged in clusters of 4 or 6 homes which share a “driveway” which is not a public street. Small lot neighborhoods are within the D-OK-2 Sub-zone of the OK Zoning District (see Figure 4).

**Single Family Detached Homes** are found in the eastern and southwestern uplands of the project site. Lot sizes range from 3750 to over 6000 square feet. These homes will not exceed 2 stories. Floorplans range from 2200 to 3500 square feet and each unit has garage space for two vehicles accessible from the neighborhood street. SFD neighborhoods are within the D-OK-1 Sub-zone of the OK Zoning District (see Figure 4).

For information on residential development standards see the proposed Oak Knoll District Zone Regulations.

These housing types are organized into distinct neighborhoods each representing a different character, response to topographic conditions, density, and mixture of housing types. The location of distinct neighborhoods and their associated densities are shown in Figure 10. Artist’s renderings showing the character of neighborhood areas are shown in Figure 11 and Figure 12.

### Creekside Village Neighborhoods

The Creekside Village neighborhoods lie south of the restored Rifle Range Creek corridor on flatter areas of the site. The Community Center/Club Knoll building lies at the center of these neighborhoods and provides nearby community facilities. A vehicular bridge and pedestrian bridge connect these neighborhoods to the Village Center retail area and multi-use path along Creekside Parkway. Creekside Village neighborhoods have narrower street widths and smaller block patterns that reinforce the smaller lots single family homes and townhome character. A neighborhood park that includes active and passive uses lies just north of the Creekside 1 neighborhood. Creekside Village neighborhoods include:

- Creekside Village 1 (Townhomes, Small Lot and Courtyard SFD homes)
- Creekside Village 2 (Townhomes)
- Creekside South (Townhomes and Courtyard SFD homes)

### Townhome Neighborhoods North of the Creek

These townhome neighborhoods all lie northwest of Rifle Range Creek. The townhomes are organized into coherent neighborhoods of 90 to 120 units on stepped terraces leading away from the creek. The graded terraces are remnants of building pads occupied by Navy buildings. Terraces will be separated by vegetation including mitigation oak trees. These neighborhoods include:

- Upland North
- Creekside North
- Village Center Townhomes





Small Lot SFD Neighborhood



Small Lot SFD Streetscape



Courtyard SFD Entry



Small Lot SFD Streetscape



Courtyard Homes along Creek

Figure 11 - Illustrative Renderings Depicting Small Lot Neighborhood Character



### Uplands Single Family Detached Neighborhoods

On the uplands east of Rifle Range Creek lie SFD neighborhoods. These neighborhoods support larger lots and homes often with dramatic views to the southwest and northwest. Upland areas closer to the high point of the site known as Admiral's Ridge (see Figure 10) have several hillside sloped lots which have finished graded slopes greater than 20%. Development standards for sloped lots over 20% are in the Oak Knoll District Zoning. Additional measures may be adopted for homes on Admiral's Ridge, which are visible from public viewpoints to the west. As shown in Figure 10 Upland Single Family neighborhoods include:

- Uplands East
- Uplands South
- Admiral's Ridge



Upland Frontyard Treatments



Upland Streetscape



Upland SFD Homes

Figure 12 - Illustrative Rendering Depicting Upland Neighborhood



### Admiral's Ridge Focus Area

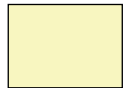
The Admirals Ridge focus area includes 18 single family lots on the eastern edge of the project flanking Keller Avenue. Due to the increased visibility of the 13 lots west of the project street, special height standards will be applied to two lot conditions; "terraced" lots and "sloped" lots. The third lot condition, on the east side of the road, does not have the same visibility issue as the lots on the west side of the street. The location of the Admiral Ridge focus area and three lot conditions are shown in Figure 13. Refer to Figure 15 for a section view showing the height limitations for each lot type. Figure 14 provides an artist's illustration of the height restrictions that apply to the terraced and sloped lot conditions.



#### KEY LOT TYPE



**Terraced Lots.** These lots have terraced building pads to better integrate homes into the hillside topography.



**Sloped Lots.** These lots are located in the steeper visible area to the north. The majority of the area within each lot's buildable envelope is sloped.



**Padded Lots.** These lots are padded to allow for a full flat building site and are not located in the highly visible areas west of the road.

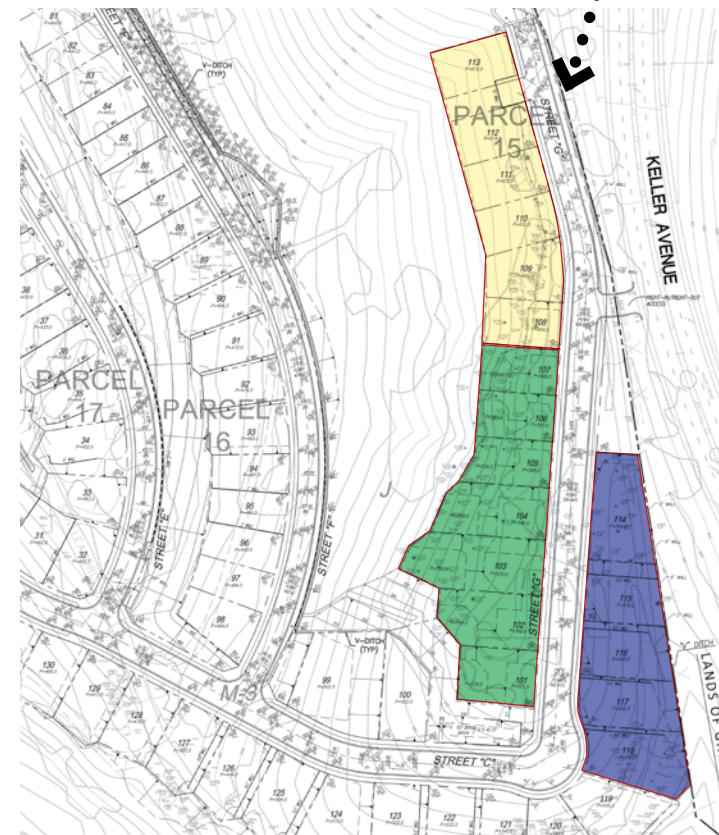


Figure 13 - Admiral's Ridge Focus Area and Lot Type Locations



Figure 14 - Artist's Concept of Sloped and Terraced Lots

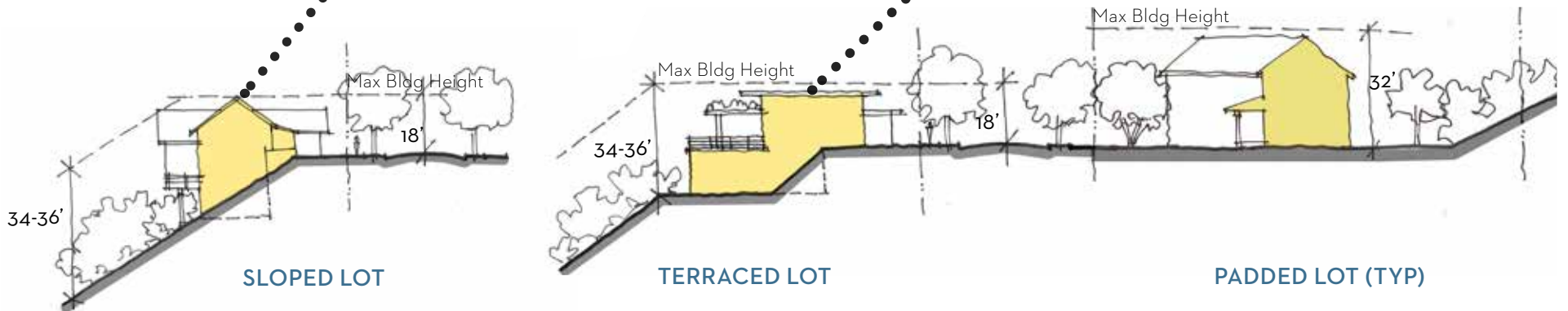


Figure 15 - Admiral's Ridge Section and Building Heights by Lot Type



## OPEN SPACE, PARKS, AND TRAILS

A major defining feature of the Oak Knoll Plan is the extensive open space network. Approximately 85 acres of permanent open space is planned within the community. Open space areas include more sensitive areas of the site such as the Rifle Range Creek corridor, visible high points, and the wooded Hardenstine property. Also included are areas for active and passive parks, wildlife habitat, and visual slope buffers providing separation between neighborhoods. Trails and complete streets provide connections between open space and neighborhoods. Smaller neighborhood parks are provided in the public realm and group open space is planned within in-tract townhome parcels. The open space and park areas are separated in four categories and the acreages are summarized in Table 3. Figure 16 shows the extent of open space and park areas proposed within Oak Knoll.

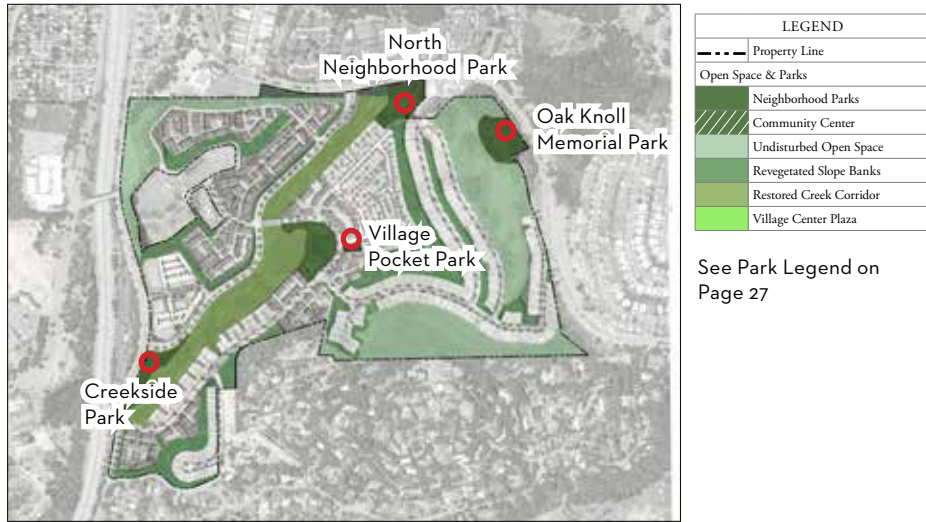
The parks and open space types are summarized in the next sections. Hiking trails are addressed more fully in the Project Circulation section. For more information on the overall site landscape concept within open space and park areas and landscape treatments within neighborhoods and private lots see the Design Guidelines.

<b>TABLE 3 OPEN SPACE AND PARKS</b>	
<b>Type</b>	<b>Acreage</b>
Public Parks	7.6
Undisturbed Open Space	39.0
Revegetated Slope Banks	20.3
Restored Creek Corridor	17.8
<b>Total</b>	<b>84.7</b>



Figure 16 - Open Space and Parks





Neighborhood Park Locations



North Neighborhood Park



Oak Knoll Memorial Park



Village Pocket Park

Figure 17 - Neighborhood Park Concepts



PARK LEGEND	
#	Use/Activity
1	Informal Playfield
2	Picnic Area
3	Tot Lot
4	Seating Area
5	Paseo
6	Trail
7	Detention Basin
8	Potential Public Art Location

Figure 18 - Creekside Park Concept

### Public Parks (7.6 acres)

Active parks include four publicly accessible neighborhood parks and private parks within the Townhome neighborhoods Village Center. Public park locations are shown in Figure 17 and 18 with concept diagrams showing potential uses and activities within the parks. Park uses are further detailed in the proposed Oak Knoll zoning districts. Potential park uses and activities are summarized below:

#### Publicly Accessible Neighborhood Parks

##### Creekside Park

- Picnic and seating areas
- Trail connections along Creek
- Potential public art location
- Stormwater control

##### North Neighborhood Park

- Informal play field
- Picnic and seating areas
- Playground/Tot lot
- Dog play area
- Trailhead connections
- Stormwater control
- Potential public art location

##### Village Pocket Park

- Shaded seating areas
- Potential public art location

##### Oak Knoll Memorial Park

- Seating, view and picnic area
- Potential public art location
- Trailhead connections





Figure 19 - Illustrative Rendering of Village Center Plaza

### Club Knoll Community Center (2.8 acres)

The relocated and renovated club knoll building sits on a 2.8 acre site (see Figure 9) providing publically accessible areas including:

- Seating areas
- Outdoor courtyard space for public and private events and gatherings
- Potential public art locations
- Walking paths/trailhead connections
- Stormwater management

### Townhome Recreation Facilities

Townhome neighborhoods will provide recreation and open space areas for the use of townhome residents. Open space requirements per OPC require 170 square feet of group open space per unit or 70 square feet of private open space such as a porch or balcony (to a maximum of 50% of required group open space). Recreation areas and uses will be identified in the FDP submittals of townhome areas. Recreation and open space areas within townhome neighborhoods are not included in Table 3. Recreational activities and uses may include:

- Seating and picnic area
- Children plays area
- Paseos providing landscape areas for internal walkways
- Green space buffering adjacent development
- Recreation facilities such as a clubhouse or swimming pool
- Balconies, patios, or decks (private open space)
- Stormwater management

### Village Center Plaza

Additional park and open space area will be provided within the Village Center. The Village Center will include spaces such as the village plaza which will include shade trees, both hardscape and softscape, benches, and other improvements. Figure 19 provides an illustrative rendering of the proposed Village Center plaza. The Village Center plaza area is not included in Table 3.

Uses and activities may include:

- Plaza which will serve public gathering and programmed events such as farmer's market, art fairs, etc.
- Informal seating and gathering in both Plaza and retail street
- Hardscape and softscape areas
- Shade and ornamental planting
- Potential public art location(s)
- Stormwater management

### Undisturbed Open Space (39.0 acres)

The largely undisturbed areas of the site include the "Oak Knoll" area, the Hardenstine Parcel, and areas on the northwest and southeast borders of the site. Some parts of these open space areas are suitable for tree mitigation as described in the Tree Preservation and Removal Permit Application. Consult the Design Guidelines for more information on the tree mitigation planting. Publicly accessible activities and uses within this area may include:

- Passive recreation
- Hiking trails
- Wildlife habitat
- Stormwater management

### Revegetated Slopes (20.3 acres)

Graded slopes between neighborhoods address grade changes, enhance both on-site and off-site views, provide neighborhood separation and opportunities for significant new vegetation. All of the revegetated slopes will receive landscape treatment including significant tree mitigation planting. These areas will not be publicly accessible and uses include:

- Visual buffer
- Wildlife habitat
- Stormwater management/erosion control





Existing Conditions



Proposed Conditions

**LEGEND**  
--- CREEK CHANNEL  
--- DEVELOPMENT BOUNDARY

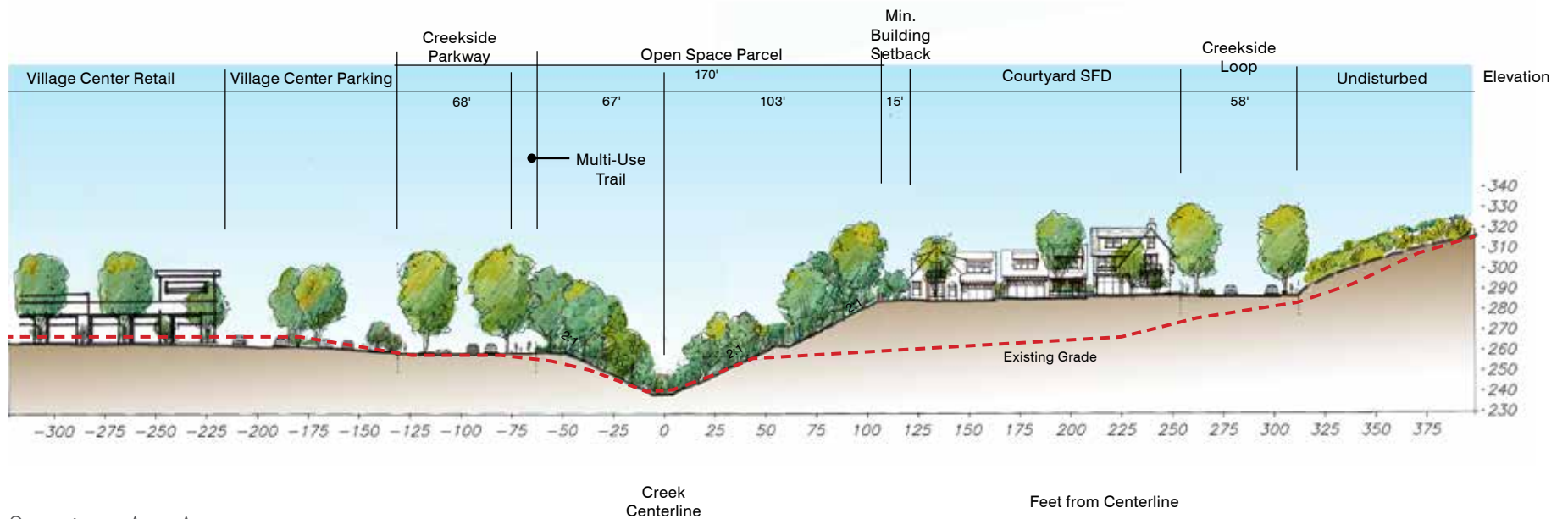
Figure 20 - Existing Rifle Range Creek Corridor and Proposed Restoration Area

## Section Key

### Restored Creek Corridor (17.8 acres)

Two major defining open space features on the site are the existing creek environment and riparian area associated with Rifle Range Creek. Currently, the northern portion of this creek lies within a pipe under abandoned parking areas and roads. The southern portion includes open creekbed and riparian vegetation. This area of the creek is deeply eroded with an incised creekbed and unstable banks. Figure 20 shows the existing extend of the creek and the extent of the planned creek restoration area.

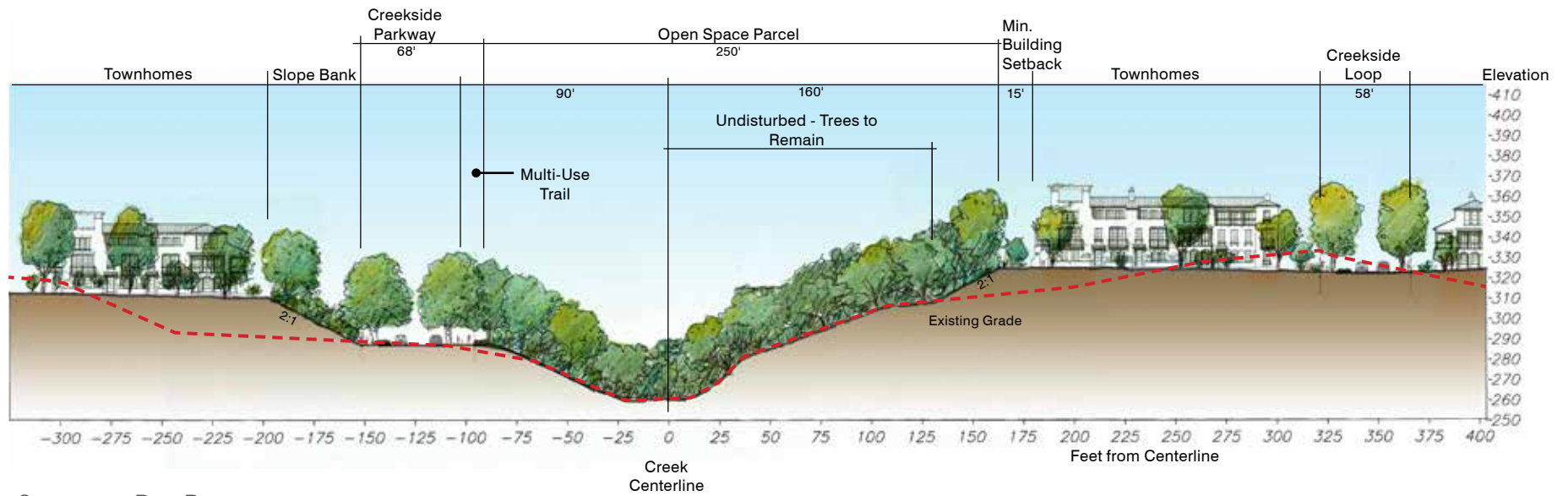
The creek will be daylighted, remedially graded, and restored for its length to include new creek bottom lands, riparian oak woodlands, other native species and public trail access. A shared multi-use trail runs parallel to the corridor on the north east side within the Creekside Parkway right-of-way (ROW). At the center of the restoration area beyond the eastern “top of bank” lies the community center/clubhouse. Shown in Figures 21 and 22 are



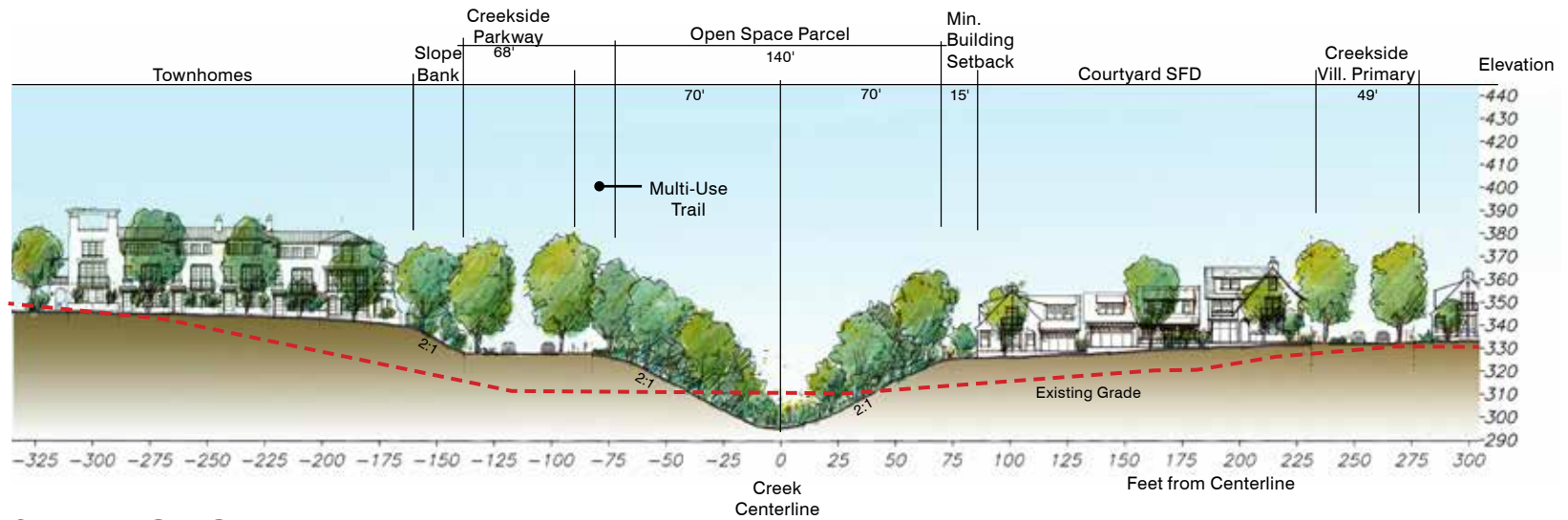
Section A - A

Figure 21 - Creek Section A





Section B - B



Section C - C

Figure 22 - Creek Sections B and C

proposed sections through the restored creek at three locations. The Creek Protection Permit Application should be consulted for detailed information on the proposed Creek restoration. Publicly accessible activities and uses in the restored creek area may include:

- Hiking/biking trails
- Wildlife habitat
- Possible public art locations along trails
- Stormwater treatment

#### *Private Open Space in Single Family Neighborhoods*

Open space is required within private residential lots either as private or group open space (see OPC, Section 17.126). These areas are not included in the public open space acreage summaries given in Table 3. Private open space is planned to be associated with each residential unit satisfying the requirement and providing both private and semi-private outdoor areas for each homeowner.

See proposed Oak Knoll District Zoning and Design Guidelines for more information on open space within private parcels.

## CIRCULATION AND COMPLETE STREETS

Complete Streets are integral component of the Project. Complete Streets provide safe, convenient and comfortable travel and access for users of all ages and abilities regardless of their mode of transportation. Complete streets are designed, implemented and operated to also provide health benefits and improve economic and environmental outcomes within communities. Complete streets make it easier to access transit, bicycle to work or shopping, walk with children to parks and schools, and allow motorists to move safely and conveniently to their destination. A complete street also combines trees and landscape for shade, to modulate reflection, create wildlife habitat, and to treat stormwater. Complete streets are also beautiful streets. Complete streets shall meet all fire and accessibility requirements.

### Complete Street Design

Complete streets integrate standard elements and additional components that are designed on a case-by-case basis depending on local conditions. Local conditions may include the land use the street serves, level of motorist traffic, pedestrian and bike usage, slope and curvature of street corridors, existing vegetation, and stormwater control requirements.

Standard elements include:

- Standard lane and sidewalk widths
- Accessible features including curb ramps and markings at crosswalks
- Street trees
- Pedestrian and bike accommodations
- Stormwater control (where feasible)

Additional components may include:

- Street parking accommodations appropriate to the location and demand
- Pedestrian signalization
- Corner curb extensions and bulb-outs



- Traffic calming devices such as a chicane
- Accommodations for transit
- Sidewalk planters/tree grates
- Special paving or materials
- Site furnishings, lighting, and signage
- Special connections to trails, parks, or other uses

### Streetscape Context at Oak Knoll

Oak Knoll streets will be situated in a suburban setting in rolling to hilly terrain. The streets will serve a variety of users and land uses including neighborhoods of varying residential density, commercial and community hubs, and park and open space areas. Collector streets will serve vehicular traffic commuting on and off-site (there will be limited jobs and no schools at Oak Knoll), pedestrians and bikes accessing transit connections, and people moving between neighborhoods to shopping and parks. Neighborhood streets will serve the neighborhoods proper with little through-vehicular traffic. Additionally, smaller neighborhood streets will serve vehicular traffic and provide a “front yard” for people and children to walk and play in a shaded safe area.

### Complete Street Strategies at Oak Knoll

Strategies implemented in the Project street designs include:

- Emergency and fire access should be paramount in street design and connectivity
- Vehicular lane widths should be narrow to control traffic speeds, conserve land, and free up space for other uses within the street corridor
- All streets should have pedestrian accommodations that connect to the internal open space and trails system and to off-site trails and transit connections
- The commercial village primary street should be urban in character, provide direct access to shops and plazas, accommodate short term parking, and be amenitized to be comfortable and inviting to pedestrians

- A major Class I bikeway should connect the main site gateways at Mountain Boulevard and Keller Avenue
- Street trees should be a major component of all streetscapes
- Street ROW stormwater should be detained within the ROW in landscaped infiltration basins. Where longitudinal slopes are too steep to allow for treatment in the ROW, centralized neighborhood basins should be utilized.

### Typical Lane Widths

Proposed collector streets in Oak Knoll will employ a travel lane width of 11' consistent with the Urban Street Design Guide, National Association of City Transportation Officials, NY, NY, 2013. The 11' lane width provides adequate width for anticipated vehicular design speeds and provides traffic calming to encourage safety for pedestrian and bicyclists. Minor streets such as neighborhood streets and alleys may have a 10' travel lane width with no street parking.

### Stormwater Detention

Stormwater falling within the street ROW will be detained locally in infiltration basins integrated into the street profile where possible. Infiltration basins are located at intervals in “bulb-outs” along the travel lanes within the street parking lanes. Stormwater from street pavements and sidewalks will flow into the infiltration basins and infiltrate into the soil. The bulb-outs also act as traffic calming devices and add greenscape to the street corridor. Additional information on the location and design of infiltration basins is presented in the Design Guidelines. Where segmented infiltration basins are not possible in the streets due to street grade or narrow lots requiring curb cuts and area for on-street parking, more localized infiltration basins will be utilized.

Additional information on street alignments, intersections, project grading, and stormwater details are contained in the Vesting Tentative Map.

## PROPOSED PROJECT CIRCULATION

The Oak Knoll site will be served by a multi-modal circulation system using the Complete Street network. In addition, there will be walking and bike trails independent of the Complete Street network integrated into open space areas and connected to the street network. The proposed project circulation is shown in Figure 20.

### OAK KNOLL COMPLETE STREETS

Project circulation is based on two types of streets:

- Collectors providing site access and connectivity to Project neighborhood and community amenities, and
- Local neighborhood streets connecting residential areas internally and externally.

All project sidewalks and parkways (tree planters) are five to six foot in width. All public streets include street trees on both sides of the street. Figure 23 should be referenced for the proposed street circulation network. Public street types are discussed below with typical sections shown in Figures 24, 25, 26, and 27:

#### Public Collector Street (Figure 24)

**A: Creekside Parkway** is the major project thoroughfare that connects the project gateways, Mountain Boulevard to Keller Avenue. It also connects to the Village commercial center and internal project and neighborhood streets. This street will have a dedicated shared trail for bikes (Class I) and pedestrians that runs parallel to the preserved Creek Corridor. There is on-street parking integrated between the bioretention basins on both sides of the street. There will not be residential driveway access or cuts on Creekside Parkway.

#### Local Neighborhood Streets (Figures 24, 25, 26 and 27)

**B: Creekside Loop** is a local street that runs roughly parallel to Creekside Parkway on the eastern side of the creek corridor. This street provides

a second project entrance from Mountain Boulevard and connects to all neighborhoods east of the Creek and the community center/clubhouse. There is on-street parking integrated between bioretention basins on one side of the street. There will be no driveway cuts on Creekside Parkway (private drive courts will connect to Creekside Loop).

**C1: Creekside Village Primary** is a local street serving the small lot single family homes and courtyard homes neighborhoods east of the creek. There is on-street parking integrated on one side of the street.

**C2: Creekside Village Secondary** is a smaller local street serving the small lot single family homes east of the creek. There is no on-street parking on this street.

**D1: Uplands Primary** This local street serves the eastern areas of the site and includes bioretention basins and on-street parking on both sides of the street. In areas that are too steep for bioretention basins localized retention basins are reserved within the ROW but outside of street pavements and sidewalk area.

**D2: Uplands Secondary** This local street serves the single loaded single family lots in upland areas of the site and includes on-street parking on one side of the street, adjacent to the home lots. The street roadbed is “super-elevated” towards the inboard, upland area of the site where stormwater from the ROW follows into a bio-retention swale that runs parallel to the street.

**E: Uplands North** This local street connects the Townhome Neighborhoods north of the Creek and also provides secondary access to the Seneca property.

#### Private Neighborhood Streets

Additional “private streets” provide access within smaller neighborhood clusters and townhome areas. These include driveways, alleys and drive courts. All private streets will meet fire accessibility standards where applicable. These private streets are described in the Landscape Design Guidelines.



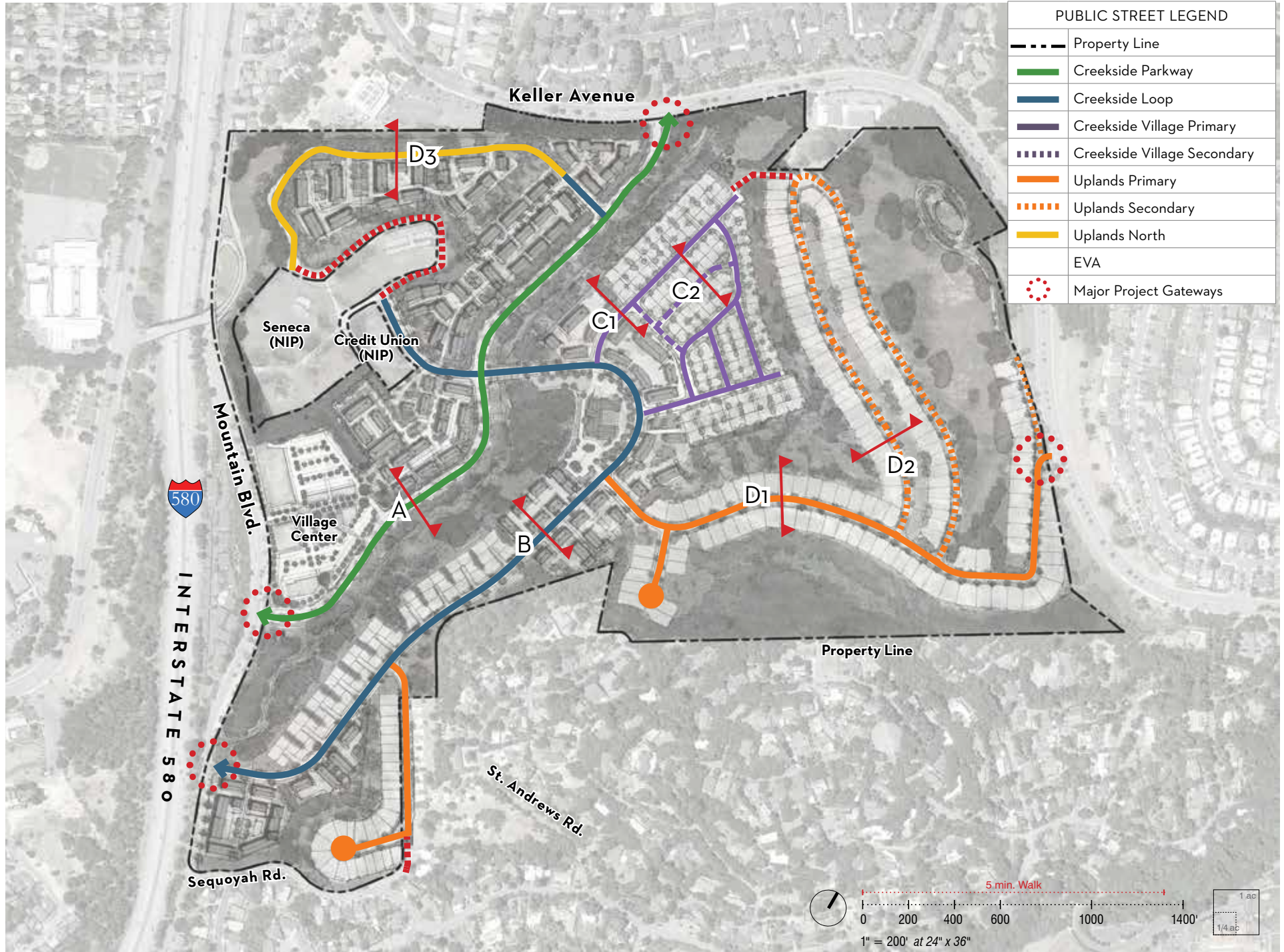
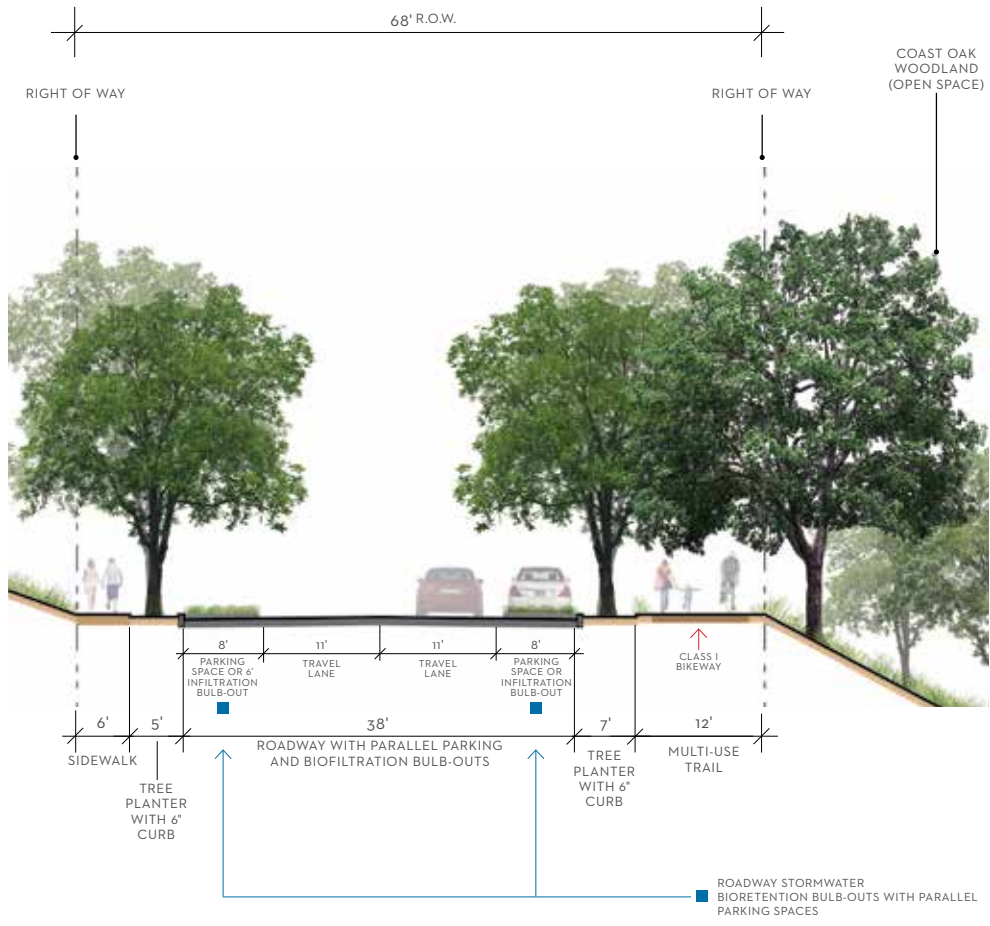


Figure 23 - Project Circulation

**A: CREEKSIDE PARKWAY**

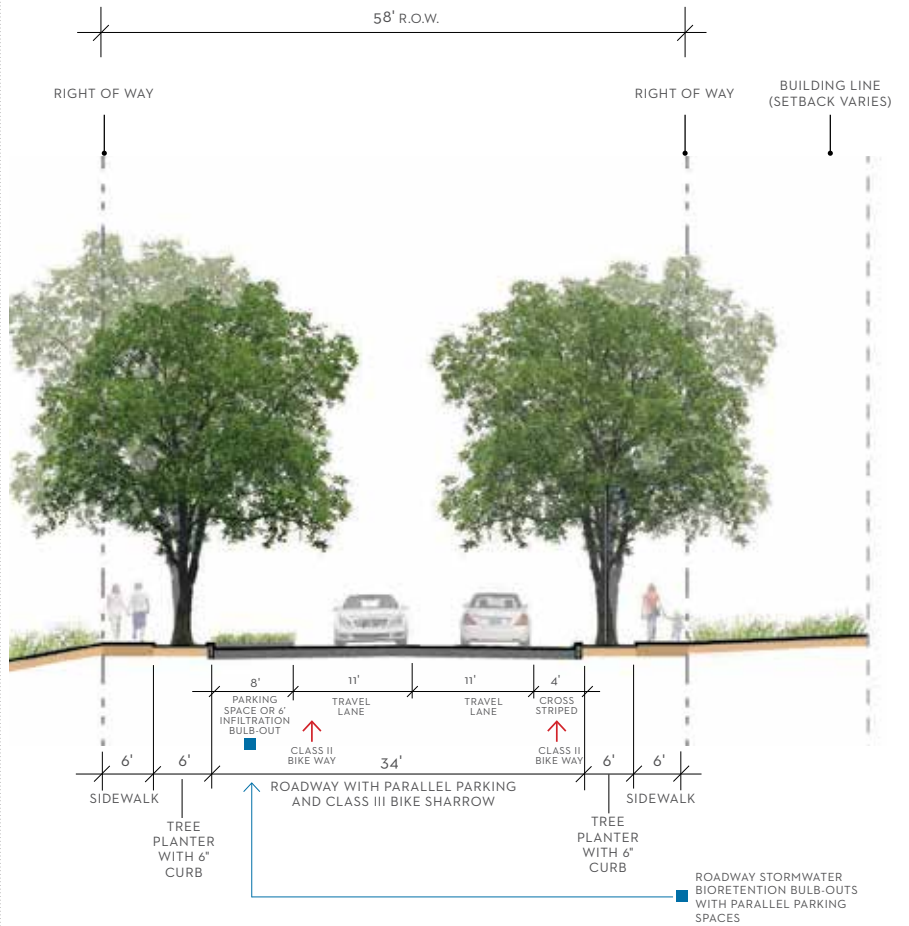
NEIGHBORHOOD



RECOMMENDED STORMWATER APPROACH

**B: CREEKSIDE LOOP**

LOCAL STREET



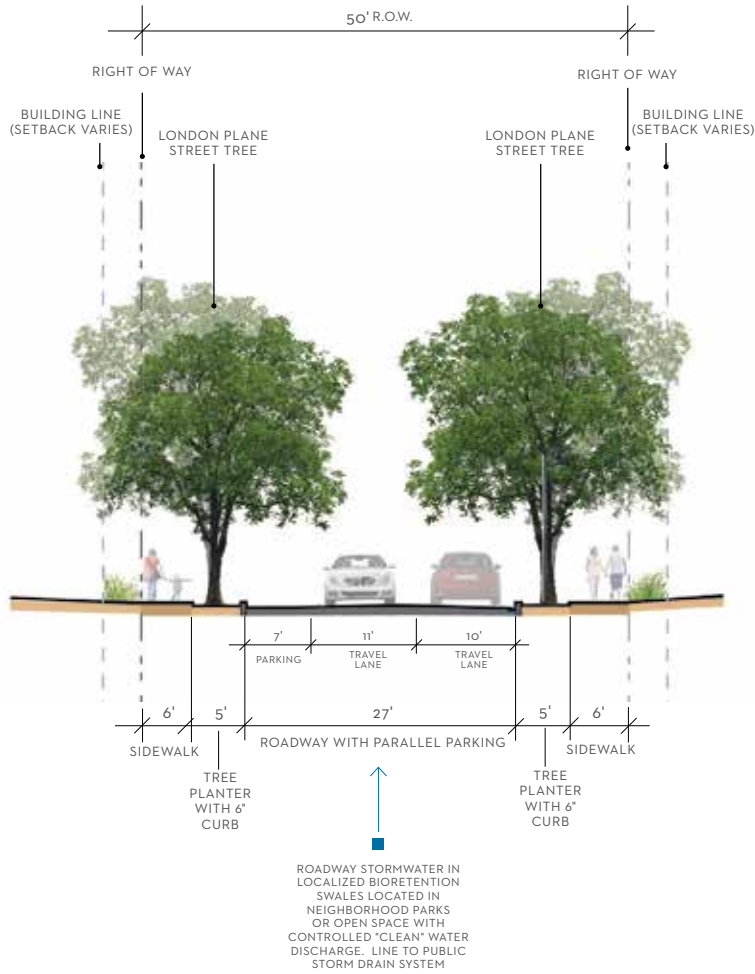
SECTIONS ARE NOT TO SCALE

Figure 24 - Public Streets

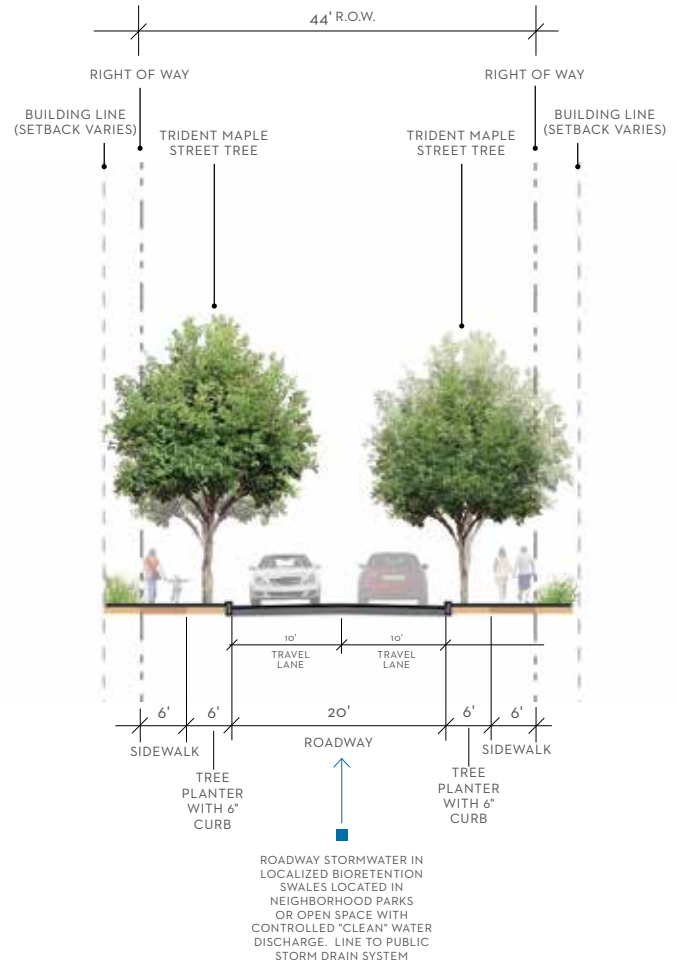


C: CREEKSID VILLAGES

C1: CREEKSID VILLAGE PRIMARY



C2: CREEKSID VILLAGE SECONDARY

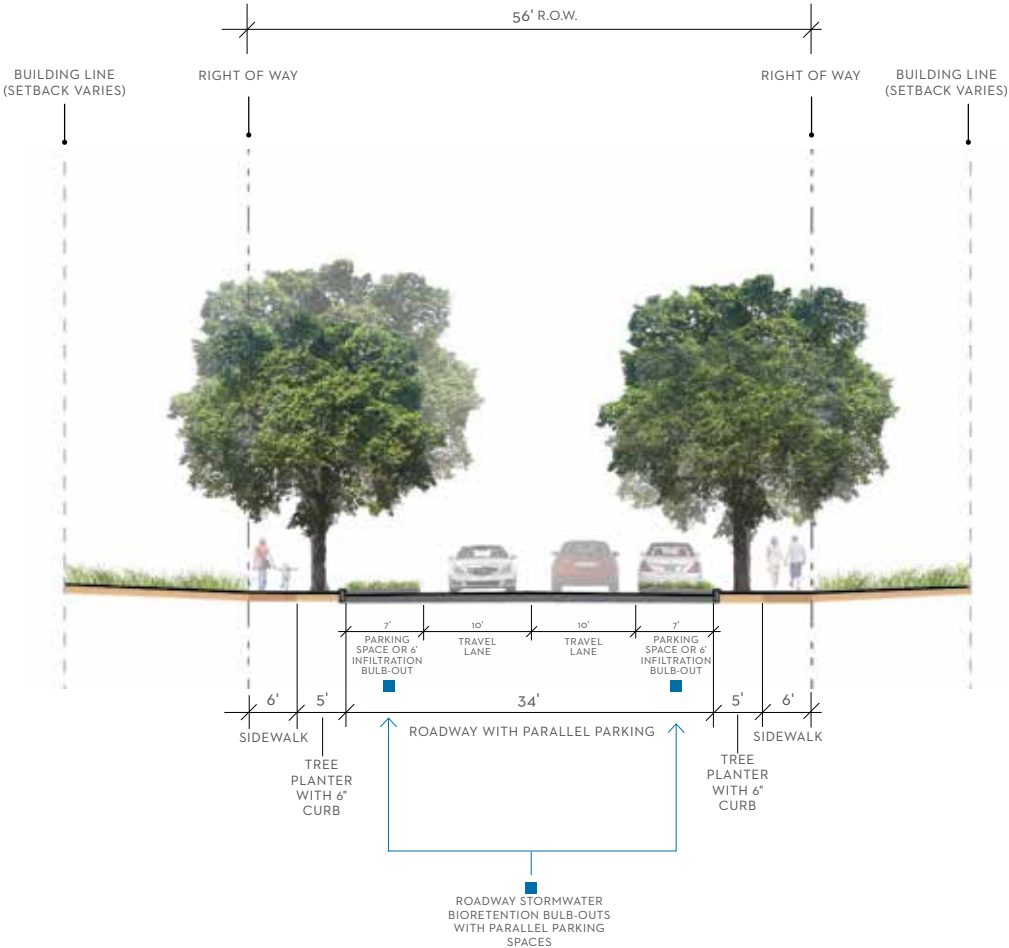


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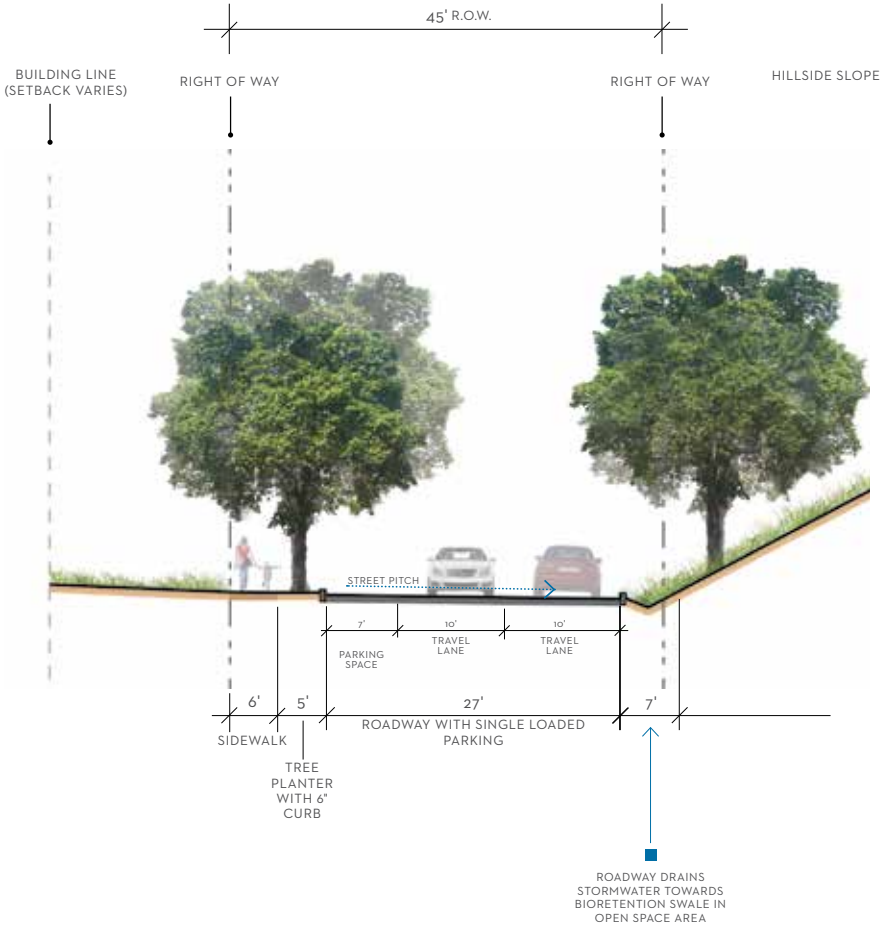
Figure 25 - Local Neighborhood Streets

D: UPLANDS

D1: UPLAND PRIMARY



D2: UPLAND SECONDARY



SECTIONS ARE NOT TO SCALE

Figure 26- Local Neighborhood Streets

4.0 PRELIMINARY DEVELOPMENT PLAN



D: UPLANDS

D3: UPLAND NORTH

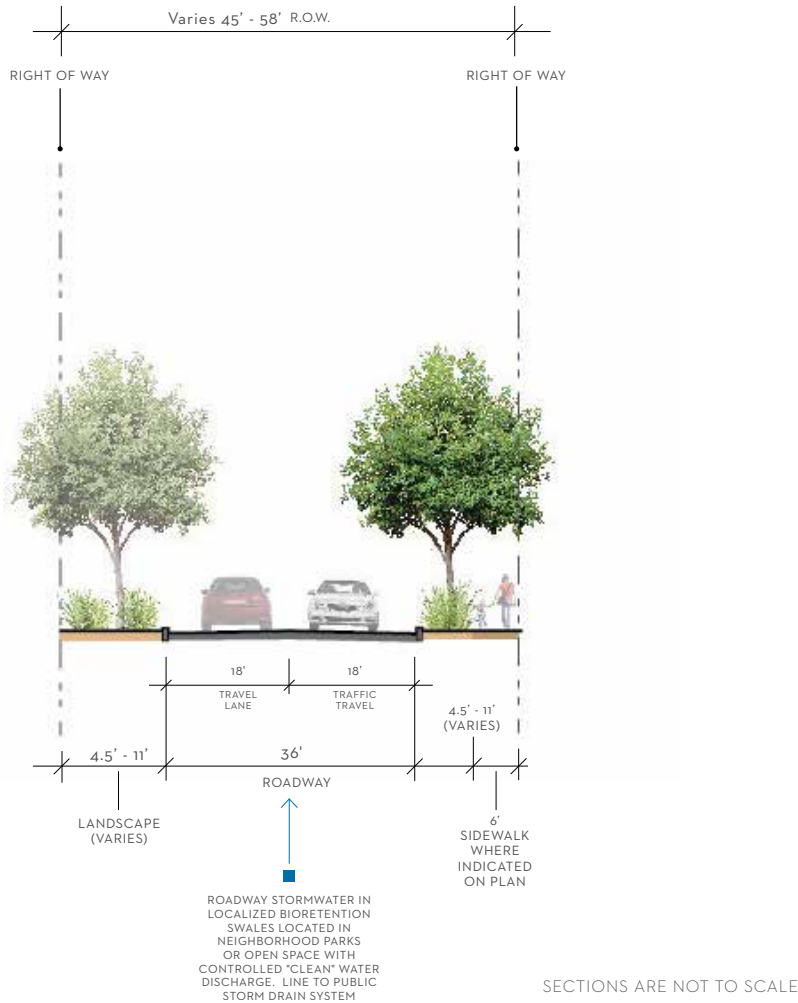


Figure 27 - Local Neighborhood Streets

PEDESTRIAN, BIKE, AND TRANSIT CIRCULATION

On-Site Pedestrian and Bike Circulation

A significant feature of the site is a system of walking trails and bike ways connecting neighborhoods and open space areas within the site and also existing and future trails/bike ways that lie outside the site. Figure 28 shows proposed on-site trails and bike ways and connections to off-site trails, bike paths, and transit connections. Proposed trails on-site include:

- Hiking trails through open space areas and parks
- Hiking trails through the restored creek corridor
- Shared-use trail paralleling the creek corridor along Creekside Parkway
- Neighborhood paths that use a combination of sidewalks and paths through local parks and open space areas.

Proposed on-site bike paths serve neighborhoods, connections to the Village Center, and connections to off-site trails, bike paths, and transit connections. Proposed bike paths on-site include:

- Class I bike path occupying the shared multi-use path along Creekside Parkway
- Class III bike way integrated within Creekside Loop
- Low traffic volume neighborhood streets, which have been traffic-calmed with Complete Street strategies, do not require formal bike way designations.

Off-Site Pedestrian, Bike, and Transit Circulation/Connections

The Oak Knoll site will be served by existing and proposed off-site multi-modal transportation options including vehicular, pedestrian, bike, and transit. Critical to developing a multi-modal transportation concept are connections to off-site transportation facilities that exist today and those that are proposed in the future. Connectivity to adjacent neighborhoods include vehicular, bike paths, sidewalks, and hiking trails. Off-site connections, both existing and planned, are shown in Figure 28.

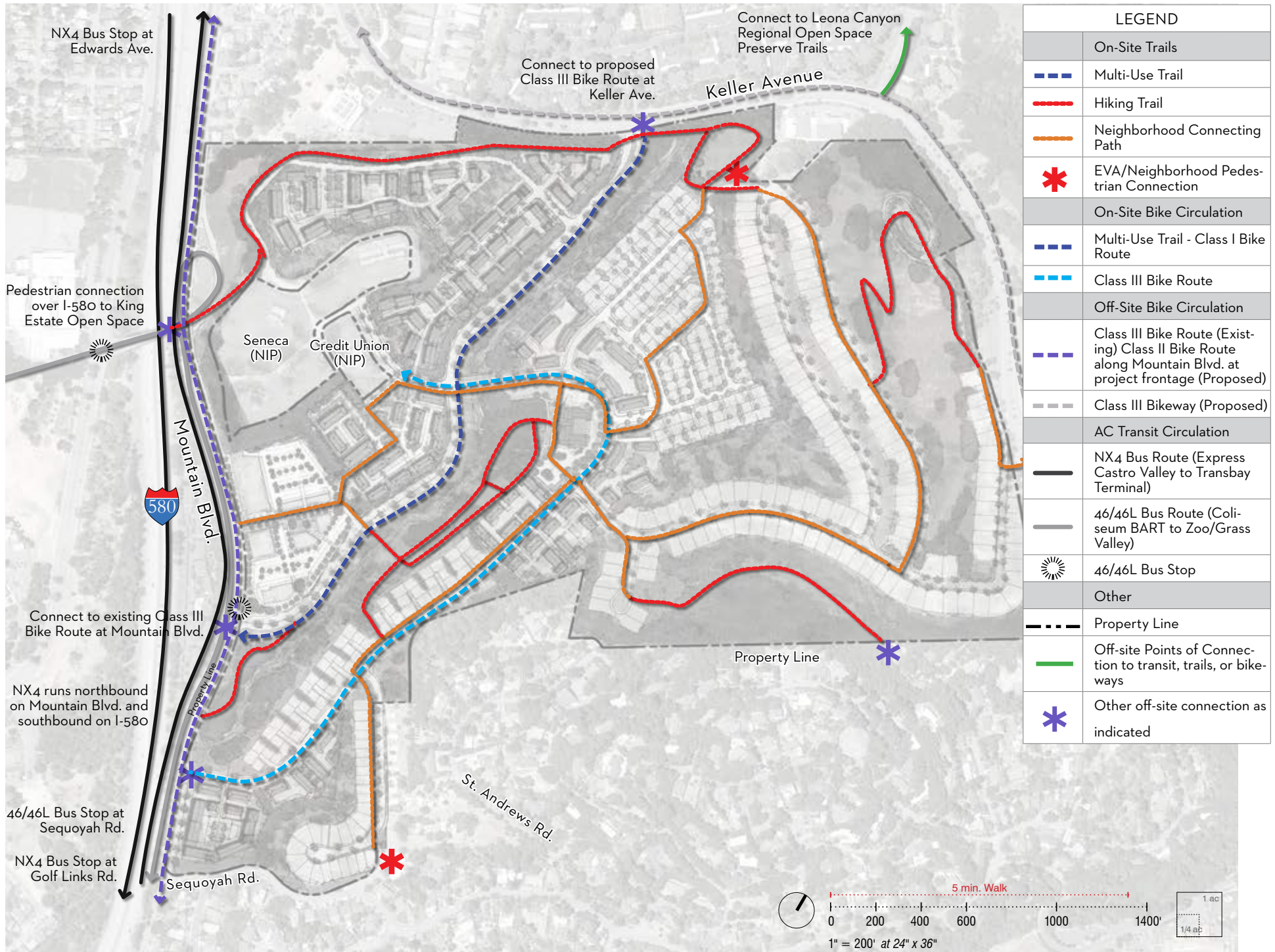


Figure 28 - Transit, Bike and Pedestrian Trails/Connections



Existing and proposed off-site trail and pedestrian connections include:

- Cross-walk at Creekside Parkway and Keller Avenue to connect the on-site shared-use trail and the Leona Canyon Regional Open Space trails. This connection requires walkers to use the sidewalk on the north side of Keller Avenue and cyclists to use the Keller roadway. A future Class III bikeway is planned for Keller Avenue.
- Trail/walking connections across and along Mountain Boulevard ultimately connecting to Knowland Park in the southern direction
- The connection of the northern on-site walking trail along the projects northern boundary with the vehicular bridge crossing both Mountain Boulevard and I-580 and connecting with Fontaine Street. This gives pedestrians a safe and clear connection to neighborhoods west of the site with Oak Knoll and Keller Avenue.
- Off-site pedestrian connections utilizing the planned project EVA at Barcelona Street
- An easterly connection from the Upland portion of the project with Keller Avenue east of Oak Knoll.

Off-site bike ways include a connection to the existing Class III bike way along Mountain Boulevard. Off-site project improvements include creating a Class II bike way along Mountain Boulevard for a portion of the project frontage.

Existing Alameda County (AC) transit serves the project site. Two local AC Transit bus lines run on Mountain Boulevard directly in front of the site. The 46 and 46L run east west from the Oakland Zoo to the Coliseum BART station. There is a bus stop at the project’s current entry and it would be anticipated that this stop would continue at the new project entry at Creekside Parkway. There is no direct transit connection from the site to downtown Oakland. However, an express bus (NX4) that originates in Castro Valley connects to the Transbay Terminal in San Francisco. An NX4 stops lies about a 10 minute walk north of the site along Mountain Boulevard.

## PARKING

The anticipated parking program for the site is summarized in Table 4. The parking meets off-street parking and loading requirements stated in Section 17.116 of the Oakland Planning Code.

<b>Parking Type</b>	<b>Spaces</b>
On-Street	500
Off-Street Village Commercial/ Community Center	400
Off-Street Residential (includes garages and townhome guest parking)	2,500
Off-Street Trailhead/Park	6
<b>Total (approximate)</b>	<b>3,406</b>

## SITE GRADING AND UTILITIES

### Site Grading

Corrective (remedial) grading will be required prior to any new development to accommodate past naval activities and facilities which often lie on poor soils. Corrective grading is required for most developed areas which consist of unconsolidated (poorly compacted) soils which will need to be excavated and re-compacted. The majority of Rifle Range Creek will need to be over-excavated, reconstructed with the installation of new keyways to create slope stability. To meet the City of Oakland's street design standards grading will be required to support safe street design in some upland residential areas. Graded areas will be contoured and planted primarily with native oak trees so that in time they appear natural and well vegetated. The VTM and Final Development Plan (FDP) as well as the Final Grading Permit should be consulted for information on proposed site grading.

The site may be graded in phases, largely coinciding with the project construction phases presented in the next section. Phase 1 may include a "borrow" area from Phase 2 to provide additional fill material to create a balance of cut and fill in Phase 1. Over all three phases the project is anticipated to balance cut and fill.

### Utilities

Proposed project infrastructure and utilities are addressed in the FDP as well as the Final P-job Permit. Stormwater management within the public ROW was discussed earlier as part of the Complete Street strategy. All stormwater generated within private lots (SFD lots, townhome parcels, and Village Center) will be handled within localized infiltration basins or street swales in the case of some upland streets.

### Stormwater

Project stormwater will be detained on-site and released as treated stormwater into the creek corridor and off-site stormwater facilities. As mentioned in the Complete Streets section of this document, street stormwater falling in the ROW will be treated in "rain gardens" or in localized treatment basins (see Design Guidelines). The public parks will also have detention basins within the park boundaries. Stormwater from private parcels, including the Club Knoll parcel, will be treated either in on-site detention areas or in localized detention basins. Reference the project VTTM for more details on the storm water system.

All detention basins visible from project streets, neighborhoods, and parks will be graded to soften the geometric shape of the basins and be screened with trees and shrubs. Localized basins will also be fenced for security. The basins and fencing will be screened with shrubs and groundcover. See Figures 29 and 30 for typical detention basin planting treatments.

## PUBLIC ARTS PROGRAM

The City ordinance requiring public art will apply to the Oak Knoll project. Funds for public art are derived based on building development costs declared on building permit applications and may be exercised in two ways. The developer can acquire and install publicly accessible works of art on the development site, or the developer can make an in-lieu contribution to the City of Oakland Public Art Project Account for the acquisition and placement of public art throughout the city.

OKVA plans to engage artists during the design phase of development. OKVA and project designers will work in a collaborative manner to ensure that the artist-provided engineering and/or design essentials are embedded in the development construction documents. Furthermore, in an effort to maximize the art budget, OKVA will be seeking site contractor bids that include any public art infrastructure requirements.

Following approval of the PDP, a project art master plan will be developed and submitted to the City of Oakland's public art advisory committee.





## PROJECT ARCHITECTURE AND LANDSCAPE

The character of the Oak Knoll community, as defined by building architecture and landscape, is presented in the Design Guidelines which are a companion document to this PDP.

## FDP SUBMITTALS SCHEDULE

### Master Developer Project Improvements FDP

A Final Development Plan (FDP) has submitted by the Master Developer in conjunction with this PDP to describe the project land development and amenities associated the master community development. These improvements allow the necessary infrastructure and access for home builder parcels. Improvements in this Master Developer FDP package include:

- Streets, street lighting and streetscape plantings for Creekside Parkway, Creekside Loop, and local street extensions
- Site grading, retaining walls, including side yard retaining walls
- Master utilities including sewer, water, storm drainage and franchise utilities
- Rifle Range Creek restoration
- Public parks, open space planting, and trails
- Monumentation and signs
- Slope erosion control/planting
- Club Knoll relocation and rehabilitation

### Club Knoll Relocation and Rehabilitation FDP

A FDP has submitted by Master Developer for the Relocation and Rehabilitation of Club Knoll.

### Home Builder and Village Center FDPs

Individual FDPs will be submitted by home builders for each of the townhome and single family detached neighborhoods. Additionally, an FDP will be

submitted by the developer of the the Village Center commercial area. The submittal of home builder FDPs are anticipated to follow the sequence of master site development. However, the submittal of home builder FDPs are subject to market conditions so the timing and rate of submittals may vary.

It is anticipated that the home builders will be submitting their FDP in conjunction with building permit submissions. More than one FDP may be submitted for individual residential neighborhoods. It is possible that multiple FDPs will be submitted by homebuilders including a subsequent phase of development that may also be underway at the same time.

## FDP'S REQUESTING EXCEPTIONS

The City of Oakland's current entitlement and approval process for new development will adequately address any future developer that seeks exceptions to the approved Oak Knoll Preliminary Development Plan (PDP) and/or the Proposed Oak Knoll District zoning ordinance. The approved plans and code provides the necessary guidance for the City to review and assess the conformance of future developer Final Development Plans with the approved PDP, as required by the PUD permit. Minor changes to an approved PDP or FDP may be approved administratively by the Planning Director.

Where a future developer seeks an exception, either the City determines the exception is substantially in compliance with the approved PDP or zoning code; or the major changes would require the developer to amend the PDP and potentially amend the zoning ordinance, or seek a zoning variance, respectively. Such amendments require discretionary action required by the Planning Commission, and in the event of an amendment to the zoning ordinance, by the City Council.



