

CITY OF OAKLAND

Parks and Recreation Advisory Commission



AGENDA FOR REGULAR MEETING

Date: Wednesday, February 12, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue

Remote access instructions:

The following options are available to join this meeting remotely via Zoom;

- To join online, please click on the following link: <https://us06web.zoom.us/j/89676813696>.
- To join by phone, please use one of the options below:
 - One-tap mobile.
 - +14086380968,,89676813696# US (San Jose), +16694449171,,89676813696# US
 - Dial one of the numbers below for higher quality based on your current location.
 - +1 669 900 6833 US (San Jose), +1 408 638 0968 US (San Jose),
 - +1 669 444 9171 US, +1 253 215 8782 US (Tacoma),
 - +1 346 248 7799 US (Houston), +1 719 359 4580 US, +1 253 205 0468 US,
 - +1 309 205 3325 US, +1 312 626 6799 US (Chicago), +1 360 209 5623 US,
 - +1 386 347 5053 US, +1 507 473 4847 US, +1 564 217 2000 US,
 - +1 646 876 9923 US (New York), +1 646 931 3860 US, +1 689 278 1000 US,
 - +1 301 715 8592 US (Washington DC), +1 305 224 1968 US
 - International numbers are available at <https://us06web.zoom.us/j/89676813696>.
 - Webinar ID: 896 7681 3696.

How to submit Public Comment before and during the meeting:

1. To submit comments for review by the Commission before the meeting date, email comments to publiccomments2prac@oaklandca.gov.
2. To comment in person during the meeting, please complete a Speaker Card and provide to meeting coordinators before the Chair opens the floor to Public Comment on that Item. Each speaker will be called to the podium by name and permitted to comment for the allotted time.
3. To comment during the meeting online, click “Raise Your Hand” when prompted. When it is your turn to speak, please unmute yourself. You will be permitted to comment on the open Item for the allotted time and re-muted. See instructions on how to “Raise Your Hand” here: https://support.zoom.com/hc/en/article?id=zm_kb&sysparm_article=KB0068290.
4. To comment during the meeting by phone, please call one of the phone numbers listed above. You will be prompted to “Raise Your Hand” by dialing *9 to request to speak. When it is your turn, please unmute yourself by dialing *6. You will be permitted to comment on the open Item for the allotted time and re-muted.

If you have questions, email publiccomments2prac@oaklandca.gov or contact Oakland Parks, Recreation and Youth Development at 510-238-7275. Thank you.

AGENDA FOR REGULAR MEETING

Parks and Recreation Advisory Commission

Date: Wednesday, February 12, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue



NOTE: Public Comment will be taken during this meeting as follows...

- If you have a comment to make about an Item on this agenda, you may do so after the Chair opens the floor to Public Comment on that Item.
- If you have a comment to make about a topic that is not on this agenda, you may do so under **Items 5 and 10**, Open Forum and Continuation of Open Forum.

Agenda Items

1. Call to Order
2. Roll Call / Determination of Quorum
 - Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo, Thomas, Snyder, Tran, Watkins, Whitmore
3. Modification of the Agenda
4. Disposition of Minutes
 - Draft minutes from regular meeting on **January 8, 2025**.
5. Open Forum
 - Comment on all items not on this agenda will be taken at this time.
6. Measure Q Update
 - Monthly OPW Informational Report on Measure Q and Performance Measure Updates.
 - Quarterly Maintenance Report
 - Updates on Measure Q-funded Minor CIPs
 - Updates on Measure Q-funded Stormwater Initiatives
7. New Business
 - 7A. Verbal Update on Pine Knoll Park by the Deputy City Administrator.
 - 7B. Verbal Update on Lakeshore Avenue Project by DOT.
 - 7C. Request for the Parks and Recreation Advisory Commission to support a gift-in-place donation in the amount not to exceed twenty-nine thousand dollars

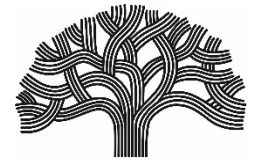
AGENDA FOR REGULAR MEETING

Parks and Recreation Advisory Commission

Date: Wednesday, February 12, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue



CITY OF OAKLAND

(\$29,000) to renovate Linden Park basketball courts by Hoop Bus Inc. and Mister F.A.B. Inc.

- 7D. Request for Parks and Recreation Advisory Commission to receive an informational report on the City of Oakland's General Plan Update process.
- 7E. Request for the Parks and Recreation Advisory Commission (PRAC) to support a gift-in-place donation in the amount not to exceed forty-seven thousand five hundred dollars (\$47,500) from Food of God (FOG) to cover the installation of new kitchen equipment at the Defremery Recreation Center kitchen.
- 7F. Request for the Parks and Recreation Advisory Commission to approve a gift-in-place donation in the amount not to exceed forty-nine thousand five hundred dollars (\$49,500) to renovate Ira Jenkins Recreation Center Gymnasium.

8. Planning and Conditional Use Permits (CUPs)

- 8A. Request for the Parks and Recreation Advisory Commission to 1) make a recommendation to the Bureau of Planning to approve an application filed by the Oakland Ballers, a professional baseball team, for improvements and upgrades to Raimondi Park, and 2) provide feedback to the Bureau of Planning for approval of a future application by the Oakland Ballers for on-site advertising at the Raimondi Park Baseball Field based on the preliminary signage concept, and 3) to make a recommendation to City Council to approve a ten (10) year license agreement for use of the Raimondi Park Baseball Field by the Oakland Ballers.
- 8B. Public hearing and request for the Parks and Recreation Advisory Commission (PRAC) to make a recommendation to grant a minor conditional use permit for a gift-in place of \$36,500 for reconstruction of the volunteer tool shed in the gardens at Lake Merritt.
- 8C. Request for the Parks and Recreation Advisory Commission (PRAC) to review and support a gift of \$36,500 (gift-in-place) for improvements at Clinton Community Center for the installation of a new exterior protective fence, electrical conduit, upgrades to a bathroom, installation of flooring, and to make a recommendation to grant a minor conditional use permit.

AGENDA FOR REGULAR MEETING

Parks and Recreation Advisory Commission

Date: Wednesday, February 12, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue



9. Update from the Director, Committees, and Announcements

10. Continuation of Open Forum

– All final comments on items not on this agenda will be taken at this time.

11. Adjournment

AGENDA FOR REGULAR MEETING

Parks and Recreation Advisory Commission

Date: Wednesday, February 12, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue



Visit [PRAC's website](#) for Agendas, Meetings, Minutes, and more information.

Next Regular PRAC Meeting

Date: Wednesday, March 12, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue

Accessibility Information:

This meeting location is wheelchair accessible.

To request disability-related accommodations or to request an ASL, Cantonese, Mandarin or Spanish interpreter, please email publiccomments2prac@oaklandca.gov or call Oakland Parks, Recreation and Youth Development at (510) 238-7275 or TDD/TTY (510) 238-3254 at least ten (10) days before the meeting. Please refrain from wearing scented products to this meeting as a courtesy to attendees with chemical sensitivities. Thank you.

Esta reunión es accesible para sillas de ruedas. Si desea solicitar adaptaciones relacionadas con discapacidades, o para pedir un intérprete de en español, Cantonés, Mandarín o de lenguaje de señas (ASL) por favor envíe un correo electrónico a publiccomments2prac@oaklandca.gov o llame al (510) 238-7275 o (510) 238-3254 por lo menos cinco (5) días hábiles antes de la reunión. Se le pide de favor que no use perfumes a esta reunión como cortesía para los que tienen sensibilidad a los productos químicos. Gracias.

會場有適合輪椅出入設施。需要殘障輔助設施, 手語, 西班牙語, 粵語或國語翻譯服務, 請在會議前五個工作天電郵 publiccomments2prac@oaklandca.gov 或致電 (510) 238-7275 或 (510) 238-3254 TDD/TTY。請避免塗搽香氛產品, 參加者可能對化學成分敏感。

CITY OF OAKLAND

Parks and Recreation Advisory Commission



DRAFT MINUTES FOR REGULAR MEETING

Date: Wednesday, January 8, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue

Agenda Items

1. Call to Order: **4:38 pm.**

2. Roll Call / Determination of Quorum

Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo, Snyder, Thomas, Tran, Watkins, Whitmore

Present (9): Chair Tran, Vice Chair Barach, Commissioners Alper, Cervantes, Henderson, Omolo, Thomas, Watkins, and Whitmore

Remote (1): Commissioner Kos-Read

Excused (0):

Absent (1): Commissioner Snyder

Action: Quorum confirmed.

3. Modification of the Agenda

– **None.**

4. Disposition of Minutes

– Approval of draft minutes from regular meeting on **December 11, 2024.**

Motion: Chair Tran entertained a motion to approve the minutes.

Moved by: Commissioner Henderson

Seconded by: Commissioner Omolo

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo, Thomas, Tran, Watkins, Whitmore

No (0)

Action: Motion Passed.

5. Open Forum

Comment on all items not on the agenda are taken at this time.

In-Person: 1

Remote: 1

Email: 1

6. Measure Q Update

- Monthly OPW Informational Report on Measure Q, Performance Measure Updates.
 - Update on OPW’s Progress on Audit Recommendations
 - Park Assessments

Public Comment

In-Person: 0

Remote: 1

Email: 0

Motion: Chair Tran entertained a motion to receive and file the Measure Q report.

Moved by: Commissioner Watkins

Seconded by: Commissioner Thomas

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo,
Thomas, Tran, Watkins, Whitmore

No (0)

Action: Motion Passed.

7. New Business

7A. Election of Chair and Vice Chair.

– **Nominations for Chair:**

- Commissioner Kos-Read nominated Vice Chair Barach.
 - Vice Chair Barach accepted.
- Commissioner Alper nominated Chair Tran.
 - Chair Tran declined.

Motion: Chair Tran entertained a motion to elect Vice Chair Barach as PRAC Chair.

Moved by: Commissioner Kos-Read

Seconded by: Commissioner Thomas

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo,
Thomas, Tran, Watkins, Whitmore

No (0)

Action: Motion Passed.

– **Nominations for Vice Chair:**

- Commissioner Henderson nominated Commissioner Kos-Read.
 - Commissioner Kos-Read declined.
- Chair Tran nominated Commissioner Omolo.
 - Commissioner Omolo accepted.

Motion: Chair Tran entertained a motion to elect Commissioner Omolo as PRAC Vice Chair.

Moved by: Chair Tran

Seconded by: Commissioner Henderson

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo, Thomas, Tran, Watkins, Whitmore

No (0)

Action: Motion Passed.

7B. Informational report for the removal of unapproved cycle trail at Joaquin Miller Park.

Public Comment

In-Person: 2

Remote: 0

Email: 0

Motion: Chair Tran entertained a motion receive and file the report.

Moved by: Vice Chair Barach

Seconded by: Commissioner Henderson

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo, Thomas, Tran, Watkins, Whitmore

No (0)

Action: Motion Passed.

7C. Draft PRAC Bylaw Review.

Motion: Chair Tran entertained a motion adopt the PRAC Bylaws.

Moved by: Commissioner Cervantes

Seconded by: Commissioner Omolo

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo, Thomas, Tran, Watkins, Whitmore

No (0)

Action: Motion Passed.

8. Planning and Conditional Use Permits (CUPs)

8A. Request for the Parks and Recreation Advisory Commission (PRAC) to review and support a minor conditional use permit for additional protective fencing and associated trail improvements within Joaquin Miller Park and the resource conservation areas situated within the Sausal Creek Watershed.

Public Comment

In-Person: 1
Remote: 0
Email: 2

Motion: Chair Tran entertained a motion to approve the request.

Moved by: Commissioner Omolo

Seconded by: Commissioner Thomas

Vote: Yes (10) Alper, Barach, Cervantes, Henderson, Kos-Read, Omolo,
Thomas, Tran, Watkins, Whitmore
No (0)

Action: Motion Passed.

- 8B. Request for the Parks and Recreation Advisory Commission (PRAC) to review and support a minor conditional use permit for installation of fitness equipment and water fountain at Courtland Creek Park.

Public Comment

In-Person: 1
Remote: 0
Email: 2

Motion: Chair Tran entertained a motion to approve the request and receive an itemized project cost breakdown.

Moved by: Commissioner Omolo

Seconded by: Vice Chair Barach

Vote: Yes (7) Barach, Henderson, Kos-Read, Omolo, Thomas, Tran, Watkins
No (0)
Abstain (3) Commissioners Alper, Cervantes, Whitmore

Action: Motion Passed.

9. Update from the Director, Committees, and Announcements

Director's Update:

- Assistant Director Aleem advised that the Director gave a presentation to the new City Council in their orientation about the (Oakland) Parks, Recreation, and Youth Development Department aka OPRYD.

Announcements:

- Chair Tran requested arrangements for an annual review of the City's goals and priorities with PRAC.
- Chair Tran requested ideas for site visits (i.e., parks, the zoo) be submit to the Chair for consideration.

10. Continuation of Open Forum

- Final comments on items not on the agenda are taken at this time.

In-Person: 0

Remote: 0

11. Adjournment: **6:58 pm.**

DRAFT

Next Regular PRAC Meeting

Date: Wednesday, January 8, 2025

Time: 4:30 PM

Location: Lakeside Park Garden Center, 666 Bellevue Avenue

Visit [PRAC's website](#) for Agendas, Meetings, Minutes, and more information.

Respectfully Submitted,



Jasmine S. Bellow

Executive Assistant to the Director

PRAC Recording Secretary

DRAFT

CITY OF OAKLAND
Oakland Public Works

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission (PRAC)
FROM: Liam Garland, Assistant to the Director of Public Works (OPW)
DATE: February 12, 2025
SUBJECT: Informational Report: OPW’s Measure Q Updates

SUMMARY

This report provides updates on delivery of:

1. Measure Q-funded park services for the second quarter of Fiscal Year 2025 (October 1-December 31);
2. Measure Q-funded minor capital improvement projects; and
3. Measure Q-funded stormwater initiatives.

BACKGROUND / LEGISLATIVE HISTORY

Measure Q, approved by Oakland voters in March 2020, provides funding for parks maintenance, homelessness services, and water quality. The City Council assigned PRAC the oversight role regarding the Measure Q expenditures related to parks maintenance and water quality services. As a component of this oversight function, OPW routinely submits updates to PRAC.

STATEMENT OF THE ISSUE

This report covers Quarter 2 (or Q2) of Fiscal Year 2025 from October 1, 2024 to December 31, 2024. The Q2 reporting format continues the use of color coding within the tables to indicate where OPW is meeting or falling short of targets. Per PRAC’s suggestion, the narrative analysis has been simplified to *target*, *results/variances*, and *addressing variances*. Similarly, this Q2 report includes equity analysis where the disparities between parks’ maintenance in equity priority neighborhoods (versus others) are identified. Parks with an asterisk (*) in the tables at Attachment A, B, C, and D are in high priority equity neighborhoods based on [OakDOT’s Geographic Equity Toolbox](https://www.oaklandca.gov/resources/oakdot-geographic-equity-toolbox) (available at <https://www.oaklandca.gov/resources/oakdot-geographic-equity-toolbox>) and parks with ** are in highest priority equity neighborhoods.

Staff seek PRAC’s input on this report and its analysis.

1. **Dedicated Staffing Per Park:** Measure Q Section 3(B)(1)(m): “Providing dedicated staff at major parks. ‘Major Parks’ means City operated Community Parks, Region-serving parks, and Resource Conservation Areas, as those terms are used in the Open Space Conservation & Recreation (OSCAR) Element of the Oakland General Plan.”

Target: Staff define Measure Q’s dedicated staffing goal as at least one full-time equivalent (FTE) devoted per month at each major park. Staff also developed a preliminary industry standard for number of parks maintenance FTEs per acre of park maintained. In 2008, the International City/County Management Association recommended a best practice of one park maintenance staff member per 12 acres of parkland and indicated a higher ratio if population density is higher. Oakland has a high population density and its parks face frequent illegal dumping, so staff adjusted

the industry standard to one park maintenance staff per 10 acres. The park assessments will re-evaluate whether this is the right standard and, if not, recommend a new one.

To approximate FTEs per major park, staff track total labor hours worked by staff at each major park. Then these total labor hours per park are converted into a FTE count per park by dividing the total work hours per park by the number of hours a FTE works per month. This results in an approximate number of FTEs per park per month.

Results/Variations: Attachment A shows the results. Q2 averages show that seven of the 11 major parks are meeting both the Measure Q-derived target of one FTE per major park and the industry standard target. Two of the parks are meeting the Measure Q-derived target but not the industry standard target. Two of the parks are not meeting either target.

Addressing variations: Of the two parks not meeting either target, one is in a high-priority equity neighborhood. Staff are taking a closer look at this park’s maintenance to see whether the failure to meet target is caused by lack of entry of work orders or lack of staff hours at the park. The City’s two largest parks are meeting the Measure Q target but not meeting the industry standard target. This is consistent with Q1. The development of parks assessments will address the maintenance needs of these region-serving parks and, if gaps exist, consider approaches to addressing these gaps.

2. Ballfield Mowing: Measure Q Section 3(B)(1)(a): “Mowing City operated sports fields frequently, every other week, or as may be required by the time of year.”

The Parks and Trees Division in Public Works’ Bureau of the Environment is responsible for ballfield mowing. Measure Q sets a priority to mow “frequently” and defines frequently as every other week (or roughly two times per month), or “as [frequently as] it may be required by the time of year.”

Target: Public Works sets a mowing target based on time of the year. During the off peak (or rainy) season from November to March, rain is likely, turf growth is slowed, and ballfields are used less, thus mowing is less frequent. Staff’s goal in the off-peak season is one mow per month at each field.

Off Peak (Rainy Season)					Peak (Dry Season)						
Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sept	Oct
ballfield mowing 1x/month					ballfield mowing 2x/month						

Peak (or dry) season is April to October. Rain is unlikely, turf growth is higher, ballfield use increases, and mowing is, and should be, occurring regularly. Measure Q says mowing should be occurring “every other week.” In peak season, staff’s target is mowing ballfields twice per month.

Results/Variations: Attachment B shows the results. Q2 includes October, which is peak season, and November and December, which are off-peak. For Q2, the average of citywide mows exceeded monthly targets. For individual parks, 67-70% are meeting the monthly mow target. Staff looked at whether the parks not meeting the mow target were in high or highest equity priority neighborhoods. The parks in the high and/or highest priority equity neighborhoods were more likely to meet the mowing target than parks in the non-priority neighborhoods.

Addressing variances: The data suggest that another 10 mows per month would enable every individual park to meet the Measure Q goal. Staff estimate that the one additional Park Equipment Operator is sufficient to ensure these 10 mows per month. As of June 2024, Measure Q funded seven Park Equipment Operators, six of which were filled and one vacant. The midcycle budget froze the vacant Equipment Operator position. Filling this frozen position would achieve the Measure Q-derived mowing goal for every ballfield at every park.

3. **Trash and Litter:** Measure Q Section 3(B)(1)(b): “Collecting trash and litter several times per week, including Saturdays and Sundays, from the City’s most used parks, assuring equitable distribution of resources...”

Staff define the City’s most used parks as the community parks and regional-serving parks in the General Plan’s OSCAR element. Trash cans are emptied and litter removed at these parks every Monday and Friday by the Bureau of Environment’s Parks and Trees Division.

Target: Staff set two goals for trash and litter pickup. The baseline goal is emptying of cans and picking up litter two times per week (or eight times per month). To derive a Measure Q goal, staff took the baseline goal of two pickups per week, added a pickup on Saturday and Sunday to reflect Measure Q’s language “including Saturdays and Sundays...”, and then included an overall completion rate of 75% of the time. This Measure Q-derived goal translates into three or more weekly pickups (four pickups multiplied by 75%) at the most used parks.

Results/Variations: Attachment C shows the results. In Q2, staff emptied an average of 1,443 trash cans monthly, several hundred short of the baseline goal of 1,888 per month and well below the Measure Q-derived goal of 2,832 per month. For Q2, an average of four parks met the baseline goal and a bit more than two on average met the Measure Q-derived goal. Staff’s analysis did not show a disparity in these results as a higher percentage of parks in the high and/or highest priority equity neighborhoods met targets than in the non-priority neighborhoods.

Addressing variances: Staff estimate an additional Gardener can empty an average of 560 cans per month. Based on this estimate, hiring three Gardeners would enable staff to meet the Measure Q-derived goal for trash can emptying. There are currently six existing Gardener vacancies, however the recruitment for those positions is not moving forward given the City’s budget challenge.

4. **Restroom Cleaning:** Measure Q Section 3(B)(1)(e): “Cleaning Park Restrooms more than once per day, especially on weekends.”

The Facilities Services Division of Public Works’ Bureau of Maintenance and Internal Services is responsible for outdoor restroom cleaning. A custodial unit is dedicated to outside restrooms and pools. Five Custodians are currently funded by Measure Q. Three of those positions are filled, two are vacant.

Target: Staff set a baseline goal of cleaning every parks’ outdoor restroom once per day. To derive a Measure Q goal, staff considered the ballot text’s language to clean park restrooms “more than once per day, especially on weekends.” Staff set a target of twice per day cleanings including weekends and then included the goal of meeting this twice-per-day standard 80% of the time.

Each cleaning count represents the restroom or restrooms being cleaned at the listed location. Where only one bathroom exists at a location, one count equals one restroom cleaning. In locations with multiple restrooms, one count represents multiple restrooms being cleaned. No reporting is provided for the cleanings of parks restrooms that are closed or operate only seasonally or by event. These include Arroyo Viejo** (McConnell Field), Caldecott, Carter Gilmore**, DeFremery**, Joaquin Miller-Sequoia Arena, Lafayette Park**, Oakport Field*, Otis Spunkmeyer Field*, Raimondi, Officer Willie Wilkins*, Willow Park*, and Woodminster.

Results/Variations: Attachment D shows the results. In Q2, average citywide restroom cleaning counts exceeded the baseline goal. However, those citywide restroom cleaning counts continue to fall short of the Measure Q-derived goal. The citywide shortfall averaged 621 cleaning per month. Further, staff's analysis showed a modest disparity in restroom cleanings. Three of five parks in equity neighborhoods met the baseline target, or 60%. Of the restrooms in non-priority neighborhoods, 17 of 24 met the baseline cleaning target, or 71%.

Addressing variations: Staff estimate that one custodian can clean 14 restrooms per day, which equates to ~280 cleanings per month. By hiring 2-3 more custodians, staff would regularly meet the Measure Q-derived goal. There are currently two vacant Measure Q-funded custodian positions for outdoor restrooms. OPW has submitted hiring requisitions for custodians; these requisitions are awaiting Finance Department approval. As far as the disparities in restroom counts, Q2 data shows a significant improvement over Q1's disparities. The addition of custodians would eliminate any disparity that exists, but staff will continue to track this data to see if disparities continue to shrink or persist.

5. **Measure Q-funded Minor CIPs**

Attachment E is a list of active Measure Q-funded minor capital improvement projects ("minor CIPs"). These 18 projects have a total budget of approximately \$3,473,193 with \$807,415 spent to date. At the last update on minor CIPs, PRAC sought more information on these projects' phases. Six minor CIPs are in the planning phase which includes project scoping, requesting bids and/or quotes, identifying project cost and troubleshooting the budget, and coordinating with various City divisions and departments. Two are in the contracting phase, typically where OPW has accepted a bid and/or quote and is working with the budget office to adjust the project budget, issue a purchase order, purchase materials, and schedule the work. Eight projects are in progress, usually meaning the project is either in construction or soon to be in construction.

Attachment F is a list of 21 closed minor CIPs with a total cost of \$4,100,216.

These minor CIPs are distinguished from capital projects included in the [FY 2023-2025 adopted Capital Improvement Program](#). Projects in the *FY 2023-2025 adopted Capital Improvement Program* "are defined as any long-term investment that builds, replaces or improves an asset, such as buildings, roads, parks, traffic signals, sewers and drainage lines. CIP projects typically have a useful design life of at least ten years and a minimum cost of approximately \$100,000." OPW's Bureau of Design and Construction (BDC) handles these projects, which typically take 4-7 years to plan, design, bid, build, and closeout. Few of these BDC-led CIPs include Measure Q funding.

In contrast, Measure Q funds many *minor CIPs*. These minor CIPs have maintenance as their scope, usually involving playground equipment repair, ball court resurfacing, roof repair/replacement, etc.

These projects are completed in six months to two years and do not involve extensive planning, design, or public input. Minor CIPs often do not have a project number in the City's financial system, which is why many minor CIPs will not appear in Measure Q's Financial Report.

Minor CIPs are delivered by OPW's Bureau of Maintenance and Internal Services, Facilities Services Division. These projects are bound by state and local procurement rules. Solicitations occur either through cooperative purchasing agreements, on-call contractors, or by using the City's informal three-bid process to select local contractors. Each procurement method has advantages and disadvantages, especially on budget and schedule. OPW's anecdotal experience is these procurements produce reasonably competitive costs for minor CIPs.

6. **Measure Q-funded stormwater initiatives:** Measure Q's stormwater provisions are section 3(B) and (3)(B)(a)+(f)+(k): "Services to address water quality and related litter reduction.... [including] [u]pdating the City Storm Drainage Master Plan...[a]cquiring, installing and maintaining full trash capture systems...[and] performing routine operations and maintenance for the City's stormwater system..."

The Watershed and Stormwater Management Division of Public Works' Bureau of Design and Construction is responsible for updating the City's Storm Drainage Master Plan (SDMP), which is the primary beneficiary of Measure Q funding for water quality and litter reduction. The plan's budget is \$4,393,107 of which \$2,252,107 is funded by Measure Q. The plan is estimated to be completed by the end of 2026.

This SDMP assesses the condition of the City's storm drainage system and identifies areas prone to flooding. It includes an inventory and assessment of the City's storm drainage system, studies to evaluate if drains and pipes have capacity to handle storm drainage flows, a prioritized list of improvements to reduce local flooding and to add green stormwater infrastructure designed to clean stormwater and provide urban greening benefits, preliminary cost estimates of storm drainage system improvements, and suggestions for funding sources to implement the planned storm drainage improvements.

To date, the plan's hydrology and hydraulic modeling effort is 100% complete. The storm drainage system's inspection and inventory is 50% complete. The inspection and video information will be used to develop condition ratings. The consultant is also identifying major system capacity deficiencies based on modeling results, complaints, and maintenance information. For the highest priority deficiencies, watershed-based capacity improvement project alternatives are being developed, e.g. upsized storm drainage pipes, diversions, detention, floodwalls, pump station improvements, etc. Designs to 10% will be developed for each selected alternative to better quantify the cost and to serve as the basis for future funding applications.

This process was completed for one section of Oakland and two flood control/storm drainage capacity improvement projects. The latter two projects are receiving initial design development funds through Measure U (Empire Road and Bernhardt Drive). In addition, the consultant has completed assessments of the City's stormwater pump stations and existing large trash capture devices, which informs future capital planning and maintenance needs. The consultant also completed a Lake Merritt bathymetry analysis and dissolved oxygen and temperature monitoring. They continue to work on a tree canopy model (51% complete), design standard updates, sea level

rise alternatives, CEQA, and a maintenance plan. The plan also identified five channel daylighting sites and nine regional green infrastructure sites with flood control and trash capture benefits.

By April 2025, staff will request City Council's approval to amend the SDMP's consultant contract to perform additional detailed hydrologic and hydraulic analyses, perform field and engineering assessments of storm drainage infrastructure impacted by winter storm flooding at multiple locations, extend the storm drainage study to assess anticipated effects of climate change and extreme precipitation on recommended capital improvement projects, perform additional inventory and analysis of the City's storm drainage inlet system to minimize clogging risks and identify safety concerns, continue updating the hydrologic and hydraulic modeling as needed to support the SDMP project's on-going capital improvement planning, complete the update of the Federal Emergency Management Agency's flood insurance rate map for the Arroyo Viejo Creek Watershed to remove errors from the current maps and address regulatory concerns, and enter into a user disclaimer of liability and warranties agreement with the Alameda County Flood Control and Water Conservation District for modeling data sharing.

Besides the SDMP, Measure Q contributes to other stormwater initiatives. These include the procurement and installation of 275 full trash capture devices in storm drains in high trash generating areas for which a contractor has been selected and contract execution is underway (\$500,000); the Lake Merritt Water Quality Management Pilot Project that installed devices to protect aquatic life from the potential impacts of harmful algal blooms (\$400,000 in FY 23/24 and \$400,000 in FY 24/25); and Courtland Creek Restoration Project improvements that reduce sediment discharges to the Courtland Creek (\$470,000).

To comply with an additional trash load reduction mandate under the City's Municipal Regional Stormwater Permit, the City is using approximately \$300,000 in Measure Q funding to implement a program (initial program costs are approximately \$450,000) to require private parcels with private storm drainage inlets that connect to the City's storm drainage system to control trash on-site or install and maintain approved full trash capture devices. The City will enter into an on-call consultant contract to assess private parcels and determine requirements to be imposed in 2025.

RECOMMENDATION

OPW recommends that PRAC receive this informational report.

Respectfully prepared and submitted by:

/s/ Liam Garland

Liam Garland

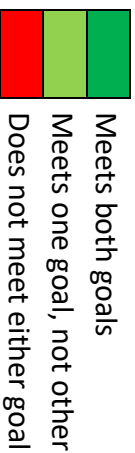
Assistant to the Public Works Director

Attachments: A – Dedicated staffing at major parks
B – Ballfield mows
C – Trash can emptying
D – Restroom cleanings
E – List of active minor CIPs funded by Measure Q
F – List of completed minor CIPs funded by Measure Q
G – Anticipated staff reporting schedule for FY 2024-2025

Attachment A – Dedicated Staffing at Major Parks

	Acres	Industry standard	Measure Q-derived goal = 1 or more staff FTE per park; Industry Standard = 1 FTE/10 acres											
			Q4 FY 2024			Q1 FY 2025			Q2 FY 2025			Q1 Avg	Q2 Avg	
			April	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec			
Arroyo Viejo Park**	10.1	1.0	5.7	4.8	3.4	6.7	6.6	4.9	6.1	5.5	4.7	4.7	5.0	
Brookdale Park	4.7	0.5	1.2	0.4	1.9	1.7	1.4	1.0	1.4	1.1	0.1	0.8	0.7	
Brookfield Park*	14	1.4	4.6	2.6	3.5	5.4	2.1	2.4	3.3	6.2	4.4	4.4	5.0	
Bushrod Park	10.1	1.0	4	3.2	2.9	3.1	3.9	3.4	3.5	2.8	2.4	2.4	2.5	
Defremery Park**	14	1.4	2.4	1.3	0.2	3.4	2.9	3.0	3.1	3.4	3.0	3.1	3.2	
Dimond Park	14.3	1.4	1.8	3	1.2	4.0	4.7	2.2	3.6	2.4	1.4	2.3	2.0	
Joaquin Miller Park***	150	15.0	1.8	0.8	2.9	3.2	5.3	11.5	6.7	12.3	4.8	10.1	9.1	
Lakeside Park	122	12.2	7	13	11.9	16.2	11.9	11.5	13.2	8.9	11.6	10.4	10.3	
Montclair Park	6.7	0.7	2.3	1.8	2.7	2.2	2.3	2.4	2.3	3.6	0.8	2.6	2.4	
Mosswood Park	11	1.1	2.5	4.8	1.3	1.1	4.1	3.4	2.9	5.0	1.9	3.1	3.3	
San Antonio Park*	11.6	1.2	1.4	0.4	2.3	0.12	0.02	0.5	0.21	1.5	0.3	0.7	0.83	
Parks meeting Measure Q Goal			11	8	10	10	10	10	10	11	8	9	9	
Parks not meeting Measure Q Goal			0	3	1	1	1	1	1	0	3	2	2	
% of Parks meeting Measure Q Goal			100%	73%	91%	91%	91%	91%	91%	100%	73%	82%	85%	
Parks meeting Industry Standard			9	7	7	9	8	8	8	9	7	7	8	
Parks not meeting Industry Standard			2	4	4	2	3	3	3	2	4	4	3	
% of Parks meeting Industry Standard			82%	64%	64%	82%	73%	73%	76%	82%	64%	64%	70%	

***Joaquin Miller Park has 500 total acres, but 360 acres are resource conservation areas.



Attachment B –Ballfied Mows

	Q4 FY 2024				Q1 FY 2025			Q2 FY 2025				
	April 1+ mow/mo	May 2+ mow/month	June 2+ mow/month	Q4 Avg	July 2+mows/month	Aug 2+mows/month	Sept 2+mows/month	Q1 Avg	Oct 2+ mow	Nov 1 mow/month	Dec 1 mow/month	Q2 Avg
Allendale Rec Center Field	1	2	2	1.7	1	1	0	0.7	1	1	0	0.7
A. Viejo-McConnell**	5	4	4	4.3	4	5	4	4.3	2	0	0	0.7
A. Viejo-R. Hndson. A's **	0	1	1	0.7	0	0	0	0.0	3	2	1	2.0
Brookdale Rec Center Field	1	4	5	3.3	4	3	2	3.0	1	1	1	1.0
Brookfield Rec Ctr Field*	0	3	3	2.0	2	2	5	3.0	6	2	2	3.3
Burckhalter Park Field	0	1	2	1.0	0	0	0	0.0	2	0	0	0.7
Bushrod- Lower Field	2	4	2	2.7	3	3	3	3.0	5	1	0	2.0
Bushrod-Upper Field	0	2	1	1.0	1	0	0	0.3	0	0	0	0.0
Cent. Reservoir Park Field*	4	3	3	3.3	0	1	0	0.3	2	1	2	1.7
Concordia Park Field*	1	2	3	2.0	1	1	2	1.3	2	2	1	1.7
Curt Flood Field*	0	3	3	2.0	4	5	4	4.3	4	2	1	2.3
Franklin Field	3	1	3	2.3	5	2	4	3.7	3	0	0	1.0
Garfield Ballfield**	4	4	2	3.3	1	3	1	1.7	1	1	1	1.0
Golden Gate Rec Ctr Field	0	3	1	1.3	0	0	1	0.3	2	0	1	1.0
Greenman/Carter Gilmore**	3	4	4	3.7	5	8	9	7.3	5	2	2	3.0
Jefferson Square Field	1	1	0	0.7	5	1	0	2.0	1	0	0	0.3
J. Miller (Robin Perry)	0	1	1	0.7	1	2	0	1.0	0	0	1	0.3
Lowell Park Field**	2	1	2	1.7	3	8	3	4.7	1	1	2	1.3
Montclair Rec Center Field	4	3	3	3.3	3	3	2	2.7	5	2	1	2.7
Mosswood Rec Ctr Field	6	3	2	3.7	6	7	7	6.7	5	1	1	2.3
Oakport Field*	3	4	2	3.0	3	2	5	3.3	2	2	1	1.7
Otis Spunkmeyer Field*	2	3	3	2.7	5	5	3	4.3	7	4	1	4.0
Poplar Rec Center Field	5	1	1	2.3	8	4	1	4.3	4	3	0	2.3
Pinto Park (Jones Field)	3	1	2	2.0	1	3	4	2.7	5	2	1	2.7
Raimondi Field	0	2	1	1.0	2	2	3	2.3	2	1	2	1.7
San Antonio Field*	5	3	1	3.0	3	3	2	2.7	2	0	3	1.7
Shepherd Canyon Park	1	2	2	1.7	2	2	0	1.3	0	1	1	0.7
Sobrante Park Field*	3	3	1	2.3	1	1	4	2.0	1	1	0	0.7
Tassafaronga Rec Ctr **	1	3	3	2.3	2	2	2	2.0	0	1	1	0.7
Wade Johnson Park Field**	2	2	1	1.7	0	4	3	2.3	4	0	1	1.7
Total Mows	62	74	64	67	76	83	74	78	78	34	28	47
% to goal citywide	207%	123%	107%	146%	127%	138%	123%	129%	130%	113%	93%	112%
Number of parks meeting goal=	16	22	20	19	18	21	19	19	20	21	21	21
Number of parks short of goal=	14	8	10	11	12	9	11	11	10	9	9	9
% of parks meeting goal=	53%	73%	67%	64%	60%	70%	63%	64%	67%	70%	70%	69%