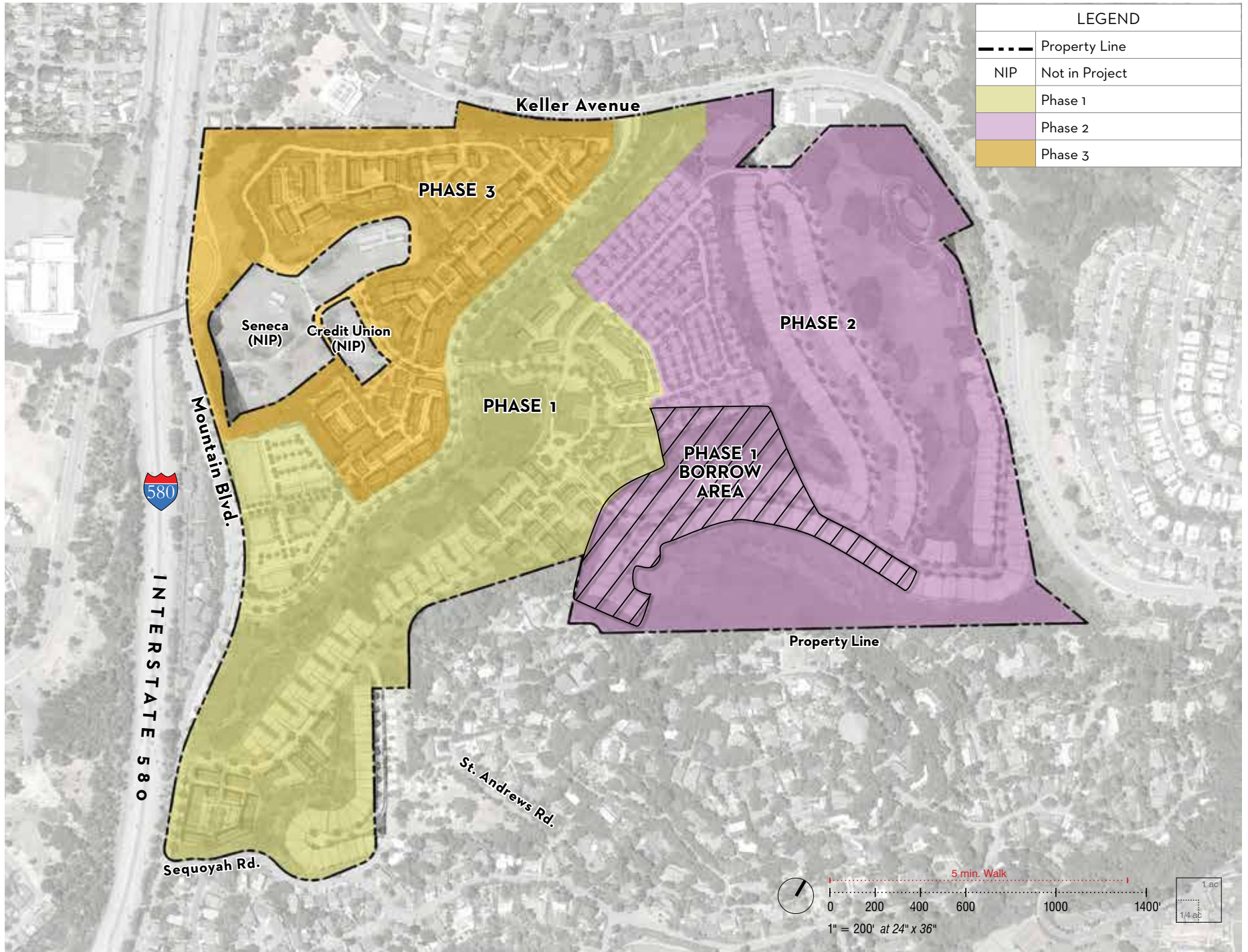


Figure 31 - Project Phasing

## PROJECT DEVELOPMENT PHASING

Refer to Figure 31 for the anticipated construction phasing. The phasing plan accommodates earthwork balance activities, logical extensions of streets and utility services, circulation and access considerations, project open space amenities, and the anticipated market timing of the home builder activity.

Within the project phases the master developer and home builder activities are anticipated to include the following. Neighborhood areas identified below can be found in Figure 10.

Phase 1 is anticipated to include the following:

- Phase 1 site grading and utilities including soil material borrow area located in Phase 2
- Master Developer Project Improvements listed on page 42 to provide streets, access and infrastructure to Phase 1 amenities and neighborhoods.
- Rifle Range Creek restoration
- Club Knoll relocation and rehabilitation
- Village Center commercial area
- Parks, open space and trails within Phase 1 areas
- Townhome parcels including Creekside South, Creekside Village 1, Creekside Village
- Single Family Detached neighborhoods including Creekside South and Uplands South parcels

Phase 2 is anticipated to include the following:

- Phase 2 site grading and utilities
- Neighborhood streets serving Creekside and Upland neighborhoods
- Neighborhood park north of Creekside Village 1 neighborhood
- Single Family Detached neighborhoods including Creekside Village 1 small lot homes and Upland East homes
- Parks, open space and trails within Phase 2 areas including Club Knoll Memorial Park (see Figure 32)

Phase 3 is anticipated to include the following:

- Phase 3 site grading and utilities
- Neighborhood streets and access way serving Village Center, Creekside North and Uplands North neighborhoods
- Townhome parcels including Village Center, Creekside North and Uplands North
- Parks, open space and trails within Phase 3

Phasing may be adjusted due to market conditions or other factors that may cause variations in both the sequencing and timing of development.



Figure 32 Artist's Concept of View West from Oak Knoll





# OAK KNOLL

## Design Guidelines

July 2017

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# 1.0 INTRODUCTION





View looking east from new bridge over Rifle Range Creek



## 1.1 COMMUNITY VISION

The Community at Oak Knoll is planned as a walkable system of neighborhoods anchored by a community scale retail village, neighborhood parks, and natural and accessible open space. The neighborhoods are woven together through an extensive system of trails and carefully designed streets. The primary landscape feature around which the land plan is shaped is the restored branch of the Rifle Range Creek and the surrounding upland areas to the north, south and east of the creek.

The local climate at Oak Knoll is characterized by temperate weather and is considered one of California's finest for habitation as well as horticulture. Because of this, the masterplan and residences are organized around the outdoors and integration with the landscape. In addition to the architectural features of the homes that provide links to the outdoors (porches, stoops, verandas, courtyards, decks, etc.), the abundance of trees and plants produces a vibrant natural setting.

Integrated into this landscape and streetscape vision, the residential neighborhoods will have a diverse mix of residential sizes and types and are planned to be executed in a range of architectural styles appropriate to the setting.



Retail Village



Community Open Space and Trails





OAK KNOLL DESIGN GUIDELINES | JULY 2017

Figure 1.1- Illustrative Master Plan



## 1.2 HOW TO USE THESE GUIDELINES

These Design Guidelines provide design principles to future builder/ applicants. Final Development Plans shall be substantially consistent with the Preliminary Development Plan. The Design Guidelines refine and clarify the direction in the Planned Unit Development and Preliminary Development Plan.

Where the Design Guidelines are silent or vague, the Preliminary Development Plan shall be used for the purposes of interpretation, and/or directly applied as appropriate.

This book is divided into three chapters: Planning, Architecture and Landscape, each of which addresses topics critical to achieving the community vision. The appendices to this book include materials, color, and plant palettes. Final Development Plans will be reviewed for their consistency with the principles and regulations set forth in these three chapters. Below is an outline of the content of each chapter:

### PLANNING

The Planning chapter addresses the selection of an appropriate plan type, the placement of the building on the lot, driveway and garage design, and building façade regulations as they relate to general neighborhood planning principles.

### ARCHITECTURE

The Architecture chapter introduces the principles of Oak Knoll architecture and how they are applied in the detailed design of a home or building. This includes the massing, roof forms, components, details, and finishes of all vertical improvements.

### LANDSCAPE

The Landscape chapter addresses landscape elements within community streetscapes, community open spaces and residential lots. This includes fences and site walls, , planting requirements, plant lists and signage regulations.



Oak Knoll Community



Typical Residential Neighborhood





Hillside Residential



## 2.0 PLANNING GUIDELINES

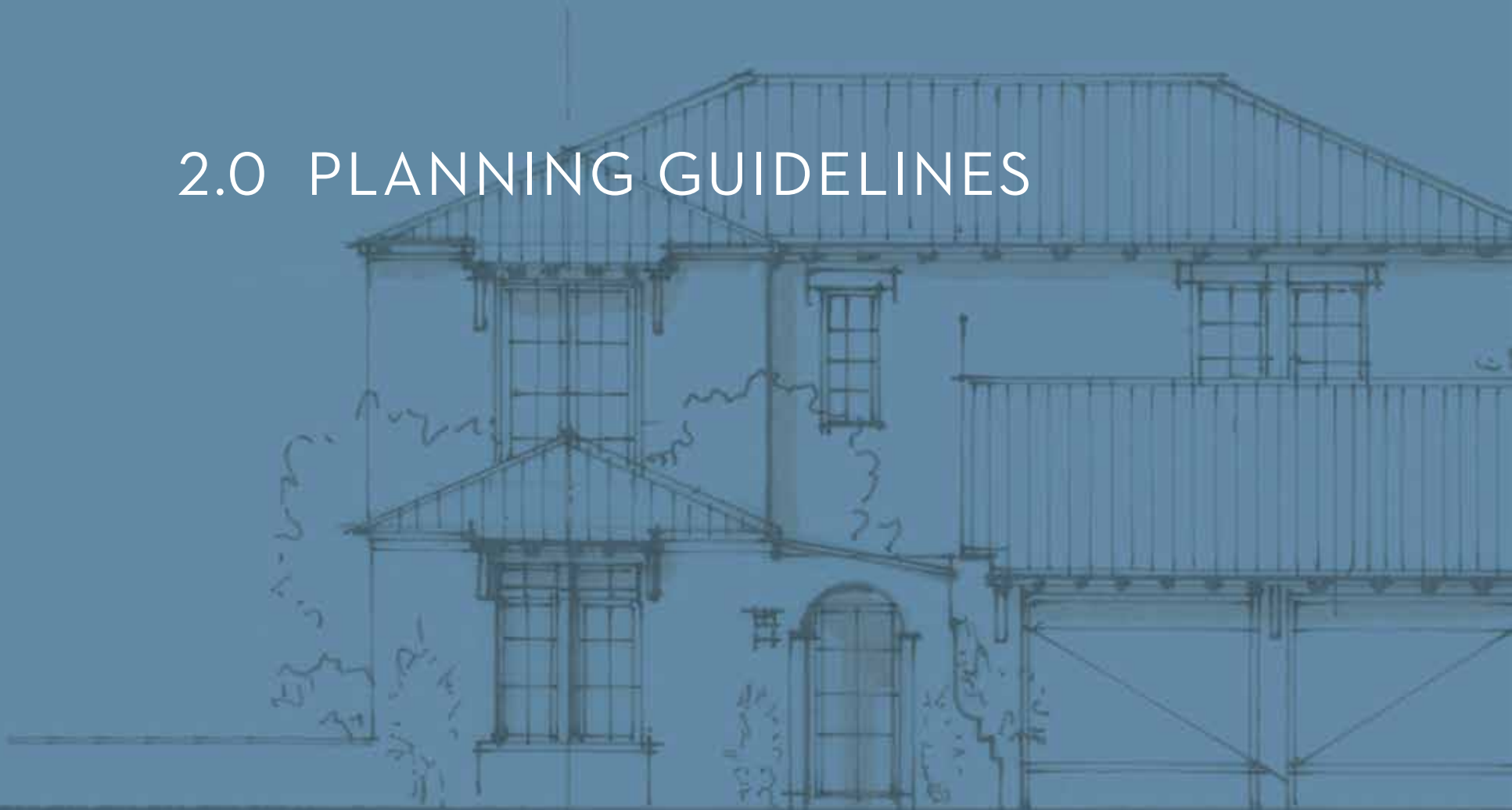




Figure 1.2- Neighborhoods Plan



## 2.1 OAK KNOLL NEIGHBORHOODS

The site at Oak Knoll features three upland areas surrounding a lowland valley with a creek running through it. The neighborhoods that make up this community are defined as the Retail Village, Creekside Village, and the Uplands areas. See Figure 1.2 Neighborhoods Plan on the facing page.

### RETAIL VILLAGE

The Retail Village area is designed as a modestly-sized gathering spot to provide basic needs to the community (such as groceries, restaurants, banking). It is envisioned as a cluster of buildings at varying scales fronting a 'Principal Drive' and a 'Plaza'. It will feature landscape and street furnishings that give it a distinct identity similar to other neighborhood centers in the Oakland hills.

### CREEKSIDE VILLAGE

The Creekside Village neighborhoods are medium density residential areas laid out in the lowland areas flanking the restored Rifle Range Creek corridor. These neighborhoods are compact and walkable and feature a framework of parks and open space which connect to the creek. The Creekside Village will offer a range of residential product types from townhomes to single family detached residences as well as the relocated and refurbished Club Knoll at its center. Club Knoll will serve as a 'community center' gathering spot.

### THE UPLANDS

Residential development in the Uplands is designed to maximize views as well as provide a pleasing appearance as viewed from adjacent areas. The Uplands neighborhoods offer a range of residential product types from townhomes to single family detached residences. The Uplands connect to the surrounding community via a system of trails and preserved open space.



Retail Village - Main Street and Plaza



Club Knoll- Community Center

## 2.2 NEIGHBORHOOD STREETSCAPES

The Oak Knoll neighborhood streets are designed to be pedestrian friendly in both function and appearance. A line of street trees flanks either side of the street, with a generous sidewalk. Planting and fences in the front yard zone between the sidewalk and the homes is designed to define an appropriate transition to the semi-private yard and porch zone. Porches will be designed as outdoor rooms and lighted to provide a sense of security to the pedestrian. Please refer to the Landscape Chapter of these Design Guidelines for specific proposed street sections.



Typical Neighborhood Streetscape



Integrated Stormwater Treatment - "Rain Gardens"



Street Trees



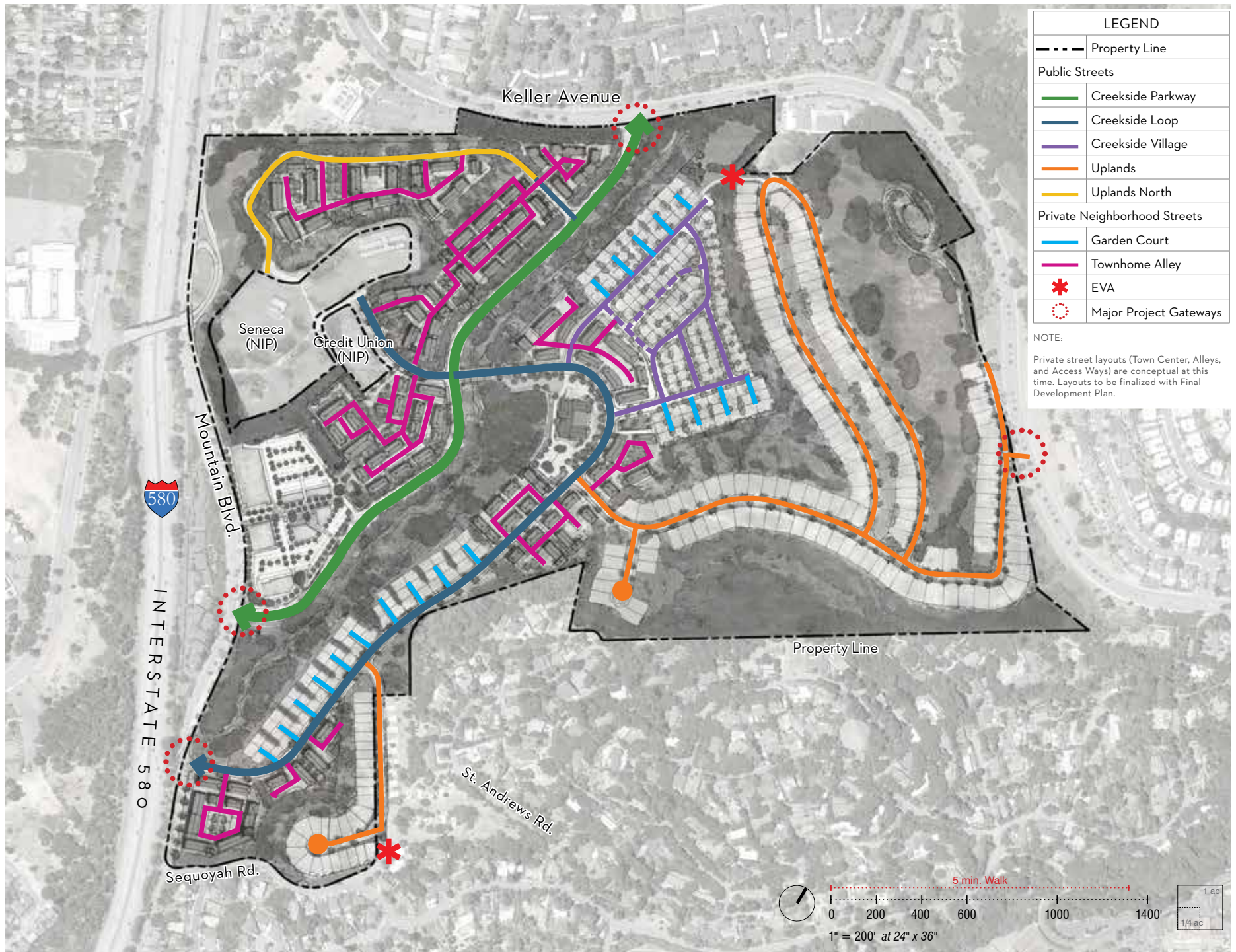


Figure 1.3- Circulation Plan

## HIGH VISIBILITY FACADES - STREET FACING

All facades which are visible from the street, including setback garages and all sections of side facing façades which are in front of garages must follow High Visibility standards as defined in the Architectural Guidelines of this book.

## HIGH VISIBILITY FACADES -OPEN SPACE FACING

The intention for an open space facing home is to present a pleasing appearance towards the open space and to take advantage of views towards the surrounding creek or woodland areas. These facades must follow High Visibility standards as well.

## ARCHITECTURAL DIVERSITY AND ‘THE MONOTONY CODE’

The purpose of the Monotony Code is to create variation and hierarchy within the Oak Knoll streetscape, giving the community an appearance of growth over time. This can be done by designing a range of home layouts and sizes that respond to the location of the lot within the overall neighborhood. The Monotony Code will be enforced through the use of a matrix tracking the following:

For each single-family detached lot type, there must be a minimum of three unique floor plan types, with three façade variations each;

- A different porch or stoop type will be considered a façade variation;
- No two detached homes of the same design may be repeated within two adjacent lots on a given Block Face or a facing Block Face;
- Homes on corner lots are encouraged to have architectural features such as wrapped porches, side porches, or bay windows facing the secondary street.
- Both the front as well as side facing facade on corner lots will be considered High Visibility Facades.



The Monotony Code aims to produce neighborhoods that have diverse streetscapes and architectural features like the images on the facing page.

The Monotony Code aims to prevent uniform streetscapes and architecture like the image to the left.





Typical Streetscapes - Creekside Village



Typical Streetscapes - Uplands



Typical Streetscapes - Uplands

## 2.3 COMMERCIAL

The Retail Village at Oak Knoll will provide public gathering spots and convenience shopping for local residents. It is envisioned as urban in character similar to other local neighborhood commercial corridors (such as sections of Park Boulevard or Lakeshore Avenue) and shall feature retail facades on the Principal Drive. The following guidelines will apply to the Retail Village:

### Design Objectives:

- Building placement that reinforces the concept of the Plaza and orients service areas away from the Plaza while keeping them screened from view from Mountain Blvd.
- 70% glazing on facades directly fronting the plaza and 50% glazing on facades fronting pedestrian pathways
- Awnings and trellis overhead canopies to provide outdoor shade and shaded gathering areas
- Sidewalk widths at primary retail facades sufficient to provide tree planting, signage, furnishings, lighting and outdoor seating areas where appropriate to adjacent retail use
- Hardscape and Planting that reinforces the outdoor pedestrian realm, but provides equal access to vehicular traffic

A final design for the retail village will be submitted by a retail developer. Retail signage locations and design shall be reviewed at the Final Development Plan submittal stage, and a Signage Master Plan will be submitted as part of that application.



Retail Village



Retail Facade Treatment Example



## RETAIL PLAZA

The Retail Village shall be designed around a centrally-located plaza. The plaza should be located at the intersection of the two primary vehicular circulation paths: one that connects Mountain Blvd and the proposed Creekside Pkwy; and one that circulates vehicles around the perimeter of the site. The plaza should be centrally located to establish the image for the project, and serve as the gathering area flanked on all sides by retail uses.

The Principal Drive section adjacent the plaza should be designed so that it may be closed off to allow for neighborhood activities such as weekend markets and street fairs. The perimeter route will continue to provide vehicular access to the surrounding retail shops and parking.

Buildings directly fronting the plaza shall orient towards the plaza. All other buildings shall orient towards the nearest primary pedestrian path.

## ARCHITECTURAL AND LANDSCAPE CHARACTER

The Character of the Retail village should be inspired by the open-air neighborhood shopping districts typical to the Bay Area. These districts are typified by simple facades with ample glazing, clerestory windows, awnings and shade structures. Recommended materials, colors and plant palettes are similar to the residential guidelines and are contained in Appendix A. The photo references on the following two pages show the features showing appropriate character for the retail buildings.



Figure 1.6- Facade Treatments, Architectural Elements,

This example of Retail Village Conceptual Layout is intended to show compliance with the design objectives of a 'Principal Drive' and 'Plaza'. Actual site and building configuration may vary.



Figure 1.5- 'Principal Drive' Conceptual Section



Trellis Structures and Integrated Planting



Clerestory Windows and Natural Lighting



Outdoor Seating Areas



Example of typical row of retail facades



Awnings and Shading Devices





Simple Facades



Lighting and Signage



Trees and plantings informally integrated into public areas



Arcades

## 2.4 TOWNHOMES

Townhome development at Oak Knoll will be designed to create functional and pedestrian friendly streetscapes. The orientation and layout of buildings should create ‘addresses’ and a sense of place for individual homes. Townhomes will feature required open space as defined in the City of Oakland’s Zoning Ordinance. Final design of towhhome parcels will be submitted to the City of Oakland in a form of a Final Development Plan, and designs will be evaluated using these guidelines.

### Design Objectives:

- Create a ‘sense of address’ and a front door for each unit by providing ‘door yards’, gates, and access to public streets and paseos;
- All units should feature covered entry areas either in the form of a stoop or entry porch;
- Variation of design is encouraged, and corner units should be treated differently than middle units;
- End facades should be treated as high visibility and should feature windows, entries where appropriate, and other design features normally on the front facade.
- Odd numbers of units in a row are encouraged;
- Stepping between units is encouraged to provide private balconies and a varied building frontage as viewed from the street.
- Landscape planting should be integrated in with streetscapes and provide screening for parking & alleys. Please refer to the Preliminary Development Plan for example designs for Paseos and Pocket Parks.



‘Door yards’ and orientation of entry onto a street or pedestrian path



Typical Elevation Example





Corner 'End Facades'



Stepped Massing



Balconies and Individualized Unit Designs



Typical Elevation Example



Typical Elevation Example

## 2.5 BUILDING MASSING AND PLACEMENT

Building setback and height requirements are contained in Zoning Ordinance and vary according to lot size and building type. In addition to those standards, the intent of these guidelines is to address additional massing considerations such as 'under the roof' or 'attic' 1/2 stories, and setback garages. These considerations are described further in the Architectural chapter.

Within the small lot area (lots of less than 4000 sf), a one story covered porch is allowed to encroach into the front yard setback as defined in the Zoning Ordinance.

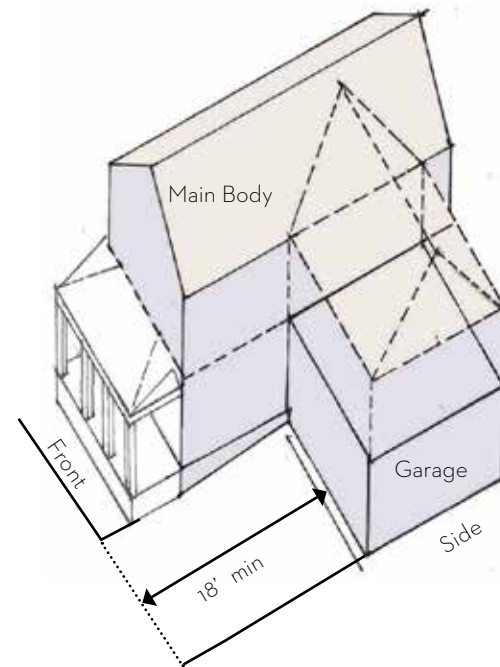


Figure 1.7, Porch and Garage Placement



## 2.5 DRIVEWAYS AND GARAGE PLACEMENT

Driveways and garages within Oak Knoll should be designed to reinforce the dominance of a tree-lined streetscape.

Limiting curb cuts to 16' in width is strongly encouraged, where feasible, for front loaded lots.

Driveway width in front of 2 car garages should be 18' wide at garage entry allowing for two off-street parking spaces in front of garage. Refer to adjacent Figure 1.8.

Refer to Chapter 4.0, Landscape Guidelines, for allowable paving materials for driveways

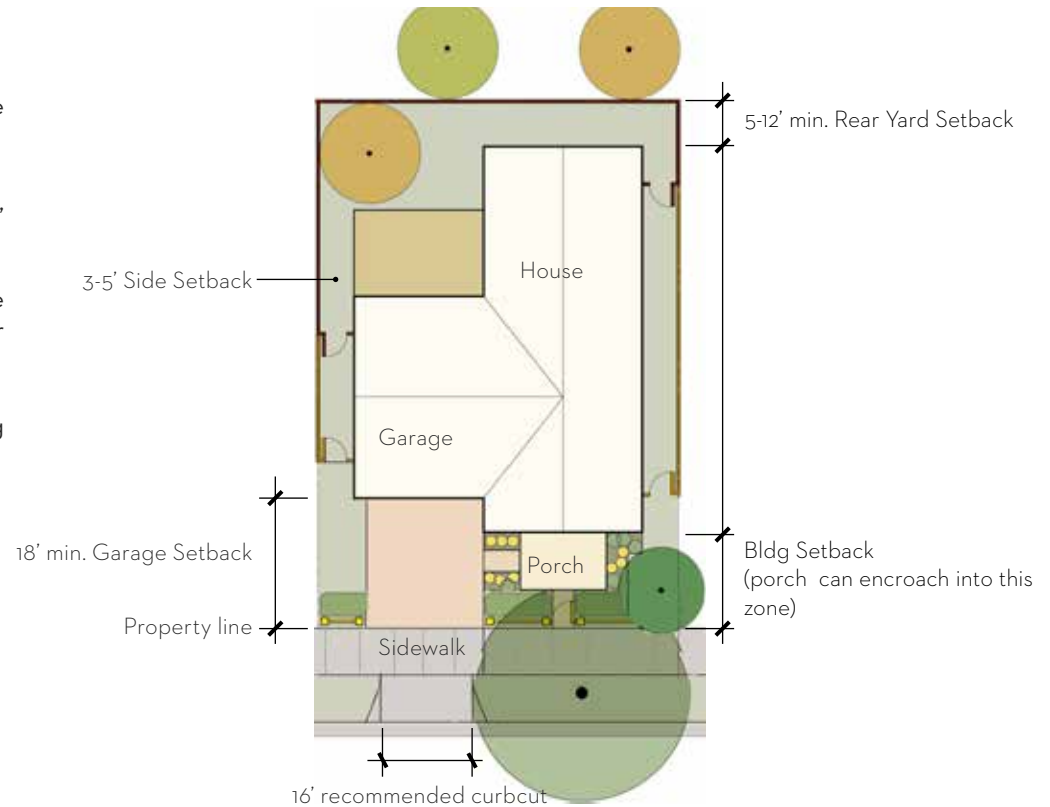


Figure 1.8, Small Lot Typical Siteplan

An architectural sketch of a building facade, rendered in a light brown or sepia tone. The drawing shows a multi-story structure with a prominent gable roof on the left side. The gable contains a window. Below the gable, there is a large window with a decorative frame. To the right of this, there are several more windows of varying sizes and styles, some with decorative elements like flower boxes or shutters. The sketch includes some loose, gestural lines representing trees or foliage on the left and bottom. The overall style is that of a preliminary architectural drawing or a conceptual sketch.

## 3.0 ARCHITECTURAL GUIDELINES



## 3.1 THE 'BAY AREA' REGIONAL STYLE

The Bay Area has a unique and home-grown residential character which has evolved in response to the local climate, cultural history, and lifestyle of its inhabitants. Many respected names in the field of architecture, landscape design and development have contributed to this character throughout history. The East Bay's early development phase included names like Bernard Maybeck, Julia Morgan, Walter Ratcliff, Henry Gutterson, Mason/McDuffie, and Frederick Law Olmsted; the mid-century included names like Joseph Eichler, William Wurster, Joe Esherick, Charles Moore, and many others. While the styles that make up this character are diverse, they are bound by common themes that form a sense of place and will inform the identity of Oak Knoll. Among these themes are:

- Buildings which connect to and are inspired by the natural setting
- Simple building mass with additive elements
- Natural Materials (wood, stone, terra cotta, stucco)
- Subdued earth-tone paint colors and light colored stuccos



Bay Area Regional Style - Shingle



Bay Area Regional Style - Contemporary



Bay Area Regional Style - Mid Century Modern

For the purposes of style classification, the following styles are identified as typical to the area and considered appropriate to Oak Knoll:

### ARTS & CRAFTS

- Craftsman Bungalow
- Shingle
- Tudor
- Arts and Crafts

### MEDITERRANEAN

- Spanish Colonial
- Mission
- Tuscan

### CALIFORNIAN

- Farmhouse
- California Modern (mid-century modern)
- California Contemporary



Bay Area Regional Style - Arts and Crafts



Bay Area Regional Style - Spanish Colonial



## THE HISTORIC PRECEDENTS

The style of the architecture at Oak Knoll draws from examples of the historic styles typical in the area. These images show inspirational historic 'ancestors'; new homes will not be replications of these but rather derived from similar design principles. These principles will then be applied to current designs taking into consideration today's materials, construction practices, and modern lifestyles.



Bay Area Regional Style - Craftsman Bungalow



Bay Area Regional Style - Tuscan



Bay Area Regional Style - Arts and Crafts



Bay Area Regional Style - Contemporary

## 3.2 ARCHITECTURAL STYLE MATRIX - BY FAMILY

	Massing / Roof Form	Windows and Doors	Porches / Balconies and Details	Materials and Color*
<b>ARTS AND CRAFTS</b>				
Craftsman Bungalow	<ul style="list-style-type: none"> <li>Lower pitch gable roofs (4/12 - 8/12)</li> <li>Broad eaves with exposed rafters</li> <li>Wide shed dormers</li> <li>1/2 story upper floors</li> </ul>	<ul style="list-style-type: none"> <li>Double hungs, single or grouped</li> <li>Casements, single or grouped</li> <li>Bay windows</li> <li>Wide panelled entry doors</li> </ul>	<ul style="list-style-type: none"> <li>Wide porches, often covered by primary roof form, integrated into primary mass</li> <li>Thick porch columns</li> </ul>	<ul style="list-style-type: none"> <li>Wood or composite siding and trim</li> <li>Cast stone brick used as accent materials</li> <li>Subdued earth tone colors with warm accent colors</li> </ul>
Shingle	<ul style="list-style-type: none"> <li>Medium pitch gable and hip roofs (6/12 - 12/12)</li> <li>Gable and shed dormers</li> <li>1/2 story upper floors</li> </ul>	<ul style="list-style-type: none"> <li>Cottage style double hungs</li> <li>Casements, single or grouped</li> <li>Bay windows</li> <li>Panelled entry doors</li> </ul>	<ul style="list-style-type: none"> <li>"innie" porches, often covered by primary roof form, integrated into primary mass</li> </ul>	<ul style="list-style-type: none"> <li>Wood shingle with wood or composite trim</li> <li>Natural stained base color with darker accent color on trim</li> </ul>
Tudor Arts and Crafts	<ul style="list-style-type: none"> <li>Steeper pitch gable roofs (8/12 - 18/12)</li> <li>Cross-gables and dormers</li> <li>1/2 story upper floors</li> <li>Assymetric massing</li> </ul>	<ul style="list-style-type: none"> <li>Casements, single or grouped</li> <li>Bay windows</li> <li>Arched entry doors, pointed or round</li> </ul>	<ul style="list-style-type: none"> <li>Assymetric entry features (stoops or porches) integrated into primary bldg mass</li> </ul>	<ul style="list-style-type: none"> <li>Wood and stucco walls</li> <li>Brick used as accent material</li> <li>White or rich earth tone base color with darker accent color on trim</li> </ul>
<b>MEDITERRANEAN</b>				
Spanish Colonial Mission Revival	<ul style="list-style-type: none"> <li>Lower pitch hip roofs (4/12-8/12)</li> <li>Secondary shed roofs</li> <li>Secondary flat roofs areas with shaped parapets</li> </ul>	<ul style="list-style-type: none"> <li>Casements, single or grouped</li> <li>Tall double hungs</li> <li>Arched entry doors</li> </ul>	<ul style="list-style-type: none"> <li>Covered entry arcades</li> <li>Juliet balconies</li> <li>Painted metal railings and window grilles</li> </ul>	<ul style="list-style-type: none"> <li>Stucco walls</li> <li>Terra cotta roof tiles</li> <li>Colored glazed tile and cast ornamental details as accents</li> <li>Light colored walls</li> </ul>
Tuscan	<ul style="list-style-type: none"> <li>Lower pitch hip roofs (4/12-8/12)</li> <li>Projected eaves with flat soffit and corbels</li> </ul>	<ul style="list-style-type: none"> <li>Tall casement style windows</li> <li>Pedimented front entry</li> </ul>	<ul style="list-style-type: none"> <li>Loggias at entry or upper level</li> <li>Balconies with balustrades or painted metal railings</li> </ul>	<ul style="list-style-type: none"> <li>Stucco walls</li> <li>Cast stone as accents</li> <li>Earth colored walls (sienna and umber)</li> </ul>
<b>CALIFORNIAN</b>				
Farmhouse Traditional	<ul style="list-style-type: none"> <li>Medium pitch gable roofs (6/12 - 12/12) and simple primary mass</li> <li>Gable and shed dormers</li> <li>1/2 story upper floors</li> </ul>	<ul style="list-style-type: none"> <li>Double hungs, single or grouped</li> <li>Casements, single or grouped</li> <li>Panelled entry doors</li> </ul>	<ul style="list-style-type: none"> <li>Front or side porches - shed roof, additive to primary mass</li> <li>Shed awnings</li> </ul>	<ul style="list-style-type: none"> <li>Vertical wood or composite siding, board and batten</li> <li>White and light colors</li> </ul>
Mid Century Modern	<ul style="list-style-type: none"> <li>Lower pitch gable and hip roofs (3/12 - 6/12)</li> <li>Broad horizontal eavelines, with exposed rafters or flat soffits</li> <li>Secondary shed roofs</li> <li>Secondary low-pitch shed roofs</li> </ul>	<ul style="list-style-type: none"> <li>Wide horizontal window rows</li> <li>Sliding, double hung, or casement</li> <li>Corner windows</li> </ul>	<ul style="list-style-type: none"> <li>Wide extended eaves over entry areas</li> </ul>	<ul style="list-style-type: none"> <li>Vertical or horizontal wood or composite siding</li> <li>Subdued natural colors</li> </ul>
Contemporary	<ul style="list-style-type: none"> <li>Stepped building massing</li> <li>Flat roofs</li> <li>Monopitch roof, split gable monopitch</li> </ul>	<ul style="list-style-type: none"> <li>Large expanses of glass, gridded or single frame</li> <li>Sliding or casement</li> <li>Corner windows</li> </ul>	<ul style="list-style-type: none"> <li>Flat roof or shed awning over entry</li> </ul>	<ul style="list-style-type: none"> <li>Vertical or horizontal wood or composite siding</li> <li>Smooth-textured stucco walls</li> <li>Whites and subdued natural colors</li> </ul>

\*See Appendix A for detailed spreads of recommended Materials and Colors





Craftsman Bungalow



Shingle



Tudor



Arts and Crafts



Spanish Colonial



Spanish Colonial



Mission



Tuscan



Farmhouse



Mid Century Modern



Contemporary



Contemporary

## 3.3 MASSING – PRIMARY VOLUMES

### BUILDING ORIENTATION

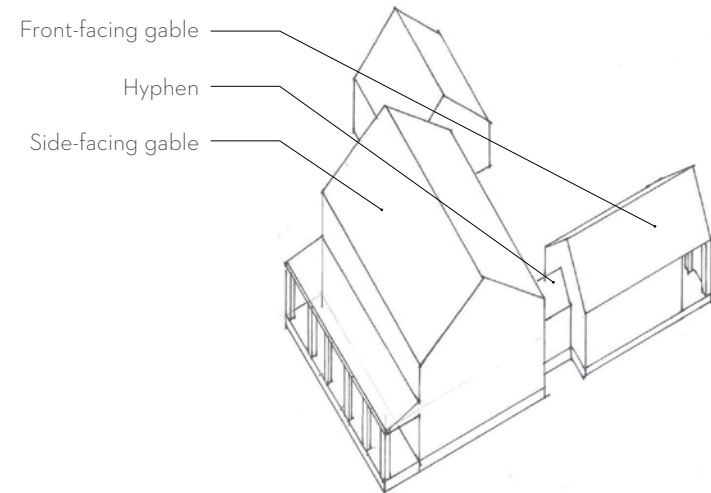
A variety of symmetrical and asymmetrical massings can be used when laying out an Oak Knoll home. First, it is important to determine the building orientation based on lot conditions. Typically, this will be perpendicular to the street.

### SECONDARY VOLUMES

Next, an assessment of secondary volumes -- garages and additional building wings -- will help determine the appropriate roof profile. This may be gable, gambrel, hip, shed, or a combination thereof. For further information on roof profiles, see Section 3.3: Roofs.

### ADDITIVE BUILDING ELEMENTS

Ultimately, the massing should be simple and understated, and should provide a backdrop to unique building elements like porches, dormers, and other details while logically shaping the interior spaces.



Primary and Secondary Volumes

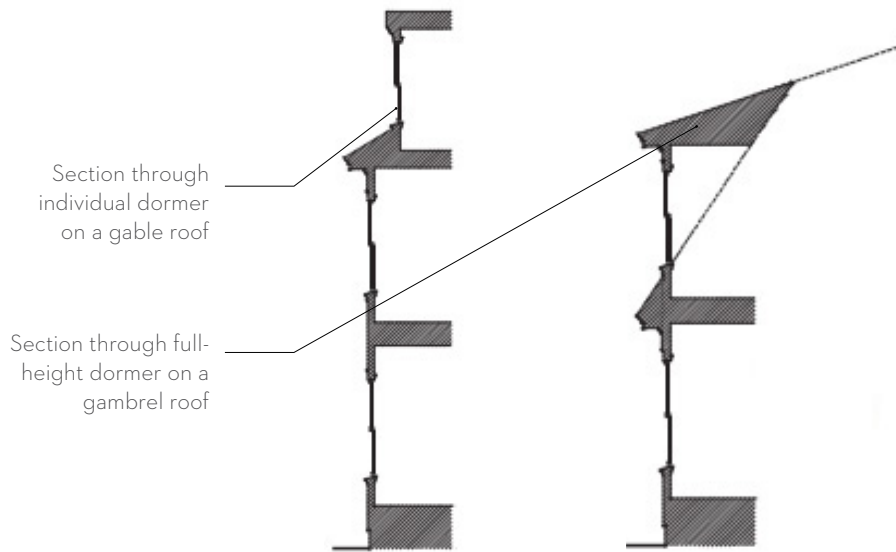


# 3.4 ROOFS

## ROOF TYPES AND SLOPES

Acceptable roof types include front-facing and side-facing gable, gambrel, hip, and shed roofs, or some combination thereof. Flat roofs are permissible but will be reviewed during the Final Development Plan (FDP) process for their visual impact on adjacent properties. Please refer to the Architectural Style Matrix on page 30 for roof design recommendations by style.

As appropriate to the chosen style, 'under the roof' style upper levels are encouraged to diminish the bulk of 2 and 3 story homes. These are referred to 1.5 or 2.5 story homes in this document.



Flat roofs



12/12 steeper pitch roof with dormers



3/12 shed roof



12/12 steeper pitch roof with dormers



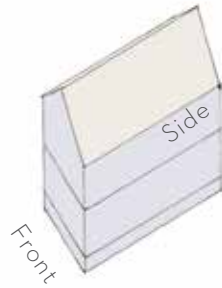
4/12 tile roof

1. PRIMARY VOLUME

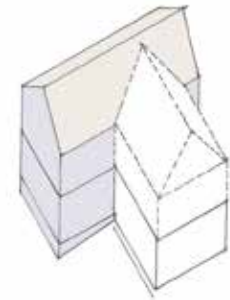
2. SECONDARY VOLUME

DETACHED LOTS

- Most small lots will have Primary Volumes that are oriented perpendicular to the street.
- May be one to 2.5 stories.



- Most Secondary Volumes on small lots will be oriented perpendicular to Primary Volumes
- Shall be setback from the Primary Volume a minimum of two (2) feet
- May be one to 2.5 stories.
- May be detached from the Primary Volume.

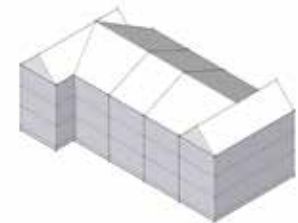


ATTACHED UNITS - TOWNHOMES

- Primary Volumes oriented parallel to the street or entry walk
- May be three stories



- Create secondary volumes to differentiate roofscape where appropriate
- Corner Units should be differentiated



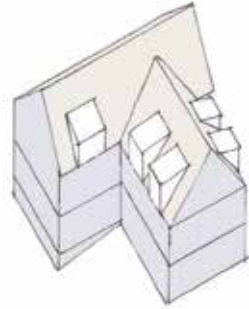


### 3. COMPONENTS/FEATURES

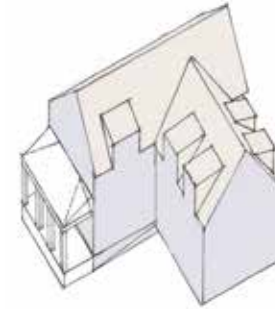
### 4. PORCHES

#### DETACHED LOTS

- Dormers, chimneys, bay windows, and other facade components shall be added to provide facade interest.

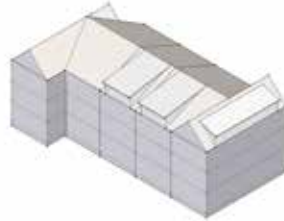


- Porches may be single-bay, full-width, wraparound or stacked.
- Refer to pages 46 and 47 for examples of porch types.

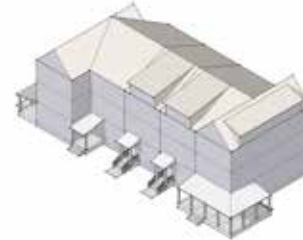


#### ATTACHED LOTS - TOWNHOMES

- Dormers, chimneys, bay windows, and other facade components shall be added to provide facade interest.

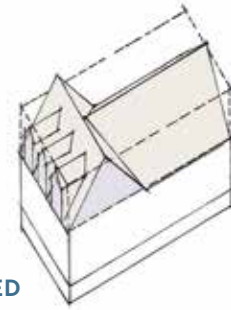
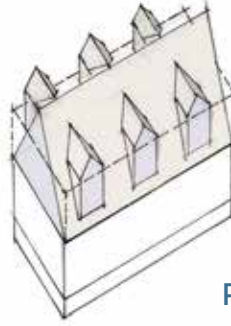
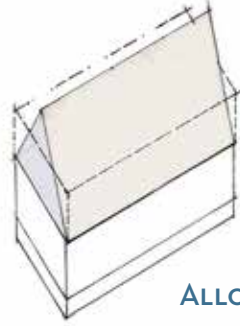
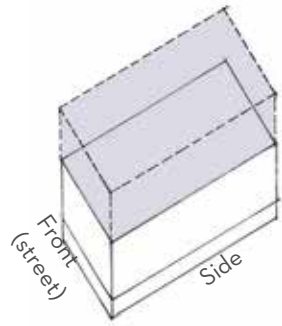


- Porches may be single-bay, full-width, wraparound or stacked.
- Refer to pages 46 and 47 for examples of porch types.

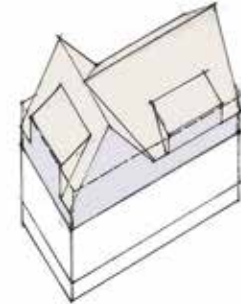
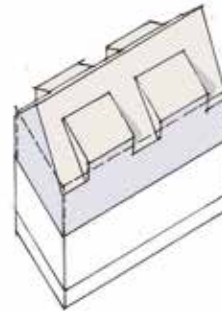
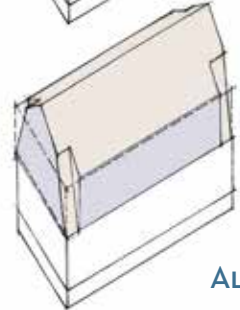
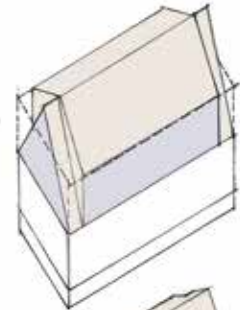
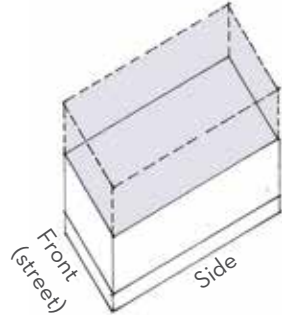
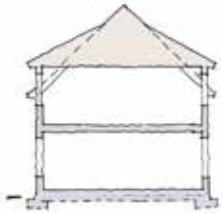


ROOF & DORMER OPTIONS- PERPENDICULAR TO THE STREET

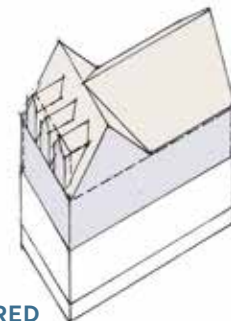
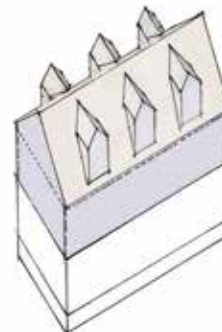
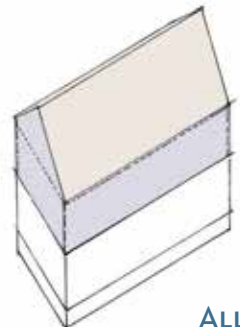
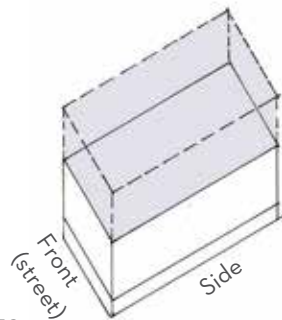
1.5 STORIES



2 STORIES



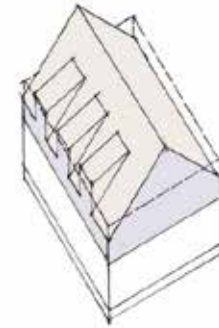
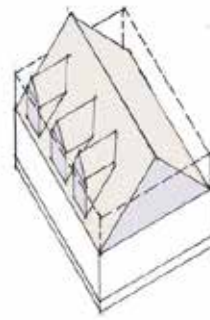
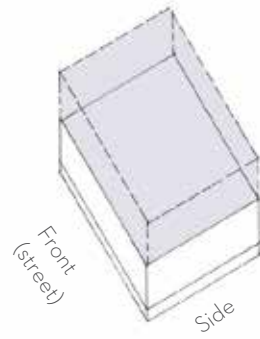
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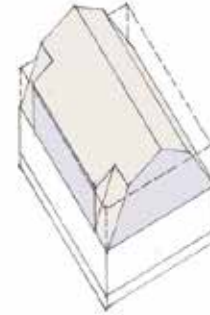
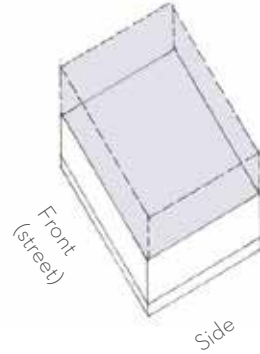
# ROOF & DORMER OPTIONS- PARALLEL TO THE STREET

1.5 STORIES



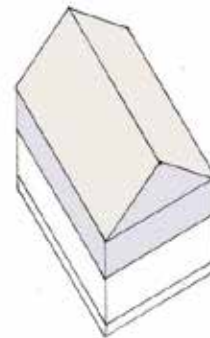
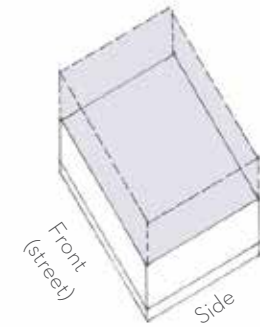
PREFERRED

2 STORIES



ALLOWED

2.5 STORIES



ALLOWED

## ROOF MATERIALS

Please refer to the Appendix A for acceptable roof materials.

## SUCCESSFUL ROOF DESIGNS

While a variety of roof types suit the Oak Knoll home, successful designs will support the simple massing of primary and secondary volumes and interior spaces, visually reduce the scale of the home, and provide adequate shading.

## DORMER SIZING

Dormers are an important element that allow upper stories of a home to be usable as well as visually diminishing the scale of the upper level. Dormers may contain one window, or a string of windows, depending on the functional needs of the spaces within.

Dormer eaves and overhang details should be scaled accordingly, and should be consistent with the overall roof details.

All dormers shall be functional and bring light into occupiable interior spaces.

## DORMER SIDING

Siding may be applied on the side or front walls of dormers either horizontally or sloped to match the adjacent roof.



1/2 Story 2nd floor with dormers



## 3.5 HIGH VISIBILITY FACADES

High Visibility Facades are visible from the street and from open space. High Visibility Facades are entry facades; hillside rear facades (facing the view); and corner lot facades. The High Visibility Facade of all homes should welcome residents, be inviting to neighbors and guests, and must follow guidelines defined in this section.

### HIGH VISIBILITY FACADES - OPEN SPACE

While the entry facades of all homes in Oak Knoll shall be considered High Visibility Facades, select facades that face the Open Space shall also be considered High Visibility Facades. Use of porches and balconies are encouraged on these facades, and they should be designed with their visibility in mind, as well as the privacy of the homeowner.

### CORNER LOT FACADES

Corner lot facades should carry distinct compositional and material elements from the entry facade to the side facade, to create a dynamic perspective of the home from the street. Corner lot facades shall have consistent details and elements on elevations facing both streets. The rhythm of openings established on the entry facade shall continue on the side facade that faces the street, and divided window patterns shall be consistent on both elevations. If shutters are incorporated on the entry facade they shall likewise be incorporated on the side facade that faces the street.

### ADDITIVE FACADE ELEMENTS

Once the design of the High Visibility Facade openings has been determined, additive building elements like porches and dormers should follow the rhythm of the facade composition. Wraparound porches are encouraged on corner lots, as well as projected window bays. Porch columns should be spaced equally to either side of facade openings. Satellite dishes and external antennas are not permitted on High Visibility Facades.

### SUCCESSFUL EXECUTION OF SECONDARY FACADES

Secondary Facades that successfully follow the above guidelines will support a composition of the Bay Area home that is balanced and continuous rather than one-sided and fragmented.



Secondary Facades

## 3.6 OPENINGS - WINDOWS

### WINDOW TYPES

Homes may have single-hung, double-hung, triple-hung, awning, and casement windows. Slider style windows are also permissible where appropriate to the chosen style (see style matrix on page 30). Square transom windows are allowed on Secondary Facades, and may be used in bedrooms, bathrooms, stairwells, etc. Arch windows shall be permitted where appropriate to the style. Please refer to the Architectural Style Matrix on page 30 for window recommendations by style. Bay windows may encroach into front and rear yard as permitted by zoning and building codes. All windows shall be fully trimmed on the exterior with appropriate head, side, and sill details. Mitered joints are not permitted.

### WINDOW PROPORTIONS AND TRIM

Windows may be mullered together to achieve wider expanses of glass, but shall not exceed 12' in total width. Windows may have no muntins, a 2 over 2, 4 over 1, 4 over 4, 6 over 1, or 6 over 6 muntin pattern. True divided lites are preferred, simulated divided lites are acceptable, and removable muntins are prohibited. Wood and composite trim materials are permitted and foam trim is not allowed.

### SHUTTERS

If shutters are incorporated in Primary Facades they should likewise be incorporated in High Visibility Facades. Each shutter shall be a minimum of half of the window dimension. Louvered or panelized shutters are acceptable.



Casement Style Windows



Double-hung Windows

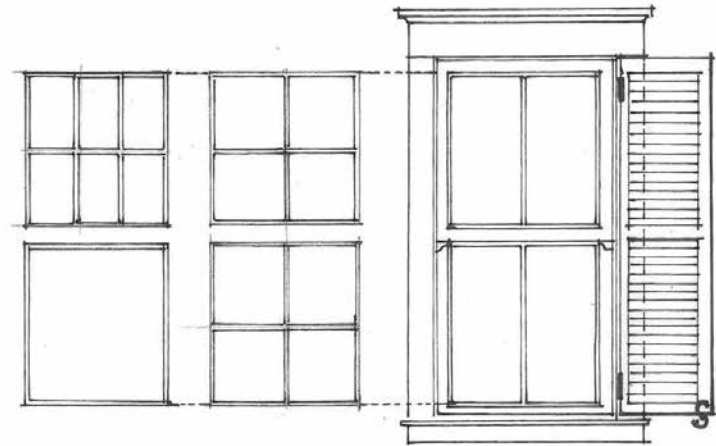


Casement Style Windows

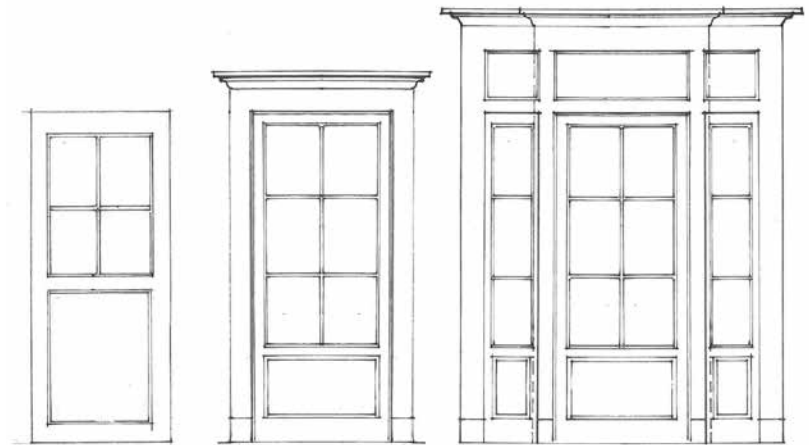




Casement Style Windows



6 over 1; 4 over 4; and 2 over 2 muntin patterns



## 3.7 EXTERIOR DOORS

Exterior Main Entry doors can be flat or traditionally paneled doors. Please refer to the Architectural Style Matrix on page 30 for design recommendations by style.

## 3.8 PORCHES & STOOPS

### TYPES

The porch or stoop is the signature element of an Oak Knoll home. As such, all homes must have either a porch or stoop. Multiple types are acceptable and encouraged, from single-bay to full-width, wraparound, and stacked. Pages 41 and 42 illustrate examples of porch and stoop types. Entry Courts are also an entry feature of certain styles of homes and are addressed in the landscape section.

### PORCH DIMENSIONS

All porches or stoops on detached homes shall have a minimum covered area as defined in the Zoning Ordinance, and designed in a manner appropriate to the style of architecture.

Single story covered porches are allowed encroachments into front yard setbacks as defined in the Zoning Ordinance.



Entry Porches



## PORCH DETAILS

Porch columns should be with round or square profiles, and shall have a minimum dimension of six (6) inches. Columns shall have defined capitals and bases. Porch design should be consistent with the chosen style of the house. Please refer to the Architectural Style Matrix on page 30 for design recommendations by style.

Porch beams shall align over supporting columns. Porch eaves and rakes should extend beyond porch beams. Porch ceilings must be fully trimmed.

Porch railings and balusters where required by code should be designed in a manner appropriate to the chosen style.

## STOOPS

Stoops should be detailed in a similar way to porches, but are smaller in size and may be in the form of a recessed entry that indents into the building wall. The landings and covered areas of stoops shall have minimum dimensions as defined in the Zoning Ordinance.



Entry Porch



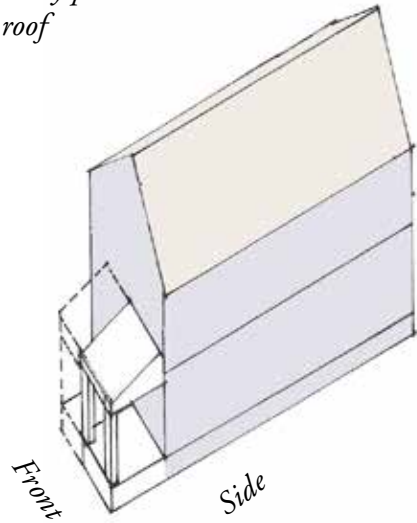
Recessed Entry



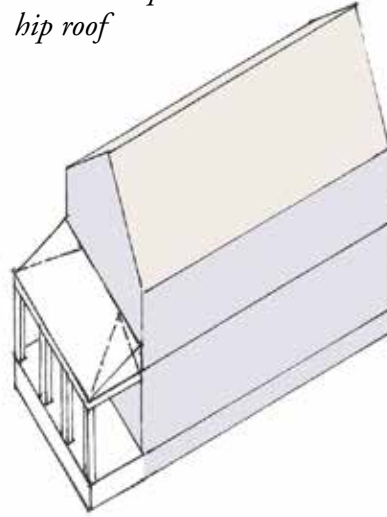
Entry Porch

3.6 PORCHES- *Porch and Stoop Types*

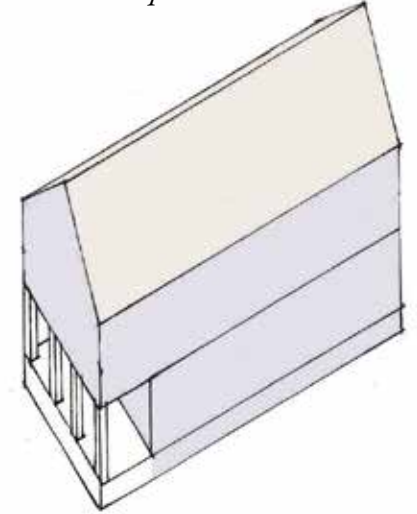
*Single-bay porch with shed roof*



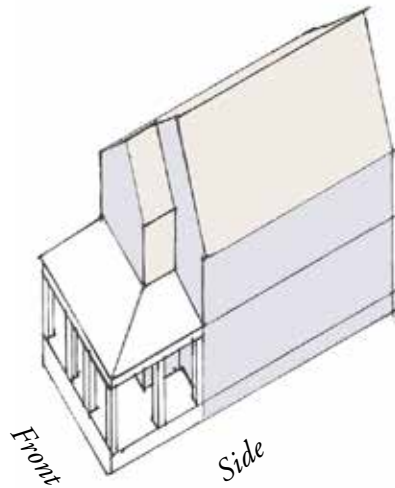
*Full-width porch with hip roof*



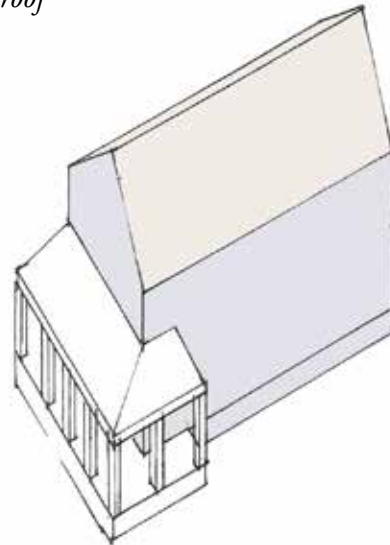
*Full-width porch*



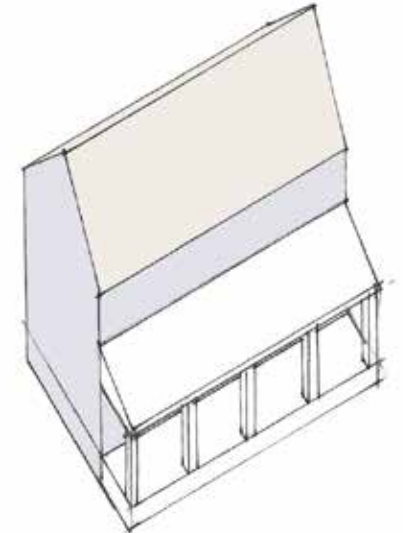
*Wraparound porch with hip roof*



*Wraparound porch with hip roof*

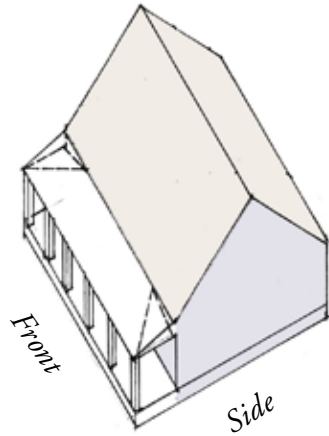


*Side porch with shed roof*

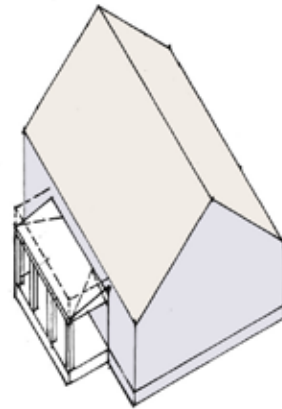




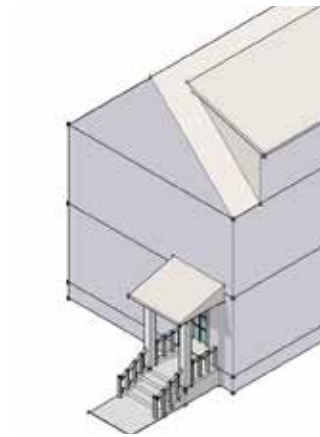
*Full-width porch*



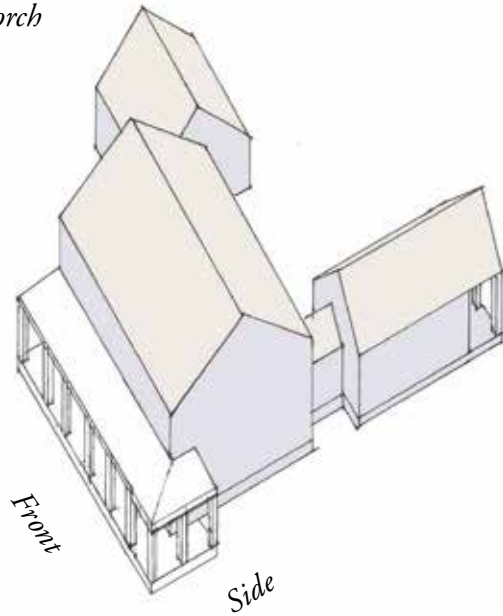
*Single-bay porch*



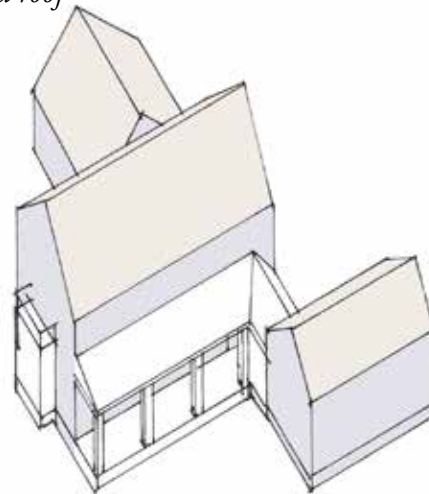
*Standard Stoop*



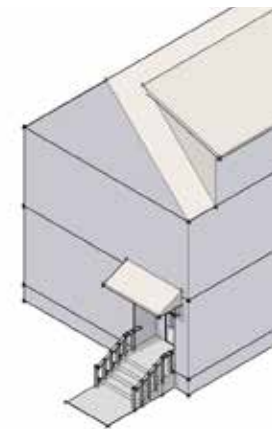
*Full-width porch*



*Single-bay porch with flat roof; side porch with shed roof*



*Inset Stoop*



## PORCH MATERIALS

Porches may be constructed from wood, wood-like composites, stucco, or concrete faced with brick or stone veneer. Entry stoops, porches, and stairs that are constructed with wood shall be screened with wood or manufactured wood trim or lattice.

Entry stoops or porches constructed of masonry may have stair risers and treads constructed of masonry and may be finished with brick pavers. When finished walking surfaces, including stair treads, are brick pavers, all vertical surfaces from the top of the porch deck to grade shall be of brick. Bare or painted concrete is also a permitted finish material.

The porch ceiling may be composite bead-board planks or wood with appropriate molding.

Composites, and fiberglass trim moldings are allowed on a case-by-case basis as reviewed in Design Review. Vinyl or foam trim is prohibited. Refer to Chapter 4.0: Landscape for planting requirements at base of porch.





## 3.9 GARAGES

### GARAGE DIMENSIONS

The design and placement of garages is important to the creation of a pleasing streetscape and a domestic 'neighborhood' feel. A minimum setback is required to allow a parked car in front of garage within the lot and not overlapping with adjacent sidewalks or common drive aisles. Exceptions may be granted through the Design Review process to allow the garage face closer to the front lot line.

Garages preferably will have individual carriage doors, but double garage doors are permissible as well. Garage doors must abide by the minimum and maximum dimensions defined in the Zoning Ordinance. Garage doors should be traditionally panelled, and windows courses at the upper panel are desirable features. Single car garages and tandem garages are also encouraged where circumstances favor this layout on the lot.

### GARAGE DETAILS

Where the garage is not integrated into the primary mass of the home, they shall be considered a Secondary Volume, and shall have the same or shallower roof pitch than that of the Primary Volume of the home, and set back from the Primary Volume. Garage doors should incorporate details that complement the design of Entry and Secondary Facades, such as windows, patterned paneling, trellis and roof details.



## 3.10 LIGHTING

### ARCHITECTURAL LIGHTING

A well-lighted porch or stoop is a critical element to the Oak Knoll streetscape. As such, all homes must have porch lighting appropriate to the style of the house.

Garages should also have exterior lighting integrated over or adjacent to the door bays, and should be shielded.

No uplighting of buildings is permitted.





## 4.0 LANDSCAPE GUIDELINES



View looking east from new bridge over Rifle Range Creek



## 4.1 LANDSCAPE VISION

The landscape design for Oak Knoll is intended to integrate the new community into an existing context of hillsides, creeks and drainages, and oak woodland. Proposed landscape elements will reinforce the new patterns of buildings, roads, public spaces, and recreational and open space amenities, creating a framework for the new community while also providing for habitat restoration and sustainability.

The proposed landscape elements are also intended to celebrate the rich heritage and historic context of the North Oakland communities, celebrating historic community values of respect for land, nature, and a tradition of craftsmanship that was expressed by neighborhood community designs at the turn-of-the century in the East Bay neighborhoods of Berkeley, Claremont, Piedmont, and Rockridge.

The community landscape is shaped to provide accessible open space and access to nature, and to encourage active and healthy outdoor living. Neighborhoods are woven together with natural open space settings and carefully designed streets with generous sidewalks, punctuated by neighborhood and pocket parks. Landscape materials emphasize the use of native materials and plant communities within natural settings, intended to restore the site's ecology and benefit wildlife, while presenting a unique, nature-rich environment for the residents of the community.

In order to promote the conservation and efficient use of water, landscaping shall comply with the provisions established in the California Code of Regulations Title 23, Division 2, Chapter 2.7 Model Water Efficient Landscape Ordinance (MWELO).

## 4.2 STREETScape DESIGN

Street trees are important thematic elements of the neighborhoods, where strong patterns reinforce the neighborhood identity, define road edges and provide a shaded overstory. A strong overhead tree canopy will modify the climate and bring streets into a more balanced scale with the overall landscape.

The community streetscape features a natural setting of Coastal Oak woodland, comprised of native London Plane and Coast Live Oak along street edges. Plantings are low groundcovers in composed drifts, using selections from the Approved Plant List. (Refer to the Preliminary Development Plan (PDP) for public street sections.)

The Creekside Village, Retail Village, Creekside Townhomes and Garden Court neighborhoods utilize native Oak woodland as the predominant framework plantings, with London Plane, Trident Maple, Red Maple and Brisbane Box as canopy street or parking lot trees.