Attachment C: Trash Can Emptying

	# of trash cans	Q4 FY 2024			Q1 FY 2025			Q2 FY 2025				
		Apr	May	June	July	Aug	Sep	Q1 Avg	Oct	Nov	Dec	Q2 Avg
Arroyo Viejo**	5	No Data	67	44	51	57	56	55	44	46	73	54
Brookdale Park	6		27	17	25	32	29	29	47	0	19	22
Brookfield Park*	6		83	57	48	64	76	63	64	57	57	59
Bushrod Park	10		227	103	95	111	136	114	73	58	81	71
Defremery Park**	13		117	13	156	156	156	156	169	156	156	160
Dimond Park	16		60	85	67	62	79	69	112	78	28	73
Joaquin Miller Park	49		395	407	40	149	350	180	659	297	427	461
Lakeside Park	95		786	828	822	885	521	743	431	548	392	457
Montclair Rec Ctr	13		16	13	29	0	29	19	36	59	30	42
Mosswood Park	2		12	0	0	43	48	30	46	10	18	25
San Antonio Field*	21		134	14	0	0	21	7	21	0	36	19
Monthly Total				1924	1581	1333	1559	1501	1464	1702	1309	1317
Baseline goal citywide			1888	1888	1888	1888	1888	1888	1888	1888	1888	1888
% to baseline goal citywide			102%	84%	71%	83%	80%	78%	90%	69%	70%	76%
Parks meeting baseline goal			6	3	5	6	5	5.3	4	2	6	4.0
Parks short of baseline goal			5	8	6	5	6	5.7	7	9	5	7.0
% of Parks meeting baseline goal			55%	27%	45%	55%	45%	48%	36%	18%	55%	36%
Measure Q goal citywide			2832	2832	2832	2832	2832	2832	2832	2832	2832	2832
% to Measure Q goal citywide			68%	56%	47%	55%	53%	52%	60%	46%	47%	51%
Parks meeting MQ goal			3	0	1	2	4	2.3	4	1	2	2.3
Parks short of MQ goal			8	11	10	9	7	8.7	7	10	9	8.7
% of Parks meeting MQ goal			27%	0%	9%	18%	36%	21%	36%	9%	18%	21%

Attachment D – Restroom Cleanings

	Baseline goal=1 cleaning/day Measure Q-derived goal=2 cleanings/day, 80% of the time											
	Q4 FY 2024				Q1 FY 2025				Q2 FY 2025			
	April	May	June	Q4 Avg	July	August	Sept.	Q1 Avg	Oct	Nov	Dec	Q2 Avg
12th St & Lake Merrit Blvd	42	37	41	40	44	47	36	42	42	32	40	38
Astro Park / Eastshore Park	52	51	32	45	50	43	33	42	49	37	38	41
Bella Vista Park	23	19	16	19	12	17	25	18	38	22	25	28
Brookdale Park	17	19	27	21	24	24	33	27	43	30	26	33
Burckhalter Park	31	30	28	30	45	37	37	40	55	38	42	45
Central Reservoir Rec Area*	18	16	2	12	12	14	26	17	36	24	25	28
Cesar Chavez Park*	27	26	21	25	25	22	30	26	43	24	26	31
Concordia Park*	16	17	12	15	19	20	26	22	41	23	26	30
Davie Tennis Stadium	23	21	20	21	26	23	19	23	22	25	27	25
Dimond Park	47	42	33	41	36	31	29	32	48	28	30	35
Estuary Park	35	29	23	29	27	28	21	25	31	24	26	27
Frog Park Rockridge-Temescal Greenbelt	27	28	29	28	62	30	49	47	52	40	46	46
Joaquin Miller-Fire Circle	24	39	18	27	24	50	29	34	73	41	42	52
Joaquin Miller-Ranger Sta.	-	-	-	-	-	-	-	-	49	43	41	44
Joaquin Miller-Redwood Glen	25	30	27	27	47	26	45	39	47	26	26	33
Joaquin Miller Park-Meadow	71	79	31	60	56	66	40	54	51	33	40	41
Lakeside Park-Sailboat Hse	49	47	42	46	45	42	34	40	47	37	41	42
Lowell Park**	29	25	17	24	15	15	24	18	34	23	19	25
Madison/Wilma Chan Park	41	35	25	34	36	37	31	35	31	26	26	28
Maxwell Park	26	24	25	25	23	26	35	28	47	29	28	35
McCrea Park	28	26	25	26	19	29	29	26	40	26	24	30
Montclair Park	48	46	34	43	54	50	44	49	52	40	40	44
Morcom Rose Garden	44	36	37	39	41	43	35	40	44	24	21	30
Mosswood Park - Theater Bld	27	28	30	28	29	26	17	24	22	9	16	16
Pine Knoll	45	40	40	42	45	47	36	43	41	37	42	40
San Antonio*	28	43	38	36	39	35	30	35	34	27	28	30
Shepherd Canyon Park	32	31	32	32	55	51	48	51	51	40	44	45
Snow Park	29	28	15	24	45	46	36	42	46	35	35	39
Union Point	36	26	20	27	21	15	22	19	26	25	27	26
Monthly Total	940	918	740	866	976	940	899	938	1235	868	917	1007
Baseline goal citywide	840	868	840	849	868	868	840	859	899	870	899	889
% to baseline goal citywide	112%	106%	88%	102%	112%	108%	107%	109%	137%	100%	102%	113%
Parks meeting baseline goal=	13	12	11	12	15	14	17	15	26	13	13	17
Parks short of baseline goal=	15	16	17	16	13	14	11	13	3	16	16	12
% of parks meeting baseline goal=	46%	43%	39%	43%	54%	50%	61%	55%	90%	45%	45%	60%
Measure Q goal citywide=	1440	1538	1440	1473	1538	1538	1440	1505	1538	1440	1538	1505
Parks meeting MQ goal=	4	2	0	3	5	4	2	3	6	0	0	3
Parks short of MQ goal=	24	26	28	25	23	24	26	25	23	29	29	25
% of Parks meeting MQ goal=	14%	7%	0%	12%	18%	14%	7%	12%	21%	0%	0%	12%

Attachment E – List of Active Minor CIPs funded by Measure Q

Project Title	Council	Priority	Project Description	Total Project Expense Budget	Expense (to-date)	Estimated Start	Estimated Complete	Project Status
DeFremery Park - Council Allocated	3	Highest	Field house repair, various park and pool improvements	\$ 179,798	\$ 17,536	Aug-24	Dec-26	Planning/Contracting
Lake Merritt Trail Repair	3	Medium	Repair damaged section of trail on the east side	\$ 300,000	-	-	-	Planning
Josie de la Cruz Park - Council Allocated	5	Highest	Repair play equipment and surfacing	\$ 179,798	\$ 21,909	Jun-24	Jun-26	Planning
Sailboat House Elevator	3	Medium	Elevator upgrade	\$ 180,000	-	-	-	Planning
Malonga Casquelourd Roof	3	Medium	Roof repair/replacement	\$ 113,886	-	-	-	Planning
Jack London Aquatic Center Roof	2	Medium	Roof repair/replacement	\$ 608,313	-	-	-	Planning
Diamond Park ADA Pathway	4	Low	ADA pathway at Fruitvale/Lyman park entrance	\$ 49,300	-	Jun-25	Aug-25	Contracting
Poplar Park Swing Set Resurfacing	3	Medium	Resurface two swing play areas with safety surface tiling	\$ 75,889	-	Mar-25	Apr-25	Contracting
San Antonio Park Repairs	2	High	Improvements to basketball courts, soccer area, gates of tennis courts, and resurfacing	\$ 250,000	\$ 185,621	Jun-22	Dec-26	In-progress
Joaquin Park Restrooms Renovations	4	Lowest	Repair Joaquin Miller bathrooms	\$ 175,592	\$ 16,373	Jun-24	Jun-26	In-progress
Willie Wilkins Park Mini-Pitch / Skate Park Buildout & Finishings	7	High	Transform tennis court area into soccer mini-pitch and skatepark	\$ 131,200	TBD	Nov-20	Dec-25	In-progress
Allendale Courts Resurfacing & Bathrooms - Council Allocated	4	Medium	Resurface basketball courts	\$ 300,000	\$ 176,247	Sep-23	Dec-25	In-progress
Joaquin Miller Park Upper Parking Lot & Trailer Connection	4	Lowest	Repair/upgrade electrical/lighting to upper parking and mtce storage lots	\$ 134,175	\$ 162,048	Oct-24	Apr-25	In-progress
FM Smith tot lot playground resurfacing	2	Low	Resurface tot lot playground	\$ 148,239	-	Feb-25	Mar-25	In-progress
FM Smith Fence Repair	2	Low	Repair fence damaged by storms	\$ 72,003	\$ 72,003	Jan-25	Jan-25	In-progress
Pine Knoll Park Fence Upgrade	2	Low	Install wooden fence and maintenance gate, upgrade pedestal	\$ 150,000	\$ 195,123	Dec-24	Feb-25	In-progress
Courtland Creek Park 1/2 Court Basketball Court Resurface	5	Medium	Resurface half court basketball area	\$ 75,000	-	-	-	Cancelled
Lake Merritt Bowling Green Restroom	3	Medium	Demolish dilapidated building and construct new restroom	\$ 350,000	-	-	-	Hold
Measure Q Projects Total				\$ 3,473,193	\$ 807,415			

Attachment F – List of Completed Minor CIPs funded by Measure Q

Project Title	Council District	Priority Neighborhood	Project Description	Actual Expense (to-date)	Start Date	Completion Date
Union Point Park Restoration	5	Medium	Park restoration	\$ 989,171	Jan-21	Aug-23
Citywide Park Signage	Citywide	Various	Restore and install park signage citywide	\$ 86,454	Jul-21	Jun-23
Restroom Paint Abatement	Citywide	Various	Restore citywide restroom paint abatement	\$ 50,000	Jul-21	Jun-23
Bella Vista Park Tot Lot Re-surfacing	2	Medium	Resurface playground w/safety surface tiling	\$ 44,652	Dec-21	Mar-22
Defremery Park Pool Filter Replacements	3	Highest	Replace pool filters	\$ 76,398	Feb-22	May-22
Dimond Park Play Structure Re-Surfacing	4	Low	Play structure resurfacing	\$ 143,446	Apr-22	Jun-22
Montclair Park Main Tot Lot Turf Restoration	4	Lowest	Tot lot turf restoration	\$ 206,182	May-22	Jun-22
Joaquin Miller Park Comm. Ctr Roof Repair	4	Lowest	Repair community center roof	\$ 200,728	May-22	Jun-22
Carter Gilmore Park Playground Re-Surfacing	6	Highest	Resurface playground w/safety surface tiling	\$ 208,801	Aug-22	Sep-22
Defremery Park Playground Re-surfacing	3	Highest	Resurface playground w/safety surface tiling	\$ 251,447	Oct-22	Oct-22
Linden Park Play Structure Re-Surfacing	1	Medium	Play structure resurfacing	\$ 122,416	Dec-22	Jan-23
Maxwell Park - Council Allocated	6	Medium	Lead paint abatement of the Maxwell House	\$ 58,861	Jul-23	Oct-23
Arroyo Viejo Park - Council Allocated	6	High	Install barriers to keep vehicles out/ball field fencing	\$ 300,393	Aug-23	Oct-24
Cesar Chavez Playground Resurfacing	5	High	Resurface playground	\$ 225,887	Sep-23	Oct-23
Concordia Park Playground surfacing	6	Highest	Playground resurfacing	\$ 102,897	Oct-23	Nov-23
William Bill Patterson Swing Set	7	High	Resurface damaged PIP at swing set and replace with	\$ 90,202	May-24	May-24
Wilma Chan playground resurfacing	2	Medium	Resurface playground	\$ 28,364	Aug-24	Sep-24
Clinton Square Community Center Roof	2	Medium	Roof repair/replacement	\$ 379,471	Aug-24	Sep-24
Temescal Pool Deck Resurfacing	1	Lowest	Resurface the pool deck	TBD	Sep-24	Sep-24
Lincoln Rec Annex Building Roof	2	Highest	Roof repair/replacement	\$ 166,443	Sep-24	Oct-24
Tassafaronga (Gym) Roof	7	Highest	Roof repair/replacement	\$ 368,003		
Total Projects	21		Total Expense To Date	\$ 4,100,216		

Attachment G – Anticipated Staff Reporting Schedule for FY 2024-2025

September	Close of FY 2024 Budget Adopted FY 2025 Budget Measure Q Minor CIPs
November	Quarter 1 Maintenance Review
January	Update on OPW's Progress on Audit Recommendations
February	Quarter 2 Maintenance Review Stormwater and Water Quality Update Minor CIPs Update
March	Measure Q Budget Update Staffing Update
April	PRAC Develops Measure Q FY 2025-2027 Budget Recommendations
May	Quarter 3 Maintenance Review Park Assessments
June	City Council Adopts FY 2025-2027 Budget

AGENDA ITEM 7B

Lakeshore Avenue Separated Bike Lanes Project

Presentation to PRAC
Feb 12, 2025

Background

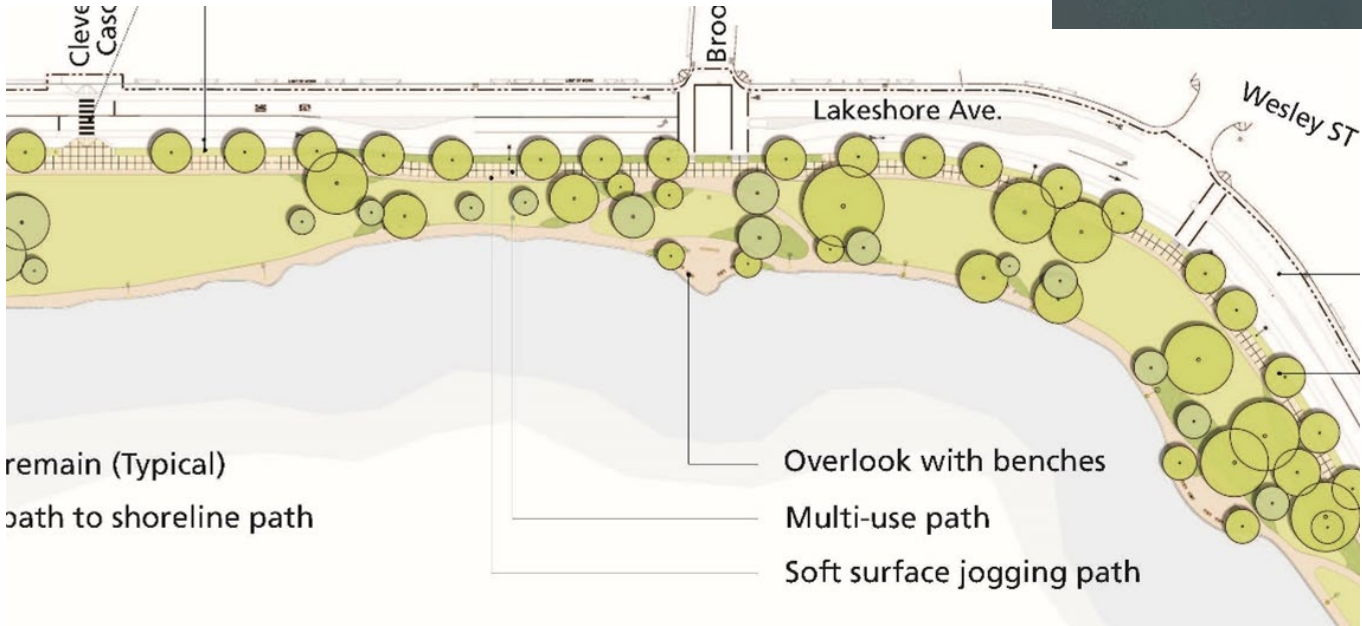


Lake Merritt Park Improvement Projects Lakeshore Avenue and El Embarcadero

SCOPE OF WORK

The park improvement projects include the following:

Lakeshore Avenue will be narrowed from four lanes to two lanes for vehicle traffic and will include six-foot-wide Class 2 bike lanes. A continuous left-turn lane will run down center of the street. The number of on-street parking spaces will remain essentially the same as existing.



Lakeshore Today

Looking south at Wayne Ave



Looking north at E18th St



Looking south at Beacon St



Looking south at Cleveland Cascade



Tragic Passing

August 6, 2023

Four-year old Maia Correia and her father were bicycling on Lakeshore Ave when a driver opening their door into the bike lane caused them to crash.

Maia subsequently passed away from injuries sustained during the crash.

Photo Credit: Kuan Butts



Rapid Response

OakDOT developed and installed signs reminding motorists to “check for bikes” before opening car doors per California Vehicle Code 22517

OakDOT is also acquiring stickers and window clings to hand out at outreach events.



Project Snapshot (Feb 2025)

- Separated bike lanes recommended in 2019 Bicycle Plan
- Prioritized for resurfacing in 2022 5-Year Paving Plan

Expected construction start: Summer/Fall 2025

Project Goals:

- 3 -> 2 lane road diet (removing continuous TWTL)
- Maintain turn pockets
- Two-way separated bike lane on the Lake side of roadway
- Improve pedestrian crossings (refuge and corner calming)
- Transit improvements including bus boarding islands
- Minimize impacts to parking & expand accessible parking
- Continue build-out of full separated pathway around Lake Merritt

Status: 100% Design
Community outreach complete
Final negotiations with contractor underway



Community Outreach

Community Meetings (2024):

- March 7: Bicycle and Pedestrian Advisory Committee (Infrastructure Subcommittee)
- March 12: Cleveland Heights Neighborhood Council 15X
- March 18: Measure DD Community Coalition

Postcard mailer to surrounding neighborhood and door-to-door business outreach:

- April 2024

City and Partner Agency Meetings

- April 2: Lake Merritt Working Group
- June: Fire Department, AC Transit
- July 31: PRAC Commissioners

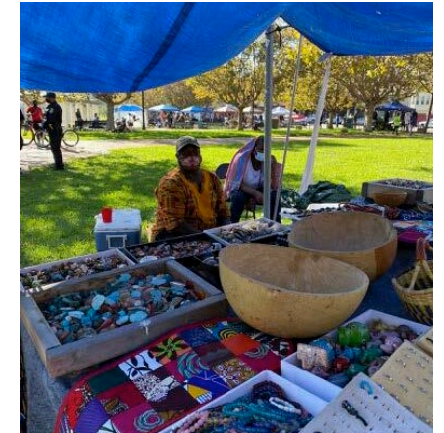
Pop-up Events

Lake Merritt
Pergola



E 18th St Landing
(Photo: Roger Rudick,
Streetsblog)

Vendors



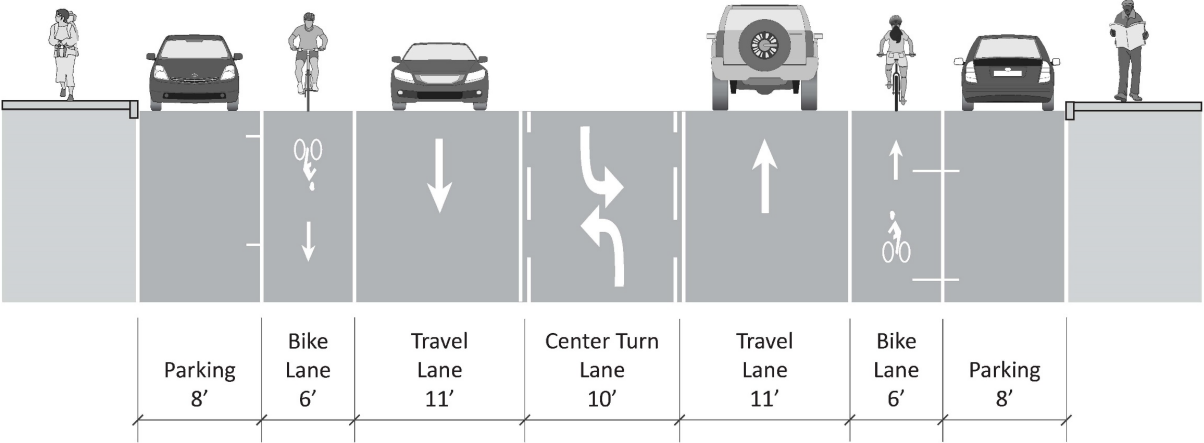
...and more

Existing/Proposed Cross-sections

60' Curb to Curb Width

Lakeshore Ave (E 18th St to El Embarcadero) Existing

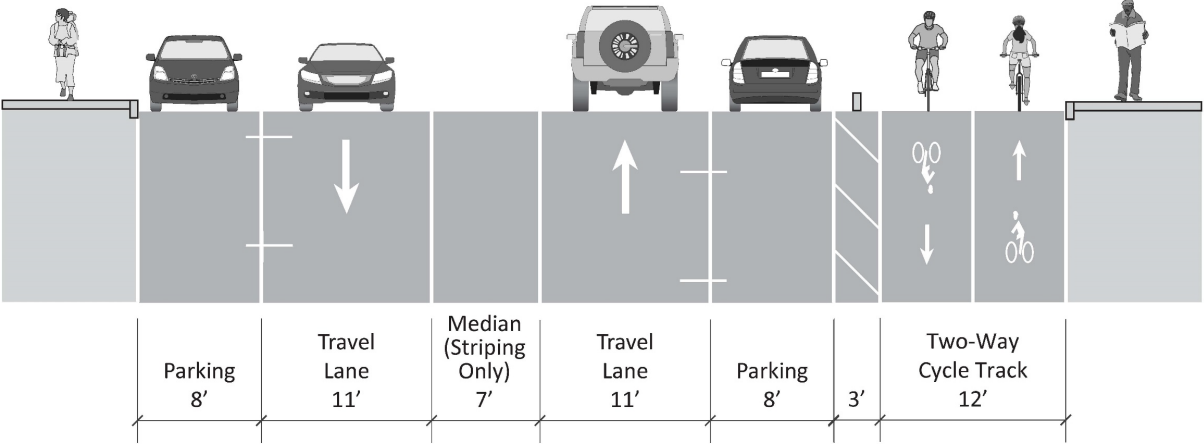
(Looking South)



(Looking South)

Lakeshore Ave (E 18th St to El Embarcadero) Proposed

(Cleveland Heights)



(Lake Merritt)

Existing



Proposed



Project Delivery

- **Delivery Method:** Paving On-call Construction Contract
- **Engineer's Estimate:** \$3.8M
- **Cash on Hand:** \$3M (Measure KK/U Local Bond funding)
- **Next Steps:**
 - Finalize bid item negotiations with selected contractor
 - Receive final bid price for project, allocate additional funding
 - Authorize construction to begin (expected Spring/Summer 2025, estimated construction duration of 6-12 months)



City of Oakland, Department of Transportation

Christopher Diano, Supervising Civil Engineer, Project Manager

cdiano@oaklandca.gov

Charlie Ream, Transportation Planner

cream@oaklandca.gov



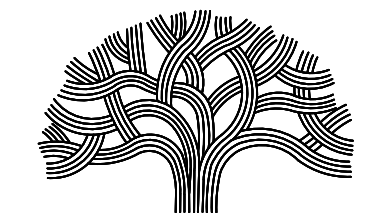
Know what's **below**.
Call before you dig.

THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT AND THE CITY PUBLIC WORKS AGENCY AT (510)622-5741 AT LEAST 48 HOURS (2 WORKING DAYS) PRIOR TO BEGINNING ANY EXCAVATION IN THE VICINITY OF UNDERGROUND FACILITIES.

THE CONTRACTOR SHALL MARK, COVER, PROTECT, AND REMOVE ANY MATERIAL PLACED OVER UTILITIES (ANY CITY OR PRIVATE UTILITY MANHOLE COVER/VALVE LID) PRIOR TO MILL/PAVE OPERATION. THE CONTRACTOR IS RESPONSIBLE FOR WORKING DIRECTLY WITH UTILITY OWNERS IN MAKING ADJUSTMENTS TO UTILITIES; LOWER TO MILL SURFACE (DRIVING SURFACE) IF OPEN TO TRAFFIC, AND RAISE TO FINISH GRADE.

PLANS FOR THE CONSTRUCTION OF LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E. 18TH STREET TO MACARTHUR BOULEVARD IN THE CITY OF OAKLAND, IN ALAMEDA COUNTY

TO BE SUPPLEMENTED BY CITY OF OAKLAND STANDARD DETAILS DATED JANUARY 2002,
CALTRANS STANDARD PLANS AND SPECIFICATIONS DATED 2023, AND THE LATEST REVISION
OF THE CALIFORNIA MUTCD DATED MARCH 2014



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA
SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

COMPLETE STREETS PAVING (DOT)



PRINCIPAL CIVIL ENGINEER

CONSTRUCTION MANAGEMENT

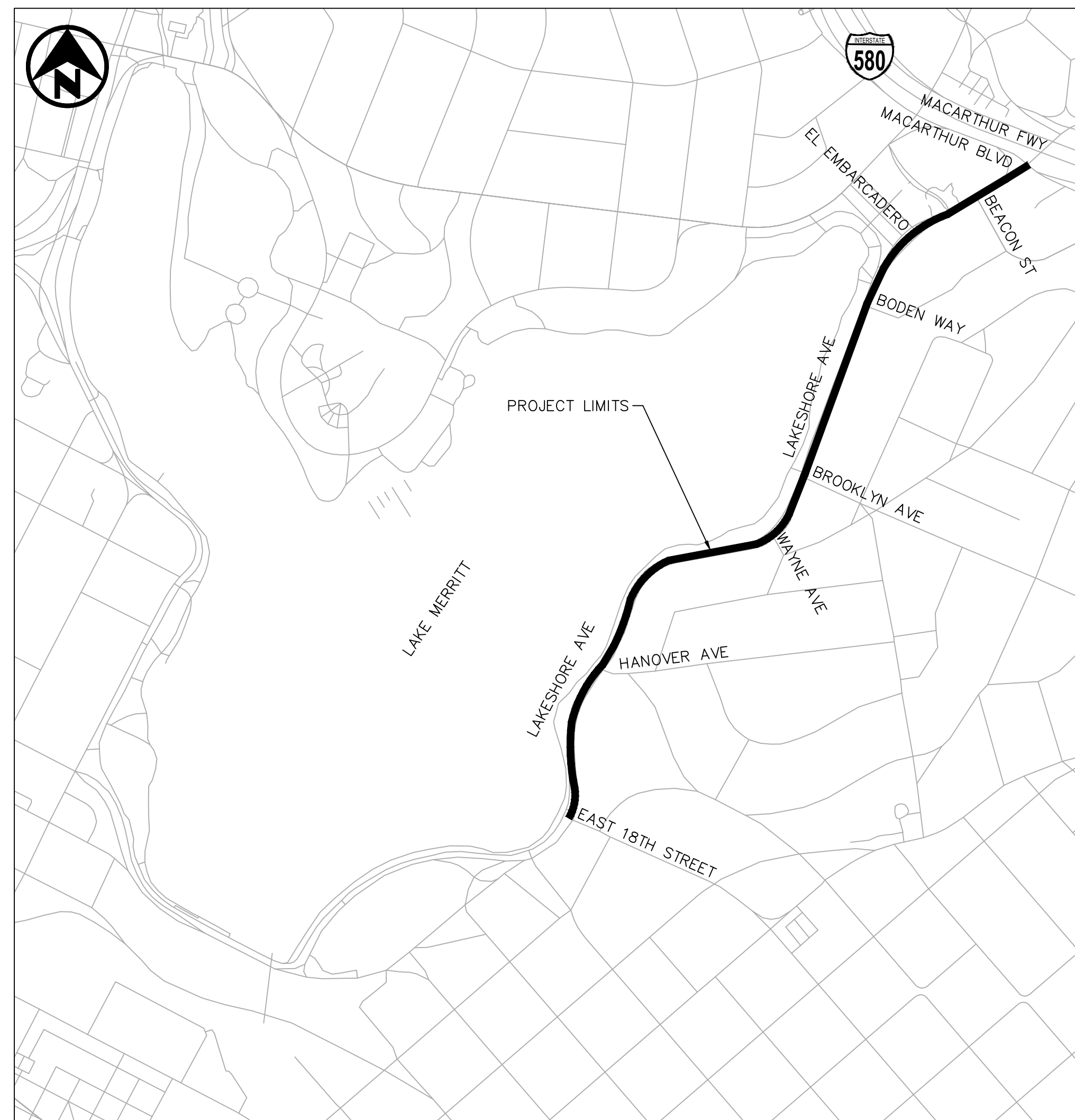
PRINCIPAL CIVIL ENGINEER

Kimley»Horn

1300 CLAY STREET, STE 900
OAKLAND, CA 94612
(510) 625-0712

SHEET INDEX

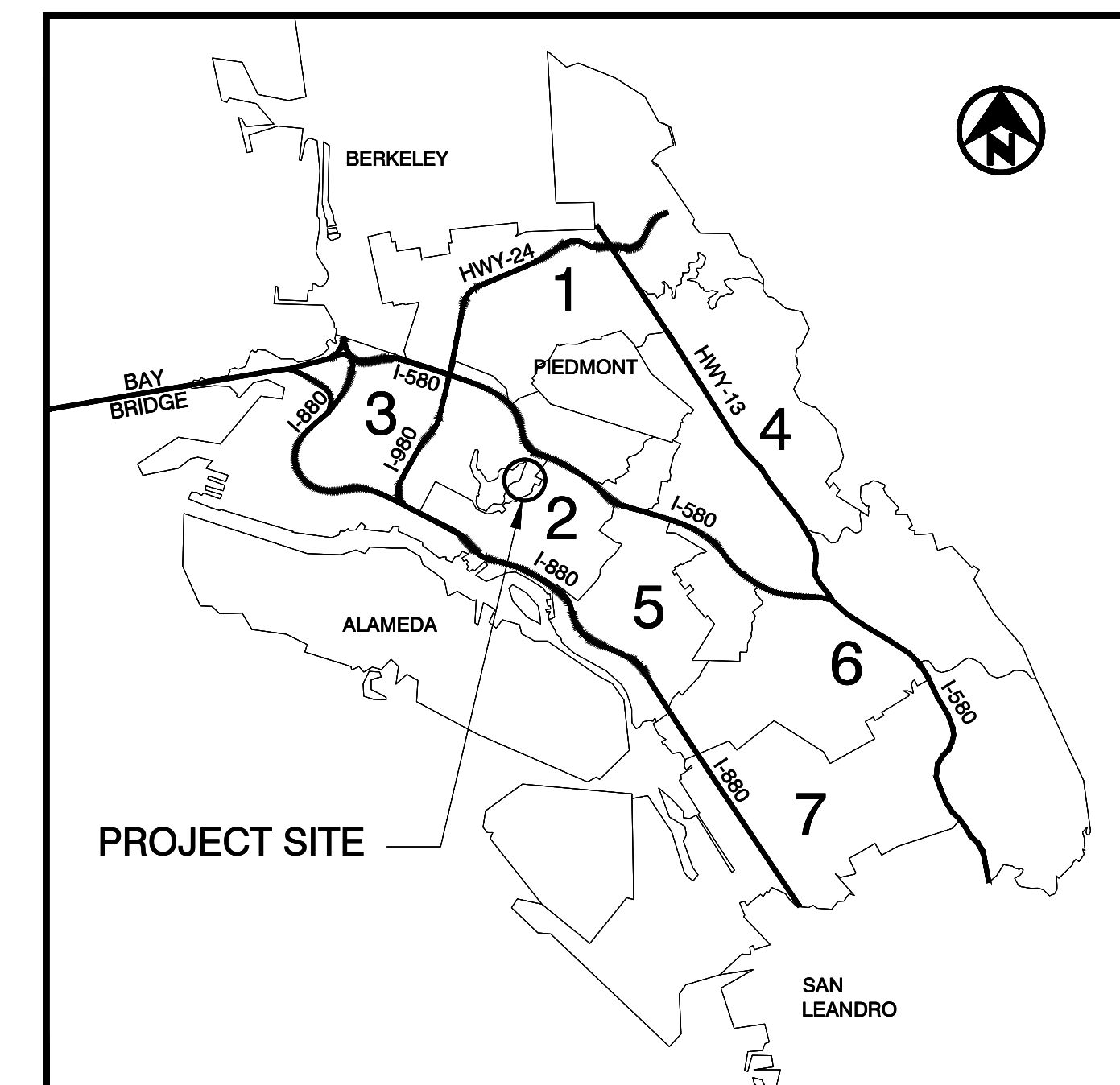
SHEET NO.	SHEET NAME	DESCRIPTION
1	CV-01	COVER SHEET
2	GN-01	GENERAL NOTES
3	WP-01	WATER POLLUTION PREVENTION NOTES
4 - 6	SC-01 - SC-03	SURVEY CONTROL
7	K-01	KEY MAP
8 - 9	X-01 - X-02	TYPICAL SECTIONS
10 - 18	DEM-01 - DEM-09	DEMOLITION PLANS
19 - 27	L-01 - L-09	LAYOUT PLANS
28 - 48	CD-01 - CD-21	CONSTRUCTION DETAILS
49 - 57	PP-01 - PP-09	PAVING PLANS
58 - 66	U-01 - U-09	DRAINAGE AND UTILITY PLANS
67 - 68	DD-01-DD-02	DRAINAGE DETAILS
69 - 77	SS-01 - SS-09	SIGNAGE AND STRIPING PLANS
78 - 80	SSD-01 - SSD-03	SIGNAGE AND STRIPING DETAILS
81 - 83	E-01 - E-03	ELECTRICAL PLANS



LOCATION MAP

NOT TO SCALE

CITY OF OAKLAND COUNCIL DISTRICT MAP



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

**ANY INCREASE IN DISTURBED AREA,
OR CHANGE TO PAVING LIMITS, IS
REQUIRED TO BE REVIEWED BY THE
RESIDENT ENGINEER PRIOR TO
DEMOLITION AND/OR CONSTRUCTION.**

MEGAN WIER
OAKLAND DEPARTMENT OF TRANSPORTATION
ASSISTANT DIRECTOR

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHICAL

No.	DATE	BY	REFERENCE

PROJECT NO.
1007299

SCALE: NTS
DATE: 12/11/2024

SHEET NO.
CV-01
1 OF 83

DRAWING NAME: K:\OAK_BID\1007299\062024 - Lakeshore Ave Cycle Track\01_CADD\Plan Sheets\CV-01.dwg
PLOT DATE: 12/11/24
PLOTTED BY: Priya Pallivathical

PROJECT NOTES	
1.	THE CONTRACTOR SHALL HAVE A CLASS A OR CLASS C12 CONTRACTOR'S LICENSE FOR THIS PROJECT. ALL WORK, MATERIAL, AND EQUIPMENT SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 2015 EDITION, THE CITY OF OAKLAND STANDARD DETAILS FOR PUBLIC WORKS CONSTRUCTION 2002 EDITION, THE APPLICABLE CALIFORNIA DEPARTMENT OF TRANSPORTATION (CALTRANS) 2023 STANDARD SPECIFICATIONS AND STANDARD AND REVISED STANDARD PLANS, THE CURRENT CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE PROJECT SPECIAL PROVISIONS
2.	THE CONTRACTOR SHALL PROVIDE WRITTEN NOTICE TO RESIDENTS AND BUSINESSES AT LEAST 72-HOURS IN ADVANCE OF CONSTRUCTION OPERATIONS.
3.	THE CONTRACTOR SHALL COORDINATE WITH BUILDING OWNERS PRIOR TO THE START OF CONSTRUCTION ADJACENT TO AND IN FRONT OF EXISTING DRIVEWAYS.
4.	THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL WORK TO BE PERFORMED BY SUBCONTRACTORS INCLUDING TRAFFIC CONTROL, CONCRETE CONSTRUCTION, AND OTHER WORK.
5.	THE EXACT LOCATION OF ASPHALT AND CONCRETE CONFORM LINES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. SAWCUTTING OF CONCRETE SIDEWALK ALONG CONFORM LINES SHALL BE LOCATED TO THE NEAREST SCORE LINE. UNLESS OTHERWISE NOTED, ELEVATION OF EXISTING SURFACE AT CONFORM SHOWN IS APPROXIMATE. CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER TO FINALIZE GEOMETRY AT CONFORM PRIOR TO CONSTRUCTION IN THE CONFORM AREAS.
6.	EXTENT OF UNDERGROUND BASEMENTS AND VAULTS HAVE BEEN VISUALLY INSPECTED BUT HAVE NOT BEEN VERIFIED THROUGH DESTRUCTIVE TESTING. CONTRACTOR TO EXAMINE EXISTING SITE CONDITIONS PRIOR TO BEGINNING DEMOLITION. ALL UNDERGROUND BASEMENTS SHALL BE PROTECTED DURING CONSTRUCTION.
7.	THE CONTRACTOR SHALL REPLACE THE PRE-EXISTING WIDTH OF CONCRETE GUTTERS WHEN REPLACING GUTTERS OR CONSTRUCTING CURB RAMPS. IF GUTTER WORK IS TO BE CONSTRUCTED AT A CORNER WHERE THERE ARE TWO DIFFERENT GUTTER WIDTHS, THE NEW GUTTER SHALL TRANSITION SMOOTHLY BETWEEN THE TWO DIFFERENT GUTTER WIDTHS.
8.	THE CONTRACTOR SHALL PROTECT ALL TREES. IF TREE ROOT PRUNING IS REQUIRED, THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER THREE WORKING (3) DAYS IN ADVANCE OF TREE ROOT PRUNING SO THAT AN ARBORIST CAN BE SCHEDULED FOR A TREE STRESS TEST.
9.	AT THE PRE-CONSTRUCTION MEETING, THE CONTRACTOR SHALL SUBMIT FOR REVIEW AND APPROVAL, A LIST OF BEST MANAGEMENT PRACTICES TO PREVENT POTABLE WATER FROM ENTERING THE STORM DRAIN SYSTEM DURING ALL CONSTRUCTION OPERATIONS. ALL PROJECT STORM DRAIN INLETS SHALL BE BLOCKED WITH SILT SACK AND/OR GRAVEL BAGS DURING CONSTRUCTION.
10.	CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE CITY FOR REVIEW AND APPROVAL, 10 WORKING DAYS PRIOR TO CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR TAKING NECESSARY PRECAUTIONS TO ENSURE PEDESTRIAN SAFETY, INCLUDING APPROPRIATE SIGNAGE AND BARRICADES.
11.	IN ACCORDANCE WITH SPECIFICATION SECTIONS 7-9, THE CONTRACTOR SHALL REPAIR OR REPLACE ALL EXISTING IMPROVEMENTS DAMAGED OR REMOVED AS A RESULT OF CONSTRUCTION OPERATIONS. IN ADDITION, ANY EXISTING FACILITIES DAMAGED OR REMOVED AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED IN-KIND BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE AND NO ADDITIONAL PAYMENT SHALL BE MADE.
12.	THE CONTRACTOR'S ATTENTION IS CALLED TO VARIOUS UTILITIES AFFECTED BY THE PROPOSED PAVING AND CONCRETE WORK. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE COORDINATION AND SCHEDULING OF UTILITY COMPANIES FOR ALL THE UTILITY STRUCTURE ADJUSTMENTS TO THE PROPOSED PAVEMENT ELEVATION. THE LOCATION AND SIZE OF MAJOR UNDERGROUND FACILITIES AND UTILITIES SHOWN HEREON ARE SCHEMATIC IN NATURE, USING INFORMATION FURNISHED BY THE RESPECTIVE OWNER AGENCIES. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL DATA WITH THE RESPECTIVE AGENCIES AND TAKING PROPER PRECAUTIONS TO PROTECT AND AVOID THE EXISTING FACILITIES AND UTILITIES. DAMAGE TO UTILITY LINES WILL BE REPAIRED BY THE RESPECTIVE AGENCY AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL ADJUST TO FINISHED GRADE ALL AFFECTED MANHOLES, STREET MONUMENTS, WATER VALVES, GAS VALVES, UTILITY BOXES, CLEANOUT FRAMES, AND COVERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR RESTORING ALL PULL BOX CONTENTS AFFECTED BY THE WORK TO ITS ORIGINAL STATE.
13.	ALL EXISTING SIGNS ARE TO REMAIN UNLESS SPECIFICALLY DESIGNATED TO BE REMOVED, SALVAGED, OR RELOCATED. REMOVED SIGNS TO BE SALVAGED SHALL BE STORED AT THE CITY OF OAKLAND MAINTENANCE YARD LOCATED AT 7101 EDGEWATER DRIVE. THE CONTRACTOR SHALL RESTORE ALL THE EXISTING SIGNS. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE FOR THE VARIOUS WORK ITEMS AND NO ADDITIONAL PAYMENT SHALL BE MADE. ALL SALVAGED EQUIPMENT SHALL BE DELIVERED TO THE MUNICIPAL SERVICE CENTER AT 7101 EDGEWATER DRIVE. CONTACT THE ELECTRICAL CONSTRUCTION AND MAINTENANCE PLANNER AT (510) 615-3685, 48 HOURS IN ADVANCE, PRIOR TO DELIVERY.
14.	ALL TRAFFIC STRIPING SHALL BE THERMOPLASTIC, AND ALL PAVEMENT MARKINGS SHALL BE WHITE THERMOPLASTIC, UNLESS NOTED OTHERWISE.
15.	EXISTING STRIPING AND PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH PROPOSED STRIPING AND PAVEMENT MARKINGS SHALL BE REMOVED AND DISPOSED OF PER SECTION 15-2.02 (B&C) AND 15-2.03 OF THE STATE STANDARD SPECIFICATIONS.
16.	PEDESTRIAN, PUBLIC ACCESS, AND WHEELCHAIR ACCESS SHALL BE MAINTAINED DURING THE CONSTRUCTION TO THE SATISFACTION OF THE CITY ENGINEER.
17.	NO TRENCHES OR HOLES SHALL BE LEFT OPEN OVERNIGHT; USE STEEL PLATING OR HOT MIX ASPHALT AS REQUIRED TO PROTECT OPEN TRENCHES OVERNIGHT.
18.	ALL REVISIONS TO THE PLANS MUST BE REVIEWED AND APPROVED BY THE CITY ENGINEER PRIOR TO CONSTRUCTION AND SHALL BE ACCURATELY SHOWN ON REVISED PLANS STAMPED AND SIGNED BY CITY ENGINEER PRIOR TO THE INSTALLATION OF THE IMPROVEMENTS.
19.	THE CONTRACTOR SHALL PROVIDE TWO (2) SETS OF PROJECT AS-BUILT DRAWINGS TO THE RESIDENT ENGINEER WITH THE FINAL PAYMENT REQUEST.

GENERAL NOTES	
1.	THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (USA) AT (800) 227-2600 OR 811 AT LEAST TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION AND REQUEST DELINEATION OF UNDERGROUND UTILITIES ON THE SURFACE OF THE STREET IN THE AREA TO BE EXCAVATED. NOTE THAT MARKINGS ARE ONLY VALID FOR FOURTEEN (14) DAYS. IF THE MOST RECENT REQUEST HAS ELAPSED THE 14-DAY PERIOD, THE CONTRACTOR NEEDS TO REQUEST USA TO RENEW THE MARKINGS.
2.	THE CONTRACTOR SHALL CALL THE CITY OF OAKLAND ELECTRICAL DEPARTMENT AT (510) 615-5438 AND THE OFFICE OF INFORMATION TECHNOLOGIES AT (510) 238-2996 AT LEAST FIVE WORKING DAYS PRIOR TO STARTING WORK FOR FIELD MARKING OF ALL CITY ELECTRICAL AND FIRE ALARM FACILITIES. REFER TO SPECIFICATION SECTION 7-9 FOR SPECIAL REQUIREMENTS REGARDING DAMAGE TO CITY ELECTRICAL FACILITIES DURING CONSTRUCTION.
3.	THE CONTRACTOR SHALL CONTROL DUST AT ALL TIMES AND SWEEP STREETS AS OFTEN AS NECESSARY DURING CONSTRUCTION AS REQUIRED BY THE CITY ENGINEER.
4.	ALL CONSTRUCTION STAKING FOR CURB, GUTTER, CURB RAMP, SIDEWALK, TRANSIT ISLAND, RAISED ISLANDS, STORM DRAIN INLETS, ELECTROLIERS, ECT. SHALL BE DONE BY A REGISTERED CIVIL ENGINEER OR LICENSED LAND SURVEYOR.
5.	THE CONTRACTOR SHALL PROVIDE FOR CONTINUOUS INGRESS AND EGRESS TO ALL PROPERTIES ADJACENT TO THE WORK THROUGHOUT THE PERIOD OF CONSTRUCTION UNLESS OTHERWISE SHOWN.
6.	THE CONTRACTOR SHALL NOTIFY AC TRANSIT, WASTE MANAGEMENT, AND EMERGENCY SERVICES OF THE CONSTRUCTION SCHEDULE TO ALLOW COORDINATION. AC TRANSIT SHALL BE NOTIFIED AT LEAST 2 WEEKS PRIOR TO THE START OF WORK AT LOCATIONS THAT INCLUDE EXISTING OR PROPOSED BUS STOP LOCATIONS.
7.	CONTRACTOR SHALL PREPARE AND SUBMIT A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) PER THE SPECIAL PROVISIONS. SEE WP-01 FOR ADDITIONAL INFORMATION. <u>SURVEY MONUMENT.</u>
8.	THIS PROJECT HAS SEVERAL MONUMENTS AS SHOWN IN THE SURVEY CONTROL SHEETS. ALTHOUGH UNEXPECTED AT THIS TIME, ADDITIONAL MONUMENTS MAY BE PRESENT OR UNCOVERED AND SHALL BE CONSIDERED TO BE INCLUDED IN THE PROJECT REQUIREMENTS FOR MONUMENTS. THE CITY SURVEY OFFICE HAS PREPARED A CD OF KNOWN BENCHMARKS AND MONUMENTS IN ADOBE ACROBAT PDF FORMAT FOR USE BY THE CONTRACTOR. THE CONTRACTOR IS HEREBY ADVISED TO BECOME FAMILIAR WITH SECTION 8771 OF THE BUSINESS AND PROFESSIONS CODE OF THE STATE OF CALIFORNIA. THIS SECTION DEALS WITH THE REQUIREMENTS IMPOSED IN MONUMENT PRESERVATION AND PROTECTION AND THAT ARE PARTIALLY ASSIGNED TO THE CONTRACTOR BY VIRTUE OF THIS PROJECT'S SPECIFICATIONS. THE CONTRACTOR'S CALIFORNIA-LICENSED SURVEYOR SHALL REFERENCE THE LOCATION AND FILE A CORNER RECORD WITH THE COUNTY SURVEYOR OF ALAMEDA FOR ALL KNOWN PROJECT MONUMENTS BEFORE DOING ANY CONSTRUCTION WORK. THE CONTRACTOR SHALL PROVIDE OFFICIAL COPIES OF THE COUNTY'S CORNER RECORDS TO THE CITY SURVEYOR BEFORE DOING ANY CONSTRUCTION WORK ON PROJECT STREETS. THE CONTRACTOR SHALL NOTE THAT IT CAN TAKE ALAMEDA COUNTY RECORDS OFFICE UP TO FOUR WEEKS TO ACCEPT AND FILE CORNER RECORDS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE PAID FOR BID ITEM <u>PRE & POST CONSTRUCTION MONUMENT VERIFICATION.</u> IF IT IS NECESSARY TO REMOVE OR REPLACE A CITY MONUMENT IN CONJUNCTION WITH THE WORK ASSOCIATED WITH THIS CONTRACT, THE CONTRACTOR SHALL PROVIDE TWO WEEKS NOTIFICATION TO THE OFFICE OF THE CITY SURVEYOR AND, UNDER THE DIRECTION AND SUPERVISION OF THE CITY SURVEYOR, THE MONUMENT WILL EITHER BE TEMPORARILY REMOVED AND LATER REPLACED OR IT WILL BE RELOCATED TO ANOTHER LOCATION. THE UNIT PRICE FOR EACH BID ITEM <u>RESET MONUMENT</u> SHALL INCLUDE FULL COMPENSATION FOR ALL LABOR, MATERIALS, SURVEYING, AND FILING COSTS AS REQUIRED BY LAW FOR RESETTLEMENT/RELOCATING A MONUMENT. IN THE EVENT THAT RAISING A MONUMENT CREATES A WELL DEPTH GREATER THAN 15" FROM MONUMENT TO SURFACE, THE MONUMENT SHALL BE RESET AND ALSO PAID FOR BY BID ITEM <u>RESET MONUMENT</u> . IF THE CONTRACTOR SHALL REMOVE, DESTROY, OR DAMAGE ANY CITY MONUMENT WITHOUT PRIOR NOTIFICATION TO THE CITY SURVEYOR, THE CONTRACTOR SHALL BE RESPONSIBLE TO RESTORE SAID MONUMENT. IN THE EVENT THAT THE MONUMENT CANNOT BE REPLACED IN ITS ORIGINAL LOCATION, THE CONTRACTOR SHALL REPLACE IT IN AN ALTERNATE LOCATION AS DIRECTED BY THE CITY SURVEYOR OR HIS REPRESENTATIVE. THE CONTRACTOR SHALL ABSORB ALL COSTS TO RESTORE OR REPLACE SUCH MONUMENTS. SUCH WORK SHALL INCLUDE ALL MATERIALS, LABOR, SURVEYING AND FILING COSTS AS MAY BE REQUIRED BY THE LAW. ALL MONUMENTS SHALL BE RAISED OR LOWERED TO THE FINISH GRADE DURING CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL SHOW THE LOCATION OF ALL CITY MONUMENTS ON THE SUBMITTED STRIPING PLANS.
9.	THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN AT ALL TIMES TEMPORARY FENCES, BARRICADES, LIGHTS, SIGNS, AND OTHER DEVICES NECESSARY TO PREVENT ACCIDENTS, DAMAGES, OR INJURIES TO THE PUBLIC.
10.	THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE EXISTING DRAINAGE PATH WITHIN THE LIMITS OF WORK.
11.	THE CONTRACTOR SHALL NOT ALLOW DEBRIS OR SILT-LADEN WATER GENERATED BY CONSTRUCTION ACTIVITIES OR SAWCUTTING TO FLOW INTO THE PUBLIC STORM DRAIN SYSTEM, NOR BE DEPOSITED INTO THE PUBLIC RIGHT OF WAY, AS SPECIFIED IN SECTION 7-8.6 OF THE SPECIAL PROVISIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING SILT MATERIAL FROM THE JOB SITE. FULL COMPENSATION FOR PERFORMING THIS WORK SHALL BE INCLUDED IN THE PRICE PAID FOR THE VARIOUS ITEMS OF WORK, AND NO ADDITIONAL PAYMENT WILL BE ALLOWED.
12.	SAWCUTTING SHALL BE TO A MINIMUM DEPTH OF 2" FOR ASPHALT CONCRETE PAVING AND FULL DEPTH FOR CONCRETE PAVING. NO STOMPING WILL BE PERMITTED.
13.	REMOVAL AND REPLACEMENT OF CONCRETE CURB AND GUTTER SHALL EXTEND TO THE NEAREST WEAKENED PLANE JOINTS. SIDEWALK REMOVAL AND REPLACEMENT SHALL EXTEND TO THE NEAREST SCORE MARK.
14.	THE CONTRACTOR SHALL PROVIDE A PROJECT SCHEDULE FOR THE CITY'S REVIEW AND APPROVAL. <u>MONUMENT REGULATIONS</u>
15.	THE CONTRACTOR'S ATTENTION IS DIRECTED SPECIFICALLY TO SECTION 8771 OF THE STATE'S BUSINESS PROFESSIONS CODE REQUIRING THE PRESERVATION AND PROTECTION OF MONUMENTS. THIS SECTION PROVIDES FOR SPECIFIC REQUIREMENTS BEFORE WORK CAN BE DONE AND THOSE REQUIREMENTS ARE OFTEN TIME CONSUMING. THE CITY OF OAKLAND WILL PROVIDE SUCH MAPPING AND FLING SERVICES AS ARE NECESSARY UPON TWO WORKING DAYS NOTICE FROM THE CONTRACTOR (DIRECTLY TO THE CITY SURVEYOR WITH A COPY TO THE INSPECTOR). IN COMPLIANCE WITH THIS LAW, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY AND PROTECT ALL MONUMENTS WITHIN WORK AREAS SHOWN ON THE INDEX SHEET AND PLANS.
16.	NO MONUMENTS MAY BE DISTURBED, NOR WORK PERFORMED WITHIN A 25 FOOT RADIUS OF SUCH MONUMENT UNTIL THE CITY SURVEYOR HAS MADE THE NECESSARY MEASUREMENTS AND ISSUED A WRITTEN AUTHORIZATION TO THE INSPECTOR.
17.	SHOULD THE CONTRACTOR, PRIOR TO SUCH AUTHORIZATION, DISTURB, REMOVE, OR DESTROY ANY MONUMENT, THE CITY HAS A FLAT CHARGE OF \$5,000 PER UNIT WHICH WILL BE DEDUCTED FROM THE NEXT PROGRESS (OR FINAL) PAYMENT. THE CONTRACTOR SHOULD NOTE THAT, PER 8771 B&P, THE CITY HAS THE RIGHT TO WITHHOLD THE ISSUANCE OF A NOTICE OF COMPLETION UNTIL ALL OUTSTANDING ISSUES RELATING TO THE MONUMENTS ARE RESOLVED.
18.	IF ANY MONUMENT IS LAWFULLY REMOVED OR DESTROYED, THE CONTRACTOR SHALL SALVAGE THE FRAME, CASTING, AND MONUMENT DISH AND DELIVER THEM TO THE CITY SURVEYOR. IF THESE ITEMS ARE NOT SALVAGED/OR DELIVERED, THE CONTRACTOR SHALL REPLACE THE LOST UNITS, IN KIND, DELIVERED TO THE CITY SURVEYOR.
19.	NOT ALL MONUMENTS MAY BE SHOWN ON THE PLAN SHEETS. PRIOR TO THE START OF ANY CONSTRUCTION, THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AND MARK ALL MONUMENTS WITH RED PAINT OR, IF FOUND IN UNPAVED AREA, WITH A 3 FOOT LATH FLAGGED AND PAINTED IN RED. PLEASE NOTE THAT STATE LAW MAKES NO DISTINCTION BETWEEN CITY MONUMENTS AND THOSE OF OTHER AGENCIES OR INDIVIDUALS.
20.	IN ADDITION TO MONUMENTS, THE CITY ALSO HAS VERTICAL MONUMENTATION (BENCHMARKS) WHICH ARE LOCATED ON THE PLAN SHEETS. IF A BENCHMARK IS TO BE DESTROYED, THE CITY WILL REQUIRE TWO WORKING DAYS ADVANCE NOTICE TO PRESERVE THE INFORMATION. THE CONTRACTOR, AT NO ADDITIONAL COST TO THE CITY, SHALL REPLACE THE MONUMENT IN A SUITABLE LOCATION TO BE DETERMINED BY THE CITY SURVEYOR. THE CITY WILL PROVIDE THE NECESSARY BRONZE DISK.
21.	THE CITY MAY, AT ITS SOLE DISCRETION, ELECT TO INSTALL A BENCHMARK TO BE SET IN A SIDEWALK OR CURB WHERE THERE WAS NONE BEFORE. THE CITY WILL PROVIDE A BRONZE DISK AND SPECIFY THE LOCATION FOR THE DISK TO BE REPLACED WHEN THE CONCRETE IS PLACED, AND DRESSED TO FINISH ALONG WITH THE CONCRETE SURFACE. THIS SHALL BE CONSIDERED AS PART OF THE COST OF THE CONCRETE REPLACEMENT AND THERE SHALL BE NO ADDITIONAL CHARGE TO THE CITY.
22.	SEE CITY OF OAKLAND STANDARD DETAIL S-9 FOR MONUMENT AND BENCHMARK DETAIL.
23.	THE CONTRACTOR SHALL PROVIDE OFFICIAL COPIES OF THE COUNTY CORNER RECORDS TO THE CITY SURVEYOR BEFORE DOING ANY CONSTRUCTION WORK ON PROJECT STREETS. CONTRACTOR SHALL FILE CORNER RECORDS TO THE CITY IN BOTH HARD COPY AS WELL AS SINGLE ADOBE FILE FOR EACH CORNER RECORD.

ABBREVIATIONS	
AB	AGGREGATE BASE
AC	ASPHALT CONCRETE
APPROX	APPROXIMATE
AT&T	AT&T TELECOMMUNICATION
AVE	AVENUE
BLVD	BOULEVARD
BOW	BACK OF WALK
BR	BASE REPAIR
CB	CATCH BASIN
C&G	CURB AND GUTTER
/CL	CENTERLINE
CO	CLEANOUT
CONT	CONTINUE
CR	CURB RAMP
CTV	CABLE TELEVISION LINE
DI	DRAINAGE INLET
DWG	DRAWING
E	ELECTRIC LINE/EAST/EASTINGS
ELEC	ELECTRICAL
EX	EXISTING
EBMUD	EAST BAY MUNICIPAL UTILITY DISTRICT
EL	ELEVATION
EP	EDGE OF PAVEMENT
ETW	EDGE OF TRAVEL WAY
F/C	FACE OF CURB
FG	FINISH GRADE
FH	FIRE HYDRANT
FL	FLOWLINE
FT	FEET
FWY	FREEWAY
G	GAS LINE
GB	GRADE BREAK
GM	GAS METER
GV	GAS VALVE
IRR	IRRIGATION
JR	JOINT POLE
LAT	LATERAL
L	LENGTH
LF	LINEAR FEET
LG	LIP OF GUTTER
MH	MAINTENANCE HOLE
MAX	MAXIMUM
MIN	MINIMUM
MON	CITY SURVEY MONUMENT
(N)	NEW
N	NORTH/NOTHING
N/A	NOT APPLICABLE
NB	NORTH BOUND
NO	NUMBER
NTS	NOT TO SCALE
OC	ON CENTER
PCC	PORTLAND CEMENT CONCRETE/POINT OF COMPOUND CURVATURE
PG&E	PACIFIC GAS & ELECTRIC
PRC	POINT OF REVERSE CURVATURE
R/W	RIGHT-OF-WAY
S	SLOPE/SOUTH
SB	SOUTH BOUND
SD	STORM DRAIN
SF	SQUARE FEET
SS	SANITARY SEWER
SPECS	STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 2015 EDITION
SP	SPECIAL PROVISIONS
ST	STREET
STA	STATION
SW	SIDEWALK
SDMH	STORM DRAIN MANHOLE
SSMH	SANITARY SEWER MANHOLE
T/TEL	TELEPHONE LINE
TI	TECHNICAL INFERENCE/ABILITY
TC	TOP OF CURB
TS	TRAFFIC SIGNAL
TP	TELEPHONE POLE
TV	TELEVISION
TYP.	TYPICAL
UG	UNDERGROUND
VAR	VARIABLE/VARIES
W	WEST
WM	WATER METER
WV	WATER VALVE
WY	WAY

LEGEND	
⊛	FIRE HYDRANT
⊙	EXISTING MONUMENT OR BENCHMARK
⊖	EX WATER VALVE
⊕	EX MAINTENANCE HOLE STRUCTURE. UTILITY AS NOTED ON PLANS, WHEN KNOWN.
⊗	EX PG&E GAS VALVE
≡	EX DRAINAGE INLET STRUCTURE
⊖	UTILITY POLE

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley-Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

GENERAL NOTES

PROJECT NO.
1007299

SCALE: N/A SHEET NO. GN-01
2 OF 83

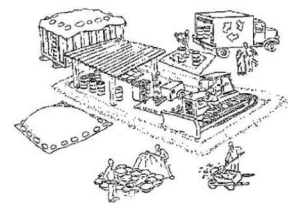
DATE: 12/11/2024

DRAWING NAME: K:\OAK_BDW\100709299- Lakeshore Ave Cycle Tracks\04_CADD\Plan_Sheets\GN-01_WP-01.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya.Pallivathucal

Pollution Prevention - It's Part of the Plan

Make sure your crews and subs do the job right!

Runoff from streets and other paved areas is a major source of pollution and damage to creeks and the San Francisco Bay. Construction activities can directly affect the health of creeks and the Bay unless contractors and crews plan ahead to keep dirt, debris, and other construction waste away from storm drains and local creeks. Following these guidelines and the project specifications will ensure your compliance with City of Oakland requirements.



Materials storage & spill cleanup

Non-hazardous materials management

- ✓ Sand, dirt, and similar materials must be stored at least 10 feet (3 meters) from catch basins. All construction material must be covered with a tarp and contained with a perimeter control during wet weather or when rain is forecasted or when not actively being used within 14 days.
- ✓ Use (but don't overuse) reclaimed water for dust control as needed.
- ✓ Sweep or vacuum streets and other paved areas daily. Do not wash down streets or work areas with water.
- ✓ Recycle all asphalt, concrete, and aggregate base material from demolition activities.
- ✓ Repair or replace leaking dumpsters promptly.
- ✓ Check dumpsters regularly for leaks and to make sure they are not overfilled.
- ✓ Comply with City of Oakland Ordinances for recycling construction materials, wood, gyp board, pipe, etc.
- ✓ Cover all dumpsters with a tarp at the end of every work day or during wet weather.

Hazardous materials management

- ✓ Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state, and federal regulations.
- ✓ Store hazardous materials and wastes in water tight containers, store in appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecasted.
- ✓ Follow manufacturer's application instructions for hazardous materials and be careful not to use more than necessary. Do not apply chemicals outdoors when rain is forecasted within 24 hours.
- ✓ Be sure to arrange for appropriate disposal of all hazardous wastes.

Spill prevention and control

- ✓ Keep a stockpile of spill cleanup materials (rags, absorbents, etc.) available at the construction site at all times.
- ✓ When spills or leaks occur, contain them immediately and be particularly careful to prevent leaks and spills from reaching the gutter, street, or storm drain.
- ✓ Never wash spilled material into a gutter, street, storm drain, or creek.
- ✓ Dispose of all containment and cleanup materials properly.
- ✓ Report any hazardous materials spills immediately! Dial 911 or City of Oakland, Public Works Agency hotline at (510) 615-5566.

Construction Entrances and Perimeter

- ✓ Establish and maintain effective perimeter controls and stabilize all construction entrances and exits to sufficiently control erosion and sediment discharges from site and tracking off site.
- ✓ Sweep or vacuum any street tracking immediately and secure sediment source to prevent further tracking.

Vehicle and equipment maintenance & cleaning

- ✓ Inspect vehicles and equipment for leaks frequently. Use drip pans to catch leaks until repairs are made; repair leaks promptly.
- ✓ Fuel and maintain vehicles on site only in a bermed area or over a drip pan that is big enough to prevent runoff.
- ✓ If you must clean vehicles or equipment on site, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or creeks.
- ✓ Do not clean vehicles or equipment on-site using soaps, solvents, degreasers, steam cleaning equipment, etc.



Earthwork & contaminated soils

- ✓ Keep excavated soil on the site where it will not collect in the street.
- ✓ Transfer to dump trucks should take place on the site, not in the street.
- ✓ Use fiber rolls, silt fences, or other control measures to minimize the flow of silt off the site.

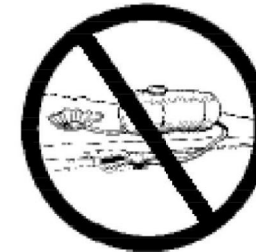


- ✓ Earth moving activities shall be approved by the City Resident Engineer in the Field.
- ✓ Mature vegetation is the best form of erosion control. Minimize disturbance to existing vegetation whenever possible.
- ✓ If you disturb a slope during construction, prevent erosion by securing the soil with erosion control fabric, or seed with fast-growing grasses as soon as possible. Place fiber rolls down-slope until soil is secure.

- ✓ If you suspect contamination (from site history, discoloration, odor, texture, abandoned underground tanks or pipes, or buried debris), call the Engineer for help in determining what should be done, and manage disposal of contaminated soil according to their instructions.

Dewatering operations

- ✓ Effectively manage all run-on, all runoff within the site, and all runoff that discharges from the site. Run-on from off site shall be directed away from all disturbed areas or shall collectively be in compliance.
- ✓ Reuse water for dust control, irrigation, or another on-site purpose to the greatest extent possible.
- ✓ Be sure to notify and obtain approval from the Engineer before discharging water to a street, gutter, or storm drain. Filtration or diversion through a basin, tank, or sediment trap may be required.
- ✓ In areas of known contamination, testing is required prior to reuse or discharge of groundwater. Consult with the Engineer to determine what testing is required and how to interpret results. Contaminated groundwater must be treated or hauled off-site for proper disposal.

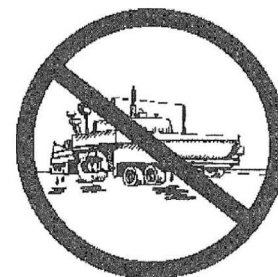


Saw cutting

- ✓ Always completely cover or barricade storm drain inlets when saw cutting. Use filter fabric, catch basin inlet filters, or sand/gravel bags to keep slurry out of the storm drain system.
- ✓ Shovel, absorb, or vacuum saw-cut slurry and pick up all waste as soon as you are finished in one location or at the end of each work day (whichever is sooner!).
- ✓ If saw cut slurry enters a catch basin, clean it up immediately.

Paving/asphalt work

- ✓ Always cover storm drain inlets and manholes when paving or applying seal coat, tack coat, slurry seal, or fog seal.
- ✓ Protect gutters, ditches, and drainage courses with sand/gravel bags, or earthen beers.
- ✓ Do not sweep or wash down excess sand from sand sealing into gutters, storm drains, or creeks. Collect sand and return it to the stockpile, or dispose of it as trash.
- ✓ Do not use water to wash down fresh asphalt concrete pavement.



Concrete, grout, and mortar storage & waste disposal

- ✓ Store concrete, grout, and mortar under cover, on pallets, and away from drainage areas. These materials must never reach a storm drain.
- ✓ Wash out concrete equipment/trucks off-site or into contained washout areas that will not allow discharge of wash water onto the underlying soil or onto the surrounding areas.



- ✓ Collect the wash water from washing exposed aggregate concrete and remove it for appropriate disposal off site.

Painting

- ✓ Never rinse paint brushes or materials in a gutter or street!
- ✓ Paint out excess water-based paint before rinsing brushes, rollers, or containers in a sink.
- ✓ Paint out excess oil-based paint before cleaning brushes in thinner.
- ✓ Filter paint thinners and solvents for reuse whenever possible.
- ✓ Dispose of oil-based paint sludge and unusable thinner as hazardous waste.



Landscape Materials

- ✓ Contain, cover, and store on pallets all stockpiled landscape materials (mulch, compost, fertilizers, etc.) during wet weather or when rain is forecasted or when not actively being used within 14 days.
- ✓ Discontinue the application of any erodible landscape material within 2 days of forecasted rain and during wet weather.

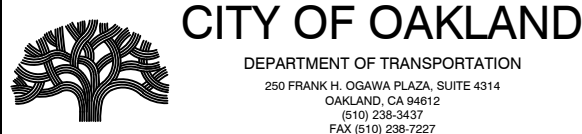
Storm drain polluters may be liable for fines of \$10,000 or more per day!

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

WATER POLLUTION
PREVENTION

PROJECT NO.
1007299

SCALE:
N/A

DATE: 12/11/2024

SHEET NO.
WP-01
3 OF **83**

SURVEYOR'S STATEMENT:

THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE PROFESSIONAL LAND SURVEYORS' ACT.

I HEREBY FURTHER STATE THAT TO THE BEST OF MY KNOWLEDGE ALL PROVISIONS OF APPLICABLE STATE LAWS AND LOCAL ORDINANCES HAVE BEEN COMPLIED WITH.

I HEREBY ACKNOWLEDGE THAT THIS SURVEY SHALL BE A PUBLIC RECORD AND MAY BE AVAILABLE FOR INSPECTION AND DISTRIBUTION TO THE GENERAL PUBLIC.

ALL HORIZONTAL COORDINATES AND LOCATIONS SHOWN ARE BASED ON THE NAD83 (CA ZONE 3) COORDINATE SYSTEM AND WERE DERIVED FROM GPS CONTROL OBSERVATIONS.

ALL GRADES AND ELEVATIONS SHOWN ARE BASED UPON THE OAKLAND CITY DATUM. BENCHMARK "61/1/9" LOCATED AT THE INTERSECTION OF LAKESHORE AND BODEN WAY, ELEVATION 4.744' WAS HELD.

DATED: 08/02/2024

Joseph Brajkovich

JOSEPH M BRAJKOVICH, PLS 5254

PUBLIC ADVISORY:

THIS MAP IS BASED ON PRIVATE SURVEYS PERFORMED BY LICENSED PROFESSIONALS AND WILL NOT BE UPDATED OR CORRECTED BY THE CITY OF OAKLAND AFTER ITS FILING. NO WARRANTY, EITHER EXPRESSED OR IMPLIED, IS MADE BY THE CITY OF OAKLAND THAT THIS MAP AND THE SURVEY INFORMATION ON WHICH IT IS BASED IS CORRECT, ACCURATE, OR CURRENT, NOR THAT THE CITY WILL RETAIN FOR PUBLIC INSPECTION ANY RELATED INFORMATION WHICH MAY BE SUBSEQUENTLY SUBMITTED TO THE CITY, INCLUDING ALLEGED OR ACTUAL DISCREPANCIES, INACCURACIES, DEFICIENCIES, AND ERRORS.

Control Point Table				
Point #	Northing	Easting	Elevation	Raw Description
1	2118996.2296	6054856.5885	3.82	CUT-X
3	2118808.3011	6054791.8281	3.53	CUT-X
4	2118809.7282	6054871.2324	5.29	CUT-X
5	2118733.3436	6054861.4628	5.40	CUT-X
6	2119077.0940	6054784.3544	5.52	CUT-X
7	2119148.8486	6054779.0629	5.67	CUT-X
8	2119077.1979	6054857.0538	4.78	CUT-X
9	2119292.6155	6054812.4467	5.55	CUT-X
10	2119412.1150	6054879.9875	4.93	CUT-X
11	2119487.1704	6054931.9146	3.43	CUT-X
12	2119452.4385	6055031.0632	5.25	CUT-X
13	2119359.1295	6054943.2891	5.76	CUT-X
14	2119351.4250	6054936.2182	5.82	FND-NAIL-TAG-LS4520
15	2119343.2853	6054930.7274	5.01	MON-PIN-1-8-IN
16	2119271.6106	6054892.5096	6.21	CUT-X
17	2119479.5171	6055024.5005	3.55	MON-PIN
18	2119575.5647	6054987.9502	4.15	CUT-X
19	2119698.1007	6055038.6217	4.73	CUT-X
20	2119776.5902	6055076.9764	5.63	CUT-X
21	2119666.8487	6055109.4568	4.62	CUT-X

Control Point Table				
Point #	Northing	Easting	Elevation	Raw Description
22	2119616.4097	6055095.0472	4.07	MON-PIN
23	2119917.4649	6055136.4951	5.18	CUT-X
24	2119955.0473	6055173.4909	5.03	CUT-X
25	2120015.4241	6055295.2546	6.33	CUT-X
26	2119942.3894	6055332.9620	4.22	MON-PIN
27	2120046.9729	6055464.7782	5.33	CUT-X
28	2120072.4246	6055606.4629	4.17	CUT-X
29	2119996.0488	6055634.9838	5.17	CUT-X
30	2119962.5742	6055436.6223	5.15	CUT-X
31	2120155.7641	6055786.8128	3.77	CUT-X
32	2120072.7165	6055755.5667	6.22	CUT-X
33	2120046.4552	6055796.2065	5.67	CUT-X
34	2120085.2368	6055825.1900	5.18	MON-PIN
35	2120317.1118	6055885.4093	4.14	CUT-X
36	2120413.7008	6055909.7798	4.00	CUT-X
37	2120459.5146	6055937.8882	4.18	CUT-X
38	2120528.7188	6055962.8598	4.32	CUT-X
39	2120589.7813	6055985.5439	4.25	CUT-X
40	2120412.4308	6056007.3140	5.13	CUT-X
41	2120299.5150	6055964.1210	5.00	CUT-X

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

PLS Surveys, Inc.
LAND SURVEYORS
(510)261-0900

CHECKED BY: JOE BRAJKOVICH, PLS
DESIGNED BY: JAMES HOUSTON, PLS
DRAWN BY: JAMES HOUSTON, PLS



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO EL EMBARCADERO**

No.	DATE	BY	REFERENCE

SURVEY CONTROL

PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
SC-01
4 OF 79

DATE: 08/02/2024

PROJECT TITLE: ... DRAWING NAME: ... PLOT DATE: 08-04-24 ... PLOTTED BY: ...