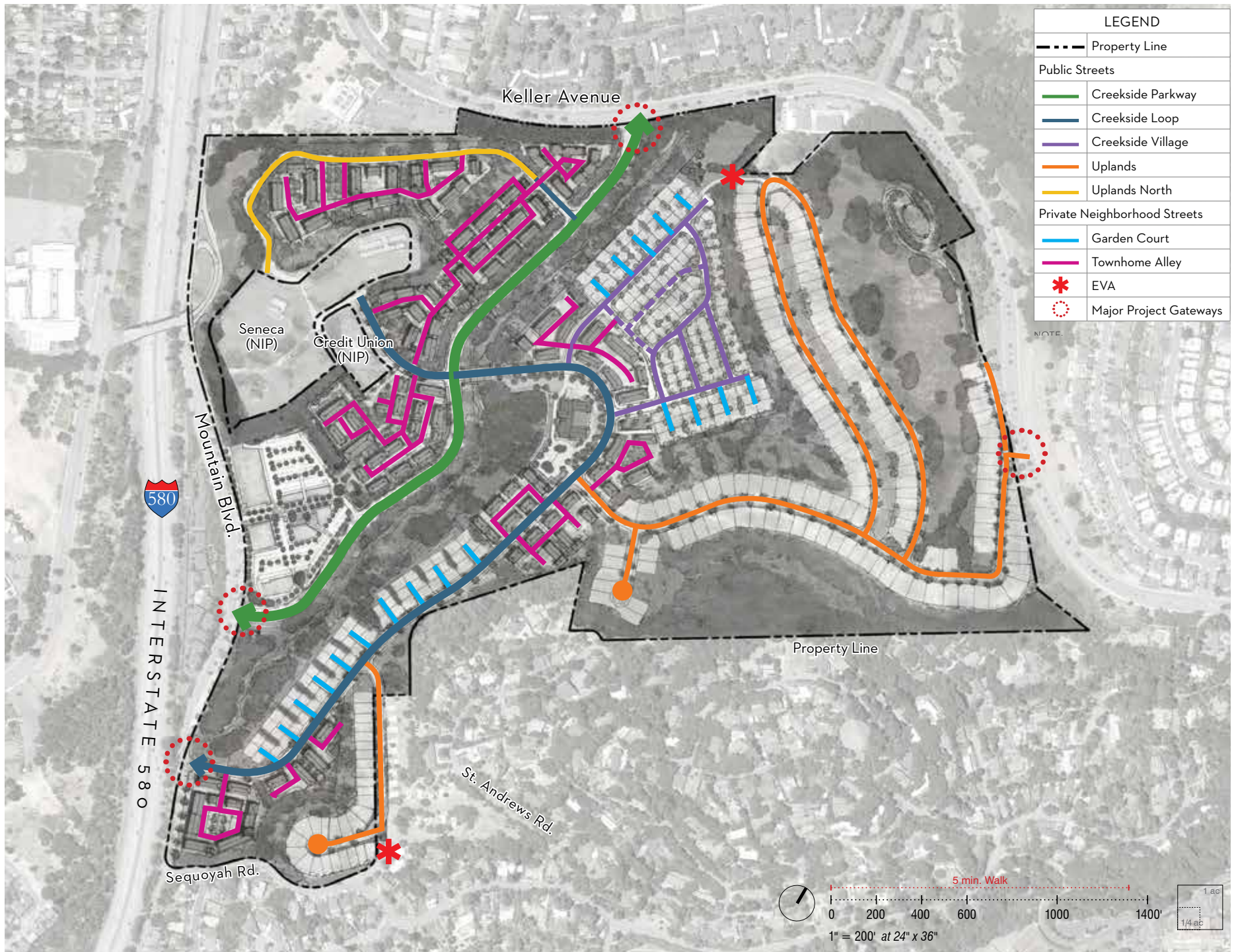


Figure 4.1- Circulation Network

	CREEKSIDE PARKWAY	CREEKSIDE LOOP	CREEKSIDE VILLAGE	UPLANDS	TOWNHOME ALLEY	GARDEN COURT
<b>On-street Parking</b>	Parallel on 2 Sides	Parallel on 1 Side	Primary: Parallel on 1 side Secondary: None	Primary: Parallel on 2 sides Secondary: Parallel on 1 side	None	None
<b>Bike Lane</b>	Class I multi-use path	Class III	None	None	None	None
<b>Landscape Character</b>	- Large deciduous street tree	- Large deciduous street trees	- Large deciduous street trees with medium evergreen and deciduous alley trees  - Fall color	- Large evergreen street tree	- small to medium deciduous or evergreen narrow trees  - Fall color	- Medium deciduous trees  - Fall color
<b>Drainage</b>	Bulb-out Infiltration Basins in ROW	Bulb-out Infiltration Basins in ROW	Localized detention and infiltration basins	Bulb-out Infiltration Basins in ROW/ In Board Bioswales	In-tract detention and infiltration basins	Localized detention and infiltration basins
<b>Drainage in R.O.W.</b>	Yes	Yes	No	Yes except Uplands North	No	No
<b>Representative Landscape Species</b>	STREET TREE: - London Plane or - Accolade Elm	STREET TREE: - London Plane 'Columbia'	STREET TREE: - London Plane  ALLEY TREE: - Trident Maple - 'Elegant' Brisbane Box	STREET TREE: - Coast Live Oak	STREET TREES (North Creekside): - 'Redpointe' Red Maple  (South Creekside): - Trident Maple	COURT TREE: - 'Redpointe' Red Maple



London Plane



Coast Live Oak



'Redpointe' Red Maple



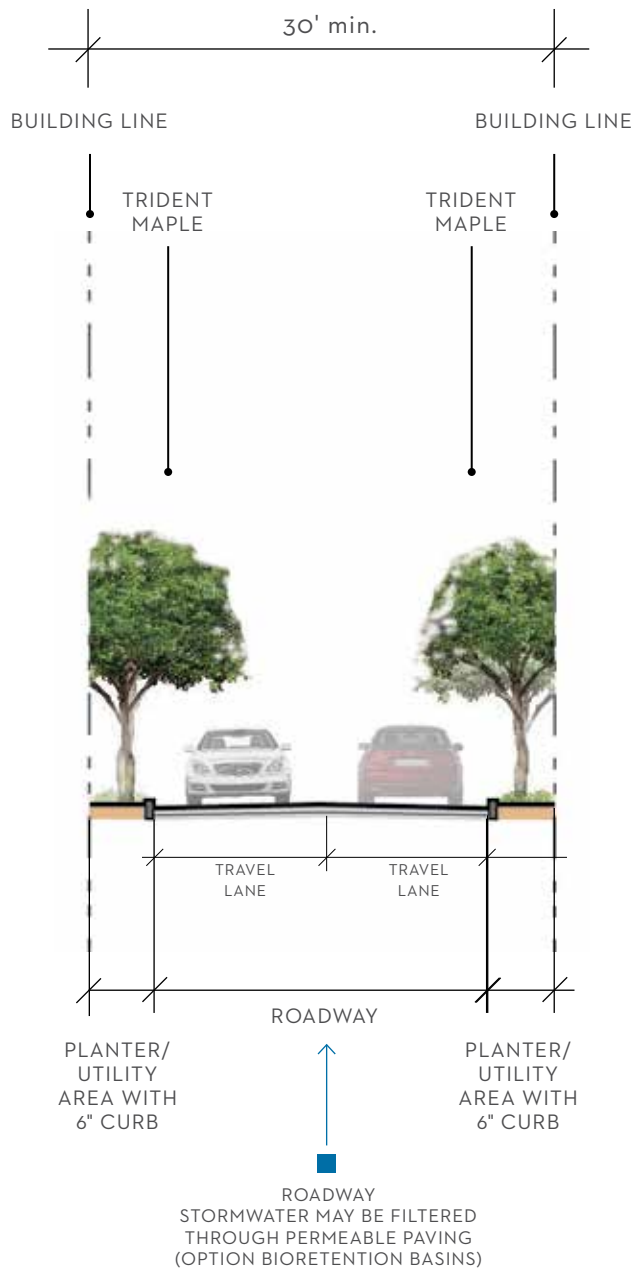
Trident Maple



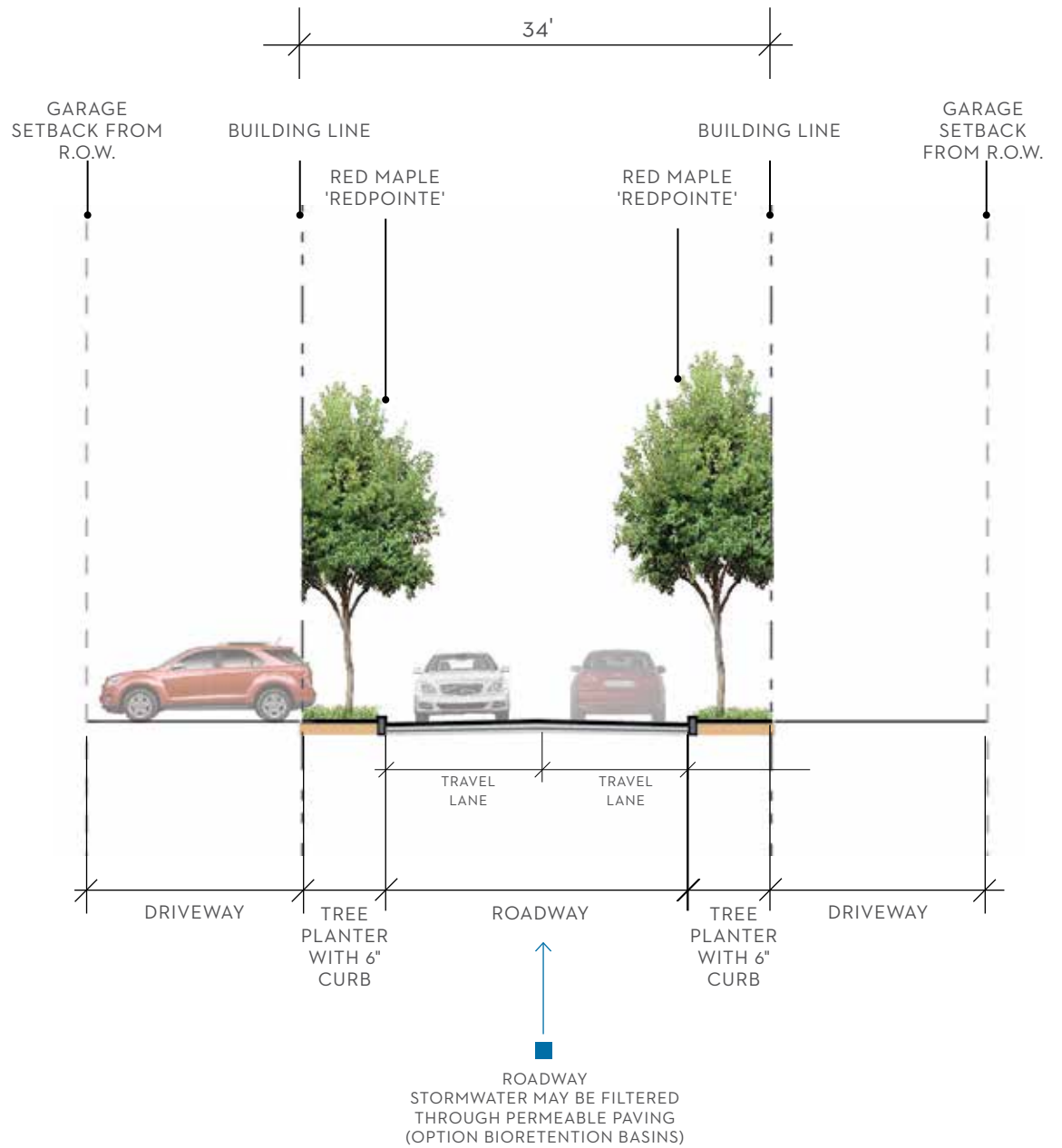
Brisbane Box



# TOWNHOME ALLEY 'TYPICAL'



# GARDEN COURT 'TYPICAL'



# Neighborhood Streetscape

## Proposed Plant List

Botanical Name	Common Name	Size/ Foliage Character										
		Native	Large Deciduous Trees >30'	Large Evergreen Trees > 30'	Small Deciduous Trees <30'	Small Evergreen Trees <30'	Large Deciduous Shrubs >4'	Large Evergreen Shrubs >4'	Small Deciduous Shrubs <4'	Small Evergreen Shrubs <4'	Fall Color	Wildlife Forage
<b>Trees</b>												
<i>Acer buergeranum</i>	Trident Maple				x						x	
<i>Acer rubrum</i> 'Redpointe'	Red Maple		x								x	
<i>Magnolia soulangiana</i> 'Alexandrina'	Saucer Magnolia											
<i>Platanus acerifolia</i> 'Yarwood' or 'Columbia'	London Plane		x								x	
<i>Quercus agrifolia</i>	Coast Live Oak	x		x								x
<i>Sequoia sempervirens</i> (Parks only)	Coast Redwood	x										
<i>Tristania laurina</i>	Brisbane Box											
<i>Tristania laurina</i> 'Elegant'	Elegant Brisbane Box					x						
<i>Ulmus Japonica</i> + <i>U. Wilsoniana</i>	Accolade Elm		x								x	
<b>Shrubs</b>												
<i>Arbutus unedo</i> 'Compacta'	Compact Strawberry Tree							x				x
<i>Arctostaphylos densiflora</i> 'Howard McMinn'	Vine Hill Manzanita	x						x				
<i>Arctostaphylos densiflora</i> 'Lutsko's Pink'	Manzanita	x						x				
<i>Arctostaphylos hookeri</i> 'Wayside'	Monterey Manzanita	x								x		
<i>Ceanothus</i> 'Dark Star'	Dark Star Wild Lilac	x						x				
<i>Ceanothus</i> 'Julia Phelps'	Julia Phelps Lilac	x						x				
<i>Cercis occidentalis</i>	Western Redbud	x					x				x	
<i>Heteromeles arbutifolia</i>	Toyon											
<i>Mahonia</i> 'Golden Abundance'	Oregon Grape											
<i>Phormium</i> spp.	Flax	x						x				
<i>Rhamnus</i> 'Mound San Bruno'	Mound San Bruno Coffeeberry										x	
<i>Rhamnus californica</i> 'Eve Case'	Eve Case Coffeeberry	x						x				
<i>Rhamnus californica</i> 'Seaview'	Seaview Coffeeberry	x						x				
<i>Rhamnus californica</i> 'Leatherneck'	Leatherneck Coffeeberry	x									x	
<i>Rosmarinus</i> spp.	Rosemary	x						x				
<i>Salvia leucantha</i>	Mexican Bush Sage	x										



# Neighborhood Streetscape

## Proposed Plant List

Botanical Name	Common Name	Native
<b>Ground Covers, Perennials and Grasses</b>		
<i>Anigozanthos cultivars (dwarf)</i>	Kangaroo Paws	
<i>Arctostaphylos 'Emerald Carpet'</i>	Emerald Carpet Manzanita	x
<i>Arctostaphylos edmundsii 'Carmel Sur'</i>	Carmel Sur Manzanita	x
<i>Arctostaphylos uva-ursi 'Point Reyes'</i>	Point Reyes Manzanita	x
<i>Berberis 'Crimson Pygmy'</i>	Berberis	
<i>Calamagrostis foliosa</i>	Pacific Reed Grass	x
<i>Ceanothus gloriosus exaltatus 'Valley Violet'</i>	Wild Lilac	x
<i>Cotoneaster 'Lowfast' and 'Coral Beauty'</i>	Cotoneaster	
<i>Echium fastuosum</i>	Pride of Madeira	
<i>Eriogonum fasciculatum</i>	California Buckwheat	x
<i>Erisimum linifolium 'Bowles Mauve'</i>	Wallflower	
<i>Festuca mairei</i>	Atlas Fescue	
<i>Festuca 'Molate'</i>	Molate Fescue	x
<i>Festuca 'Siskiyou Blue'</i>	Fescue	x
<i>Lavandula angustifolia</i>	English Lavender	
<i>Lavandula ang. Hidcote Improved</i>	Hidcote Lavender	
<i>Myoporum parvifolium 'Putah Creek'</i>	Creeping Myoporum	
<i>Pennisetum spathiolatum</i>	Slender Veldt Grass	
<i>Ribes viburnifolium</i>	Catalina Fragrance	x
<i>Rosmarinus 'Huntington Carpet'</i>	Huntington Carpet Rosemary	
<i>Teucrium chamaedrys dwarf</i>	Dwarf Germander	

## 4.3 OPEN SPACE DESIGN

The open space network consists of a range of open spaces including existing undisturbed open space, the restored Rifle Range Creek Corridor, revegetated hillsides and publicly accessible neighborhood parks.

- The existing grassland on the upper hillside and areas of existing preserved oak woodland are protected natural resources.
- The lower hillside will be extensively planted as a restored oak woodland natural setting, consisting of several native oak species, Toyon and California Buckeye.
- The restored Rifle Range Creek will be revegetated with an appropriate and diverse native plant community to recreate a natural setting that benefits wildlife, and includes a multi-use trail serving the community. Refer to the Oak Knoll Mixed Use Community Development Project Regulatory Permit Application Package.
- Tree mitigation occurs site-wide in a variety of locations. Refer to the Tree Removal Permit Package for recommended mitigation locations and species.







Figure 4.2- Open Space and Parks

## 4.4 PARKS AND PLAZA DESIGN INTENT

There are three kinds of public parks offering active and passive recreational opportunities as described in the PDP. Larger more active community parks (see Figure 4.2) include the park areas at the Club Knoll community center and the neighborhood park near the project's northern boundary. Smaller more passive neighborhood parks are planned near the Community Center and within the townhome in-tract parcels. The plaza at the retail Village Center will serve as a social gathering and event space for the community, with decorative hardscape, benches, informal seating and canopy shade trees. Landscape guidelines for parks and plazas include:

- The parks should emphasize use of native trees, shrubs, and groundcovers in both organic and formal settings. Refer to the Neighborhood Streetscape Plant List for Proposed Plants.
- Parks should incorporate community-wide furnishings and signage consistent with other design elements in the community.
- Parks should provide shaded seating areas, picnic tables, and trash receptacles.
- Hardscape areas should avoid asphalt and large expanses of concrete. Natural stone, pavers, high quality stamped concrete, and decomposed granite should be utilized in the appropriate settings.
- A tot lot with play structures and picnic benches and lawn areas will be located at two locations and should include play equipment that is durable, safe, appropriately scaled, shaded and maintainable.
- Recreation areas such as playfields and multi-use courts should employ high quality turf and/or hardscape surfaces. Site drainage shall be extensively utilized on playfields and other higher impact natural areas.
- All fencing should be natural in character and follow these design guidelines, see Appendix B.
- Parks should provide connections and wayfinding to the project-wide trail and bikeway system.
- Where public art is included in park settings, designers should coordinate with artists prior to park design to ensure art elements are well integrated, accessible and compliment other elements of the park design.



COMMUNITY PARK



RETAIL VILLAGE



NEIGHBORHOOD PARKS



## 4.5 COMMUNITY TRAILS AND RECREATION

An extensive network of trails extend throughout the community, affording a range of experiences, challenges levels and activity options, including running, hiking, walking, dog walking and accessibility.

- Emphasis is on use of natural materials and simple treatments that are intended to integrate fully with the natural setting.
- Use of reclaimed timber for benches, signage, and trail markers with opportunities to incorporate hand-crafted artisan designs.

Trails for Oak Knoll are classified as follows:

- Hiking Trails
- Multi-Use Path (Walking/ Running/Biking)
- Neighborhood Path
- Bike Route

Location of the trails systems should meet the following design objectives:

- Safety
- Connectivity to on-site and off-site destinations
- Diversity in experiences and user types
- Conforms to site attributes, opportunities and constraints



Wayfinding Sign



Trailhead Signage



Bay View telescope at Vista Point



Soil cement surface



Unique wood benches



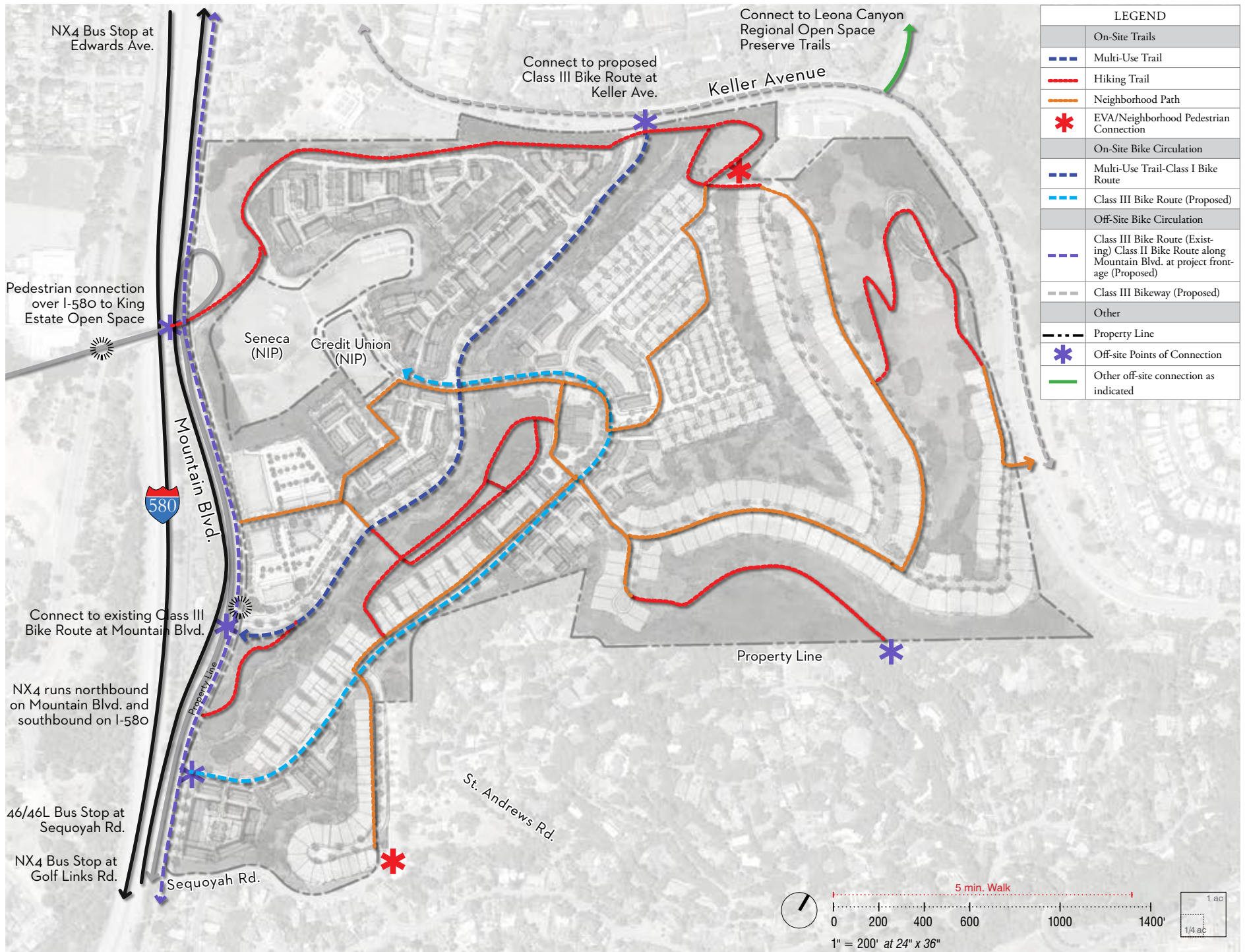


Figure 4.3- Bike and Pedestrian Trails and Connections



## 4.6 SIGNAGE AND MONUMENTATION

Stone piers with historic details are envisioned for use to celebrate the main entry points and secondary neighborhood entry points. A hierarchy of scale will differentiate the entry and neighborhood monuments, while incorporating unique details and establishing a common approach. Materials and craftsmanship will employ a unified approach to unify the community design, including stone details at the vehicular bridge crossing the creek and community center entrance.

The vision for the main entry monument on Mountain Boulevard and Keller Avenue is for a collection of native trees (oaks, manzanita, etc) in a naturalized composition with stone piers marking the entry. Community signage may be integrated into the piers similar to the tradition of the neighborhood street names at neighborhood entries.



Community Entry Monument Concept



Stone Neighborhood Entry Piers



Community Entry Conceptual Plan - Mountain Boulevard and Creekside Parkway (NTS)





Figure 4.4 - Signage and Monumentation Plan

## 4.7 WALLS

### SITE RETAINING WALLS

Retaining walls will be needed due to the sloping topography of the site. See the Appendices for Approved Retaining Wall. Low shrubs are required where space between the face of retaining walls and right of way or swale exceeds 2'. Retaining walls greater than 4' tall require top of wall planting, Retaining walls greater than 6' tall shall have vines planted on the face of walls in drifts of single species; each drift of vine planting should not exceed 50' before blending into another species.

Retaining walls taller than 30" shall incorporate a 42" guardrail. See figure and detail in Appendix B. Fences shall be set behind the top of wall (or top of wall swale where occurs) 4' to allow for shrub massing.

The Approved Plants include the following:

Vines for Retaining Walls (plant on face in drifts of single species, 8' spacing to achieve 50% cover when mature):

- Parthenocissus tricuspidata 'Veitchii'/Dwarf Boston Ivy
- Solanum rantonetti/Potato vine

Shrub massing for base of walls, where planting area (clear of drainage swales) measures 2'-4':

- 40% Ceanothus gloriosus 'Valley Violet'/Valley Violet Wild Lilac
- 40% Westringia fruticosa 'Grey Box' or 'Jervis Gem'
- 20% Erysimum Bowle's Mauve'

Shrub massing for base of walls (clear of drainage swales) where planting area measure 4' to 10':

- 40% low shrub massings:
- Ceanothus maritimus cultivars planted in drifts alternating with Manzanita:
- "Frosty Dawn", 'Valley Violet' and/or 'Point Sierra'
- 30% Arctostaphylos hookeri 'Wayside'
- 30% Feijoa sellowiana/Pineapple Guava (taller shrub massings where walls exceed 7 feet)

The Approved Site Retaining Wall is:

- Pavestone 'Anchor Diamond Pro' Retaining Wall
- Face Style: Straight
- Color: Sandstone Blend



Vines planted on face of retaining wall



## 4.8 RESIDENTIAL LANDSCAPE DESIGN

The following principles embedded in the landscape design philosophy closely mirror the architectural design principles:

- Oak Knoll landscapes and gardens are versatile, imaginative and offer a range of expressions.
- Landscapes encourage a relaxed, informal and practical approach while accommodating contemporary lifestyles.
- Landscapes are designed to respond to unique characteristics, such as lot configuration, topography, existing vegetation, and the design and location of the house and ancillary structures.

Residential landscaping will be the responsibility of the individual Homeowners and Builders and shall be thoughtfully designed according to these guidelines. A palette of plant and landscape materials is established in these guidelines to ensure visual unity within visible lot areas while allowing room for individual creative design solutions.

## 4.9 SINGLE FAMILY RESIDENTIAL

Landscape guidelines and requirements within this Section address unique landscape conditions that occur for the single family residential lots found at Oak Knoll.

### OBJECTIVES

- Integrate the built environment with a dominant landscape.
- Blend landscapes between lots and neighborhood streets as a unified community landscape setting.
- Establish a healthy, sustainable and natural landscape environment.
- Prioritize front yard landscapes to reinforce neighborhood streets as livable, walkable places. The combination of front porches and front yard gardens within the private frontages activate the streetscape, and shall contribute to a consistent, high quality neighborhood landscape.
- Low groundcovers have low water requirements and are composed in drifts, using selections from the Approved Plant (see Appendices).

Three general landscape zones have been defined for each home site. Objectives and guidelines regarding landscaping, planting, paving, walls and fencing within each zone are described in this chapter. The three landscape zones are described as follows:

### FRONT YARD ZONE

The front yard is defined by the area between the front property line and the front face of the building, extending to the side property lines. The objectives within this zone are to reinforce and enrich the neighborhood street scene and to provide a transition from the street to the private landscape. All plantings within this zone are to occur prior to home occupation.

### SIDE YARD ZONE

The side yard areas maintain privacy for indoor and outdoor living spaces while also providing access around the home. A range of creative solutions such as using architectural site walls and fences combined with neat vertical plantings and groundcovers are encouraged. Sensitivity to neighboring lots is required in the design of side yards in order to avoid blocking light or creating maintenance and nuisance issues, while protecting for privacy.

### REAR YARD ZONE

This area is defined as the area between the rear property line and the rear face of the building, extending to the side property lines. The objective within this zone is to provide privacy, pleasant outdoor living spaces and shade for building western exposures. Sensitivity to neighboring lots is required in the design of the rear yard areas in order to avoid blocking light or creating maintenance and nuisance issues.

Refer to Figure 4.5: Typical Home Site Planting Diagram for planting requirements for each zone.





Figure 4.5: Typical Home Site Planting Diagram

## PLANTING PALETTE

### Small Trees

(4) 15 gallon total

- Arbutus unedo/Strawberry Tree (compact form)
- Magnolia x s. 'Lilliputian'/ Saucer Magnolia (dwarf form)

### Medium Trees

(1) 15 gallon total

- Quercus agrifolia/Coast Live Oak
- Arbutus 'Marina'/ 'Marina' Madrone

### Planting Type 1

(perennials, low shrubs and groundcover)

Lavandula angustifolia 'Hidcote'/English Lavender (dwarf cultivars) qty:10

- Arctostaphylos 'Point Reyes'/Manzanita
- Ceanothus gloriosus 'Frosty Dawn'/Wild Lilac
- Rhamnus 'Seaview'/Seaview Coffeeberry
- Festuca rubra 'Molate'/Creeping Red Fescue

### Planting Type 2

(rain garden grasses/perennials, accent taller shrubs and fence vine)

- Festuca rubra 'Molate'/Creeping Red Fescue
- Ribes sanguineum/Pink winter Currant
- Pacific Coast Iris cultivars
- Hardenbergia comptoniana/Lilac Vine

### Planting Type 3

(lawn, meadow grasses, groundcovers and low shrubs)

- Dwarf Tall Fescue lawn sod or seed
- Carex pansa/California Meadow Sedge pots or plugs
- Calamagrostis foliosa/Feather Reed Grass
- Rhamnus 'Leatherneck'/Coffeeberry

## FRONT YARDS ON SLOPED LOTS

Lots located on streets with slopes greater than 10% shall be completely vegetated and may use a combination of low walls and plantings to achieve changes in grade. Plants that aid in erosion control are recommended.

### GUIDELINES

- Front yard slopes may not exceed 2:1.
- Retaining walls, if used, should be terraced where possible and not exceed a maximum height as set forth in the Zoning Ordinance.
- Retaining walls shall be integrated with shrub planting to soften and screen walls.

See Section 4.11 Retaining Walls on Lots for Approved Materials.



Sloped Lot Planting



Combination of Planting and Retaining Walls at Sloped Lots



## 4.10 SIDE AND REAR YARD FENCING

Side yard fencing is required for functional and safety reasons. All fencing shall use quality materials and follow the Approved Standard Fence Details in Appendix B.

### GUIDELINES

- All fencing may either slope with grades or adjust as a vertical offset between panels. Offsets shall not exceed 12 inches.
- All fencing between adjoining lots shall have a height of 6 feet. Corner lots and end lots are encouraged to reduce fence heights at side yards to allow views with a minimum height of 4 feet.
- All fencing shall be softened with flowering vines and shrubs to soften their visual appearance where visible from public areas.
- A few upper hillside home sites with sloped rear yards in excess of 20% shall utilize the Approved Hillside Fence in the rear yard.
- Lots with pools and spas require fencing and gates that meet all applicable codes.
- Typical side and rear yard fencing is a solid cedar or redwood fence with a stained finish.
- For upland lots with rear yards with onsite and offsite visibility, rear yard fencing, if used, shall use the Approved Hillside Fence to ensure visual consistency.
- For lots with side or rear yards that front on the creek corridor, a neighborhood park or open space, please refer to appropriate wall design examples in Appendix B.



Typical Side Yard Board-on Batten Style Privacy Fence



Typical Hillside Fence where slopes exceed 20% (stepped or sloping rear yard conditions)

## 4.11 RETAINING WALLS ON LOTS

Retaining walls may be needed due to sloping topography on individual lots. Retaining walls shall be minimized and designed to fit the topography. Retaining walls in the side or rear yard shall use the Approved Retaining Wall design shown in the Appendices.

### GUIDELINES

- Use of stucco, brick, painted brick or natural stone veneer may be used for site walls in front and side yards that are visible from public areas. Materials shall complement the building architecture.
- Wall heights shall be appropriate to context and shall not exceed 6' in height per Code.
- Tiered walls shall be integrated landscape design.
- Tops of walls may either slope or step with the topography as required. Walls may slope at 1:8 maximum or use vertical offsets of 12" maximum.
- Use of vines, trailing evergreen groundcovers and shrub massings are encouraged to soften walls.
- Retaining walls in side and rear yards- Walls not closely associated with the architecture and not visible from public areas may use the Approved Standard Wall System described in the Appendices.
- Retaining walls in rear yards shall be located a minimum of 4' from the property line to allow room for fencing.
- Retaining walls and steps at front walkways are allowed to resolve site grading.

- The following retaining wall materials are allowed:
  - Brick
  - Painted brick
  - Natural stone veneer
  - Approved concrete block wall system in rear and side yards (refer to Appendices)
  - Gabions
  - Pressure-treated wood
- The following retaining wall materials are **not** allowed:
  - Railroad ties
  - Metal cribs
  - Concrete pylons





# APPENDICES





# APPENDIX A

The following colors and materials have been assembled to give developers guidance. They are intended to communicate the vision of the Oak Knoll character. Nonetheless, creativity is encouraged, and alternatives to these materials, which adhere to the spirit of these guidelines will be considered by the DRC. A more detailed and design-specific palette of colors and materials shall accompany all FDP applications and be reviewed by the DRC.

APPENDIX A - APPROVED MATERIALS & COLOR PALETTE

**ARCHITECTURAL BRICK (FACE BRICK) AND STONE  
VENEER PALETTE**



EXAMPLES OF ACCEPTABLE BRICK FINISHES



EXAMPLES OF ACCEPTABLE STONE VENEER FINISHES





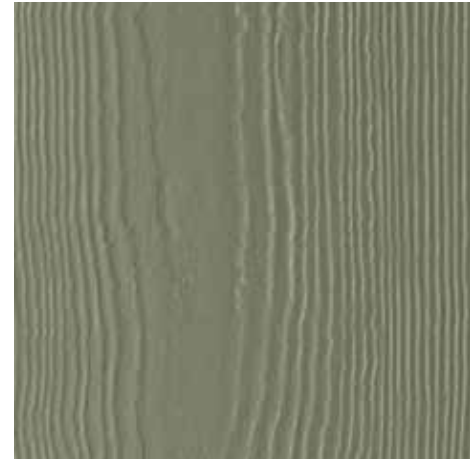
ARCHITECTURAL SIDING AND ROOF PALETTE



STAINED OR PAINTED SHINGLE SIDING



STAINED OR PAINTED SHINGLE SIDING



PAINTED WOOD OR WOOD COMPOSITE SIDING



CLAY TILE ROOF



ASPHALT TILE ROOF - GRAY



ASPHALT TILE ROOF - BLACK

**ARCHITECTURAL PAINT COLORS**







# APPENDIX B

## APPROVED STANDARD RETAINING WALL SYSTEM ON LOTS

- 'Anchor Highland Stone Retaining Wall 6" Combo' (Product 876) by Pavestone, Inc. : 3 piece system, sizes 18"x12"x6", 12"x12"x6", and 6"x12"x6". Color to be determined. Cap #819. (<http://www.pavestone.com/anchor-highland-stone-retaining-wall-6-combo/>)



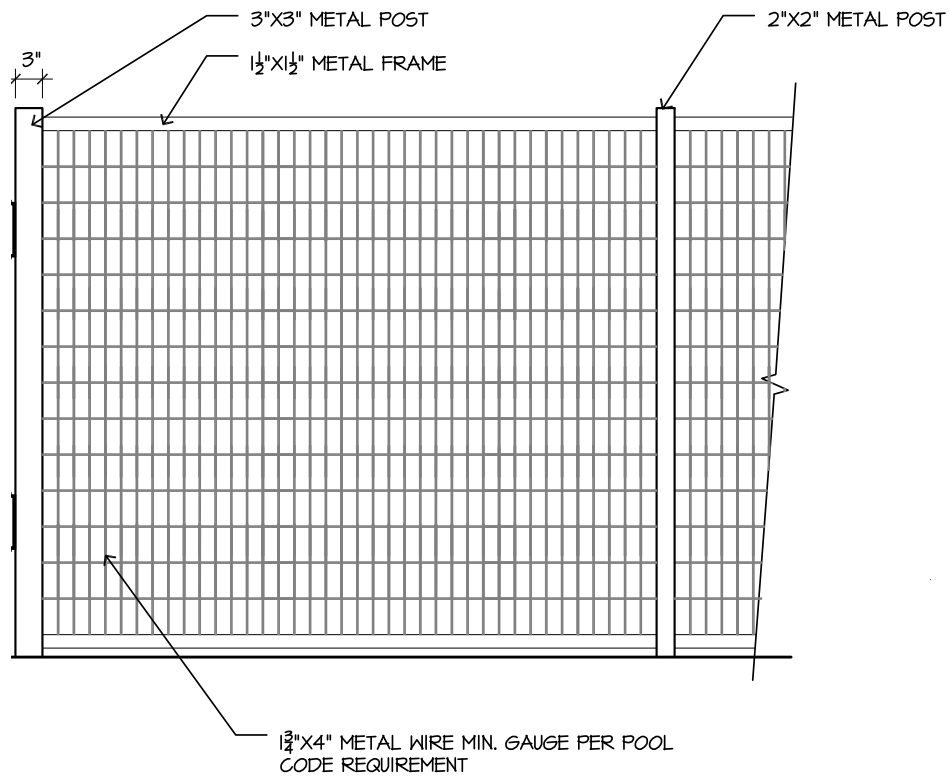


## STANDARD APPROVED SIDE YARD AND REAR YARD FENCES

- Minimum Standard Material: #1 grade Western Red Cedar or Redwood
- Stain finish: Cabot 'Red Cedar'
- Note: Both sides are finished equally
- 4x6 posts Western Red Cedar posts 6' apart
- Boards: 1x8, Battens: 1x3 (both sides), Cap: 1x6 with 1x4 facer
- Height: 6'-0"
- add low plants at base like a low grass or liriopce



### STANDARD APPROVED GUARDRAIL





# APPENDIX C

The following Approved Plant List and Prohibited Plant List are intended to communicate the vision of the Oak Knoll landscape character. Nonetheless, creativity is encouraged, and alternatives to these materials, which adhere to the spirit of these guidelines will be considered.

APPENDIX C - APPROVED PLANT LIST

Botanical Name	Common Name	Street Trees	Large	Medium	Small	Native	Full Sun	Part Sun/Shade	Full Shade	Rain Garden
<b>TREES</b>										
<i>Acer buergeranum</i>	Trident Maple			x	x		x		x	
<i>Arbutus 'Marina'</i>	Madrone 'Marina'			x			x		x	
<i>Agonis flexuosa</i>	Peppermint Tree				x		x			
<i>Acer japonicum</i>	Japanese Maple			x			x		x	
<i>Lagerstroemia indica</i>	Crape Myrtle			x	x		x			
<i>Lepotospermum scoparium</i>	New Zealand Tea Tree				x		x	x		
<i>Magnolia grandifolia 'Little Gem'</i>	Dwarf Southern Magnolia				x		x	x		
<i>Magnolia soulangiana 'Lilliputian'</i>	Dwarf Saucer Magnolia				x		x	x		
<i>Ceanothus 'Ray Hartman'</i>	Ray Hartman Wild Lilac				x	x	x			
<i>Cercis occidentalis</i>	Western Redbud				x	x	x	x		
<i>Heteromeles arbutifolia</i>	Toyon				x	x	x	x		
<i>Arbutus unedo</i>	Strawberry Tree				x		x	x		
<i>Malus floribunda</i>	Crabapple				x		x	x		
<i>Citrus - dwarf cultivars</i>					x		x			
<i>Fruit Trees - dwarf cultivars</i>					x		x			

<b>SHRUBS</b>										
<i>Carpenteria californica</i>	Bush Anemone			x		x	x	x		
<i>Ceanothus spp.</i>	Wild Lilac		x			x	x			
<i>Camelia sasanqua</i>	Apple Blossom		x				x	x	x	
<i>Chondropetalum tectorum</i>	Cape Rush					x				x
<i>Ribes sanguineum</i>	Pink Flowering Currant			x		x	x	x		
<i>Loropetalum chinense</i>	Fringe Flower					x				
<i>Hebe species</i>	Hebe			x	x		x	x		
<i>Mahonia aquifolium</i>	Oregon Grape		x			x	x	x		
<i>Phormium spp. (dwarf cultivars)</i>	Flax			x	x		x	x		
<i>Rhamnus californica cultivars</i>	Coffeeberry		x			x	x	x		
<i>Podocarpus gracilior</i>	Fern Pine			x		x	x			
<i>Viburnum suspensum</i>	Sandankwa Viburnum		x				x	x		



Botanical Name	Common Name	Street Trees	Large	Medium	Small	Native	Full Sun	Part Sun/Shade	Full Shade	Rain Garden	Notes
<b>GROUNDCOVERS, PERENNIALS AND GRASSES</b>											
<i>Anigozanthos flavidus</i>	Kangaroo Paws			x	x		x				
<i>Arctostaphylos 'Emerald Carpet'</i>	'Emerald Carpet' Manzanita				x	x	x				
<i>Arctostaphylos edmundsii 'Carmel Sur'</i>	'Carmel Sur' Manzanita				x	x	x				
<i>Arctostaphylos uva ursi 'Pt. Reyes'</i>	'Pt. Reyes' Manzanita				x	x	x	x			
<i>Berberis 'Crimson Pygmy'</i>	Burberry				x		x	x			
<i>Calamagrostis foliosa</i>	Pacific Reed Grass				x	x	x				
<i>Ceanothus griseus horizontalis</i>	Wild Lilac				x	x	x	x			
<i>Ceanothus gloriosus</i>	Point Reyes Ceanothus				x	x	x	x			
<i>Ceanothus 'Hearts Desire'</i>	Heart's Desire Ceanothus				x	x	x	x			
<i>Ceanothus 'Centennial'</i>	Centennial Lilac				x	x	x	x			
<i>Cotoneaster dammeri Lowfast' and 'Coral Beauty'</i>	Cotoneaster				x		x	x			
<i>Dymondia margaratae</i>	Dymondia				x		x	x			
<i>Erigeron species</i>	Seaside Daisy						x				
<i>Festuca rubra 'Molate'</i>	'Molate' Red Fescue				x	x	x	x		x	
<i>Festuca 'Siskiyou Blue'</i>	Fescue				x		x	x			
<i>Lavandula angustifolia 'Munstead'</i>	English Lavender				x		x				
<i>Lavandula ang. 'Hidcote Improved'</i>	'Hidcote' Lavender				x		x				
<i>Myoporum parvifolium</i>	Creeping Myoporum				x		x	x			
<i>Teucrium chamaedrys 'Nanum'</i>	Dwarf Germander				x		x				
<i>Iris PCH 'Canyon Snow'</i>	Pacific Coast Iris				x	x	x	x			
<i>Euphorbia characias</i>	Euphorbia			x			x				
<i>Lessingia filaginifolia 'Silver Carpet'</i>	Silver Carpet Beach Aster				x	x	x				
<i>Carex testacea</i>	New Zealand Sedge			x			x				
<i>Libertia grandiflora</i>	New Zealand Iris				x		x	x			
<i>Festuca glauca</i>	Blue Fescue				x	x	x				
<i>Agrostis pallens</i>	Bent Grass				x	x	x				Lawn substitute
<i>Carex pansa</i>	California Meadow Sedge				x	x	x				Lawn substitute
<i>Sesleria autumnalis</i>	Autumn Moor Grass				x	x	x				
<i>Carex divulsa</i>	Berkeley Sedge			x		x	x			x	
<i>Carex praegracilis</i>	Western Meadow Grass				x	x	x			x	
<i>Achillea spp.</i>	Yarrow			x			x				

APPENDIX C - APPROVED PLANT LIST

Botanical Name	Common Name	Street Trees								Notes
		Large	Medium	Small	Native	Full Sun	Part Sun/Shade	Full Shade	Rain Garden	
<b>GROUNDCOVERS, PERENNIALS AND GRASSES</b>										
<i>Aeonium</i> spp.	Canary Island Rose			x		x	x			
<i>Baccharis pilularis</i> 'Pigeon Pt.'	Dwarf Coyote Bush			x	x	x	x			
<i>Dietes</i> species	Fortnight Lily		x			x	x			
<i>Penstemon</i> spp.	Penstemon		x	x	x	x	x			
<i>Salvia greggii</i>	Salvia		x	x		x	x			
<i>Stachys byzantina</i>	Lamb's Ears			x	x	x	x			
<i>Zauschneria</i> spp.	California Fuchsia			x	x	x	x			
<i>Helictotrichon sempervirens</i>	Blue Oat Grass			x		x				
<i>Geranium</i> spp.	Geranium			x		x	x			
<i>Lantana sellowiana</i>	Lantana			x		x				
<i>Juncus patens</i>	California Gray Rush		x	x	x	x	x		x	
<i>Liriope muscari</i>	Lily Turf			x		x	x			
<i>Libertia peregrinans</i>	New Zealand Iris			x		x				

Botanical Name	Common Name	Street Trees								Notes
		Large	Medium	Small	Native	Full Sun	Part Sun/Shade	Full Shade	Rain Garden	
<b>VINES</b>										
<i>Clytostoma callistegioides</i>	Lavendar Trumpet vine					x	x			
<i>Ficus repens</i>	Creeping Fig					x	x			
<i>Hardenbergia violacea</i>	Lilac Vine					x	x			
<i>Mandevilla laxa</i>	Chilean Bower Vine					x	x			
<i>Rosa</i> spp.	Roses					x	x			
<i>Solanum jasminoides</i>	Potato Vine					x	x			
<i>Vitis californica</i>	California Grape				x	x	x			
<i>Calystegia macrostegia</i>	Coastal Morning Glory				x	x				
<i>Jasminum polyanthum</i>	Pink Jasmine					x	x			
<i>Gelsemium sempervirens</i>	Carolina Jessamine					x	x			
<i>Pandorea jasminoides</i>	Bower Vine					x	x			



## PROHIBITED PLANT LIST

The following list of invasive species are prohibited. As information is constantly changing, this list may be updated from time to time. This list is derived from Cal-IPC.

Latin binomial / Common names

Acacia dealbata / Silver wattle  
Acacia melanoxylon / Blackwood acacia  
Acanthus mollis / Bears breech  
Agapanthus spp. / Lily of the Nile  
Ailanthus altissima / Ailanthus, Tree-of-heaven  
Albizia julibrissen / Silk floss  
Alhagi maurorum / Camelthorn  
Aptenia cordifolia / Red apple, Baby sun rose  
Arcotroche calendula / Capeweed, Cape dandelion  
Arundo donax / Giant reed, Giant cane  
Atriplex semibaccata / Australian saltbush  
Avena barbata / Slender oat  
Avena fatua / Wild oats  
Bassia hyssopifolia / Five-hook bassia, Thorn orache  
Bellardia trixago / Bellardia, Mediterranean linseed  
Brassica spp. / Mustards  
Bromus madritensis ssp. rubens / Foxtail chess  
Bromus tectorum / Cheatgrass, Downy brome  
Cardaria chalepensis / Lens-podded hoary cress  
Cardaria draba / Heart-podded hoary cress, White-top  
Cardaria pubescens / Hairy whitetop  
Carduus spp. / Thistles  
Carpobrotus edulis / Highway iceplant  
Centaurea spp. / Hardheads, Knapweed

Centranthus rubra / Valarian  
Cirsium spp. / Thistles  
Cistus ladanifer / Crimson spot rock rose  
Conicosia pugioniformis / Narrow-leafed iceplant  
Conium maculatum / Poison hemlock  
Cortaderia spp. / Pampasgrass  
Cotoneaster spp. / Cotoneaster  
Crataegus monogyna / Singleseed hawthorn  
Cynara cardunculus / Artichoke thistle, Cardoon  
Cytisus spp. / Broom  
Delairea odorata / Cape ivy, German ivy  
Digitalis purpurea / Foxglove  
Dimorphotheca sinuata / African daisy  
Drosantemum spp. / Ice plant  
Echium candicans, E. fastuosum / Pride-of-Madeira  
Egeria densa / Brazilian egeria  
Ehrharta spp. / Veldtgrass  
Eichhornia crassipes / Water hyacinth  
Elaeagnus angustifolia / Russian olive, Oleaster  
Elaeagnus pungenis / Silverberry  
Erechtites spp. / Fireweed  
Eucalyptus camaldulensis / Red gum  
Eucalyptus globulus / Bluegum  
Euphorbia spp. / Spurge  
Festuca arundinacea / Tall fescue  
Ficus carica / Edible fig, Common fig  
Foeniculum vulgare / Fennel, Sweet anise  
Gazania linearis / Gazania  
Genista spp. / Broom  
Halogeton glomeratus / Halogeton  
Hedera helix / English ivy

APPENDIX C - APPROVED PLANT LIST

Hedera canariensis / Algerian ivy  
Helichrysum petiolare / Licorice plant  
Holcus lanatus / Common velvet grass  
Hydrilla verticillata / Hydrilla, Water thyme  
Hypericum spp. / St. John's Wort  
Ilex aquifolium / English holly  
Iris pseudacorus / Yellow flag iris  
Juniperus spp. / Juniper  
Lampranthus spp. / Ice plant  
Lepidium latifolium / Perennial pepperweed  
Leucanthemum vulgare / Ox-eye daisy  
Ligustrum lucidum / Glossy privet  
Ludwigia hexapetala / Creeping water primrose  
Ludwigia peploides / California water primrose  
Lythrum hyssopifolium / Hyssop loosestrife  
Lythrum salicaria / Purple loosestrife  
Malephora spp. / Ice plant  
Marrubium vulgare / Horehound  
Maytenus boaria / Mayten  
Mentha pulegium / Pennyroyal  
Mesembryanthemum spp. / Iceplant  
Myoporum laetum / Ngaio tree  
Myriophyllum aquaticum / Brazilian watermilfoil  
Myriophyllum spicatum / Spike watermilfoil  
Nandina spp. / Bamboo  
Olea spp. / Olive  
    \*Except Fruitless cultivar 'Swan Hill'  
Pennisetum spp. / Fountain grass  
Pistacia chinensis / Chinese pistache  
    \*Except Fruitless cultivar 'Keith Davey'  
Phalaris aquatica / Harding grass  
Pittosporum spp. / Mock orange  
Platanus acerifolia / Bloodgood plane tree  
Pyracantha spp. / Firethorn  
Retama monosperma / Bridal veil broom  
Ricinus communis / Castor bean  
Robinia pseudoacacia / Black locust  
Rubus armeniacus / Himalayan blackberry  
Saponaria officinalis / Bouncing bet  
Schinus terebinthifolius / Brazilian pepper tree  
Schismus spp. / Mediterranean grass, Arabian grass  
Senecio jacobaea / Tansy ragwort, Ivy  
Sesbania punicea / Scarlet wisteria  
Silybum spp. / Thistles  
Spartina spp. / Cord grass, Marsh grass  
Spartina patens / Salt marsh hay  
Stipa manicata / Tropical needlegrass  
Taeniatherum caput-medusae / Medusahead  
Tamarix parviflora / Tamarisk, Saltcedar  
Ulex europaeus / Common gorse  
Verbena bonariensis / Tall vervain  
Verbascum spp. / Mullein  
Vinca spp. / Periwinkle  
Zantedeschia aethiopica / Calla lily





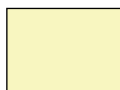
# APPENDIX D

## ADMIRALS RIDGE HILLSIDE GUIDELINES

The Admirals Ridge area includes 18 single family lots on the eastern edge of the project flanking Keller Ave. Due to the increased visibility of the 13 lots west of the road, special height standards will be applied (see section diagrams on facing page).



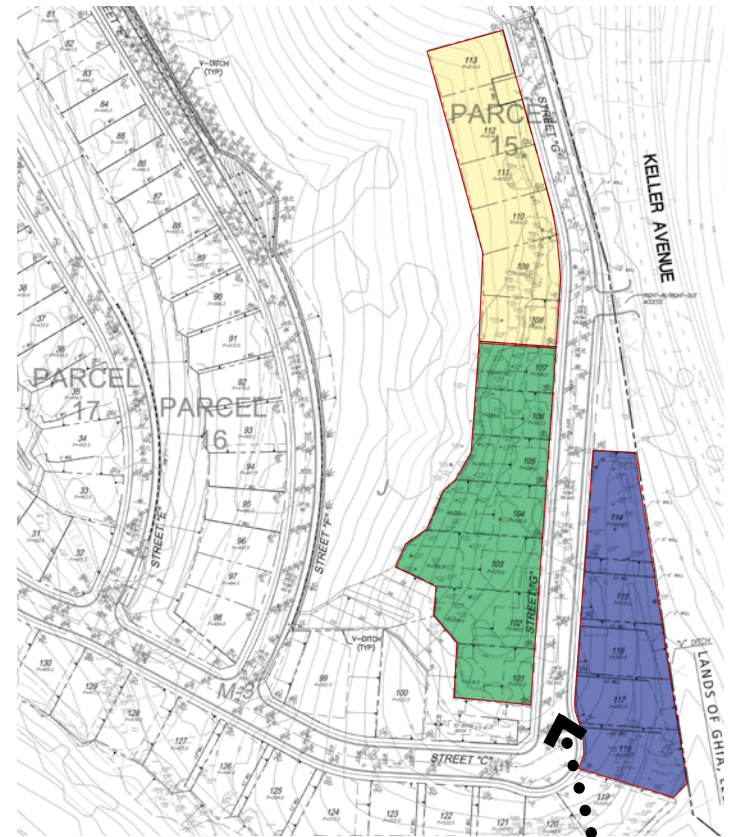
**Terraced Lots.** These lots have terraced building pads to better integrate into the hillside topography, height limits are applied as shown in figure 4.6.



**Sloped Lots.** These lots are located in the most visible area to the north and the majority of the area within the buildable envelope is sloped, height limits are applied as shown in figure 4.6.



**Padded Lots.** These lots are padded to allow for a full flat building site and are not located in the highly visible areas west of the road.



ADMIRALS RIDGE LOT TYPES



ADMIRALS RIDGE FOCUS AREA



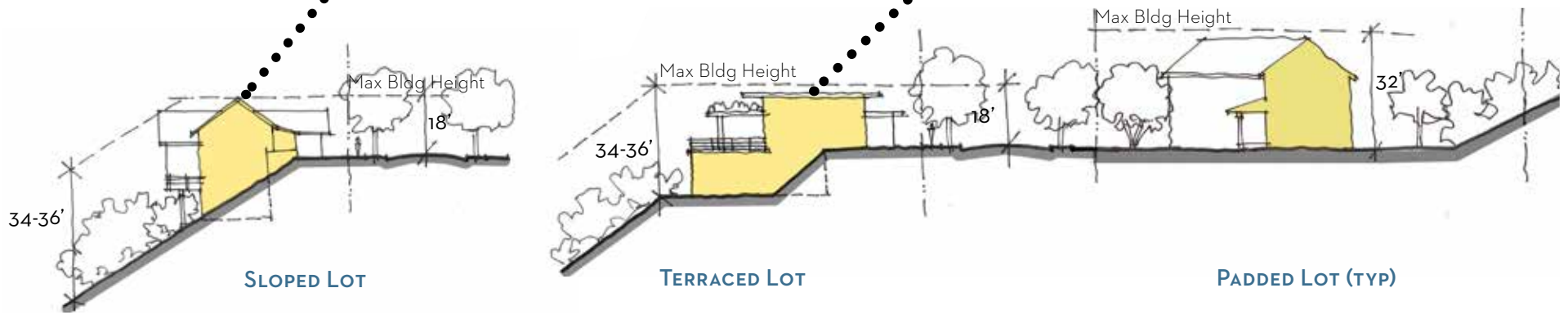


Figure 4.6: Section and Building Height diagrams at hillside lots.

**PART 1: STANDARD CONDITIONS OF APPROVAL –  
GENERAL ADMINISTRATIVE CONDITIONS**

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**1. Approved Use**

The project shall be constructed and operated in accordance with the authorized use as described in the approved application materials dated June 21, 2017 and as amended through October 18, 2017 and included in the Planning Commission staff report, and as may be amended in the final City Council decision, as also amended by the following conditions of approval and mitigation measures, if applicable (“Conditions of Approval” or “Conditions”).

This action by the City Council (“this Approval”) includes the approvals set forth below. This Approval includes:

- a. General Plan Land Use Diagram amendment for the area covered by the Oak Knoll PUD permit (October 18, 2017)
- b. Amended zoning text (June 2017) and zoning diagram (October 18, 2017) for the area covered by the Oak Knoll PUD permit
- c. Oak Knoll PUD permit, including Oak Knoll Preliminary Development Plan (September 2017) and Oak Knoll Design Guidelines (January 2017), Final Development Plans for the Master Developer Improvements (May 30, 2017), and Final Development Plans for Club Knoll (April 3, 2017).
- d. Transportation Demand Management (TDM) Plan, as included in the April 27, 2017 Final Supplemental EIR
- e. Greenhouse Gas Reduction Plan (GGRP), as included in the April 27, 2017 Final Supplemental EIR
- f. Creek Protection Permit and Creek Restoration Plan (February 2016)
- g. Vesting Tentative Tract Map No 8320 (September 2017)
- h. Conditional Use Permit for Shared Access Facilities (October 18, 2017)

**2. Effective Date, Expiration, Extensions and Extinguishment**

Pursuant to the City’s Subdivision Code, an approved tentative tract map expires two years after its approval, but may be extended for an additional year, for a maximum for a three-



year period. The California Subdivision Map Act, however, specifies that an approved tentative map expires two years after its approval and that upon application of the subdivider prior to the expiration of the approved tentative map, the life of the tentative map may be extended for an additional six years. Case law indicates that these provisions in the California Subdivision Map Act preempt the City's Subdivision Code. The applicant has requested that a Vesting Tentative Tract Map (VTTM) for the project be extended the additional six-years pursuant to the California Subdivision Map Act, and has requested permission to file phased final maps. Accordingly, the VTTM shall expire at least eight years after the date of this approval. Nothing herein shall be in derogation of any additional extensions to the VTTM arising by the operation of law under the California Subdivision Map Act or other provision of state law. All Approvals, including but not limited to the Preliminary Development Plan (PDP) and Final Development Plans (FDPs) for the Planned Unit Development Permit, the Creek Protection Permit and Creek Restoration Plan and the Conditional Use Permit for Shared Access Facilities, shall expire at the same time as the VTTM.

Upon written request and payment of appropriate fees submitted no later than the expiration date of this Approval, the Director of City Planning or designee may grant a one-year extension of expiration dates, with additional extensions subject to approval by the approving body. Expiration of any necessary building permit or other construction-related permit for this project may invalidate this Approval if said Approval has also expired. If litigation is filed challenging this Approval, or a development moratorium affecting the Project is imposed, or its implementation, then the time period stated above for obtaining necessary permits for construction or alteration and/or commencement of authorized activities is automatically extended for the duration of the litigation or development moratorium affecting the Project.

**3. Compliance with Other Requirements**

The project applicant shall comply with all other applicable federal, state, regional, and local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City's Bureau of Building, Fire Marshal, and Public Works Department. Compliance with other applicable requirements may require changes to the approved use and/or plans. These changes shall be processed in accordance with the procedures contained in Condition #4.

**4. Minor and Major Changes**

Minor changes to the approved project, plans, Conditions, facilities, or use may be approved administratively by the Director of City Planning.

Major changes to the approved project, plans, Conditions, facilities, or use shall be reviewed by the Director of City Planning to determine whether such changes require submittal and approval of a revision to the Approval by the original approving body or a new independent permit/approval. Major revisions shall be reviewed in accordance with

the procedures required for the original permit/approval. A new independent permit/approval shall be reviewed in accordance with the procedures required for the new permit/approval. Major Changes include but are not limited to changes to any of the following: density or intensity of uses in the project, changes to relocation and rehabilitation of Club Knoll, increases to building heights along roadways 26' wide or less, reduction in the amount of stormwater treatment capacity, diminution of the acreage proposed for public access, or changes that will result in any of the circumstances requiring further environmental review pursuant to CEQA Guidelines section 15162 or Public Resources Code section 21166. Refinements to engineering plans resulting in minor adjustments in lot sizes, and lot shapes are not considered to be major changes.

**5. Compliance with Conditions of Approval**

- a. The project applicant and property owner, including successors and subsequent merchant builders or homebuilders, (collectively referred to hereafter as the “project applicant” or “applicant”) shall be responsible for compliance with all the Conditions of Approval and any recommendations contained in any submitted and approved technical report at his/her sole cost and expense, subject to review and approval by the City of Oakland.
- b. The City of Oakland reserves the right at any time during construction to require certification by a licensed professional at the project applicant’s expense that the as-built project conforms to all applicable requirements, including but not limited to, approved maximum heights and minimum setbacks. Failure to construct the project in accordance with the Approval may result in remedial reconstruction, permit revocation, permit modification, stop work, permit suspension, or other corrective action.
- c. Violation of any term, Condition, mitigation measure or project description relating to the Approval is unlawful, prohibited, and a violation of the Oakland Municipal Code. The City of Oakland reserves the right to initiate civil and/or criminal enforcement and/or abatement proceedings, or after notice and public hearing, to revoke the Approval or alter these Conditions if it is found that there is violation of any of the Conditions or the provisions of the Planning Code or Municipal Code, or the project operates as or causes a public nuisance. This provision is not intended to, nor does it, limit in any manner whatsoever the ability of the City to take appropriate enforcement actions. The project applicant shall be responsible for paying fees in accordance with the City’s Master Fee Schedule for inspections conducted by the City or a City-designated third party to investigate alleged violations of the Approval or Conditions.



6. **Compliance with the Terms of this Approval**

*Ongoing*

For the duration of the project, the Director of Planning and Building or his/her designee shall have the authority to determine whether the Project Applicant and the project comply with the terms and conditions of this approval, including, without limitation, these Conditions of Approval, and shall have the authority to suspend further Project approvals, including without limitation, final subdivision maps, grading permits, building permits or certificates of occupancy for the duration of such noncompliance. =

The City shall take reasonable steps to promptly notify, in writing, the Project Applicant of any request (including a request by City staff or by the public) that the Director Planning and Building make a determination of noncompliance, and shall provide the Project Applicant a copy of all public documents related to such requests and a reasonable amount of time to respond and to cure any such alleged noncompliance. The City further shall take reasonable steps to promptly notify, in writing, the Project Applicant of any noncompliance determination by the Director of Building and Planning and, as applicable, shall provide the Project Applicant a copy of all documents used or relied upon in making such determination. On or before June 30 of each year, the Project Applicant shall submit to the Director of Planning and Building for review and approval the Compliance Matrix described in Condition 14 demonstrating that the legal entities implementation of its portion(s) or phase(s) of the Project comply with the terms and conditions of the Project Approvals. Such matrix may be used by the Director of Planning and Building to evaluate the Project Applicant's and the Project's compliance with the terms and conditions of the Project Approvals. The Project Applicant's obligation to submit such matrix shall terminate only upon the City's written determination that the Project, or the part or phase being undertaken by that legal entity, is completed.

Any failure by the City or Project Applicant to perform any action specified herein, or failure of any party timely to execute any agreement specified herein shall not be construed to limit any right or obligation otherwise specified in these Conditions of Approval. Any failure by City to insist upon the strict or timely performance of any obligation of Project Applicant, including, without limitation compliance with these Conditions of Approval, regardless of the length of time for which failure continues, shall not constitute a waiver of City's right to demand strict compliance with such requirements in the future. No waiver by City of any failure of performance with these Conditions of Approval or other requirements associated with the Project Approval or any law or regulation shall be effective or binding upon City unless made in writing by City, and no such waiver shall be implied from any delay or omission by City to take any action with respect to such failure.

7. **Enforcement**

It is recognized that separate legal entities may own and develop different phases or parts of the Project, and that these separate legal entities will be responsible for compliance with

all Conditions of Approval applicable to each such phase, part or facility as stated in Condition 5 above and Condition 8 below. The City shall enforce these Conditions against each legal entity/owner separately and independently from each other as long as the entities are not in common ownership and as long as the violation does not prevent compliance with Conditions on other phases or parts of the Project.

**8. Signed Copy of the Approval/Conditions**

A copy of the Approval letter and Conditions shall be signed by the project applicant, attached to each set of permit plans submitted to the appropriate City agency for the project, and made available for review at the project job site at all times. The Project Applicant and its agents, heirs, successors (including, without limitation, any successive owner of any portion of the Project Site) and assigns (collectively, "Project Applicant") shall be bound by these Conditions of Approval, any other terms and conditions and any other applicable legal requirements for implementation of the Project. The Project Applicant shall be responsible for assuring that any agent, heirs, successors and assigns are fully informed of, and bound by, the terms and conditions of this Approval.

**9. Blight/Nuisances**

The project site shall be kept in a blight/nuisance-free condition. Any existing blight or nuisance shall be abated within 60 days of approval, unless an earlier date is specified elsewhere.

**10. Indemnification**

- a. To the maximum extent permitted by law, the project applicant shall defend (with counsel acceptable to the City), indemnify, and hold harmless the City of Oakland, the Oakland City Council, the Oakland Redevelopment Successor Agency, the Oakland City Planning Commission, and their respective agents, officers, employees, and volunteers (hereafter collectively called "City") from any liability, damages, claim, judgment, loss (direct or indirect), action, causes of action, or proceeding (including legal costs, attorneys' fees, expert witness or consultant fees, City Attorney or staff time, expenses or costs) (collectively called "Action") against the City to attack, set aside, void or annul this Approval or implementation of this Approval. The City may elect, in its sole discretion, to participate in the defense of said Action and the project applicant shall reimburse the City for its reasonable legal costs and attorneys' fees.
- b. Within ten (10) calendar days of the filing of any Action as specified in subsection (a) above, the project applicant shall execute a Joint Defense Letter of Agreement with the City, acceptable to the Office of the City Attorney, which memorializes the above obligations.



- c. These obligations and the Joint Defense Letter of Agreement shall survive termination, extinguishment, or invalidation of the Approval. Failure to timely execute the Letter of Agreement does not relieve the project applicant of any of the obligations contained in this Condition or other requirements or Conditions of Approval that may be imposed by the City.

**11. Severability**

The Approval would not have been granted but for the applicability and validity of each and every one of the specified Conditions, and if one or more of such Conditions is found to be invalid by a court of competent jurisdiction this Approval would not have been granted without requiring other valid Conditions consistent with achieving the same purpose and intent of such Approval.

**12. Special Inspector/Inspections, Independent Technical Review, Project Coordination and Monitoring**

The project applicant may be required to cover the full costs of independent third-party technical review and City monitoring and inspection, including without limitation, special inspector(s)/inspection(s) during times of extensive or specialized plan-check review or construction, and inspections of potential violations of the Conditions of Approval. The project applicant shall establish a deposit with the Bureau of Building, if directed by the Building Official, Director of City Planning, or designee, prior to the issuance of a construction-related permit and on an ongoing as-needed basis.

**13. Public Improvements**

The project applicant shall obtain all necessary permits/approvals, such as encroachment permits, obstruction permits, curb/gutter/sidewalk permits, and public improvement (“p-job”) permits from the City for work in the public right-of-way, including but not limited to, streets, curbs, gutters, sidewalks, utilities, and fire hydrants. Prior to any work in the public right-of-way, the applicant shall submit plans for review and approval by the Bureau of Planning, the Bureau of Building, and other City departments as required. Public improvements shall be designed and installed to the satisfaction of the City. The Applicant shall offer public improvements for dedication to the City as further described in Condition No. 54.