Control Point Table				
Point #	Northing	Easting	Elevation	Raw Description
42	2120256.4300	6055948.5706	4.25	MON 1/8 PIN W/WASHER-CITY ENG
43	2120228.4359	6055932.2201	4.87	CUT-X
44	2120681.3560	6056019.3819	4.45	CUT-X
45	2120779.2164	6056055.3477	4.29	CUT-X
46	2120835.3248	6056076.2245	4.13	CUT-X
47	2120893.8633	6056097.7720	4.16	CUT-X
48	2120787.4734	6056144.4449	5.49	CUT-X
49	2120704.9497	6056115.1743	5.46	CUT-X
50	2120548.0195	6056050.1321	5.01	CUT-X
51	2120971.3296	6056126.5031	4.08	CUT-X
52	2121043.6620	6056153.2081	4.41	CUT-X
53	2121112.1034	6056178.5264	4.65	CUT-X
54	2121180.4291	6056203.8939	4.76	CUT-X
55	2121235.5044	6056223.9706	4.62	CUT-X
56	2121308.6171	6056251.0142	4.64	CUT-X
57	2121396.6959	6056367.4925	4.12	MON-PIN
58	2121368.7157	6056275.8813	4.86	CUT-X
59	2121423.8433	6056299.5142	4.99	CUT-X
60	2121478.1025	6056322.1506	5.03	CUT-X
61	2121531.1416	6056350.4649	5.10	CUT-X

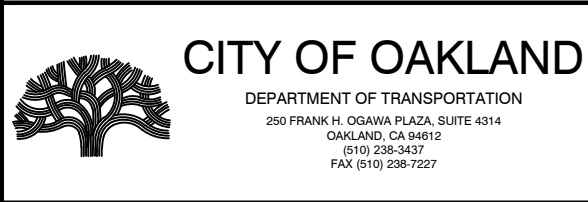
Control Point Table				
Point #	Northing	Easting	Elevation	Raw Description
62	2121257.1681	6056316.9369	5.54	CUT-X
63	2121155.4625	6056278.0187	5.59	CUT-X
64	2121239.6701	6056309.4612	4.74	MON-1-8-PIN
65	2121584.6926	6056397.0947	5.49	CUT-X
66	2121627.4317	6056429.9504	5.35	CUT-X
67	2121598.7348	6056477.2338	5.19	CUT-X
68	2121681.4963	6056476.8714	5.20	CUT-X
69	2121680.5188	6056534.6623	5.01	CUT-X
70	2121545.4234	6056510.2578	5.66	CUT-X
71	2121583.0139	6056548.3920	4.37	MON-PIN
72	2121787.9356	6056740.1798	5.17	CUT-X
73	2121820.3586	6056797.4944	4.87	CUT-X
74	2121854.6347	6056857.1446	4.89	CUT-X
75	2118915.8096	6054876.1206	3.30	MON-1-8-PIN
76	2118700.6177	6054935.0643	5.83	CUT-X
77	2118686.4299	6054991.5212	5.50	CUT-X
78	2121993.9604	6056879.3880	6.90	CUT-X
79	2121981.2247	6056908.6426	6.71	CUT-X
80	2121940.2449	6056936.1429	6.25	CUT-X
81	2121958.9019	6056974.1713	6.96	CUT-X

Control Point Table				
Point #	Northing	Easting	Elevation	Raw Description
82	2121904.5062	6056923.9590	5.76	CUT-X
83	2121732.7172	6056847.6908	5.68	CUT-X
84	2121713.1833	6056771.4474	5.24	CUT-X
671	2119243.5791	6054881.5084	6.18	FND-TAG-4520
1677	2119797.3581	6055160.5326	4.15	MON-PIN
2518	2119996.1415	6055637.0108	4.32	MON-PIN

Joseph Brajkovich

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON
 ANSI B (11" X 17") SHEETS. REFER TO
 GRAPHIC SCALE FOR SHEETS OF
 DIFFERENT SIZES.

PLS Surveys, Inc. LAND SURVEYORS (510)261-0900	
CHECKED BY	JOE BRAJKOVICH, PLS
DESIGNED BY	JAMES HOUSTON, PLS
DRAWN BY	JAMES HOUSTON, PLS



**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO EL EMBARCADERO**

No.	DATE	BY	REFERENCE

SURVEY CONTROL

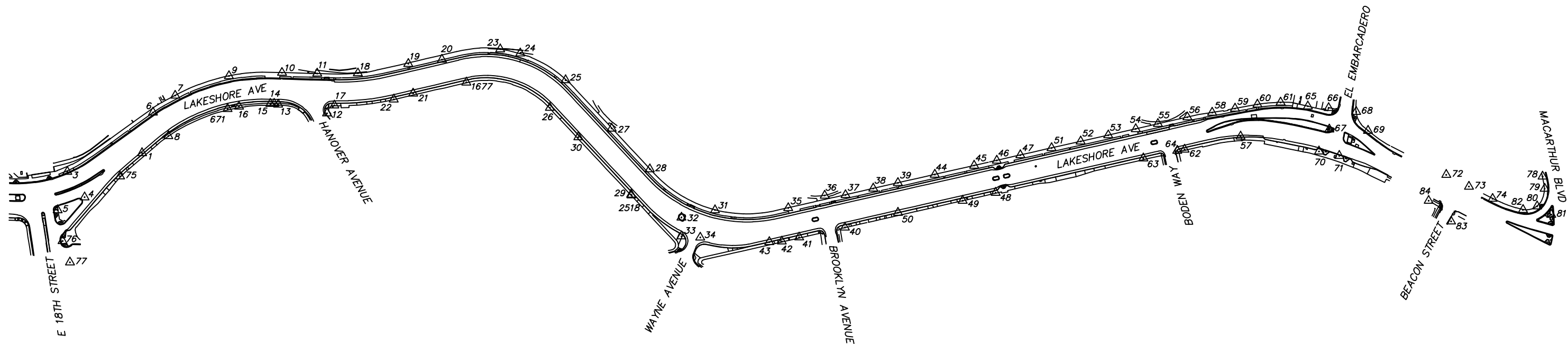
PROJECT NO.
1007299

SCALE:
NO SCALE

SHEET NO.
SC-02
5 OF 79

DATE: 08/02/2024

PROJECT TITLE: 08-04-24 08:04:24 PLOTTED BY: houst

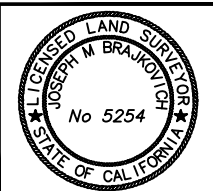


Joseph Brajkovich

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON
 ANSI B (11" X 17") SHEETS. REFER TO
 GRAPHIC SCALE FOR SHEETS OF
 DIFFERENT SIZES.

PLS Surveys, Inc.
 LAND SURVEYORS
 (510)261-0900

CHECKED BY	JOE BRAJKOVICH, PLS
DESIGNED BY	JAMES HOUSTON, PLS
DRAWN BY	JAMES HOUSTON, PLS



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO EL EMBARCADERO**

No.	DATE	BY	REFERENCE

SURVEY CONTROL

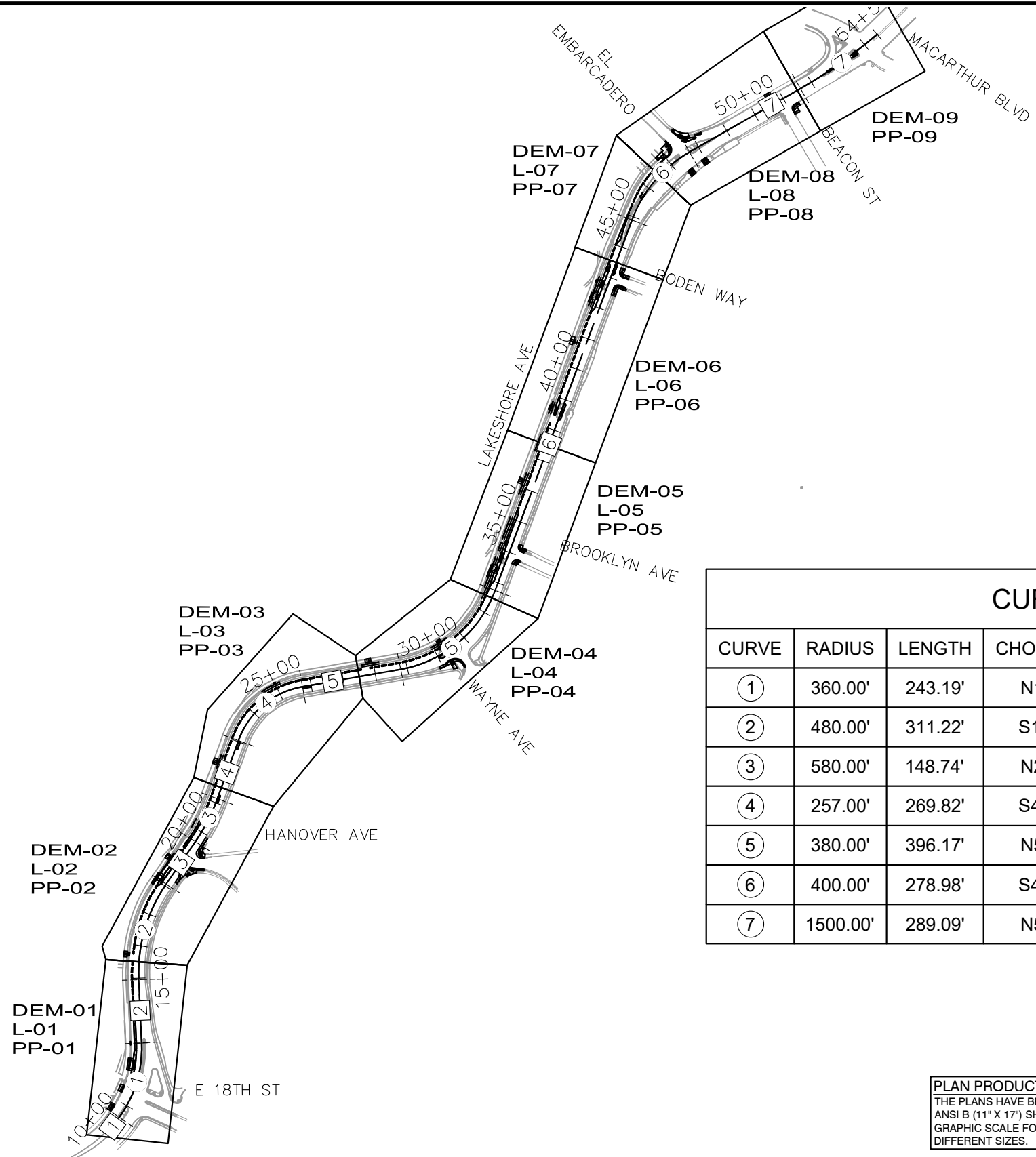
PROJECT NO.
1007299

SCALE:
 NO SCALE

DATE: 08/02/2024

SHEET NO.
SC-03
 6 OF 79

PROJECT TITLE: _____
 DRAWING NAME: C:\Users\jhoust1\OneDrive\Desktop\24-097-SENT-CONTROL_072924.dwg
 PLOT DATE: 08-04-24
 PLOTTED BY: jhoust1



LINE TABLE		
LINE	LENGTH	BEARING
①	57.52	N36°05'36.70"E
②	194.33	N2°36'38.78"W
③	158.35	N34°32'19.62"E
④	189.99	N19°50'43.09"E
⑤	306.17	N79°59'53.66"E
⑥	1213.81	N20°15'53.17"E
⑦	392.96	N60°13'34.89"E

CURVE TABLE						
CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD	DELTA	TANGENT
①	360.00'	243.19'	N16°44'29"E	238.59'	38°42'15"	126.44'
②	480.00'	311.22'	S15°57'50"W	305.80'	37°08'58"	161.30'
③	580.00'	148.74'	N27°11'31"E	148.33'	14°41'37"	74.78'
④	257.00'	269.82'	S49°55'18"W	257.59'	60°09'11"	148.84'
⑤	380.00'	396.17'	N50°07'53"E	378.47'	59°44'00"	218.22'
⑥	400.00'	278.98'	S40°14'44"W	273.36'	39°57'42"	145.44'
⑦	1500.00'	289.09'	N54°42'18"E	288.65'	11°02'33"	145.00'



95% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

KEY MAP

PROJECT NO.
1007299

SCALE:
NTS

SHEET NO.
K-01
7 OF 83

DATE: 12/03/2024

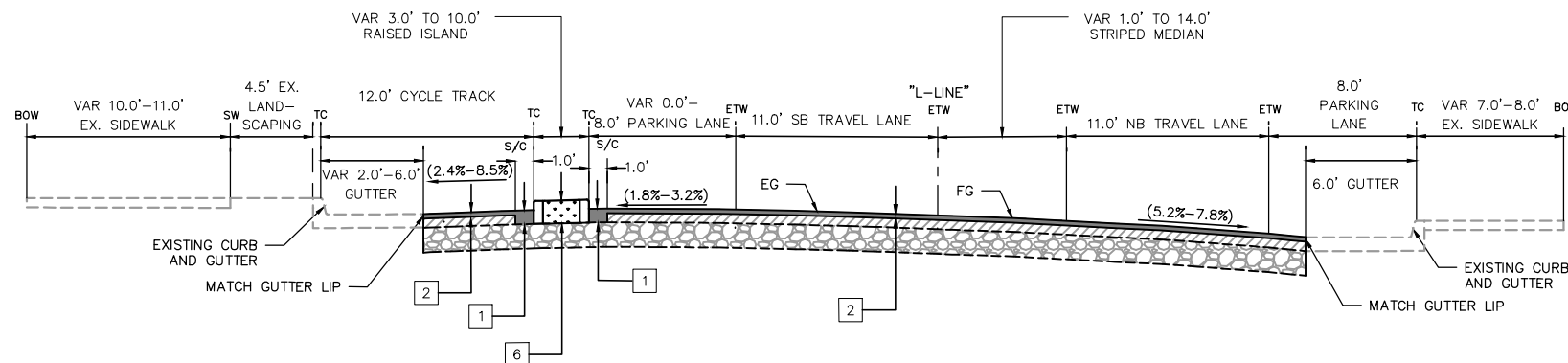
DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Trace\04_CADD\Plan_Sheets\K-1.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal

NOTES:

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE PER THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING INC., DATED MAY 20, 2024. SEE PAVING PLANS FOR APPROXIMATE CORING AND BORING LOCATIONS AND A SUMMARY OF EXISTING PAVEMENT SECTIONS.
- SEE PAVING PLANS FOR EXACT LIMITS OF PAVING.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS.
- CONTRACTOR SHALL SAWCUT TO A NEAT LINE.
- FOR LIMITS AND TYPE OF CURB, SEE LAYOUT PLANS.
- FOR GRADING DETAILS AND LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
- FOR DRAINAGE IMPROVEMENTS, SEE DRAINAGE AND UTILITY PLANS.
- FOR COLD PLANE AND OVERLAY CONFORM LIMITS, SEE PAVING PLANS.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHOULD BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR DIGOUT LOCATIONS, SEE PAVING PLANS.
- DIGOUTS SHOULD BE REPAIRED USING PAVEMENT STRUCTURAL SECTION 1. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHOULD BE REMOVED. THE UNDERLYING SUBGRADE SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED NOVEMBER 15, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED NOVEMBER 15, 2024.

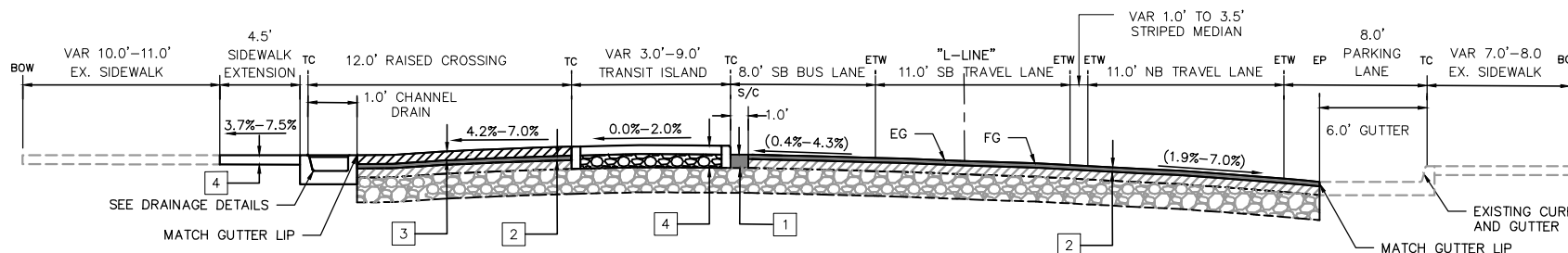
PAVEMENT STRUCTURAL SECTIONS:

- 1 [12" DEEP LIFT HMA (TYPE A)]
- 2 [COLD PLANE AC PAVEMENT (3" MAX) 3" HMA (TYPE A) INLAY]
- 3 [HMA (TYPE A) OVERLAY (6" MAX)]
- 4 [3.5" MIN PCC 4" MIN AGGREGATE BASE (TYPE A)]
- 5 [6" MIN PCC]
- 6 [3" WOOD MULCH 15" IMPORTED SOIL]



"L" 12+80.87 TO 14+45.32
 "L" 18+95.73 TO 20+70.63
 "L" 33+88.95 TO 35+39.17
LAKESHORE AVENUE

NOT TO SCALE



"L" 12+02.78 TO 12+22.80
 "L" 18+07.63 TO 18+19.63
 "L" 33+31.04 TO 33+43.04
 "L" 41+24.53 TO 42+36.53
LAKESHORE AVENUE

NOT TO SCALE

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON
 ANSI B (11" X 17") SHEETS. REFER TO
 GRAPHIC SCALE FOR SHEETS OF
 DIFFERENT SIZES.

Kimley»Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

PROJECT NO.
1007299

TYPICAL SECTIONS

SCALE:
 NTS

SHEET NO.
 X-01

DATE: 12/11/2024

8 OF 83

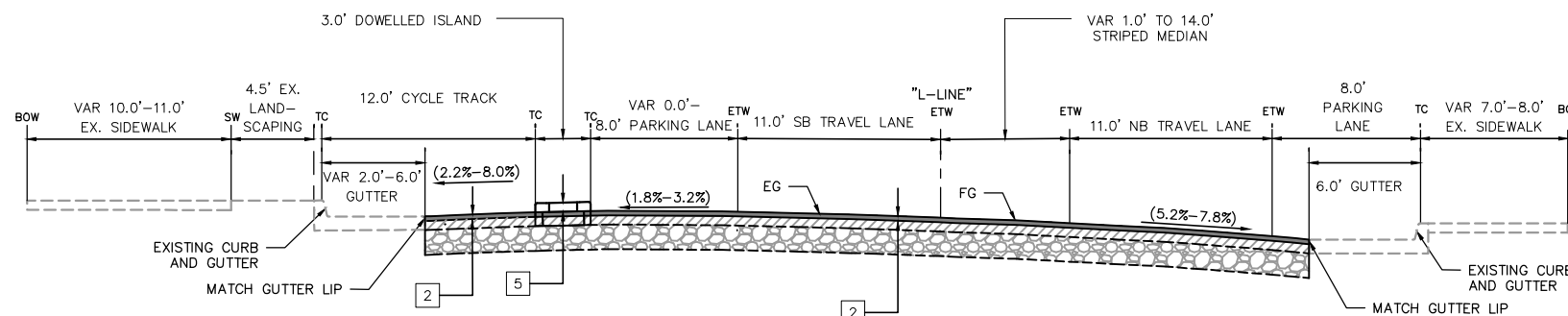
DRAWING NAME: K:\OAK_RDW\09766204 - Lakeshore Ave. Cycle Track\04_CADD\Plan Sheets\TX-01.dwg
 PLOT DATE: 12/11/2024
 PLOTTED BY: Priya Pallivathucal

NOTES:

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE PER THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING INC., DATED MAY 20, 2024. SEE PAVING PLANS FOR APPROXIMATE CORING AND BORING LOCATIONS AND A SUMMARY OF EXISTING PAVEMENT SECTIONS.
- SEE PAVING PLANS FOR EXACT LIMITS OF PAVING.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS.
- CONTRACTOR SHALL SAWCUT TO A NEAT LINE.
- FOR LIMITS AND TYPE OF CURB, SEE LAYOUT PLANS.
- FOR GRADING DETAILS AND LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
- FOR DRAINAGE IMPROVEMENTS, SEE DRAINAGE AND UTILITY PLANS.
- FOR COLD PLANE AND OVERLAY CONFORM LIMITS, SEE PAVING PLANS.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHOULD BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR DIGOUT LOCATIONS, SEE PAVING PLANS.
- DIGOUTS SHOULD BE REPAIRED USING PAVEMENT STRUCTURAL SECTION 1. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHOULD BE REMOVED. THE UNDERLYING SUBGRADE SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED NOVEMBER 15, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED NOVEMBER 15, 2024.

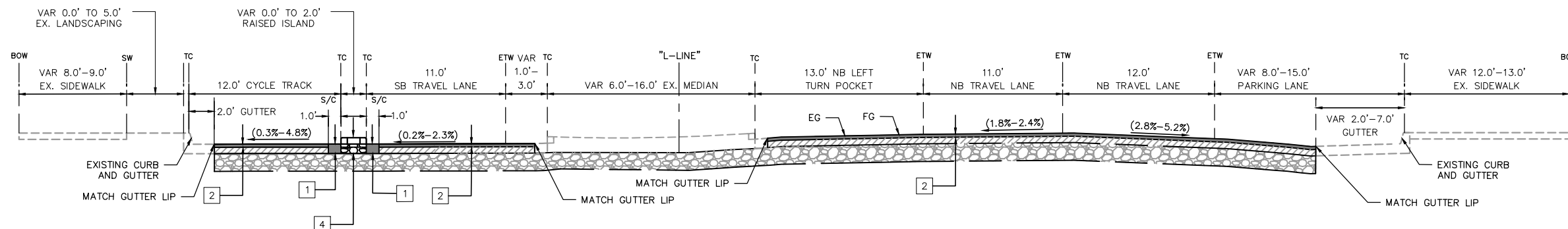
PAVEMENT STRUCTURAL SECTIONS:

- 1 - 21" DEEP LIFT HMA (TYPE A)
- 2 - COLD PLANE AC PAVEMENT (3" MAX) 3" HMA (TYPE A) INLAY
- 3 - HMA (TYPE A) OVERLAY (6" MAX)
- 4 - 3.5" MIN PCC 4" MIN PROCESSED AGGREGATE BASE (TYPE A)
- 5 - 6" MIN PCC
- 6 - 3" WOOD MULCH 15" IMPORTED SOIL



"L" 14+51.82 TO 17+73.02
 "L" 20+77.37 TO 29+95.17
 "L" 30+78.24 TO 32+73.06
 "L" 35+45.67 TO 38+16.42
 "L" 39+11.44 TO 41+66.94
 "L" 43+56.64 TO 43+92.30
LAKESHORE AVENUE

NOT TO SCALE



"L" 45+65.77 TO 47+29.70
LAKESHORE AVENUE

NOT TO SCALE

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON
 ANSI B (11" X 17") SHEETS. REFER TO
 GRAPHIC SCALE FOR SHEETS OF
 DIFFERENT SIZES.

Kimley»Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

TYPICAL SECTIONS

PROJECT NO.
1007299

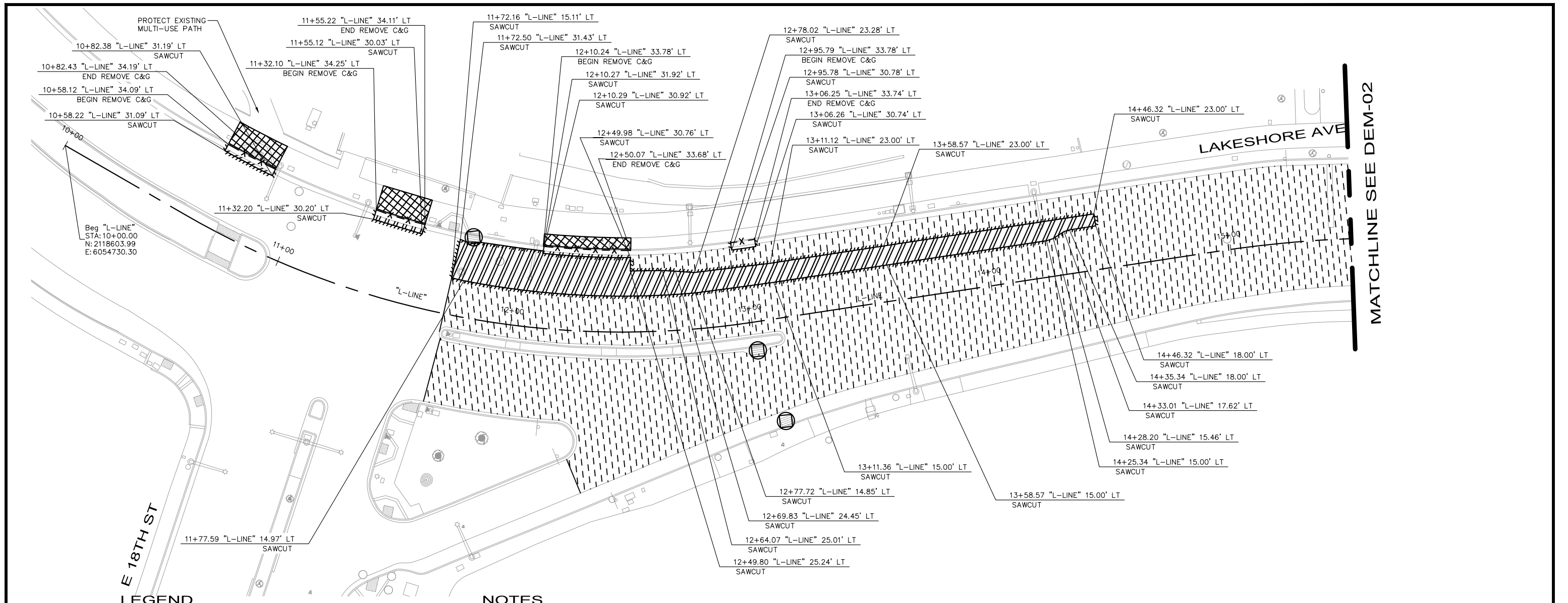
SCALE:
 NTS

SHEET NO.
X-02


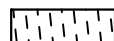



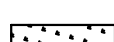
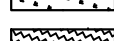
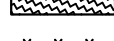


DATE: 12/11/2024

9 OF 83

DRAWING NAME: K:\OAK_RDW\09766204 - Lakeshore Ave. Cycle Track\04_CADD\Plan Sheets\TX-01.dwg
 PLOT DATE: 12/11/2024
 PLOTTED BY: Priya Pallivathucal



LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  - X - X - X - REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS. REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

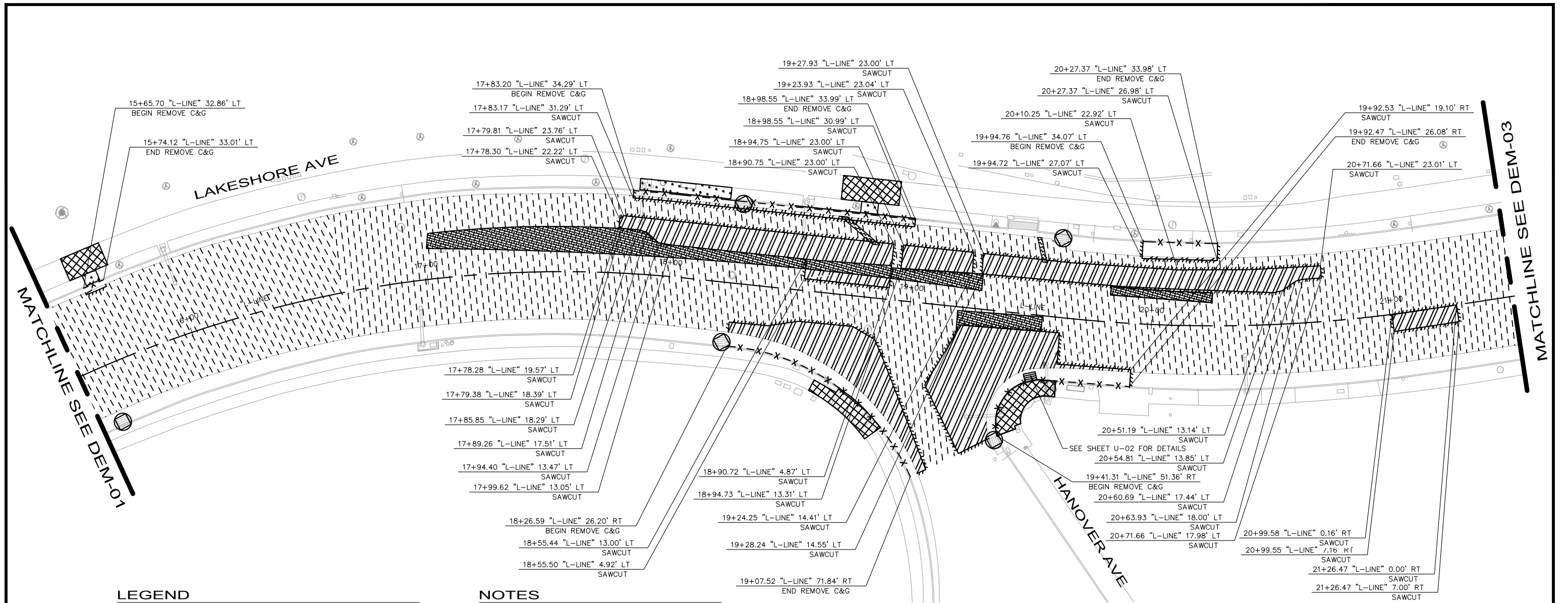
DEMOLITION PLANS

PROJECT NO.
1007299






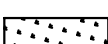

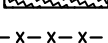


SCALE: AS SHOWN SHEET NO. DEM-01
 10 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal



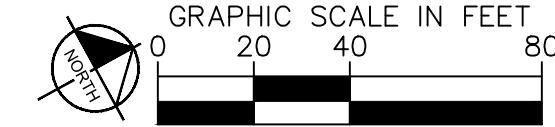
LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  - X - X - X - REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS: REMOVE EXISTING SIDEWALK, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.




Kimley-Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

DEMOLITION PLANS

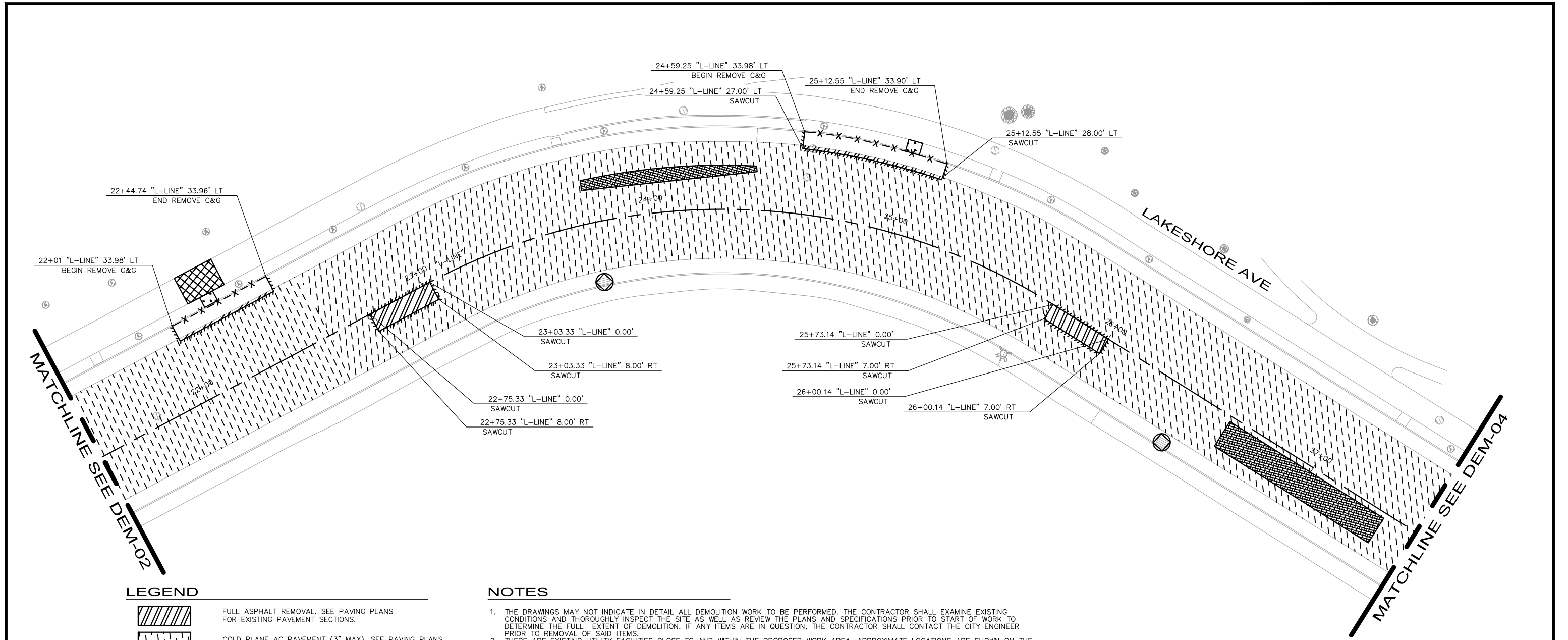
PROJECT NO.
1007299

SCALE:
 AS SHOWN

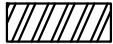


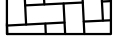

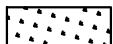

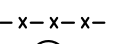


SHEET NO.
 DEM-02
 11 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya.Pallivathucal



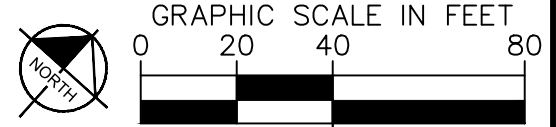
LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS: REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.




Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

DEMOLITION PLANS

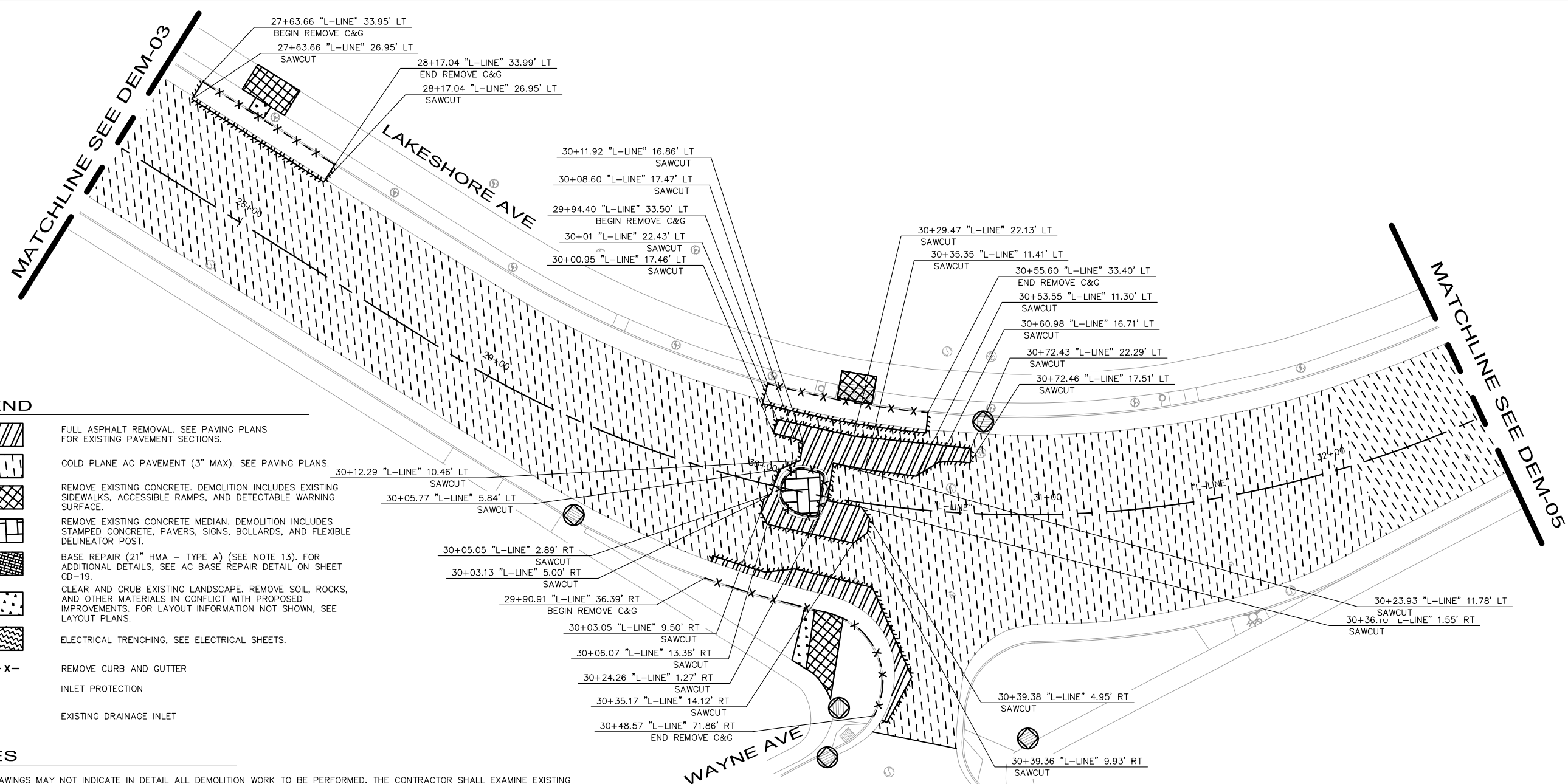
PROJECT NO.
1007299

SCALE:
 AS SHOWN

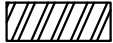




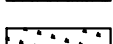
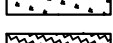



DATE: 12/11/2024

SHEET NO.
 DEM-03
 12 OF 83

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal



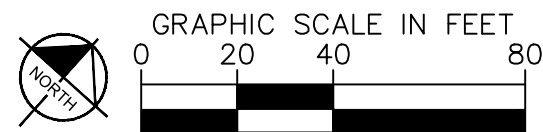
LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  -X-X-X- REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS. REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

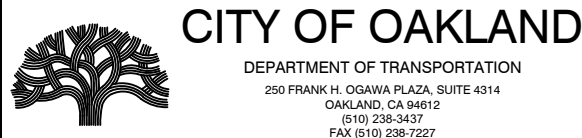
100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

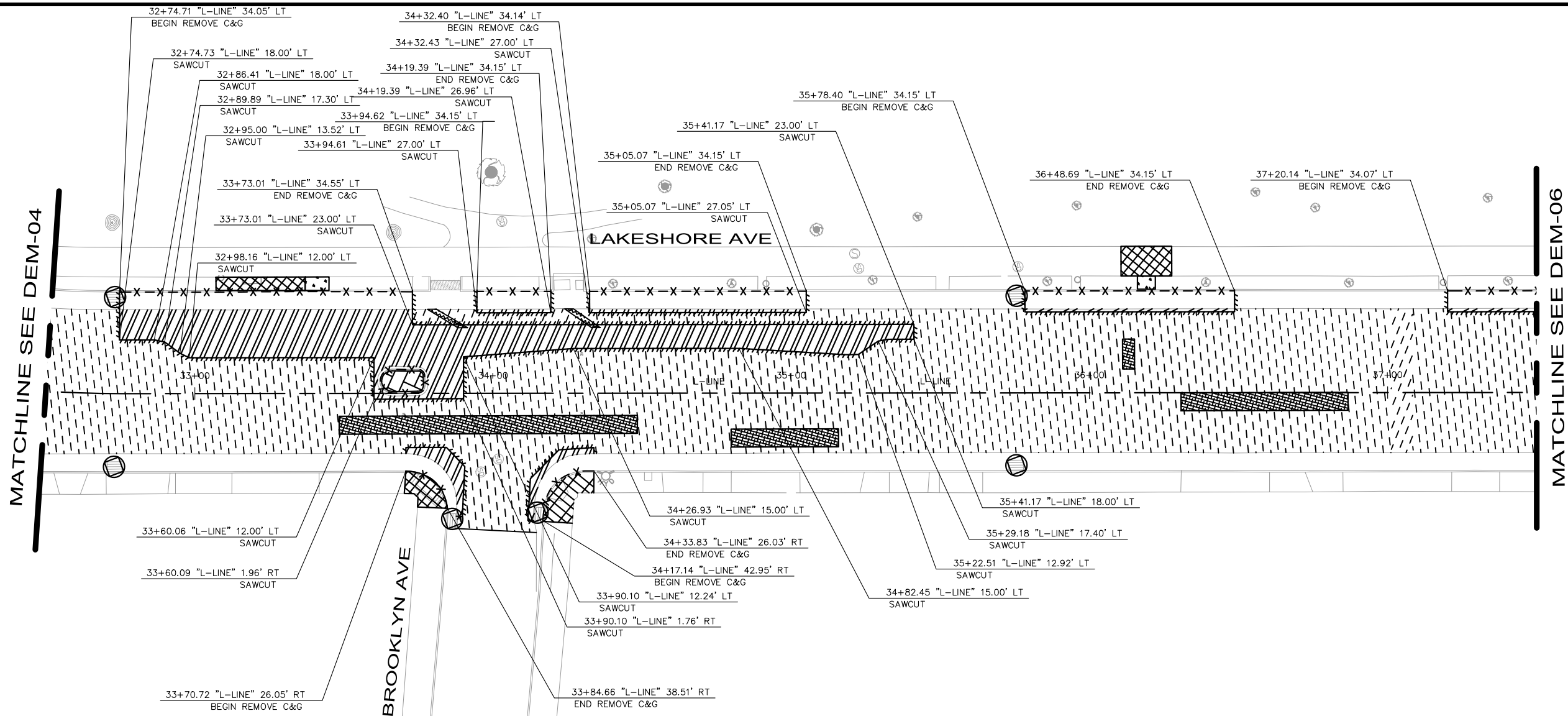
No.	DATE	BY	REFERENCE

DEMOLITION PLANS


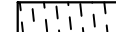

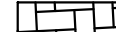

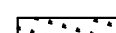
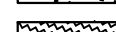
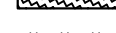


PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. DEM-04
DATE: 12/11/2024 13 OF 83

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\DEM-01-08.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

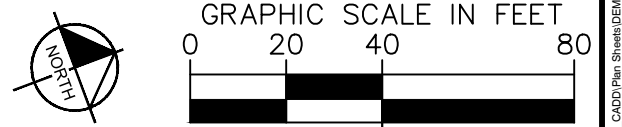


LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  - x - x - x - REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS. REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.



Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

DEMOLITION PLANS

PROJECT NO.
1007299

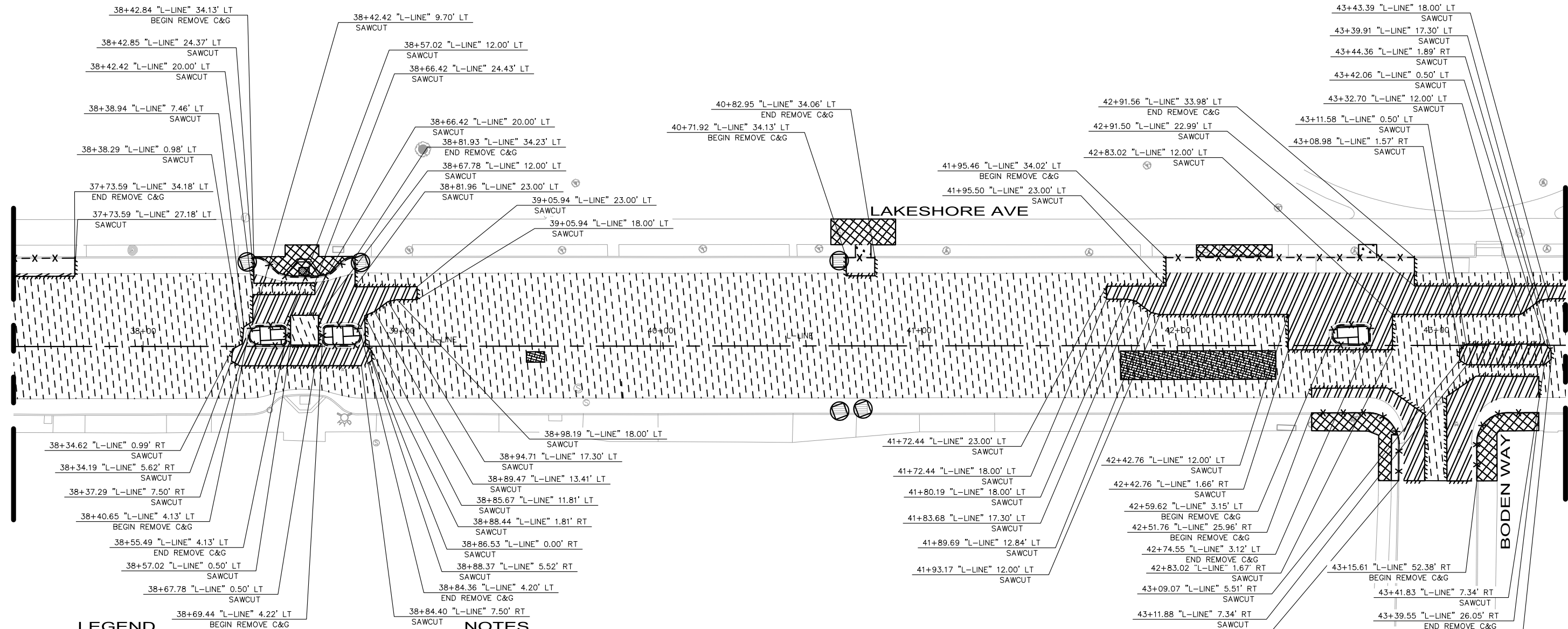
SCALE: AS SHOWN SHEET NO. DEM-05
 14 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal

MATCHLINE SEE DEM-05

MATCHLINE SEE DEM-07



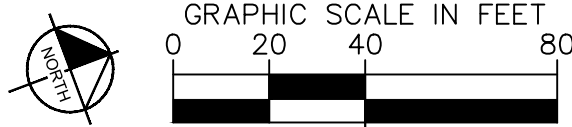
LEGEND

- FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
- COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
- REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
- REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
- ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
- X - X - X - REMOVE CURB AND GUTTER
- INLET PROTECTION
- EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS. REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley-Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

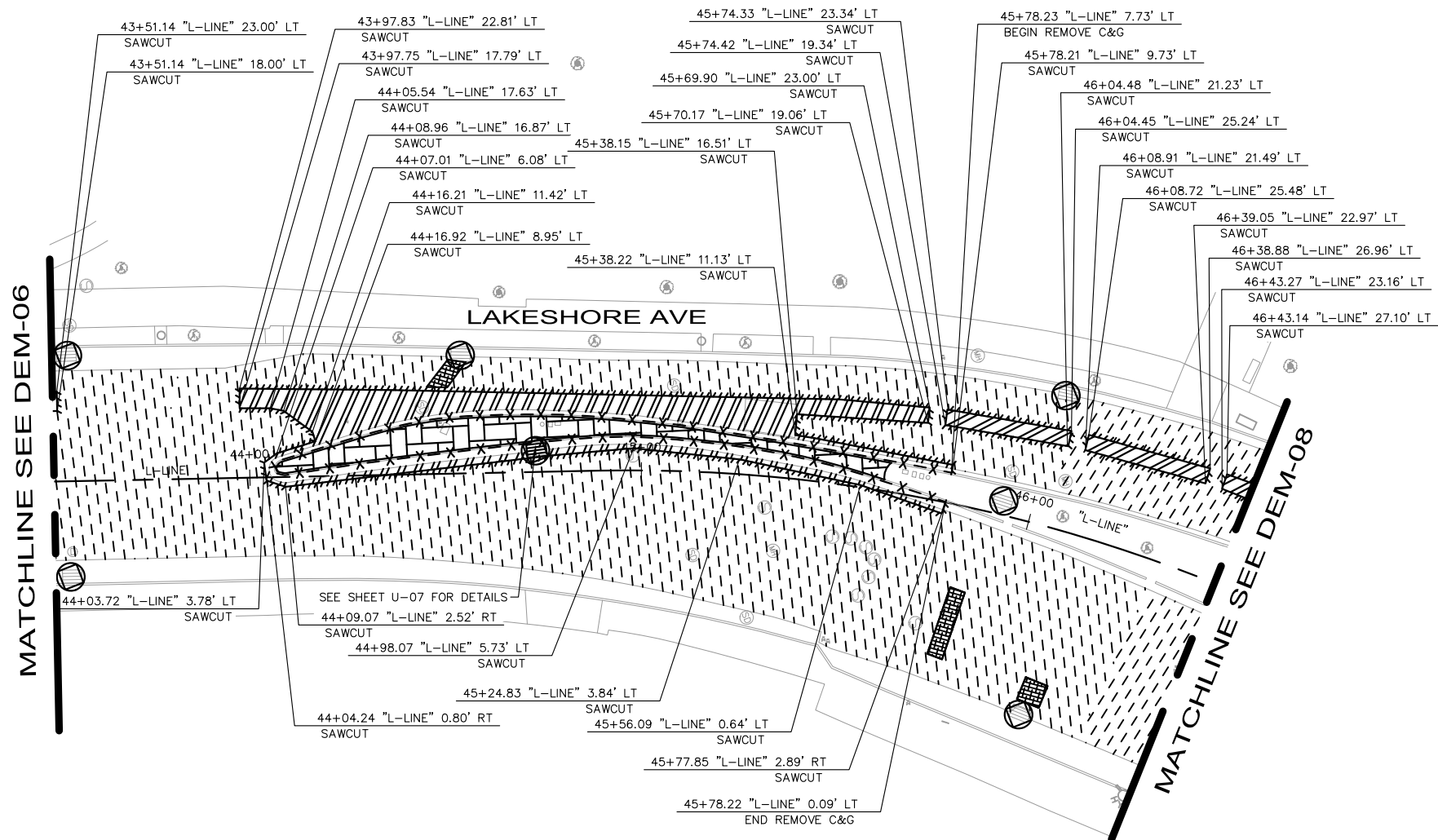
No.	DATE	BY	REFERENCE

DEMOLITION PLANS


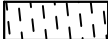



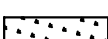

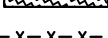


PROJECT NO.
1007299

SCALE: AS SHOWN
 SHEET NO. DEM-06
 15 OF 83
 DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal



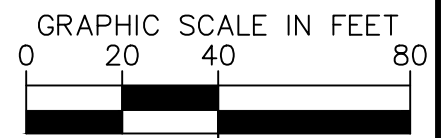
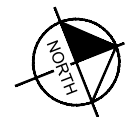
LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  - X - X - X - REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS: REMOVE EXISTING SIDEWALK, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

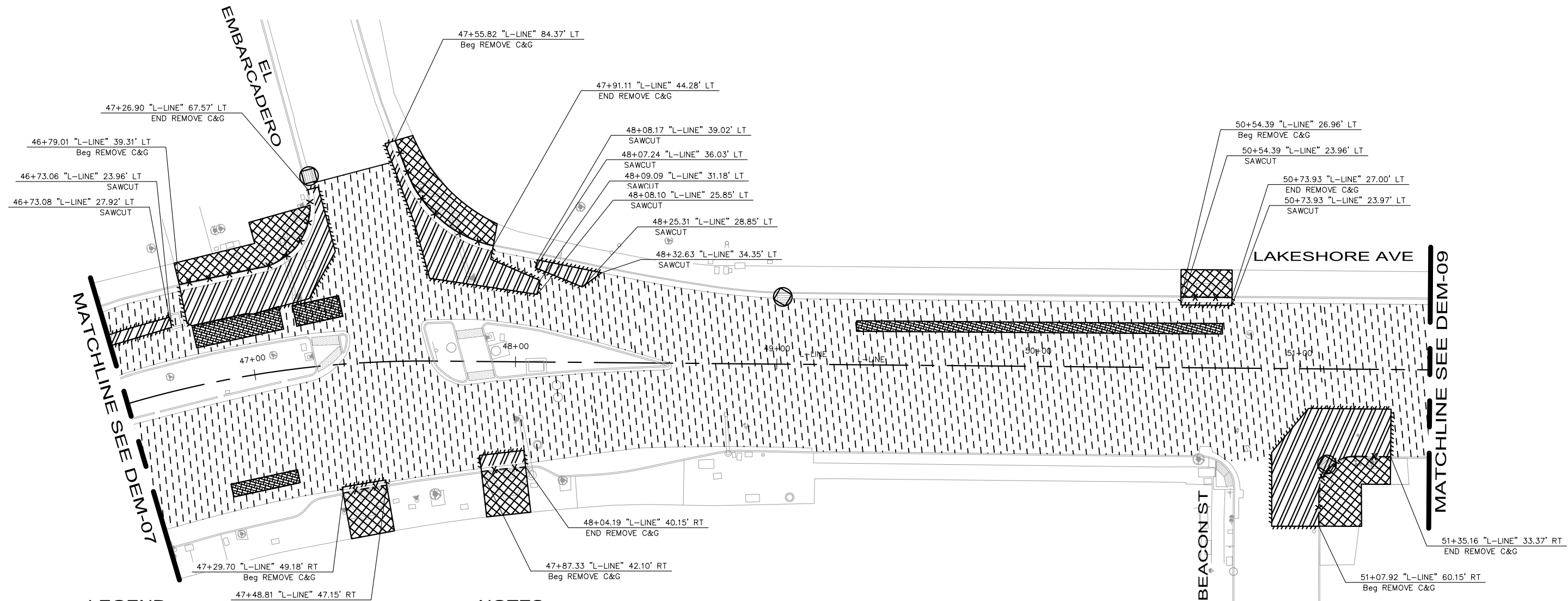
DEMOLITION PLANS

PROJECT NO.
1007299


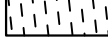



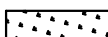

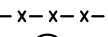


SCALE: AS SHOWN SHEET NO. DEM-07
 16 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\1007299\04 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal



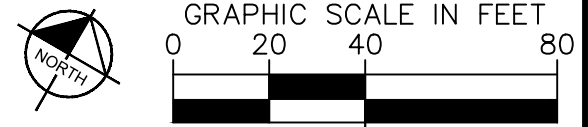
LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS: REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley-Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

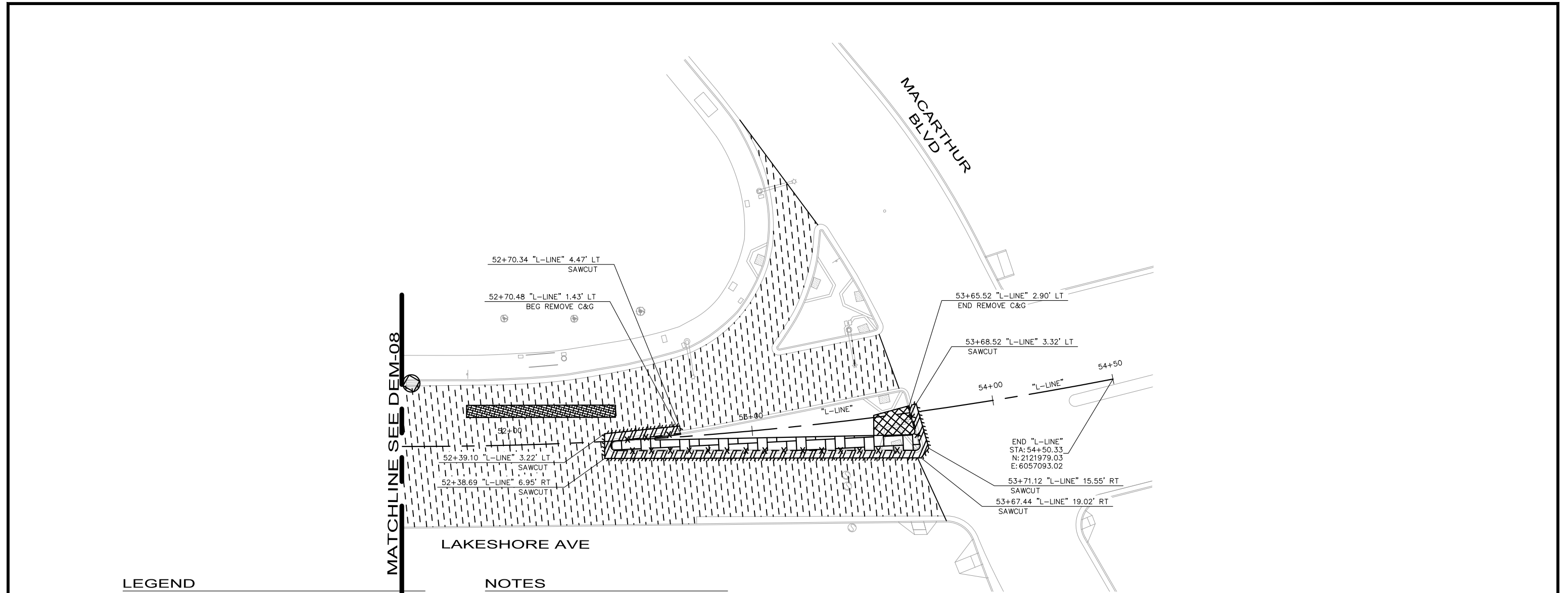
DEMOLITION PLANS

PROJECT NO. **1007299**




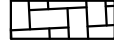



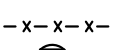


SCALE: AS SHOWN SHEET NO. DEM-08
 17 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Trac\04_CADD\Plan_Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal



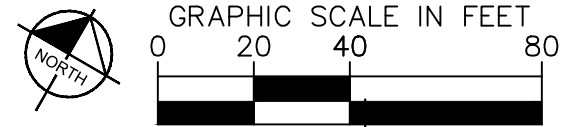
LEGEND

-  FULL ASPHALT REMOVAL. SEE PAVING PLANS FOR EXISTING PAVEMENT SECTIONS.
-  COLD PLANE AC PAVEMENT (3" MAX). SEE PAVING PLANS.
-  REMOVE EXISTING CONCRETE. DEMOLITION INCLUDES EXISTING SIDEWALKS, ACCESSIBLE RAMPS, AND DETECTABLE WARNING SURFACE.
-  REMOVE EXISTING CONCRETE MEDIAN. DEMOLITION INCLUDES STAMPED CONCRETE, PAVERS, SIGNS, BOLLARDS, AND FLEXIBLE DELINEATOR POST.
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 13). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  CLEAR AND GRUB EXISTING LANDSCAPE. REMOVE SOIL, ROCKS, AND OTHER MATERIALS IN CONFLICT WITH PROPOSED IMPROVEMENTS. FOR LAYOUT INFORMATION NOT SHOWN, SEE LAYOUT PLANS.
-  ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
-  - X - X - X - REMOVE CURB AND GUTTER
-  INLET PROTECTION
-  EXISTING DRAINAGE INLET

NOTES

1. THE DRAWINGS MAY NOT INDICATE IN DETAIL ALL DEMOLITION WORK TO BE PERFORMED. THE CONTRACTOR SHALL EXAMINE EXISTING CONDITIONS AND THOROUGHLY INSPECT THE SITE AS WELL AS REVIEW THE PLANS AND SPECIFICATIONS PRIOR TO START OF WORK TO DETERMINE THE FULL EXTENT OF DEMOLITION. IF ANY ITEMS ARE IN QUESTION, THE CONTRACTOR SHALL CONTACT THE CITY ENGINEER PRIOR TO REMOVAL OF SAID ITEMS.
2. THERE ARE EXISTING UTILITY FACILITIES CLOSE TO AND WITHIN THE PROPOSED WORK AREA. APPROXIMATE LOCATIONS ARE SHOWN ON THE DRAINAGE AND UTILITY PLANS. ALL EXISTING UTILITIES ARE TO BE PROTECTED IN PLACE OR ADJUSTED TO NEW FINISH GRADE UNLESS SPECIFICALLY IDENTIFIED OTHERWISE. SEE SHEETS U-01 TO U-09 FOR ADDITIONAL DETAILS.
3. CURB, PAVEMENT, AND SIDEWALK REMOVAL LIMITS MAY VARY SLIGHTLY FROM THE DEMOLITION PLANS DUE TO CHANGES IN REMOVAL LIMITS AS FOLLOWS. REMOVE EXISTING SIDEWALK, MEDIAN, CURB, AND GUTTER AT A SCORE MARK OR WEAKENED PLANE JOINT NEAREST TO LIMITS SHOWN ON THESE PLANS. EXACT LIMITS TO BE DETERMINED IN THE FIELD BY ENGINEER AND CONTRACTOR.
4. REMOVAL AND REGRADING OF BACK OF WALK LANDSCAPING SHALL INCLUDE ROOTS AND ORGANIC MATERIAL.
5. SAWCUT AND REMOVAL OF PAVEMENT SECTION SHALL BE TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO THE ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO A CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
6. DEMOLITION AND REMOVAL OF PAVEMENT INCLUDES PAVEMENT THICKNESS AS WELL AS BASE COURSE THICKNESS.
7. PRIOR TO STARTING ANY WORK, CONTRACTOR SHALL PRESERVE ALL SURVEY MARKERS AND MONUMENTS. SEE GENERAL NOTES ON SHEET GN-01 FOR FURTHER SURVEY MARKER AND MONUMENT REQUIREMENTS.
8. ALL WORK MUST BE PERFORMED WITHIN THE LIMITS OF EXISTING R/W AS SHOWN ON THESE PLANS AND AS COORDINATED WITH THE CITY ENGINEER. IF R/W IS NOT SHOWN, R/W IS ASSUMED TO BE AT THE BACK OF EXISTING SIDEWALK.
9. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO PREVENT DEBRIS AND UNSUITABLE MATERIALS FROM ENTERING STORM DRAINS, SANITARY SEWERS, AND STREETS. SEE SHEET WP-01 FOR ADDITIONAL WATER POLLUTION PREVENTION NOTES.
10. ALL PAVEMENT MARKINGS AND STRIPING IN CONFLICT SHALL BE REMOVED. SEE SHEETS SS-01 TO SS-09 FOR SIGNING AND STRIPING DETAILS.
11. SEE SHEETS E-01 TO E-03 FOR ELECTRICAL AND TRAFFIC SIGNAL REMOVALS.
12. FOR SAWCUT LIMITS NOT SHOWN, SEE LAYOUT PLANS AND CONSTRUCTION DETAIL SHEETS.
13. FOR DIGOUT LIMITS NOT SHOWN, SEE PAVING PLANS.
14. CONTRACTOR SHOULD MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.
15. ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.




Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

DEMOLITION PLANS

PROJECT NO.
1007299












SCALE:
 AS SHOWN

SHEET NO.
 DEM-09
 18 OF 83

DATE: 12/11/2024

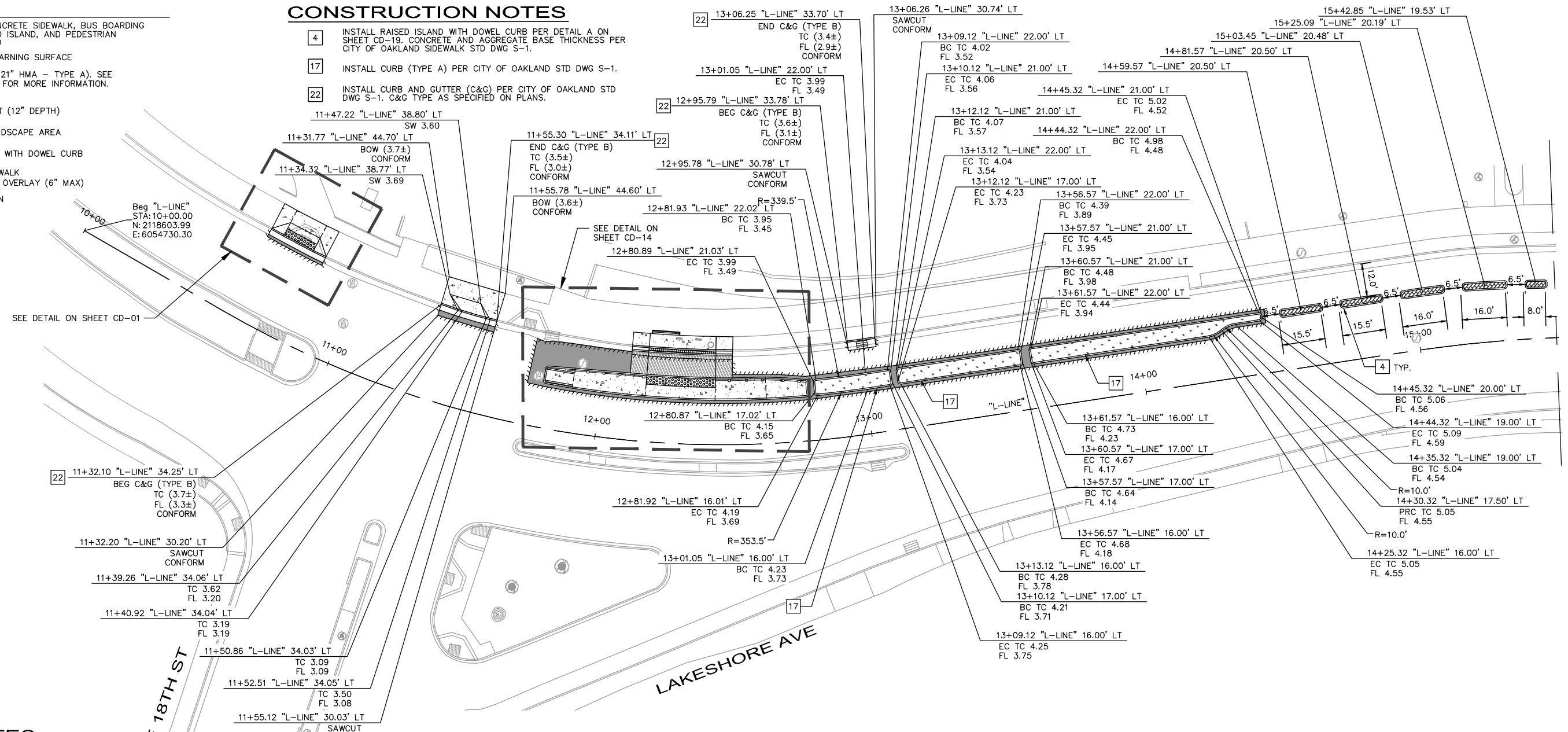
DRAWING NAME: K:\OAK_R\DWY\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\DEM-01-08.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal

LEGEND

-  PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
-  DETECTABLE WARNING SURFACE
-  BASE REPAIR (21" HMA - TYPE A). SEE PAVING PLANS FOR MORE INFORMATION.
-  HMA PAVEMENT (12" DEPTH)
-  PROPOSED LANDSCAPE AREA
-  RAISED ISLAND WITH DOWEL CURB
-  RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)
-  CHANNEL DRAIN (SEE DD-01)
-  GRADE BREAK
-  SAWCUT LIMITS
-  LEAN RAIL

CONSTRUCTION NOTES

- 4** INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 17** INSTALL CURB (TYPE A) PER CITY OF OAKLAND STD DWG S-1.
- 22** INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.



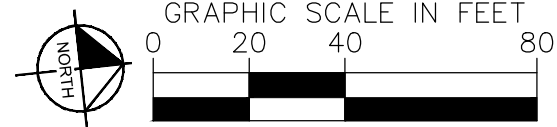
NOTES

1. ALL LOCATIONS WHERE R/W IS NOT IDENTIFIED OR DOES NOT COVER LIMITS OF IMPROVEMENTS, EXISTING BACK OF WALK SHALL BE ASSUMED AS RIGHT OF WAY LIMITS.
2. CONTRACTOR SHALL SAWCUT TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
3. CONFORM SIDEWALK, CURB, AND GUTTER AT NEAREST SCOREMARK OR EXPANSION JOINT. EXACT LIMITS TO BE DETERMINED IN THE FIELD.
4. PROPOSED BACK OF WALK HORIZONTAL ALIGNMENT TO MATCH EXISTING, UNLESS OTHERWISE SHOWN ON THESE PLANS.
5. ALL EXISTING GRADES AT LIMITS OF WORK LABELED AS CONFORM SHALL BE VERIFIED PRIOR TO CONSTRUCTION AND ANY CONFLICT SHALL BE PROMPTLY BROUGHT TO THE ATTENTION OF THE ENGINEER.
6. GUTTER CROSS SLOPE AT BOTTOM OF RAMP OR BLENDED TRANSITIONS SHALL NOT EXCEED 5%.
7. FOR TRENCH GRATE DETAIL, SEE DRAINAGE DETAILS.

8. CONTRACTOR TO VERIFY ALL DEPTHS AND LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES.
9. SEE DRAINAGE AND UTILITY PLANS FOR UTILITIES THAT SHOULD BE ADJUSTED TO GRADE. EXISTING UTILITY BOXES AND COVERS SHALL BE ADJUSTED TO BE FLUSH WITH THE CURB RAMP, SIDEWALK, AND PAVEMENT SURFACE. ALL OTHER UTILITY PIPES, DUCTS, BOXES, VAULTS, AND STRUCTURE SHALL BE PROTECTED IN PLACE UNLESS OTHERWISE SHOWN.
10. ALL CONCRETE ISLAND CURB CORNERS TO BE ROUNDED USING 1' RADIUS, UNLESS OTHERWISE SHOWN ON THESE PLANS. FOR DOWEL CURB CONCRETE CORNER CURVE RADIUS, SEE DETAIL A ON SHEET CD-19.
11. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY BENCHMARK AND MONUMENT. IF A BENCHMARK OR MONUMENT IS DISTURBED, THE CONTRACTOR SHALL HAVE A LICENSED LAND SURVEYOR RESTORE THE BENCHMARK OR MONUMENT AT THE CONTRACTOR'S EXPENSE.
12. PROPOSED CURB AND GUTTER SHALL BE DOWELED WITH #4 DOWEL TO EXISTING CURB AND GUTTER AT ALL CONFORM LOCATIONS.
13. FOR PAVEMENT STRUCTURAL SECTIONS, SEE TYPICAL CROSS SECTIONS SHEETS AND PAVING PLANS.
14. FOR GRADING DETAILS AND LAYOUT INFORMATION NOT SHOWN, SEE CONSTRUCTION DETAIL SHEETS.
15. FOR SIGNING AND STRIPING IMPROVEMENTS, SEE SIGNAGE AND STRIPING PLANS.
16. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.
17. LANDSCAPE AREA SHOULD INCLUDE 3IN OF WOOD MULCH AND AT LEAST 15IN OF IMPORTED SOIL, PER CITY OF OAKLAND STD.
18. CONTRACTOR SHALL MATCH EXISTING CONCRETE COLOR AND/OR PATTERN OF EXISTING SIDEWALK WITHIN ALL SIDEWALK RECONSTRUCTION LIMITS SPECIFIED ON PLANS.