**14. Compliance Matrix**

The Project Applicant shall submit a Compliance Matrix, in both written and electronic form, for review and approval by the Bureau of Planning and the Bureau of Building that lists each Condition of Approval (including each mitigation measure if applicable) in a sortable spreadsheet. The Compliance Matrix shall contain, at a minimum, each required Condition of Approval, when compliance with the Condition is required, and the status of compliance with each Condition. For multi-phased projects, the Compliance Matrix shall

indicate which Condition applies to each phase. The project applicant shall submit the initial Compliance Matrix prior to the issuance of the first construction-related permit and shall submit an updated matrix upon request by the City.

**15. Construction Management Plan**

Prior to the issuance of each construction-related permit and project phase, the project applicant or his/her general contractor shall submit a Construction Management Plan (CMP) for review and approval by the Bureau of Planning, Bureau of Building, and other relevant City departments such as the Fire Department and the Public Works Department as directed. The CMP shall contain measures to minimize potential construction impacts including measures to comply with all construction-related Conditions of Approval (and mitigation measures if applicable) such as dust control, construction emissions, hazardous materials, construction days/hours, construction traffic control, waste reduction and recycling, stormwater pollution prevention, noise control, complaint management, and cultural resource management (see applicable Conditions below). The CMP shall provide project-specific information including descriptive procedures, approval documentation, and drawings (such as a site logistics plan, fire safety plan, construction phasing plan, proposed truck routes, traffic control plan, complaint management plan, construction worker parking plan, and litter/debris clean-up plan) that specify how potential construction impacts will be minimized and how each construction-related requirement will be satisfied throughout construction of the project.

**16. Standard Conditions of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP)**

All mitigation measures identified in the *Oak Knoll Mixed Use Community Plan Project Supplemental EIR* are included in the Standard Condition of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP) which is included in these Conditions of Approval and are incorporated herein by reference, as Attachment N, as Conditions of Approval of the project. The Standard Conditions of Approval identified in the *Oak Knoll Mixed Use Community Plan Project Supplemental EIR* are also included in the SCAMMRP, and are, therefore, incorporated into these Conditions by reference but are not repeated in these Conditions. To the extent that there is any inconsistency between the SCAMMRP and these Conditions, the more restrictive Conditions shall govern. In the event a Standard Condition of Approval or mitigation measure recommended in the *Oak Knoll Mixed Use Community Plan Project Supplemental EIR* has been inadvertently omitted from the SCAMMRP, that Standard Condition of Approval or mitigation measure is adopted and incorporated from the *Oak Knoll Mixed Use Community Plan Project Supplemental EIR* into the SCAMMRP by reference. The project applicant and property owner and subsequent merchant builders or developers, shall be responsible for compliance with the requirements of any submitted and approved technical reports, all applicable mitigation measures adopted, and with all Conditions of Approval set forth herein at his/her sole cost and expense, unless otherwise expressly provided in a specific mitigation measure or Condition of Approval, and subject to the review and approval by the City of Oakland.



The SCAMMRP identifies the timeframe and responsible party for implementation and monitoring for each Standard Condition of Approval and mitigation measure. Monitoring of compliance with the Standard Conditions of Approval and mitigation measures will be the responsibility of the Bureau of Planning and the Bureau of Building, with overall authority concerning compliance residing with the Environmental Review Officer. Adoption of the SCAMMRP will constitute fulfillment of the CEQA monitoring and/or reporting requirement set forth in section 21081.6 of CEQA.

Prior to the issuance of the first construction-related permit, the project applicant shall pay the applicable mitigation and monitoring fee to the City in accordance with the City's Master Fee Schedule.

**17. Payment of Fees**

- a. Prior to issuance of a building permit, the project Applicant shall submit payment for all applicable and required fees including but not limited to the School fees, transportation fees, etc.
- b. Within one year following the Effective Date, the Project Applicant shall enter into an agreement to specify how fees and deposits will be managed to implement the Project. The City and the Project Applicant acknowledge the Standard Conditions of Approval/Mitigation Monitoring and Reporting Program (SCAMMRP) requires the Project Applicant to directly contract with a number of independent experts monitoring construction or operation activities, including but not limited to biological, historic architectural and tribal monitors. In addition, the Project Applicant shall fund the full costs of all independent technical and other consultants the City reasonably deems necessary to comply with the Conditions of Approval, the Project Approvals and the SCAMMRP, as the final design and building permit plans for each Development Phase are submitted. All work performed pursuant to this Condition of Approval shall be under the direct supervision and direction of the City. Accordingly, the Applicant shall deposit funds in amounts acceptable to the City to cover the full costs of such consultants and other types or review, monitoring and inspection, including, without limitation, third-party plan check fees.

**PART 2: PROJECT SPECIFIC STANDARD CONDITIONS OF APPROVAL**

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**Recommended Measures from Supplemental EIR**

**18. Star Tulip: Recommendation BIO-1**

Prior to construction, the following measures shall be implemented:

- a. A qualified botanist shall flag the location of Oakland star tulip plants during the flowering period prior to site grading. Under the direction of the qualified botanist, bulbs and associated soil plugs shall be harvested from 100 percent of the Oakland star tulip plants within the Project site following flowering and withering of leaves.
- b. Harvested bulbs shall be replanted on site in an area designated for passive open space preservation.
- c. The Project sponsor shall prepare a Monitoring Plan for relocated/transplanted Oakland star tulip plants within the Project site. The plan shall detail methods and location for relocating or reintroducing Oakland star tulip population, annual monitoring for successful establishment, and reporting protocols. The success criteria for relocated plants is 0.5:1 ratio [number of plants established: number of plants impacted] after two years.
- d. Contingency measures such as obtaining bulbs from other locations should be included in the plan if it appears the success criterion will not be met after two years.
- e. The plan shall be developed in consultation with the appropriate agencies prior to the start of local construction activities.
- f. Monitoring reports shall include photo-documentation, planting specifications, a site layout map, descriptions of materials used, and justification for any deviations from the monitoring plan.

**19. Sudden Oak Death (during relocation of existing trees within the Project site or introduction of new trees): Recommendation BIO-2)**

The following measures shall be implemented during relocation of existing trees within the Project site or introduction of new trees to the Project site through mitigation plantings to prevent the spread of *Phytophthora ramorum*, the pathogen that causes SOD.

- a. Before working:
  - i. Provide crews with sanitations kits. (Sanitation kits should contain the following: Chlorine bleach [10/90 mixture bleach to water], or Clorox Clean-up®, scrub-brush, metal scraper, boot brush and plastic gloves).
  - ii. Ensure that work crews have properly cleaned and sanitized pruning gear, trucks and chippers prior to entering the Project Area.
  - iii. Clean and sanitize shoes, pruning gear and other equipment before working in an area with susceptible species (i.e. coast live oak, canyon live oak and California bay).



- b. While working:
  - i. When possible, conduct all tree work on *P. ramorum*-infected and susceptible species during the dry season (June - October). The pathogen is most likely to spread during periods of high rainfall especially in spring (April and May). Working during wet conditions should be avoided.
  - ii. If working in wet conditions cannot be avoided, keep equipment on paved or dry surfaces and avoid mud.
  - iii. Work in disease-free areas before proceeding to suspected-infestation areas.
  - iv. All debris from California bay trees, the primary vector of the pathogen, shall be mulched and spread in place, moved to a sunny dry area free of coast live oak, or disposed of offsite in a permitted disposal facility in accordance with state and federal regulations.
  - v. When removing California bay trees, all mulch and debris shall be segregated from other species when chipping, and all pruning gear and equipment, including chippers and trucks shall be cleaned and sanitized before working on coast live oaks.
  
- c. After working:
  - i. Use all reasonable methods to clean and sanitize personal gear and crew equipment before leaving a *P. ramorum*-infested site. Scrape, brush and/or hose off accumulated soil and mud from clothing, gloves, boots and shoes. Remove mud and plant debris, especially California bay, by blowing it out or power washing chipper trucks, chippers, buckets trucks, fertilization and soil aeration equipment, cranes, and other vehicles.
  - ii. Restrict the movement of soil and leaf litter under California bay trees as spores are most abundant on California bay leaves. Contaminated soil, particularly mud, and plant debris on vehicle tires, workers boots, shovels, chippers, stump grinders, trenchers, etc., may result in pathogen spread if moved to a new, un-infested site. Thoroughly clean all equipment and remove or wash soil, mud and plant debris from these items before use at another site. If complete on-site sanitation is not possible, complete the work at a local power wash facility.
  - iii. Tools used in tree removal/pruning may become contaminated and should be cleaned thoroughly with a scrub brush and disinfected with Lysol® spray, a 70% or greater solution of alcohol, or a Clorox® solution (1 part Chlorox® to 9 parts water or Clorox Clean-up®).
  
- d. When planting:

- i. Replanting should occur in the early fall when the pathogen is less active, and in order to take advantage of seasonal rains. Replanting activities should avoid late winter and spring.
- ii. Planting sites for susceptible species including coast live oak and canyon live oak should be selected in areas that are at least 20 yards away from California bay trees, brush and/or plant material.
- iii. California bay shall not be used as mulch for new plantings.
- iv. Small, non-protected (less than 9 inches diameter) California bay trees and brush should be cleared within a 20-yard or greater buffer where feasible to protect susceptible oak trees that are selected for preservation.

**20. CPTED: Recommendation PSR-1**

As part of the City's standard development review process, the Project Applicant should submit the Project plans for Crime Prevention through Environmental Design (CPTED) review by the Oakland Police Department and Bureau of Planning staff. The Project should consider design features included on the City's CPTED Checklists for residential, commercial, and civic uses. The Project Applicant shall incorporate the Police Department's recommendations into the final Project design and shall implement the design measures to an extent generally consistent with this Project Approval. CPTED review and recommendations may address points of access to the Project site or adjacent parcels, adequate public lighting, landscaping and buffering that provides visual access, particularly in parks, open spaces, and pedestrian and bicycle facilities, etc.

**21. Pedestrian Safety (Crosswalks)**

- a. Recommendation TRANS-1: Provide high-visibility crosswalks (meaning denoted with reflective striping or other high-visibility pavement markings) across Mountain Boulevard at Creekside Parkway, across Mountain Boulevard at Sequoyah Road, across Keller Avenue at Creekside Parkway, and at the unsignalized or uncontrolled movements at intersections within the site, consistent with City of Oakland's guidelines in place at the time of final design.
- b. Recommendation TRANS-3: Provide sidewalk along southbound Mountain Boulevard to close the existing gap between the Oak Knoll Heights exit driveway and the intersection of Sequoyah Road and Mountain Boulevard.

**22. Off-site Transportation Improvements and Capital Improvements**

- a. Applicant shall design and install the off-site intersection improvements described as intersections #2, 3, 12, 13, 16, 38, and 40 in the Final EIR, provided that Caltrans and City issue all necessary permits for such improvements.



- b. Applicant shall complete installation of all such off-site improvements in accordance with the timing provisions as set forth in the EIR. The EIR's timing requirements are expressed below, in terms of Equivalent Housing Units ("EHU's"), defined in Exhibit A to these Conditions.
- i. #2: I-580 Eastbound On-Ramp/Seminary Avenue/Kuhnle Avenue (390<sup>th</sup> EHU)
  - ii. #3: I-580 Westbound Off-Ramp/Seminary Avenue/Kuhnle Avenue (940<sup>th</sup> EHU)
  - iii. #12: I-580 Eastbound Off-Ramp/Fontaine Street/Keller Avenue (280<sup>th</sup> EHU)
  - iv. #13: Mountain Boulevard/Keller Avenue (60<sup>th</sup> EHU)
  - v. #16: I-580 Westbound Off-Ramp/Mountain Boulevard/Shone Avenue (500<sup>th</sup> EHU)
  - vi. #38: Improvements to Golf Links Road/I-580 Eastbound off ramp/98<sup>th</sup> Avenue (230<sup>th</sup> EHU)
  - vii. #40: Mountain Boulevard/Golf Links Road/I-580 WB Ramp (230<sup>th</sup> EHU)
- c. Applicant shall design and install public parks and trails in accordance with this Approval and shall dedicate such parks and trails, as well as certain open space areas ("Parks and Open Space Facilities") to the City or the Geologic Hazard Abatement District ("GHAD") in accordance with Exhibit B.
- d. These off-site transportation improvements and onsite parks and open space facilities may be considered "developer constructed facilities" (i.e., transportation or capital facilities that would otherwise be funded in whole or in part by the City's Transportation and Capital Improvements Impact Fee program (Oakland Municipal Code section 15.74) if approved by the City. The Applicant may seek a Credit and Reimbursement Agreement (Agreement) with the City (pursuant to Municipal Code section 15.74.120), whereby the Applicant may receive credit against the amount of the impact fee due, and possibly reimbursement from impact fees paid by other development projects. The applicant may also apply to the City Administrator for reductions or waivers of the impact fees (pursuant to Municipal Code section 15.74.080), whereby the City Administrator may find the Project will not generate a need for transportation or capital improvements infrastructure or the need will be limited so as to justify a reduced impact fee, because the Project will instead provide for these transportation or capital infrastructure improvements. The ultimate approval of any credits, reductions or waivers are at the sole discretions of the City.

**Planned Unit Development Permit, Preliminary Development Plan and Final Development Plan****23. Phase 1 Public Improvements**

Multiple final maps may be filed for the Project, subject to the Phasing Schedule set forth in Oak Knoll Preliminary Development Plan, and a more fully set forth below. Modifications to this Phasing Schedule are subject to the review and approval of the Development Director, and at his/her sole discretion; any modifications may be subject to review of the Planning Commission. The Oak Knoll Preliminary Development Plan anticipates that the property will be developed in three phases, as shown in **Exhibit C**. The Developer shall have the right to develop the Project at such time as Developer deems appropriate; however, Developer may not proceed with development of Phase 2 or 3 until all public improvements for Phase 1 (see **Exhibit D**) are complete, as further described below for specific Phase 1 public improvements.

- a. Prior to issuance of the first building permit for Phase 1, Developer shall obtain all necessary permits and shall commence construction of all **Rifle Range Creek improvements**, consistent with the Hydrology Report (Restoration Plan and Preliminary Creek Protection Plan, ESA, as revised February 24, 2016) and Master Developer Site Improvements for Creek Restoration Planting Plan and Restoration Sections (FDP Sheets L043 through L045), and including City-issued conditions of approval pursuant to the Creek Permit for Rifle Range Creek, and all applicable EIR mitigation measures. All Rifle Range Creek improvements must be deemed complete and satisfactory by City and any other applicable regulatory agencies prior to issuance of a certificate of occupancy for the final unit in Phase 1, and prior to issuance of any building permits for Phases 2 or 3. The term “all Rifle Range Creek improvements” include the following:
  - i. The **Creekside Loop Vehicle Bridge**, consistent with Master Developer Site Improvements FDP, Sheets L012 through L014
  - ii. The **Pedestrian Bridge** across Rifle Range Creek, consistent with Master Developer Site Improvements FDP, Sheets L012 through L014
  - iii. Trail improvements and trail signage through Open Space Parcel A (i.e., the **Rifle Range Creek Creekside Trail**), connecting from Creekside Parkway to the relocated Club Knoll parcel, consistent with Master Developer Site Improvements FDP Sheet L005.
- b. To facilitate orderly development, the construction of primary access roadways (see Creekside Parkway and Creekside Loop, below) may be individually phased to meet initial Phase 1 development requirements. Prior to issuance of the first certificate of occupancy for the first residential unit within any individual portion of Phase 1, any sub-phased roadway improvements shall meet the requirements of the City of Oakland Subdivision Ordinance and the provisions of the Subdivision

Map Act, and shall be approved by the Oakland Fire Department as providing two acceptable means of access, inclusive of construction of the Creekside Loop Vehicle Bridge.

- c. Prior to issuance of any building permits for Phases 2 or 3, Developer shall grade and construct all remaining **Creekside Parkway** improvements from Mountain Boulevard to Keller Avenue, consistent with Master Developer Site Improvements FDP Sheets L002 and L003, inclusive of intersection improvements at Mountain Boulevard, streetlights, entry monuments and streetscape landscape improvements. The Creekside Parkway shall be designed and installed design fully consistent with the Creekside Parkway Design Details as presented in Master Developer Site Improvements FDP Sheet L007. These improvements shall include the following:
  - i. A 68-foot right-of-way inclusive of **Class I bikeway and multi-use trail along Rifle Range Creek**.
  - ii. Installation of a new traffic signal at the **Mountain Boulevard/Creekside Parkway intersection**
  - iii. Installation of an all-way-stop control at the **Keller Avenue/Creekside Parkway intersection**
  - iv. Design and relocation of the **bus stop and bus shelter** near the Mountain Boulevard/Creekside Parkway intersection
- d. Prior to issuance of any building permits for Phases 2 or 3, Developer shall grade and construct all remaining **Creekside Loop** improvements, from Mountain Boulevard to Creekside Parkway (or to the Seneca parcel, if the Seneca parcel is operating), consistent with Master Developer Site Improvements FDP Sheets L003, inclusive of streetlights and streetscape landscape improvements. The Creekside Loop design shall be fully consistent with the Creekside Loop Design Details as presented in Master Developer Site Improvements FDP Sheet L008.
  - i. The Developer shall install improvements as necessary at the **Mountain Boulevard/Creekside Loop intersection** to ensure right-in/right-out only access.
- e. Prior to issuance of any development-related permit for VTTM Parcel 7, Developer shall commence relocation of the Club Knoll building, including obtaining a necessary demolition permit, grading of the new building receiver site (VTTM Parcel H), disassembly of the existing Club Knoll building, and moving all disassembled building components to safe storage at VTTM Parcel 7, consistent with **Club Knoll Relocation and Rehabilitation** FDP (Architectural Dimensions, April 03, 2017), and all applicable EIR mitigation measures. Prior to issuance of any building permits for Phases 2 or 3, or within 2 years from issuance of the Club Knoll demolition permit for relocation, whichever comes



first, Developer shall complete (via City issuance of a certificate of occupancy) restoration of Club Knoll.

- f. Prior to issuance of a certificate of occupancy for any residential units on VTTM Parcel 7, Developer shall install street and sidewalk improvements, including approved *Emergency Vehicle Access* (EVA) improvements through the “Barcelona” property, with EVA-only connection to Sequoyah Road.
- g. Prior to issuance of the 100th residential building permit in Phase 1, Developer shall construct the *Creekside Entry Park* improvements (including trail and trail signage), consistent with Master Developer Site Improvements FDP, Sheets L027 through L028.
- h. Prior to issuance of a certificate of occupancy for any development within the Retail Village that is adjacent to the public plaza, Developer shall install the *Public Plaza* as indicated in the Preliminary Development Plan. The proposed grocery store site can be constructed first, without the plaza.

#### **24. Phase 2 and 3 Public Improvements**

The Developer shall have the right to develop Phases 2 and 3 of the Project at such time as Developer deems appropriate; however, Developer may not proceed with development of Phases 2 or 3 until all public improvements for Phase 1 (see Condition #23 above) are completed, and as further described below for specific Phase 2 and 3 public improvements.

- a. Developer shall construct the *North Neighborhood Park*, consistent with Master Developer Site Improvements FDP, Sheets L029 through L031, prior to issuance of a certificate of occupancy for any residential units on VTTM Parcels 21 and/or 25.
- b. Developer shall construct the *Village Pocket Park*, consistent with Master Developer Site Improvements FDP, Sheets L032 through L034, prior to issuance of the final certificate of occupancy for residential units on VTTM Parcels 19 or 20.
- c. Developer shall construct the *Oak Knoll Memorial Park* including the trail and trail signage through Open Space Parcel J, consistent with Master Developer Site Improvements FDP, Sheets L035 and L038, prior to issuance the first certificate of occupancy for residential units on VTTM Parcel 15, or prior to the dedication of Open Space Parcel J.
- d. Developer shall construct the *Emergency Vehicle Assess* improvements to VTTM Parcels 16 and 17 as shown on the VTTM, prior to issuance of a certificate of occupancy for any residential units on VTTM Parcels 16 or 17.

- e. Developer shall implement trail improvements and trail signage through *Open Space Parcel I* (i.e., the Hardenstein property), prior to issuance of a certificate of occupancy for any residential units on VTTM Parcels 13 or 14.
- f. Developer shall implement trail improvements and trail signage through *Open Space Parcels B and R* (i.e., adjacent to lower Keller Avenue and Mountain Boulevard), prior to issuance of a certificate of occupancy for any residential units on VTTM Parcels 1 or 2.

**25. Club Knoll Work Plan, Approval and Monitoring**

*Prior to approval of a construction-related permit for Club Knoll*

A Final Work Plan for Club Knoll Relocation and Rehabilitation shall be prepared and submitted for City review and approval. The contents of the Final Work Plan shall include:

- a. Final Relocation Travel Route Plan
- b. Complete Baseline Building Conditions Study, Structural
- c. Complete Building Features Inventory and Plan
- d. All identified Specific Relocation/ Rehabilitation Measures as included in the SEIR

At City's discretion, City may retain third-party independent professional consultants to review and make recommendation on the Final Work Plan prior to approval. The Final Work Plan shall be submitted to LPAB for their review and approval prior to implementation. A third-party independent professional preservation architect and structural engineer (as defined in the Carey & Co. report dated May 3, 2016) shall be on site to monitor dismantlement and reassembly of Club Knoll.

**26. Owner's Completion Bond**

*Prior to issuance of a construction-related permit*

Consistent with Municipal Code Section 15.44.030, the project applicant or their designated representative shall file with the Building Inspector a surety company bond executed by owner or lessee as principal, and conditioned as follows:

- a. That all of the work required to be done to complete the relocation and rehabilitation shall be as set forth in the Final Work Plan for Club Knoll pursuant to the conditions of approval set forth herein.

- b. Such bond shall be in principal amount equal to the estimated cost of the work proposed to be done, plus ten percent, and shall name the City of Oakland as obligee.
- c. If the Building Inspector determines that the conditions and obligations of the Club Knoll relocation and rehabilitation permit as set forth in the Final Work Plan have not been met by the property owner or lessee, the Building Inspector may notify the property owner of the obligations with a 60-day opportunity to cure. If such cure is not effected, the City may proceed at the property owner's expense to complete all remaining obligations under the Final Work Plan. Remaining relocation and rehabilitation work may be done by private contractors, and the Building Inspector shall keep an itemized account of all such reasonable costs.
- d. Upon completion of the work, the City Manager or his or her designee shall provide written notice to the owner or lessee, showing the itemized reasonable cost of such work and giving notice of the day, hour and place when the City Council will hear and pass upon a report by the Building Inspector or his or her representative of said costs.

**27. Surety Bond**

*Prior to issuance of a construction-related permit*

Consistent with Municipal Code Section 15.44.070 and 080, the project applicant shall file a surety bond with the City Clerk, executed by the project applicant and by a surety company authorized to do business in California as surety.

- a. The surety bond shall name the city of Oakland as obligee, and shall be a principal amount as may be fixed by the City Manager based upon the facts and conditions of the proposed relocation and rehabilitation of Club Knoll.
- b. The surety bond shall be conditioned as to the following:
  - i. That the project applicant (or their designated representative) shall well, truly, honestly and faithfully perform and execute the duties of a building mover as regulated by the Oakland Municipal Code.
  - ii. That the project applicant (or their designated representative) shall strictly comply with all the applicable conditions and requirements of the Oakland Municipal Code regulating the moving of buildings.
  - iii. That the project applicant (or their designated representative) shall pay any and all losses or damages that may result from moving the Club Knoll building, to any property owned or controlled by the city or for which it may be responsible, and to any property belonging to any public utility company or public carrier.



- iv. That the project applicant (or their designated representative) shall save, indemnify and keep harmless the city against all liabilities, judgments, costs and expenses which may in any way accrue against the city as a consequence of the granting of the permit to move the building.
- v. That the project applicant (or their designated representative) shall file an insurance policy of public liability and property damage with the City Clerk that satisfies all City requirements.

**28. Club Knoll Relocation**

The relocation of Club Knoll shall follow all building permit procedures including but not limited to noticing requirements, as applicable.

**29. Historic Maintenance**

*Ongoing*

The project applicant and/or successors shall keep in good repair all exterior and interior portions of Club Knoll, the maintenance of which is necessary to prevent deterioration and decay of the building. The Covenants, Conditions and Restrictions (CC&Rs) approved for the operation of the HOA shall include:

- a. a requirement to contract with a professional property management firm to operate and maintain the Club Knoll building on their behalf, potentially off-setting these management expenses through revenue derived from commercial lease of certain space within the building.
- b. a provision requiring the HOA to maintain the exterior façades of the building and the landscaping around the building at commercially reasonable standards of repair and appearance (which standards shall be defined in the CC&Rs)
- c. a provision stating that, after adequate notice to the HOA and an opportunity for the HOA to cure any alleged failure to maintain, the City shall have the right to perform the required maintenance and repairs in the event the HOA fails to do so,
- d. a written procedure for the reimbursement of costs incurred by the City in so maintaining to the standard set forth in the CC&Rs;
- e. a provision stating that the City shall have the right to enforce the Club Knoll maintenance provisions contained in the CC&Rs as a third-party beneficiary.

**30. Public Art for Private Development Condition of Approval (Commercial)**

The commercial portions of the Project are subject to the City's Public Art Requirements for Private Development, adopted by Ordinance No. 13275 C.M.S. ("Ordinance"). As a

non-residential project, the public art contribution requirement is equivalent to one percent (1%) of building development costs for the project. The contribution requirement can be met through the commission or acquisition and installation of publicly accessible art on the development site, payment of an in-lieu contribution to the City's established public art fund, or satisfaction of alternative compliance methods described in the Ordinance. The applicant shall provide proof of full payment of the in lieu contribution, or provide proof of installation of artwork on the development site prior to the City's issuance of a final certificate of occupancy for the applicable Phase in which the public art is located unless a separate, legal binding instrument is executed ensuring compliance within a timely manner, subject to City approval. On-site art installation shall be designed by independent artists, or artists working in conjunction with arts or community organizations, that are verified by the City to either hold a valid Oakland business license or be an Oakland-based 501(c)(3) tax designated organization in good standing. If the applicant or owner desires to install art created by an artist not verified by the City, the applicant or owner shall pay a verification fee to the City in accordance with the Master Fee Schedule.

When Required: Prior to issuance of Final Certificate of Occupancy and Ongoing

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

**31. Public Art for Private Development Condition of Approval (Residential)**

The residential portions of the Project are subject to the City's Public Art Requirements for Private Development, adopted by Ordinance No. 13275 C.M.S. ("Ordinance"). As a residential project, the public art contribution requirement is equivalent to one-half percent (0.5%) of building development costs for the project. The contribution requirement can be met through the commission or acquisition and installation of publicly accessible art on the development site, payment of an in-lieu contribution to the City's established public art fund, or satisfaction of alternative compliance methods described in the Ordinance. The applicant shall provide proof of full payment of the in lieu contribution, or provide proof of installation of artwork on the development site prior to the City's issuance of a final certificate of occupancy for each Phase unless a separate, legal binding instrument is executed ensuring compliance within a timely manner, subject to City approval. On-site art installation shall be designed by independent artists, or artists working in conjunction with arts or community organizations, that are verified by the City to either hold a valid Oakland business license or be an Oakland-based 501(c)(3) tax designated organization in good standing. If the applicant or owner desires to install art created by an artist not verified by the City, the applicant or owner shall pay a verification fee to the City in accordance with the Master Fee Schedule.

The project sponsor is encouraged to allocate the public art funds to hire Oakland-based artists to provide public art on or near the site.

When Required: Prior to issuance of Final Certificate of Occupancy for the first unit and Ongoing

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

**32. Club Knoll Hours of Operation and Operations**

Hours of Operation for community events, commercial operations and Home Owner's Association (HOA) use shall be included and specifically outlined in the CC&R's for HOA. In addition, the Club uses shall operate within the Performance Standards outlined in Planning Code Section 17.120. Furthermore, any potential Alcohol and Beverage Sales shall meet Planning Code Section 17.103.030.

**33. Club Knoll Restaurant/Kitchen Uses**

Prior to issuance of a building permit, the project Applicant shall "plumb" Club Knoll for restaurant/kitchen uses including grease interceptors and exhaust, subject to the requirements of the Historic Building Code.

**34. Plug-In Electric Vehicle (PEV) Charging Infrastructure**

- a. PEV-Ready Parking Spaces: For on-site parking, the project applicant shall comply with of the requirements of Chapter 15.04 of the Oakland Municipal Code for the installation of parking spaces equipped with full electrical circuits designated for future PEV charging (i.e. "PEV-Ready) per requirements of Chapter 15.04 of the Oakland Municipal Code. Building electrical plans shall indicate sufficient electrical capacity to supply the required PEV-Ready parking spaces.

When Required: Prior to building permit final

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

- b. PEV-Capable Parking Spaces: For residential and non-residential projects with more than 11 onsite parking spaces, the project applicant shall comply with requirements of Chapter 15.04 of the Oakland Municipal Code for the installation of ADA-inaccessible conduit to supply PEV-capable parking spaces. The Building electrical plans shall indicate sufficient electrical capacity to supply the required PEV-capable parking spaces.

When Required: Prior to building permit final



Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

- c. ADA-Accessible Spaces: For public buildings, public accommodations, commercial buildings, and publicly funded housing, the project applicant shall indicate the location of future accessible EV parking spaces as required under Title 24 Chapter 11B Table 11B-228.3.2.1, and specify plans to construct all future accessible EV parking spaces with appropriate grade, vertical clearance, and accessible path of travel to allow installation of accessible EV charging station(s).

When Required: Prior to building permit final

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

**35. Bike Lane Requirements.**

Commensurate with required traffic intersection improvements, the following adjacent or proximate bike lanes shall be installed, confined within existing roadbeds through the restriping of traffic lanes. Modifications to the City's standard lane widths may be required to install these bike lanes. The installation of these bike lanes shall not require any modifications to any roadbeds, curb, gutter, bridge or other structures, other than restriping. The plans will be reviewed by the Department of Transportation's Bicycle & Pedestrian Facilities Program. If these bike lanes are not feasible under the foregoing limitations or result in new traffic impacts that were not studied in the City's Bicycle Master Plan or the Oak Knoll Project Supplemental EIR, they shall not be required to be installed as a Condition of Approval.

- a. Mountain Blvd (Maynard Ave/I-580 WB on-ramp to Golf Links Rd): If engineering studies indicate bike lanes are feasible, install bike lanes through restriping in each direction on Mountain Blvd from Maynard Ave to Golf Links Rd. Minimize the elimination of on-street parking from Shone Ave to Keller Ave by having one northbound lane. If a second northbound lane is deemed necessary to meet City traffic standards on the approach to Keller Ave., and is feasible to construct, minimize its length to what is necessary for the traffic signal operations. From Sequoyah Rd to Golf Links Rd, maintain the northbound on street parking that is immediately in front of private residences. This will leave some gaps in the bike lanes. Eliminate the parking that is not in front of private residences in order to install the bike lanes. As part of the intersection design for Mountain Blvd/Golf Links Rd, include bike lanes in both directions on Mountain Blvd between Golf Links Rd and the point approximately 400 feet north of the intersection where the curb-to-curb width of Mountain Blvd narrows.

- b. Golf Links Rd (Mountain Blvd to 98th Ave) and 98th Ave (Golf Links Rd to Stanley Ave): Install bike lanes in each direction on Golf Links Rd (Mountain Blvd to 98<sup>th</sup> Ave) and on 98<sup>th</sup> Ave (Golf Links Rd to Stanley Ave). On 98<sup>th</sup> Ave, maintain the existing on-street parking at the frontage of the Bishop O'Dowd High School parking lot, resulting in a short bike lane gap.
- c. Edwards Ave (Mountain Blvd/I-580 WB on-ramp to I-580 EB off-ramp): Install bike lanes in each direction on this one block of Edwards Ave (Mountain Blvd/I-580 WB on-ramp to I-580 EB off-ramp) to improve bicyclist safety on the existing bike route in the Mountain Blvd corridor. Specifically, the freeway overpass creates low light conditions and visibility issues that may be ameliorated by separating bicyclists from motor vehicles with dedicated bicycle lanes.
- d. Kunhle Ave (Mountain Blvd to Seminary Ave/Sunnymere Ave): In conjunction with the installation of two traffic signals, redesign through restriping the one block of Kunhle Ave from Mountain Blvd to Seminary Ave as follows: one travel lane per direction, left turn pockets, and one bike lane per direction. This redesign will eliminate one travel lane per direction.

**36. North Neighborhood Park along Keller**

No sports field lighting or permanent field striping shall be installed in the play lawn (informal ball field).

**Design Guidelines**

**37. Facade Materials**

No foam materials are appropriate as a façade, trim, parapet or detail material.

**38. Wall Design along Mountain**

Prior to issuance of a Building Permit, the project Applicant shall submit the design of the privacy wall, consistent with the entitlement documents, along Mountain Boulevard to the Bureau of Planning for review and approval with the Final Development Plan submitted for that area of the Project.

**39. Master Signage Program**

The applicant for the Retail Village FDP shall submit a master signage program for the Village Retail Center for review and approval per the Planning Code. No signage shall be visible from the freeway without subsequent CEQA review for affects to a scenic highway.

**40. Retail Signage**

No retail or commercial signage shall be visible from I-580, a scenic highway.

**41. Trail, Creek, and Bike Lane Signage / Markers**

The applicant shall submit construction drawings of trail, creek and bike lane signage and markers prior to the issuance of grading and building permits for each phased FDP. The construction drawings shall be consistent with the signage program submitted as part of the Master Developer FDP.

**Vesting Tentative Tract Map**

**42. City Surveyor Conditions**

*Prior to recording of each map*

Multiple phased final maps may be filed subject to the Phasing Schedule set forth in Oak Knoll Preliminary Development Plan. Modifications to the Phasing Schedule are subject to the review and approval of the Development Director, and at his/her sole discretion; any modifications may be subject to review of the Planning Commission.

- a. All street shall have monuments installed for each Final Map as follows:
  - i. Monuments shall be shown on the tentative map at all BC's, PC's PRC's, Intersections (with other monument lines) and Center of Cul-de Sacs or within 25 feet of the end of a road.
  - ii. This requirement is for ALL roads and driveways, public or private.
  - iii. Monuments shall be coordinated to be parallel with the right of way and equally offset from the centerlines. No utility lines shall be allowed to be placed within 3 feet on either side of the monument lines.
  - iv. Monuments shall be no greater than 600 feet apart on tangent sections. Within tangent sections greater than 600' the monuments shall be evenly spaced.
  - v. All monuments shall comply with the Standard City Monument drawings in use at that time which will be available from the City Surveyor.
  - vi. All monuments not in place at the time of the submission of the first final map shall be bonded to insure installation.
  - vii. Any existing monuments by the federal government or others shall be mapped, identified and the appropriate Record of Survey or Corner Record filed with the County. Castings and disks shall be salvaged and delivered to the City of Oakland.
- b. Benchmarks (BMs) shall be installed with the installation of improvements at the time of the filing of each final map at intersections or mid-block as follows:
  - i. BMs shall be spaced at approximately a 1/4 mile (a 1250 to 1500 foot) radii throughout the entire site.



- ii. All BMs shall be established on City of Oakland datum
  - iii. For Each BM, when set, the surveyor shall prepare an official ‘card’ (electronic) which will be provided by the City Surveyor’s office which will include level notes, descriptions, elevation, etc.
  - iv. Benchmarks not in place at the time of the submission of each final map shall be bonded to insure installation.
  - v. The approximate locations of all BM’s shall be shown upon the tentative map.
- c. All perimeter property corners shall be field established by each submitted final map. Corners shall be established with 1/1/2”x4’ iron pipes (and tags) in a concert collar and will be clearly identified by fiberglass posts (Carsonite or equal) corner markers acceptable to the City Engineer.

**Fire Department Conditions**

**43. Fire Prevention Bureau Requirements**

*Ongoing.*

The project shall comply with all the requirements from the Fire Marshall memo dated September 29, 2017 (Attachment T).

**Public Works Agency Conditions**

**44. Pedestrian Bridge**

The pedestrian bridge shall be a minimum of 8’ in width. The cross section shall be submitted for review and approval by the Department of Transportation. The pedestrian bridge shall be dedicated to and owned by the City of Oakland and shall be used solely for public pedestrian access. The pedestrian bridge shall include adequate lighting in accordance with the City of Oakland Outdoor Street Lighting Standards.

**45. FEMA Regulations**

All final design of buildings and structures, public or private, shall meet any applicable FEMA regulations to the satisfaction of the City Engineer in consultation with City Building Official, and the City’s Floodplain Administrator.

**46. Vehicle Bridge**

The new Vehicle Bridge should include new sidewalks that are a minimum of 6’ in width on both. The Vehicle Bridge shall include adequate lighting in accordance with the City of Oakland Outdoor Street Lighting Standards.

47. **Bulb-outs Design**

Bulb-outs within the City right-of-way shall be curved and designed to accommodate street sweeping trucks.

48. **Sanitary Sewers Design**

The sanitary sewer design for the project shall be gravity flow.

49. **Geotechnical Peer Review**

*Prior to issuance of a grading permit*

At the discretion of the City Engineer or the City Building Official, the applicant shall provide a Geotechnical Peer Review by a licensed Geotechnical Engineer. The recommendations provided in the peer review report shall be responded to in writing by the Geotechnical Engineer. The recommendations provided in the peer review shall be incorporated into improvement plans to the satisfaction of the City Engineer and City Building Official prior to issuance of related grading and/or building permits. The developer shall be responsible for the costs of any Geotechnical peer review as required by the City.

50. **Construction, Ownership and Maintenance of Certain Improvements**

Ownership and maintenance of certain improvements will be as set forth in the “Oak Knoll Development Ownership, Funding Sources and Maintenance Responsibilities” (“Oak Knoll Matrix”) attached hereto as **Exhibit B**. The Applicant shall dedicate, and the City shall accept, all facilities designated for City of Oakland ownership on Exhibit B, and which, in the City’s discretion, are completed in accordance with approved improvement plans, including but not limited to public parks and trails, public roads, bio-retention swales and stormdrain. For parks to be dedicated to the City, park improvement plans shall be subject to review by the Bureau of Planning. Except for public parks, Applicant shall retain the right to maintain ornamental landscaping on any City-owned property, including but not limited to street trees, street planters, and decorative signage.

Prior to approval of the first final map for the Project:

- a. **Formation of Community Facilities District**: City shall establish a community facilities district (“CFD”) pursuant to the Mello-Roos Community Facilities Act of 1982, as amended (the “CFD Act”). The CFD will include within its boundaries all of the Oak Knoll Development Project. All costs of forming and implementing such CFD, including, without limitation, costs for consultants, elections and any legal challenge, shall be at Applicant’s sole costs, and the Project Applicant shall make an initial advance payment to the City for formation costs of the CFD, and shall advance additional amounts within fifteen (15)

business days after the written request by the City, subject to subject to reimbursement out of the proceeds of bonds or facilities special taxes collected in the CFD. All contractors and consultants paid or reimbursed by the CFD or the City shall be subject to the direction of the City. The CFD shall have full responsibility for improvements and maintenance, which obligations shall be responsibilities of Applicant until such time as the CFD is fully formed and financed to City satisfaction and City approves Applicant's release from such obligations. The CFD shall be advanced by the Project Applicant, The CFD will contain two separate special taxes, described as follows:

- i. Facilities Special Tax. The facilities special tax shall be levied to finance the construction and acquisition of the Facilities (defined below) and to secure bonds issued to finance the construction and acquisition of the Facilities. The facilities special tax will be pre-payable and will escalate annually by 2%.
  - ii. Services Special Tax. The services special tax shall be levied to finance the maintenance of the Maintained Facilities (defined below). The services special tax will be levied in perpetuity and will not be pre-payable. The services special tax will escalate annually by 2%.
- b. Authorized Facilities. The CFD, through the facilities special taxes, shall be authorized to finance all of the following facilities (herein, the "Facilities"), irrespective of the geographic location of the improvements financed:
- i. Project capital improvements such as streets, utility lines, grading and drainage.
  - ii. Affordable Housing Fees of the City of Oakland, as may be used for designated capital improvements.
  - iii. Capital fees of East Bay Municipal Utilities District.
  - iv. Capital fees of the Oakland Unified School District.
- c. Authorized Services. The CFD, through the services special taxes, which shall be based on an amount determined by the City Council as necessary to maintain public facilities within the CFD, to meet City-defined standards and cost parameters, shall be authorized to maintain the following improvements (herein, the "Maintained Facilities"), which are in or adjacent to the CFD:
- i. Publicly-owned parks
  - ii. Publicly-owned bridges
- d. Joint Community Facilities Agreements. Under the CFD Act, City may be required to enter into one or more joint community facilities agreements with other governmental entities that will own or operate any of the Facilities to be financed by the CFD. The City and Project Applicant agree that they will take all



steps necessary to procure the authorization and execution of any required joint community facilities agreements with other governmental entities before the issuance of any CFD Bonds that will finance the construction or acquisition of Facilities that will be owned or operated by such other governmental entities.

- e. The RMA. In setting the tax rates in the Rate and Method of Apportionment (the “RMA”) for the CFD, the Total Tax Obligation (as defined below) on any residential unit within the CFD will not exceed two percent (2.00%) of the market value of that residential unit at the time the bonds are sold, secured by Special taxes of the CFD (the “2.00% Limitation”). The appraised value of a residential unit shall be determined by appraisal within no more than 90 days prior to the sale of bonds secured by such residential unit. The term “Total Tax Obligation” means, with respect to a residential unit at the time of calculation, the sum of: (a) the ad valorem taxes actually levied or projected to be levied at the time of calculation; (b) the assigned facilities special tax rates and the services special tax rates levied or projected to be levied at the time of calculation; and (c) all other special taxes (based on assigned special tax rates) or assessments secured by a lien on the residential unit levied or projected to be levied at the time of calculation. The RMA for the CFD will provide that the facilities special taxes will be levied on parcels of Developed Property (property for which a building permit has been pulled) at the maximum assigned rates both before and after bonds have been issued, and any facilities special taxes collected that are not needed for debt service on the bonds, administrative expenses, or replenishment of reserve funds will be available to finance the Facilities.
  
- f. Issuance of CFD Bonds. City, on behalf of the CFD, intends to issue one or more series of CFD Bonds for purposes of financing the Facilities. Project Applicant may submit written requests that City issue CFD Bonds, specifying requested issuance dates, amounts, and main financing terms. Following Project Applicant’s request, Project Applicant and City will meet with City’s public financing consultants to determine reasonable and appropriate issuance dates, amounts, and main financing terms that are consistent with these conditions of development and the CFD Goals. The CFD Bonds shall be issued pursuant to an indenture, trust agreement, or fiscal agent agreement (however denominated, an “Indenture”) between the CFD and a fiscal agent or trustee (however denominated, the “Fiscal Agent”). CFD Bonds will have a term of not less than thirty (30) years and not more than thirty-five (35) years unless Project Applicant and City agree otherwise.
  
- g. Maintenance of Facilities. The RMA will provide that the services special taxes will be used to finance the Maintained Facilities. The annual amount of the services special taxes to be levied will depend on the budgetary process described below:

- i. Services special taxes shall be levied to create a reserve fund to provide for restoration, maintenance, replacement repair or other work associated with the Maintained Facilities.
  - ii. The Project Applicant shall provide start-up funds for the CFD in an amount to be determined by the City Engineer in accordance with the approved capital development and maintenance plan, which shall be provided no later than recordation of the first final map for the Project. The Project Applicant shall also assume financial responsibility for all related work for a warranty period determined by the Public Works Director.
  - iii. The services special taxes shall be authorized to finance both on-going maintenance activities as well as a plan for unexpected maintenance and events, including events or damages that could occur as the result of site improvements associated with geotechnical, drainage or related matters. This work shall be based on the final grading, site soils conditions and specifications for improvements unless otherwise covered by the GHAD.
  - iv. The services budget shall separately identify the projected costs associated with standard annual operation, administration and maintenance work on the Maintained Facilities; long-term operation and maintenance including life cycle replacement costs of major features including but not limited to the Roadway and Pedestrian Bridge;—and the reserve fund debt service requirements described in item 1 above.
- h. Other Obligations and Requirements:
- i. The CFD shall submit an annual report to the City Council detailing compliance with Minimum Maintenance Standards, and budgetary and other financial information relevant to the CFD operations.
  - ii. The CFD shall obtain general liability insurance and directors' insurance for the Board of Directors to the extent that the CFD Board determines in its sole discretion that such insurance is available at commercially reasonable rates.
  - iii. The assessments or taxes necessary to fund the above requirements must be determined following a thorough financial analysis and must include adequate funding for indemnity and insurance obligations. The City's attorney and Risk Manager shall also review the adequacy of the funding for the indemnity and insurance and may make recommendations regarding such funding.
  - iv. The taxes or assessments shall be fully authorized and imposed on the project site prior to approval of the first final map.

**51. Annexation of Project Area into Oakland Area Geological Hazard Abatement District (GHAD)**

At Developer's request and sole cost, the City shall annex all of the properties within the boundary of the Project into the Oakland Area Geological Hazard Abatement District ("GHAD") and shall cooperate in the preparation of all documents and plans necessary for the GHAD's ownership and maintenance of the open space facilities within the Project, as set forth in Exhibit B, including but not limited to any Resolution(s) of Annexation, Engineer's Report(s) and Plan(s) of Control. To the extent the City is the fee owner of the parcels to be included within the GHAD, City shall fully cooperate with the Developer in the annexation of these parcels into the GHAD and with the implementation of all of the GHAD's operations and activities. The Applicant shall dedicate to the GHAD all facilities and land areas indicated to be owned in fee by the GHAD on Exhibit B, subject to a reservation of rights by Applicant for the purpose of maintenance of ornamental landscaping.

**52. Confirmation of Substantial Compliance with Vesting Tentative Tract Maps**

*Prior to the recordation of each Final Map*

Prior to recordation of each Final Map, site improvement plans and a title report shall be submitted for the review and approval of the City Planning Director or his/her designee demonstrating substantial compliance with the approved VTTM and the "Project Plans" as set forth in Condition of Approval 1, as well as any subsequent permit received from a responsible or other agency with authority over the project site.

**53. Financing and Conveyance Maps**

As used in these conditions of approval, "final map" means only those final maps filed for construction purposes.

- a. An Ordinance for accepting and processing Finance and Conveyance Maps, (F&C Maps) has not been adopted by the City at the time of the approval of this project. Any tentative and final F&C Maps submitted for this project shall be processed in the same manner as a Tentative Parcel Map application and Parcel Map application, respectively, and fees owed per the City's adopted Master Fee Schedule for Tentative Parcel Map applications and Parcel Map applications shall apply, respectively, unless an Ordinance for F&C Maps has been adopted and the Master Fee Schedule updated to include specific fees for F&C Maps.
- b. After approval of a Tentative F&C Map, any application for a Final F&C Map shall be submitted to the Department of Transportation, Engineering Services, and the appropriate fees shall be paid in full by the applicant prior to Staff beginning any review.



- c. The following statements shall be clearly printed in a prominent and easily legible on the face of each proposed Tentative and Final F&C Map:
  - i. “FOR FINANCE AND CONVEYANCE PURPOSES ONLY.”
  - ii. “THIS MAP DOES NOT CREATE A LEGAL BUILDING SITE. FURTHER APPLICATIONS ARE NECESSARY TO DEVELOP THIS PROPERTY.”
  - iii. “THIS MAP DOES NOT REMOVE ANY DEVELOPMENT REQUIREMENTS SET FORTH WITH APPROVAL OF THE VESTING TENTATIVE MAP 8320 AND CITY’S PLANNING PROJECT NUMBER PLN15378 WHICH MUST BE SATISFIED WITH CONTINUED DEVELOPMENT OF THE PROPERTY.”
- d. Tentative F&C Maps shall provide sufficient information on future uses and feasibility of future uses to ensure consistency with the approved vesting tentative tract map number 8320 and the project conditions of approval, including the project’s environmental mitigation measures, to the satisfaction of the Planning Director and the City Engineer.
- e. All parcel lines shown on tentative and final F&C Maps shall not conflict with any existing easements or proposed easements as identified on the approved vesting tentative map number 8320.
- f. No building permits shall be issued for development of any parcel or parcels presented in any recorded F&C Map until a Final Tract Map or Parcel Map is recorded with the County.
- g. Prior to recording any final F&C Map it shall conform to and meet the requirements of the Subdivision Map Act and the City’s Municipal Code as determined by the City Engineer.
- h. Parcels identified on the approved vesting tentative tract map 8320, 1) for which, a Final Parcel or Tract Map is already recorded with an executed Subdivision Improvement or Public Infrastructure Agreement, or 2) included in a mylar map signed by the Owner(s) and delivered to the City but not yet recorded by the County, shall be excluded, removed from, any F&C Map application as determined by the City Engineer.

**54. Offers of Dedications at time of Final Map**

*Prior to recordation of each Final Map*

Prior to recordation of each Final Map, all reservations and offers for dedications of easements, parcels and improvements and all other easements deemed necessary for all existing and proposed utilities shall be identified, to the satisfaction of the Transportation

and Right of Way Management Division, for the portion of the project included in the Final Map. The Applicant shall prepare all required documentation for such reservations and offers of dedication, including, but not limited to, deeds, legal descriptions and plat maps as requested by the City in its absolute discretion. Closure calculations for all easements, except the public utility easements (PUE) located adjacent to streets, shall be submitted for review and approval by the Transportation and Right of Way Management Division.

**55. Selection of Street Names**

*Prior to the recordation of each Final Map*

Prior to recordation of each Final Map, street names shall be selected and submitted for approval by the Bureau of Building, PWA Engineering Services, Fire Department and Police Department.

**56. Subdivision Conditions, Covenants & Restrictions (CC&Rs)**

*Prior to the recordation of the first Final Map for the first Project Phase*

Conditions, Covenants, and Restrictions (CC&Rs) for the total master planned community shall be prepared and submitted with an application for the first Final Map and shall be reviewed and approved by the Planning and Zoning Division and the City Attorney's Office. Such CC&Rs shall: (1) reserve all necessary nonexclusive easements for private streets, public utility easements or private utility access easements for pedestrian and vehicular ingress and egress, emergency vehicular access, and any necessary emergency exiting and/or public utility purposes identified by the City; and (2) include a covenant prohibiting any permanent improvements (with the exception of those shown on approved improvement plans or otherwise approved by the City) that may obstruct private streets, public utility easements, or private utility easements so as to restrict emergency vehicle ingress or egress or access to public utilities.

- a. The master community CC&Rs may include procedures whereby property within the development may be added to the CC&Rs by means of annexation as subsequent Final Maps are processed.
- b. In addition, neighborhood CC&Rs for any sub-project common interest developments shown on a Final Map (whether condominium projects or planned developments or shared access facilities) shall be prepared and submitted prior to the issuance of building permits for those individual merchant builders, and shall be reviewed and approved by the Planning and Zoning Division and the City Attorney's Office with respect to that Final Map. It is acknowledged by the City that common interest development CC&Rs are be subject to review and approval of the California Bureau of Real Estate (CalBRE) and may, subsequent to City review, be subject to revision as directed by CalBRE or as otherwise necessary to

comply with California Subdivided Lands Act, related regulations, and California common interest development laws.

- c. Neighborhood CC&Rs which include common interest of shared access facilities shall provide for shared maintenance responsibilities for any such shared access facilities.

**57. Subdivision Improvement Plans for Each Final Map**

Engineered subdivision improvement plans in accordance with this Approval shall be prepared to meet all of the requirements of the City of Oakland Subdivision Ordinance and the provisions of the Subdivision Map Act and shall include the following: water, storm water drainage, sanitary sewer, street improvement, traffic and utility service plans. The Subdivision Improvement Agreement shall include requirements for security for all public improvements required as a condition of approval of the corresponding Tentative Subdivision Map (or the Tentative F&C Map pursuant to Condition No. 50, as applicable) not completed as of the time of Final Map (or Final F&C Map, as applicable), as described in the California Subdivision Map Act and Title 16 of the Municipal Code.

**58. Subdivision Improvement Agreement**

Prior to recordation of each Final Map, a subdivision agreement, in a form acceptable to the Design and Construction Services Division and the City Attorney's Office, shall be prepared and executed for the construction of all public improvements.

**59. Cost Estimates for Improvements**

Prior to the City Engineer's acceptance of each Final Map, an Engineer's Estimate shall be submitted for the cost of all public improvements. The estimate shall be subject to approval by the Design and Construction Services Division. Based on the engineer's estimate, bonds or other approved securities must be furnished to the City in accordance with the Subdivision Improvement Agreement to ensure completion of public ~~and private~~ improvements.

**60. Final Grading Plan for Mass Grading or for Individual Project Phases**

Prior to the issuance of a grading permit related to a building permit(s) or execution of a subdivision improvement agreement, Applicant shall file a final grading plan that is to accompany the subdivision improvements plans that shall be prepared and submitted with the grading permit application, and shall include the following:

- a. The grading plan shall show all proposed and existing contours as well as proposed drainage improvements.
- b. As applicable to the grading phase, final grading, drainage and foundation plan shall be prepared in accordance with the recommendations of the geotechnical



report (including recommendations of the soil engineer) and supplemental letters. These reports shall identify the specific amount of fill material, if any, that is to be imported on the site. Retaining walls shall be a split-face or scored concrete block, or consistent with the approved PDP, FDP, and Design Guidelines, and shall not exceed the heights as specified on the approved Vesting Tentative Map, and shall require separate building permits.

- c. The final grading plan of all pads shall substantially comply with the preliminary grading presented on the approved Vesting Tentative Map, and shall include any inconsistencies between the contours and numeric grade shown on the Vesting Tentative Map and the final grading plan shall be subject to the approval of the City Engineer and Building Official.
- d. No mass grading shall occur between October 15th and April 15th unless approved by the City Engineer and Building Official.

**61. Construction Plans for Fire Apparatus Access Roads, Off-street Parking and Access to Lots.**

Prior to recordation of a final map for each project phase the improvement plans for each Final Map shall be submitted to the Fire Department for review and approval. These construction plans shall show for fire apparatus access roads, off-street parking and fire access to all lots/parcels within the Oak Knoll community. These plans shall include the following:

- a. Construction documents. Construction plans for fire access roads and plans for the water supply and distribution. CFC 501.3 and 501.4.
- b. Construction of buildings. Access roads and on-site hydrants shall be installed, operating and available prior to and during construction unless approved otherwise by the Fire Department.
- c. Fire apparatus access road widths shall adopt the Fire Department's access guidelines as adopted in the CFC Appendix D.
- d. Fire watch and fire apparatus access shall be provided per CFC Chapter 5 and Appendix C during all phases of construction, especially upon delivery of combustible construction materials at the site.
- e. All fire apparatus access roads shall not exceed 18%. The apparatus turnaround shall not exceed a 5% slope.

**62. Additional Required Information on Final Map(s)**

Prior to recordation, the Final Map prepared and submitted for each project phase shall include the following information, as applicable:

- a. All easements to be maintained shall be clearly indicated and easements to be abandoned shall be memorialized on the map by written notation of each easement to be abandoned, shown by reference to the recording data that created the easement.
- b. All existing utilities not intended for future use in the subdivision, and not serving other off-site areas shall be abandoned, and new utilities shall be established and dedicated as needed to serve on-site and off-site areas.
- c. The Fontaine Overpass approach ramp is within the property lines with a notation on the ALTA survey that abutters rights have been relinquished to the State of California. Prior to the recordation of the first Final Map, the parcel of land underlying and separated from the main portion of the site by Fontaine shall be offered for dedication to the State of California.
- d. Provide documentation to show that permission, conditioned or not, has been granted by EBMUD, or that there is no restriction or limitation, under the EBMUD easement, to the construction of the roadway and the proposed housing on the EBMUD tunnel easement (766 OR 472).
- e. The extension of Barcelona Street shall be designed and shown across APN 048-6870-002, from the Project boundary to the existing terminus of Barcelona Street, including an emergency vehicle access for public street and utilities purposes, and for the work necessary to accomplish these purposes.

**63. Changes to the Vesting Tentative Tract Map**

*Ongoing*

Any final map must substantially comply with the approved Vesting Tentative Tract Map (VTTM) per required findings. Significant changes to an approved VTTM shall require re-approval of the VTTM. Significant changes would be nonconformance with the Conditions of Approval for the VTTM as well as the following: increases in the lot count, reconfiguration of the site that alters the grading concept, road widths, road slopes that exceed Fire Department requirements, and major changes to creek parcel widths, or any change deemed significant by the City Surveyor and/or the Engineering Services Division. Minor changes to the approved VTTM shall be administratively approved by the City Surveyor and/or the Engineering Services Division prior to final map approval and recordation.

**64. Street Lighting Plan and Photometric Analysis**

*Prior to issuance of a building permit or recordation of a Final Map for each Neighborhood or Project Phase, whichever occurs first:*

- a. The applicant shall submit a detailed street lighting plan and photometric analysis for review and approval, with the improvement plans for construction of all new roadways. Planning Division review shall ensure that the lighting plan and photometric analysis comply with the requirements set forth in Mitigation Monitoring Program and in the Oak Knoll Design Guidelines.
- b. Construction documents shall meet the City of Oakland Public Works Agency Outdoor Street Lighting Standards.

**65. Transportation - Installation of AC Transit Bus Stop and Shelter/Landscaping**

- a. Improvement plans for the Mountain Boulevard/Creekside Parkway intersection shall incorporate design and development of a relocated bus stop and bus shelter. The location, design and specifications for the bus stop and shelter shall be to the satisfaction of the City of Oakland Transportation Services Division and AC Transit.
- b. The project Applicant and its successors shall landscape and maintain the parkway area of northbound Mountain Boulevard along the adjacent Project frontage. The parkway area is defined as the area between the back of curb shown on the VTTM and the Project property line and shall be considered part of the public street so long as the City of Oakland continues to own such right of way. The landscape improvements and maintenance obligations shall continue until such time that the City of Oakland no longer owns the right of way in whole or part in which the parkway is located. The landscape improvements shall be consistent with the existing landscaping along Mountain Boulevard.

**66. Gated Entries**

All roads and streets shall be dedicated to the City and accepted as public streets. As such, no such roads/streets shall be gated. Exceptions will only be granted for emergency vehicle access.

**67. Barcelona Street Extension/Improvement as a Public Street**

As shown on **Exhibit E**, the Applicant shall improve the “Barcelona Road Reservation” as a public street across APN 048-6870-002. This street shall run from the project boundary to a designated emergency vehicle access point to be located at the existing terminus of Barcelona Street. The improvements shall include tree removal, foundation removal, etc., to prepare the area for grading to create the roadbed, installation of utilities that are appurtenant to a public street, and installation of an appropriate emergency vehicle access gate or bollards. The City shall grant Applicant all access rights necessary for the installation of the improvements, including but not limited to an encroachment permit and shall maintain the street as a public street.



**68. EBMUD and Right of Way Easements**

*Ongoing*

EBMUD owns and operates water supply tunnels and pipelines along the northern edge of the development site in the EBMUD right-of way (R/W 206 and R/W 1634) and property (506 and 217-A). The integrity of these tunnels and pipelines needs to be maintained at all times. Any proposed construction activity in EBMUD rights-of-way and property would be subject to the terms and conditions determined by EBMUD including relocation of water mains and/or rights-of-way at the project sponsor's expense. The Community Park (North Neighborhood Park) to be developed by Oak Knoll on EBMUD property is subject to the execution of the land exchange between Oak Knoll and the District. If the land exchange does not occur, the applicant shall submit revised plans to exclude the EBMUD parcel from the North Neighborhood Park.

**69. EBMUD Water Service**

When development plans are finalized the applicant shall contact EBMUD's New Business Office and request a water service estimate to determine costs and conditions for providing water service to the proposed development. The project applicant shall comply with the Landscape Water Conservation Section, Article 10 of chapter 7 of the Oakland Municipal Code. The applicant should be aware that Section 31 of EBMUD's Water Service Regulations requires that water service shall not be furnished for new or expanded service unless all applicable water-efficiency measures described in the regulation are installed at the project applicant's expense.

**70. Open Space Dedication and Trail Construction**

*Prior to the recordation of the Final Map that contains the trails*

All areas designated as Open Space (VTTM Parcels A (the creek), B, D, E J and O) shall be dedicated to the City or the GHAD as set forth in Exhibit B. Prior to acceptance of open space parcels, all trails through these open space areas shall be constructed.

- a. Trail designs shall be based on the standards and practices of the East Bay Regional Park District for width and surfacing of multi-use trails, and shall include applicable ADA criteria
- b. Trails shall have a public access easements, restrictive covenant or other method to ensure ongoing and continued access for benefit of the public.

**Creek Permit and Stormwater**

**71. Regulatory Agencies**

*Prior to issuance of a Grading Permit*

Consistent with SCAMMPR Condition of Approval, SCA GEN-1: Regulatory Permits and Authorizations from Other Agencies (#15), the project applicant shall obtain all necessary regulatory permits and authorizations from applicable resource/regulatory agencies related to the creeks. If these permits necessitate changes to the design of the creek parcel, the Conditions regarding major and minor changes apply and might necessitate re-review.

**72. Final Stormwater Control Plan**

*Prior to issuance of a Grading Permit*

The Final Stormwater Control Plan, including narrative, shall be submitted for approval prior to the issuance of the Grading Permit. The treatment devices shall be located in accordance with the VTTM.

**73. Creek Maintenance**

*After Creek Restoration and Ongoing*

Upon sign off of the creek restoration by the Regulatory Agencies, the project applicant and successors shall submit a creek maintenance plan to ensure the successful and ongoing long-term maintenance of the creek parcels including the creek channel, and banks, stability, erosion, and infrastructure (bridges, culverts, stormwater facilities, etc.) Long term creek maintenance shall be guaranteed through the formation of a Geologic Hazard Abatement District or other means approved by the Bureau of Planning, Engineering Services and Watershed Division.

**74. Trash Capture Devices**

Requirement: Plans shall be submitted for review and approval by the Director of Public Works or his/her designee that show a full trash capture device installed at all private storm drain inlets or catch basins located on the property and on the public storm drain inlets in adjacent right of way area(s) – as applicable. The plans shall show the design of the device and must meet requirements of the Regional Water Quality Control Board for full trash capture. The applicant shall install these devices according to the approved plans.

When Required: Plans shall be approved prior to approval of any construction-related permit. Installation shall be completed prior to issuance of certificate of occupancy or final permit approval.

Initial Approval: City Engineer or Designee

Monitoring/Inspection: Owner of private storm drain must maintain the full trash capture device in accordance with the requirements in the Municipal Regional NPDES Permits. Records of Inspections and maintenance must be made available to the City upon request.

Upon pre-approval of the City, project applicant may pay an annual fee to maintain devices installed in the public right-of-way.

**75. Stormwater Treatment Devices**

*Prior to issuance of a building permit*

In the event stormwater pollution prevention control devices shown on the approved vesting tentative map do not qualify as FTC devices, prior to approval of the first Final Map submittal or issuance of related construction permits, the design for stormwater pollution prevention control devices must meet C.3 requirements and include separate trash capture devices. All storm drainage improvements shall be designed and constructed to meet C.3 requirements to the satisfaction of the City Engineer. To “qualify” as FTC devices, the device design must receive approval from the Water Board.

**Tree Permit**

**76. Tree Permit (T1500124) and Removal by Phase**

*Prior to issuance of building permits*

A Tree Removal/Preservation permit application shall be approved by the Tree Services Division for removal or preservation of all protected trees on the site and adjacent properties. The applicant shall abide by all Conditions of Approval of that permit.

**77. Tree Relocation**

*Prior to issuance of a grading permit*

The applicant shall retain a qualified arborist to survey the project site and identify 20 mature oak trees that shall be transplanted. The arborist shall submit a report for review and approval that includes the following information: trees to be relocated, removal procedures, storage area for the trees, watering and care during the timeframe that the trees are out of the ground, transplant procedures, and care and timeframe of care to ensure the tree survival. The arborist report shall be submitted to the Bureau of Planning for review and approval. The trees shall be located in the Village Center, around Club Knoll and at the main entrances.

**Other**

**78. Pre-Construction Meeting with the City**

*Prior to issuance of a grading, demolition, or building permit*

A pre-construction meeting shall be held with job inspectors and the general contractor/on-site project manager with the City’s project building coordinator to conform that conditions



of approval that must be completed prior to issuance of a grading, demolition, or building permit have been completed (including pre-construction meeting with neighborhood, construction hours, neighborhood notification, posted signs, etc.) The project applicant will coordinate and schedule this meeting.

**79. Transportation and Parking Demand Management**

The Project applicant has submitted a final master Transportation and Parking Demand Management (TDM) Plan. The subsequent merchant builders and successor's will submit a final plan noting the specific TDM measures, implement the plan, and achieve the required twenty percent (20%) vehicle traffic reduction (VTR) and reduced parking demand generated by the project. The TDM Plan indicates the estimated VTR for each identified strategy based on published research or guidelines where feasible.

**80. Native American Tribal Monitor**

*During creek grading*

At least seven days before ground-disturbing activities in the creek corridor are scheduled to begin, one tribal monitor of the choosing of the tribes that have expressed interest in the Project shall be invited to monitor such ground-disturbing activities, and shall be afforded the opportunity to monitor such activities if the tribal monitor chooses to be present. If there is a change in the construction schedule or an unscheduled need to undertake a ground-disturbing activity in the creek corridor, the tribal monitor shall be notified as soon as feasible.

**Exhibits to Conditions of Approval**

**Exhibit A: Equivalent Housing Unit Summary**

**Exhibit B: Oak Knoll Ownership and Maintenance Matrix**

**Exhibit C: Project Phasing Diagram**

**Exhibit D: Phase 1 Diagram**

**Exhibit E: Extension of Barcelona Street**

**Applicant Statement**

I have read and accept responsibility for the Conditions of Approval. I agree to abide by and conform to the Conditions of Approval, as well as to all provisions of the Oakland Planning Code and Oakland Municipal Code pertaining to the project.

\_\_\_\_\_  
Name of Project Applicant

\_\_\_\_\_  
Signature of Project Applicant

\_\_\_\_\_  
Date

# **OAK KNOLL SUPPLEMENTAL EIR**

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## **Standard Conditions of Approval / Mitigation Monitoring and Reporting Program (SCA/MMRP)**

This Standard Conditions of Approval and Mitigation Monitoring and Reporting Program (SCA/MMRP) is based on the findings identified in the Oak Knoll Mixed Use Community Project Supplemental EIR (Oak Knoll SEIR), published April 27, 2017.

This document is in compliance with Section 15097 of the CEQA Guidelines, which requires that the Lead Agency (City of Oakland) “adopt a program for monitoring or reporting on the revisions which it has required in the project and the measures it has imposed to mitigate or avoid significant environmental effects.” The SCA/MMRP lists mitigation measures recommended in the Oak Knoll SEIR. The SCA/MMRP also lists the City’s Standard Conditions of Approval (“SCAs”) identified in the Oak Knoll SEIR to ensure the conditions are implemented and monitored. The SCAs are measures that would minimize potential adverse effects that could result from the project. The SCA/MMRP also identifies the implementation and monitoring requirements for each mitigation measure and SCA.

While not required to mitigate or avoid significant environmental effects, the SCA/MMRP includes implementation and monitoring guidance as “SCA Implementation Measures” identified in the Oak Knoll SEIR and to which the project sponsor has agreed. These are included in the SCA/MMRP for ease of tracking by City staff.

To the extent that there is any inconsistency between any mitigation measures and/or SCAs, the more restrictive conditions shall govern. To the extent any mitigation measure and/or SCA identified in the Oak Knoll SEIR were inadvertently omitted from this SCA/MMRP, they are automatically incorporated herein by reference.

The SCA/MMRP is organized by environmental topic, in the order they are presented in the Oak Knoll Draft SEIR.

- The first column of the table identifies the impact statement (for internal reference throughout the document).
- The second column states in full the mitigation measure and SCA (or SCA Implementation Measure) applicable to each impact. To avoid repetition, while a mitigation measure or SCA (or SCA Implementation Measure) can apply to more than one impact or topic, it is



listed in its entirety only under its primary environmental topic (as indicated in the designator, e.g., “AES” for Aesthetics).