100% SUBMITTAL NOT FOR BID OR CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

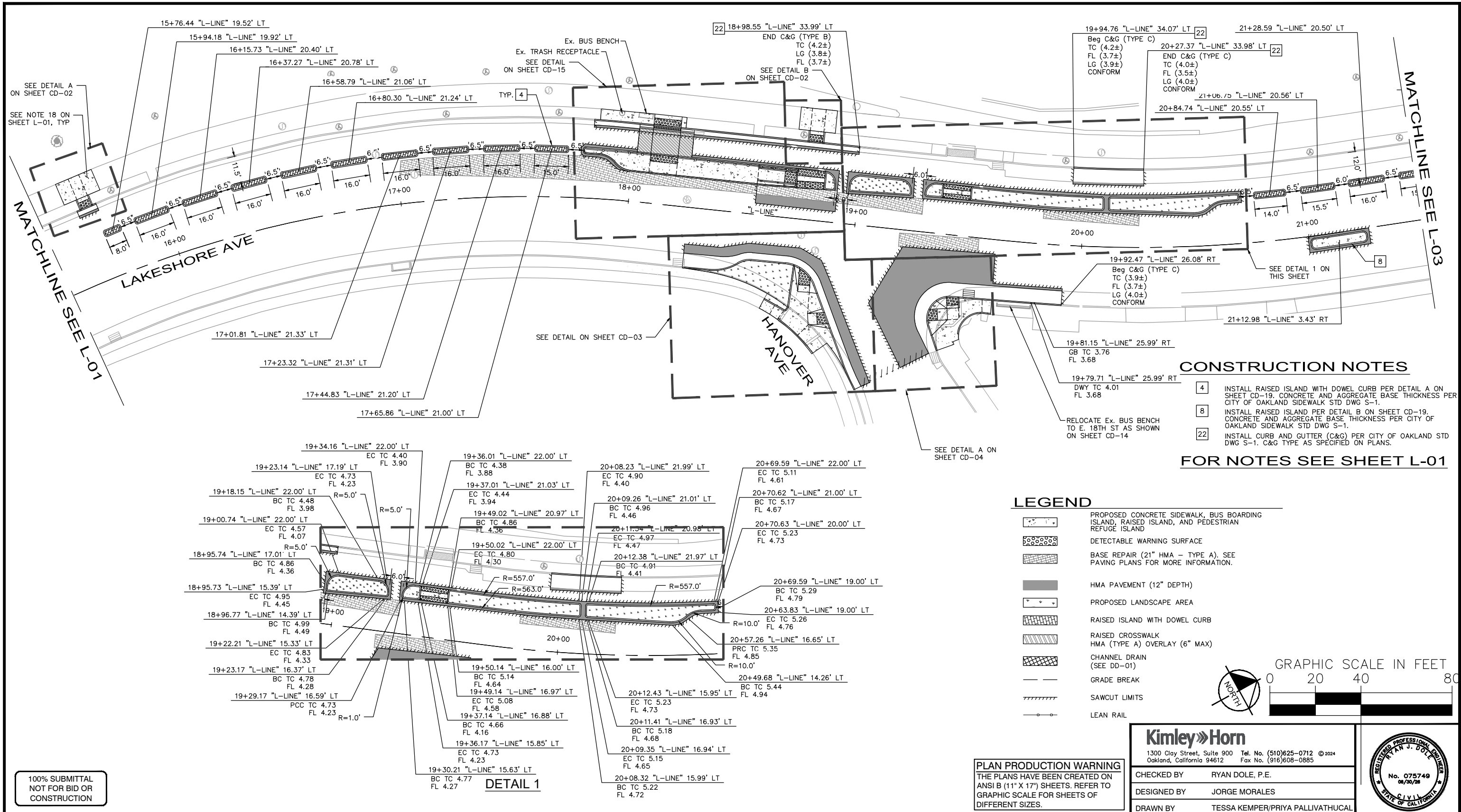
LAYOUT

PROJECT NO.
1007299

SCALE: AS SHOWN
SHEET NO. L-01
19 OF 83
DATE: 12/11/2024

MATCHLINE SEE L-02

DRAWING NAME: K:\OAK_RDWY\1007299\204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\L-XX.dwg
PLOT DATE: 12-13-24
PLOTTED BY: Priya Pallivathucal



CONSTRUCTION NOTES

4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.

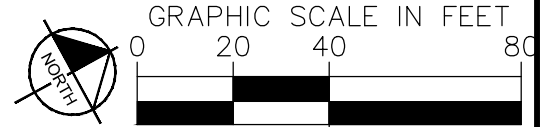
8 INSTALL RAISED ISLAND PER DETAIL B ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.

22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

FOR NOTES SEE SHEET L-01

LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A). SEE PAVING PLANS FOR MORE INFORMATION.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)
- CHANNEL DRAIN (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- LEAN RAIL



DETAIL 1

19+34.16 "L-LINE" 22.00' LT EC TC 4.40 FL 3.90

19+23.14 "L-LINE" 17.19' LT EC TC 4.73 FL 4.23

19+18.15 "L-LINE" 22.00' LT BC TC 4.48 FL 3.98

19+00.74 "L-LINE" 22.00' LT EC TC 4.57 FL 4.07

18+95.74 "L-LINE" 17.01' LT BC TC 4.86 FL 4.36

18+95.73 "L-LINE" 15.39' LT EC TC 4.95 FL 4.45

18+96.77 "L-LINE" 14.39' LT BC TC 4.99 FL 4.49

19+22.21 "L-LINE" 15.33' LT EC TC 4.83 FL 4.33

19+23.17 "L-LINE" 16.37' LT BC TC 4.78 FL 4.28

19+29.17 "L-LINE" 16.59' LT PCC TC 4.73 FL 4.23

19+36.01 "L-LINE" 22.00' LT BC TC 4.38 FL 3.88

19+37.01 "L-LINE" 21.03' LT EC TC 4.44 FL 3.94

19+49.02 "L-LINE" 20.97' LT BC TC 4.86 FL 4.36

19+50.02 "L-LINE" 22.00' LT EC TC 4.80 FL 4.30

19+50.14 "L-LINE" 16.00' LT BC TC 5.14 FL 4.64

19+49.14 "L-LINE" 16.97' LT EC TC 5.08 FL 4.58

19+37.14 "L-LINE" 16.88' LT BC TC 4.66 FL 4.16

19+36.17 "L-LINE" 15.85' LT EC TC 4.73 FL 4.23

19+30.21 "L-LINE" 15.63' LT BC TC 4.77 FL 4.27

20+08.23 "L-LINE" 21.99' LT EC TC 4.90 FL 4.40

20+09.26 "L-LINE" 21.01' LT BC TC 4.96 FL 4.46

20+11.34 "L-LINE" 20.98' LT EC TC 4.97 FL 4.47

20+12.38 "L-LINE" 21.97' LT BC TC 4.91 FL 4.41

20+12.43 "L-LINE" 15.95' LT EC TC 5.23 FL 4.73

20+11.41 "L-LINE" 16.93' LT BC TC 5.18 FL 4.68

20+09.35 "L-LINE" 16.94' LT EC TC 5.15 FL 4.65

20+08.32 "L-LINE" 15.99' LT BC TC 5.22 FL 4.72

20+69.59 "L-LINE" 22.00' LT EC TC 5.11 FL 4.61

20+70.62 "L-LINE" 21.00' LT BC TC 5.17 FL 4.67

20+70.63 "L-LINE" 20.00' LT EC TC 5.23 FL 4.73

20+69.59 "L-LINE" 19.00' LT BC TC 5.29 FL 4.79

20+63.83 "L-LINE" 19.00' LT EC TC 5.26 FL 4.76

20+57.26 "L-LINE" 16.65' LT PRC TC 5.35 FL 4.85

20+49.68 "L-LINE" 14.26' LT BC TC 5.44 FL 4.94

100% SUBMITTAL NOT FOR BID OR CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 © 2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

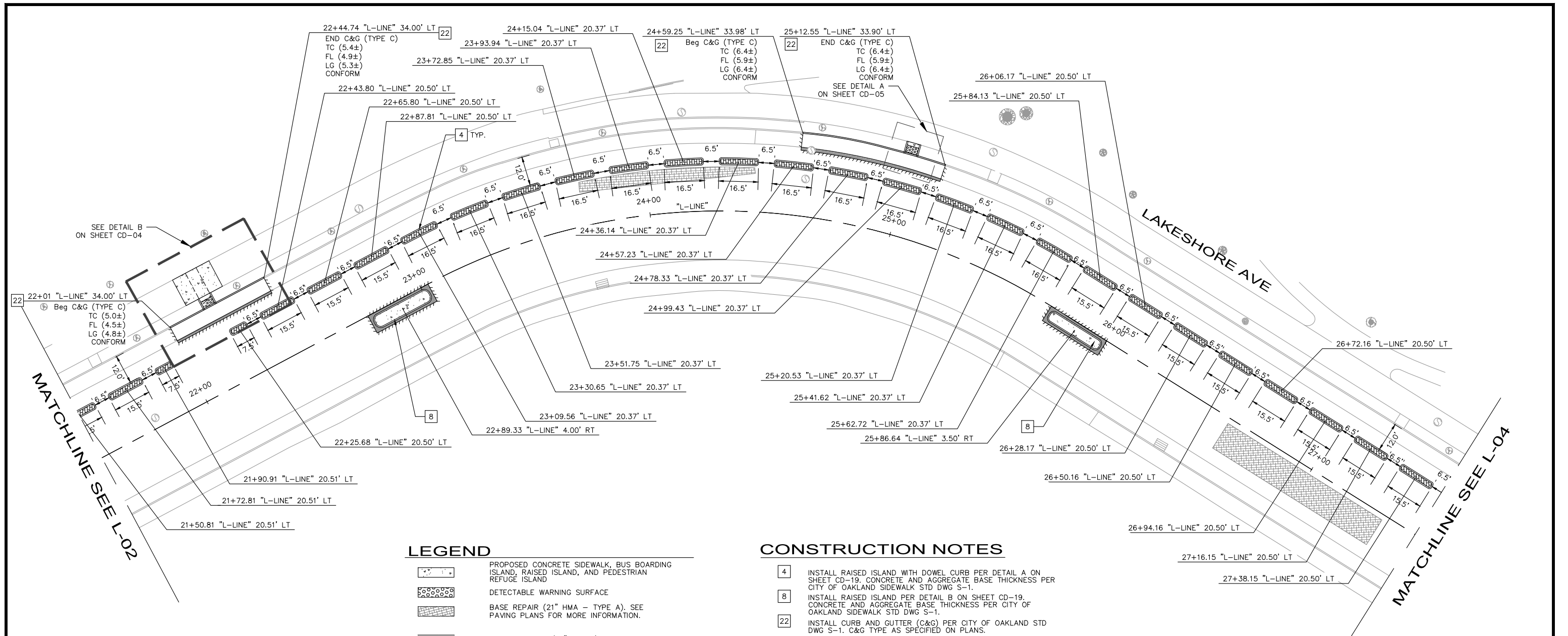
No.	DATE	BY	REFERENCE

LAYOUT

PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. L-02 OF 83

DATE: 12/11/2024



LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
-

CONSTRUCTION NOTES

- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 8 INSTALL RAISED ISLAND PER DETAIL B ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

FOR NOTES SEE SHEET L-01

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

LAYOUT

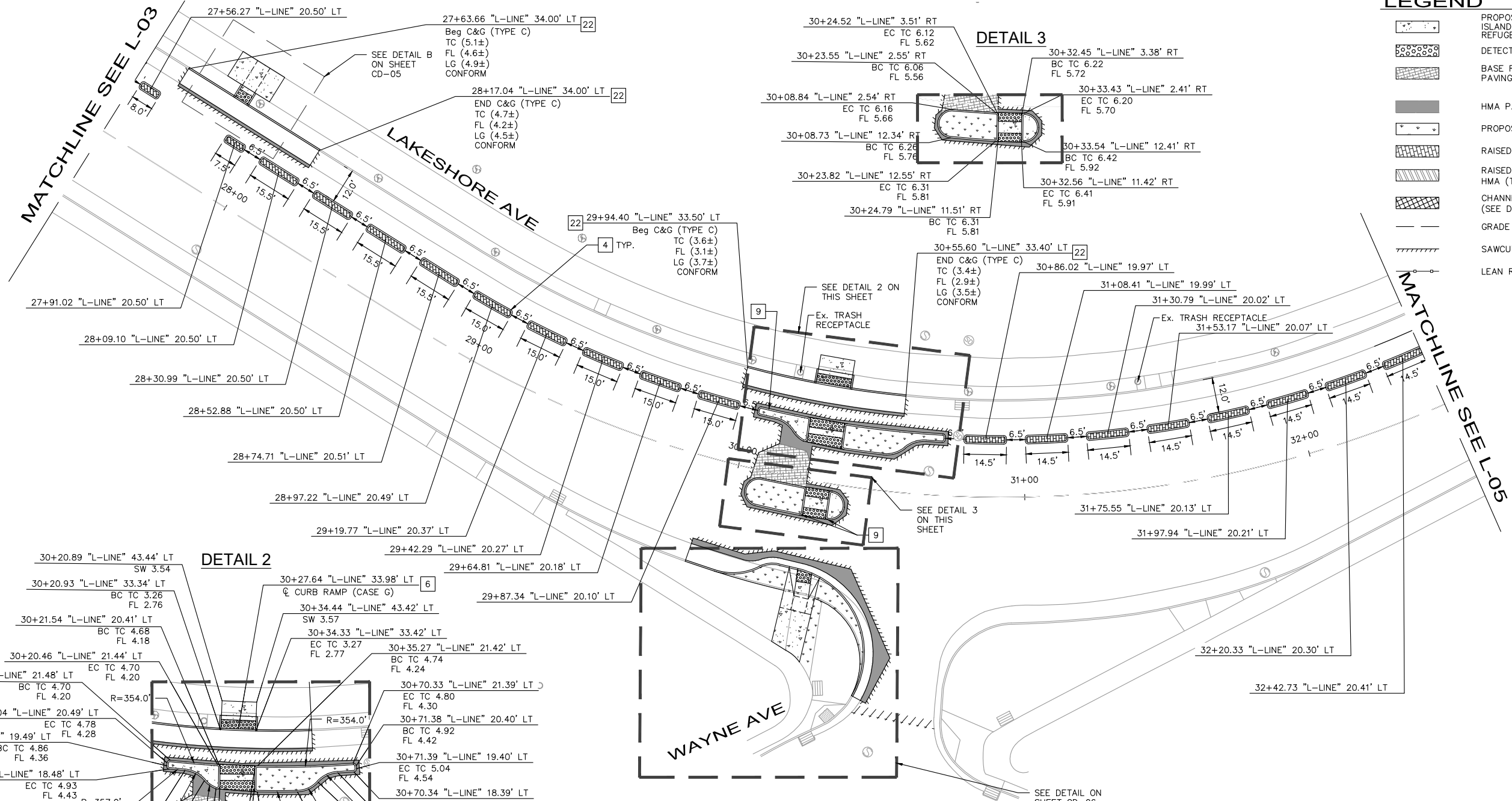
PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. L-03
DATE: 12/11/2024 21 OF 83

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Trac\04_CADD\Plan_Sheets\L-XX.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A). SEE PAVING PLANS FOR MORE INFORMATION.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)
- CHANNEL DRAIN (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- LEAN RAIL



CONSTRUCTION NOTES

- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 6 INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 9 INSTALL PEDESTRIAN REFUGE ISLAND PER DETAIL C ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

FOR NOTES SEE SHEET L-01

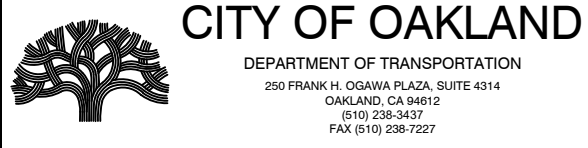
PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION














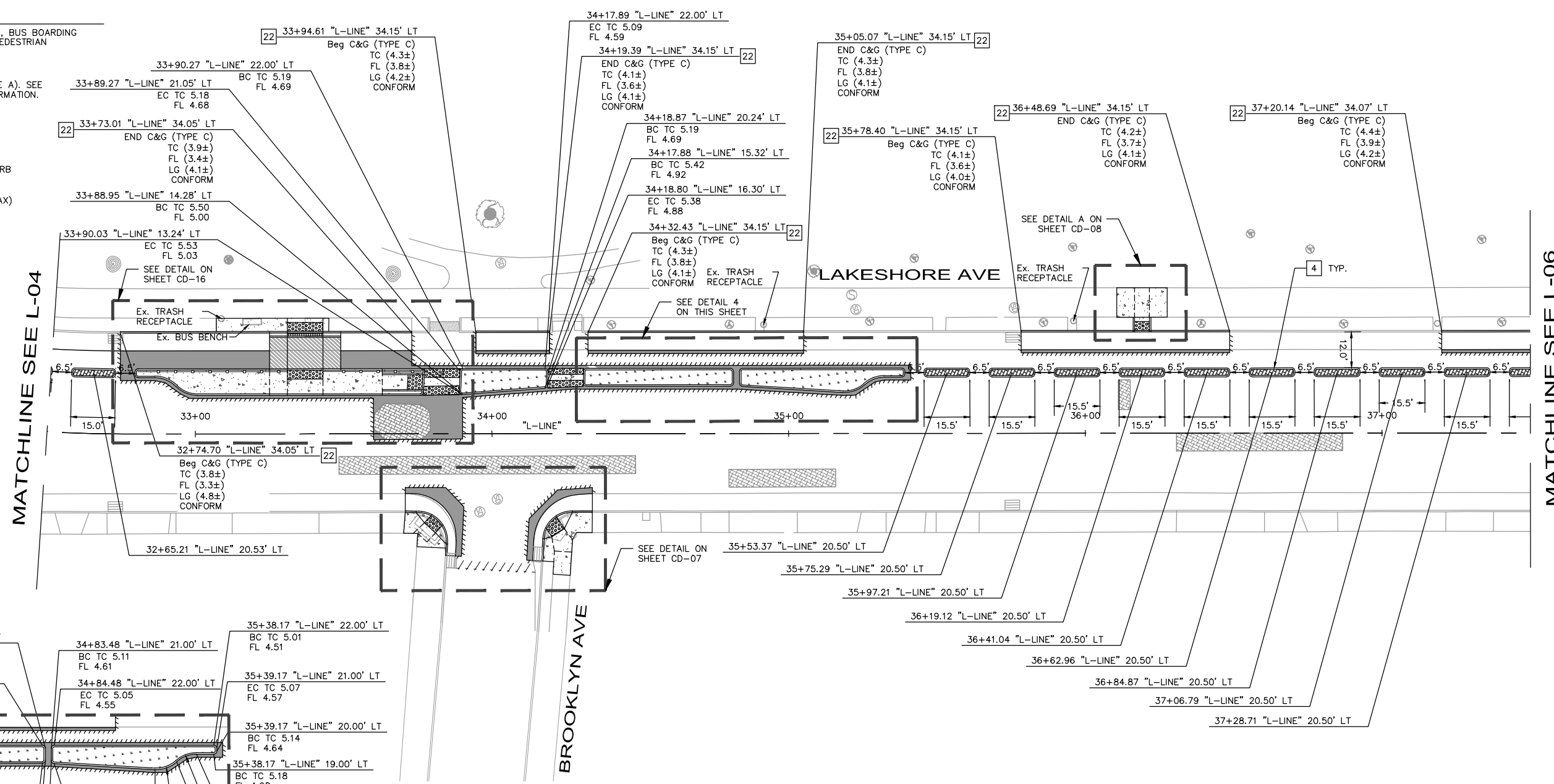
**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

LAYOUT	PROJECT NO. 1007299
	SCALE: AS SHOWN
L-04	SHEET NO. 22 OF 83
	DATE: 12/11/2024

LEGEND

-  PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
-  DETECTABLE WARNING SURFACE
-  BASE REPAIR (21" HMA - TYPE A). SEE PAVING PLANS FOR MORE INFORMATION.
-  HMA PAVEMENT (12" DEPTH)
-  PROPOSED LANDSCAPE AREA
-  RAISED ISLAND WITH DOWEL CURB
-  RAISED CROSSWALK
-  CHANNEL DRAIN (SEE DD-01)
-  GRADE BREAK
-  SAWCUT LIMITS
-  LEAN RAIL



MATCHLINE SEE L-04

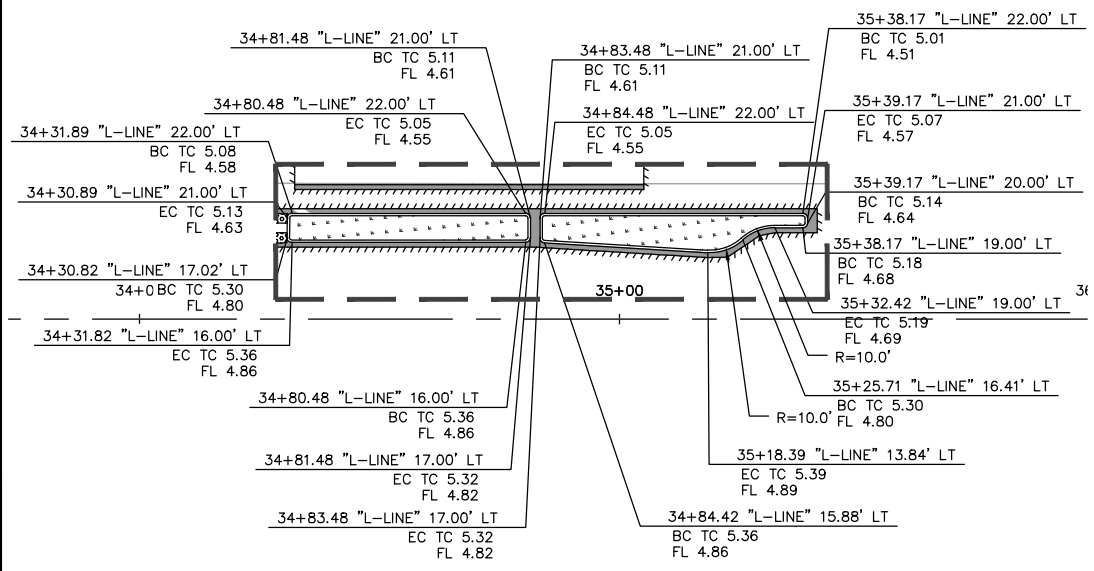
MATCHLINE SEE L-06

CONSTRUCTION NOTES

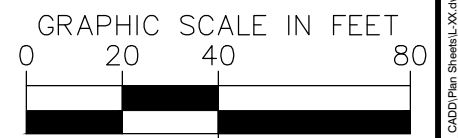
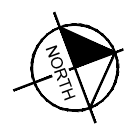
- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

FOR NOTES SEE SHEET L-01

DETAIL 4



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

LAYOUT

PROJECT NO.
1007299

SCALE:
AS SHOWN

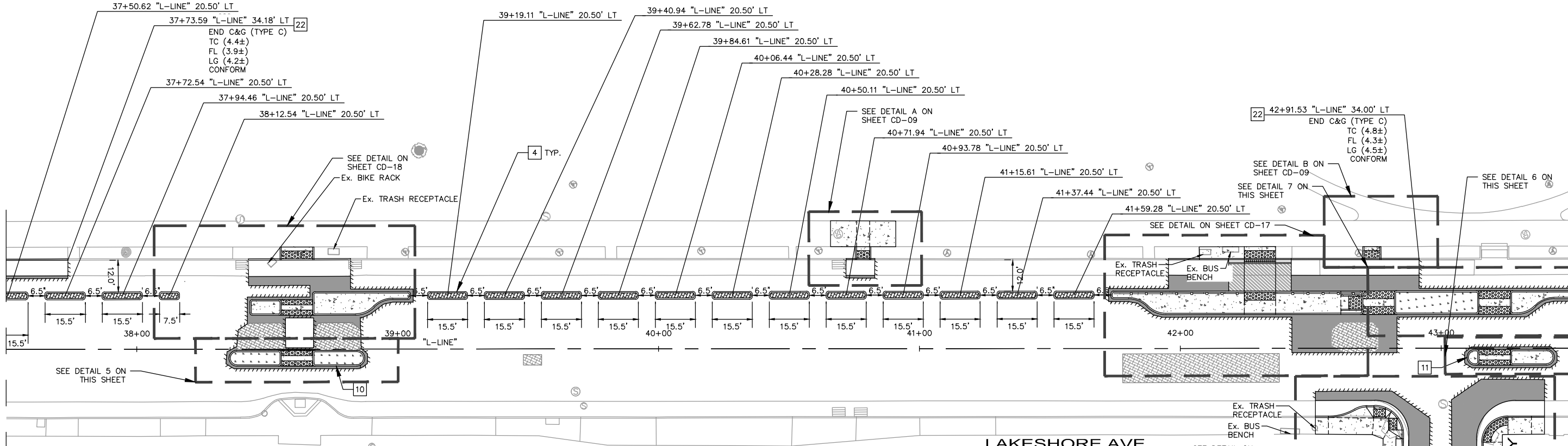
SHEET NO.
L-05
23 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\00709204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\L-XX.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

MATCHLINE SEE L-05

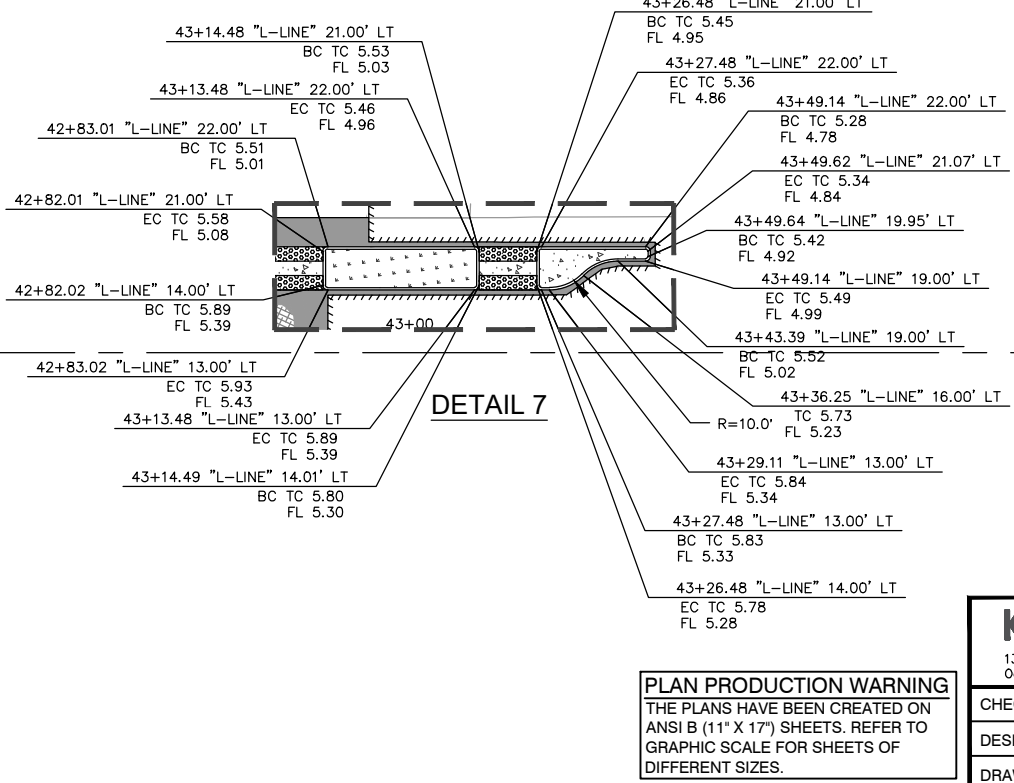
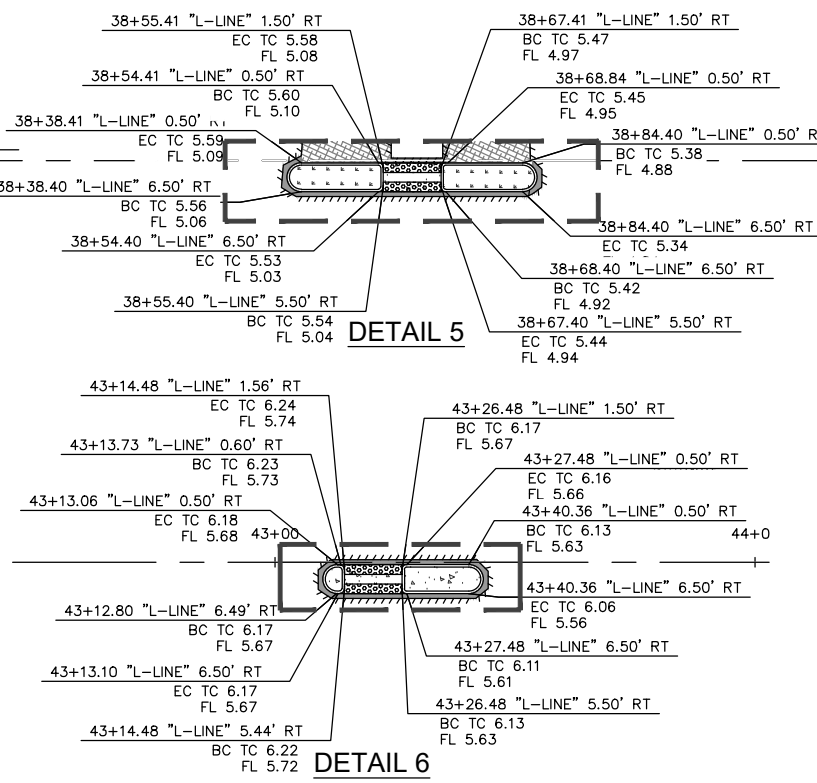
MATCHLINE SEE L-07



LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A). SEE PAVING PLANS FOR MORE INFORMATION.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK
- HMA (TYPE A) OVERLAY (6" MAX)
- CHANNEL DRAIN (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- LEAN RAIL

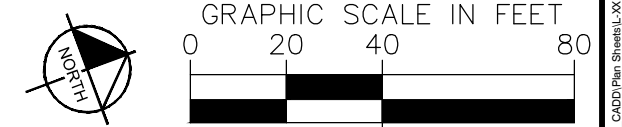
100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



CONSTRUCTION NOTES

- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 10 INSTALL PEDESTRIAN REFUGE ISLAND PER DETAIL D ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 11 INSTALL PEDESTRIAN REFUGE ISLAND PER DETAIL E ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

FOR NOTES SEE SHEET L-01



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

LAYOUT

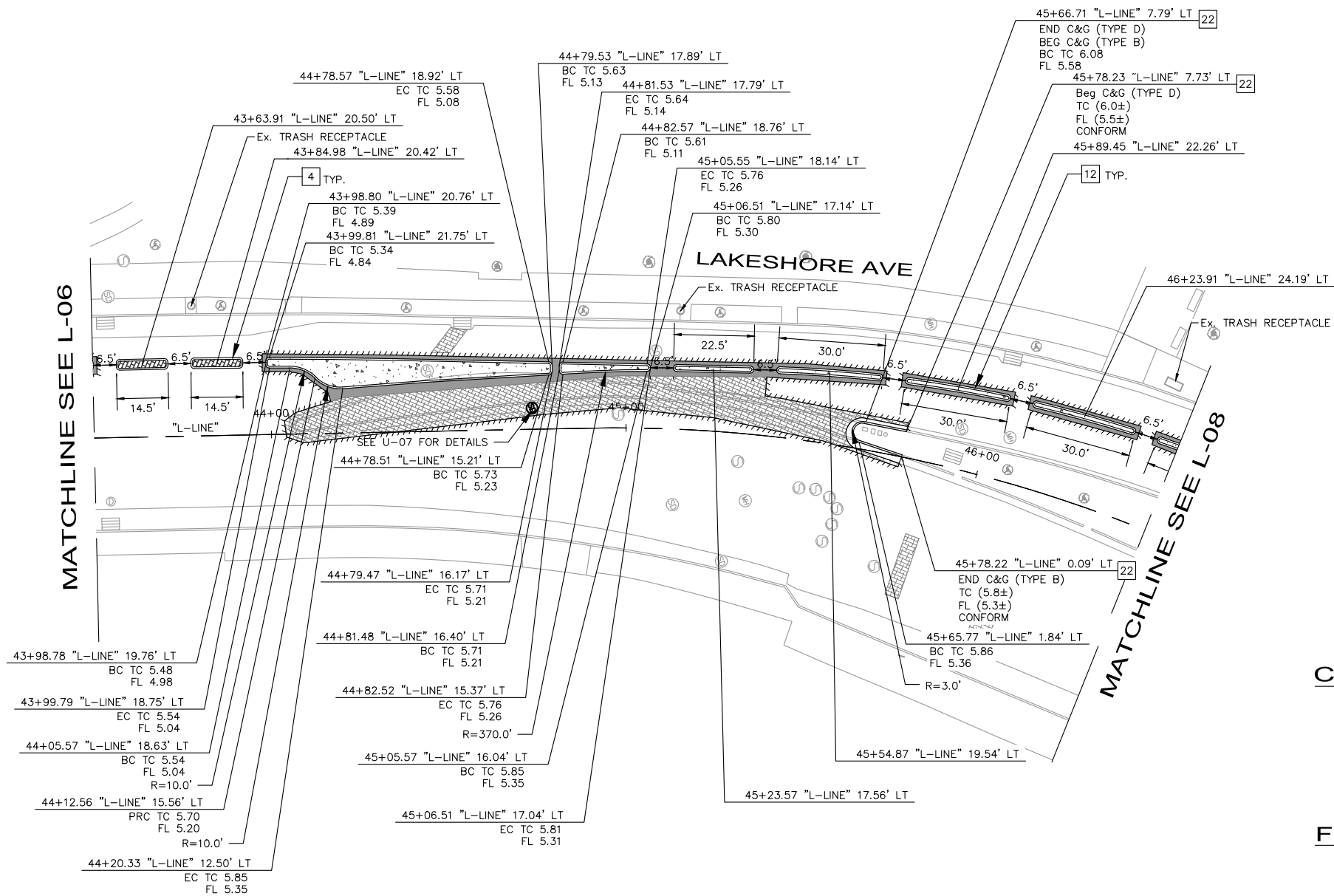
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
L-06
24 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RD\WY\1007299\204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\L-XX.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya.Pallivathucal



- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 12 INSTALL RAISED ISLAND PER DETAIL F ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

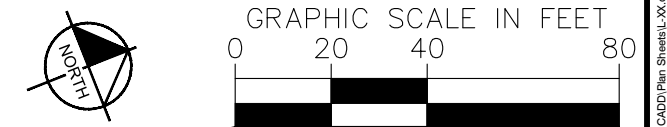
FOR NOTES SEE SHEET L-01

LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
-

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

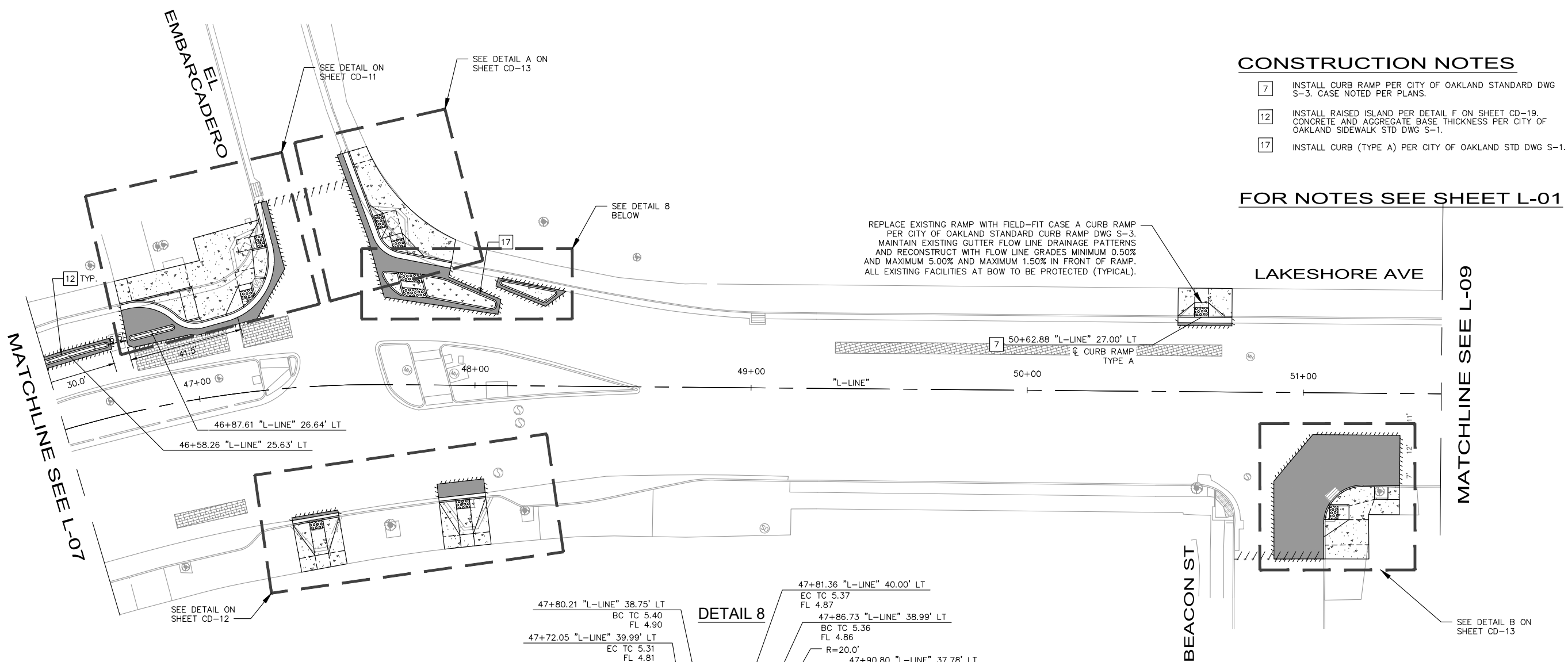
LAYOUT

SCALE: AS SHOWN
DATE: 12/11/2024

PROJECT NO.
1007299

SHEET NO.
L-07
25 OF 83

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\L-XX.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

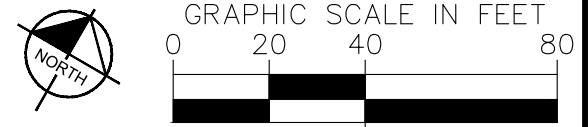
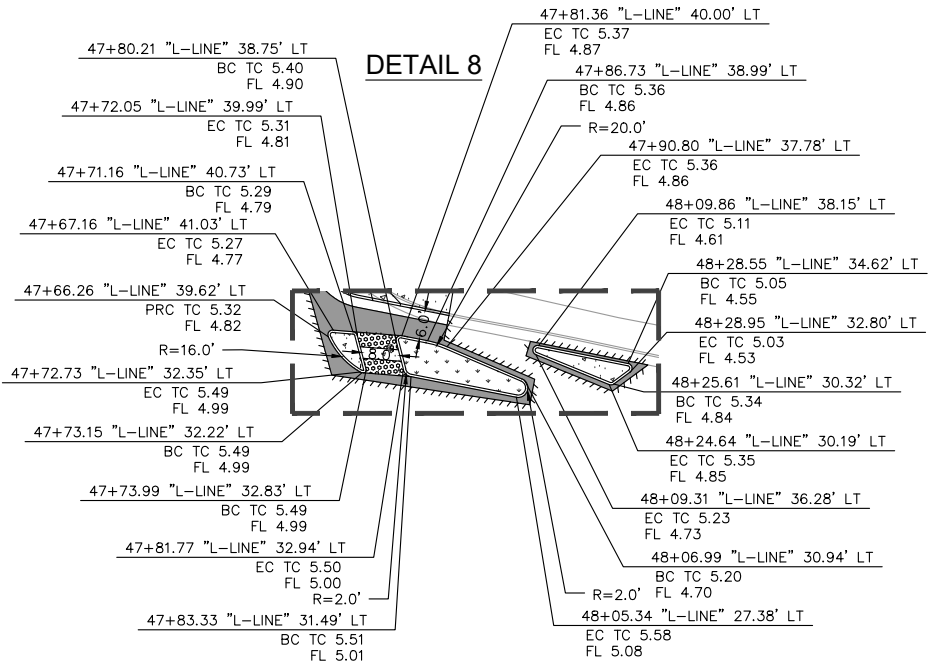


- ### CONSTRUCTION NOTES
- 7 INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
 - 12 INSTALL RAISED ISLAND PER DETAIL F ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
 - 17 INSTALL CURB (TYPE A) PER CITY OF OAKLAND STD DWG S-1.

FOR NOTES SEE SHEET L-01

REPLACE EXISTING RAMP WITH FIELD-FIT CASE A CURB RAMP PER CITY OF OAKLAND STANDARD CURB RAMP DWG S-3. MAINTAIN EXISTING GUTTER FLOW LINE DRAINAGE PATTERNS AND RECONSTRUCT WITH FLOW LINE GRADES MINIMUM 0.50% AND MAXIMUM 5.00% AND MAXIMUM 1.50% IN FRONT OF RAMP. ALL EXISTING FACILITIES AT BOW TO BE PROTECTED (TYPICAL).

- ### LEGEND
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 -



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

LAYOUT

PROJECT NO.
1007299

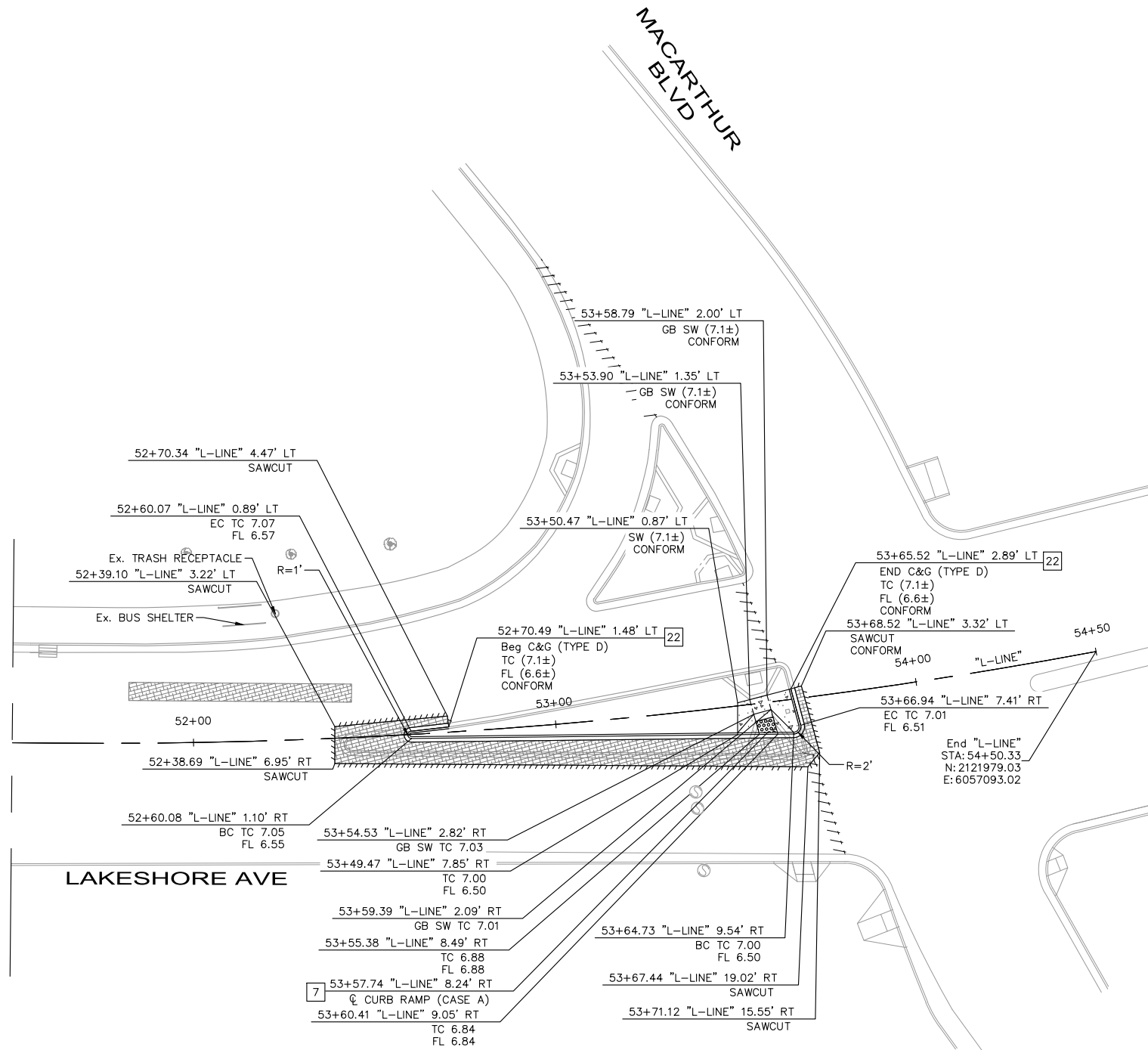
SCALE:
 AS SHOWN

SHEET NO.
 L-08
 26 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100708294 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\L-XX.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal

MATCHLINE SEE L-08



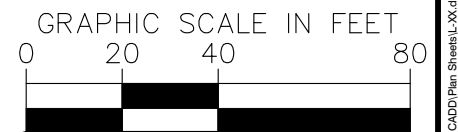
LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A). SEE PAVING PLANS FOR MORE INFORMATION.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)
- CHANNEL DRAIN (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- LEAN RAIL

CONSTRUCTION NOTES

- 7 INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

FOR NOTES SEE SHEET L-01



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

LAYOUT

PROJECT NO.
1007299

SCALE: AS SHOWN

SHEET NO.
L-09
27 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08709204 - Lakeshore Ave Cycle Trace\04_CADD\Plan_Sheets\L-XX.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal

GENERAL NOTES:

1. ALL LOCATIONS WHERE R/W IS NOT IDENTIFIED OR DOES NOT COVER LIMITS OF IMPROVEMENTS, EXISTING BACK OF WALK SHALL BE ASSUMED AS RIGHT OF WAY LIMITS.
2. CONTRACTOR SHALL SAWCUT TO A NEAT LINE WITHOUT DAMAGING EXISTING AC OR CONCRETE THAT IS TO REMAIN IN PLACE. ANY DAMAGE TO ADJACENT SURFACE MATERIAL TO REMAIN MUST BE REPAIRED OR REPLACED TO CONDITION SATISFACTORY TO THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
3. CONFORM SIDEWALK, CURB, AND GUTTER AT NEAREST SCOREMARK OR EXPANSION JOINT. EXACT LIMITS TO BE DETERMINED IN THE FIELD.
4. PROPOSED BACK OF WALK HORIZONTAL ALIGNMENT TO MATCH EXISTING, UNLESS OTHERWISE SHOWN ON THESE PLANS.
5. ALL EXISTING GRADES AT LIMITS OF WORK LABELED AS CONFORM SHALL BE VERIFIED PRIOR TO CONSTRUCTION AND ANY CONFLICT SHALL BE PROMPTLY BROUGHT TO THE ATTENTION OF THE ENGINEER.
6. GUTTER CROSS SLOPE AT BOTTOM OF RAMP OR BLENDED TRANSITIONS SHALL NOT EXCEED 5%.
7. ALL CROSSWALKS SLOPE SHOULD NOT EXCEED 5% MAX IN THE DIRECTION OF TRAVEL AND 2% MAX CROSS SLOPE.
8. CONTRACTOR TO VERIFY ALL DEPTHS AND LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES.
9. SEE DRAINAGE AND UTILITY PLANS FOR UTILITIES THAT SHOULD BE ADJUSTED TO GRADE. EXISTING UTILITY BOXES AND COVERS SHALL BE ADJUSTED TO BE FLUSH WITH THE CURB RAMP, SIDEWALK, AND PAVEMENT SURFACE. ALL OTHER UTILITY PIPES, DUCTS, BOXES, VAULTS, AND STRUCTURE SHALL BE PROTECTED IN PLACE UNLESS OTHERWISE SHOWN.
10. ALL CONCRETE ISLAND CURB CORNERS TO BE ROUNDED USING 1' RADIUS, UNLESS OTHERWISE SHOWN ON THESE PLANS.
11. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY BENCHMARK AND MONUMENTATION. IF A BENCHMARK OR MONUMENT IS DISTURBED, THE CONTRACTOR SHALL HAVE A LICENSED LAND SURVEYOR RESTORE THE BENCHMARK OR MONUMENT AT THE CONTRACTOR'S EXPENSE.
12. PROPOSED CURB AND GUTTER SHALL BE DOWELED WITH #4 DOWEL TO EXISTING CURB AND GUTTER AT ALL CONFORM LOCATIONS.
13. FOR PAVEMENT STRUCTURAL SECTIONS, SEE TYPICAL CROSS SECTIONS SHEETS AND PAVING PLANS.
14. FOR GRADING DETAILS AND LAYOUT INFORMATION NOT SHOWN, SEE CONSTRUCTION DETAIL SHEETS.
15. FOR SIGNING AND STRIPING IMPROVEMENTS, SEE SIGNAGE AND STRIPING PLANS.
16. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

LEGEND

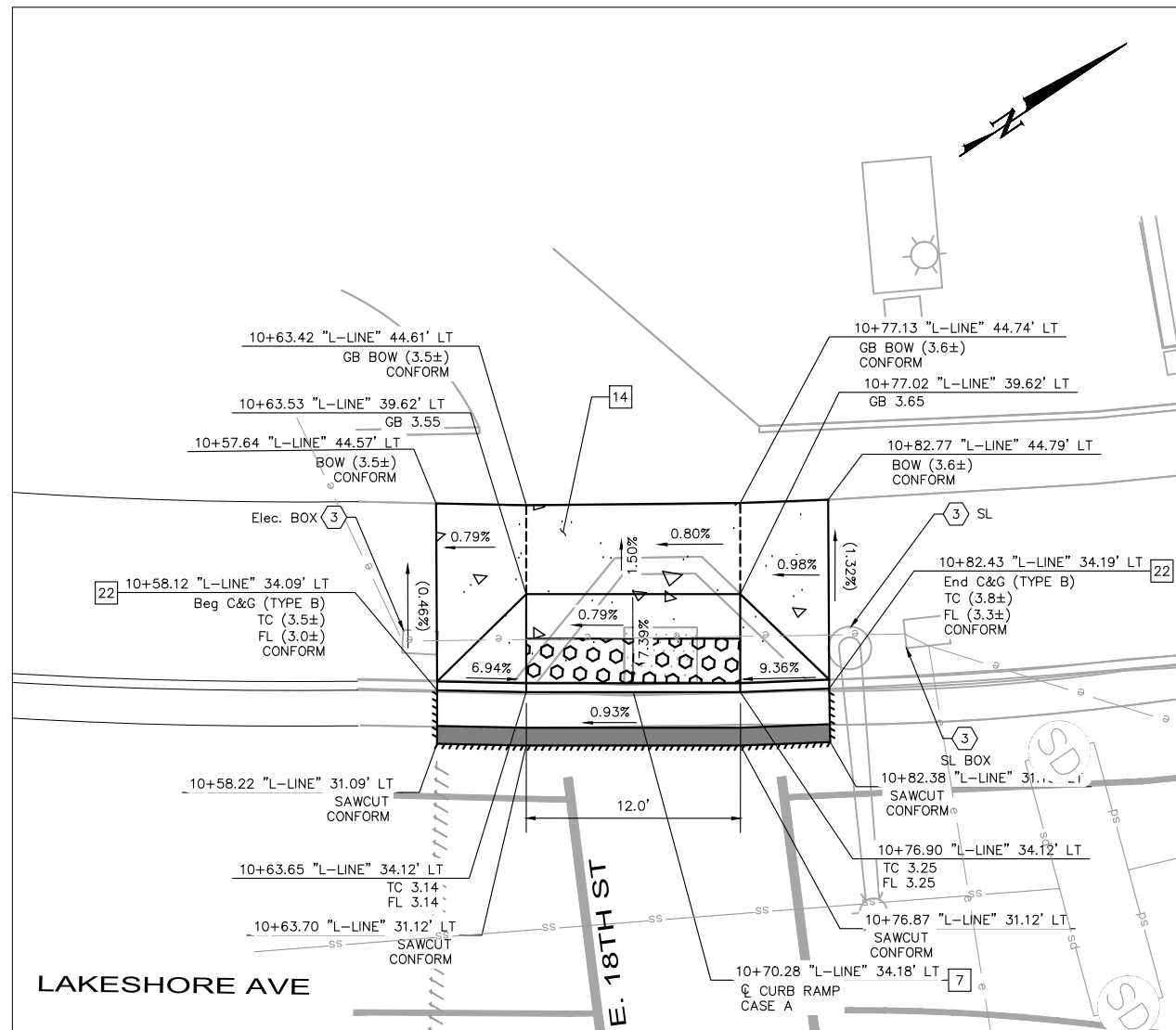
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
- TRENCH GRATE (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- Exist SLOPE
- PROPOSED SLOPE

CONSTRUCTION NOTES

- 7 INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

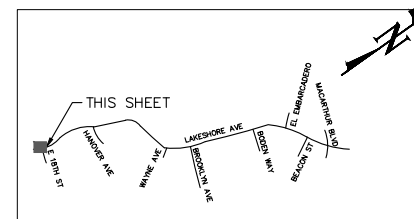
UTILITY AND DEMOLITION NOTES

- 3 PROTECT IN PLACE.



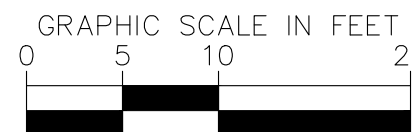
**LAKESHORE AVE & E. 18TH ST
CURB RAMP DETAILS (NORTHWEST CORNER)**

SEE SHEET L-01
SCALE 1"=10'



KEY MAP

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE













**CONSTRUCTION
DETAILS
CURB RAMP**

PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. CD-01
DATE: 12/11/2024 28 OF 83

DRAWING NAME: K:\OAK\RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-01.dwg
PLOT DATE: 12/11/24
PLOTTED BY: Priya Pallivathucal

LEGEND

-  PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
-  DETECTABLE WARNING SURFACE
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  HMA PAVEMENT (12" DEPTH)
-  PROPOSED LANDSCAPE AREA
-  RAISED ISLAND WITH DOWEL CURB
-  RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
-  TRENCH GRATE (SEE DD-01)
-  GRADE BREAK
-  SAWCUT LIMITS
-  Exist SLOPE
-  PROPOSED SLOPE

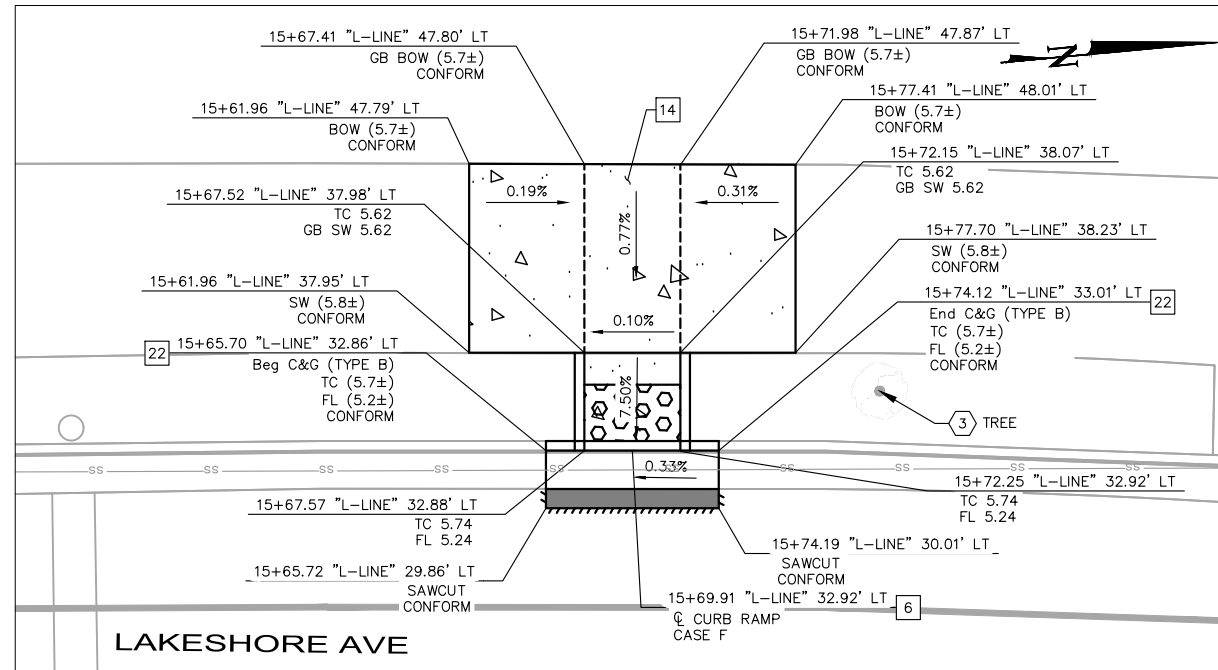
FOR GENERAL NOTES SEE SHEET CD-01

CONSTRUCTION NOTES

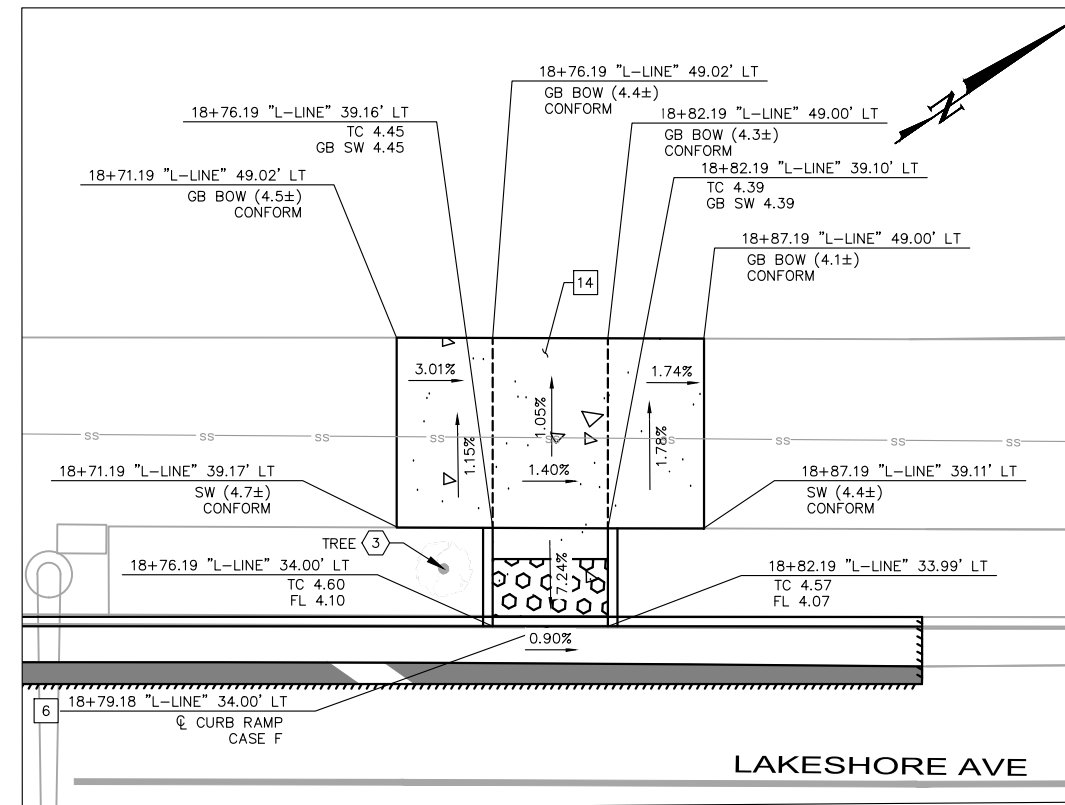
- 6 INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

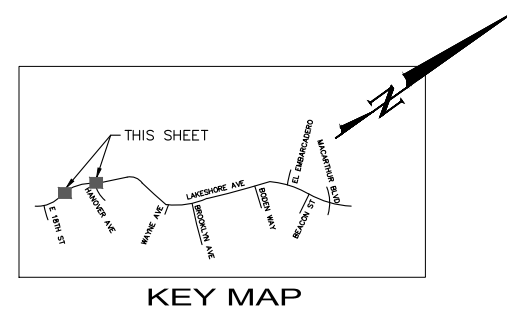
- 3 PROTECT IN PLACE.



**DETAIL A
LAKESHORE AVE**
SEE SHEET L-02
SCALE 1"=10'

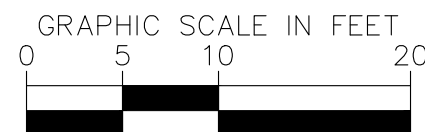


**DETAIL B
LAKESHORE AVE & HANOVER AVE
CURB RAMP DETAILS (NORTHWEST CORNER)**
SEE SHEET L-02
SCALE 1"=10'



KEY MAP

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
CURB RAMP**

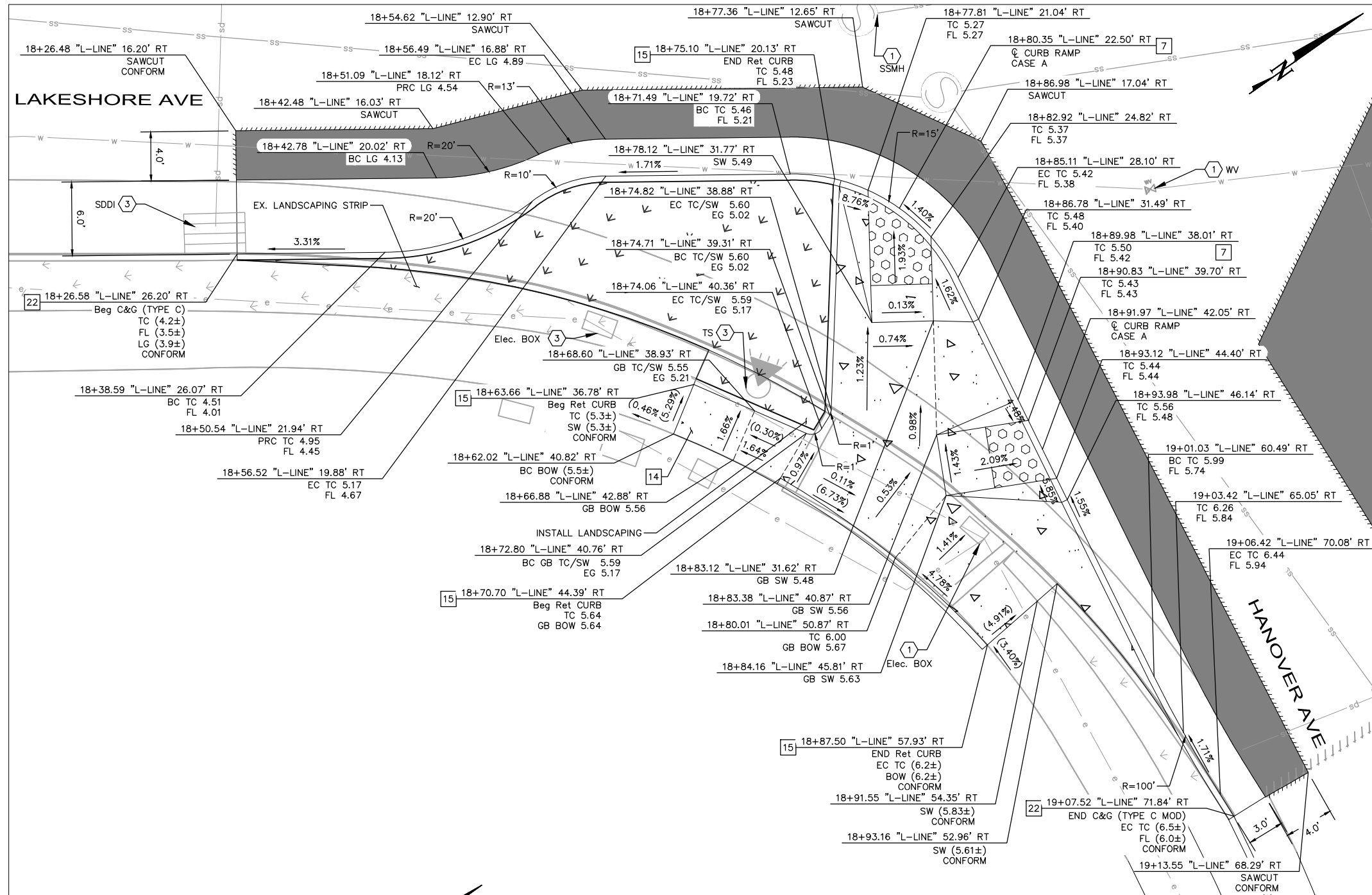
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-02
29 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK\RDWY\10706204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-01.dwg
PLOT DATE: 12/11/24
PLOTTED BY: Priya Pallivathucal



- ### LEGEND
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 -

SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

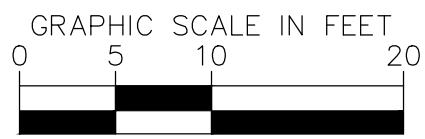
- 7 INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 15 INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

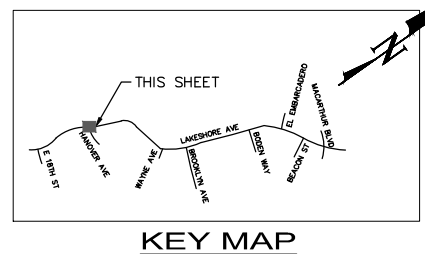
- 1 ADJUST TO GRADE.
- 3 PROTECT IN PLACE.

LAKESHORE AVE & HANOVER AVE CURB EXTENSION AND RAMP DETAILS (SOUTHWEST CORNER)

SEE SHEET L-02
SCALE 1"=10'



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

CONSTRUCTION DETAILS CURB RAMP

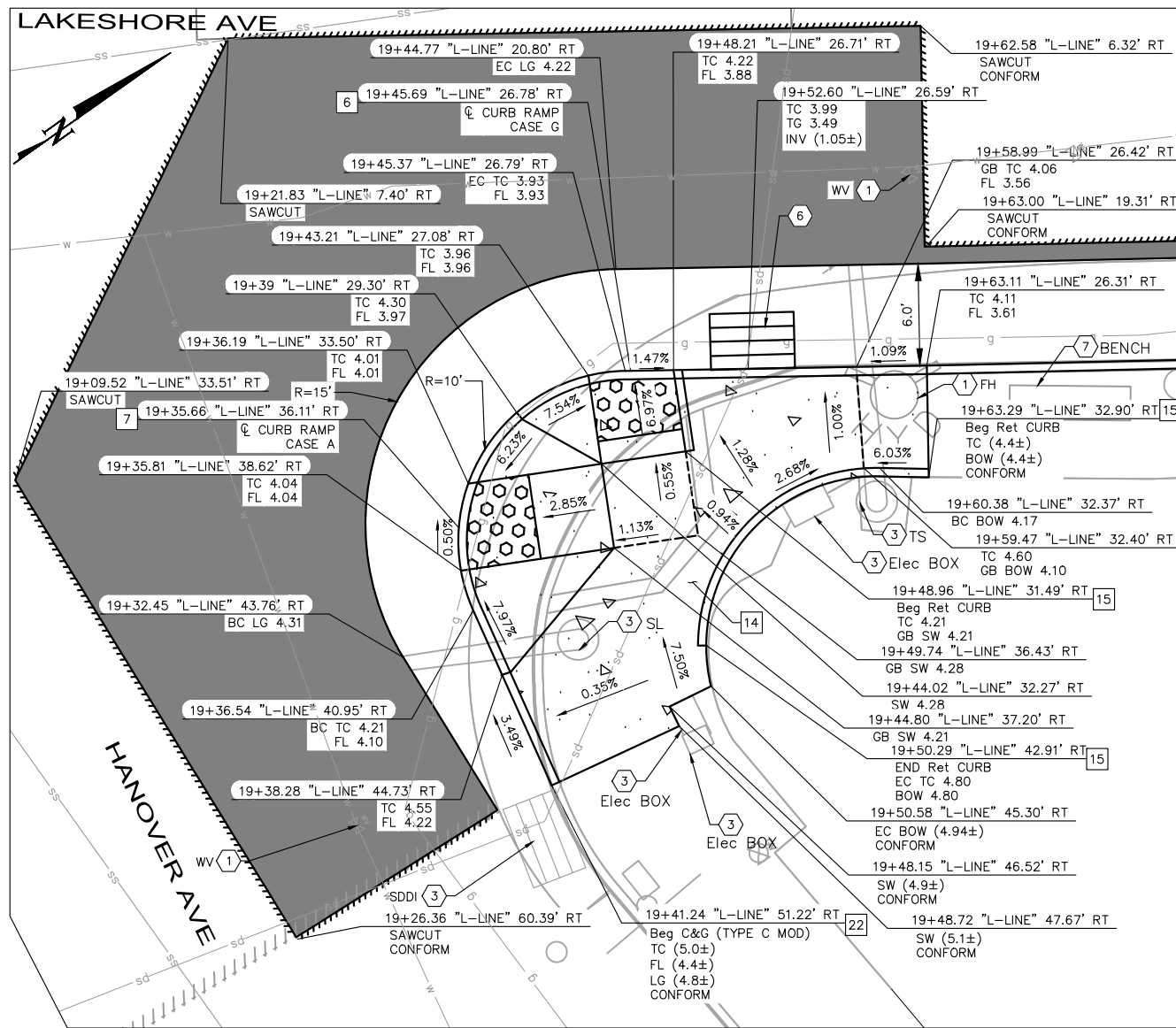
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-03
30 OF **83**

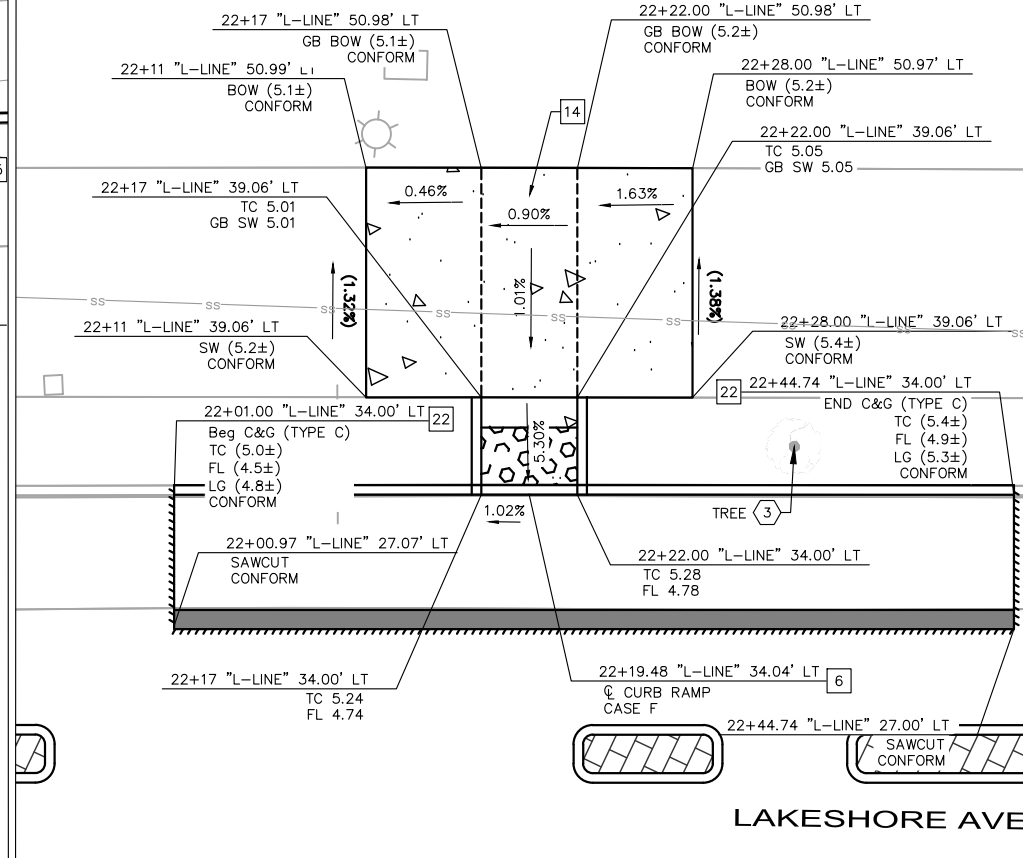
DATE: 12/11/2024

DRAWING NAME: K:\OAK_BDWY\02082024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-01.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal



DETAIL A
LAKESHORE AVE & HANOVER AVE
CURB EXTENSION AND RAMP DETAILS (SOUTHEAST CORNER)

SEE SHEET L-02
 SCALE 1"=10'



DETAIL B
LAKESHORE AVE
CURB RAMP DETAILS

SEE SHEET L-03
 SCALE 1"=10'

LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
- TRENCH GRATE (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- Exist SLOPE
- PROPOSED SLOPE

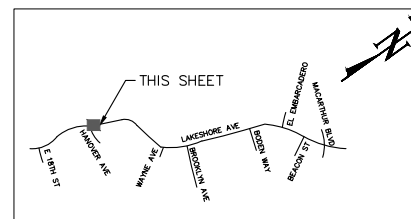
SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

- 6** INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN AB8A. CASE NOTED PER PLANS.
- 7** INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- 14** INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 15** INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
- 22** INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

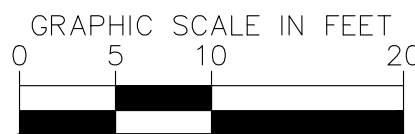
UTILITY AND DEMOLITION NOTES

- 1** ADJUST TO GRADE.
- 3** PROTECT IN PLACE.
- 6** REMOVE AND REPLACE STORM DRAIN INLET.
- 7** REMOVE AND RELOCATE. CONTRACTOR TO INSTALL AND COORDINATE FINAL LOCATION WITH ENGINEER.