- The SCAs are numbered as identified in the Oak Knoll SEIR, and for convenience, the reference number to the same or similar SCA as listed in the City’s Standard Conditions of Approval document<sup>1</sup> is included in parenthesis, e.g. (#17), for cross-reference purposes.
- The third column identifies the schedule or timing for all implementation activities required for each mitigation measure or SCA (or each SCA Implementation Measure).
- The fourth column specifies the implementation action and responsibility required of the Project applicant(s).
- The fifth column specifies the party(ies) responsible for monitoring or verifying that the actions are implemented, and specifies the monitoring and verification actions.
- The sixth column will identify the date and signature to confirm implementation of each action over time.

The Project sponsor is responsible for compliance with any recommendations identified in City-approved technical reports and all applicable mitigation measures adopted, and with all SCAs set forth herein at its sole cost and expense, unless otherwise expressly provided in a specific mitigation measure, SCA, Oak Knoll conditions of approval (separate document), and subject to the review and approval of the City of Oakland. Overall monitoring and compliance with the SCA/MMRP will be the responsibility of the Bureau of Planning and the Bureau of Building - Zoning Inspections Division. Prior to the issuance of a demolition, grading, and/or construction permit, the Project sponsor shall pay the applicable mitigation and monitoring fee to the City in accordance with the City’s Master Fee Schedule.

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<sup>1</sup> Adopted November 3, 2008, as amended and/or supplemented through April 11, 2017.

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics</b>					
<p><b>Impact AES-1:</b> The proposed Project could adversely affect an existing scenic vista or substantially damage scenic resources within a state or locally designated scenic highway. (Criteria a and b) (Potentially Significant)</p>	<p><b>SCA AES-2:</b> Landscape Plan (#17).</p> <p>a. <b>Landscape Plan Required.</b> <i>Prior to approval of construction-related permit.</i> The project applicant shall submit a final Landscape Plan for City review and approval that is consistent with the approved Landscape Plan. The Landscape Plan shall be included with the set of drawings submitted for the construction-related permit and shall comply with the landscape requirements of chapter 17.124 of the Planning Code.</p>	<p><b>SITEWIDE MASTER PLAN</b></p> <ul style="list-style-type: none"> <li>Prior to approval/issuance of grading permit for the Master Grading Plan, per the Oak Knoll "Final Development Plan: Master Developer Site Improvements" (Master Developer FDP).</li> </ul> <p><b>FDP PROJECTS</b></p> <ul style="list-style-type: none"> <li>Prior to approval/issuance of grading permit for each project FDP.</li> </ul>	<p><b>SITEWIDE MASTER PLAN</b></p> <p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit to the Bureau of Planning a final "Master Landscape Plan" that is consistent with the Master Developer FDP (excepting the Community Center area per New Mitigation Measure CUL-1.5 and certain Eastern Ridge lots per Mitigation Measure AES-1[c]).</li> </ul> <p><b>FDP PROJECTS</b></p> <p><b>Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit to the Bureau of Planning a final Landscape Plan for each project FDP (excepting the Community Center area and certain Eastern Ridge lots, as specified above).</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>Review and approve final landscape plans (sitewide and each project FDP).</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau or Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Verify submittal of the landscape plans with or prior to submittal of grading plans (sitewide and each project FDP).</li> </ul>	
	<p>b. <b>Landscape Installation.</b> <i>Prior to building permit final.</i> The project applicant shall implement the approved Landscape Plan unless a bond, cash deposit, letter of credit, or other equivalent instrument acceptable to the Director of City Planning, is provided. The financial instrument shall equal the greater of \$2,500 or the estimated cost of implementing the Landscape Plan based on a licensed contractor's bid.</p>	<p><b>SITEWIDE MASTER PLAN</b></p> <ul style="list-style-type: none"> <li>Prior to the first building permit for the first project FDP in Phase 1; <b>or</b></li> <li>Prior to approval/issuance of Master Grading Plan permit, if a City-accepted funding instrument is secured by the Master Developer.</li> </ul> <p><b>FDP PROJECTS</b></p> <ul style="list-style-type: none"> <li>Prior to final inspection of building permit for each project FDP, if landscaping is implemented by an FDP Project Applicant; <b>or</b></li> </ul>	<p><b>SITEWIDE MASTER PLAN</b></p> <p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Implement the final Master Landscape Plan per the Master Developer FDP; <b>or</b></li> <li>Present a City-accepted funding instrument.</li> </ul> <p><b>FDP PROJECTS</b></p> <p><b>Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Implement the final Landscape Plan for each project FDP (excepting the Community Center area and the certain Eastern Ridge lots, as specified above); <b>or</b></li> </ul>	<p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Verify that landscape materials are planted and comply with the SCA, the final Master Landscape Plan, and/or each project FDP; <b>or</b></li> <li>Verify that a City-accepted funding instrument is in place and Landscape Plan is implemented by a licensed contractor.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
<b>Impact AES-1 (cont.)</b>		<ul style="list-style-type: none"> <li>Prior to approval/issuance of grading permit for each project FDP, if a City-accepted funding instrument (e.g., bond) is secured by an FDP Project Applicant.</li> </ul>	<ul style="list-style-type: none"> <li>Present a City-accepted funding instrument.</li> </ul>		
	<p>c. <b>Landscape Maintenance. Ongoing.</b> All required planting shall be permanently maintained in good growing condition and, whenever necessary, replaced with new plant materials to ensure continued compliance with applicable landscaping requirements. The property owner shall be responsible for maintaining planting in adjacent public rights-of-way. All required fences, walls, and irrigation systems shall be permanently maintained in good condition and, whenever necessary, repaired or replaced.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Permanently maintain in good condition (or repair/replace as needed) all required plantings and landscape elements identified in the SCA.</li> </ul>	<p><b>City of Oakland, Tree Services Division of the Public Works Agency; Bureau of Building Services - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Verify that planted trees comply with the SCA and/or City arborist recommendations.</li> <li>Verify that required planting and landscape elements identified in the SCA are permanently maintained in good condition.</li> </ul>	
	<p><b>Replacement Mitigation Measure AES-1</b> (modifies and expands 1998 EIS/EIR Mitigation 1):</p> <p><i>Intent.</i> The intent of this Replacement Mitigation Measure AES-1 is to require that new single family homes on the Eastern Ridge that are silhouetted against the sky from public viewing points identified in this Draft SEIR (whether the homes are to be built on graded flat or sloped lots) shall be custom designed utilizing appropriate techniques to minimize the appearance of a monotonous row of skylined development visible from off-site vantage points.</p> <p><i>Requirements.</i> The homes on the Eastern Ridge of the Oak Knoll Project site that will be silhouetted against the sky from off-site public viewing points identified in this Draft SEIR shall be individually designed by architects to meet client needs. The Oak Knoll Design Guidelines, as supplemented by this mitigation measure, require that the</p>	<p><b>FDP Project Involving Eastern Ridge (Admiral's Hill) Lots 1-01-118:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of the first grading permit for Eastern Ridge Lots 101-118 for Phase 2.</li> </ul>	<p><b>FDP Project Applicant of Eastern Ridge Lots 101-118 on the Oak Knoll Vesting Tentative Tract Map:</b></p> <ul style="list-style-type: none"> <li>Submit project FDP showing detailed grading, landscaping, building, and elevation plans per the Replacement Mitigation Measure AES-1(c), for single family homes on Eastern Ridge Lots 101-118.</li> </ul>	<p><b>City of Oakland, Planning Commission; Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>Review and approve project FDP plans for single family homes on Eastern Ridge Lots 101-118 for conformance with approved PDP, Oak Knoll Design Guidelines, and Replacement Mitigation Measure AES-1(c).</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
<b>Impact AES-1 (cont.)</b>	<p>architects meet the community standards while providing flexibility to enable site specific alternatives for each lot. While not each of the specific principles will be applicable to each lot or building, the whole of this mitigation measure in concert with the Oak Knoll Design Guidelines direct the architect to combine form, bulk, scale, texture, and color in a manner which ensures the overall building and site design will meet City standards. The project applicant shall demonstrate alignment with the applicable standards through the Design Review and Final Development Plan (FDP) approval process prior to the issuance of any building permit for these lots.</p> <p>The Preliminary Development Plan (PDP) for the Oak Knoll Project shall incorporate the following measures to be applied at the time of approval of an FDP for single family homes on the Eastern Ridge, which include applicable methods and techniques specified primarily in the <i>Oakland Interim Design Review Manual for One- and Two-Unit Residences</i> (2005). These measures that shall specifically be implemented, as applicable, to each development lot proposed for the custom homes on the Eastern Ridge to ensure careful siting and design of new construction on custom home lots and to require specified landscaping on the lots.</p> <p><b>A. General Site Design</b></p> <p>Design structures to minimize the appearance of an monotonous row of ridgetop development visible from off-site vantage points; clustering structures is one appropriate technique.</p> <p>(1) On sloped lots, use courtyards and other spaces to organize building volumes and create transitions from house to land. Avoid filling up side yards with concrete stairs or paved areas that limit landscape and potential usable space. Maintain openness between structures to the extent feasible given the lot configurations and sizes. Avoid long and high building walls close to side lot lines. Provide sufficient side yard setbacks, especially at the front and rear elevations, to allow plantings between the structures to help the perceived mass.</p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
<b>Impact AES-1 (cont.)</b>	<p>(2) On sloped lots, major shifts in siting from the neighborhood pattern may be warranted to help break-up continuous walls of downslope facades and minimize their collective bulk.</p> <p>(3) On sloped lots, step building massing with terrain. Step or slope rooflines with the terrain and avoid large gables on downslope lots.</p> <p>(4) On sloped lots, position the building on the site to minimize height on the downslope side.</p> <p>(5) At the time of FDP approval for each custom lot created on the Eastern Ridge, designate a "buildable development zone" and a "landscape zone" for each sloped lot which shall be delineated to minimize loss of existing vegetation and ensure existing and new vegetation around and between new structures, except as limited for wildfire risk management.</p> <p><b>B. Building Design</b></p> <p>(1) Where applicable, adhere to all special height restrictions and measuring methods for buildings and retaining walls on sloped lots on the Eastern Ridge, which are established in Sections 17.108.020(B) and 17.09.040 of the Oakland Planning Code, as consistent with the approved PUD for the project, pursuant to Section 17.122.110c.</p> <p>(2) Use materials and colors having naturalistic quality that will blend into the surrounding landscape.</p> <p>(3) Avoid blank or under-designed walls from the street. Use multiple materials and/or detailing to break up walls and make large surfaces seem smaller.</p> <p>(4) On front elevations on upslope lots, emphasize eave lines/roof planes as visually dominant features, group windows horizontally within all planes and at building corners.</p> <p>(5) On sloped lots, break the building into multiple volumes with staggered setbacks to reflect the irregularity of hillside terrain.</p>				

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
<b>Impact AES-1 (cont.)</b>	<p>(6) On sloped lots, place floor levels close to and/or partially inset into grade to avoid or minimize tall skirt walls and other tall support structures. Deemphasize skirt walls where they cannot be avoided, as follows:</p> <ul style="list-style-type: none"> <li>a) Incorporate a strong horizontal molding or cap at the top of the skirt wall;</li> <li>b) Change materials and/or colors at the skirt wall to contrast with primary building volume</li> <li>c) Outwardly taper the skirt wall to create a buttress effect</li> <li>d) Recess skirt wall from the face of the upper floors</li> </ul> <p>(7) On rear elevations on downslope lots, symmetrically organize windows, decks (etc.) within individual building masses and aligned floor-to-floor, and incorporate windows that appear as "punch-outs" with adequate wall space between windows and balcony columns that read as a lighter open frame.</p> <p>(8) Provide strong shadow patterns on downslope elevations.</p> <p>(9) Consider the visual impact on neighborhood appearance and natural in the siting and design of long fences. Fences should not be dominant visual elements on hillsides. Tall fences around the property perimeter are often discouraged.</p> <p>(10) Discourage placement of antennas on roofs.</p> <p><b>C. Landscaping and Open Space</b></p> <p>(1) Maintain ample open space between houses or cluster development to increase open space areas as feasible given lot sizes and configuration to assist in reducing building bulk.</p> <p>(2) Incorporate landscaping that is consistent with the more natural appearing vegetation on the surrounding hills to provide some screening and shade for new buildings.</p>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
Impact AES-1 (cont.)	<p>(3) At the skirt walls, intersperse native species' of trees and/or other landscaping with City-approved, non-native species.</p> <p>(4) Use irregular plant spacing and plant trees in undulating groups to achieve a grove effect. Especially consider native, fire-resistant species such as coast live oaks, etc. Plant shrubs of varying heights and sizes among trees. (Guideline 10.8, text and figure)</p> <p>(5) On sloped portions of lots, either maintain natural topography or use a series of stepped terrace/retaining walls to create grade transition between the street and the houses.</p> <p>(6) Fully landscape all graded surfaces and buffer the structure using quantities of vegetation beyond the basic landscaping requirements of the Oak Knoll Design Guidelines Aim for a natural appearance on graded slopes.</p> <p>(7) Plant feature trees to diffuse building mass.</p> <p>(8) Preference should be given to planting and encouraging the growth of desirable low-combustion plant types found in the area. Contrived, non-native landscaping, such as cactus gardens, extreme plant shaping, etc., are inappropriate. Whenever removal of ordinance protected live trees, especially oaks and oak woodlands, is necessary, they shall be replaced by planting, prior to building occupancy, of trees, elsewhere on the property within view from public vantage points.</p> <p><i>Implementation.</i> Prior to the issuance of any single family residential building permit for the Project, the applicant shall submit FDP project plans that specify "detailed building and landscaping plans and elevations" pursuant to the City's Planned Unit Development (PUD) procedures for review and approval of Final Development Plans (Chapter 17.140 of the Oakland Planning Code), as well as the City's Residential Design Review and approval process (Chapter 17.136 of the Oakland Planning Code). The City Planning Commission will review the FDP and determine whether it conforms to the approved PDP and Oak Knoll Design Guidelines and to these enhanced design measures.</p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
<p><b>Impact AES-2:</b> The Project would not substantially degrade the existing visual character or quality of the site and its surroundings. (Criterion c) (Less than Significant with SCA / Beneficial)</p>	<p><b>SCA AES-1:</b> Graffiti Control (#16). <i>Ongoing</i></p> <ul style="list-style-type: none"> <li>a. During construction and operation of the project, the project applicant shall incorporate best management practices reasonably related to the control of graffiti and/or the mitigation of the impacts of graffiti. Such best management practices may include, without limitation:                             <ul style="list-style-type: none"> <li>i. Installation and maintenance of landscaping to discourage defacement of and/or protect likely graffiti-attracting surfaces.</li> <li>ii. Installation and maintenance of lighting to protect likely graffiti-attracting surfaces.</li> <li>iii. Use of paint with anti-graffiti coating.</li> <li>iv. Incorporation of architectural or design elements or features to discourage graffiti defacement in accordance with the principles of Crime Prevention Through Environmental Design (CPTED).</li> <li>v. Other practices approved by the City to deter, protect, or reduce the potential for graffiti defacement.</li> </ul> </li> <li>b. The project applicant shall remove graffiti by appropriate means within seventy-two (72) hours. Appropriate means include the following:                             <ul style="list-style-type: none"> <li>i. Removal through scrubbing, washing, sanding, and/or scraping (or similar method) without damaging the surface and without discharging wash water or cleaning detergents into the City storm drain system.</li> <li>ii. Covering with new paint to match the color of the surrounding surface.</li> <li>iii. Replacing with new surfacing (with City permits if required).</li> </ul> </li> </ul>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Implement best management practices for graffiti control.</li> </ul>	<p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Conduct periodic site visits to verify compliance with the SCA.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.1 Aesthetics (cont.)</b>					
<p><b>Impact AES-3:</b> The proposed Project would not create a new source of substantial light or glare which would substantially and adversely affect day or nighttime views in the area. (Criterion d) (Less than Significant with SCA)</p>	<p><b>SCA AES-3:</b> Lighting (#18). <i>Prior to building permit final.</i> Proposed new exterior lighting fixtures shall be adequately shielded to a point below the light bulb and reflector to prevent unnecessary glare onto adjacent properties.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Prior to issuance of final electrical or building permits.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit lighting plans to the Bureau of Planning (sitewide and each project FDP) with lighting that complies with the SCA.</li> </ul>	<p><b>City of Oakland, Public Works Agency - Electrical Services Division; Bureau of Planning</b></p> <ul style="list-style-type: none"> <li>• Review and approve final lighting plans (sitewide and each project FDP).</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Verify exterior lighting (sitewide and each project FDP) is installed and operates pursuant to the SCA.</li> </ul>	
<p><b>Impact AES-6:</b> The proposed Project would not result in a significant cumulative aesthetics impact when considering the combined effect of the Project, and past, present, approved, pending, and reasonably foreseeable future projects. (Less than Significant with SCAs)</p>	<p><b>SCA AES-1:</b> Graffiti Control (<i>See under Impact AES-2</i>) <b>SCA AES-2:</b> Landscape Plan (<i>See under Impact AES-1</i>) <b>SCA AES-3:</b> Lighting (<i>See under Impact AES-3</i>)</p>				



## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality</b>					
<p><b>Impact AIR-1:</b> Demolition and construction associated with the Project would not result in average daily emissions that would exceed the City's construction significance thresholds of 54 pounds per day of ROG, NOX, or PM<sub>2.5</sub> or 82 pounds per day of PM<sub>10</sub>. (Criterion a) (Less than Significant with SCA)</p>	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) (#19).</p> <p><i>During Project Construction.</i> The project applicant shall implement all of the following applicable air pollution control measures during construction of the project:</p> <p><i>Basic Controls:</i></p> <ol style="list-style-type: none"> <li>Water all exposed surfaces of active construction areas at least twice daily. Watering should be sufficient to prevent airborne dust from leaving the site. Increased watering frequency may be necessary whenever wind speeds exceed 15 miles per hour. Reclaimed water should be used whenever feasible.</li> <li>Cover all trucks hauling soil, sand, and other loose materials or require all trucks to maintain at least two feet of freeboard (i.e., the minimum required space between the top of the load and the top of the trailer).</li> <li>All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.</li> <li>Pave all roadways, driveways, sidewalks, etc. within one month of site grading or as soon as feasible. In addition, building pads should be laid within one month of grading or as soon as feasible unless seeding or soil binders are used.</li> <li>Enclose, cover, water twice daily, or apply (non-toxic) soil stabilizers to exposed stockpiles (dirt, sand, etc.).</li> <li>Limit vehicle speeds on unpaved roads to 15 miles per hour.</li> <li>Idling times on all diesel-fueled commercial vehicles over 10,000 lbs. shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes (as required by the California airborne toxics control measure Title 13, Section 2485, of the California Code of Regulations). Clear signage to this effect shall be provided for construction workers at all access points.</li> </ol>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout demolition, grading and/or construction.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Require/ensure construction contractor to implement all the applicable measures identified in the SCA.</li> <li>Submit an Oak Knoll Dust Control Program as part of the Construction Management Plan to the Bureau of Building-Zoning Inspections, per <i>Enhanced Controls</i> condition (e).</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>Review and approve Dust Control Program.</li> </ul> <p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular verification of the implementation of dust control measures and equipment and vehicle operation protocols and the Oak Knoll Dust Control Program.</li> <li>Verify that a designated dust control monitor is on-call during construction periods, per <i>Enhanced Controls</i> condition (e).</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Conduct periodic site visits to verify dust control measures and equipment and vehicle operation protocols and the Oak Knoll Dust Control Program are being implemented.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-1 (cont.)</b>	<p>h. Idling times on all diesel-fueled off-road vehicles over 25 horsepower shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes and fleet operators must develop a written policy as required by Title 23, Section 2449, of the California Code of Regulations (“California Air Resources Board Off-Road Diesel Regulations”).</p> <p>i. All construction equipment shall be maintained and properly tuned in accordance with the manufacturer’s specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.</p> <p>j. Portable equipment shall be powered by electricity if available. If electricity is not available, propane or natural gas shall be used if feasible. Diesel engines shall only be used if electricity is not available and it is not feasible to use propane or natural gas.</p> <p><i>Enhanced Controls (applies to projects that involve 114 or mores single family residential units or 240 or more multi-family residential units):</i></p> <p>a. All exposed surfaces shall be watered at a frequency adequate to maintain minimum soil moisture of 12 percent. Moisture content can be verified by lab samples or moisture probe.</p> <p>b. All excavation, grading, and demolition activities shall be suspended when average wind speeds exceed 20 mph.</p> <p>c. Install sandbags or other erosion control measures to prevent silt runoff to public roadways.</p> <p>d. Hydroseed or apply (non-toxic) soil stabilizers to inactive construction areas (previously graded areas inactive for one month or more).</p> <p>e. Designate a person or persons to monitor the dust control program and to order increased watering, as necessary, to prevent transport of dust offsite. Their duties shall include holidays and weekend periods when work may not be in progress.</p>				

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-1 (cont.)</b>	<ul style="list-style-type: none"> <li>f. Install appropriate wind breaks (e.g., trees, fences) on the windward side(s) of actively disturbed areas of the construction site to minimize wind blown dust. Wind breaks must have a maximum 50 percent air porosity.</li> <li>g. Vegetative ground cover (e.g., fast-germinating native grass seed) shall be planted in disturbed areas as soon as possible and watered appropriately until vegetation is established.</li> <li>h. Activities such as excavation, grading, and other ground-disturbing construction activities shall be phased to minimize the amount of disturbed surface area at any one time.</li> <li>i. All trucks and equipment, including tires, shall be washed off prior to leaving the site.</li> <li>j. Site accesses to a distance of 100 feet from the paved road shall be treated with a 6 to 12 inch compacted layer of wood chips, mulch, or gravel.</li> <li>k. All equipment to be used on the construction site and subject to the requirements of Title 13, Section 2449, of the California Code of Regulations ("California Air Resources Board Off-Road Diesel Regulations") must meet emissions and performance requirements one year in advance of any fleet deadlines. Upon request by the City, the project applicant shall provide written documentation that fleet requirements have been met.</li> <li>l. Use low VOC (i.e., ROG) coatings beyond the local requirements (i.e., BAAQMD Regulation 8, Rule 3: Architectural Coatings).</li> <li>m. All construction equipment, diesel trucks, and generators shall be equipped with Best Available Control Technology for emission reductions of NOx and PM.</li> <li>n. Off-road heavy diesel engines shall meet the California Air Resources Board's most recent certification standard.</li> <li>o. Post a publicly-visible large on-site sign that includes the contact name and phone number for the project complaint manager responsible for responding to dust complaints and the telephone numbers of the City's</li> </ul>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-1</b> (cont.)	Code Enforcement unit and the Bay Area Air Quality Management District. When contacted, the project complaint manager shall respond and take corrective action within 48 hours.				
<p><b>Impact AIR-2:</b> Operation of the Project would result in operational average daily emissions of more than 54 pounds per day of ROG, NOX, or PM<sub>2.5</sub> or 82 pounds per day of PM<sub>10</sub>; or result in maximum annual emissions of 10 tons per year of ROG, NOX, or PM<sub>2.5</sub> or 15 tons per year of PM<sub>10</sub>. (Criterion b) (Significant and Unavoidable)</p>	<p><b>SCA TRA-4:</b> Parking and Transportation Demand Management (<i>See under Section 4.13, Transportation and Traffic - Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation</i>)</p> <p><b>SCA GHG-1:</b> Greenhouse Gas (GHG) Reduction Plan (GGRP) (<i>See under Impact GHG-1</i>)</p> <p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape (<i>See under Impact GHG-1</i>)</p>				
	<p><b>New Mitigation Measure AIR-2.1: Use Low and Super-compliant VOC Architectural Coatings in Maintaining Buildings through CC&amp;Rs.</b> While Regulation 8 Rule 3 of the BAAQMD places limits on the VOC content of paint and other architectural coatings, use of lower VOC coatings available to consumers can further reduce operational ROG emissions. Low- and Super-Compliant VOC paints are manufactured and sold by numerous companies. "Low-VOC" refers to paints that meet the more stringent regulatory limits in South Coast AQMD Rule 1113; however, many manufacturers have reformulated to levels well below these limits. These are referred to as "Super-Compliant" Architectural Coatings (<a href="http://www.aqmd.gov/home/regulations/compliance/architectural-coatings/super-compliant-coatings">http://www.aqmd.gov/home/regulations/compliance/architectural-coatings/super-compliant-coatings</a>).</p>	<p><b>FDP PROJECTS</b></p> <p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of first building permit for each project FDP.</li> </ul>	<p><b>Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit FDP plans that specify proposed use of Low- and Super-Compliant VOC paints for all architectural structures.</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>Verify inclusion of Low- and Super-Compliant VOC paints in plans for each project FDP.</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Verify use of Low- and Super-Compliant VOC paints.</li> </ul>	
	<p><b>New Mitigation Measure AIR-2.2: Promote use of Green Consumer Products.</b> To reduce ROG emissions associated with the project, the project sponsor and/or future developer(s) shall provide education for residential and commercial tenants concerning green consumer products. Prior to receipt of any certificate of final occupancy and every five years thereafter, the project sponsor and/or future developer(s) shall work with the City of Oakland to develop electronic correspondence to be distributed by email annually to residential and/or commercial tenants of each building on the project site that encourages the purchase of consumer products that generate lower than typical VOC emissions. The</p>	<p><b>FDP PROJECTS</b></p> <p><b>Plan Preparation:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of certificate of occupancy for each project FDP.</li> </ul> <p><b>Distribution of Plan and Information:</b></p> <ul style="list-style-type: none"> <li>Include with all sale and/or lease information and distributed upon sale or lease of each building.</li> </ul>	<p><b>Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit to the Bureau of Planning FDP plans that specify proposed distribution materials about green consumer products.</li> <li>Work with Bureau of Planning staff to develop annual electronic correspondence to be distributed regarding environmentally preferable</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>Review/verify green consumer products public education materials</li> <li>Confirm annual emailing of public education materials to residential and commercial tenants regarding Green Consumer Products.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-2 (cont.)</b>	correspondence shall encourage environmentally preferable purchasing.	<b>Re-coordination with City on Correspondence:</b> <ul style="list-style-type: none"> <li>• Every five years after date of each certificate of occupancy.</li> </ul>	purchasing. Confirm logistics for email distribution and proof of compliance with the SCA.		
<b>Impact AIR-4:</b> Construction and operation of the Project would not generate substantial levels of toxic air contaminants (TACs). (Criterion d) (Less than Significant with SCAs)	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) (<i>See under Impact AIR-1</i>)</p> <p><b>SCA AIR-2:</b> Exposure to Air Pollution (Toxic Air Contaminants) (#20).</p> <p><b>a. Health Risk Reduction Measures. Prior to approval of construction-related permit.</b> The project applicant shall incorporate appropriate measures into the project design in order to reduce the potential health risk due to exposure to toxic air contaminants. The project applicant shall choose <u>one</u> of the following methods:</p> <p>i. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with California Air Resources Board (CARB) and Office of Environmental Health and Hazard Assessment requirements to determine the health risk of exposure of project residents/occupants/users to air pollutants. The HRA shall be submitted to the City for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then health risk reduction measures are not required. If the HRA concludes that the health risk exceeds acceptable levels, health risk reduction measures shall be identified</p> <p>Identified risk reduction measures shall be to reduce the health risk to acceptable levels. submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City.</p> <p>- or -</p>	<p><b>Certification Statement and On-site Requirements:</b></p> <ul style="list-style-type: none"> <li>• Prior to start of demolition, grading, or construction activities.</li> </ul> <p><b>Compliance Reporting:</b></p> <ul style="list-style-type: none"> <li>• Quarterly throughout construction activities.</li> </ul> <p><b>Plan Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout demolition, grading, and/or construction</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Require/ensure construction contractor to implement all the applicable measures identified in the SCA as part of the Construction Management Plan to the Bureau of Building-Zoning Inspections.</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>• Review Construction Emissions Minimization Plan, Compliance Certification Statement, and On-site Requirements specified in the HRA and SCA, to verify that appropriate methods to ensure NOx emissions below the BAAQMD threshold of significance are implemented.</li> <li>• Review and approve quarterly reports in compliance with the Plan.</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Verify construction operations in compliance with the implementation actions #1 through #4, including the Construction Emissions Minimization Plan.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-4 (cont.)</b>	<p>ii. The project applicant shall incorporate the following health risk reduction measures into the project. These features shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City:</p> <ul style="list-style-type: none"> <li>– Installation of air filtration to reduce cancer risks and Particulate Matter (PM) exposure for residents and other sensitive populations in the project that are in close proximity to sources of air pollution. Air filter devices shall be rated MERV-13 or higher. As part of implementing this measure, an ongoing maintenance plan for the building's HVAC air filtration system shall be required.</li> <li>– Where appropriate, install passive electrostatic filtering systems, especially those with low air velocities (i.e., 1 mph).</li> <li>– Phasing of residential developments when proposed within 500 feet of freeways such that homes nearest the freeway are built last, if feasible.</li> <li>– The project shall be designed to locate sensitive receptors as far away as feasible from the source(s) of air pollution. Operable windows, balconies, and building air intakes shall be located as far away from these sources as feasible. If near a distribution center, residents shall be located as far away as feasible from a loading dock or where trucks concentrate to deliver goods.</li> <li>– Sensitive receptors shall be located on the upper floors of buildings, if feasible.</li> <li>– Planting trees and/or vegetation between sensitive receptors and pollution source, if feasible. Trees that are best suited to trapping PM shall be planted, including one or more of the following: Pine (<i>Pinus nigra</i> var. <i>maritima</i>), Cypress (<i>X Cupressocyparis leylandii</i>), Hybrid poplar (<i>Populus deltoids X trichocarpa</i>), and Redwood (<i>Sequoia sempervirens</i>).</li> </ul>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-4 (cont.)</b>	<ul style="list-style-type: none"> <li>- Sensitive receptors shall be located as far away from truck activity areas, such as loading docks and delivery areas, as feasible.</li> <li>- Existing and new diesel generators shall meet CARB's Tier 4 emission standards, if feasible.</li> <li>- Emissions from diesel trucks shall be reduced through implementing the following measures, if feasible:                             <ul style="list-style-type: none"> <li>▪ Installing electrical hook-ups for diesel trucks at loading docks.</li> <li>▪ Requiring trucks to use Transportation Refrigeration Units (TRU) that meet Tier 4 emission standards.</li> <li>▪ Requiring truck-intensive projects to use advanced exhaust technology (e.g., hybrid) or alternative fuels.</li> <li>▪ Prohibiting trucks from idling for more than two minutes.</li> <li>▪ Establishing truck routes to avoid sensitive receptors in the project. A truck route program, along with truck calming, parking, and delivery restrictions, shall be implemented.</li> </ul> </li> </ul>				
	<p><b>b. Maintenance of Health Risk Reduction Measures. Ongoing.</b> The project applicant shall maintain, repair, and/or replace installed health risk reduction measures, including but not limited to the HVAC system (if applicable), on an ongoing and as-needed basis. Prior to occupancy, the project applicant shall prepare and then distribute to the building manager/operator an operation and maintenance manual for the HVAC system and filter including the maintenance and replacement schedule for the filter.</p>	<p><b>Maintenance:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul> <p><b>Distribute Manual and Replacement Schedule:</b></p> <ul style="list-style-type: none"> <li>• Include with all sale and/or lease information and distributed upon sale or lease of each building and every 3 years thereafter.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Maintain, repair, and/or replace installed health risk reduction measures.</li> <li>• Distribute filter manual and filter replacement schedule to the building manager/operator of building with an HVAC system.</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>• Ensure distribution of HVAC system and filter manual, and filter replacement schedule.</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Verify installed health risk reduction measures are maintained/replaced when necessary.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<p><b>Impact AIR-4 (cont.)</b></p>	<p><i>The proposed Project does not include stationary sources of TACs. The following SCA will apply if the Project operations change to include such sources.</i></p> <p><b>SCA AIR-3:</b> Stationary Sources of Air Pollution (Toxic Air Contaminants) (#21).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall incorporate appropriate measures into the project design in order to reduce the potential health risk due to on-site stationary sources of toxic air contaminants. The project applicant shall choose one of the following methods:</p> <p>a. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with California Air Resources Board (CARB) and Office of Environmental Health and Hazard Assessment requirements to determine the health risk associated with proposed stationary sources of pollution in the project. The HRA shall be submitted to the City for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then health risk reduction measures are not required. If the HRA concludes the health risk exceeds acceptable levels, health risk reduction measures shall be identified to reduce the health risk to acceptable levels. Identified risk reduction measures shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City.</p> <p>- or -</p> <p>b. The project applicant shall incorporate the following health risk reduction measures into the project. These features shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City:</p> <p>i. Installation of non-diesel fueled generators, if feasible, or;</p>	<p><b>Design/Health Risk Reduction Measures Incorporated into Project or Health Risk Assessment:</b></p> <ul style="list-style-type: none"> <li>• Prior to start of construction activities.</li> </ul> <p><b>Compliance Reporting:</b></p> <ul style="list-style-type: none"> <li>• Regularly (quarterly) throughout construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit FDP plans that specify the applicable measures identified in the SCA as part of the Construction Management Plan to the Bureau of Building-Zoning Inspections.</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>• Review HRA or design measures as specified in the SCA, to verify that appropriate measures are implemented.</li> <li>• Review and approve compliance.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.2 Air Quality (cont.)</b>					
<b>Impact AIR-4 (cont.)</b>	ii. Installation of diesel generators with an EPA-certified Tier 4 engine or engines that are retrofitted with a CARB Level 3 Verified Diesel Emissions Control Strategy, if feasible.				
<b>Impact AIR-5:</b> Construction of the Project would not expose proposed sensitive receptors to substantial levels of toxic air contaminants (TACs). (Criterion e) (Less than Significant with SCAs)	<b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) ( <i>See under Impact AIR-1</i> )  <b>SCA AIR-2:</b> Exposure to Air Pollution (Toxic Air Contaminants) ( <i>See under Impact AIR-4</i> )				
<b>4.3 Biological Resources</b>					
<b>Impact BIO-1:</b> The Project could have a substantial adverse effect, either directly or through habitat modifications, on any plant or animal species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service. (Criterion a) (Potentially Significant).	<b>SCA BIO-1:</b> Tree Removal during Breeding Bird Season (#26). <i>Prior to removal of trees.</i> To the extent feasible, removal of any tree and/or other vegetation suitable for nesting of birds shall not occur during the bird breeding season of February 1 to August 15 (or during December 15 to August 15 for trees located in or near marsh, wetland, or aquatic habitats). If tree removal must occur during the bird breeding season, all trees to be removed shall be surveyed by a qualified biologist to verify the presence or absence of nesting raptors or other birds. Pre-removal surveys shall be conducted within 15 days prior to the start of work and shall be submitted to the City for review and approval. If the survey indicates the potential presence of nesting raptors or other birds, the biologist shall determine an appropriately sized buffer around the nest in which no work will be allowed until the young have successfully fledged. The size of the nest buffer will be determined by the biologist in consultation with the California Department of Fish and Wildlife, and will be based to a large extent on the nesting species and its sensitivity to disturbance. In general, buffer sizes of 200 feet for raptors and 50 feet for other birds should suffice to prevent disturbance to birds nesting in the urban environment, but these buffers may be increased	<b>Conduct Pre-Removal Surveys:</b>  • Within 15 days prior to removal of any trees and/or other vegetation suitable for nesting of birds.  <b>Agency Consultation for Nesting Raptors/Birds:</b>  • Prior to the start of work involving ground disturbance or building dismantling, relocation or demolition.	<b>Master Developer and Each FDP Project Applicant:</b>  • Conduct pre-removal surveys by a qualified biologist if work occurs during the bird breeding season.  • Submit pre-removal surveys to City of Oakland.	<b>City of Oakland, Bureau of Planning; Bureau of Building - Zoning Inspections:</b>  • Review and accept pre-removal surveys.  <b>Applicant:</b>  • Ensure regular verification of the implementation of this SCA during breeding season.  <b>City of Oakland, Bureau of Building - Zoning Inspections; qualified biologist approved by the Bureau of Planning:</b>  • Conduct periodic site visits during bird breeding season to verify compliance per the SCA.	



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	or decreased, as appropriate, depending on the bird species and the level of disturbance anticipated near the nest.			<p><b>California Department of Fish and Wildlife (CDFW); qualified biologist approved by the Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>• If pre-removal surveys indicate the potential presence of nesting raptors or other birds, consult with qualified biologist on size of nest buffer.</li> </ul>	
	<p><b>SCA BIO-2: Bird Collision Reduction Measures (#25).</b></p> <p><i>Prior to approval of a construction-related permit. The following measures apply to all construction projects which include glass as part of the building's exterior AND at least one of the following: a) The project is located immediately adjacent to a substantial water body larger than 1 acre (i.e. Oakland Estuary, San Francisco Bay, Lake Merritt or other lake, reservoir or wetland). OR b) The project is located immediately adjacent to a substantial recreation area or park (i.e. a region-serving park, resource conservation area, neighborhood park, linear park, or special use park and generally over 1 acre in size) which contains substantial vegetation. OR c) The project includes substantial vegetated or green roof or green wall (roof or wall with growing medium and plants taking the place of conventional roofing such as asphalt, tile, gravel or shingles) but excluding container gardens OR d) The project includes an existing or proposed substantial vegetated area (generally contiguous one acre in size or larger) located directly adjacent to project buildings. e) The structure contains an atrium which will contain vegetation.</i></p> <p>The project applicant shall submit a Bird Collision Reduction Plan for City review and approval to reduce potential bird collisions to the maximum feasible extent. The Plan shall include all of the following mandatory measures, as well as applicable and specific project Best</p>	<p><b>Submit Bird Collision Reduction Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to issuance of first building permit for each project FDP.</li> </ul> <p><b>Implement Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout Project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit Oak Knoll Bird Collision Reduction Plan (Plan).</li> <li>• Incorporate measures into Project building and landscape plans for implementation.</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>• Review and approve the Plan.</li> </ul> <p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>• Ensure regular verification of compliance with the Plan.</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Conduct periodic site visits to verify compliance with the Plan.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	<p>Management Practice (BMP) strategies to reduce bird strike impacts to the maximum feasible extent. The project applicant shall implement the approved Plan. Mandatory measures include all of the following:</p> <ul style="list-style-type: none"> <li>i. For large buildings subject to federal aviation safety regulations, install minimum intensity white strobe lighting with three second flash instead of solid red or rotating lights.</li> <li>ii. Minimize the number of and co-locate rooftop-antennas and other rooftop structures.</li> <li>iii. Monopole structures or antennas shall not include guy wires.</li> <li>iv. Avoid the use of mirrors in landscape design.</li> <li>v. Avoid placement of bird-friendly attractants (i.e., landscaped areas, vegetated roofs, water features) near glass unless shielded by architectural features taller than the attractant that incorporate bird friendly treatments no more than two inches horizontally, four inches vertically, or both (the “two-by-four” rule), as explained below.</li> <li>vi. Apply bird-friendly glazing treatments to no less than 90 percent of all windows and glass between the ground and 60 feet above ground or to the height of existing adjacent landscape or the height of the proposed landscape. Examples of bird-friendly glazing treatments include the following: <ul style="list-style-type: none"> <li>• Use opaque glass in window panes instead of reflective glass.</li> <li>• Uniformly cover the interior or exterior of clear glass surface with patterns (e.g., dots, stripes, decals, images, abstract patterns). Patterns can be etched, fritted, or on films and shall have a density of no more than two inches horizontally, four inches vertically, or both (the “two-by-four” rule).</li> </ul> </li> </ul>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	<ul style="list-style-type: none"> <li>• Install paned glass with fenestration patterns with vertical and horizontal mullions no more than two inches horizontally, four inches vertically, or both (the “two-by-four” rule).</li> <li>• Install external screens over non-reflective glass (as close to the glass as possible) for birds to perceive windows as solid objects.</li> <li>• Install UV-pattern reflective glass, laminated glass with a patterned UV-reflective coating, or UV-absorbing and UV-reflecting film on the glass since most birds can see ultraviolet light, which is invisible to humans.</li> <li>• Install decorative grilles, screens, netting, or louvers, with openings no more than two inches horizontally, four inches vertically, or both (the “two-by-four” rule).</li> <li>• Install awnings, overhangs, sunshades, or light shelves directly adjacent to clear glass which is recessed on all sides.</li> <li>• Install opaque window film or window film with a pattern/design which also adheres to the “two-by-four” rule for coverage.</li> </ul> <p>vii. Reduce light pollution. Examples include the following:</p> <ul style="list-style-type: none"> <li>• Extinguish night-time architectural illumination treatments during bird migration season (February 15 to May 15 and August 15 to November 30).</li> <li>• Install time switch control devices or occupancy sensors on non-emergency interior lights that can be programmed to turn off during non-work hours and between 11:00 p.m. and sunrise.</li> <li>• Reduce perimeter lighting whenever possible.</li> <li>• Install full cut-off, shielded, or directional lighting to minimize light spillage, glare, or light trespass.</li> <li>• Do not use beams of lights during the spring (February 15 to May 15) or fall (August 15 to November 30) migration.</li> </ul>				



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	<p>viii. Develop and implement a building operation and management manual that promotes bird safety. Example measures in the manual include the following:</p> <ul style="list-style-type: none"> <li>• Donation of discovered dead bird specimens to an authorized bird conservation organization or museums (e.g., UC Berkeley Museum of Vertebrate Zoology) to aid in species identification and to benefit scientific study, as per all federal, state and local laws.</li> <li>• Distribution of educational materials on bird-safe practices for the building occupants. Contact Golden Gate Audubon Society or American Bird Conservancy for materials.</li> <li>• Asking employees to turn off task lighting at their work stations and draw office blinds, shades, curtains, or other window coverings at end of work day.</li> <li>• Install interior blinds, shades, or other window coverings in windows above the ground floor visible from the exterior as part of the construction contract, lease agreement, or CC&amp;Rs.</li> <li>• Schedule nightly maintenance during the day or to conclude before 11 p.m., if possible.</li> </ul>				
	<p><b>Special-Status Animal Species</b></p> <p><b>SCA Implementation Measure BIO-1.1:</b> To further implement SCA BIO-1 during construction, to the extent feasible, grading and building or structure relocation or demolition (i.e., Club Knoll Garage) shall not occur during the bird breeding season of February 1 to August 15. If such activities must occur during the bird breeding season, areas where ground disturbance or building relocation or demolition will occur shall be surveyed by a qualified biologist to verify the presence or absence of nesting raptors or other birds.</p> <p>Surveys shall be conducted within 15 days prior to the start of work and shall be submitted to the City for review and approval. If the survey indicates the potential presence of nesting raptors or other birds protected under federal or state regulations, the biologist, in</p>	<p><b>Conduct Pre-Construction Surveys:</b></p> <ul style="list-style-type: none"> <li>• Within 15 days prior to the start of ground disturbance or building relocation or demolition/dismantling.</li> </ul> <p><b>Agency Consultation for Nesting Raptors/Birds (protected under federal or state regulations):</b></p> <ul style="list-style-type: none"> <li>• Prior to the start of ground disturbance or building relocation or demolition/dismantling.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Conduct pre-construction surveys by a qualified biologist if work occurs during the bird breeding season.</li> <li>• Submit pre-construction surveys to City of Oakland.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>• Ensure regular verification of the implementation during bird breeding season and verify compliance per the SCA Implementation Measure.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Review and approve pre-construction surveys</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	consultation with the City, shall determine an appropriately sized buffer around the nest in which no work will be allowed to ensure no significant impacts and will maintain that buffer until the young have successfully fledged. The size of the nest buffer will be based on the nesting species and its sensitivity to disturbance. In general, buffer sizes of 200 feet for raptors and 50 feet for other birds should suffice to prevent disturbance to birds nesting in the urban environment.	<p><b>Conduct Work Per Survey Results:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout ground disturbance or building relocation or demolition/dismantling.</li> </ul>		<ul style="list-style-type: none"> <li>Conduct periodic site visits during bird breeding season to verify compliance per the SCA Implementation Measure.</li> </ul> <p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>If pre-construction surveys indicate the potential presence of nesting raptors or other birds, consult with qualified biologist on size of nest buffer.</li> </ul>	
	<p><b>New Mitigation Measure BIO-1.1:</b> A preconstruction habitat assessment for special-status bats shall be conducted by a qualified biologist in advance of tree removal and building demolition within the Project site to characterize potential bat habitat and identify potentially active roost sites. Should the preconstruction survey find no bat habitat or potential bat roosting sites then no further action is required. Should potential roosting habitat or active bat roosts be found in trees to be removed or buildings to be relocated or demolished (i.e. Club Knoll Garage) under the project, the Project sponsor shall implement avoidance and minimization measures. Bats utilize trees and buildings differently depending on the species and the time of year. Tree and building specific measures are outlined below. These measures include the following, subject to modification and augmentation by the terms of applicable permits issued by the CDFW:</p> <p>a) To avoid impacts to tree roosting bats, trees and snags should be removed between October 1 and March 31, which is outside of the maternity roosting season, when female bats aggregate to give birth and raise their young.</p> <p>b) If tree removal must occur between April 1 and September 30, and the bat roost habitat assessment identified suitable or potentially occupied roosts within the Project Area, a preconstruction bat survey</p>	<p><b>Conduct Pre-Construction Bat Habitat Surveys:</b></p> <ul style="list-style-type: none"> <li>Prior to any tree removal or building demolition/ dismantling; or</li> <li>At least 14 days prior to any tree removal and building demolition, if activities must occur between April 1 and September 30), and the bat roost habitat assessment identified suitable or potentially occupied roosts within the Project Area.</li> </ul> <p><b>Agency Consultation for Avoidance and Protection Measures (if special-status bat species or maternity roosts are detected):</b></p> <ul style="list-style-type: none"> <li>Prior to the start of work involving ground disturbance or building relocation or demolition/dismantling.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Conduct preconstruction habitat surveys (assessments) for special-status bats.</li> <li>Ensure a qualified biologist conducts the surveys.</li> <li>Implement avoidance and minimization measures according to the survey results.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular verification of compliance with avoidance and minimization measures per New Mitigation Measures BIO-1.1.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building-Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Review and approve pre-construction surveys.</li> <li>Conduct periodic site visits to verify compliance with avoidance and minimization measures per New Mitigation Measures BIO-1.1.</li> <li>Verify qualified biologist is present during activities specified in New Mitigation Measures BIO-1.1.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-1 (cont.)</b></p>	<p>should be performed by a qualified bat biologist no more than 14 days prior to tree removal to determine if potential roost structures are occupied. Surveys may include acoustic monitoring to identify species within suspected roost sites. If special-status bat species or maternity roosts are detected during these surveys, appropriate species and roost specific avoidance and protection measures will be developed in consultation with CDFW. Such measures may include postponing the removal of trees or snags until the end of the maternity roosting season, implementing exclusionary work buffers, or other compensatory mitigation.</p> <p>c) Removal of trees or snags with potential bat roosting habitat or active bat roost sites shall occur only when no rain is forecast for three days, when daytime temperatures are at least 50°F, and shall follow a two-step removal process:</p> <ol style="list-style-type: none"> <li>i. On the first day of tree removal and under supervision of the qualified biologist, branches and limbs not containing cavities or fissures in which bats could roost, shall be cut only using chainsaws.</li> <li>ii. On the following day and under the supervision of the qualified biologist, the remainder of the tree may be removed, either using chainsaws or other equipment (e.g. excavator or backhoe).</li> <li>iii. All felled trees should remain on the ground for at least 24 hours prior to chipping, off-site removal, or other processing to allow any bats to escape.</li> </ol> <p>d) Irrespective of the time of year, all buildings or structures should be surveyed for active bat roosts or signs of roosting (guano, urine staining, dead bats) by a qualified bat biologist no more than 14 days prior to removal to determine if the building or structure is used for roosting. If evidence of roosting is present, the qualified bat biologist will determine, if possible, the type of roost and species. If special-status bat species or maternity or hibernation roosts</p>	<p><b>Conduct Work Per Survey Results:</b></p> <p>Ongoing, throughout work involving ground disturbance or building relocation or demolition/dismantling.</p>		<p><b>City of Oakland, Bureau of Planning; CDFW:</b></p> <ul style="list-style-type: none"> <li>• If surveys indicate special-status bat species or maternity roosts, consult with qualified biologist specific avoidance and protection measures per measure (b).</li> <li>• If surveys identify active bat roosts on or in the immediate vicinity of where tree removal and building demolition is planned, consult with qualified biologist to establish disturbance buffer per measure (e).</li> </ul> <p>Verify a permitted bat biologist performs any roost exclusions per measure (g).</p>	



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<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	<p>are detected during these surveys, appropriate species and roost specific avoidance and protection measures will be developed in consultation with CDFW. Such measures may include postponing the removal of buildings or structures, exclusionary work buffers, or other compensatory mitigation.</p> <p>e) If surveys identify active bat roosts are found on or in the immediate vicinity of the Project site where tree removal and building demolition is planned, a no disturbance buffer shall be established around these roost sites until they are determined to be no longer active by the qualified bat biologist. The size of the no disturbance buffer would be determined by the qualified bat biologist in conjunction with CDFW and would depend on existing screening around the roost site (such as dense vegetation or a building), the roost type, species present, as well as the type of construction activity which would occur around the roost site.</p> <p>f) The qualified biologist shall be present during tree removal and building relocation or demolition if potential bat roosting habitat is present or if such work is to occur in the vicinity of any identified active bat roosts.</p> <p>g) Relocation or demolition of buildings containing or suspected to contain potential bat roosting habitat or active bat roosts shall be dismantled under the supervision of the qualified bat biologist. If relocation or demolition of buildings containing active non-maternity roosting bats is necessary, a permitted bat biologist will perform a roost exclusion by installation of one-way exits and modification of the roost to render it unsuitable. Under no circumstances will active maternity roosts be disturbed until the roost disbands at the completion of the maternity roosting season.</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-1 (cont.)</b></p>	<p><b>New Mitigation Measure BIO-1.2:</b> A preconstruction survey for San Francisco dusky-footed woodrat middens shall be conducted by a qualified wildlife biologist prior to the start of construction in suitable habitat within the Project site. Middens identified during surveys shall be flagged as a sensitive resource and avoided during construction, if feasible.</p> <p>Should avoidance of woodrat middens within the Project site not be feasible, the Project sponsor, shall consult with CDFW regarding a qualified biologist dismantling of the middens by hand for relocation outside of the Project site. If approved by CDFW, a qualified wildlife biologist shall dismantle only middens within the Project site that would be disturbed by construction activities. If young are encountered during dismantling of the midden, any removed material shall be replaced and a 50-foot no-disturbance buffer shall be established around the active midden. The buffer shall remain until young are weaned and are able to disperse on their own accord (typically for a period of 14 days). All removed midden substrate shall be collected and relocated to suitable woodland habitat outside of the Project footprint. Appropriate personal protective equipment (e.g., respirator, gloves, and Tyvek suit) shall be used while dismantling and relocating woodrat nest material to protect against disease carried by rodents (e.g. hantavirus).</p>	<p><b>Conduct Pre-Construction San Francisco Dusky-footed Woodrat Midden Surveys:</b></p> <ul style="list-style-type: none"> <li>Prior to the start of construction within areas of suitable dusky-footed woodrat habitat.</li> </ul> <p><b>Agency Consultation for Midden Relocation and/or No-Disturbance Buffers:</b></p> <ul style="list-style-type: none"> <li>Prior to dismantling middens or establishing no-disturbance buffers.</li> </ul> <p><b>Conduct Work Per Surveys:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout ground disturbance or construction.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Conduct preconstruction midden surveys for San Francisco dusky-footed woodrat. Ensure a qualified wildlife biologist conducts the surveys.</li> <li>Implement avoidance and dismantling/ relocation activities according to the survey results.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular verification of compliance with avoidance and dismantling/ relocation activities per New Mitigation Measure BIO-1.2.</li> </ul> <p><b>City of Oakland, Bureau of Planning; and Bureau of Building - Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Review and approve pre-construction surveys.</li> <li>Conduct periodic site visits to verify compliance with avoidance and dismantling/ relocation activities per New Mitigation Measure BIO-1.2.</li> </ul> <p><b>City of Oakland, Bureau of Planning; CDFW:</b></p> <ul style="list-style-type: none"> <li>If surveys indicate special-status bat species or maternity roosts, consult with qualified biologist specific avoidance and protection measures per measure b.</li> <li>If surveys identify active bat roosts on or in the immediate vicinity of where tree removal and building demolition is planned, consult with qualified biologist to establish disturbance buffer per measure e.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-1 (cont.)</b>	<p><b>SCA Implementation Measure BIO-1.2:</b> To further implement SCA BIO-1, a Project-specific Worker Environmental Awareness Program (WEAP) training shall be developed and implemented by a qualified biologist and attended by all Project construction personnel prior to beginning work onsite. The training could consist of a recorded presentation that could be reused for new personnel throughout the duration of construction. The WEAP training shall generally include but not be limited to the following:</p> <ul style="list-style-type: none"> <li>a) Applicable State and federal laws, environmental regulations, Project permit conditions, and penalties for non-compliance;</li> <li>b) Special-status plant and animal species with potential to occur on or in the vicinity of the Project site, avoidance and protection measures, and a protocol for encountering such species including a communication chain;</li> <li>c) Known sensitive resource areas in the Project site which are to be avoided and/or protected (e.g. tree to be retained under the Project) as well as approved Project work areas;</li> <li>d) Preconstruction surveys and biological monitoring requirements associated with each phase of work and restrictions for working nearby sensitive resources within the Project site; and</li> <li>e) Best Management Practices (BMPs) and their location on the Project site for erosion control, pursuant to SCA HYD-1 (Erosion and Sedimentation Control Plan for Construction).</li> </ul>	<p><b>Submit WEAP Training:</b></p> <ul style="list-style-type: none"> <li>• Prior to start of construction activities.</li> </ul> <p><b>Implement Approved WEAP Training:</b></p> <ul style="list-style-type: none"> <li>• Prior to start of any construction activity and ongoing.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Develop and submit a WEAP training. Ensure a qualified biologist develops and implements the WEAP training.</li> <li>• Specify location of BMPs on the Erosion and Sedimentation Control Plan for Construction (SCA HYD-1).</li> </ul>	<p>Verify qualified wildlife biologist performs surveys and dismantling/ relocation activities per Mitigation Measures BIO-1.1.</p> <p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>• Review and approve WEAP.</li> </ul> <p><b>City of Oakland, Bureau of Building - Zoning Inspections</b></p> <ul style="list-style-type: none"> <li>• Verify implementation of WEAP training.</li> </ul>	



## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-2:</b> The Project could have a substantial adverse effect on riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service. (Criterion b) (Potentially Significant)</p>	<p><b>SCA BIO-3:</b> Creek Protection Plan (#54).</p> <p>a. <b>Creek Protection Plan Required.</b> Prior to approval of construction-related permit. The project applicant shall submit a Creek Protection Plan for review and approval by the City. The Plan shall be included with the set of project drawings submitted to the City for site improvements and shall incorporate the contents required under section 13.16.150 of the Oakland Municipal Code including Best Management Practices (“BMPs”) during construction and after construction to protect the creek. Required BMPs are identified below in sections (b), (c), and (d).</p> <p>b. <b>Construction BMPs.</b> Prior to approval of construction-related permit. The Creek Protection Plan shall incorporate all applicable erosion, sedimentation, debris, and pollution control BMPs to protect the creek during construction. The measures shall include, but are not limited to, the following:</p> <p>i. On sloped properties, the downhill end of the construction area must be protected with silt fencing (such as sandbags, filter fabric, silt curtains, etc.) and hay bales oriented parallel to the contours of the slope (at a constant elevation) to prevent erosion into the creek.</p> <p>ii. The project applicant shall implement mechanical and vegetative measures to reduce erosion and sedimentation, including appropriate seasonal maintenance. One hundred (100) percent degradable erosion control fabric shall be installed on all graded slopes to protect and stabilize the slopes during construction and before permanent vegetation gets established. All graded areas shall be temporarily protected from erosion by seeding with fast growing annual species. All bare slopes must be covered with staked tarps when rain is occurring or is expected.</p> <p>iii. Minimize the removal of natural vegetation or ground cover from the site in order to minimize the potential for erosion and sedimentation problems. Maximize the replanting of the area with native vegetation as soon as possible.</p>	<p><b>SITEWIDE MASTER PLAN – PHASE 1</b></p> <p><b>Submit Creek Protection Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Implement (b) Construction BMPs (ii and xii), and (c) Post-Construction BMPs:</b></p> <ul style="list-style-type: none"> <li>(Same as <i>Monitoring/ Inspections and Written Monitoring Report</i> for condition (e) <i>Creek Protection Plan Implementation</i> below.)</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Develop and submit Creek Protection Plan, incorporating specified BMPs, per sections (b) Construction BMPs, (c) Post-construction BMPs, and (d) Final landscaping details of the SCA.</li> <li>Implement approved mechanical and vegetative measures to reduce erosion and sedimentation, per BMP (b)(ii); and all erosion and sedimentation control measures in strict accordance with Regional Water Quality Control Board (RWQCB) controls, per BMP (b)(xii).</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Public Works Agency – Environmental Services:</b></p> <ul style="list-style-type: none"> <li>Review and approve Creek Protection Plan incorporating specified BMPs, per sections (b) Construction BMPs, (c) Post-construction BMPs, and (d) Final landscaping details of the SCA.</li> <li>Verify effectiveness of erosion and sedimentation control measures. (Also see condition (e) <i>Creek Protection Plan Implementation</i> below, and SCA HYD-1.)</li> <li>Verify consistency of Creek Protection Plan with the Master Developer’s submittal of regulatory permit applications to U.S. Army Corps (Section 404 permit); RWQCB (NPDES permit and 401 permit); and CDFW (Lake/Streambed Alteration Agreement).</li> </ul>	<p><i>Creek Protection Plan</i>, submitted to City 2/24/2016</p>

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-2 (cont.)</b>	<ul style="list-style-type: none"> <li>iv. All work in or near creek channels must be performed with hand tools and by a minimum number of people. Immediately upon completion of this work, soil must be repacked and native vegetation planted. Note: Measure is not feasible due to scale and proposed alterations to the creek channel. The City has made a Finding that the other measures imposed as part of the creek restoration plan and analyzed in the CEQA analysis are equal or better protective measures</li> <li>v. Install filter materials (such as sandbags, filter fabric, etc.) acceptable to the City at the storm drain inlets nearest to the project site prior to the start of the wet weather season (October 15); site dewatering activities; street washing activities; saw cutting asphalt or concrete; and in order to retain any debris flowing into the City storm drain system. Filter materials shall be maintained and/or replaced as necessary to ensure effectiveness and prevent street flooding.</li> <li>vi. Ensure that concrete/granite supply trucks or concrete/plaster finishing operations do not discharge wash water into the creek, street gutters, or storm drains.</li> <li>vii. Direct and locate tool and equipment cleaning so that wash water does not discharge into the creek.</li> <li>viii. Create a contained and covered area on the site for storage of bags of cement, paints, flammables, oils, fertilizers, pesticides, or any other materials used on the project site that have the potential for being discharged to the creek or storm drain system by the wind or in the event of a material spill. No hazardous waste material shall be stored on site.</li> <li>ix. Gather all construction debris on a regular basis and place it in a dumpster or other container which is emptied or removed at least on a weekly basis. When appropriate, use tarps on the ground to collect fallen debris or splatters that could contribute to stormwater pollution.</li> </ul>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-2 (cont.)</b>	<ul style="list-style-type: none"> <li>x. Remove all dirt, gravel, refuse, and green waste from the sidewalk, street pavement, and storm drain system adjoining the project site. During wet weather, avoid driving vehicles off paved areas and other outdoor work.</li> <li>xi. Broom sweep the street pavement adjoining the project site on a daily basis. Caked-on mud or dirt shall be scraped from these areas before sweeping. At the end of each workday, the entire site must be cleaned and secured against potential erosion, dumping, or discharge to the creek, street, gutter, or storm drains.</li> <li>xii. All erosion and sedimentation control measures implemented during construction activities, as well as construction site and materials management shall be in strict accordance with the control standards listed in the latest edition of the Erosion and Sediment Control Field Manual published by the Regional Water Quality Control Board (RWQCB).</li> <li>xiii. Temporary fencing is required for sites without existing fencing between the creek and the construction site and shall be placed along the side adjacent to construction (or both sides of the creek if applicable) at the maximum practical distance from the creek centerline. This area shall not be disturbed during construction without prior approval of the City.</li> <li>c. <b>Post-Construction BMPs.</b> <i>Prior to approval of construction-related permit.</i> The project shall not result in a substantial increase in stormwater runoff volume or velocity to the creek or storm drains. The Creek Protection Plan shall include site design measures to reduce the amount of impervious surface to maximum extent practicable. New drain outfalls shall include energy dissipation to slow the velocity of the water at the point of outflow to maximize infiltration and minimize erosion.</li> </ul>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-2 (cont.)</b></p>	<p>d. <b>Creek Landscaping.</b> <i>Prior to approval of construction-related permit.</i> The project applicant shall include final landscaping details for the site on the Creek Protection Plan, or on a Landscape Plan, for review and approval by the City. Landscaping information shall include a planting schedule, detailing plant types and locations, and a system to ensure adequate irrigation of plantings for at least one growing season.</p> <p>Plant and maintain only drought-tolerant plants on the site where appropriate as well as native and riparian plants in and adjacent to riparian corridors. Along the riparian corridor, native plants shall not be disturbed to the maximum extent feasible. Any areas disturbed along the riparian corridor shall be replanted with mature native riparian vegetation and be maintained to ensure survival.</p>				
	<p>e. <b>Creek Protection Plan Implementation.</b> <i>During construction; ongoing.</i> The project applicant shall implement the approved Creek Protection Plan during and after construction. During construction, all erosion, sedimentation, debris, and pollution control measures shall be monitored regularly by the project applicant. The City may require that a qualified consultant (paid for by the project applicant) inspect the control measures and submit a written report of the adequacy of the control measures to the City. If measures are deemed inadequate, the project applicant shall develop and implement additional and more effective measures immediately.</p>	<p><b>SITEWIDE MASTER PLAN – PHASE 1</b></p> <p><b>Implement Creek Protection Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul> <p><b>Monitor/Inspect:</b></p> <ul style="list-style-type: none"> <li>Ongoing, bi-weekly, throughout all construction activities; and if construction occurs during wet weather season (October 15 through April 15) timeframes may change as necessary and determine by the City or other oversight agency, based on findings of the monitoring/ inspections.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Implement approved Creek Protection Plan.</li> <li>Ensure qualified consultant to monitor/inspect and submit written report on adequacy of erosion, sedimentation, debris, and pollution control measures, per BMP (b)(ii) and BMP (b)(xii) in strict accordance with RWQCB controls (also see SCA HYD-1.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular monitoring /inspections by a qualified consultant, to verify compliance with approved Creek Protection Plan and success of the creek protection measures per this condition (e) and BMPs (b), (c) and (d) (see <i>Timing</i>).</li> <li>Ensure additional and/or more effective erosion, sedimentation, debris, and pollution control measures are immediately developed and implemented as needed.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-2 (cont.)</b>		<p><b>Submit Monitoring Report:</b> Monthly, during construction; every three months for one year after construction.</p>		<p><b>City of Oakland, Bureau of Planning; Bureau of Building - Zoning Inspections; Public Works Agency – Environmental Services:</b>  Conduct periodic site visits and/or confirm monitoring /inspections by a qualified consultant, to verify compliance with approved Creek Protection Plan and success of the creek protection measures per this condition (e) and BMPs (b), (c) and (d) (see <i>Timing</i>).</p>	
	<p><b>SCA Implementation Measure BIO-3.1:</b> To further implement SCA BIO-3, buildings adjacent to Powerhouse Creek must be constructed at least 15 feet from the parcel line that is adjacent to the creek, or at least 20 feet from the established top of creek bank. Alternatively, the Project shall set aside a “Building-free Powerhouse Creek Corridor” that is least 80 feet wide for the total length of Powerhouse Creek. The final total length of the altered Power House Creek channel must be equal to or greater than the existing length of creek channel.</p>	<p><b>Submit Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of a construction-related permit for Phase 1 lots adjacent to Powerhouse Creek and/or alteration of Powerhouse Creek channel.</li> </ul> <p><b>Implement Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Phase 1 FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit and implement project FDP showing building-creek buffers, and/or proposed Powerhouse Creek channel alterations, consistent with SCA.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>• Ensure regular verification of compliance with the SCA.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building - Zoning Inspections; Public Works Agency – Environmental Services:</b></p> <ul style="list-style-type: none"> <li>• Review and approve project FDP showing adequate building-creek buffers.</li> <li>• Conduct periodic site visits to verify compliance with the SCA.</li> </ul>	
	<p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (<i>See under Impact HYD-1</i>)</p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-2 (cont.)</b></p>	<p><b>New Mitigation Measure BIO-2:</b> The Project sponsor shall mitigate for temporary disturbance of riparian habitat and oak woodland in support of the Project through restoration or preservation / enhancement of riparian habitat or oak woodland at a ratio of 2:1 (restored/preserved area : impacted area) through one of the following options:</p> <ol style="list-style-type: none"> <li>1. On Site Mitigation                             <ol style="list-style-type: none"> <li>a. Planting replacement trees, and</li> <li>b. Establishing a restrictive covenant or similar instrument to protect existing riparian woodland habitat.</li> </ol> </li> </ol> <p>The Project sponsor shall prepare a Habitat Mitigation and Monitoring Plan (HMMP) for riparian and oak woodland habitat restored under the Project. The HMMP would be subject to approval by the entity with jurisdiction over the restored areas (City of Oakland). The HMMP shall include a detailed description of restoration/enhancement/ preservation actions proposed such as a planting plan, a weed control plan to prevent the spread of invasive and non-native species within restored areas, and erosion control measures to be installed around the restored area following mitigation planting in order to avoid or minimize sediment runoff into the adjacent creeks; restoration performance criteria for each restored area that establish success thresholds over a specific amount of time, as determined by regulatory agencies with jurisdiction of the affected areas; and proposed monitoring/maintenance program to evaluate the restoration performance criteria, under which progress of restored areas are tracked to ensure survival of the mitigation plantings. The program shall document overall health and vigor of mitigation plantings throughout the monitoring period and provide recommendations for adaptive management as needed to ensure the site is successful, according to the established performance criteria. An annual report documenting the results</p>	<p><b>SITEWIDE MASTER PLAN</b></p> <p><b>Submit HMMP or Payment of In-Lieu Fee:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Approved HMMP:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activity.</li> </ul> <p><b>Submit Annual Monitoring Reports:</b></p> <p>Within one year of final inspection of work within riparian and oak woodland habitat, and annually thereafter for up to ten years.</p> <p><b>Implement Recommendations for Periodic Improvements:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, as needed.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Develop and submit HMMP, incorporating replacement tree plantings (1)(a); restrictive covenant or similar to protect existing riparian and oak woodland habitat (1)(b), per measure (1) of the SCA.</li> <li>• Implement approved HMMP; or</li> <li>• Pay and present proof of payment of in-lieu fee of the approved amount to the approved recipient(s).</li> <li>• Prepare and submit annual monitoring reports and, as needed, recommendations for periodic improvement.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Public Works Agency – Environmental Services.</b></p> <p><b>U.S. Army Corps (Section 404 permit); RWQCB (NPDES permit and 401 permit); and CDFW (Lake/Streambed Alteration Agreement):</b></p> <ul style="list-style-type: none"> <li>• Review and approve HMMP, including restoration performance criteria and On Site Mitigations, per measure (1), unless in-lieu fee is elected.</li> </ul> <p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>• If replacement plantings are planned, ensure regular verification of the success of plantings per the HMMP, implementation of periodic recommendations, and all SCA conditions.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Public Works Agency – Environmental Services:</b></p> <ul style="list-style-type: none"> <li>• Verify calculation, amount, payment and recipient(s) of in-lieu fee, per measure (2), if in-lieu fee is elected.</li> </ul>	



## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-2</b> (cont.)	<p>and providing recommendations for improvements throughout the year shall be provided to the City, or</p> <p>2. Paying an in-lieu fee to a natural resource agency or a non-profit organization that would use the fees to protect or enhance oak woodland habitat of the region.</p> <p>If an in-lieu fee is used for mitigation, there must be a direct nexus between the amount of fees paid and mitigation required in terms of oak tree replacement and oak woodland preservation. The amount of the in-lieu fee shall be determined either by calculating the value of the land with oak woodland habitat proposed for removal, or by some other calculation developed by a qualified biologist in collaboration with the City of Oakland. This alternate calculation shall reflect differences in the quality of habitat proposed for removal, and may consider the cost of comparable habitat (fee title or easement) in nearby areas.</p>			<ul style="list-style-type: none"> <li>Review and approve Annual HMMP Monitoring</li> <li>Report and periodic recommendations.</li> </ul> <p>If replacement plantings are planned, conduct periodic site visits to verify success of plantings per the HMMP, implementation of periodic recommendations, and all SCA conditions.</p>	
<b>Impact BIO-3:</b> The Project would not have a substantial adverse effect on federally protected wetlands or other waters (as defined by section 404 of the Clean Water Act) or state protected wetlands or waters, through direct removal, filling, hydrological interruption, or other means. (Criterion c) (Less than Significant with SCAs)	<p><b>SCA BIO-3:</b> Creek Protection Plan (See under Impact BIO-2)</p> <p><b>SCA Implementation Measure BIO-3.1</b> (to further implement SCA BIO-3) (See under Impact BIO-2)</p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (See under Impact HYD-1)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (See under Impact HYD-1)</p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-4:</b> The Project would not substantially interfere with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites. (Criterion d) (Less than Significant with SCAs)</p>	<p><b>SCA BIO-4:</b> Dewatering/Diversion (#55). <i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Dewatering and Diversion Plan for review and approval by the City, and shall implement the approved Plan. The Plan shall comply, at a minimum, with the following:</p> <p>a. All dewatering and diversion activities shall comply with the requirements of all necessary regulatory permits and authorizations from other agencies (e.g., Regional Water Quality Control Board, California Department of Fish and Wildlife, U.S. Fish and Wildlife Service, and Army Corps of Engineers).</p> <p>All native aquatic life (e.g., fish, amphibians, and turtles) within the work site shall be relocated by a qualified biologist prior to dewatering, in accordance with applicable regional, state, and federal requirements. Captured native aquatic life shall be moved to the nearest appropriate site on the stream channel downstream. The biologist shall check daily for stranded aquatic life as the water level in the dewatering area drops. All reasonable efforts shall be made to capture and move all stranded aquatic life observed in the dewatered areas. Capture methods may include fish landing nets, dip nets, buckets, and by hand. Captured aquatic life shall be released immediately in the nearest appropriate downstream site. This condition does not allow the take or disturbance of any state or federally listed species, nor state-listed species of special concern, unless the applicant obtains a project specific authorization from the California Department of Fish and Wildlife and/or the U.S. Fish and Wildlife Service, as applicable.</p> <p>b. If any dam or other artificial obstruction is constructed, maintained, or placed in operation within the stream channel, ensure that sufficient water is allowed to pass down channel at all times to maintain native aquatic life below the dam or other artificial obstruction.</p> <p>c. Construction and operation of dewatering/diversion devices shall meet the standards contained in the</p>	<p><b>SITEWIDE MASTER PLAN – PHASE 1</b></p> <p><b>Submit D&amp;D Plan (as part of Creek Protection Plan, SCA BIO-3[a]):</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Implement D&amp;D Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, during any alterations to, or construction in, the creek channel.</li> </ul> <p><b>Monitor Aquatic Life Movement:</b></p> <ul style="list-style-type: none"> <li>Daily, throughout alterations or construction in the creek channel.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Develop and submit Dewatering and Diversion (D&amp;D) Plan, compliant with regulatory permits and authorizations from other permitting agencies (U.S. Army Corps, RWQCB, U.S. DFW, CDGW).</li> <li>Implement approved D&amp;D Plan, and all RWQCB erosion and sedimentation control standards, per condition (e) (also see SCA HYD-1).</li> <li>Ensure daily monitoring for stranded aquatic life by qualified biologist.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular verification of compliance with the SCA.</li> <li>Ensure verification of daily monitoring by a qualified biologist for stranded aquatic life, and captures/releases per condition (b).</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Public Works Agency – Environmental Services.</b></p> <p><b>U.S. Army Corps; RWQCB; USFWS, CDFW:</b></p> <ul style="list-style-type: none"> <li>Review and approve D&amp;D Plan, in concert with the Creek Protection Plan (SCA BIO-3 [a]).</li> <li>Conduct periodic site visits to verify compliance with the SCA.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-4 (cont.)</b>	<p>latest edition of the Erosion and Sediment Control Field Manual published by the Regional Water Quality Control Board.</p> <p>d. Cofferdams and/or water diversion system shall be constructed of a non-erodible material which will cause little or no siltation. Cofferdams and the water diversion system shall be maintained in place and functional throughout the construction period. If the cofferdams or water diversion systems fail, they shall be repaired immediately based on the recommendations of a qualified environmental consultant. The devices shall be removed after construction is complete and the site is stabilized.</p> <p>e. Pumped water shall be passed through a sediment settling device before returning to the stream channel. Velocity dissipation measures are required at the outfall to prevent erosion.</p>				
<p><b>Impact BIO-5:</b> The Project would not fundamentally conflict with the City of Oakland Tree Protection Ordinance (Oakland Municipal Code (OMC) Chapter 12.36) by removal of protected trees under certain circumstances. (Criterion f) (Less than Significant with SCAs)</p>	<p><b>SCA BIO-5:</b> Tree Permit (#27). <i>Prior to approval of a construction-related permit.</i></p> <p>a. <b>Tree Permit Required.</b> Pursuant to the City's Tree Protection Ordinance (OMC chapter 12.36), the project applicant shall obtain a tree permit and abide by the conditions of that permit.</p> <p>b. <b>Tree Protection During Construction.</b> Adequate protection shall be provided during the construction period for any trees which are to remain standing, including the following, plus any recommendations of an arborist:</p> <p>i. Before the start of any clearing, excavation, construction, or other work on the site, every protected tree deemed to be potentially endangered by said site work shall be securely fenced off at a distance from the base of the tree to be determined by the project's consulting arborist. Such fences shall remain in place for duration of all such work. All trees to be removed shall be clearly marked. A scheme shall be established for the removal and disposal of logs, brush, earth and other debris which will avoid injury to any protected tree.</p>	<p><b>Submit Tree Permit Application:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Tree Permit:</b></p> <ul style="list-style-type: none"> <li>Ongoing, as needed.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit Tree Permit application and proposed tree removal/planting plans.</li> <li>Conduct work, tree removal, and tree replacements pursuant to the approved tree removal/planting plans, the Tree Permit, and the SCA.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular verification of compliance.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building - Zoning Inspections; and Oakland Public Works Agency - Tree Division:</b></p> <ul style="list-style-type: none"> <li>Review and approve Tree Permit application and proposed tree removal/planting plans.</li> <li>Conduct periodic site visits to verify compliance.</li> </ul>	<p>Tree Permit application and proposed tree removal/planting plans submitted to City, dated 10/21/2016 (Tree Removal Impact Mitigation Plan submitted 3/24/2017)</p>



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<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-5 (cont.)</b>	<ul style="list-style-type: none"> <li>ii. Where proposed development or other site work is to encroach upon the protected perimeter of any protected tree, special measures shall be incorporated to allow the roots to breathe and obtain water and nutrients. Any excavation, cutting, filling, or compaction of the existing ground surface within the protected perimeter shall be minimized. No change in existing ground level shall occur within a distance to be determined by the project's consulting arborist from the base of any protected tree at any time. No burning or use of equipment with an open flame shall occur near or within the protected perimeter of any protected tree.</li> <li>iii. No storage or dumping of oil, gas, chemicals, or other substances that may be harmful to trees shall occur within the distance to be determined by the project's consulting arborist from the base of any protected trees, or any other location on the site from which such substances might enter the protected perimeter. No heavy construction equipment or construction materials shall be operated or stored within a distance from the base of any protected trees to be determined by the project's consulting arborist. Wires, ropes, or other devices shall not be attached to any protected tree, except as needed for support of the tree. No sign, other than a tag showing the botanical classification, shall be attached to any protected tree.</li> <li>iv. Periodically during construction, the leaves of protected trees shall be thoroughly sprayed with water to prevent buildup of dust and other pollution that would inhibit leaf transpiration.</li> <li>v. If any damage to a protected tree should occur during or as a result of work on the site, the project applicant shall immediately notify the Public Works Department and the project's consulting arborist shall make a recommendation to the City Tree Reviewer as to whether the damaged tree can be preserved. If, in the professional opinion of the Tree Reviewer, such tree cannot be preserved in a</li> </ul>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

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<b>4.3 Biological Resources (cont.)</b>					
<b>Impact BIO-5 (cont.)</b>	<p>healthy state, the Tree Reviewer shall require replacement of any tree removed with another tree or trees on the same site deemed adequate by the Tree Reviewer to compensate for the loss of the tree that is removed.</p> <p>vi. All debris created as a result of any tree removal work shall be removed by the project applicant from the property within two weeks of debris creation, and such debris shall be properly disposed of by the project applicant in accordance with all applicable laws, ordinances, and regulations.</p> <p>c. <b>Tree Replacement Plantings.</b> Replacement plantings shall be required for tree removals for the purposes of erosion control, groundwater replenishment, visual screening, wildlife habitat, and preventing excessive loss of shade, in accordance with the following criteria:</p> <p>i. For Sequoia sempervirens, three hundred fifteen (315) square feet per tree;</p> <p>ii. For other species listed, seven hundred (700) square feet per tree.</p> <p>iii. In the event that replacement trees are required but cannot be planted due to site constraints, an in lieu fee in accordance with the City's Master Fee Schedule may be substituted for required replacement plantings, with all such revenues applied toward tree planting in city parks, streets and medians.</p> <p>iv. The project applicant shall install the plantings and maintain the plantings until established. The Tree Reviewer of the Tree Division of the Public Works Department may require a landscape plan showing the replacement plantings and the method of irrigation. Any replacement plantings which fail to become established within one year of planting shall be replanted at the project applicant's expense.</p>				
	<b>SCA HYD-8:</b> Vegetation Management on Creekside Properties (See under Impact HYD-3)				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.3 Biological Resources (cont.)</b>					
<p><b>Impact BIO-6:</b> The Project would not fundamentally conflict with the City of Oakland Creek Protection Ordinance (OMC Chapter 13.16) intended to protect biological resources. (Criterion g) (Less than Significant with SCAs)</p>	<p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p> <p><b>SCA Implementation Measure BIO-3.1</b> <i>(to further implement SCA BIO-3) (See under Impact BIO-2)</i></p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-2:</b> State Construction General Permit <i>(See under Impact HYD-1)</i></p>				
<p><b>Impact BIO-7:</b> The Project, in combination with other past, present, existing, approved, pending, and reasonably foreseeable future projects within and around the Project area, would not have a considerable contribution to any cumulative impacts related to biological resources. (Potentially Significant)</p>	<p><b>SCA BIO-1:</b> Tree Removal during Breeding Bird Season <i>(See under Impact BIO-1)</i></p> <p><b>SCA Implementation Measure BIO-1.1</b> <i>(to further implement SCA BIO-1) (See under Impact BIO-1)</i></p> <p><b>SCA Implementation Measure BIO-1.2</b> <i>(to further implement all BIO SCAs and BIO mitigation measures) (See under Impact BIO-1)</i></p> <p><b>SCA BIO-2:</b> Bird Collision Reduction Measures <i>(See under Impact BIO-1)</i></p> <p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p> <p><b>SCA BIO-4:</b> Dewatering/Diversion <i>(See under Impact BIO-4)</i></p> <p><b>SCA BIO-5:</b> Tree Permit <i>(See under Impact BIO-5)</i></p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-2:</b> State Construction General Permit <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-8:</b> Vegetation Management on Creekside Properties <i>(See under Impact HYD-3)</i></p> <p><b>New Mitigation Measure BIO-1.1</b> <i>(to further implement SCA BIO-1) (See under Impact BIO-1)</i></p> <p><b>New Mitigation Measure BIO-1.2</b> <i>(See under Impact BIO-1)</i></p> <p><b>New Mitigation Measure BIO-2</b> <i>(See under Impact BIO-1)</i></p>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources</b>					
<p><b>Impact CUL-1:</b> Relocation and Rehabilitation of Club Knoll could result in a substantial adverse change in the significance of a historical resource by adversely affecting the character-defining features that convey its historic significance and justify its inclusion in the City of Oakland's Local Register of Historic Resources. (Criterion a) (Potentially Significant)</p>	<p><b>New Mitigation Measure CUL-1.1: HABS Documentation.</b> <i>Prior to approval of a construction-related permit for Club Knoll</i>, the Project sponsor shall document Club Knoll according to the Historic American Building Survey (HABS) standards, which requires:</p> <ol style="list-style-type: none"> <li>Drawings: A full set of measured drawings depicting the building. Consideration may be given to using 3D laser scanning at an appropriate resolution to aid in the creation of the drawings.</li> <li>Photographs: Photographs with large-format negatives of exterior and interior views of the existing building. Photocopies with large-format negatives, or high resolution digital copies of historic photographs. Consideration may be given to the use of high resolution digital photography in lieu of large-format negatives. If digital photography is selected, photo quality should meet the standards outlined in the National Register Photo Policy Factsheet updated 5/15/2013.</li> <li>Written data: A historical report in Outline Format.</li> <li>A qualified architectural historian or historical architect meeting the qualifications in the Secretary of the Interior's Professional Qualification Standards shall oversee the preparation of the plans, photographs and written data.</li> <li>The documentation shall be submitted for review and approval by qualified staff of the City of Oakland Bureau of Planning, Oakland Cultural Heritage Survey (OCHS).</li> <li>The documentation shall be filed with the Oakland Cultural Heritage Survey, the Oakland History Room at the Oakland Public Library, and the Northwest Information Center at Sonoma State University, the repository for the California Historical Resources Information System.</li> </ol>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit for Club Knoll.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Prepare HABS documentation of Club Knoll per measures (a) through (d), and submit documentation to the City of Oakland Bureau of Planning per measure (e).</li> <li>File approved HABS documentation per measure (f).</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS:</b></p> <ul style="list-style-type: none"> <li>Review and approve HABS documentation of Club Knoll.</li> <li>Verify filing of HABS documentation at the OCHS, Oakland Public Library, and the Northwest Information Center at Sonoma State University, per measure (f).</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<b>Impact CUL-1 (cont.)</b>	<p><b>New Mitigation Measure CUL-1.2 Baseline Building Conditions Study (Structural).</b> Prior to approval of a construction-related permit for Club Knoll, the Project sponsor shall prepare a Baseline Building Conditions Study to establish the baseline condition of the building and determine what kind of stabilization might be necessary to relocate the building. Specifically:</p> <p>a. A preservation architect and a structural engineer, as defined in the Carey &amp; Co. report dated May 3, 2016, shall undertake an existing condition study of Club Knoll.</p> <p>b. The documentation shall take the form of written descriptions and visual illustrations, including of those physical characteristics of Club Knoll that convey its historic significance and must be protected and preserved, and recommendations for any structural reinforcement, stabilization, or protection before the relocation or any other alteration.</p> <p>c. The Project sponsor shall implement work in accordance with the approved plan.</p>	<p><b>Submit Baseline Building Conditions Study:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit for Club Knoll.</li> </ul> <p><b>Conduct Work Pursuant to Approved Study:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout Club Knoll demolition/dismantling, relocation, and construction.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit a Baseline Building Conditions Study and stabilization method, prepared by a preservation architect and a structural engineer per measure (a).</li> <li>Submit Club Knoll plans/visual illustrations and implement Club Knoll work pursuant to the approved Study per measure (c).</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS; Bureau of Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Review and approve Baseline Building Conditions Study and stabilization method, and corresponding Club Knoll plans/visual illustrations.</li> <li>Verify all applicable measures in the mitigation are implemented</li> </ul>	
	<p><b>New Mitigation Measure CUL-1.3: Relocation Travel Route.</b> Prior to approval of a construction-related permit for Club Knoll, the Project sponsor shall prepare a Relocation Travel Route Plan for review and approval by qualified staff of the City of Oakland Bureau of Planning, OCHS. Specifically, the plan shall:</p> <p>a) Show the location of the proposed travel route from the existing Club Knoll location to the new location.</p> <p>b) Identify and locate on-site covered, secured and enclosed storage</p> <p>c) areas where components of Club Knoll may be temporarily stored before or during relocation, if required.</p> <p>d) Identify how the relocation site will be prepared to accept the relocated components of Club Knoll, including but not limited to grading and construction of the foundation.</p> <p>e) The Project sponsor shall implement work in accordance with the approved plan.</p>	<p><b>Prepare Relocation Travel Route Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit for Club Knoll.</li> </ul> <p><b>Implement Work Pursuant to Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout Club Knoll demolition/dismantling, relocation, and construction.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Prepare and submit a Relocation Travel Route Plan.</li> <li>Implement Club Knoll relocation pursuant to the approved Relocation Travel Route Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS:</b></p> <ul style="list-style-type: none"> <li>Review and approve the Relocation Travel Route Plan for Club Knoll.</li> <li>Verify all applicable measures in the mitigation are implemented.</li> </ul>	<p><i>Relocation Travel Route Plan</i> submitted to City as part of a Club Knoll Final Development Plan, dated 4/3/2017.</p>

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
Impact CUL-1 (cont.)	<p><b>New Mitigation Measure CUL-1.4: Building Features Inventory and Plan.</b> Prior to approval of a construction-related permit for Club Knoll, the Project sponsor shall prepare a Building Features Inventory and Plan for review and approval by qualified staff of the City of Oakland Bureau of Planning, OCHS. Specifically, the inventory shall include the following, without limitation:</p> <p><i>Character-defining Features</i></p> <p>a) Identify the character-defining features of Club Knoll to be relocated, specifying features that cannot be repaired, are deteriorated or damaged beyond repair and will need to be replaced.</p> <p>b) Describe how the character-defining features will be treated and cleaned to remove graffiti and/or mold.</p> <p><i>Existing and Proposed Building Plans</i></p> <p>c) Provide a complete set of schematic floor and roof plans and elevations showing existing conditions (which may come from the HABS report in Mitigation Measure CUL-1.1 or Baseline Building Conditions Study for Mitigation Measure CUL-1.2). The existing floor plans should identify elements and spaces proposed for demolition, as well as the location of where the building will be cut into moveable components (horizontally and vertically).</p> <p>d) Provide a complete set of schematic proposed floor plans identifying new walls, insertions, and other alterations proposed to interior spaces.</p> <p>e) The existing and proposed building plans shall be prepared by a qualified preservation architect and structural engineer.</p> <p><i>Materials Compatibility</i></p> <p>f) Tests shall be conducted of the exterior stucco and interior plaster to ensure new materials match the original.</p>	<p><b>Preparation of Building Features Inventory and Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of a construction-related permit for Club Knoll.</li> </ul> <p><b>Conduct Work Pursuant to Approved Inventory and Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout Club Knoll demolition/dismantling, relocation, and construction.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Prepare and submit a Building Features Inventory and Plan (Plan), prepared by a qualified preservation architect and a structural engineer per measure (e).</li> <li>Implement Club Knoll work pursuant to the approved Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS; Bureau of Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Review and approve Building Features Inventory and Plan.</li> <li>Verify all applicable measures in the mitigation are implemented.</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<b>Impact CUL-1 (cont.)</b>	<p><i>Qualifications</i></p> <p>g) Identify the vendors and subcontractors to undertake restoration and relocation work. The contractor responsible for the relocation and rehabilitation work shall be experienced in the Secretary of the Interior's Standards.</p> <p>h) The Project sponsor shall implement work in accordance with the approved plans and requirements.</p>				
	<p><b>New Mitigation Measures CUL-1.5: Specific Relocation/ Rehabilitation Measures.</b> Ongoing, during the relocation activities for Club Knoll. The Project sponsor shall incorporate the following mitigation measures into a final Club Knoll relocation work plan which it shall submit for review and approval by qualified staff of the City of Oakland Bureau of Planning, OCHS:</p> <p>a) Ensure that all temporary work to shore and brace the building will be reversible, additive, and shall not destroy any surviving historic fabric in the building.</p> <p>b) Ensure that a preservation architect and a structural engineer, as defined in the Carey &amp; Co. report dated May 3, 2016, will be on site to monitor dismantlement and reassembly of Club Knoll.</p> <p>c) New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired</p> <p>d) Ensure components and parts of the building dismantled during the relocation process are catalogued, protected, stored in a secure area, if necessary, and reassembled in their original location on the relocated building.</p> <p>e) Ensure that the proposed steel frame and new interior systems will not be visible in the relocated building, except as necessary for life safety or in newly installed kitchen, bathrooms, elevators, or similar systems.</p>	<p><b>Submit Final Club Knoll Relocation/Rehabilitation Work Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit for Club Knoll.</li> </ul> <p><b>Conduct Work Pursuant to Approved Work Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout Club Knoll demolition/ dismantling, relocation, and construction.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit a Final Club Knoll Relocation/Rehabilitation Work Plan (Plan).</li> <li>Implement Club Knoll relocation/rehabilitation pursuant to the approved Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS; Bureau of Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Review and approve Club Knoll Relocation/ Rehabilitation Work Plan.</li> <li>Verify all applicable measures in the mitigation are implemented.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<b>Impact CUL-1 (cont.)</b>	<ul style="list-style-type: none"> <li>f) Ensure that protective barriers or buffers are provided to further protect the building from potential damage by construction activities from new construction around the relocated building, including the operation of construction equipment.</li> <li>g) Ensure that if original wood floor material is found beneath more recent finishes, it shall be inspected for soundness and as much as possible shall be retained. Any deteriorated wood flooring shall be replaced with in-kind material.</li> <li>h) Ensure all work, including improvements in compliance with the American Disabilities Act (ADA), will adhere to the Secretary of the Interior's Standards for the Treatment of Historic Properties, using the Rehabilitation Standards.</li> <li>i) Ensure character-defining features that are not deteriorated beyond repair, including historic windows and surviving window hardware, are preserved during dismantling, and properly installed and reassembled in their original location.</li> <li>j) Ensure the foundation is constructed such that the building, at the exterior stair location on the west elevation, is raised above to the surrounding finished grade.</li> <li>k) Ensure the foundation is constructed such that the building, at the exterior stair location on the west elevation, is raised above the surrounding finished grade, and that the orientation is such that Club Knoll will maintain the important relationships with its setting identified in the Carey &amp; Co. Historic Report (May 2016).</li> </ul>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<p><b>Impact CUL-3:</b> The Project could result in significant impacts to unknown archaeological resources. (Criterion b) (<i>Less than Significant with SCAs</i>)</p>	<p><b>SCA CUL-1:</b> Archaeological and Paleontological Resources – Discovery During Construction (#29).</p> <p><i>During construction.</i> Pursuant to CEQA Guidelines section 15064.5(f), in the event that any historic or prehistoric subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and the project applicant shall notify the City and consult with a qualified archaeologist or paleontologist, as applicable, to assess the significance of the find. In the case of discovery of paleontological resources, the assessment shall be done in accordance with the Society of Vertebrate Paleontology standards. If any find is determined to be significant, appropriate avoidance measures recommended by the consultant and approved by the City must be followed unless avoidance is determined unnecessary or infeasible by the City. Feasibility of avoidance shall be determined with consideration of factors such as the nature of the find, project design, costs, and other considerations. If avoidance is unnecessary or infeasible, other appropriate measures (e.g., data recovery, excavation) shall be instituted. Work may proceed on other parts of the project site while measures for the cultural resources are implemented.</p> <p>In the event of data recovery of archaeological resources, the project applicant shall submit an Archaeological Research Design and Treatment Plan (ARDTP) prepared by a qualified archaeologist for review and approval by the City. The ARDTP is required to identify how the proposed data recovery program would preserve the significant information the archaeological resource is expected to contain. The ARDTP shall identify the scientific/historic research questions applicable to the expected resource, the data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. The ARDTP shall include the analysis and specify the curation and storage methods. Data recovery, in general, shall be limited to the portions of the archaeological resource that could be impacted by the proposed project. Destructive data recovery methods shall not be applied to portions of the archaeological resources if nondestructive</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Adhere to conditions and standards regarding the discovery of historic or prehistoric subsurface cultural resources and paleontological resources; avoidance measures; excavation plans; preparation of an ARDTP; and qualifications of consulting archaeologists and paleontologists.</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS; Bureau of Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Verify qualifications of as-needed consulting archeologist and/or paleontologist.</li> <li>Review and approve the ATDTP if one is required under conditions of the SCA.</li> <li>Verify all applicable conditions in the SCA are implemented.</li> </ul>	



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<b>Impact CUL-3 (cont.)</b>	<p>methods are practicable. Because the intent of the ARDTP is to save as much of the archaeological resource as possible, including moving the resource, if feasible, preparation and implementation of the ARDTP would reduce the potential adverse impact to less than significant. The project applicant shall implement the ARDTP at his/her expense.</p> <p>In the event of excavation of paleontological resources, the project applicant shall submit an excavation plan prepared by a qualified paleontologist to the City for review and approval. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and/or a report prepared by a qualified paleontologist, as appropriate, according to current professional standards and at the expense of the project applicant.</p>				
	<p><b>SCA CUL-2:</b> Archaeologically Sensitive Areas – Pre-Construction Measures (#30).</p> <p><i>Prior to approval of construction-related permit; during construction.</i> The project applicant shall implement either Provision A (Intensive Pre-Construction Study) <u>or</u> Provision B (Construction ALERT Sheet) concerning archaeological resources.</p> <p><b>Provision A: Intensive Pre-Construction Study.</b> The project applicant shall retain a qualified archaeologist to conduct a site-specific, intensive archaeological resources study for review and approval by the City prior to soil-disturbing activities occurring on the project site. The purpose of the site-specific, intensive archaeological resources study is to identify early the potential presence of history-period archaeological resources on the project site. At a minimum, the study shall include:</p> <ol style="list-style-type: none"> <li>Subsurface presence/absence studies of the project site. Field studies may include, but are not limited to, auguring and other common methods used to identify the presence of archaeological resources.</li> <li>A report disseminating the results of this research.</li> </ol>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Provision A: Prior to approval of any construction-related permit.</li> <li>Provision B: Prior to any soil-disturbing activities, and ongoing throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Implement either Provision A or Provision B concerning archaeological resources on the Project site.</li> </ul>	<p><b>City of Oakland, Bureau of Planning – OCHS; Bureau of Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>Provision A: Review and approve intensive pre-construction survey workplan and study.</li> <li>Provision B: As needed, review and approve the ALERT sheet.</li> <li>Verify all applicable conditions in the SCA are implemented.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<p><b>Impact CUL-3 (cont.)</b></p>	<p>c. Recommendations for any additional measures that could be necessary to mitigate any adverse impacts to recorded and/or inadvertently discovered cultural resources.</p> <p>If the results of the study indicate a high potential presence of historic-period archaeological resources on the project site, or a potential resource is discovered, the project applicant shall hire a qualified archaeologist to monitor any ground disturbing activities on the project site during construction and prepare an ALERT sheet pursuant to Provision B below that details what could potentially be found at the project site. Archaeological monitoring would include briefing construction personnel about the type of artifacts that may be present (as referenced in the ALERT sheet, required per Provision B below) and the procedures to follow if any artifacts are encountered, field recording and sampling in accordance with the Secretary of Interior’s Standards and Guidelines for Archaeological Documentation, notifying the appropriate officials if human remains or cultural resources are discovered, and preparing a report to document negative findings after construction is completed if no archaeological resources are discovered during construction.</p> <p><b>Provision B: Construction ALERT Sheet.</b> The project applicant shall prepare a construction “ALERT” sheet developed by a qualified archaeologist for review and approval by the City prior to soil-disturbing activities occurring on the project site. The ALERT sheet shall contain, at a minimum, visuals that depict each type of artifact that could be encountered on the project site. Training by the qualified archaeologist shall be provided to the project’s prime contractor, any project subcontractor firms (including demolition, excavation, grading, foundation, and pile driving), and utility firms involved in soil-disturbing activities within the project site.</p> <p>The ALERT sheet shall state, in addition to the basic archaeological resource protection measures contained in other standard conditions of approval, all work must stop and the City’s Environmental Review Officer</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<b>Impact CUL-3 (cont.)</b>	contacted in the event of discovery of the following cultural materials: concentrations of shellfish remains; evidence of fire (ashes, charcoal, burnt earth, fire-cracked rocks); concentrations of bones; recognizable Native American artifacts (arrowheads, shell beads, stone mortars [bowls], humanly shaped rock); building foundation remains; trash pits, privies (outhouse holes); floor remains; wells; concentrations of bottles, broken dishes, shoes, buttons, cut animal bones, hardware, household items, barrels, etc.; thick layers of burned building debris (charcoal, nails, fused glass, burned plaster, burned dishes); wood structural remains (building, ship, wharf); clay roof/floor tiles; stone walls or footings; or gravestones. Prior to any soil-disturbing activities, each contractor shall be responsible for ensuring that the ALERT sheet is circulated to all field personnel, including machine operators, field crew, pile drivers, and supervisory personnel. The ALERT sheet shall also be posted in a visible location at the project site.				
<b>Impact CUL-4:</b> The Project could directly or indirectly destroy a unique paleontological resource or site or unique geologic feature. (Criterion c) ( <i>Less than Significant with SCAs</i> )	<b>SCA CUL-1:</b> Archaeological and Paleontological Resources – Discovery During Construction. ( <i>See under Impact CUL-3</i> )				
<b>Impact CUL-5:</b> The Project could disturb human remains, including those interred outside of formal cemeteries (Criterion d). ( <i>Less than Significant with SCAs</i> )	<b>SCA CUL-3:</b> Human Remains – Discovery During Construction (#31).  <i>During construction.</i> Pursuant to CEQA Guidelines section 15064.5(e)(1), in the event that human skeletal remains are uncovered at the project site during construction activities, all work shall immediately halt and the project applicant shall notify the City and the Alameda County Coroner. If the County Coroner determines that an investigation of the cause of death is required or that the remains are Native American, all work shall cease within 50 feet of the remains until appropriate	<b>Implementation:</b> <ul style="list-style-type: none"><li>Ongoing, throughout all construction activities.</li></ul>	<b>Master Developer and Each FDP Project Applicant:</b> <ul style="list-style-type: none"><li>Adhere to conditions regarding the discovery of human skeletal remains; avoidance measures; work stop and restart; and monitoring.</li></ul>	<b>City of Oakland, Bureau of Planning; Alameda County Coroner:</b> <ul style="list-style-type: none"><li>As needed, review and approve plans to address human skeletal remains, including plans for avoidance or other treatment.</li></ul>	



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.4 Cultural Resources (cont.)</b>					
<b>Impact CUL-5 (cont.)</b>	arrangements are made. In the event that the remains are Native American, the City shall contact the California Native American Heritage Commission (NAHC), pursuant to subdivision (c) of section 7050.5 of the California Health and Safety Code. If the agencies determine that avoidance is not feasible, then an alternative plan shall be prepared with specific steps and timeframe required to resume construction activities. Monitoring, data recovery, determination of significance, and avoidance measures (if applicable) shall be completed expeditiously and at the expense of the project applicant.			<p><b>City of Oakland, Bureau of Planning</b></p> <ul style="list-style-type: none"> <li>• Contact the NAHC, pursuant to the California Health and Safety Code if Native American remains are discovered and to determine feasibility of avoidance.</li> <li>• Verify all other conditions in the SCA are implemented.</li> </ul>	
<p><b>Impact CUL 6:</b> The Project, in combination with other past, present, existing, approved, pending and reasonably foreseeable future projects, would not result in a significant impact to historic or cultural resources. (Potentially Significant)</p>	<p><b>SCA CUL-1:</b> Archaeological and Paleontological Resources – Discovery During Construction (<i>See under Impact CUL-3</i>)</p> <p><b>SCA CUL-2:</b> Archaeologically Sensitive Areas – Pre-Construction Measures. (<i>See under Impact CUL-3</i>)</p> <p><b>SCA CUL-3:</b> Human Remains – Discovery During Construction. (<i>See under Impact CUL-5</i>)</p> <p><b>New Mitigation Measure CUL-1.1:</b> HABS Documentation (see above)</p> <p><b>New Mitigation Measure CUL-1.2:</b> Baseline Building Conditions Study (Structural) (see above)</p> <p><b>New Mitigation Measure CUL-1.3:</b> Relocation Travel Route (see above)</p> <p><b>New Mitigation Measure CUL-1.4:</b> Building Features Inventory and Plan (see above)</p> <p><b>New Mitigation Measures CUL-1.5:</b> Specific Relocation/Rehabilitation Measures (see above)</p>				

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils</b>					
<p><b>Impact GEO-1:</b> The Project could expose people or structures to substantial risk of loss, injury, or death involving strong seismic ground shaking. (Criterion a.2) (Less than Significant with SCAs)</p>	<p><b>SCA GEO-1:</b> Soils Report (#34).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall submit a soils report prepared by a registered geotechnical engineer for City review and approval. The soils report shall contain, at a minimum, field test results and observations regarding the nature, distribution and strength of existing soils, and recommendations for appropriate grading practices and project design. The project applicant shall implement the recommendations contained in the approved report during project design and construction.</p>	<p><b>Submit Soils Report:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any subdivision improvement-related permit.</li> <li>• Prior to the approval of any building permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Report:</b></p> <ul style="list-style-type: none"> <li>• During Project design and construction.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit a soils report prepared by a registered design professional.</li> <li>• Incorporate recommendations from the approved soils report into the project design and implement the recommendations.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Engineering Services; Bureau of Building:</b></p> <ul style="list-style-type: none"> <li>• Review and approve soils report and confirm recommendations are incorporated into the project design and construction.</li> </ul>	
	<p><b>SCA GEO-2:</b> Seismic Hazards Zone (Landslide/Liquefaction) (#36).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall submit a site-specific geotechnical report, consistent with California Geological Survey Special Publication 117 (as amended), prepared by a registered geotechnical engineer for City review and approval containing at a minimum a description of the geological and geotechnical conditions at the site, an evaluation of site-specific seismic hazards based on geological and geotechnical conditions, and recommended measures to reduce potential impacts related to liquefaction and/or slope stability hazards. The project applicant shall implement the recommendations contained in the approved report during project design and construction.</p>	<p><b>Submit Site-Specific Geotechnical Report:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Report:</b></p> <ul style="list-style-type: none"> <li>• During Project design and construction.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit a site-specific geologic report prepared by a registered geologist.</li> <li>• Incorporate recommendations from the approved geologic report into the project design and implement the recommendations.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building:</b></p> <ul style="list-style-type: none"> <li>• Review and approve geologic report and confirm recommendations are incorporated into the project design and construction.</li> <li>• Verify all other conditions in the SCA are implemented.</li> </ul>	
	<p><b>SCA Implementation Measure GEO-2.1:</b> To further implement SCA GEO-2, the Project applicant shall implement the following measures, as applicable, based on the site-specific geotechnical report to be developed pursuant to SCA GEO-2:</p> <p>The contact between the Jurassic Volcanics and the Knoxville Formation should be further examined during grading for the Project to determine if supplemental corrective grading measures are needed to address potential engineering issues, such as weak sheared material or a groundwater barrier. If the determination is affirmative, the Project applicant shall identify and implement required additional corrective grading measures.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout grading activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Conduct examinations to determine if supplemental corrective grading measures are needed.</li> <li>• Implement any supplemental corrective grading measures, if any are identified.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections:</b></p> <ul style="list-style-type: none"> <li>• Review and approve of supplemental corrective grading measures, as needed.</li> <li>• Verify all other conditions in the SCA are implemented.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
<p><b>Impact GEO-2:</b> The Project could expose people or structures to substantial risk of loss, injury, or death involving seismic-related ground failure, including liquefaction, lateral spreading, subsidence or collapse. (Criterion a.3) (Less than Significant with SCAs)</p>	<p><b>SCA GEO-1:</b> Soils Report. (See under Impact GEO-1)</p> <p><b>SCA GEO-2:</b> Seismic Hazards Zone (Landslide/Liquefaction). (See under Impact GEO-1)</p> <p><b>SCA Implementation Measure GEO-2.2:</b> To further implement SCA GEO-2, the Project applicant shall implement these following corrective measures to repair existing unstable site conditions, as applicable, based on the site-specific geotechnical report to be developed pursuant to SCA GEO-2:</p> <p><i>Liquefaction</i> - To address potential effects of liquefaction, the project could implement any one or more of the following, as necessary:</p> <ul style="list-style-type: none"> <li>• Avoid development within 50 feet of the potential liquefaction zone, as designated by the State seismic hazard zones map;</li> <li>• Conduct in-situ treatment, such as dynamic compaction;</li> <li>• Perform remedial grading measures, such as removal and replacement of a portion or all of the potentially liquefiable soil with engineered fill; and</li> <li>• Ensure placement of a compacted fill cap over the potential liquefaction zones, potentially with use of geogrid reinforced fill.</li> <li>• Employ foundation design measures, such as deep foundations that extend through the potential liquefaction zone.</li> </ul> <p><i>Lateral Spreading</i> – To address potential effects of lurching and lateral spreading, the project could include any one or more of the following, as necessary:</p> <ul style="list-style-type: none"> <li>• Ensure that, if a setback of improvements from creek banks is used to reduce the susceptibility to lurching and lateral spreading in areas identified along Rifle Range Creek, improvements should be set back outside an upward 4:1 (horizontal:vertical) projection from the toe of the creek bank;</li> </ul>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, during all grading or earthwork activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Implement corrective site stabilization measures per the SCA, as applicable.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building:</b></p> <ul style="list-style-type: none"> <li>• Review and approve plans incorporating corrective site stabilization measures based on the site-specific geologic report (see SCA GEO-2), as needed.</li> <li>• Verify all other measures are implemented.</li> </ul>	



## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
<b>Impact GEO-2 (cont.)</b>	<ul style="list-style-type: none"> <li>Key and bench where fills are placed on sloping ground; and</li> <li>Use drilled pier foundation systems designed to accommodate expected lateral loads for structures situated on slopes, as determined on case-by-case basis.</li> </ul>				
	<p><b>SCA GEO-4:</b> Oakland Area Geologic Hazard Abatement District – GHAD (#37).</p> <p><i>Ongoing as specified in the condition.</i> Prior to approval of the final map or issuance of a building permit (whichever occurs first), the project applicant shall provide to the City 1) all required resolutions from the GHAD and City Council showing that the project property has been annexed into the GHAD, and 2) a statement from the GHAD Manager stating that an adequate funding mechanism is in place to fund the GHAD operations for the annexed property. To begin the annexation process, the project applicant shall submit a petition for annexation to the GHAD Manager which shall include but is not limited to a proposed Plan of Control as defined in Public Resource Code Section 26509, specifying all anticipated operations and maintenance responsibilities of the GHAD for the annexed property. The project applicant will be required to pay to the GHAD costs and fees associated with the annexation request, which includes the preparation and review of all necessary documents and resolutions by the GHAD Manager and/or GHAD Attorney. The GHAD Manager may require the project applicant to provide initial funding to allow the GHAD to operate with respect to the annexed property during the time a secure and stable financing source is obtained to ultimately fund the long term operations of the GHAD for the annexed property. If a real property assessment is proposed as a financing mechanism, the project applicant shall prepare an engineer's report identifying the projected costs and budget for GHAD operations for the annexed property and comply with all assessment voting requirements and other requirements in Proposition 218. If annexation is not approved by the GHAD and/or City Council, the project applicant shall demonstrate to the City's satisfaction that 1) another entity will and has</p>	<p><b>SITEWIDE MASTER PLAN</b></p> <p><b>Submit GHAD Resolutions and Funding:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of the final map or issuance of a building permit (whichever occurs first)</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Establish an Oakland Area GHAD, per the conditions, processes, payments and reporting.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Public Works Director/City Engineer:</b></p> <ul style="list-style-type: none"> <li>Review relevant engineering report/documentation as needed (Public Works Director/City Engineer)</li> <li>Confirm submittal of the petition for annexation to the GHAD Manager per the SCA.</li> <li>Confirm and review resolutions from the GHAD and City Council showing that the project property has been annexed into the GHAD, along with a statement that an adequate funding mechanism is in place to fund the GHAD operations for the annexed property.</li> <li>Verify that the action has taken place in accordance with the SCA, and approve if found acceptable.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
<b>Impact GEO-2 (cont.)</b>	<p>assumed the responsibilities proposed for the GHAD (“Other Responsible Entity”) and 2) there is an adequate financing mechanism in place to carry out those responsibilities.</p> <p>The project applicant shall defend, hold harmless, and indemnify the GHAD, its officers, and agents against any and all liability, damages, claims, demands, judgments, losses, or other forms of legal or equitable relief relating to the GHAD annexation process and the securing/approval of funding sources by the GHAD and in the case of the City Council members, actions taken by said members while acting as the GHAD Board of Directors.</p> <p>The project applicant shall request the GHAD or Other Responsible Entity to defend, hold harmless, and indemnify the Indemnified Parties (as defined in these Conditions of Approval) and their insurers against any and all liability, damages, claims, demands, judgments, losses, or other forms of legal or equitable relief related to the responsibilities and operation of the GHAD or Other Responsible Entity (including, without limitation, maintenance of GHAD/Other Responsibility Entity owned property) relating to the annexed property (“Indemnified Geologic Claims”) and in the case of the City Council members, actions taken by said members while acting as the GHAD Board of Directors. This indemnity shall include, without limitation, payment of litigation expenses relating to the qualified Indemnified Geologic Claims. The Indemnified Parties shall take all reasonable steps to promptly notify the GHAD/Other Responsible Entity of any claim, demand, or legal actions that may create a claim for indemnification under this condition of approval. Within 90 days of the annexation to the GHAD or acceptance by the Other Responsible Entity, the applicant shall request the GHAD or Other Responsible Entity to enter into an Indemnification Agreement to establish in more specific detail the terms and conditions of the indemnification obligations set forth herein. The parties acknowledge that the GHAD can only provide indemnification as allowed by law. Any failure of any party to timely execute such Indemnification Agreement shall not be construed to limit any right or obligation otherwise specified in these Conditions of Approval.</p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
<p><b>Impact GEO-3:</b> The Project could expose people or structures to substantial risk of loss, injury, or death involving landslides. (Criterion a.4) (Less than Significant with SCAs)</p>	<p><b>SCA GEO-1:</b> Soils Report. <i>(See under Impact GEO-1)</i></p> <p><b>SCA GEO-2:</b> Seismic Hazards Zone (Landslide/Liquefaction). <i>(See under Impact GEO-1)</i></p> <p><b>SCA Implementation Measures GEO-2.3:</b> To further implement SCA GEO-2, the Project applicant shall implement these following corrective measures to repair existing unstable site conditions, as applicable, based on the site-specific geotechnical report to be developed pursuant to SCA GEO-2:</p> <p>a) Removal of existing fill, colluvium and slide debris to expose rock (in specific upland areas of the site);</p> <p>b) Removal of existing fill and compressible soil to expose stiff native material (generally throughout the central area of the site); and</p> <p>c) Reconstruct specific slopes with select granular fills and/or geogrid reinforced fill, based on final design slope stability analysis (focusing on creek bank slopes and upland fill slopes).</p> <p><i>Seismically-Induced Landslides</i> – To address potential effects of seismically-induced landslides, the project shall:</p> <p>d) Ensure properly engineered cut and fill slopes, stabilization of landslides, and/or creation of sufficient buffers between identified landslide areas and development areas, as determined on case-by-case basis.</p> <p><i>Landslides and Slope Instability</i> – To address potential effects of landslides and slope instability, the project shall (in addition to implementation of Mitigation Measure GEO-3 regarding slope stability):</p> <p>e) Where development encroaches into the mapped landslide areas, conduct remedial grading as determined on case-by-case basis;</p> <p>f) Minimize potential for adverse impacts from soil creep by benching through surficial soils during fill placement and by design of drill pier foundation systems to accommodate lateral loads from soil creep, as determined on case-by-case basis;</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, during all grading or earthwork activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Implement corrective site stabilization measures per the SCA, based on the site-specific geotechnical report SCA GEO-2), as applicable.</li> </ul>	<p><b>City of Oakland, Oakland Department of Transportation Engineering Services Unit (for subdivision improvements; Bureau of Building (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approve plans incorporating corrective site stabilization measures based on the site-specific geotechnical report (see SCA GEO-2), as needed.</li> <li>Verify all other measures are implemented.</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
<b>Impact GEO-3 (cont.)</b>	<p>g) Limit graded slopes for the project to within the following preliminary criteria although findings of further design-level geotechnical exploration and use of specific treatments (such as geogrid reinforced fill slopes and use of higher strength fill material based on laboratory testing) may support fill slopes that exceed these preliminary criteria:</p> <p>h) Remove existing fills located within the development area and replace them with engineered fill; existing fill materials that are free of deleterious debris may be placed onsite as engineered fill;</p> <p>i) Use of heavy duty or larger-track mounted excavators or removal of bedrock to the depth of planned utilities (and replacement with engineered fill) may be required for trenching in localized areas of deeper bedrock cuts that may generate oversized material (i.e. rocks larger than one foot in diameter); and</p> <p>j) In the eastern hilltop area of the site, larger-track mounted excavators may be needed to excavate rock at depths of 10 feet or more below original grade, and overexcavation during mass grading of street sections in areas of deeper cuts to depths below the level of proposed utilities may be appropriate.</p>				
<b>SCA GEO-4:</b> Oakland Area Geologic Hazard Abatement District – GHAD (See under Impact GEO-2)					
<b>Impact GEO-4:</b> The Project could result in substantial soil erosion or loss of topsoil, creating substantial risks to life, property, or creeks/waterways. (Criterion b) (Less than Significant with SCAs)	<b>SCA GEO-3: Construction-Related Permit(s).</b> Prior to approval of construction-related permit. (#33). The project applicant shall obtain all required construction-related permits/approvals from the City. The project shall comply with all standards, requirements and conditions contained in construction-related codes, including but not limited to the Oakland Building Code and the Oakland Grading Regulations, to ensure structural integrity and safe construction.	<b>Implementation:</b>	<b>Master Developer and Each FDP Project Applicant:</b>	<b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections:</b>	
		<ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul>	<ul style="list-style-type: none"> <li>Obtain all required construction-related permits/approvals from the City.</li> </ul>	<ul style="list-style-type: none"> <li>Confirm all required construction-related permits are obtained.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
<b>Impact GEO-5:</b> The Project could occur on expansive soils, creating substantial risks to life and property. (Criterion c) (Less than Significant with SCAs)	<b>SCA GEO-1:</b> Soils Report. <i>(See under Impact GEO-1)</i>				
<b>Impact GEO-6:</b> The Project could be located above a well, pit, swamp, mound, tank vault, unmarked sewer line, a landfill for which there is no approved closure and post-closure plan, or unknown fill soils, creating substantial risks to life or property. (Criteria d and e) (Potentially Significant)	<b>New Mitigation Measure GEO-3:</b> If during construction activities previously unidentified conditions such as wells, pits, swamps, mounds, tank vaults, unmarked sewer lines, suspected landfill areas, or unknown fill soils are encountered, construction in the immediate area shall cease until the City of Oakland Fire Department Hazardous Materials Unit or other applicable oversight agency has been notified. If there is any indication that the condition includes hazardous materials or waste or otherwise creates a substantial risk to life or property, then the lead agency shall direct any appropriate remediation measures, consistent with any and all applicable laws and regulations. Construction can resume at the discretion of the oversight agency.	<b>Implementation:</b> <ul style="list-style-type: none"><li>Ongoing, throughout all construction activities.</li></ul>	<b>Master Developer and Each FDP Project Applicant:</b> <ul style="list-style-type: none"><li>Stop construction if previously unidentified conditions are discovered.</li><li>Comply with all standards, requirements and conditions contained in construction-related codes.</li></ul>	<b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments); Oakland Fire Department (Hazardous Materials):</b> <ul style="list-style-type: none"><li>Confirm conditions have been addressed, and approve restart of construction activities.</li></ul>	
<b>Impact GEO-7:</b> The Project would not have a considerable contribution to cumulative impacts related to geology and soils, considering the combined effect of the Project and past, present, approved, pending, and reasonably foreseeable future projects in the area and citywide. (Potentially Significant)	<b>SCA GEO-1:</b> Soils Report. <i>(See under Impact GEO-1)</i> <b>SCA GEO-2:</b> Seismic Hazards Zone (Landslide/Liquefaction). <i>(See under Impact GEO-1)</i> <b>SCA Implementation Measure GEO-2.1</b> <i>(to further implement SCA GEO-2) (see above)</i> <b>SCA Implementation Measure GEO-2.2</b> <i>(to further implement SCA GEO-2) (see above)</i> <b>SCA Implementation Measure GEO-2.3</b> <i>(to further implement SCA GEO-2) (see above)</i> <b>SCA GEO-3:</b> Construction-Related Permit(s). <i>(See under Impact GEO-4)</i> <b>SCA GEO-4:</b> Oakland Area Geologic Hazard Abatement District (GHAD). <i>(See under Impact GEO-2)</i>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.5 Geology and Soils (cont.)</b>					
Impact GEO-7 (cont.)	<p><b>New Mitigation Measure GEO-3</b> (see above)</p> <p><b>SCA BIO-3:</b> Creek Protection Plan (See under Impact BIO-2)</p>				
<b>4.6 Greenhouse Gas Emissions and Climate Change</b>					
<p><b>Impact GHG-1:</b> The proposed Project would produce greenhouse gas emissions that exceed both 1,100 metric tons of CO<sub>2</sub>e per year and 4.6 metric tons of CO<sub>2</sub>e per service population annually in Phase 1 only. (Criterion a) (Less than Significant with SCAs).</p>	<p><b>SCA AIR-1:</b> Construction related Air Pollutant Controls (Dust and Equipment Emissions) (See under Impact AIR-1)</p> <p><b>SCA GHG-1:</b> Greenhouse Gas (GHG) Reduction Plan (#38).</p> <p><i>The Project sponsor has prepared a Greenhouse Gas Reduction Plan for the Project that is capable of increasing energy efficiency and reducing GHG emissions to below 4.6 metric tons of CO<sub>2</sub>e per year per service population, and is capable of reducing GHG emissions by 36 percent below the Project's 2005 "business-as-usual" baseline. Because the Project is to be constructed in phases, the GHG Reduction Plan provides GHG emission scenarios by phase. The GHG Reduction Plan includes measures as recommended in BAAQMD's latest CEQA Air Quality Guidelines; the California Air Resources Board Scoping Plan; the California Air Pollution Control Officers Association Quantifying Greenhouse Gas Mitigation Measures; the California Attorney General's website; and Reference Guides on Leadership in Energy and Environmental Design (LEED) published by the U.S. Green Building Council. These measures include physical design features, operational features and payment of fees to fund GHG-reducing programs.</i></p> <p>a. <b>Greenhouse Gas (GHG) Reduction Plan.</b></p> <p><u>Requirement.</u> The project applicant shall retain a qualified air quality consultant to develop a Greenhouse Gas (GHG) Reduction Plan for City review and approval and shall implement the approved GHG Reduction Plan.</p> <p>The goal of the GHG Reduction Plan shall be to increase energy efficiency and reduce GHG emissions to below <u>at least one</u> of the Bay Area Quality Management District's (BAAQMD's) CEQA Thresholds</p>	<p><b>SITEWIDE MASTER PLAN</b></p> <p><b>Submit GHG Reduction Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of a construction-related permit.</li> </ul> <p><b>Submit Annual Monitoring / Reports and Corrective Action Plans:</b></p> <ul style="list-style-type: none"> <li>Generally, starting two years after the City issues the first certificate of occupancy for the project, then ongoing for a period of at least 40 years.</li> </ul> <p><b>SITEWIDE MASTER PLAN and FDP PROJECTS</b></p> <p><b>Conduct Work Per Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Prepare and submit a GHG Reduction Plan, prepared by a qualified air quality consultant.</li> <li>Conduct ongoing monitoring and reporting of implemented GHG reduction measures.</li> </ul> <p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Implement GHG Reduction Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approve GHG Reduction Plan.</li> <li>Review Annual Report and, if needed, a Corrective GHG Action Plan.</li> <li>Verify all applicable conditions in the SCA are implemented.</li> </ul>	<p><i>Draft GHG Reduction Plan submitted to City (as appendix to the SEIR), dated December 2016.</i></p>



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<p><b>Impact GHG-1 (cont.)</b></p>	<p>of Significance (1,100 metric tons of CO<sub>2</sub>e per year or 4.6 metric tons of CO<sub>2</sub>e per year per service population) <u>AND</u> to reduce GHG emissions by 36 percent below the project's 2005 "business-as-usual" baseline GHG emissions (as explained below) to help implement the City's Energy and Climate Action Plan (adopted in 2012) which calls for reducing GHG emissions by 36 percent below 2005 levels. The GHG Reduction Plan shall include, at a minimum, (a) a detailed GHG emissions inventory for the project under a "business-as-usual" scenario with no consideration of project design features, or other energy efficiencies, (b) an "adjusted" baseline GHG emissions inventory for the project, taking into consideration energy efficiencies included as part of the project (including the City's Standard Conditions of Approval, proposed mitigation measures, project design features, and other City requirements) and additional GHG reduction measures available to further reduce GHG emissions, and (c) requirements for ongoing monitoring and reporting to demonstrate that the additional GHG reduction measures are being implemented. If the project is to be constructed in phases, the GHG Reduction Plan shall provide GHG emission scenarios by phase.</p> <p>Potential GHG reduction measures to be considered include, but are not be limited to, measures recommended in BAAQMD's latest CEQA Air Quality Guidelines, the California Air Resources Board Scoping Plan (December 2008, as may be revised), the California Air Pollution Control Officers Association (CAPCOA) Quantifying Greenhouse Gas Mitigation Measures (August 2010, as may be revised), the California Attorney General's website, and Reference Guides on Leadership in Energy and Environmental Design (LEED) published by the U.S. Green Building Council.</p> <p>The types of allowable GHG reduction measures include the following (listed in order of City preference): (1) physical design features; (2) operational features; and (3) the payment of fees to fund GHG-reducing programs (i.e., the purchase of "carbon credits") as explained below.</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<p><b>Impact GHG-1 (cont.)</b></p>	<p>The allowable locations of the GHG reduction measures include the following (listed in order of City preference): (1) the project site; (2) off-site within the City of Oakland; (3) off-site within the San Francisco Bay Area Air Basin; (4) off-site within the State of California; then (5) elsewhere in the United States.</p> <p>As with preferred locations for the implementation of all GHG reductions measures, the preference for carbon credit purchases include those that can be achieved as follows (listed in order of City preference): (1) within the City of Oakland; (2) within the San Francisco Bay Area Air Basin; (3) within the State of California; then (4) elsewhere in the United States. The cost of carbon credit purchases shall be based on current market value at the time purchased and shall be based on the project's operational emissions estimated in the GHG Reduction Plan or subsequent approved emissions inventory, which may result in emissions that are higher or lower than those estimated in the GHG Reduction Plan.</p> <p>For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be included on the drawings submitted for construction-related permits.</p> <p><b>b. <i>GHG Reduction Plan Implementation During Construction.</i></b></p> <p><u>Requirement.</u> The project applicant shall implement the GHG Reduction Plan during construction of the project. For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be implemented during construction. For physical GHG reduction measures to be incorporated into off-site projects, the project applicant shall obtain all necessary permits/approvals and the measures shall be included on drawings and submitted to the City Planning Director or his/her designee for review and approval. These off-site improvements shall be installed prior to completion of the subject project (or prior to completion of the project phase for phased projects). For GHG reduction measures involving the purchase of carbon credits, evidence of the payment/purchase shall be</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<p><b>Impact GHG-1 (cont.)</b></p>	<p>submitted to the City for review and approval prior to completion of the project (or prior to completion of the project phase, for phased projects).</p> <p>c. <b><i>GHG Reduction Plan Implementation After Construction.</i></b></p> <p><u>Requirement.</u> The project applicant shall implement the GHG Reduction Plan after construction of the project (or at the completion of the project phase for phased projects). For operational GHG reduction measures to be incorporated into the project or off-site projects, the measures shall be implemented on an indefinite and ongoing basis.</p> <p>The project applicant shall satisfy the following requirements for ongoing monitoring and reporting to demonstrate that the additional GHG reduction measures are being implemented. The GHG Reduction Plan requires regular periodic evaluation over the life of the project (generally estimated to be at least 40 years) to determine how the Plan is achieving required GHG emissions reductions over time, as well as the efficacy of the specific additional GHG reduction measures identified in the Plan.</p> <p><i>Annual Report.</i> Implementation of the GHG reduction measures and related requirements shall be ensured through compliance with Conditions of Approval adopted for the project. Generally, starting two years after the City issues the first Certificate of Occupancy for the project, the project applicant shall prepare each year of the useful life of the project an Annual GHG Emissions Reduction Report (“Annual Report”), for review and approval by the City Planning Director or his/her designee. The Annual Report shall be submitted to an independent reviewer of the City’s choosing, to be paid for by the project applicant.</p> <p>The Annual Report shall summarize the project’s implementation of GHG reduction measures over the preceding year, intended upcoming changes, compliance with the conditions of the Plan, and include a brief summary of the previous year’s Annual Report results (starting the second year). The Annual Report shall include a comparison of annual project emissions to the baseline emissions reported in the GHG Plan.</p>				



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<b>Impact GHG-1 (cont.)</b>	<p><i>Corrective Procedure.</i> If the third Annual Report, or any report thereafter, indicates that, in spite of the implementation of the GHG Reduction Plan, the project is not achieving the GHG reduction goal, the project applicant shall prepare a report for City review and approval, which proposes additional or revised GHG measures to better achieve the GHG emissions reduction goals, including without limitation, a discussion on the feasibility and effectiveness of the menu of other additional measures (“Corrective GHG Action Plan”). The project applicant shall then implement the approved Corrective GHG Action Plan. If, one year after the Corrective GHG Action Plan is implemented, the required GHG emissions reduction target is still not being achieved, or if the project applicant fails to submit a report at the times described above, or if the reports do not meet City requirements outlined above, the City may, in addition to its other remedies, (a) assess the project applicant a financial penalty based upon actual percentage reduction in GHG emissions as compared to the percent reduction in GHG emissions established in the GHG Reduction Plan; or (b) refer the matter to the City Planning Commission for scheduling of a compliance hearing to determine whether the project’s approvals should be revoked, altered or additional conditions of approval imposed.</p> <p>The penalty as described in (a) above shall be determined by the City Planning Director or his/her designee and be commensurate with the percentage GHG emissions reduction not achieved (compared to the applicable numeric significance thresholds) or required percentage reduction from the “adjusted” baseline.</p> <p>In determining whether a financial penalty or other remedy is appropriate, the City shall not impose a penalty if the project applicant has made a good faith effort to comply with the GHG Reduction Plan.</p> <p>The City would only have the ability to impose a monetary penalty after a reasonable cure period and in accordance with the enforcement process outlined in Planning Code Chapter 17.152. If a financial penalty is imposed, such penalty sums shall be used by the City</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<b>Impact GHG-1 (cont.)</b>	<p>solely toward the implementation of the GHG Reduction Plan.</p> <p><i>Timeline Discretion and Summary.</i> The City shall have the discretion to reasonably modify the timing of reporting, with reasonable notice and opportunity to comment by the applicant, to coincide with other related monitoring and reporting required for the project.</p>				
	<p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape (#77).</p> <p><b>[The SCA below applies to the projects listed below AND that are rated using the Bay Friendly Basic Landscape Checklists:</b></p> <p><b>Construction projects with over 25,000 sq. ft. of total floor area of new construction requiring a landscape plan.]</b></p> <p>a. <b><i>Compliance with Green Building Requirements During Plan-Check</i></b></p> <p><u>Requirement:</u> The project applicant shall comply with the applicable requirements of the City of Oakland Green Building Ordinance (chapter 18.02 of the Oakland Municipal Code) for projects using the Bay Friendly Basic Landscape Checklist.</p> <p>i. The following information shall be submitted to the City for review and approval with the application for a building permit:</p> <ul style="list-style-type: none"> <li>• Completed copy of the final green building checklist approved during the review of the Planning and Zoning permit.</li> <li>• Copy of the Unreasonable Hardship Exemption, if granted, during the review of the Planning and Zoning permit.</li> <li>• Permit plans that show, in general notes, detailed design drawings, and specifications as necessary, compliance with the items listed in subsection (ii) below.</li> </ul>	<p><b>Submit “Plan Check” Compliance Information:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of first construction (building) permit.</li> </ul> <p><b>Submit “During Construction” Compliance Information:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul> <p><b>Submit “After Construction” Compliance Information:</b></p> <ul style="list-style-type: none"> <li>• Prior to the finaling the Building Permit.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit information and plans to demonstrate compliance with the applicable requirements of the City of Oakland Green Building Ordinance and the Bay Friendly Basic Landscape.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>• Review and approve GHG Reduction Plan.</li> <li>• Review and approve project plans and required information to confirm compliance with all Green Building requirements.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<p><b>Impact GHG-1 (cont.)</b></p>	<ul style="list-style-type: none"> <li>• Copy of the signed statement by the Green Building Certifier approved during the review of the Planning and Zoning permit that the project complied with the requirements of the Green Building Ordinance.</li> <li>• Signed statement by the Green Building Certifier that the project still complies with the requirements of the Green Building Ordinance, unless an Unreasonable Hardship Exemption was granted during the review of the Planning and Zoning permit.</li> <li>• Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance.</li> </ul> <p>ii. The set of plans in subsection (i) shall demonstrate compliance with the following:</p> <ul style="list-style-type: none"> <li>• All green building points identified on the checklist approved during review of the Planning and Zoning permit, unless a Request for Revision Plan-check application is submitted and approved by the Bureau of Planning that shows the previously approved points that will be eliminated or substituted.</li> </ul> <p><b>b. Compliance with Green Building Requirements During Construction</b></p> <p><u>Requirement:</u> The project applicant shall comply with the applicable requirements of the Oakland Green Building Ordinance and the Bay Friendly Basic Landscape Checklist during construction of the project.</p> <p>The following information shall be submitted to the City for review and approval:</p> <ul style="list-style-type: none"> <li>i. Completed copies of the green building checklists approved during the review of the Planning and Zoning permit and during the review of the building</li> <li>ii. Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance.</li> </ul>				



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
Impact GHG-1 (cont.)	<p>c. <b>Compliance with Green Building Requirements After Construction</b></p> <p><u>Requirement:</u> The Green Building Certifier shall submit the appropriate documentation to City staff and attain the minimum required point level.</p>				
	<p><b>SCA GHG-3:</b> Green Building Requirements (#77). <i>During construction.</i></p> <p>d. <b>Compliance with Green Building Requirements During Plan-Check</b></p> <p><u>Requirement:</u> The project applicant shall comply with the requirements of the California Green Building Standards (CALGreen) mandatory measures and the applicable requirements of the City of Oakland Green Building Ordinance (chapter 18.02 of the Oakland Municipal Code).</p> <p>i. The following information shall be submitted to the City for review and approval with the application for a building permit:</p> <ul style="list-style-type: none"> <li>• Documentation showing compliance with Title 24 of the current version of the California Building Energy Efficiency Standards.</li> <li>• Completed copy of the final green building checklist approved during the review of the Planning and Zoning permit.</li> <li>• Copy of the Unreasonable Hardship Exemption, if granted, during the review of the Planning and Zoning permit.</li> <li>• Permit plans that show, in general notes, detailed design drawings, and specifications as necessary, compliance with the items listed in subsection (ii) below.</li> <li>• Copy of the signed statement by the Green Building Certifier approved during the review of the Planning and Zoning permit that the project complied with the requirements of the Green Building Ordinance.</li> </ul>	<p><b>Submit “Plan Check” Compliance Information:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of first construction (building) permit.</li> </ul> <p><b>Submit “During Construction” Compliance Information:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul> <p><b>Submit “After Construction” Compliance Information:</b></p> <ul style="list-style-type: none"> <li>• Prior to the finaling the Building Permit.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit information and plans to demonstrate compliance with the applicable requirements of the City of Oakland Green Building Ordinance and the Bay Friendly Basic Landscape.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>• Review and approve GHG Reduction Plan.</li> <li>• Review and approve project plans and required information to confirm compliance with all Green Building requirements.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<p><b>Impact GHG-1 (cont.)</b></p>	<ul style="list-style-type: none"> <li>• Signed statement by the Green Building Certifier that the project still complies with the requirements of the Green Building Ordinance, unless an Unreasonable Hardship Exemption was granted during the review of the Planning and Zoning permit.</li> <li>• Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance.</li> </ul> <p>ii. The set of plans in subsection (i) shall demonstrate compliance with the following:</p> <ul style="list-style-type: none"> <li>• CALGreen mandatory measures.</li> <li>• Green building point level/certification requirement, per the appropriate checklist approved during the Planning entitlement process.</li> <li>• All green building points identified on the checklist approved during review of the Planning and Zoning permit, unless a Request for Revision Plan-check application is submitted and approved by the Bureau of Planning that shows the previously approved points that will be eliminated or substituted.</li> <li>• The required green building point minimums in the appropriate credit categories.</li> </ul> <p><u>When Required: Prior to approval of construction-related permit</u></p> <p><u>Initial Approval: Bureau of Building</u></p> <p><u>Monitoring/Inspection: N/A</u></p> <p>e. <b><i>Compliance with Green Building Requirements During Construction</i></b></p> <p><u>Requirement: The project applicant shall comply with the applicable requirements of CALGreen and the Oakland Green Building Ordinance during construction of the project.</u></p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<b>Impact GHG-1 (cont.)</b>	<p>The following information shall be submitted to the City for review and approval:</p> <ul style="list-style-type: none"> <li>i. Completed copies of the green building checklists approved during the review of the Planning and Zoning permit and during the review of the building permit.</li> <li>ii. Signed statement(s) by the Green Building Certifier during all relevant phases of construction that the project complies with the requirements of the Green Building Ordinance.</li> <li>iii. Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance.</li> </ul> <p><u>When Required: During construction</u></p> <p><u>Initial Approval: N/A</u></p> <p><u>Monitoring/Inspection: Bureau of Building</u></p> <p><b>f. <i>Compliance with Green Building Requirements After Construction</i></b></p> <p><u>Requirement: Prior to the finaling the Building Permit, the Green Building Certifier shall submit the appropriate documentation to City staff and attain the minimum required point level.</u></p> <p><u>When Required: Prior to Final Approval</u></p> <p><u>Initial Approval: Bureau of Planning</u></p> <p><u>Monitoring/Inspection: Bureau of Building</u></p> <p><b>SCA TRA-4:</b> Transportation and Parking Demand Management Program (See under Section 4.13, <i>Transportation and Traffic - Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation</i>)</p> <p><b>SCA UTIL-6:</b> Water Efficient Landscapes (WELO) (See under Impact UTIL-3)</p>				



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.6 Greenhouse Gas Emissions and Climate Change (cont.)</b>					
<p><b>Impact GHG-2:</b> The proposed Project would not conflict with an applicable plan, policy or regulation of an appropriate regulatory agency adopted for the purpose of reducing greenhouse gas emissions (Criterion b). (Less than Significant with SCAs)</p>	<p><b>SCA AIR-1:</b> Construction related Air Pollutant Controls (Dust and Equipment Emissions) <i>(See under Impact AIR-1)</i></p> <p><b>SCA TRA-4:</b> Transportation and Parking Demand Management Program <i>(See under Section 4.13, Transportation and Traffic - Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation)</i></p> <p><b>SCA GHG-1:</b> Greenhouse Gas Reduction Plan (GGRP) <i>(See under Impact GHG-1)</i></p> <p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape <i>(See under Impact GHG-1)</i></p> <p><b>SCA UTIL-6:</b> Water Efficient Landscapes (WELO) <i>(See under Impact UTIL-3)</i></p>				
<b>4.7 Hazards and Hazardous Materials</b>					
<p><b>Impact HAZ-1:</b> The Project would include the routine transport, use and disposal of hazardous materials during construction and operation, but would not create a significant hazard to the public or the environment. (Criterion a) (Less than Significant with SCAs)</p>	<p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (#39). <i>During construction.</i> The project applicant shall ensure that Best Management Practices (BMPs) are implemented by the contractor during construction to minimize potential negative effects on groundwater, soils, and human health. These shall include, at a minimum, the following:</p> <ol style="list-style-type: none"> <li>Follow manufacture's recommendations for use, storage, and disposal of chemical products used in construction;</li> <li>Avoid overtopping construction equipment fuel gas tanks;</li> <li>During routine maintenance of construction equipment, properly contain and remove grease and oils;</li> <li>Properly dispose of discarded containers of fuels and other chemicals;</li> <li>Implement lead-safe work practices and comply with all local, regional, state, and federal requirements concerning lead (for more information refer to the Alameda County Lead Poisoning Prevention Program); and</li> </ol>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Implement BMPs to minimize potential negative effects on groundwater, soils, and human health.</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure regular verification of implementation of construction BMPs.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Conduct periodic site visits to verify that construction BMPs are implemented.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<b>Impact HAZ-1 (cont.)</b>	<p>f. If soil, groundwater, or other environmental medium with suspected contamination is encountered unexpectedly during construction activities (e.g., identified by odor or visual staining, or if any underground storage tanks, abandoned drums or other hazardous materials or wastes are encountered), the project applicant shall cease work in the vicinity of the suspect material, the area shall be secured as necessary, and the applicant shall take all appropriate measures to protect human health and the environment. Appropriate measures shall include notifying the City and applicable regulatory agency(ies) and implementation of the actions described in the City's Standard Conditions of Approval, as necessary, to identify the nature and extent of contamination. Work shall not resume in the area(s) affected until the measures have been implemented under the oversight of the City or regulatory agency, as appropriate.</p>				
	<p><b>SCA HAZ-2: Site Contamination (#40).</b></p> <p>a. <b>Environmental Site Assessment Required.</b> <i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Phase I Environmental Site Assessment report, and Phase II Environmental Site Assessment report if warranted by the Phase I report, for the project site for review and approval by the City. The report(s) shall be prepared by a qualified environmental assessment professional and include recommendations for remedial action, as appropriate, for hazardous materials. The project applicant shall implement the approved recommendations and submit to the City evidence of approval for any proposed remedial action and required clearances by the applicable local, state, or federal regulatory agency.</p> <p>b. <b>Health and Safety Plan Required.</b> <i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Health and Safety Plan for the review and approval by the City in order to protect project construction workers from risks associated with hazardous materials. The project applicant shall implement the approved Plan.</p>	<p><b>Submit Environmental Site Assessment and Health and Safety Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Approved Plans and BMPs:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and/or Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit and implement a Phase I Environmental Site Assessment report, and Phase II Environmental Site Assessment report if warranted, for the project site, per condition (a).</li> <li>• Submit and implement a Health and Safety Plan, per condition (b).</li> <li>• Ensure BMPs to minimize potential soil and groundwater hazards are implemented, per condition (c).</li> </ul>	<p><b>Applicant:</b></p> <ul style="list-style-type: none"> <li>• Ensure regular verification of the implementation of construction BMPs.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department - Hazardous Materials Division:</b></p> <ul style="list-style-type: none"> <li>• Review and approve Environmental Site Assessment reports.</li> <li>• Review and approve the Health and Safety Plan.</li> <li>• Conduct periodic site visits to verify that construction BMPs are implemented.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
Impact HAZ-1 (cont.)	<p>c. <b>Best Management Practices (BMPs) Required for Contaminated Sites.</b> <i>During construction.</i> The project applicant shall ensure that Best Management Practices (BMPs) are implemented by the contractor during construction to minimize potential soil and groundwater hazards. These shall include the following:</p> <ul style="list-style-type: none"> <li>i. Soil generated by construction activities shall be stockpiled on-site in a secure and safe manner. All contaminated soils determined to be hazardous or non-hazardous waste must be adequately profiled (sampled) prior to acceptable reuse or disposal at an appropriate off-site facility. Specific sampling and handling and transport procedures for reuse or disposal shall be in accordance with applicable local, state, and federal requirements.</li> <li>ii. Groundwater pumped from the subsurface shall be contained on-site in a secure and safe manner, prior to treatment and disposal, to ensure environmental and health issues are resolved pursuant to applicable laws and policies. Engineering controls shall be utilized, which include impermeable barriers to prohibit groundwater and vapor intrusion into the building.</li> </ul>				
	<p><b>SCA HAZ-3: Hazardous Materials Business Plan (#41).</b></p> <p><i>Prior to final building permit.</i> The project applicant shall submit a Hazardous Materials Business Plan for review and approval by the City, and shall implement the approved Plan. The approved Plan shall be kept on file with the City and the project applicant shall update the Plan as applicable. The purpose of the Hazardous Materials Business Plan is to ensure that employees are adequately trained to handle hazardous materials and provides information to the Fire Department should emergency response be required. Hazardous materials shall be handled in accordance with all applicable local, state, and federal requirements. The Hazardous Materials Business Plan shall include the following:</p> <ul style="list-style-type: none"> <li>a. The types of hazardous materials or chemicals stored and/or used on-site, such as petroleum fuel products, lubricants, solvents, and cleaning fluids.</li> </ul>	<p><b>Submit Hazardous Materials Business Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of final building permit.</li> </ul> <p><b>Conduct Work Per Approved Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit a Hazardous Materials Business Plan for review and approval by the City, and implement the approved Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department - Hazardous Materials Division:</b></p> <ul style="list-style-type: none"> <li>• Review and approve the Hazardous Materials Building Plan.</li> </ul>	



## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<b>Impact HAZ-1 (cont.)</b>	<p>b. The location of such hazardous materials.</p> <p>c. An emergency response plan including employee training information.</p> <p>A plan that describes the manner in which these materials are handled, transported, and disposed.</p>				
	<p><b>SCA HAZ-4:</b> Fire Safety Phasing Plan (#42).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Fire Safety Phasing Plan for City review and approval, and shall implement the approved Plan. The Fire Safety Phasing Plan shall include all of the fire safety features incorporated into each phase of the project and the schedule for implementation of the features.</p>	<p><b>Submit Fire Safety Phasing Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement a Fire Safety Phasing Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department - Hazardous Materials Division:</b></p> <ul style="list-style-type: none"> <li>Review and approve the Fire Safety Plan.</li> </ul>	
	<p><b>SCA HAZ-6:</b> Asbestos in Structures (#23).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall comply with all applicable laws and regulations regarding demolition and renovation of Asbestos Containing Materials (ACM), including but not limited to California Code of Regulations, Title 8; California Business and Professions Code, Division 3; California Health and Safety Code sections 25915-25919.7; and Bay Area Air Quality Management District, Regulation 11, Rule 2, as may be amended. Evidence of compliance shall be submitted to the City upon request.</p>	<p><b>Implementation:</b></p> <p>Ongoing, throughout all demolition activities for structures that may contain ACM.</p>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <p>Implement according to applicable regulatory agency with jurisdiction.</p>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>If structures that may contain ACM are planned for demolition, City will request evidence of compliance as needed.</li> </ul>	
<p><b>Impact HAZ-2:</b> The Project would not create a significant hazard to the public or environment through an upset or accident involving the release of hazardous materials. (Criterion b) (Less than Significant with SCAs)</p>	<p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (See under Impact HAZ-1)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (See under Impact HYD-1)</p> <p><b>SCA HAZ-4:</b> Fire Safety Phasing Plan (See under Impact HAZ-1)</p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<p><b>Impact HAZ-3:</b> The Project would not result in the new storage or use of acutely hazardous materials near sensitive receptors, and would not as a result create a significant hazard to the public. (Criterion c) (Less than Significant with SCAs)</p>	<p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (See under Impact HAZ-1)  <b>SCA HAZ-3:</b> Hazardous Materials Business Plan (See under Impact HAZ-1)</p>				
<p><b>Impact HAZ-4:</b> The Project would not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school. (Criterion d) (Less than Significant with SCAs)</p>	<p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (See under Impact HAZ-1)  <b>SCA HAZ-2:</b> Site Contamination (See under Impact HAZ-1)  <b>SCA HAZ-3:</b> Hazardous Materials Business Plan (See under Impact HAZ-1)</p>				
<p><b>Impact HAZ-5:</b> The Project would be located on a site that is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and could, but would not, result in a safety hazard to the public or environment. (Criterion e) (Less than Significant with SCAs )</p>	<p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (See under Impact HAZ-1)  <b>SCA HAZ-2:</b> Site Contamination (See under Impact HAZ-1)  <b>SCA Implementation Measure HAZ-2.1:</b> To further implement SCA HAZ-4, the project sponsor shall submit the results of any CLRRRA site assessment work required by DTSC. The Fire Prevention Bureau’s Hazardous Materials Division shall review and provide a determination on the completeness of the reports for the City’s purposes.</p>	<p><b>Submit CLRRRA Site Assessment:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of any construction-related permit.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit the results of any CLRRRA site assessment work required by DTSC.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department - Hazardous Materials Division:</b></p> <ul style="list-style-type: none"> <li>Review and determine completeness of the CLRRRA site assessment reports.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
Impact HAZ-5 (cont.)	<p><b>SCA Implementation Measure HAZ-2.2:</b> To further implement SCA HAZ-2, if DTSC determines that remediation pursuant to a CLRRA response plan is required, the project sponsor must:</p> <p>a) Submit documentation confirming that any remaining environmental assessment and remediation required by DTSC will be performed under the oversight of DTSC or other regulatory agencies, and will be conducted by qualified professionals with experience in soil and groundwater contamination remediation.</p> <p>b) The project sponsor shall submit a Soil Management Plan that has been reviewed and approved by DTSC or other appropriate regulatory agency. That plan shall outline required procedures for handling and disposing impacted soil. All disposal and transportation of contaminated soil shall be done in accordance with applicable state and federal laws and regulations. All contaminated soil determined to be hazardous or non-hazardous waste must be adequately profiled for acceptable disposal before it can be removed from the site. The project sponsor shall ensure that impacted soil is handled in accordance with the approved Soil Management Plan.</p> <p>c) If groundwater contamination is discovered at level in excess of applicable regulatory thresholds used by DTSC or other appropriate regulatory agency, ensure that groundwater pumped from the subsurface shall be contained onsite prior to treatment and disposal to ensure environmental and, if any, health issues are resolved pursuant to oversight agencies.</p> <p>d) If soil vapor contamination is discovered at levels that DTSC determines require remediation, and the source of the vapor is not removed pursuant to DTSC supervision, engineering controls shall be utilized, which include impermeable barriers to mitigate vapor intrusion into the building.</p> <p>e) The project sponsor shall provide written verification that the appropriate State, Federal or County authorities, including but not limited to DTSC and the Alameda County Public Health Department, have granted all required clearances and confirmed that all</p>	<p><b>Submit Pre-Work Verification Reports per measures (a), (b) and (f).</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of demolition or grading permits.</li> </ul> <p><b>Submit Post-work Verification Reports per measures (e) and (g).</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of any demolition permits for buildings containing lead-based paint.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit confirmation that qualified preparer and DTSC oversight of site assessment/remediation work, if applicable (a); a Soil Management Plan (b); compliance with the City of Oakland Hazardous Material Assessment and Reporting Program (f).</li> <li>Submit written verification of remediation compliance (e); and safe handling and disposal of lead-based paint d, if found, (g).</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department, Office of Emergency Services:</b></p> <ul style="list-style-type: none"> <li>Review and accept verification reports specified in the SCA</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<b>Impact HAZ-5 (cont.)</b>	<p>applicable standards, regulations, and conditions are in compliance, for all existing contamination at the site.</p> <p>f) The project sponsor shall provide evidence from the City's Fire Department, Office of Emergency Services, indicating compliance with the City of Oakland Hazardous Material Assessment and Reporting Program, pursuant to City Ordinance No. 12323.</p> <p>g) Prior to issuance of any demolition permits for buildings containing lead-based paint, the project sponsor shall demonstrate to the satisfaction of the Office of Fire Department, Office of Emergency Services, that the site has been investigated for the presence of lead will be handled and disposed of safely conduct during demolition.</p>				
	<p><b>SCA Implementation Measure HAZ-2.3:</b> To further implement SCA HAZ-2, pursuant to the Soils Management Plan required in SCA Implementation Measure HAZ-2.2b, the contractor shall cease any earthwork activities upon discovery of any suspect soils (e.g., petroleum odor and/or discoloration) during construction. The contractor shall notify DTSC and retain a qualified environmental firm to collect soil samples to confirm the level of contamination that may be present. If contamination is found to be present, any further proposed groundbreaking activities within areas of identified or suspected contamination shall be conducted according to a site specific health and safety plan, prepared by a licensed professional. The contractor shall follow all procedural direction given by DTSC to ensure that suspect soils are isolated, protected from runoff, and disposed of in accordance with transportation laws and the requirements of the licensed receiving facility.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Cease work upon discovery of suspect soils and comply with DTSC procedural direction.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department - Hazardous Materials Division:</b></p> <ul style="list-style-type: none"> <li>Verify all applicable measures are implemented.</li> </ul>	
	<p><b>SCA Implementation Measure HAZ-2.4:</b> To further implement SCA HAZ-2, if the assessment required by DTSC under CLRRRA finds presence of lead-based paint at levels not suitable for residential use in proposed residential areas or for commercial use in other areas, the project sponsor shall develop and implement a lead-based paint response plan under CLRRRA. The plan shall:</p> <p>a) Develop a removal specification approved by a Certified Lead Project Designer.</p>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit, if applicable.</li> </ul> <p><b>Implement Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement a lead-based paint response plan under CLRRRA, if applicable.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department - Hazardous Materials Division:</b></p> <ul style="list-style-type: none"> <li>Review and approve lead-based paint response plan</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<b>Impact HAZ-5 (cont.)</b>	b) Ensure that all removal workers are properly trained. c) Contain all work areas to prohibit off-site migration of paint chip debris. d) Remove all peeling and stratified lead-based paint from the Club Knoll building and any other existing non-building surfaces to the degree necessary to safely and properly complete relocation or demolition activities according to recommendations of the survey. The relocation contractor shall be responsible for the proper containment and disposal of intact lead-based paint on all equipment to be cut and/or removed during relocation or demolition. e) Provide on-site personnel and area air monitoring during all removal activities to ensure that workers and the environment are adequately protected by the control measures used. f) Clean up and/or vacuum paint chips with a high efficiency particulate air (HEPA) filter. g) Collect, segregate, and profile waste for disposal determination. h) Properly dispose of all waste.			<ul style="list-style-type: none"> <li>Review and approve report on results of lead-based paint response</li> <li>Verify all applicable measures are implemented.</li> </ul>	
<b>Impact HAZ-8:</b> The Project would not expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands. (Criterion j) (Less than Significant with SCA)	<p><b>SCA HAZ-4:</b> Fire Safety Phasing Plan (See under Impact HAZ-1)</p> <p><b>SCA HAZ-5</b> (Wildfire Prevention Area – Vegetation Management) (#43).</p> <p>a. <b>Vegetation Management Plan Required.</b> Prior to approval of construction-related permit. The project applicant shall submit a Vegetation Management Plan for City review and approval, and shall implement the approved Plan prior to, during, and after construction of the project. The Vegetation Management Plan may be combined with the Landscape Plan otherwise required by the Conditions of Approval. The Vegetation Management Plan shall include, at a minimum, the following measures:</p> <p>i. Removal of dead vegetation overhanging roof and chimney areas;</p>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit, if applicable.</li> </ul> <p><b>Implement Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit and implement Vegetation Management Plan, which can be part of the Landscape Plan (SCA AES-2).</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments); Oakland Fire Department:</b></p> <ul style="list-style-type: none"> <li>Review and approve Vegetation Management Plan.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<b>Impact HAZ-8 (cont.)</b>	<ul style="list-style-type: none"> <li>ii. Removal of leaves and needles from roofs;</li> <li>iii. Planting and placement of fire-resistant plants around the house and phasing out flammable vegetation;</li> <li>iv. Trimming back vegetation around windows;</li> <li>v. Removal of flammable vegetation on hillside slopes greater than 20%;</li> <li>vi. Pruning the lower branches of tall trees;</li> <li>vii. Clearing out ground-level brush and debris; and</li> <li>viii. Stacking woodpiles away from structures.</li> </ul>			<ul style="list-style-type: none"> <li>• Verify all applicable conditions are implemented.</li> </ul>	
	<p><b>SCA Implementation Measure HAZ-4.1:</b> To further implement SCA HAZ-4: Fire Safety: The project sponsor and construction contractor shall ensure that during Project construction, all construction vehicles and equipment will be fitted with spark arrestors to minimize accidental ignition of dry construction debris and surrounding dry vegetation.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Ensure spark arrestors are fitted on all construction vehicles and equipment.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; Oakland Fire Department:</b></p> <ul style="list-style-type: none"> <li>• Verify all applicable measures are implemented.</li> </ul>	
<p><b>Impact HAZ-9:</b> The Project would not have a considerable contribution to any cumulative impacts related to hazards and hazardous materials, considering the combined effect of the Project, and past, present, approved, pending, and reasonably foreseeable future projects in the area and citywide. (Less than Significant with SCAs)</p>	<p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (See under Impact HAZ-1)</p> <p><b>SCA HAZ-2:</b> Site Contamination (See under Impact HAZ-1)</p> <p><b>SCA Implementation Measure HAZ-2.1</b> (to further implement SCA HAZ-2) (see above)</p> <p><b>SCA Implementation Measure HAZ-2.2</b> (to further implement SCA HAZ-2) (see above)</p> <p><b>SCA Implementation Measure HAZ-2.3</b> (to further implement SCA HAZ-2) (see above)</p> <p><b>SCA Implementation Measure HAZ-2.4</b> (to further implement SCA HAZ-2) (see above)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (See under Impact HYD-1)</p>				



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.7 Hazards and Hazardous Materials (cont.)</b>					
<b>Impact HAZ-9</b> (cont.)	<p><b>SCA HAZ-3:</b> Hazardous Materials Business Plan (See under <i>Impact HAZ-1</i>)</p> <p><b>SCA HAZ-4:</b> Fire Safety Phasing Plan (See under <i>Impact HAZ-1</i>)</p> <p><b>SCA Implementation Measure HAZ-4.1</b> (to further implement SCA HAZ-4) (see above)</p> <p><b>SCA HAZ-5:</b> Wildfire Prevention Area – Vegetation Management (See under <i>Impact HAZ-8</i>)</p>				
<b>4.8 Hydrology and Water Quality</b>					
<p><b>Impact HYD-1:</b> Runoff from the proposed Project would be different from existing conditions; however, the Project would not violate any water quality standards or waste discharge requirements. (Criteria a and g) (Less than Significant with SCAs)</p>	<p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (#45).</p> <p>a) <i>Erosion and Sedimentation Control Plan Required.</i> The project applicant shall submit an Erosion and Sedimentation Control Plan to the City for review and approval. The Erosion and Sedimentation Control Plan shall include all necessary measures to be taken to prevent excessive stormwater runoff or carrying by stormwater runoff of solid materials on to lands of adjacent property owners, public streets, or to creeks as a result of conditions created by grading and/or construction operations. The Plan shall include, but not be limited to, such measures as short-term erosion control planting, waterproof slope covering, check dams, interceptor ditches, benches, storm drains, dissipation structures, diversion dikes, retarding berms and barriers, devices to trap, store and filter out sediment, and stormwater retention basins. Off-site work by the project applicant may be necessary. The project applicant shall obtain permission or easements necessary for off-site work. There shall be a clear notation that the plan is subject to changes as changing conditions occur. Calculations of anticipated stormwater runoff and sediment volumes shall be included, if required by the City. The Plan shall specify that, after construction is complete, the project applicant shall ensure that the storm drain system shall be inspected and that the project applicant shall clear the system of any debris or sediment.</p>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Post-Construction Inspection and Clearance:</b></p> <ul style="list-style-type: none"> <li>Prior to final permit.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit an Erosion and Sedimentation Control Plan.</li> <li>Obtain permission or easements necessary for off-site work.</li> <li>Ensure post-construction inspection and maintenance.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and confirm Erosion and Sedimentation Control Plan.</li> <li>Conduct post-construction inspection.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
Impact HYD-1 (cont.)	<p>b) <i>Erosion and Sedimentation Control During Construction.</i> The project applicant shall implement the approved Erosion and Sedimentation Control Plan. No grading shall occur during the wet weather season (October 15 through April 15) unless specifically authorized in writing by the Bureau of Building.</p>	<p><b>Conduct Work Pursuant to Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit an Erosion and Sedimentation Control Plan. (Also see SCA BIO-3 <i>Creek Protection Plan</i>; SCA BIO-4 <i>Dewatering/Diversion</i>.)</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Ensure implementation of Erosion and Sedimentation Control Plan.</li> <li>If applicable, authorize grading during wet weather season.</li> </ul>	
	<p><b>SCA HYD-2:</b> State Construction General Permit (#46).</p> <p>The project applicant shall comply with the requirements of the Construction General Permit issued by the State Water Resources Control Board (SWRCB). The project applicant shall submit a Notice of Intent (NOI), Stormwater Pollution Prevention Plan (SWPPP), and other required Permit Registration Documents to SWRCB. The project applicant shall submit evidence of compliance with Permit requirements to the City.</p>	<p><b>Submit Documents to SWRCB:</b></p> <ul style="list-style-type: none"> <li>Prior issuance of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved SWPPP and General Permit:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit a NOI, SWPPP, and other required Permit Registration Documents to SWRCB, and evidence of compliance to the City.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments); Public Works Agency – Environmental Services</b></p> <ul style="list-style-type: none"> <li>Verify compliance with all Permit requirements.</li> </ul>	
	<p><b>SCA HYD-3:</b> Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties (#47).</p> <p>The project applicant shall submit and implement a Drainage Plan to be reviewed and approved by the City. The Drainage Plan shall include measures to reduce the volume and velocity of post-construction stormwater runoff to the maximum extent practicable. Stormwater runoff shall not be augmented to adjacent properties, creeks, or storm drains. The Drainage Plan shall be included with the project drawings submitted to the City for site improvements.</p>	<p><b>Submit Drainage Plan:</b></p> <ul style="list-style-type: none"> <li>Concurrent with project site improvement plans.</li> </ul> <p><b>Implement Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement a post-construction stormwater Drainage Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approval Drainage Plan.</li> <li>Verify compliance with the Drainage Plan.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
<b>Impact HYD-1 (cont.)</b>	<p><b>SCA HYD-4:</b> Site Design Measures to Reduce Stormwater Runoff (#48).</p> <p>Pursuant to Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES), the project applicant is encouraged to incorporate appropriate site design measures into the project to reduce the amount of stormwater runoff. These measures may include, but are not limited to, the following:</p> <ol style="list-style-type: none"> <li>Minimize impervious surfaces, especially directly connected impervious surfaces and surface parking areas;</li> <li>Utilize permeable paving in place of impervious paving where appropriate;</li> <li>Cluster structures;</li> <li>Direct roof runoff to vegetated areas;</li> <li>Preserve quality open space; and</li> <li>Establish vegetated buffer areas.</li> </ol>	<p><b>Submit Design Measures in Project Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of any construction-related permit.</li> </ul> <p><b>Implement Design Measures:</b></p> <ul style="list-style-type: none"> <li>Prior to final permit approval.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement project plans incorporating stormwater runoff reduction design measures.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Verify incorporation of design measures into approved project plans.</li> <li>Verify ongoing implementation or and compliance with approved design measures.</li> </ul>	
	<p><b>SCA HYD-5:</b> Source Control Measures to Limit Stormwater Pollution (#49).</p> <p>Pursuant to Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES), the project applicant is encouraged to incorporate appropriate source control measures to limit pollution in stormwater runoff. These measures may include, but are not limited to, the following:</p> <ol style="list-style-type: none"> <li>Stencil storm drain inlets “No Dumping – Drains to Bay;”</li> <li>Minimize the use of pesticides and fertilizers;</li> <li>Cover outdoor material storage areas, loading docks, repair/maintenance bays and fueling areas;</li> <li>Cover trash, food waste, and compactor enclosures; and</li> <li>Plumb the following discharges to the sanitary sewer system, subject to City approval:</li> </ol>	<p><b>Submit Source Control Measures in Project Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of any construction-related permit.</li> </ul> <p><b>Implement Source Control Measures:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement project plans incorporating stormwater runoff source control measures.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Verify that the applicant complies with the requirements of Provision C.3 of the NPDES permit.</li> <li>Verify incorporation of source control measures into approved project plans.</li> <li>Verify ongoing implementation or and compliance with approved source control measures.</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
<b>Impact HYD-1 (cont.)</b>	<ul style="list-style-type: none"> <li>i. Discharges from indoor floor mats, equipment, hood filter, wash racks, and, covered outdoor wash racks for restaurants;</li> <li>ii. Dumpster drips from covered trash, food waste, and compactor enclosures;</li> <li>iii. Discharges from outdoor covered wash areas for vehicles, equipment, and accessories;</li> <li>iv. Swimming pool water, if discharge to on-site vegetated areas is not feasible; and</li> <li>v. Fire sprinkler test water, if discharge to on-site vegetated areas is not feasible.</li> </ul>				
	<p><b>SCA HYD-6:</b> NPDES C.3 Stormwater Requirements for Regulated Projects (#50).</p> <p>a) <i>Post-Construction Stormwater Management Plan Required.</i> The project applicant shall comply with the requirements of Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES). The project applicant shall submit a Post-Construction Stormwater Management Plan to the City for review and approval with the project drawings submitted for site improvements, and shall implement the approved Plan during construction. The Post-Construction Stormwater Management Plan shall include and identify the following:</p> <ul style="list-style-type: none"> <li>i. Location and size of new and replaced impervious surface;</li> <li>ii. Directional surface flow of stormwater runoff;</li> <li>iii. Location of proposed on-site storm drain lines;</li> <li>iv. Site design measures to reduce the amount of impervious surface area;</li> <li>v. Source control measures to limit stormwater pollution;</li> <li>vi. Stormwater treatment measures to remove pollutants from stormwater runoff, including the method used to hydraulically size the treatment measures; and</li> </ul>	<p><b>Submit Post-Construction Stormwater Management Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to issuance of any construction-related permit.</li> </ul> <p><b>Verify Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to final permit approval.</li> </ul> <p><b>Implement Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit Post-Construction Stormwater Management Plan with project improvement plans, and implement Plan.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>• Verify that the applicant complies with the requirements of Provision C.3 of the NPDES permit.</li> <li>• Verify that a completed Stormwater Supplemental Form and Post-Construction Stormwater Management Plan are adequately prepared.</li> <li>• Verify Plan is implemented.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
Impact HYD-1 (cont.)	<p>vii. Hydromodification management measures, if required by Provision C.3, so that post-project stormwater runoff flow and duration match pre-project runoff.</p> <p>b) <i>Maintenance Agreement Required.</i> The project applicant shall enter into a maintenance agreement with the City, based on the Standard City of Oakland Stormwater Treatment Measures Maintenance Agreement, in accordance with Provision C.3, which provides, in part, for the following:</p> <p>i. The project applicant accepting responsibility for the adequate installation/construction, operation, maintenance, inspection, and reporting of any on-site stormwater treatment measures being incorporated into the project until the responsibility is legally transferred to another entity; and</p> <p>ii. Legal access to the on-site stormwater treatment measures for representatives of the City, the local vector control district, and staff of the Regional Water Quality Control Board, San Francisco Region, for the purpose of verifying the implementation, operation, and maintenance of the on-site stormwater treatment measures and to take corrective action if necessary.</p> <p>The maintenance agreement shall be recorded at the County Recorder's Office at the applicant's expense.</p> <p><b>SCA HYD-7:</b> Architectural Copper (#52). The project applicant shall implement Best Management Practices (BMPs) concerning the installation, treatment, and maintenance of exterior architectural copper during and after construction of the project in order to reduce potential water quality impacts in accordance with Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES). The required BMPs include, but are not limited to, the following:</p>	<p><b>Enter and Record Agreement:</b></p> <ul style="list-style-type: none"> <li>• Prior to final permit approval.</li> </ul> <p><b>Implementations:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Enter into Standard City of Oakland Stormwater Treatment Measures Maintenance Agreement.</li> <li>• Record Agreement at the County Recorder's Office.</li> </ul> <p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Implement BMPs regarding exterior architectural copper.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspection</b></p> <ul style="list-style-type: none"> <li>• Verify that the applicant has entered into the "Standard City of Oakland Stormwater Treatment Measures Maintenance Agreement."</li> <li>• Verify that BMPs are implemented.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
<b>Impact HYD-1 (cont.)</b>	<ul style="list-style-type: none"> <li>a) If possible, use copper materials that have been pre-patinated at the factory;</li> <li>b) If patination is done on-site, ensure rinse water is not discharged to the storm drain system by protecting storm drain inlets and implementing one or more of the following:</li> <li>c) Discharge rinse water to landscaped area;</li> <li>d) Collect rinse water in a tank and discharge to the sanitary sewer, with approval by the City; or haul off-site for proper disposal;</li> <li>e) During maintenance activities, protect storm drain inlets to prevent wash water discharge into storm drains; and</li> <li>f) Consider coating the copper with an impervious coating that prevents further corrosion.</li> </ul> <p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p> <p><b>SCA BIO-4:</b> Creek Dewatering/Diversion <i>(See under Impact BIO-4)</i></p> <p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction <i>(See under Impact HAZ-1)</i></p> <p><b>SCA HAZ-2:</b> Site Contamination <i>(See under Impact HAZ-1)</i></p> <p><b>SCA UTIL-6:</b> Water Efficient Landscapes (WELO) <i>(See under Impact UTIL-3)</i></p> <p>No Mitigation Measure Required</p>				
<b>Impact HYD-2:</b> The Project would not substantially deplete groundwater supplies or interfere with groundwater recharge. (Criterion b) (Less than Significant with SCA)	<b>SCA BIO-4:</b> Creek Dewatering/Diversion <i>(See under Impact BIO-4)</i>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
<p><b>Impact HYD-3:</b> The Project would not result in substantial erosion or siltation on- or off-site that would affect the quality of receiving waters. (Criteria c and g) (Less than Significant with SCAs)</p>	<p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-3:</b> Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-4:</b> Site Design Measures to Reduce Stormwater Runoff (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-5:</b> Source Control Measures to Limit Stormwater Pollution (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-6:</b> NPDES C.3 Stormwater Requirements for Regulated Projects (<i>See under Impact HYD-1</i>)</p>				
	<p><b>SCA HYD-8:</b> Vegetation Management on Creekside Properties (#53).</p> <p><i>Ongoing.</i> The project applicant shall comply with the following requirements when managing vegetation prior to, during, and after construction of the project:</p> <ol style="list-style-type: none"> <li>Identify and leave "islands" of vegetation in order to prevent erosion and landslides and protect habitat;</li> <li>Trim tree branches from the ground up (limbing up) and leave tree canopy intact;</li> <li>Leave stumps and roots from cut down trees to prevent erosion;</li> <li>Plant fire-appropriate, drought-tolerant, preferably native vegetation;</li> <li>Provide erosion and sediment control protection if cutting vegetation on a steep slope;</li> <li>Fence off sensitive plant habitats and creek areas if implementing goat grazing for vegetation management;</li> </ol>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant for Creekside Lots:</b></p> <ul style="list-style-type: none"> <li>Implement vegetation management requirements. (Also see SCA HAZ-5 <i>Wildfire Prevention Area Vegetation Management Plan</i>).</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Verify all applicable conditions are implemented and maintained.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

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<b>4.8 Hydrology and Water Quality (cont.)</b>					
<b>Impact HYD-3 (cont.)</b>	<ul style="list-style-type: none"> <li>g. Obtain a Tree Permit before removing a Protected Tree (any tree 9 inches dbh or greater and any oak tree 4 inches dbh or greater, except eucalyptus and Monterey pine);</li> <li>h. Do not clear-cut vegetation. This can lead to erosion and severe water quality problems and destroy important habitat;</li> <li>i. Do not remove vegetation within 20 feet of the top of the creek bank. If the top of bank cannot be identified, do not cut within 50 feet of the centerline of the creek or as wide a buffer as possible between the creek centerline and the development;</li> <li>j. Do not trim/prune branches that are larger than 4 inches in diameter;</li> <li>k. Do not remove tree canopy;</li> <li>l. Do not dump cut vegetation in the creek;</li> <li>m. Do not cut tall shrubbery to less than 3 feet high; and</li> <li>n. Do not cut short vegetation (e.g., grasses, ground-cover) to less than 6 inches high.</li> </ul>				
<b>Impact HYD-4:</b> The Project would not result in substantial flooding on or off-site. (Criterion d) (Less than Significant with SCAs)	<p><b>SCA HYD-3:</b> Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-4:</b> Site Design Measures to Reduce Stormwater Runoff (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-5:</b> Source Control Measures to Limit Stormwater Pollution (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-6:</b> NPDES C.3 Stormwater Requirements for Regulated Projects (<i>See under Impact HYD-1</i>)</p> <p><b>SCA BIO-3:</b> Creek Protection Plan (<i>See under Impact BIO-2</i>)</p> <p>No Mitigation Measure Required</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
<p><b>Impact HYD-5:</b> The Project would not create or contribute substantial runoff which would exceed the capacity of existing or planned stormwater drainage systems or would be an additional source of polluted runoff. (Criteria e and f) (Less than Significant with SCAs)</p>	<p><b>SCA HYD-3:</b> Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-4:</b> Site Design Measures to Reduce Stormwater Runoff (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-5:</b> Source Control Measures to Limit Stormwater Pollution (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-6:</b> NPDES C.3 Stormwater Requirements for Regulated Projects (<i>See under Impact HYD-1</i>)</p>				
<p><b>Impact HYD-6:</b> The Project would not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course, or increasing the rate or amount of flow, of a creek, river, or stream in a manner that would result in substantial erosion, siltation, or flooding, both on- or offsite. (Criterion letter "I") (Less Than Significant with SCAs)</p>	<p><i>The proposed Project is not located within a Flood Zone. However, if revisions to the flood zone maps occur, including as a result of the creek restoration, this SCA shall apply.</i></p> <p><b>SCA HYD-9:</b> Structures in a Flood Zone (#56).</p> <p><i>Prior to approval of construction-related permit. The project shall be designed to ensure that new structures within a 100-year flood zone do not interfere with the flow of water or increase flooding. The project applicant shall submit plans and hydrological calculations for City review and approval with the construction-related drawings that show finished site grades and floor elevations elevated above the Base Flood Elevation (BFE).</i></p>	<p><b>Submit Project Design Plans and Hydrological Calculations:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of any construction-related permit.</li> </ul> <p><b>Verification:</b></p> <ul style="list-style-type: none"> <li>Prior to final permit approval.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit FDP plans and hydrological calculations that show that no new structures within a 100-year flood zone would interfere with water flow or increase flooding, and implement plans.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Verify that the sponsor complies with the requirements of the SCA.</li> <li>Verify plans are implemented.</li> </ul>	
	<p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-3:</b> Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-4:</b> Site Design Measures to Reduce Stormwater Runoff (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-5:</b> Source Control Measures to Limit Stormwater Pollution (<i>See under Impact HYD-1</i>)</p>				



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<b>4.8 Hydrology and Water Quality (cont.)</b>					
<b>Impact HYD-6</b> (cont.)	<p><b>SCA HYD-6:</b> NPDES C.3 Stormwater Requirements for Regulated Projects <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-7:</b> Architectural Copper <i>(See under Impact HYD-1)</i></p> <p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p> <p><b>SCA BIO-4:</b> Creek Dewatering/Diversion <i>(See under Impact BIO-4)</i></p> <p>No Mitigation Measure Required</p>				
<b>Impact HYD-7:</b> The Project would not fundamentally conflict with the City of Oakland Creek Protection Ordinance (OMC Chapter 13.16) intended to protect hydrologic resources. (Criterion m) (Less than Significant with SCAs)	<p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p>				
<b>Impact HYD-8:</b> The Project would not have a considerable contribution to any cumulative impacts related to hydrology and water quality, considering the combined effect of the Project and past, present, approved, pending, and reasonably foreseeable future projects in the relevant geographic area. (Less than Significant with SCAs)	<p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-2:</b> State Construction General Permit <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-3:</b> Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-4:</b> Site Design Measures to Reduce Stormwater Runoff <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-5:</b> Source Control Measures to Limit Stormwater Pollution <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-6:</b> NPDES C.3 Stormwater Requirements for Regulated Projects <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-7:</b> Architectural Copper <i>(See under Impact HYD-1)</i></p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.8 Hydrology and Water Quality (cont.)</b>					
<b>Impact HYD-8 (cont.)</b>	<p><b>SCA HYD-8:</b> Vegetation Management on Creekside Properties (<i>See under Impact HYD-3</i>)</p> <p><b>SCA BIO-3:</b> Creek Protection Plan (<i>See under Impact BIO-2</i>)</p> <p><b>SCA BIO-4:</b> Dewatering/Diversion (<i>See under Impact BIO-4</i>)</p> <p><b>SCA HAZ-1:</b> Hazardous Materials Related to Construction (<i>See under Impact HAZ-1</i>)</p> <p><b>SCA HAZ-2:</b> Site Contamination (<i>See under Impact HAZ-1</i>)</p> <p><b>SCA UTIL-6:</b> Water Efficient Landscapes (WELO) (<i>See under Impact UTIL-3</i>)</p>				
<b>4.10 Noise and Vibration</b>					
<p><b>Impact NOI-1:</b> Construction of the proposed Project would not result in substantial temporary or periodic increases in ambient noise or vibration levels in the Area above existing levels or in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies. (Criteria a, b, and h) (Less than Significant with SCAs)</p>	<p><b>SCA NOI-1:</b> Construction Days/Hours (#58). <i>During construction.</i> The Project applicant shall comply with the following restrictions concerning construction days and hours:</p> <p>a. Construction activities are limited to between 7:00 a.m. and 7:00 p.m. Monday through Friday, except that pier drilling and/or other extreme noise generating activities greater than 90 dBA shall be limited to between 8:00 a.m. and 4:00 p.m.</p> <p>b. Construction activities are limited to between 9:00 a.m. and 5:00 p.m. on Saturday. In residential zones and within 300 feet of a residential zone, construction activities are allowed from 9:00 a.m. to 5:00 p.m. only within the interior of the building with the doors and windows closed. No pier drilling or other extreme noise generating activities greater than 90 dBA are allowed on Saturday.</p> <p>c. No construction is allowed on Sunday or federal holidays.</p> <p>Construction activities include, but are not limited to, truck idling, moving equipment (including trucks, elevators, etc.) or materials, deliveries, and construction meetings held on-site in a non-enclosed area.</p>	<p><b>Submit Source Control Measures in Project Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to issuance of any construction-related permit.</li> </ul> <p><b>Implement Source Control Measures:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement project plans incorporating stormwater runoff source control measures.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Verify that the applicant complies with the requirements of Provision C.3 of the NPDES permit.</li> <li>Verify incorporation of source control measures into approved project plans.</li> <li>Verify ongoing implementation or and compliance with approved source control measures.</li> </ul>	

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<b>4.10 Noise and Vibration (cont.)</b>					
<b>Impact NOI-1 (cont.)</b>	Any construction activity proposed outside of the above days and hours for special activities (such as concrete pouring which may require more continuous amounts of time) shall be evaluated on a case-by-case basis by the City, with criteria including the urgency/emergency nature of the work, the proximity of residential or other sensitive uses, and a consideration of nearby residents'/occupants' preferences. The Project applicant shall notify property owners and occupants located within 300 feet at least 14 calendar days prior to construction activity proposed outside of the above days/hours. When submitting a request to the City to allow construction activity outside of the above days/hours, the Project applicant shall submit information concerning the type and duration of proposed construction activity and the draft public notice for City review and approval prior to distribution of the public notice.				
	<p><b>SCA NOI-2: Construction Noise (#59).</b></p> <p><i>During construction.</i> The Project applicant shall implement noise reduction measures to reduce noise impacts due to construction. Noise reduction measures include, but are not limited to, the following:</p> <ol style="list-style-type: none"> <li>Equipment and trucks used for project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically-attenuating shields or shrouds) wherever feasible.</li> <li>Except as provided herein, impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for project construction shall be hydraulically or electrically powered to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used; this muffler can lower noise levels from the exhaust by up to about 10 dBA. External jackets on the tools themselves shall be used, if such jackets are commercially available, and this could achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than</li> </ol>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Require construction contractors limit standard construction activities.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Verify construction activity noise is appropriately controlled.</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.10 Noise and Vibration (cont.)</b>					
<b>Impact NOI-1 (cont.)</b>	<p>impact equipment, whenever such procedures are available and consistent with construction procedures.</p> <p>c. Applicant shall use temporary power poles instead of generators where feasible.</p> <p>d. Stationary noise sources shall be located as far from adjacent properties as possible, and they shall be muffled and enclosed within temporary sheds, incorporate insulation barriers, or use other measures as determined by the City to provide equivalent noise reduction.</p> <p>e. The noisiest phases of construction shall be limited to less than 10 days at a time. Exceptions may be allowed if the City determines an extension is necessary and all available noise reduction controls are implemented.</p>				
	<p><b>SCA NOI-3: Extreme Construction Noise (#60).</b></p> <p>a. <b>Construction Noise Management Plan Required.</b> <i>Prior to approval of construction-related permit.</i> Prior to any extreme noise generating construction activities (e.g., pier drilling, pile driving and other activities generating greater than 90dBA), the project applicant shall submit a Construction Noise Management Plan prepared by a qualified acoustical consultant for City review and approval that contains a set of site-specific noise attenuation measures to further reduce construction impacts associated with extreme noise generating activities. The project applicant shall implement the approved Plan during construction. Potential attenuation measures include, but are not limited to, the following:</p> <p>i. Erect temporary plywood noise barriers around the construction site, particularly along on sites adjacent to residential buildings;</p> <p>ii. Implement “quiet” pile driving technology (such as pre-drilling of piles, the use of more than one pile driver to shorten the total pile driving duration), where feasible, in consideration of geotechnical and structural requirements and conditions;</p>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit and implement a Construction Noise Management Plan prepared by a qualified acoustical consultant to address extreme noise generating activities. (Also see SCA NOI-4.)</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>• Review and approve Plan if required noise attenuation will be achieved.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.10 Noise and Vibration (cont.)</b>					
<b>Impact NOI-1 (cont.)</b>	iii. Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site;  iv. Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings by the use of sound blankets for example <b>and implement such measure if such measures are feasible and would noticeably reduce noise impacts</b> ; and  v. Monitor the effectiveness of noise attenuation measures by taking noise measurements.				
	b. <b>Public Notification Required. During construction.</b> The project applicant shall notify property owners and occupants located within 300 feet of the construction activities at least 14 calendar days prior to commencing extreme noise generating activities. Prior to providing the notice, the project applicant shall submit to the City for review and approval the proposed type and duration of extreme noise generating activities and the proposed public notice. The public notice shall provide the estimated start and end dates of the extreme noise generating activities and describe noise attenuation measures to be implemented.	<b>Conduct Work Pursuant to Approved Measures:</b> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<b>Master Developer and Each FDP Project Applicant:</b> <ul style="list-style-type: none"> <li>Submit description of type and duration of extreme noise generating activities, noise attenuation measures, and the proposed public notice.</li> <li>Post public notice of extreme noise generating activities.</li> </ul>	<b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b> <ul style="list-style-type: none"> <li>Review and approve proposed noise attenuation measures and public notice.</li> </ul>	
	<b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures (#61).  <i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Construction Noise Management Plan prepared by a qualified acoustical consultant for City review and approval that contains a set of site-specific noise attenuation measures to further reduce construction noise impacts. The project applicant shall implement the approved Plan during construction.	<b>Submit Plan:</b> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <b>Conduct Work Pursuant to Approved Plan:</b> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<b>Master Developer and Each FDP Project Applicant:</b> <ul style="list-style-type: none"> <li>Submit and implement a Construction Noise Management Plan prepared by a qualified acoustical consultant (same as required for SCA NOI-3).</li> </ul>	<b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b> <ul style="list-style-type: none"> <li>Review and approve Plan if required noise attenuation will be achieved.</li> <li>Verify compliance with the Plan.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.10 Noise and Vibration (cont.)</b>					
<b>Impact NOI-1 (cont.)</b>	<p><b>SCA NOI-5:</b> Construction Noise Complaints (#62).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall submit to the City for review and approval a set of procedures for responding to and tracking complaints received pertaining to construction noise, and shall implement the procedures during construction. At a minimum, the procedures shall include:</p> <ol style="list-style-type: none"> <li>Designation of an on-site construction complaint and enforcement manager for the project;</li> <li>A large on-site sign near the public right-of-way containing permitted construction days/hours, complaint procedures, and phone numbers for the project complaint manager and City Code Enforcement unit;</li> <li>Protocols for receiving, responding to, and tracking received complaints; and</li> <li>Maintenance of a complaint log that records received complaints and how complaints were addressed, which shall be submitted to the City for review upon the City's request.</li> </ol>	<p><b>Submit Procedures:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement procedures for responding to and tracking construction noise complaints.</li> <li>Maintain log of complaints and actions taken.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approve construction noise complaints procedures.</li> <li>As needed, request complaint log for review.</li> </ul>	
	<p><b>SCA NOI-8:</b> Exposure to Vibration (#65).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Vibration Reduction Plan prepared by a qualified acoustical consultant for City review and approval that contains vibration reduction measures to reduce groundborne vibration to acceptable levels per Federal Transit Administration (FTA) standards. The applicant shall implement the approved Plan during construction. Potential vibration reduction measures include, but are not limited to, the following:</p> <ol style="list-style-type: none"> <li>Isolation of foundation and footings using resilient elements such as rubber bearing pads or springs, such as a "spring isolation" system that consists of resilient spring supports that can support the podium or residential foundations. The specific system shall be selected so that it can properly support the structural loads, and provide adequate filtering of groundborne vibration to the residences above.</li> <li>Trenching, which involves excavating soil between the railway and the project so that the vibration path is</li> </ol>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement a Vibration Reduction Plan prepared by a qualified acoustical consultant.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approve Plan.</li> <li>Verify compliance with the Plan.</li> </ul>	



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.10 Noise and Vibration (cont.)</b>					
<b>Impact NOI-1 (cont.)</b>	<p>c. interrupted, thereby reducing the vibration levels before they enter the project's structures. Since the reduction in vibration level is based on a ratio between trench depth and vibration wavelength, additional measurements shall be conducted to determine the vibration wavelengths affecting the project. Based on the resulting measurement findings, an adequate trench depth and, if required, suitable fill shall be identified (such as foamed styrene packing pellets [i.e., Styrofoam] or low-density polyethylene).</p> <p><b>SCA NOI-9:</b> Vibration Impacts on Adjacent Historic Structures or Vibration-Sensitive Activities (#66).</p> <p><i>Prior to construction.</i> The project applicant shall submit a Vibration Analysis prepared by an acoustical and/or structural engineer or other appropriate qualified professional for City review and approval that establishes pre-construction baseline conditions and threshold levels of vibration that could substantially interfere with activities located at the Project site and/or the historic Club Knoll building. The Vibration Analysis shall identify design means and methods of construction that shall be utilized in order to not exceed the thresholds. The applicant shall implement the recommendations during construction.</p>	<p><b>Submit Analysis:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Analysis:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement Vibration Analysis prepared by appropriate qualified professional.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approve construction design means and methods identified in the Analysis.</li> <li>Verify compliance with construction design means and methods identified in the Analysis.</li> </ul>	
<b>Impact NOI-2:</b> The proposed Project would not increase operational noise levels in the project vicinity to levels in excess of standards established in the Oakland Noise Ordinance (Oakland Planning Code Section 17.120.050) regarding operational noise. (Criterion c) (Less than Significant with SCA)	<p><b>SCA NOI-7:</b> Operational Noise (#64).</p> <p>Noise levels from the Project site after completion of the Project (i.e., during Project operation) shall comply with the performance standards of chapter 17.120 of the Oakland Planning Code and chapter 8.18 of the Oakland Municipal Code. If noise levels exceed these standards, the activity causing the noise shall be abated until appropriate noise reduction measures have been installed and compliance verified by the City.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Ensure noise levels from the activity, property, or any mechanical equipment on site shall comply with the performance standards described in the SCA.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>If noise levels exceed performance standards, verify compliance after action is taken by project applicant to regain compliance as described in the SCA.</li> </ul>	

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.10 Noise and Vibration (cont.)</b>					
<p><b>Impact NOI-3:</b> The proposed Project would not propose land uses in conflict with the land use compatibility guidelines of the Oakland General Plans. (Criterion f) (Less than Significant with SCA)</p>	<p><b>SCA NOI-6:</b> Exposure to Community Noise (#63). <i>Prior to approval of construction-related permit.</i> The project applicant shall submit a Noise Reduction Plan prepared by a qualified acoustical engineer for City review and approval that contains noise reduction measures (e.g., sound-rated window, wall, and door assemblies) to achieve an acceptable interior noise level in accordance with the land use compatibility guidelines of the Noise Element of the Oakland General Plan. The applicant shall implement the approved Plan during construction. To the maximum extent practicable, interior noise levels shall not exceed the following:</p> <ol style="list-style-type: none"> <li>45 dBA: Residential activities, civic activities, hotels</li> <li>50 dBA: Administrative offices; group assembly activities</li> <li>55 dBA: Commercial activities</li> <li>65 dBA: Industrial activities</li> </ol>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Pursuant to Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement a Noise Reduction Plan prepared by a qualified acoustical consultant.</li> <li>Ensure acceptable interior noise level comply with the performance standards described in the SCA.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments):</b></p> <ul style="list-style-type: none"> <li>Review and approve Plan.</li> <li>Verify compliance with the Plan.</li> </ul>	
<p><b>Impact NOI-4:</b> The proposed Project would not expose persons to interior Ldn or CNEL greater than 45 dBA for residential dwellings to noise levels in excess of standards established in the Oakland Noise Ordinance and Planning Code or the California Noise Insulation Standards. (Criterion e) (Less than Significant with SCAs)</p>	<p><b>SCA NOI-6:</b> Exposure to Community Noise (<i>See under Impact NOI-3</i>)</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.10 Noise and Vibration (cont.)</b>					
<p><b>Impact NOI-6:</b> Traffic generated by the proposed Project, in combination with traffic from past, present, existing, approved, pending and reasonably foreseeable future projects, would not substantially increase ambient noise levels in the Project Area; and construction and operational noise levels from the Project combined with noise levels from past, present, existing, approved, pending and reasonably foreseeable future projects, could increase ambient noise, but to less than significant levels. (Less than Significant with SCAs)</p>	<p><b>SCA NOI-1:</b> Construction Days/Hours (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-2:</b> Construction Noise (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-3:</b> Extreme Construction Noise (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-5:</b> Construction Noise Complaints (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-6:</b> Exposure to Community Noise (<i>See under Impact NOI-3</i>)</p> <p><b>SCA NOI-7:</b> Operational Noise (<i>See under Impact NOI-2</i>)</p> <p><b>SCA NOI-8:</b> Exposure to Vibration (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-9:</b> Vibration Impacts on Adjacent Historic Structures or Vibration-Sensitive Activities (<i>See under Impact NOI-1</i>)</p>				
<p><b>Impact NOI-7:</b> The proposed Project would not have stationary noise sources (such as rooftop mechanical equipment and back-up generators) that, in combination with traffic generated by the proposed Project; and from past, present, existing, approved, pending and reasonably foreseeable future projects would result in a significant cumulative impact. (Criterion d, cumulative, combined sources) (Less than Significant with SCAs)</p>	<p><b>SCA NOI-6:</b> Exposure to Community Noise (<i>See under Impact NOI-3</i>)</p> <p><b>SCA NOI-7:</b> Operational Noise (<i>See under Impact NOI-2</i>)</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.12 Public Services and Recreation</b>					
<p><b>Impact PSR-1:</b> The proposed Project would result in an increase in demand for fire protection and emergency medical response services that would not require new or physically altered fire protection facilities in order to maintain acceptable performance objectives. (Criterion a.1) (Less than Significant with SCAs)</p>	<p><b>SCA PSR-1:</b> Compliance with Other Requirements (#3). The project applicant shall comply with all other applicable federal, state, regional, and local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City's Bureau of Building, Fire Marshal, and Public Works Department. Compliance with other applicable requirements may require changes to the approved use and/or plans. These changes shall be processed in accordance with the procedures contained in Condition #4.</p>	<p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Comply with all other applicable federal, state, regional and/or local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections (for single lot developments); Oakland Fire Department; Oakland Public Works Agency:</b></p> <ul style="list-style-type: none"> <li>• Confirm compliance with all applicable codes, regulations, requirements, and guidelines.</li> <li>• Process any changes to requirements, regulations, and guidelines in accordance with Oakland SCA #4 Minor and Major Changes except as otherwise provided in a Development Agreement.</li> </ul>	
	<p><b>SCA HAZ-4:</b> Fire Safety Plan (See under Impact HAZ-1)</p> <p><b>SCA HAZ-5:</b> Wildfire Prevention Area – Vegetation Management (See under Impact HAZ-8)</p>				
<p><b>Impact PSR-5:</b> The proposed Project would include new recreational facilities; however, the construction and long-term use of these facilities would not have an adverse physical effect on the environment. (Criterion c) (Less than Significant with SCAs)</p>	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) (See under Impact AIR-1)</p> <p><b>SCA BIO-1:</b> Tree Removal during Breeding Bird Season (See under Impact BIO-1)</p> <p><b>SCA BIO-3:</b> Creek Protection Plan (See under Impact BIO-2)</p> <p><b>SCA BIO-4:</b> Dewatering/Diversion (See under Impact BIO-4)</p> <p><b>SCA CUL-2:</b> Archaeological and Paleontological Resources – Discovery During Construction (See under Impact CUL-3)</p>				



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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.12 Public Services and Recreation (cont.)</b>					
<b>Impact PSR-5 (cont.)</b>	<p><b>SCA CUL-3:</b> Archaeologically Sensitive Areas – Pre-Construction Measures <i>(See under Impact CUL-5)</i></p> <p><b>SCA GEO-3:</b> Construction-Related Permit(s) <i>(See under Impact GEO-4)</i></p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-2:</b> State Construction General Permit <i>(See under Impact HYD-1)</i></p> <p><b>SCA NOI-1:</b> Construction Days/Hours <i>(See under Impact NOI-1)</i></p> <p><b>SCA NOI-2:</b> Construction Noise <i>(See under Impact NOI-1)</i></p> <p><b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures <i>(See under Impact NOI-1)</i></p> <p><b>SCA NOI-5:</b> Construction Noise Complaints <i>(See under Impact NOI-1)</i></p> <p><b>SCA PSR-1:</b> Compliance with Other Requirements <i>(See under Impact PSR-1)</i></p> <p><b>SCA TRA-1:</b> Construction Activity in the Public Right-of-Way <i>(See under Section 4.13, Transportation and Traffic - Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation)</i></p>				
<b>Impact PSR-6:</b> The proposed Project, in combination with other past, present, existing, approved, pending, and reasonably foreseeable future projects within and around the Project area, would not result in an adverse cumulative increase in demand for public services or recreational facilities. (Less than Significant with SCAs)	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) <i>(See under Impact AIR-1)</i></p> <p><b>SCA BIO-1:</b> Tree Removal during Breeding Bird Season <i>(See under Impact BIO-1)</i></p> <p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p> <p><b>SCA BIO-4:</b> Dewatering/Diversion <i>(See under Impact BIO-4)</i></p> <p><b>SCA CUL-2:</b> Archaeological and Paleontological Resources – Discovery During Construction <i>(See under Impact CUL-3)</i></p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.12 Public Services and Recreation (cont.)</b>					
<b>Impact PSR-6 (cont.)</b>	<p><b>SCA CUL-3:</b> Archaeologically Sensitive Areas – Pre-Construction Measures; Provision B: Construction ALERT Sheet (<i>See under Impact CUL-5</i>)</p> <p><b>SCA GEO-3:</b> Construction-Related Permit(s) (<i>See under Impact GEO-4</i>)</p> <p><b>SCA HAZ-4:</b> Fire Safety Plan (<i>See under Impact HAZ-1</i>)</p> <p><b>SCA HAZ-5:</b> Wildfire Prevention Area – Vegetation Management (<i>See under Impact HAZ-8</i>)</p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (<i>See under Impact HYD-1</i>)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (<i>See under Impact HYD-1</i>)</p> <p><b>SCA NOI-1:</b> Construction Days/Hours (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-2:</b> Construction Noise (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-3:</b> Extreme Construction Noise (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-5:</b> Construction Noise Complaints (<i>See under Impact NOI-1</i>)</p> <p><b>SCA PSR-1:</b> Compliance with Other Requirements (<i>See under Impact PSR-1</i>)</p> <p><b>SCA TRA-1</b> Construction Activity in the Public Right-of-Way (<i>See under Section 4.13, Transportation and Traffic - Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation</i>)</p>				

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Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic</b>					
<p><b>Impact TRANS-1:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Eastbound On Ramp/Seminary Avenue/Kuhnle Avenue (intersection #2) and after project completion, this intersection would continue to satisfy the MUTCD peak hour volume traffic signal warrant during the PM peak hour (Criterion f). This intersection operates at LOS E during the AM and PM peak hours, and meets the peak hour signal warrant during the PM peak hour under Existing conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-1:</b> Implement the following measures at the I-580 Eastbound On-Ramp/Seminary Avenue/Kuhnle Avenue intersection:</p> <p>a) Signalize intersection providing actuated operations, with split phasing on all approaches to maximize the green time within each cycle for the southbound turning movements, and</p> <p>b) Coordinate the signal timing at this intersection with the adjacent intersection at I-580 Westbound Off-Ramp/Mountain Boulevard/Kuhnle Avenue (intersection #3, signalization proposed as part of Mitigation Measure TRANS-2). This intersection is under the jurisdiction of Caltrans so any equipment or facility upgrades must be approved by Caltrans prior to installation.</p> <p>To implement this measure, the following shall be submitted to the City of Oakland’s Transportation Services Division and Caltrans for review and approval:</p> <ul style="list-style-type: none"> <li>• Plans, Specifications, and Estimates (PS&amp;E) to modify intersection. All elements shall be designed to City and Caltrans standards in effect at the time of construction and all new or upgraded signals should include these enhancements. All other facilities supporting vehicle travel and alternative modes through the intersection should be brought up to both City standards and Americans with Disabilities Act (ADA) standards (according to Federal and State Access Board guidelines) at the time of construction. Current City Standards call for the elements listed below:                             <ul style="list-style-type: none"> <li>– 2070L Type Controller with cabinet assembly</li> <li>– GPS communications (clock)</li> <li>– Accessible pedestrian crosswalks according to Federal and State Access Board guidelines with signals (audible and tactile)</li> <li>– Countdown pedestrian head module switch out</li> </ul> </li> </ul>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>• By approximately 35 percent of project buildout or 390 equivalent housing units (EHU), whichever occurs first.</li> <li>• Alternatively, the City may implement this mitigation measure prior to the time the 35 percent buildout/390 EHU trigger occurs.</li> <li>• In either case, the applicant and/or project developer/s shall provide funds (or confirm a funding mechanism) to support implementation.</li> </ul> <p><b>Pay Fair Share Contribution or Confirm Funding Mechanism:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger or as negotiated by the Development Agreement between the City and the Project sponsor.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Submit PS&amp;E and signal timing plans.</li> <li>• Track Master Plan project buildout by percentage and EHU.</li> <li>• Pay or confirm a funding mechanism to pay the City a fair share contribution to implement the specified improvements; or as negotiated by the Development Agreement between the City and the Project sponsor.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>• Review and approve PS&amp;E and signal timing plans.</li> <li>• Establish fair share amount and confirm receipt of funds or confirmed funding mechanism.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Impact TRANS-1 (cont.)</b></p>	<ul style="list-style-type: none"> <li>- City standard ADA wheelchair ramps</li> <li>- Video detection on existing (or new, if required)</li> <li>- Mast arm poles, full actuation (where applicable)</li> <li>- Polara push buttons (full actuation)</li> <li>- Bicycle detection (full actuation)</li> <li>- Pull boxes</li> <li>- Signal interconnect and communication with trenching (where applicable), or through (E) conduit (where applicable)- 600 feet maximum</li> <li>- Conduit replacement contingency</li> <li>- Fiber Switch</li> <li>- PTZ Camera (where applicable)</li> <li>- Transit Signal Priority (TSP) equipment consistent with other signals along corridor</li> <li>• Signal timing plans for the signals in the coordination group</li> </ul> <p>These improvements are not currently included in any TIF program. The project applicant would pay the City for a fair share contribution to these improvements. Payment of its fair share would mitigate the project's contribution to the cumulative impact.</p>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Impact TRANS-2:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Westbound Off Ramp/Mountain Boulevard/Kuhnle Avenue (intersection #3) and after project completion, this intersection would continue to satisfy the MUTCD peak hour volume traffic signal warrant during the PM peak hour (Criterion f). This intersection operates at LOS F during the AM and PM peaks, and meets the peak hour signal warrant during the PM peak hour under Existing conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-2:</b> Implement the following measures at the I-580 Westbound Off-Ramp/Mountain Boulevard/Kuhnle Avenue intersection:</p> <p>a) Signalize intersection providing actuated operations, with split phasing on the east-west approaches (Mountain Boulevard/I-580 Westbound Off-Ramp) and permitted phasing on north-south (Kuhnle Avenue), and</p> <p>b) Coordinate the signal timing at this intersection with the adjacent intersection at I-580 Eastbound On-Ramp/Seminary Avenue/Kuhnle Avenue (intersection #2, signalization proposed as part of Mitigation Measure TRANS-2). This intersection is under the jurisdiction of Caltrans so any equipment or facility upgrades must be approved by Caltrans prior to installation.</p> <p>To implement this measure, the following shall be submitted to the City of Oakland’s Transportation Services Division and Caltrans for review and approval:</p> <ul style="list-style-type: none"> <li>• PS&amp;E to modify intersection as detailed in Mitigation Measure TRANS-1</li> <li>• Signal timing plans for the signals in the coordination group</li> </ul> <p>This improvement is included in the Southeast Oakland Area TIF Program. Upon acceptance by the City, the applicant shall:</p> <ul style="list-style-type: none"> <li>• Pay the applicable Southeast Oakland TIF fee, or</li> <li>• Install the improvements and obtain a credit against its applicable TIF obligations and/or obtain reimbursement from monies collected under the Southeast Oakland TIF program for the amount the installation cost exceeds its TIF obligations; or</li> <li>• Some combination of the above two mitigation methods.</li> </ul>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>• Implementation shall occur by approximately 85 percent of project buildout or 940 equivalent housing units (EHU), whichever occurs first.</li> <li>• Alternatively, the developer may implement this mitigation measure prior to the time the 85 percent buildout/940 EHU trigger occurs.</li> <li>• In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per the mitigation measure.</li> </ul> <p><b>Pay TIF Obligation and/or Install Improvement:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Submit PS&amp;E and signal timing plans.</li> <li>• Track Master Plan project buildout by percentage and EHU.</li> <li>• Pay the applicable Southeast Oakland TIF fee, or as negotiated by the Development Agreement between the City and the Project sponsor; <b>and/or</b></li> <li>• Install the improvements and/or obtain a credit against its applicable TIF obligation and/or obtain reimbursement, per the mitigation measure.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>• Review and approve PS&amp;E and signal timing plans.</li> <li>• Verify receipt of TIF obligation and/or proper installation of improvement.</li> <li>• Issue credit or reimbursement for improvement installation costs, less the TIF obligation.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Impact TRANS-3:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Eastbound Off Ramp/Fontaine Street /Keller Avenue (intersection #12) which would meet the peak hour signal warrant (Criterion f) during the AM and PM peak hours under Existing Plus Project conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-3:</b> Implement the following measures at the I-580 Eastbound Off-Ramp/Fontaine Street/Keller Avenue intersection:</p> <ul style="list-style-type: none"> <li>a) Restripe westbound Keller Avenue approach to provide one left-turn lane and one shared through/right-turn lane,</li> <li>b) Signalize intersection providing actuated operations, with protected left-turn phasing on the westbound Keller Avenue approach, and</li> <li>c) Coordinate the signal timing at this intersection with the adjacent intersection at Mountain Boulevard/Keller Avenue (intersection #13, signalization proposed as part of Mitigation Measure TRANS-4) and I-580 Westbound Off-Ramp/Mountain Boulevard/Shone Avenue (intersection #16, signalization proposed as part of Mitigation Measure TRANS-5). This intersection is under the jurisdiction of Caltrans so any equipment or facility upgrades must be approved by Caltrans prior to installation.</li> </ul> <p>To implement this measure, the following shall be submitted to the City of Oakland’s Transportation Services Division and Caltrans for review and approval:</p> <ul style="list-style-type: none"> <li>• PS&amp;E to modify intersection as detailed in Mitigation Measure TRANS-1</li> <li>• Signal timing plans for the signals in the coordination group</li> </ul> <p>This improvement is included in the Southeast Oakland Area TIF Program. Upon acceptance by the City, the applicant shall:</p> <ul style="list-style-type: none"> <li>• Pay the applicable Southeast Oakland TIF fee, or</li> <li>• Install the improvements and obtain a credit against its applicable TIF obligations and/or obtain reimbursement from monies collected under the Southeast Oakland TIF program for the amount the installation cost exceeds its TIF obligations; or</li> <li>• Some combination of the above two mitigation methods.</li> </ul>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>• Implementation shall occur by approximately 25 percent of project buildout or 280 equivalent housing units (EHU), whichever occurs first.</li> <li>• Alternatively, the developer may implement this mitigation measure prior to the time the 25 percent buildout/280 EHU trigger occurs.</li> <li>• In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per this mitigation measure.</li> </ul> <p><b>Pay TIF Obligation and/or Install Improvement:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Submit PS&amp;E and signal timing plans.</li> <li>• Track Master Plan project buildout by percentage and EHU.</li> <li>• Pay the applicable Southeast Oakland TIF fee; <b>and/or</b></li> <li>• Install the improvements and/or obtain a credit against its applicable TIF obligations and/or obtain reimbursement, per the mitigation measure; or as negotiated by the Development Agreement between the City and the Project sponsor.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>• Review and approve PS&amp;E and signal timing plans.</li> <li>• Verify receipt of TIF obligation and/or proper installation of improvement.</li> <li>• Issue credit or reimbursement for improvement installation costs, less the TIF obligation.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Impact TRANS-4:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized Mountain Boulevard/Keller Avenue (intersection #13) and after project completion, this intersection would continue to satisfy the MUTCD peak hour volume traffic signal warrant during the AM and PM peak hours (Criterion f). This intersection meets the peak hour signal warrant during the AM and PM peak hours under Existing conditions. (Potentially Significant)</p>	<p><b>Mitigation Measure TRANS-4:</b> Implement the following measures at the Mountain Boulevard/Keller Avenue intersection:</p> <ul style="list-style-type: none"> <li>a) Restripe eastbound Keller Avenue approach to provide one shared left-turn/through lane and one shared through/right-turn lane,</li> <li>b) Restripe westbound Keller Avenue approach to provide one shared left-turn/through lane and one right-turn lane,</li> <li>c) Restripe southbound Mountain Boulevard Avenue approach to provide one left-turn lane and one right-turn lane,</li> <li>d) Signalize intersection providing actuated operations, with split phasing on the east-west approaches (Keller Avenue) and permitted phasing on north-south (Mountain Boulevard) approaches, and</li> <li>e) Coordinate the signal timing at this intersection with the adjacent intersections at I-580 Eastbound Off-Ramp/Fontaine Street/Keller Avenue (intersection #12, signalization proposed as part of Mitigation Measure TRANS-3) and I-580 Westbound Off-Ramp/Mountain Boulevard/Shone Avenue (intersection #16, signalization proposed as part of Mitigation Measure TRANS-5).</li> </ul> <p>To implement this measure, the following shall be submitted to the City of Oakland's Transportation Services Division and Caltrans for review and approval:</p> <ul style="list-style-type: none"> <li>• PS&amp;E to modify intersection as detailed in Mitigation Measure TRANS-1</li> <li>• Signal timing plans for the signals in the coordination</li> </ul> <p>This improvement is included in the Southeast Oakland Area TIF Program. Upon acceptance by the City, the applicant shall:</p> <ul style="list-style-type: none"> <li>• Pay the applicable Southeast Oakland TIF fee, or</li> </ul>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>• Implementation shall occur by approximately 5 percent of project buildout or 60 equivalent housing units (EHU), whichever occurs first.</li> <li>• Alternatively, the developer may implement this mitigation measure prior to the time the 5 percent buildout/60 EHU trigger occurs.</li> <li>• In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per this mitigation measure.</li> </ul> <p><b>Pay TIF Obligation and/or Install Improvement:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Submit PS&amp;E and signal timing plans.</li> <li>• Track Master Plan project buildout by percentage and EHU.</li> <li>• Pay the applicable Southeast Oakland TIF fee, or as negotiated by the Development Agreement between the City and the Project sponsor; <b>and/or</b></li> <li>• Install the improvements and/or obtain a credit against its applicable TIF obligations and/or obtain reimbursement, per the mitigation measure.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>• Review and approve PS&amp;E and signal timing plans.</li> <li>• Verify receipt of TIF obligation and/or proper installation of improvement.</li> <li>• Issue credit or reimbursement for improvement installation costs, less the TIF obligation.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<b>Impact TRANS-4 (cont.)</b>	<ul style="list-style-type: none"> <li>Install the improvements and obtain a credit against its applicable TIF obligations and/or obtain reimbursement from monies collected under the Southeast Oakland TIF program for the amount the installation cost exceeds its TIF obligations; or</li> <li>Some combination of the above two mitigation methods.</li> </ul>				
<p><b>Impact TRANS-5:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Westbound Off Ramp/Mountain Boulevard/Shone Avenue (intersection #16) which would meet the peak hour signal warrant (Criterion f) during the AM and PM peak hours under Existing Plus Project conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-5:</b> Implement the following measures at the I-580 Westbound Off-Ramp/Mountain Boulevard/Shone Avenue intersection:</p> <ol style="list-style-type: none"> <li>Restripe the I-580 westbound off-ramp approach to provide one left-turn lane and one shared left-turn/right-turn lane and re-stripe Mountain Boulevard to provide two receiving lanes,</li> <li>Signalize intersection providing actuated operations, with split phasing on the east-west approaches (I-580 Westbound Off-Ramp/Shone Avenue) and permitted phasing on north-south (Mountain Boulevard) approaches, and</li> <li>Coordinate the signal timing at this intersection with the adjacent intersections at I-580 Eastbound Off-Ramp/Fontaine Street/Keller Avenue (intersection #12, signalization proposed as part of Mitigation Measure TRANS-3) and Mountain Boulevard/Keller Avenue (intersection #13, signalization proposed as part of Mitigation Measure TRANS-4). This intersection is under the jurisdiction of Caltrans so any equipment or facility upgrades must be approved by Caltrans prior to installation.</li> </ol> <p>To implement this measure, the following shall be submitted to the City of Oakland's Transportation Services Division and Caltrans for review and approval:</p> <ul style="list-style-type: none"> <li>PS&amp;E to modify intersection as detailed in Mitigation Measure TRANS-1</li> <li>Signal timing plans for the signals in the coordination</li> </ul>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>Implementation shall occur by approximately 45 percent of project buildout or 500 equivalent housing units (EHU), whichever occurs first.</li> <li>Alternatively, the developer may implement this mitigation measure prior to the time the 45 percent buildout/500 EHU trigger occurs.</li> <li>In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per this mitigation measure.</li> </ul> <p><b>Pay TIF Obligation and/or Install Improvement:</b></p> <ul style="list-style-type: none"> <li>Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit PS&amp;E and signal timing plans.</li> <li>Track Master Plan project buildout by percentage and EHU.</li> <li>Pay the applicable Southeast Oakland TIF fee, or as negotiated by the Development Agreement between the City and the Project sponsor; <b>and/or</b></li> <li>Install the improvements and/or obtain a credit against its applicable TIF obligations and/or obtain reimbursement, per the mitigation measure.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>Review and approve PS&amp;E and signal timing plans.</li> <li>Verify receipt of TIF obligation and/or proper installation of improvement.</li> <li>Issue credit or reimbursement for improvement installation costs, less the TIF obligation.</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<b>Impact TRANS-5 (cont.)</b>	<p>This improvement is included in the Southeast Oakland Area TIF Program. Upon acceptance by the City, the applicant shall:</p> <ul style="list-style-type: none"> <li>• Pay the applicable Southeast Oakland TIF fee, or</li> <li>• Install the improvements and obtain a credit against its applicable TIF obligations and/or obtain reimbursement from monies collected under the Southeast Oakland TIF program for the amount the installation cost exceeds its TIF obligations; or</li> <li>• Some combination of the above two mitigation methods.</li> </ul>				
<p><b>Impact TRANS-6:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized Mountain Boulevard/Golf Links Road (intersection #40) which would meet the peak hour signal warrant (Criterion f) during the AM peak hour under Existing Plus Project conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-6:</b> Implement the following measures at the Mountain Boulevard/Golf Links Road intersection:</p> <ol style="list-style-type: none"> <li>Restripe the eastbound Golf Links Road approach to provide one left-turn lane and one shared left-turn/through/right-turn lane, and restripe Mountain Boulevard to provide two receiving lanes for a minimum of 100 feet,</li> <li>Signalize intersection providing actuated operations, with split phasing on the east-west approaches (Golf Links Road) and permitted phasing on north-south (Mountain Boulevard/Oakland Zoo Entrance) approaches, and</li> <li>Coordinate the signal timing at this intersection with the adjacent intersections at Golf Links Road/I-580 Eastbound Off-Ramp/98th (#38) and Golf Links Road/I-580 Westbound Ramps (#39) intersections. The Golf Links Road/I-580 ramp-terminal intersections are under the jurisdiction of Caltrans so any equipment or facility upgrades must be coordinated and approved by Caltrans prior to installation.</li> </ol> <p>To implement this measure, the following shall be submitted to the City of Oakland's Transportation Services Division and Caltrans for review and approval:</p> <ul style="list-style-type: none"> <li>• PS&amp;E to modify intersection as detailed in Mitigation Measure TRANS-1</li> </ul>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>• Implementation shall occur by approximately 20 percent of project buildout or 230 equivalent housing units (EHU), whichever occurs first.</li> <li>• Alternatively, the developer may implement this mitigation measure prior to the time the 20 percent buildout/230 EHU trigger occurs.</li> <li>• In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per this mitigation measure.</li> </ul> <p><b>Pay TIF Obligation and/or Install Improvement:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Submit PS&amp;E and signal timing plans.</li> <li>• Track Master Plan project buildout by percentage and EHU.</li> <li>• Pay the applicable Southeast Oakland TIF fee, or as negotiated by the Development Agreement between the City and the Project sponsor; <b>and/or</b></li> <li>• Install the improvements and/or obtain a credit against its applicable TIF obligations and/or obtain reimbursement, per the mitigation measure.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>• Review and approve PS&amp;E and signal timing plans.</li> <li>• Verify receipt of TIF obligation and/or proper installation of improvement.</li> <li>• Issue credit or reimbursement for improvement installation costs, less the TIF obligation.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<b>Impact TRANS-6</b> (cont.)	<ul style="list-style-type: none"> <li>Signal timing plans for the signals in the coordination</li> </ul> <p>This improvement is included in the Southeast Oakland Area TIF Program. In the absence of any applicable Southeast Oakland TIF for this improvement, the applicant shall install the improvements and may seek any applicable credits against its Citywide TIF obligations and/or reimbursement from monies collected under the Citywide TIF program for the amount the installation cost exceeds its Citywide TIF obligations or fair share contribution.</p>				
<b>Impact TRANS-8:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Eastbound On Ramp/ Seminary Avenue/Kuhnle Avenue (intersection #2) and after project completion this intersection would continue to satisfy the MUTCD peak hour volume traffic signal warrant during the AM and PM peak hours (Criterion f) under 2040 Plus Project conditions. (Significant and Unavoidable)	<b>Mitigation Measure TRANS-8:</b> Implement Mitigation Measure TRANS-1.		Same as Mitigation Measures TRANS-1.		
<b>Impact TRANS-9:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Westbound Off Ramp/ Mountain Boulevard/Kuhnle Avenue (intersection #3) and after project completion, this intersection would continue to satisfy	<b>Mitigation Measure TRANS-9:</b> Implement Mitigation Measure TRANS-2.		Same as Mitigation Measures TRANS-2.		

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p>the MUTCD peak hour volume traffic signal warrant during the AM and PM peak hours (Criterion f) under 2040 Plus Project conditions. (Significant and Unavoidable)</p>					
<p><b>Impact TRANS-10:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Eastbound Off Ramp/ Fontaine Street /Keller Avenue (intersection #12) which would meet the peak hour signal warrant (Criterion f) during the AM and PM peak hours under 2040 Plus Project conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-10:</b> Implement Mitigation Measure TRANS-3.</p>		<p>Same as Mitigation Measures TRANS-3.</p>		
<p><b>Impact TRANS-11:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized Mountain Boulevard/Keller Avenue (intersection #13) and after project completion, this intersection would continue to satisfy the MUTCD peak hour volume traffic signal warrant during the AM and PM peak hours (Criterion f) under 2040 Plus Project conditions. (Less than Significant after Mitigation)</p>	<p><b>Mitigation Measure TRANS-11:</b> Implement Mitigation Measure TRANS-4.</p>		<p>Same as Mitigation Measures TRANS-4.</p>		

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Impact TRANS-12:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized I-580 Westbound Off Ramp/ Mountain Boulevard/Shone Avenue (intersection #16) which would meet the peak hour signal warrant (Criterion f) during the AM and PM peak hours under 2040 Plus Project conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-12:</b> Implement Mitigation Measure TRANS-5.</p>		<p>Same as Mitigation Measures TRANS-5.</p>		
<p><b>Impact TRANS-14:</b> Traffic generated by the Oak Knoll Project would increase the average intersection delay and degrade the LOS from LOS C to LOS E (Criterion a) at the signalized Golf Links Road/I-580 Eastbound Off-Ramp/98th Avenue (Intersection #38), during the PM peak hour. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-14:</b> Implement the following measures at the Golf Links Road/I-580 Eastbound Off-Ramp/98th Avenue intersection:</p> <p>a) Extend the shared through/right-turn lane on the I-580 eastbound off-ramp to provide a minimum 450 feet of storage length, and</p> <p>b) Reconfigure Golf Links Road between the I-580 eastbound off-ramp and the I-580 westbound ramps to provide two left-turn lanes and one through lane along eastbound Golf Links Road and one left-turn lane and one shared left-turn/ through lane along westbound Golf Links Road.</p> <p>These improvements are not currently included in any TIF program. The project applicant would pay the City for a fair share contribution to these improvements. Payment of its fair share would mitigate the project’s contribution to the cumulative impact.</p>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>• Implementation shall occur by approximately 95 percent of project buildout or 1,050 equivalent housing units (EHU), whichever occurs first.</li> <li>• Alternatively, the developer may implement this mitigation measure prior to the time the 95 percent buildout/1,050 EHU trigger occurs.</li> <li>• In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per this mitigation measure.</li> </ul> <p><b>Pay Fair Share Contribution:</b></p> <ul style="list-style-type: none"> <li>• Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Submit PS&amp;E and signal timing plans.</li> <li>• Track Master Plan project buildout by percentage and EHU.</li> <li>• Pay or confirm a funding mechanism to pay the City a fair share contribution to implement the specified improvements; or as negotiated by the Development Agreement between the City and the Project sponsor.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements);Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>• Review and approve PS&amp;E and signal timing plans.</li> <li>• Establish fair share amount and confirm receipt of funds or confirmed funding mechanism.</li> </ul>	



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Impact TRANS-15:</b> Traffic generated by the Oak Knoll Project would add more than ten peak hour vehicle trips to a critical movement at the unsignalized Mountain Boulevard/Golf Links Road (intersection #40) which would meet the peak hour signal warrant (Criterion f) during the AM and PM peak hours under 2040 Plus Project conditions. (Significant and Unavoidable)</p>	<p><b>Mitigation Measure TRANS-15:</b> Implement Mitigation Measure TRANS-6 and the following measure:</p> <p>a) Widen I-580 westbound off-ramp to provide one shared left-turn/through lane and two right-turn lanes (minimum 300 feet of storage length) approaching the intersection.</p> <p>These improvements are not currently included in any TIF program. If, at the time the improvements are needed to mitigate the impact, signal warrants have been met, and Caltrans has approved the improvements to their facilities, then the project applicant shall fully fund and construct the improvements, and may seek reimbursement for the portion that is beyond their fair share contribution, from other potentially available funding sources.</p>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to development trigger.</li> </ul> <p><b>Development Trigger for Installation:</b></p> <ul style="list-style-type: none"> <li>Implementation shall occur by approximately 20 percent of project buildout or 230 equivalent housing units (EHU), whichever occurs first.</li> <li>Alternatively, the developer may implement this mitigation measure prior to the time the 20 percent buildout/230 EHU trigger occurs.</li> <li>In either case, the applicant and/or project developer/s shall provide funds or confirm a funding mechanism to support implementation, per this mitigation measure.</li> </ul> <p><b>Pay Improvement Cost:</b></p> <ul style="list-style-type: none"> <li>Prior to development trigger.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit PS&amp;E and signal timing plans.</li> <li>Track Master Plan project buildout by percentage and EHU.</li> <li>Pay or confirm a funding mechanism to pay the City a fair share contribution to implement the specified improvements.</li> <li>Fully fund and construct the improvements, and seek reimbursement; or as negotiated by the Development Agreement between the City and the Project sponsor.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements);Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>Review and approve PS&amp;E and signal timing plans.</li> <li>Establish fair share amount and ensure reimbursement for amount beyond the fair share contribution paid.</li> </ul>	
<p><b>Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation</b></p> <p>The proposed Project would not conflict with adopted City policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities.</p>	<p><b>SCA TRA-4:</b> Transportation and Parking Demand Management (#71).</p> <p><i>The Project sponsor has prepared a Transportation and Parking Demand Management (TDM) Plan that is capable of achieving the required twenty percent (20 percent) vehicle traffic reduction (VTR) and reducing parking demand generated by the Project. The TDM Plan indicates the estimated VTR for each identified strategy based on published research or guidelines where feasible.</i></p> <p>a. <b>Transportation and Parking Demand Management (TDM) Plan Required.</b> Prior to approval of construction-related permit. The project applicant shall submit a Transportation and Parking Demand Management (TDM) Plan for review and approval by the City.</p>	<p><b>Submit Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Prior to development of any construction-related permit.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit TDM Plan, including a VTR strategy, and ongoing monitoring and enforcement program</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Transportation Services Division:</b></p> <ul style="list-style-type: none"> <li>Review and approve TDM Plan.</li> </ul>	<p>Draft TDM Plan submitted to City (as appendix to the SEIR), dated December 2016.</p>

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation (cont.)</b></p>	<ul style="list-style-type: none"> <li>i. The goals of the TDM Plan shall be the following:                             <ul style="list-style-type: none"> <li>– Reduce vehicle traffic and parking demand generated by the project to the maximum extent practicable, consistent with the potential traffic and parking impacts of the project.</li> <li>– Achieve the following project vehicle trip reductions (VTR):                                     <ul style="list-style-type: none"> <li>▪ Projects generating 50-99 net new a.m. or p.m. peak hour vehicle trips: 10 percent VTR</li> <li>▪ Projects generating 100 or more net new a.m. or p.m. peak hour vehicle trips: 20 percent VTR</li> </ul> </li> <li>– Increase pedestrian, bicycle, transit, and carpool/vanpool modes of travel. All four modes of travel shall be considered, as appropriate.</li> <li>– Enhance the City’s transportation system, consistent with City policies and programs.</li> </ul> </li> <li>ii. TDM strategies to consider include, but are not limited to, the following:                             <ul style="list-style-type: none"> <li>– Inclusion of additional long-term and short-term bicycle parking that meets the design standards set forth in chapter five of the Bicycle Master Plan and the Bicycle Parking Ordinance (chapter 17.117 of the Oakland Planning Code), and shower and locker facilities in commercial developments that exceed the requirement.</li> <li>– Construction of and/or access to bikeways per the Bicycle Master Plan; construction of priority bikeways, on-site signage and bike lane striping.</li> <li>– Installation of safety elements per the Pedestrian Master Plan (such as crosswalk striping, curb ramps, count down signals, bulb outs, etc.) to encourage convenient and safe crossing at arterials, in addition to safety elements required to address safety impacts of the project.</li> </ul> </li> </ul>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation (cont.)</b></p>	<ul style="list-style-type: none"> <li>- Installation of amenities such as lighting, street trees, and trash receptacles per the Pedestrian Master Plan and any applicable streetscape plan.</li> <li>- Construction and development of transit stops/shelters, pedestrian access, way finding signage, and lighting around transit stops per transit agency plans or negotiated improvements.</li> <li>- Direct on-site sales of transit passes purchased and sold at a bulk group rate (through programs such as AC Transit Easy Pass or a similar program through another transit agency).</li> <li>- Provision of a transit subsidy to employees or residents, determined by the project applicant and subject to review by the City, if employees or residents use transit or commute by other alternative modes.</li> <li>- Provision of an ongoing contribution to transit service to the area between the project and nearest mass transit station prioritized as follows: 1) Contribution to AC Transit bus service; 2) Contribution to an existing area shuttle service; and 3) Establishment of new shuttle service. The amount of contribution (for any of the above scenarios) would be based upon the cost of establishing new shuttle service (Scenario 3).</li> <li>- Guaranteed ride home program for employees, either through 511.org or through separate program.</li> <li>- Pre-tax commuter benefits (commuter checks) for employees.</li> <li>- Free designated parking spaces for on-site car-sharing program (such as City Car Share, Zip Car, etc.) and/or car-share membership for employees or tenants.</li> </ul>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation (cont.)</b></p>	<ul style="list-style-type: none"> <li>- On-site carpooling and/or vanpool program that includes preferential (discounted or free) parking for carpools and vanpools.</li> <li>- Distribution of information concerning alternative transportation options.</li> <li>- Parking spaces sold/leased separately for residential units. Charge employees for parking, or provide a cash incentive or transit pass alternative to a free parking space in commercial properties.</li> <li>- Parking management strategies including attendant/valet parking and shared parking spaces.</li> <li>- Requiring tenants to provide opportunities and the ability to work off-site.</li> <li>- Allow employees or residents to adjust their work schedule in order to complete the basic work requirement of five eight-hour workdays by adjusting their schedule to reduce vehicle trips to the worksite (e.g., working four, ten-hour days; allowing employees to work from home two days per week).</li> <li>- Provide or require tenants to provide employees with staggered work hours involving a shift in the set work hours of all employees at the workplace or flexible work hours involving individually determined work hours.</li> </ul> <p>The TDM Plan shall indicate the estimated VTR for each strategy, based on published research or guidelines where feasible. For TDM Plans containing ongoing operational VTR strategies, the Plan shall include an ongoing monitoring and enforcement program to ensure the Plan is implemented on an ongoing basis during project operation. If an annual compliance report is required, as explained below, the TDM Plan shall also specify the topics to be addressed in the annual report.</p>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation (cont.)</b></p>	<p>b. <b>TDM Implementation – Physical Improvements.</b>  <i>Prior to building permit final.</i> For VTR strategies involving physical improvements, the project applicant shall obtain the necessary permits/approvals from the City and install the improvements prior to the completion of the project.</p>	<p><b>Implement Physical Improvements:</b></p> <ul style="list-style-type: none"> <li>• Prior to the building permit final.</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Obtain the necessary City permits/approvals and install the improvements per the TDM.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Transportation Services Division:</b></p> <ul style="list-style-type: none"> <li>• Ensure the applicant obtains necessary permits/approvals;</li> <li>• Confirm installation of any improvements.</li> </ul>	
	<p>c. <b>TDM Implementation – Operational Strategies.</b>                      Ongoing. For projects that generate 100 or more net new a.m. or p.m. peak hour vehicle trips and contain ongoing operational VTR strategies, the project applicant shall submit an annual compliance report for the first five years following completion of the project (or completion of each phase for phased projects) for review and approval by the City. The annual report shall document the status and effectiveness of the TDM program, including the actual VTR achieved by the project during operation. If deemed necessary, the City may elect to have a peer review consultant, paid for by the project applicant, review the annual report. If timely reports are not submitted and/or the annual reports indicate that the project applicant has failed to implement the TDM Plan, the project will be considered in violation of the Conditions of Approval and the City may initiate enforcement action as provided for in these Conditions of Approval. The project shall not be considered in violation of this Condition if the TDM Plan is implemented but the VTR goal is not achieved.</p>	<p><b>Implement Operational Strategies:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, for the first five years following completion of the project (or completion of each phase).</li> </ul> <p><b>Submit Annual Compliance Reports</b></p> <ul style="list-style-type: none"> <li>• Annually, for the first five years following completion of the project (or completion of each phase).</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>• Obtain the necessary City permits/approvals and install the improvements per the TDM.</li> <li>• Conduct and submit annual compliance reports.</li> <li>• If deemed necessary, pay for peer review consultant to review annual compliance reports.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Transportation Services Division:</b></p> <ul style="list-style-type: none"> <li>• Ensure the applicant obtains necessary permits/approvals;</li> <li>• Review and approve annual compliance reports, which may be conducted by a peer review consultant, paid for by the project applicant, if deemed necessary.</li> <li>• Initiate enforcement action if warranted based outcome of annual compliance reports</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<b>Consistency with Adopted Policies, Plans or Programs Supporting Alternative Transportation (cont.)</b>	<b>SCA TRA-2: Bicycle Parking (#69).</b>  <i>Prior to approval of construction-related permit.</i> The project applicant shall comply with the City of Oakland Bicycle Parking Requirements (chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall demonstrate compliance with the requirements.	<b>Submit Plans:</b> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <b>Implementation:</b> <ul style="list-style-type: none"> <li>Prior to building permit final, parking shall be installed.</li> </ul>	<b>Each FDP Project Applicant:</b> <ul style="list-style-type: none"> <li>Submit project plans demonstrating compliance with the City's bicycle parking requirements.</li> </ul>	<b>City of Oakland, Bureau of Planning; Oakland Transportation Services Division:</b> <ul style="list-style-type: none"> <li>Review and approve project plans for compliance with the City's bicycle parking requirements.</li> <li>Verify project compliance per the approved plan.</li> </ul>	
<b>Construction-Period Impacts</b>  There may be short-term temporary, adverse effects on the circulation system during construction of each project phase but these would not rise to the level of a significant impact due to their temporary nature.	<b>SCA TRA-1: Construction Activity in the Public Right-of-Way (#68).</b>  a. <b>Obstruction Permit Required.</b> <i>Prior to approval of construction-related permit.</i> The project applicant shall obtain an obstruction permit from the City prior to placing any temporary construction-related obstruction in the public right-of-way, including City streets and sidewalks.  b. <b>Traffic Control Plan Required.</b> <i>Prior to approval of construction-related permit.</i> In the event of obstructions to vehicle or bicycle travel lanes, the project applicant shall submit a Traffic Control Plan to the City for review and approval prior to obtaining an obstruction permit. The project applicant shall submit evidence of City approval of the Traffic Control Plan with the application for an obstruction permit. The Traffic Control Plan shall contain a set of comprehensive traffic control measures for auto, transit, bicycle, and pedestrian detours, including detour signs if required, lane closure procedures, signs, cones for drivers, and designated construction access routes. The project applicant shall implement the approved Plan during construction.	<b>Obtain Approved Permit:</b> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <b>Physical Conditions Assessment:</b> <ul style="list-style-type: none"> <li>Prior to building permit final.</li> </ul>	<b>Master Developer and Each FDP Project Applicant:</b> <ul style="list-style-type: none"> <li>Obtain an obstruction permit to place any temporary construction-related obstruction in the public right-of-way.</li> </ul>	<b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit; Public Works Agency:</b> <ul style="list-style-type: none"> <li>Review and approve obstruction permit application.</li> </ul>	
		<b>Submit Evidence of Approved Plan:</b> <ul style="list-style-type: none"> <li>Prior to obtaining an obstruction permit.</li> </ul> <b>Implementation:</b> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<b>Master Developer and Each FDP Project Applicant:</b> <ul style="list-style-type: none"> <li>Submit and implement a Traffic Control Plan</li> <li>Submit evidence of approved Plan with obstruction permit (per SCA TRA-1).</li> </ul>	<b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit; Public Works Agency:</b> <ul style="list-style-type: none"> <li>Review and approve Traffic Control Plan.</li> <li>Verify project compliance with the Plan during construction.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Construction-Period Impacts</b> (cont.)</p>	<p>c. <b>Repair of City Streets.</b> <i>Prior to building permit final.</i> The project applicant shall repair any damage to the public right-of way, including streets and sidewalks caused by project construction at his/her expense within one week of the occurrence of the damage (or excessive wear), unless further damage/excessive wear may continue; in such case, repair shall occur prior to approval of the final inspection of the construction-related permit. All damage that is a threat to public health or safety shall be repaired immediately.</p>	<p><b>Implement Repairs:</b></p> <ul style="list-style-type: none"> <li>If no further damage/excessive wear is expected, within one week of the occurrence of the damage (or excessive wear) and prior to building permit final.</li> <li>If further damage/excessive wear may continue, prior to approval of the final inspection of the construction-related permit.</li> </ul> <p><b>Conduct Conditions Assessment:</b></p> <ul style="list-style-type: none"> <li>Ongoing, and prior to final inspection of construction-related permit.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Repair any damage or excessive wear caused to the public right-of way during project construction activities.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation:</b></p> <ul style="list-style-type: none"> <li>Review and approve obstruction permit application.</li> <li>Verify whether damage or excessive wear to public right-of-way has occurred during construction. If so, verify adequate repair or replacement by the project applicant.</li> </ul>	
<p><b>Vehicles Miles of Travel</b></p>	<p><b>SCA TRA-4:</b> Transportation and Parking Demand Management (<i>See under Section 4.13, Transportation/Traffic - Detailed Policies, Plans or Programs Supporting Alternative Transportation</i>)</p>				
<p><b>Compliance with Required Transportation Improvements</b></p>	<p><i>Any differing requirements stated in the traffic mitigation measures in this SCAMMRP supersede this SCA.</i></p> <p><b>SCA TRA-5:</b> Transportation Improvements (#70).</p> <p><i>Prior to building permit final or as otherwise specified.</i> The project applicant shall implement the recommended on- and off-site transportation-related improvements contained within the Transportation Impact Study for the project (e.g., signal timing adjustments, restriping, signalization, traffic control devices, roadway reconfigurations, and pedestrian and bicyclist amenities). The project applicant is responsible for funding and installing the improvements, and shall obtain all necessary permits and approvals from the City and/or other applicable regulatory agencies such as, but not limited to, Caltrans (for improvements related to Caltrans facilities) and the California Public Utilities Commission (for improvements related to railroad crossings), prior to installing the improvements. To implement this measure</p>	<p><b>Submit PS&amp;E and Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to development trigger (as specified in relevant mitigation measures).</li> </ul>	<p><b>Master Developer:</b></p> <ul style="list-style-type: none"> <li>Submit PS&amp;E and signal timing plans.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Unit (for subdivision improvements); Bureau of Building – Zoning Inspections; Oakland Transportation Services Division; Caltrans:</b></p> <ul style="list-style-type: none"> <li>Review and approve PS&amp;E and signal timing plans.</li> </ul>	

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<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Compliance with Required Transportation Improvements (cont.)</b></p>	<p>for intersection modifications, the project applicant shall submit Plans, Specifications, and Estimates (PS&amp;E) to the City for review and approval. All elements shall be designed to applicable City standards in effect at the time of construction and all new or upgraded signals shall include these enhancements as required by the City. All other facilities supporting vehicle travel and alternative modes through the intersection shall be brought up to both City standards and ADA standards (according to Federal and State Access Board guidelines) at the time of construction. Current City Standards call for, among other items, the elements listed below:</p> <ul style="list-style-type: none"> <li>a. 2070L Type Controller with cabinet accessory</li> <li>b. GPS communication (clock)</li> <li>c. Accessible pedestrian crosswalks according to Federal and State Access Board guidelines with signals (audible and tactile)</li> <li>d. Countdown pedestrian head module switch out</li> <li>e. City Standard ADA wheelchair ramps</li> <li>f. Video detection on existing (or new, if required)</li> <li>g. Mast arm poles, full activation (where applicable)</li> <li>h. Polara Push buttons (full activation)</li> <li>i. Bicycle detection (full activation)</li> <li>j. Pull boxes</li> <li>k. Signal interconnect and communication with trenching (where applicable), or through existing conduit (where applicable), 600 feet maximum</li> <li>l. Conduit replacement contingency</li> <li>m. Fiber switch</li> <li>n. PTZ camera (where applicable)</li> <li>o. Transit Signal Priority (TSP) equipment consistent with other signals along corridor</li> <li>p. Signal timing plans for the signals in the coordination group</li> </ul>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.13 Transportation and Traffic (cont.)</b>					
<p><b>Compliance with Regulatory Permits and Authorizations from Other Agencies (cont.)</b></p>	<p><i>Any differing requirements stated in the traffic mitigation measures in this SCAMMRP supersede this SCA.</i></p> <p><b>SCA GEN-1:</b> Regulatory Permits and Authorizations from Other Agencies (#15).</p> <p><i>Prior to activity requiring permit/authorization from regulatory agency.</i> The project applicant shall obtain all necessary regulatory permits and authorizations from applicable resource/regulatory agencies including, but not limited to, the Regional Water Quality Control Board, Bay Area Air Quality Management District, Bay Conservation and Development Commission, California Department of Fish and Wildlife, U. S. Fish and Wildlife Service, and Army Corps of Engineers and shall comply with all requirements and conditions of the permits/authorizations. The project applicant shall submit evidence of the approved permits/authorizations to the City, along with evidence demonstrating compliance with any regulatory permit/authorization conditions of approval.</p>				
<b>4.14 Utilities and Service Systems</b>					
<p><b>Impact UTIL-1:</b> Sanitary wastewater generated by construction and operation of the proposed Project would not exceed wastewater treatment requirements of the San Francisco Bay Regional Water Quality Control Board nor result in a determination by the wastewater treatment provider that it does not have adequate capacity to serve the Project's projected demand. (Criteria a and d) (Less than Significant with SCAs)</p>	<p><b>SCA UTIL-4:</b> Sanitary Sewer System (#79).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall prepare and submit a Sanitary Sewer Impact Analysis to the City for review and approval in accordance with the City of Oakland Sanitary Sewer Design Guidelines. The Impact Analysis shall include an estimate of pre-project and post-project wastewater flow from the project site. In the event that the Impact Analysis indicates that the net increase in project wastewater flow exceeds City-projected increases in wastewater flow in the sanitary sewer system, the project applicant shall pay the Sanitary Sewer Impact Fee in accordance with the City's Master Fee Schedule for funding improvements to the sanitary sewer system.</p>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Approved Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities and project operations.</li> </ul> <p><b>Pay Fee:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any improvements to the sanitary sewer system.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit a Sanitary Sewer Impact Analysis.</li> <li>• Comply with wastewater flow estimates identified in the Analysis.</li> <li>• Pay Sanitary Sewer Impact Fee as needed for system improvements.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Division:</b></p> <ul style="list-style-type: none"> <li>• Review and approve Sanitary Sewer Impact Analysis.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
<p><b>Impact UTIL-2:</b> Construction and operation of the proposed Project would result in construction of new storm water drainage facilities or expansion of existing facilities, but the construction of which would not cause significant environmental effects. (Criterion b) (Less than Significant with SCAs)</p>	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) <i>(See under Impact AIR-1)</i></p> <p><b>SCA BIO-1:</b> Tree Removal During Bird Breeding Season <i>(See under Impact BIO-1)</i></p> <p><b>SCA BIO-3:</b> Creek Protection Plan <i>(See under Impact BIO-2)</i></p> <p><b>SCA BIO-4:</b> Dewatering/Diversion <i>(See under Impact BIO-4)</i></p> <p><b>SCA CUL-2:</b> Archaeological and Paleontological Resources – Discovery During Construction <i>(See under Impact CUL-3)</i></p> <p><b>SCA CUL-3:</b> Archaeologically Sensitive Areas – Pre-Construction Measures <i>(See under Impact CUL-5)</i></p> <p><b>SCA GEO-3:</b> Construction-Related Permit(s) <i>(See under Impact GEO-4)</i></p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction <i>(See under Impact HYD-1)</i></p> <p><b>SCA HYD-2:</b> State Construction General Permit <i>(See under Impact HYD-1)</i></p> <p><b>SCA NOI-1:</b> Construction Days/Hours <i>(See under Impact NOI-1)</i></p> <p><b>SCA NOI-2:</b> Construction Noise <i>(See under Impact NOI-1)</i></p> <p><b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures <i>(See under Impact NOI-1)</i></p> <p><b>SCA NOI-5:</b> Construction Noise Complaints <i>(See under Impact NOI-1)</i></p> <p><b>SCA TRA-1:</b> Construction Activity in the Public Right-of-Way <i>(See under Section 4.13, Transportation/Traffic - Detailed Policies, Plans or Programs Supporting Alternative Transportation)</i></p>				

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
<b>Impact UTIL-2 (cont.)</b>	<p><b>SCA UTIL-5:</b> Storm Drain System (#80).</p> <p><i>Prior to approval of construction-related permit.</i> The project storm drainage system shall be designed in accordance with the City of Oakland’s Storm Drainage Design Guidelines. To the maximum extent practicable, peak stormwater runoff from the project site shall be reduced by at least 25 percent compared to the pre-project condition.</p>	<p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Approved Plan:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit Storm Drainage Design Plans pursuant to City guidelines and performance measures.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Oakland Department of Transportation Engineering Services Division; Bureau of Building (single lot developments):</b></p> <ul style="list-style-type: none"> <li>• Review and approve Storm Drainage Design Plans.</li> </ul>	
<p><b>Impact UTIL-3:</b> The water demand generated by the proposed Project would not exceed water supplies available to serve the Project from existing entitlements and resources, but would result in construction of water facilities and expansion of existing facilities, construction of which would not cause significant environmental effects. (Criterion c) (Less than Significant with SCA)</p>	<p><b>SCA UTIL-6:</b> Water Efficient Landscapes (WELO). <i>Prior to approval of construction-related permit.</i></p> <p>The following condition applies to all landscape projects (the total area of planting, turf, and water features) that meet one of the following criteria:</p> <ol style="list-style-type: none"> <li>New Construction Projects with an aggregate landscape area equal to or greater than 500 sq.ft. (For the purpose of this condition “New Construction” means a new building with a landscape or other new landscape not associated with a building);</li> <li>Rehabilitated Landscape Projects with an aggregate landscape area equal to or greater than 2,500 sq. ft. (For the purpose of this Condition “Rehabilitated” means any re-landscaping project);</li> </ol> <p><b>New Water Efficient Landscapes - WELO</b></p> <p><u>Requirement:</u> The project applicant shall comply with California’s Water Efficient Landscape Ordinance (WELO) in order to reduce landscape water usage. For any landscape project with an aggregate (total noncontiguous) landscape area equal to 2,500 sq. ft. or less. The project applicant may implement either the Prescriptive Measures or the Performance Measures, of, and in accordance with the California’s Model Water Efficient Landscape Ordinance. For any landscape project with an aggregate (total noncontiguous) landscape area over 2,500 sq. ft., the project applicant shall implement the Performance Measures in accordance with the WELO.</p>	<p><b>SITEWIDE MASTER PLAN</b></p> <p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval/issuance of grading permit for the Master Landscape Plan per the Master Developer FDP.</li> </ul> <p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities and project operations.</li> </ul> <p><b>Submit a Certificate of Completion:</b></p> <ul style="list-style-type: none"> <li>• Upon installation of Master Landscape Plan and irrigation system.</li> </ul> <p><b>FDP PROJECTS</b></p> <p><b>Submit Plan:</b></p> <ul style="list-style-type: none"> <li>• Prior to approval of final landscape plan for each project FDP.</li> </ul> <p><b>Implementation:</b></p> <ul style="list-style-type: none"> <li>• Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>• Submit plans that comply with WELO to reduce landscape water usage, incorporating either the Prescriptive Measures or the Performance Measures.</li> <li>• Incorporate requirements into Soil Management Report (SCA GEO-1), Landscape Design Plan and Irrigation Design Plan (SCA AES-2), and Grading Plans</li> <li>• Submit a Certificate of Completion and landscape and irrigation maintenance schedule to City and EBMUD.</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Bureau of Building:</b></p> <ul style="list-style-type: none"> <li>• Review and approve all landscape plans and projects for incorporation of measures compliant with WELO.</li> </ul> <p><b>City of Oakland, Bureau of Planning; Bureau of Building – Zoning Inspections; EBMUD:</b></p> <ul style="list-style-type: none"> <li>• Confirm receipt of Certification of Completion.</li> </ul>	

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
Impact UTIL-3 (cont.)	<p><u>Prescriptive Measures:</u> Prior to construction, the project applicant shall submit documentation showing compliance with Appendix D of California's Model Water Efficient Landscape Ordinance (see website below starting on page 23). <a href="http://www.water.ca.gov/wateruseefficiency/landscapeordinance/docs/Title%2023%20extract%20-%20Official%20CCR%20pages.pdf">http://www.water.ca.gov/wateruseefficiency/landscapeordinance/docs/Title%2023%20extract%20-%20Official%20CCR%20pages.pdf</a></p> <p><u>Performance Measures:</u> Prior to construction, the project applicant shall prepare and submit a Landscape Documentation Package for review and approval, which includes the following</p> <ol style="list-style-type: none"> <li>a. Project Information: <ol style="list-style-type: none"> <li>i. Date,</li> <li>ii. Applicant and property owner name,</li> <li>iii. Project address,</li> <li>iv. Total landscape area,</li> <li>v. Project type (new, rehabilitated, cemetery, or home owner installed),</li> <li>vi. Water supply type and water purveyor,</li> <li>vii. Checklist of documents in the package, and</li> <li>viii. Applicant signature and date with the statement: "I agree to comply with the requirements of the water efficient landscape ordinance and submit a complete Landscape Documentation Package."</li> </ol> </li> <li>b. Water Efficient Landscape Worksheet <ol style="list-style-type: none"> <li>i. Hydrozone Information Table</li> <li>ii. Water Budget Calculations with Maximum Applied Water Allowance (MAWA) and Estimated Total Water Use</li> </ol> </li> <li>c. Soil Management Report</li> <li>d. Landscape Design Plan</li> <li>e. Irrigation Design Plan, and</li> <li>f. Grading Plan</li> </ol>	<p><b>Submit a Certificate of Completion:</b></p> <ul style="list-style-type: none"> <li>• Upon installation of the landscaping and irrigation systems for each project FDP.</li> </ul>			



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

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<b>4.14 Utilities and Service Systems (cont.)</b>					
<b>Impact UTIL-3 (cont.)</b>	<p>Upon installation of the landscaping and irrigation systems, the Project applicant shall submit a Certificate of Completion and landscape and irrigation maintenance schedule for review and approval by the City. The Certificate of Compliance shall also be submitted to the local water purveyor and property owner or his or her designee.</p> <p>For the specific requirements within the Water Efficient Landscape Worksheet, Soil Management Report, Landscape Design Plan, Irrigation Design Plan and Grading Plan, see the link below starting on page 5.</p> <p><a href="http://www.water.ca.gov/wateruseefficiency/landscapeordinance/docs/Title%2023%20extract%20-%20Official%20CCR%20pages.pdf">http://www.water.ca.gov/wateruseefficiency/landscapeordinance/docs/Title%2023%20extract%20-%20Official%20CCR%20pages.pdf</a></p>				
	<p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape (See under Impact GHG-1)</p> <p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) (See under Impact AIR-1)</p> <p><b>SCA BIO-1:</b> Tree Removal During Bird Breeding Season (See under Impact BIO-1)</p> <p><b>SCA BIO-3:</b> Creek Protection Plan (See under Impact BIO-2)</p> <p><b>SCA BIO-4:</b> Dewatering/Diversion (See under Impact BIO-4)</p> <p><b>SCA CUL-2:</b> Archaeological and Paleontological Resources – Discovery During Construction (See under Impact CUL-3)</p> <p><b>SCA CUL-3:</b> Archaeologically Sensitive Areas – Pre-Construction Measures (See under Impact CUL-5)</p> <p><b>SCA GEO-3:</b> Construction-Related Permit(s) (See under Impact GEO-4)</p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (See under Impact HYD-1)</p>				

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
<b>Impact UTIL-3</b> (cont.)	<p><b>SCA HYD-2:</b> State Construction General Permit (<i>See under Impact HYD-1</i>)</p> <p><b>SCA NOI-1:</b> Construction Days/Hours (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-2:</b> Construction Noise (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures (<i>See under Impact NOI-1</i>)</p> <p><b>SCA NOI-5:</b> Construction Noise Complaints (<i>See under Impact NOI-1</i>)</p> <p><b>SCA TRA-1:</b> Construction Activity in the Public Right-of-Way (<i>See under Section 4.13, Transportation/Traffic - Detailed Policies, Plans or Programs Supporting Alternative Transportation</i>)</p>				
<p><b>Impact UTIL-4:</b> The Project would be served by a landfill with sufficient permitted capacity to accommodate the Project's solid waste disposal needs and would not require the construction of landfill facilities or expansion of existing facilities, construction of which could cause significant environmental effects nor would it violate applicable federal, state, and local statutes and regulations related to solid waste. (Criteria e and f) (Less than Significant with SCAs)</p>	<p><b>SCA UTIL-1:</b> Construction and Demolition Waste Reduction and Recycling (#74).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall comply with the City of Oakland Construction and Demolition Waste Reduction and Recycling Ordinance (chapter 15.34 of the Oakland Municipal Code) by submitting a Construction and Demolition Waste Reduction and Recycling Plan (WRRP) for City review and approval, and shall implement the approved WRRP. Projects subject to these requirements include all new construction, renovations/alterations/modifications with construction values of \$50,000 or more (except R-3 type construction), and all demolition (including soft demolition) except demolition of type R-3 construction. The WRRP must specify the methods by which the project will divert construction and demolition debris waste from landfill disposal in accordance with current City requirements. The WRRP may be submitted electronically at <a href="http://www.greenhalosystems.com">www.greenhalosystems.com</a> or manually at the City's Green Building Resource Center. Current standards, FAQs, and forms are available on the City's website and in the Green Building Resource Center.</p>	<p><b>Submit WRRP:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Approved Plan:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities.</li> </ul>	<p><b>Master Developer and Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit Construction and Demolition Waste Reduction and Recycling Plan (WRRP).</li> </ul>	<p><b>City of Oakland, Bureau of Planning; Public Works – Environmental Services:</b></p> <ul style="list-style-type: none"> <li>Review and approve WRRP.</li> </ul>	

**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
<b>Impact UTIL-4</b> (cont.)	<p><b>SCA UTIL-3:</b> Recycling Collection and Storage Space (#76).</p> <p><i>Prior to approval of construction-related permit.</i> The project applicant shall comply with the City of Oakland Recycling Space Allocation Ordinance (chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall contain recycling collection and storage areas in compliance with the Ordinance. For residential projects, at least two cubic feet of storage and collection space per residential unit is required, with a minimum of ten cubic feet. For nonresidential projects, at least two cubic feet of storage and collection space per 1,000 square feet of building floor area is required, with a minimum of ten cubic feet.</p>	<p><b>Submit Plans:</b></p> <ul style="list-style-type: none"> <li>Prior to approval of any construction-related permit.</li> </ul> <p><b>Conduct Work Per Ordinance:</b></p> <ul style="list-style-type: none"> <li>Ongoing, throughout all construction activities and project operations.</li> </ul>	<p><b>Each FDP Project Applicant:</b></p> <ul style="list-style-type: none"> <li>Submit and implement project plans demonstrating compliance with the Oakland Recycling Space Allocation Ordinance.</li> </ul>	<p><b>City of Oakland, Bureau of Planning:</b></p> <ul style="list-style-type: none"> <li>Review and approve project plans for compliance with the Ordinance.</li> </ul>	
<b>Impact UTIL-5:</b> The proposed Project would not violate applicable federal, state and local statutes and regulations relating to energy standards nor would it result in a determination by the energy provider that it would not have adequate capacity to serve the Project's projected demand. (Criteria g and h) (Less than Significant)	<p><b>SCA GHG-1:</b> Greenhouse Gas (GHG) Reduction Plan (GGRP) (<i>See under Impact GHG-1</i>)</p> <p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape (<i>See under Impact GHG-1</i>)</p>				
<b>Impact UTIL-6:</b> The Project would not have a considerable contribution to any cumulative impacts related to utilities and service systems, considering the combined effect of the Project, and past, present, approved, pending, and reasonably foreseeable future projects in the area and citywide. (Less than Significant with SCAs)	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) (<i>See under Impact AIR-1</i>)</p> <p><b>SCA BIO-1:</b> Tree Removal During Bird Breeding Season (<i>See under Impact BIO-1</i>)</p> <p><b>SCA BIO-3:</b> Creek Protection Plan (<i>See under Impact BIO-2</i>)</p> <p><b>SCA BIO-4:</b> Dewatering/Diversion (<i>See under Impact BIO-4</i>)</p> <p><b>SCA CUL-2:</b> Archaeological and Paleontological Resources – Discovery During Construction (<i>See under Impact CUL-3</i>)</p>				

## OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
<b>Impact UTIL-6 (cont.)</b>	<p><b>SCA CUL-3:</b> Archaeologically Sensitive Areas – Pre-Construction Measures (See under Impact CUL-5)</p> <p><b>SCA GEO-3:</b> Construction-Related Permit(s) (See under Impact GEO-4)</p> <p><b>SCA GHG-1:</b> Greenhouse Gas (GHG) Reduction Plan (GGRP) (See under Impact GHG-1)</p> <p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape (See under Impact GHG-1)</p> <p><b>SCA HYD-1:</b> Erosion and Sedimentation Control Plan for Construction (See under Impact HYD-1)</p> <p><b>SCA HYD-2:</b> State Construction General Permit (See under Impact HYD-1)</p> <p><b>SCA NOI-1:</b> Construction Days/Hours (See under Impact NOI-1)</p> <p><b>SCA NOI-2:</b> Construction Noise (See under Impact NOI-1)</p> <p><b>SCA NOI-4:</b> Project-Specific Construction Noise Reduction Measures (See under Impact NOI-1)</p> <p><b>SCA NOI-5:</b> Construction Noise Complaints (See under Impact NOI-1)</p> <p><b>SCA TRA-1:</b> Construction Activity in the Public Right-of-Way (See under Section 4.13, Transportation/Traffic - Detailed Policies, Plans or Programs Supporting Alternative Transportation)</p> <p><b>SCA UTIL-1:</b> Construction and Demolition Waste Reduction and Recycling (See under Impact UTIL-4)</p> <p><b>SCA UTIL-3:</b> Recycling Collection and Storage Space (See under Impact UTIL-4)</p> <p><b>SCA UTIL-4:</b> Sanitary Sewer System (See under Impact UTIL-1)</p> <p><b>SCA UTIL-5:</b> Storm Drain System (See under Impact UTIL-2)</p> <p><b>SCA UTIL-6:</b> Water Efficient Landscapes (WELO) (See under Impact UTIL-3)</p> <p><b>SCA UTIL-7:</b> Underground Utilities (#75).</p>				



**OAK KNOLL STANDARD CONDITIONS OF APPROVAL, MITIGATION MONITORING AND REPORTING PROGRAM (SCA/MMRP)**

Impact (for Reference)	Mitigation Measures and/or Standard Condition of Approval (SCA), and SCA Implementation Measures	Timing	Implementation Responsibility & Action	Monitoring Responsibility & Action	Date Completed / Signature
<b>4.14 Utilities and Service Systems (cont.)</b>					
<b>General SCA Applicable to the Project</b>	<i>During construction.</i> The project applicant shall place underground all new utilities serving the project and under the control of the project applicant and the City, including all new gas, electric, cable, and telephone facilities, fire alarm conduits, street light wiring, and other wiring, conduits, and similar facilities. The new facilities shall be placed underground along the project's street frontage and from the project structures to the point of service. Utilities under the control of other agencies, such as PG&E, shall be placed underground if feasible. All utilities shall be installed in accordance with standard specifications of the serving utilities.				
<b>4.15 Energy</b>					
<b>Impact ENER-1:</b> Construction and operation of the proposed Project would not result in the wasteful, inefficient or unnecessary use of energy resources (Criterion 1). (Less than Significant with SCAs)	<p><b>SCA AIR-1:</b> Construction-Related Air Pollution Controls (Dust and Equipment Emissions) (<i>See under Impact AIR-1</i>)</p> <p><b>SCA GHG-1:</b> Greenhouse Gas (GHG) Reduction Plan (GGRP) (<i>See under Impact GHG-1</i>)</p> <p><b>SCA GHG-2:</b> Green Building Requirements – Bay Friendly Landscape (<i>See under Impact GHG-1</i>)</p> <p><b>SCA TRA-4:</b> Parking and Transportation Demand Management (<i>See under Section 4.13, Transportation/Traffic - Detailed Policies, Plans or Programs Supporting Alternative Transportation</i>)</p>				

**ATTACHMENT D - PARCEL 12 (PLN15378-PUDF04)  
DESIGN REVIEW CONFORMANCE MATRIX**

<b>Design Guideline</b>	<b>Compliance Analysis</b>	<b>Discussion</b>
<b>2.0 Planning Guidelines</b>		
<b>2.1 Oak Knoll Neighborhoods</b>		
Retail Village area is designed as a modestly-sized gathering spot to provide basic needs to the community (such as groceries, restaurants, banking).	Not applicable	<i>Parcel 12 is not located in a Retail Village. This guideline is intended for development that "a cluster of buildings at varying scales fronting a 'Principal Drive' and a 'Plaza.'"</i>
Creekside Village neighborhoods are medium density residential areas laid out in the lowland areas flanking the restored Rifle Range Creek corridor.	Complies	<i>Parcel 12 is located in a Creekside Village neighborhood.</i>
The Uplands is the residential development designed to maximize views as well as provide a pleasing appearance as viewed from adjacent areas.	Not applicable	<i>Parcel 12 is not located in The Upland neighborhood.</i>
<b>2.2 Neighborhood Streetscape</b>		
High Visibility Façades -- Street Facing	Complies	Proposed townhomes comply with this guideline and corresponding Arch. Guideline #3.3 Massing Primary Volumes - Building Orientation
High Visibility Façades -- Open Space Facing	Complies	Proposed townhomes comply with this guideline and corresponding Arch. Guideline #3.3 Massing Primary Volumes - Building Orientation

**ATTACHMENT D - PARCEL 12 (PLN15378-PUDF04)  
DESIGN REVIEW CONFORMANCE MATRIX**

Architecture Diversity and 'The Monotony Code' - For each single-family detached lot type, there must be a minimum of three (3) unique floor plan types, with three (3) façade variations each:	Complies	The proposal complies with this guideline for the proposed townhomes by featuring two façade variations (Mission and Craftsman) and three different floor plan types (Unit 1, Unit 1 R, Unit 2 R, and Unit 3).
A different porch or stoop type will be considered a façade variation;	Complies	Rear patios and end-unit wraparound porches have been incorporated into the design to achieve façade variation.
No two (2) detached homes of the same design may be repeated within two (2) adjacent lots on a given Block Face or a facing Block Face;	Not applicable	The proposal is for townhomes, not single family detached residences.
Homes on corner lots are encouraged to have architectural features such as wrap porches, side porches, or bay windows facing the secondary street.	Complies	The duplex and 5-plex units propose a wraparound porch on the end-unit.
Both the front as well as side facing façade on corner lots will be considered High Visibility Facades.	Complies	The proposal highlights compliance this requirement on the site plans.
<b>2.3 Commercial</b>		
Building placement that reinforces the concept of the Plaza and orients and service areas away from the Plaza while keeping them screened from view from Mountain Blvd.	Not applicable	This is a proposal for a residential development.
70% glazing on facades directly fronting the plaza and 50% glazing on facades fronting pedestrian pathways.	Not applicable	This is a proposal for a residential development.
Awning and trellis overhead canopies to provide outdoor shade and shaded gathering areas.	Not applicable	This is a proposal for a residential development.

ATTACHMENT D - PARCEL 12 (PLN15378-PUDF04)  
DESIGN REVIEW CONFORMANCE MATRIX

Sidewalk widths at primary retail facades sufficient to provide tree planting, signage, furnishings, lighting, and outdoor seating areas where appropriate to adjacent retail use.	Not applicable	This is a proposal for a residential development.
Hardscape and Planting that reinforces the outdoor pedestrian realm, but provides equal access to vehicular traffic.	Not applicable	This is a proposal for a residential development.
Retail Plaza	Not applicable	This is a proposal for a residential development.
Architectural and Landscape Character - The Character of the Retail village should be inspired by the open-air neighborhood shopping districts typical to the Bay Area	Not applicable	This is a proposal for a residential development.
<b>2.4 Townhomes</b>		
Create a 'sense of address' and a front door for each unit by providing 'door yards,' gates, and access to public streets and paseos.	Complies	The proposal includes front doors featuring door yards, gates and access to public streets and paseos/pathways.
All units should feature covered entry areas either in the form of a stopp or entry porch.	Complies	The proposal uses covered front doors with small foyers adjacent and separate from patios for some of the interior units.
Variation of design is encouraged and corner units should be treated differently than middle units.	Complies	Corner units feature variety in design and are differentiated from the middle units.
End facades should treated as high visibility and should feature windows, entries where appropriate, and other design features normally on the front façade.	Complies	The projecting window and roof articulations at the third floor of mission and craftsman style buildings satisfy this guideline.
Odd numbers of units in a row are encouraged.	Complies	The proposal uses this configuration of odd numbers of units within a row.



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Stepping between units is encouraged to provide private balconies and a varied building frontage as viewed from the street.	Does not comply	There does not appear to be stepping between units or varied frontage. This variation could help break up the facades of the triplexes facing the street.
Landscape planting should be integrated in with streetscapes and provide screening for parking and alleys. Please refer to the Preliminary Development Plan for example designs for Paseos and Pocket Parks.	Complies	Landscape planting has been integrated in with streetscapes.
<b>2.5 Building Massing and Placement</b>		
Massing, building setback and height are considered in more detail in the Architectural Guidelines	See 3.0 Architectural Guidelines	See 3.0 Architectural Guidelines
<b>2.5 (aka 2.6) Driveways and Garage Placement</b>		
Refer to Chapter 4.0, Landscape Guidelines, for allowable paving materials for driveways.	See 4.0 Landscape Guidelines	See 4.0 Landscape Guidelines

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Design Guideline	Compliance Analysis	Discussion
<b>3.0 Architectural Guidelines</b>		
<b>3.1 The 'Bay Area' Regional Style</b>		
Building which connect to and are inspired by the natural setting.	Complies	The buildings connect well with the natural setting.
Simple building mass with additive elements	Complies	The buildings multi-unit buildings incorporate several additive elements.
Natural materials (wood, stone, terra cotta, stucco)	Complies	Exterior materials such as wood, stucco, stone veneer and board and batten siding are proposed.
Subdued earth-tone paint colors and light colored stuccos.	Complies	The exterior colors incorporate earth-tone paint colors, stuccos, tiled roofs, and stone.
<b>3.2 Architectural Style Matrix - By Family</b>		
Arts & Crafts: Craftsman Bungalow; Shingle; Tudor; Arts and Craft	Complies	The proposal includes Craftsman styled townhomes.
Mediterranean: Spanish Colonial; Mission; Tuscan	Complies	The proposal includes Mission styled townhomes.
Californian: Farmhouse; California Modern (mid-century modern); California Contemporary	Not applicable	Although Farmhouse style is mentioned on the plans, no Farmhouse styled townhomes appear to be proposed.
<b>3.3 Massing - Primary Volumes</b>		
<i>Building orientation</i>		
Secondary Volumes	Complies	Mostly gable roof profiles.
Additive Building Elements	Complies	Bay windows and side dormers have been employed to create simple and effective massing.
<b>3.4 Roofs</b>		

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Roof materials	Complies	Flat concrete tile, standing seam metal, s-tile roofs are proposed.
Successful roof designs	Complies	The proposed design complies with this guideline by proposing gable roofs and roof slopes that complement each architectural style of building.
Dormer sizing	Complies	The proposed design and sizing are appropriate for the architectural designs and comply with this guideline
Dormer siding	Complies	The proposed design and sizing are appropriate for the architectural designs and comply with this guideline
<b>3.5 High Visibility Façades</b>		
High Visibility Façades - Open Space - Use of porches and balconies are encouraged on these facades, and they should be designed with their visibility in mind, as well as the privacy of the homeowner	Complies	Porches and balconies have been included on high visibility façades.
Corner lot façades - Corner lot façades shall have consistent details and elements on elevations facing both streets. The rhythm of openings established on the entry façade shall continue on the side façade that faces the street, and divided window patterns shall be consistent on both elevations.	Complies	Entrances face the street and have been enhanced with high quality design elements on both streets such as shutters to add to the exterior material changes for visual impact.

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<p>Additive façade elements - Once the design of the High Visibility Façade openings has been determined, additive building elements like porches and dormers should follow the rhythm of the façade composition. Wraparound porches are encouraged on corner lots, as well as projected window bays. Porch columns should be spaced equally to either side of façade openings.</p>	<p>Complies</p>	<p>Sidefacing gables and wraparound porches have been incorporated into the design of end units. The porch columns widths and the dominance of second-story decks have been revised and reduces to lessen overshadowing of the ground floor entry areas.</p>
<p>Successful execution of second façade - Secondary Facades that successfully follow the above guidelines will support a composition of the Bay Area home that is balanced and continuous rather than one-sided and fragmented.</p>	<p>Complies</p>	<p>The proposed design has been revised to increase articulation to the wall façades such as recessing siding, mixing exterior materials, and adding bay window projection.</p>
<p><i>3.6 Openings - Windows</i></p>		
<p>Window types</p>	<p>Complies</p>	<p>Casement, single-hung, true or simulated divided lite windows with wood trim.</p>
<p>Window proportions and trim</p>	<p>Complies</p>	<p>The proposed design provides details on window proportions and trim and complies with this guideline.</p>
<p>Shutters</p>	<p>Complies</p>	<p>Shutters are employed to enhance elevations on high visibility façades.</p>
<p><i>3.7 Exterior Doors</i></p>		



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Exterior main entry doors can be flat or traditionally paneled doors. Please refer to the Architectural Style Matrix on page 30 for design recommendations by style.	Complies	Exterior main entry doors are traditionally paneled.
<b>3.8 Porches and Stoops</b>		
Types - The porch or stoop is the signature element of an Oak Knoll home. As such, all homes must have either a porch or stoop.	Complies	The proposed design complies with this guideline.
Porch Dimensions	Complies	The proposed design complies with this guideline.
Porch Details	Complies	The proposed design complies with this guideline.
Stoops	Complies	The proposed design complies with this guideline.
Porch Materials	Complies	The proposed design complies with this guideline.
<b>3.9 Garages</b>		
Garage Dimensions	Complies	The proposed design complies with this guideline.
Garage Details	Complies	The proposed design complies with this guideline.
<b>3.10 Lighting</b>		
Architectural Lighting	Complies	The proposed design complies with this guideline.

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Design Guideline	Compliance Analysis	Discussion
<b>4.0 Landscape Guidelines</b>		
<i>4.1 Landscape Vision</i>		
<i>4.2 Streetscape Design</i>		
<i>4.3 Open Space Design</i>		
The existing grassland on the upper hillside and areas of existing preserved oak woodland are protected natural resources.	Not applicable	Not applicable to this parcel.
The lower hillside will be extensively planted as a restored oak woodland natural setting, consisting of several native oak species, Toyon and California Buckeye.	Not applicable	Not applicable to this parcel.
The restored Rifle Range Creek will be revegetated with an appropriate and diverse native plant community to recreate a natural setting that benefits wildlife, and includes a multi-use trail serving the community. Refer to Oak Knoll Mixed Use Community Development Project Regulatory Permit Application Package.	Not applicable	Not applicable to this parcel.
Tree mitigation occurs site-wide in a variety of locations. Refer to the Tree Removal Permit Package for recommended mitigation locations and species.	Not applicable	Not applicable to this parcel.
<b>4.4 Parks and Plaza Design Intent</b>		
The parks should emphasize use of native trees, shrubs, and groundcovers in both organic and formal settings. Refer to the Neighborhood Streetscape Plant List for Proposed Plants.	Not applicable	Not applicable to this parcel.
Parks should incorporate community-wide furnishings and signage consistent with other design elements in the community.	Not applicable	Not applicable to this parcel.
Parks should provide shaded seating areas, picnic tables, and trash receptacles.	Not applicable	Not applicable to this parcel.

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Hardscape areas should avoid asphalt and large expanses of concrete. Natural stone, pavers, high quality stamped concrete, and decomposed granite should be utilized in the appropriate settings.	Complies	Hardscape areas will avoid large expanses of concrete and natural pavers and other high quality materials are proposed.
<b>4.5 Community Trails and Recreation</b>		
Emphasis is on use of natural materials and simple treatments that are indeed to integrate fully with the natural setting.	Complies	The proposed design complies with this guideline.
Use of reclaimed timber for benches, signage, and trail markers with opportunities to incorporate hand-crafted artisan designs.	Complies	The proposed design complies with this guideline.
Trails for Oak Knoll are classified as follows: Hiking Trails; Multi-Use Path (Walking/Running/Biking); Neighborhood Path; Bike Route	Not applicable	Not applicable to this parcel.
Location of the trails system should meet the following design objectives: Safety; Connectivity to on-site and off-site destinations; Diversity in a experiences and user types; conforms to site attributes, opportunities, and constraints.	Not applicable	Not applicable to this parcel.
<b>4.6 Signage and Monumentation</b>		
<b>4.7 Walls</b>		
Site Retaining Walls		
The approved site retaining wall is: Pavestone 'Anchor Diamond Pro' Retaining Wall; Face Style; Straight; Color: Sandstone Blend.	Complies	The proposed design complies with this guideline.
<b>4.8 Residential Landscape Design</b>		
Oak Knoll landscapes and gardens are versatile, imaginative, and offer a range of expressions.	Complies	The proposed design complies with this guideline.
Landscapes encourage a relaxed, informal, and practical approach while accommodating contemporary lifestyles.	Complies	The proposed design complies with this guideline.

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Landscapes are designed to respond to unique characteristics, such as lot configuration, topography, existing vegetation, and the design and location of the house and ancillary structures.	Complies	The proposed design complies with this guideline.
<i>4.9 Single Family Residential</i>		
Integrate the built environment with a dominant landscape	Not applicable	Not applicable to this parcel.
Blend landscapes between lots and neighborhood streets as a unified community landscape setting.	Not applicable	Not applicable to this parcel.
Establish a healthy, sustainable, and natural landscape environment.	Not applicable	Not applicable to this parcel.
Prioritize front yard landscapes to reinforce neighborhood streets as livable, walkable places. The combination of front porches and front yard gardens within the private frontages activate the streetscape, and shall contribute to a consistent, high quality neighborhood landscape.	Not applicable	Not applicable to this parcel.
Low groundcovers have low water requirements and are composed in drifts, using selections from the Approved Plant (see Appendices).	Not applicable	Not applicable to this parcel.
Three general landscape zones have been defined for each home site: front yard zone, side yard zone, and rear yard zone.	Not applicable	Not applicable to this parcel.
Front yards on sloped lots guidelines: Front yard slopes may not exceed 2:1; Retaining walls, if used, should be terraced where possible and not exceed a maximum height as set forth in the Zoning Ordinance; and Retaining walls shall be integrated with shrub planting to soften and screen walls.	Not applicable	Not applicable to this parcel.
<i>4.10 Side and Rear Yard Fencing</i>		



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All fencing may either slope with grades or adjust as vertical offset between panels. Offsets shall not exceed 12-inches.	Complies	The proposed design complies with this guideline.
All fencing between adjoining lots shall have a height of 6-feet. Corner lots and end lots are encouraged to reduce fence heights at side yards to allow views with a minimum height of 4-feet.	Complies	The proposed design complies with this guideline.
All fencing shall be softened with flowering vines and shrubs to soften their visual appearance where visible from public areas.	Complies	The proposed design complies with this guideline.
A few upper hillside home sites with sloped rear yards in excess of 20% shall utilize the Approved Hillside Fence in the rear yard.	Not applicable	Not applicable to this parcel.
Lots with pools and spas require fencing and gates that meet all applicable codes.	Not applicable	Not applicable to this parcel.
Typical side and rear yard fencing is a solid cedar or redwood fence with a stained finish.	Not applicable	Not applicable to this parcel.
For upland lots with rear yards with onsite and offsite visibility, rear yard fencing, if used, shall use the Approved Hillside Fence to ensure visual consistency.	Not applicable	Not applicable to this parcel.
<i>4.11 Retaining Walls on Lots</i>		
Use of stucco, brick, painted brick, or natural stone veneer may be used for site walls in front and side yards that are visible from public areas. Materials shall complement the building architecture.	Complies	The proposed design complies with this guideline.
Wall heights shall be appropriate to context and shall not exceed 6 feet in height per code.	Complies	The proposed design complies with this guideline.

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Tiered walls shall be integrated landscape design.	Complies	The proposed design complies with this guideline.
Tops of walls may either slope or step with the topography as required. Walls may slope at 1:8 maximum or use vertical offsets of 12-inch maximum.	Complies	The proposed design complies with this guideline.
Use of vines, trailing evergreen groundcovers and shrub massings are encouraged to soften walls.	Complies	The proposed design complies with this guideline.
Retaining walls in side and rear yards. Walls not closely associated with the architecture and not visible from public areas may use the Approved Standard Wall System described in the Appendices.	Not applicable	Not applicable to this parcel.
Retaining walls in side and rear yards. Walls not closely associated with the architecture and not visible from public areas may use the Approved Standard Wall System described in the Appendices.	Not applicable	Not applicable to this parcel.
Retaining walls in rear yards shall be located a minimum of four feet from the property line to allow room for fencing.	Not applicable	Not applicable to this parcel.
Retaining walls and steps at front walkways are allowed to resolve site grading.	Complies	The proposed design complies with this guideline.
The following retaining wall materials are allowed: brick; painted brick; natural stone veneer; approved concrete block wall system in rear and side yards (refer to Appendices); gabions; and pressure-treated wood.	Complies	The proposed design complies with this guideline.

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The following retaining wall materials are <b>not</b> allowed: railroad ties; metal cribs; and concrete pylons.	Complies	The proposed design complies with this guideline.
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