KEY MAP

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

KimleyHorn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

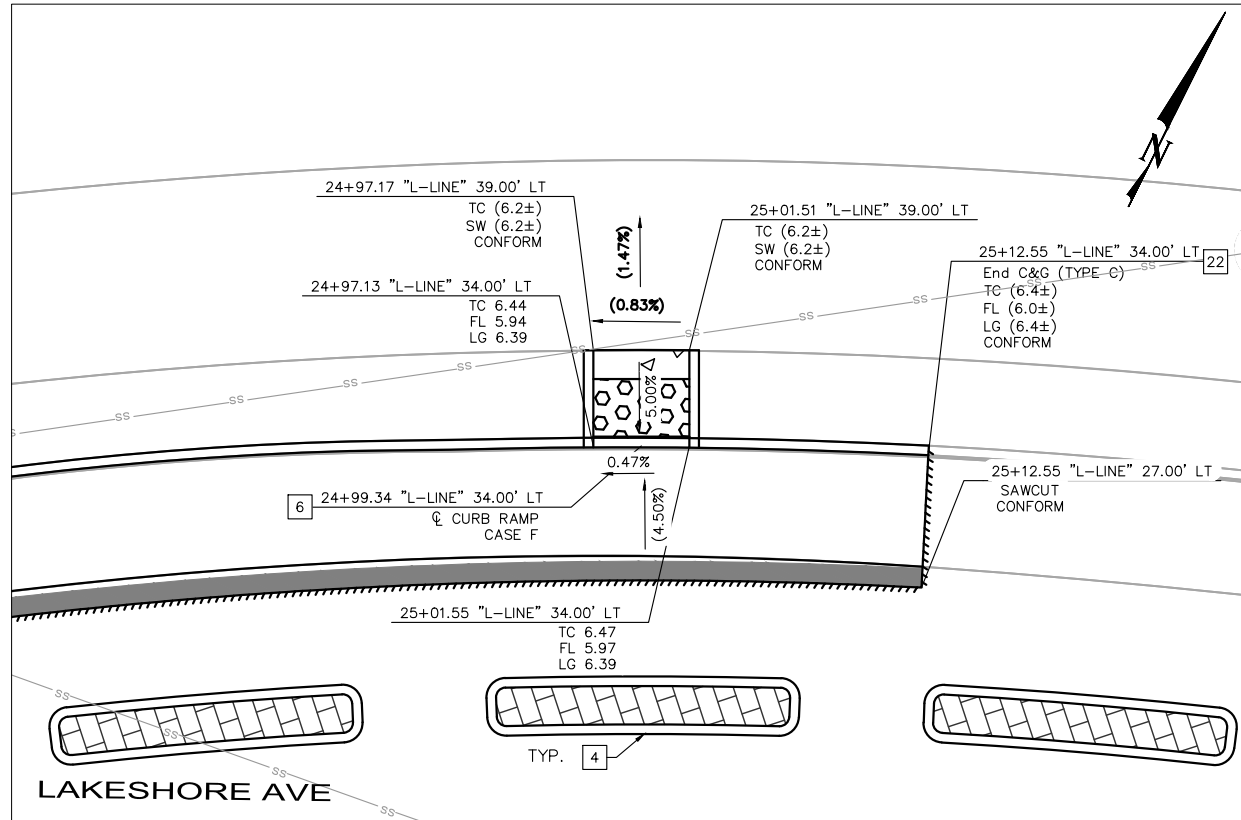
No.	DATE	BY	REFERENCE

CONSTRUCTION DETAILS CURB RAMP

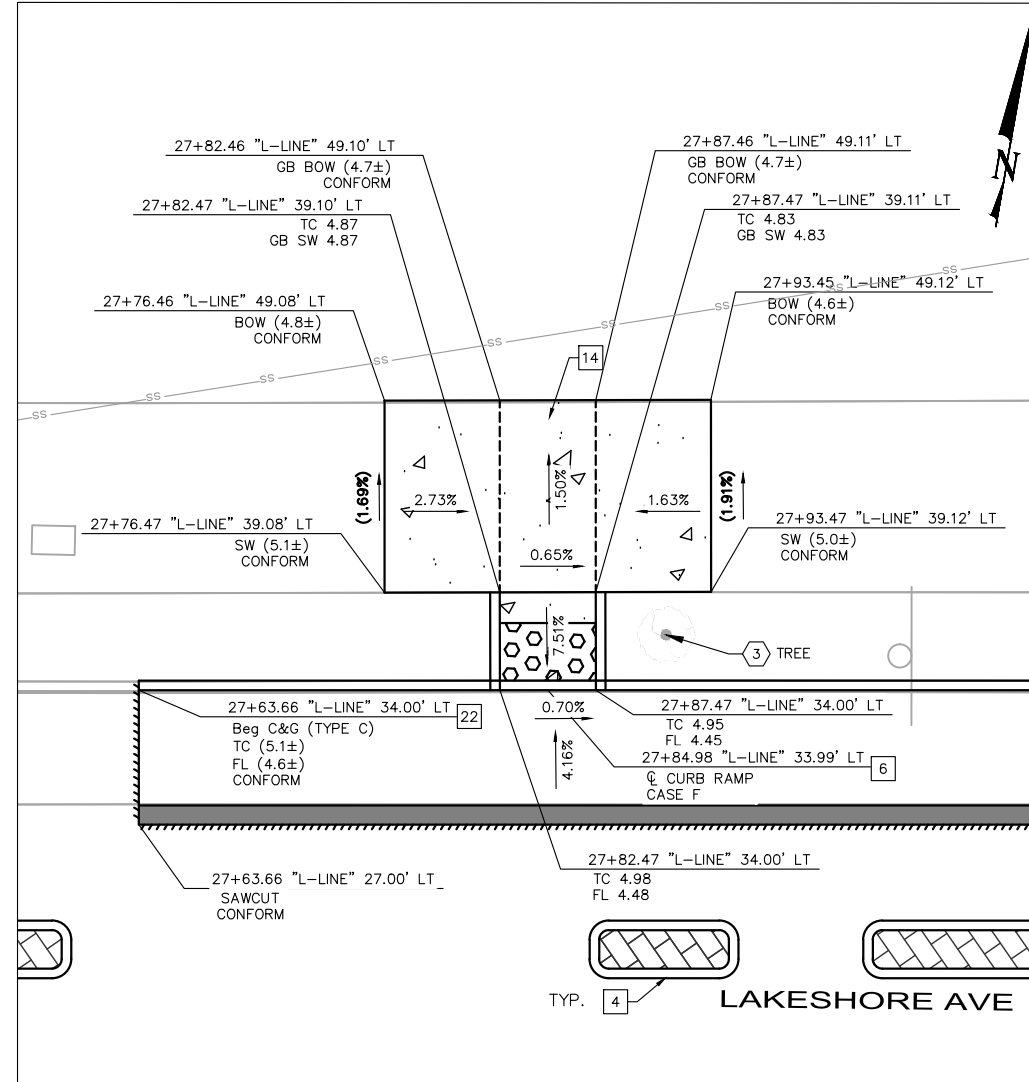
PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. CD-04
 DATE: 12/11/2024 **31** OF **83**

DRAWING NAME: K:\OAK\RD\WY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-01.dwg
 PLOT DATE: 12/11/24
 PLOTTED BY: Priya Pallivathucal















DETAIL A
LAKESHORE AVE
CURB RAMP DETAILS
 SEE SHEET L-03
 SCALE 1"=10'



DETAIL B
LAKESHORE AVE
CURB RAMP DETAILS
 SEE SHEET L-04
 SCALE 1"=10'

LEGEND

-  PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
-  DETECTABLE WARNING SURFACE
-  BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
-  HMA PAVEMENT (12" DEPTH)
-  PROPOSED LANDSCAPE AREA
-  RAISED ISLAND WITH DOWEL CURB
-  RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
-  TRENCH GRATE (SEE DD-01)
-  GRADE BREAK
-  SAWCUT LIMITS
-  EXIST SLOPE
-  PROPOSED SLOPE

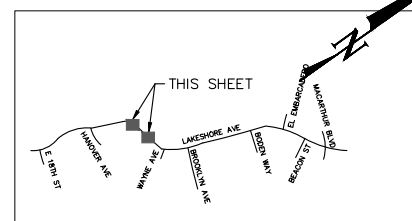
SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 6 INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

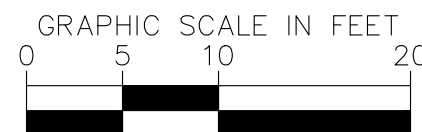
UTILITY AND DEMOLITION NOTES

- 3 PROTECT IN PLACE.



KEY MAP

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
 DETAILS
 CURB RAMP**

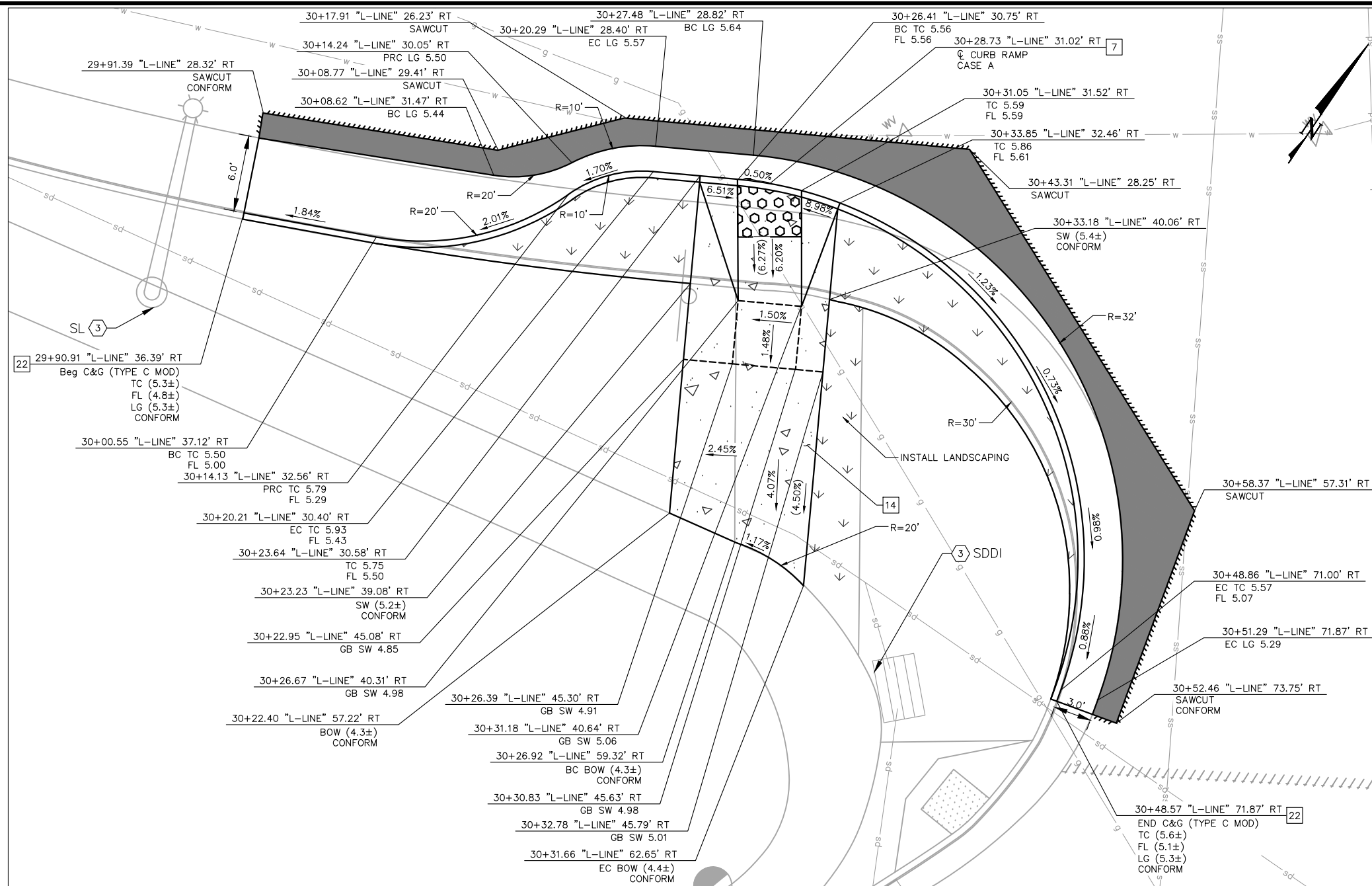
PROJECT NO.
1007299

SCALE:
 AS SHOWN

DATE: 12/11/2024

SHEET NO.
 CD-05
32 OF **83**

DRAWING NAME: K:\OAK\RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-01.dwg
 PLOT DATE: 12/11/24
 PLOTTED BY: Priya Pallivathucal



LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
-

SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

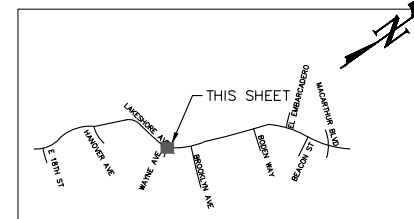
- 7** INSTALL "CASE A" AND "BLENDED TRANSITION" CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- 14** INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 16** INSTALL MODIFIED CURB AND GUTTER (TYPE C) PER CITY OF OAKLAND STD DWG S-1. CURB HEIGHT AS SHOWN ON PLANS.

UTILITY AND DEMOLITION NOTES

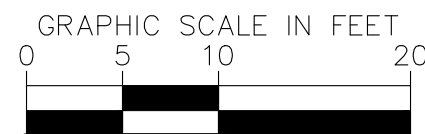
- 1** ADJUST TO GRADE.
- 3** PROTECT IN PLACE.

**LAKESHORE AVE & WAYNE AVE
CURB EXTENSION AND RAMP DETAILS (SOUTHWEST CORNER)**

SEE SHEET L-04
SCALE 1"=10'



KEY MAP



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
CURB RAMP**

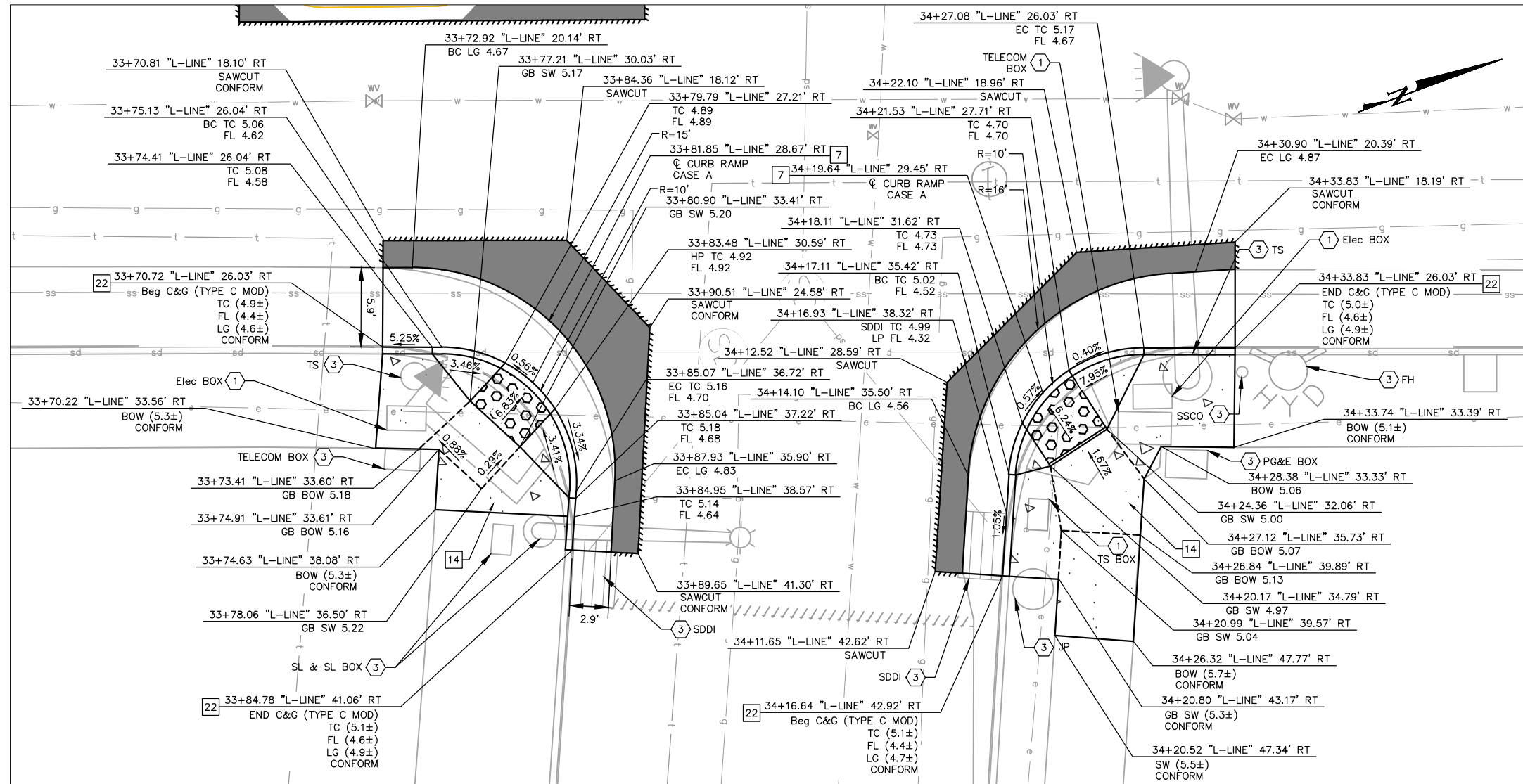
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-06
33 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK\RD\WY\021006204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-06.dwg
PLOT DATE: 12/11/24
PLOT BY: Priya Pallivathucal



**LAKESHORE AVE & BROOKLYN AVE
CURB RAMP DETAILS (SOUTHWEST AND SOUTHEAST CORNERS)**

SEE SHEET L-05

SCALE 1"=10'

LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
- TRENCH GRATE (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- Exist SLOPE
- PROPOSED SLOPE

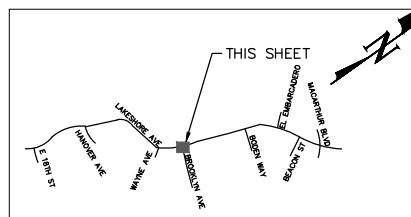
SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

- 7 INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

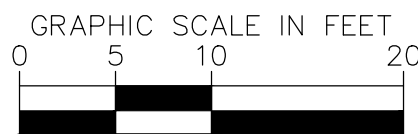
UTILITY AND DEMOLITION NOTES

- 1 ADJUST TO GRADE.
- 3 PROTECT IN PLACE.



KEY MAP

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

KimleyHorn

1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

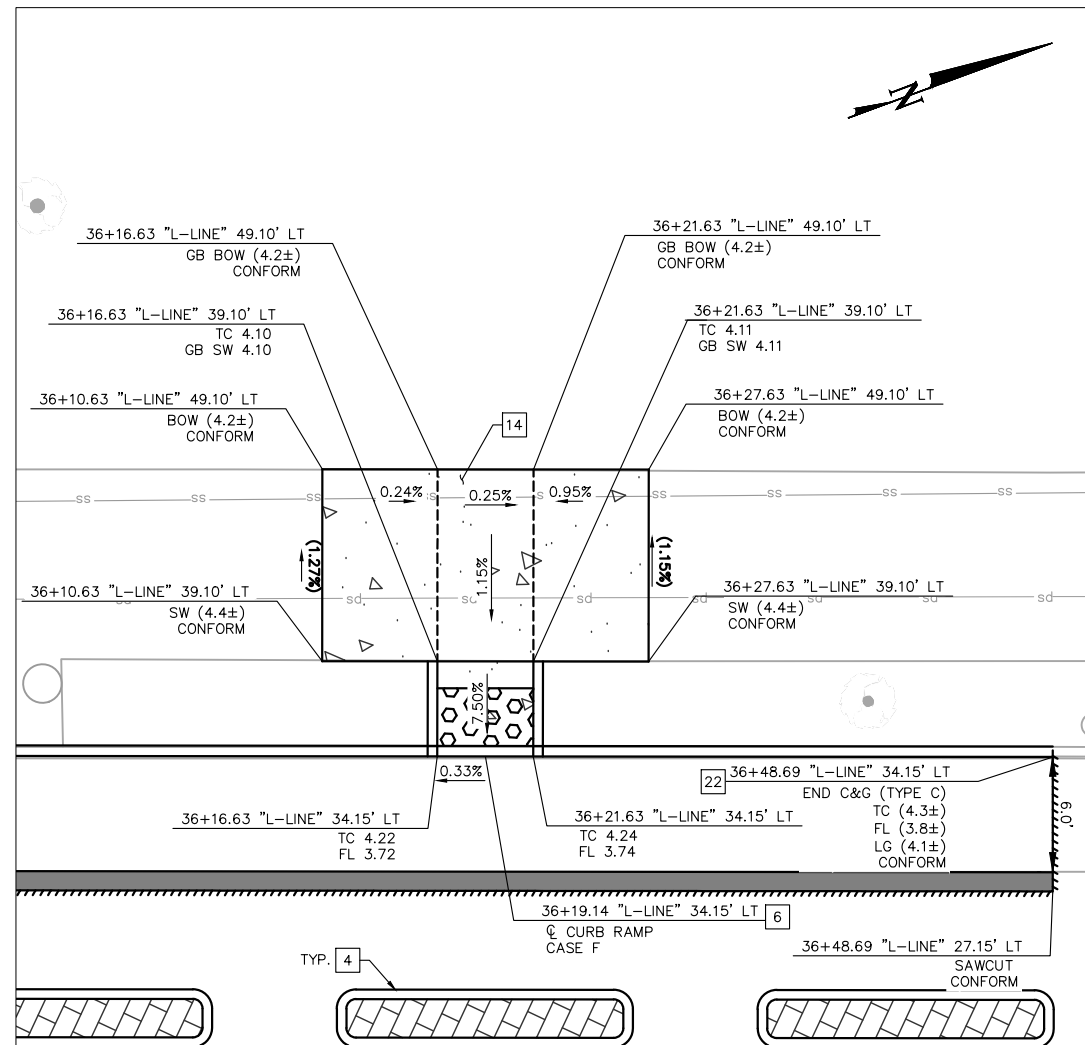
**CONSTRUCTION
DETAILS
CURB RAMP**

PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-07
34 OF **83**

DATE: 12/11/2024



**DETAIL A: LAKESHORE AVE
CURB RAMP DETAILS**

SEE SHEET L-05
SCALE 1"=10'

LEGEND

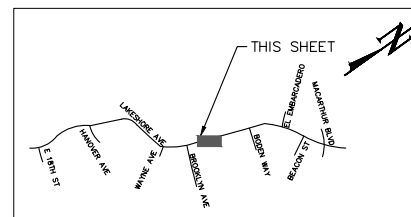
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
- TRENCH GRATE (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- Exist SLOPE
- PROPOSED SLOPE

SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

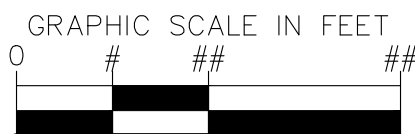
- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 6 INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES



KEY MAP

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

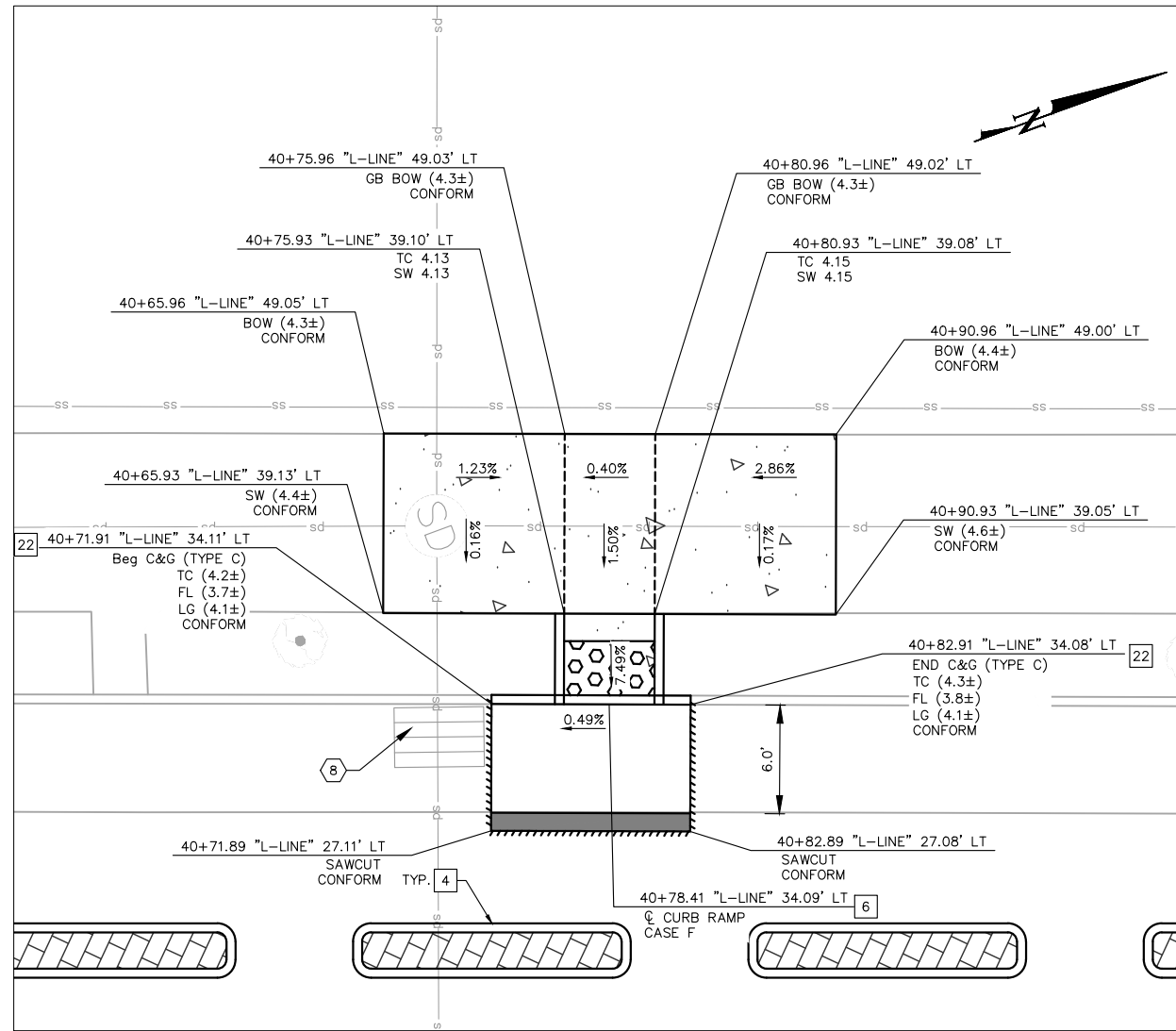
No.	DATE	BY	REFERENCE

**CONSTRUCTION DETAILS
CURB RAMP**

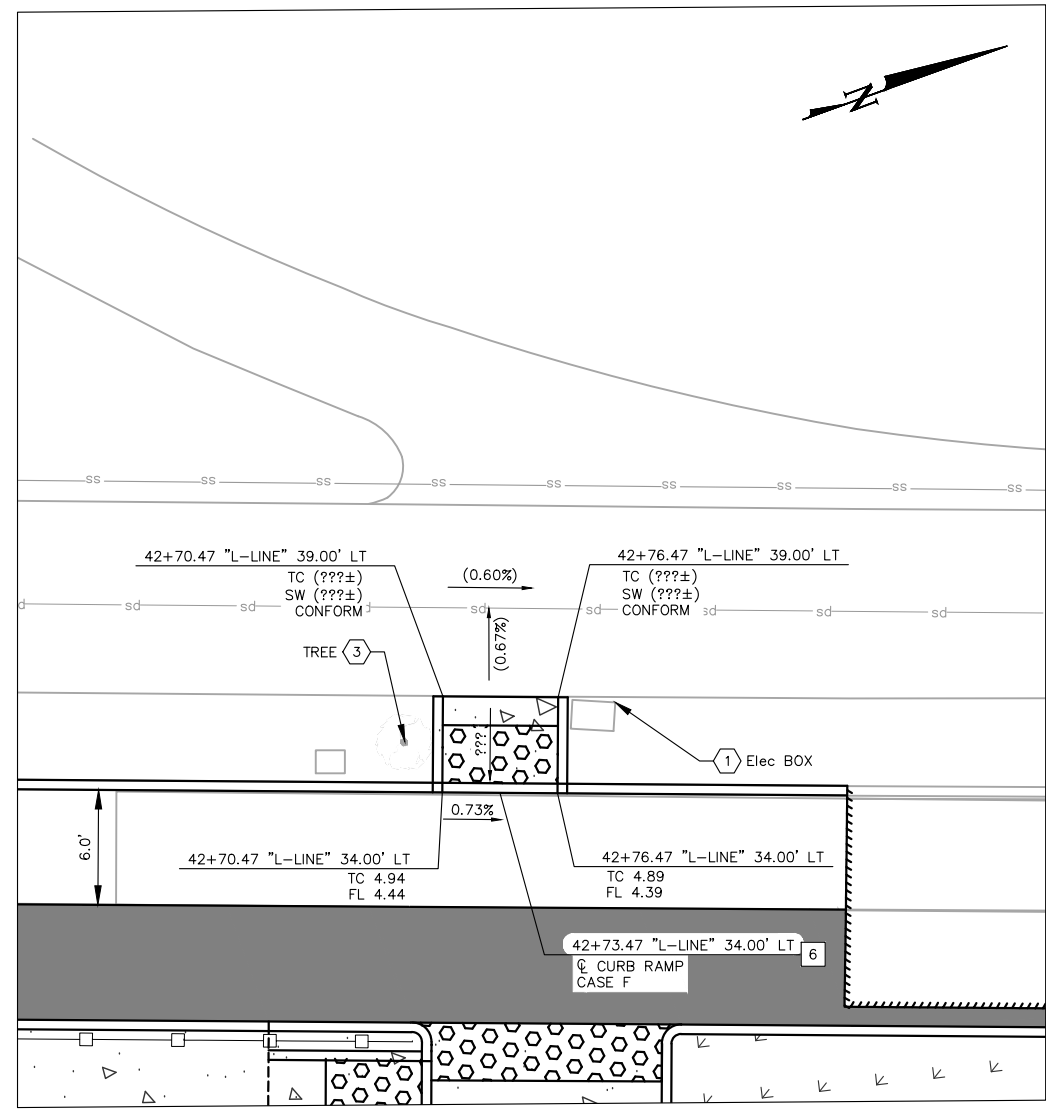
PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. CD-08
DATE: 12/11/2024 35 OF 83

DRAWING NAME: K:\OAK\RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-06.dwg
PLOT DATE: 12/11/24
PLOT BY: Priya Pallivathucal



**DETAIL A: LAKESHORE AVE
CURB RAMP DETAILS**
SEE SHEET L-06
SCALE 1"=10'



**DETAIL B: LAKESHORE AVE
CURB RAMP DETAILS**
SEE SHEET L-06
SCALE 1"=10'

- LEGEND**
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 - BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
 - HMA PAVEMENT (12" DEPTH)
 - PROPOSED LANDSCAPE AREA
 - RAISED ISLAND WITH DOWEL CURB
 - RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
 - TRENCH GRATE (SEE DD-01)
 - GRADE BREAK
 - SAWCUT LIMITS
 - Exist SLOPE
 - PROPOSED SLOPE

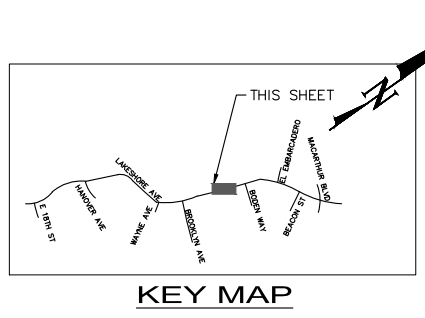
SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

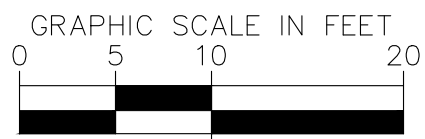
- 4 INSTALL RAISED ISLAND WITH DOWEL CURB PER DETAIL A ON SHEET CD-19, CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 6 INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

- 1 ADJUST TO GRADE.
- 3 PROTECT IN PLACE.
- 8 REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

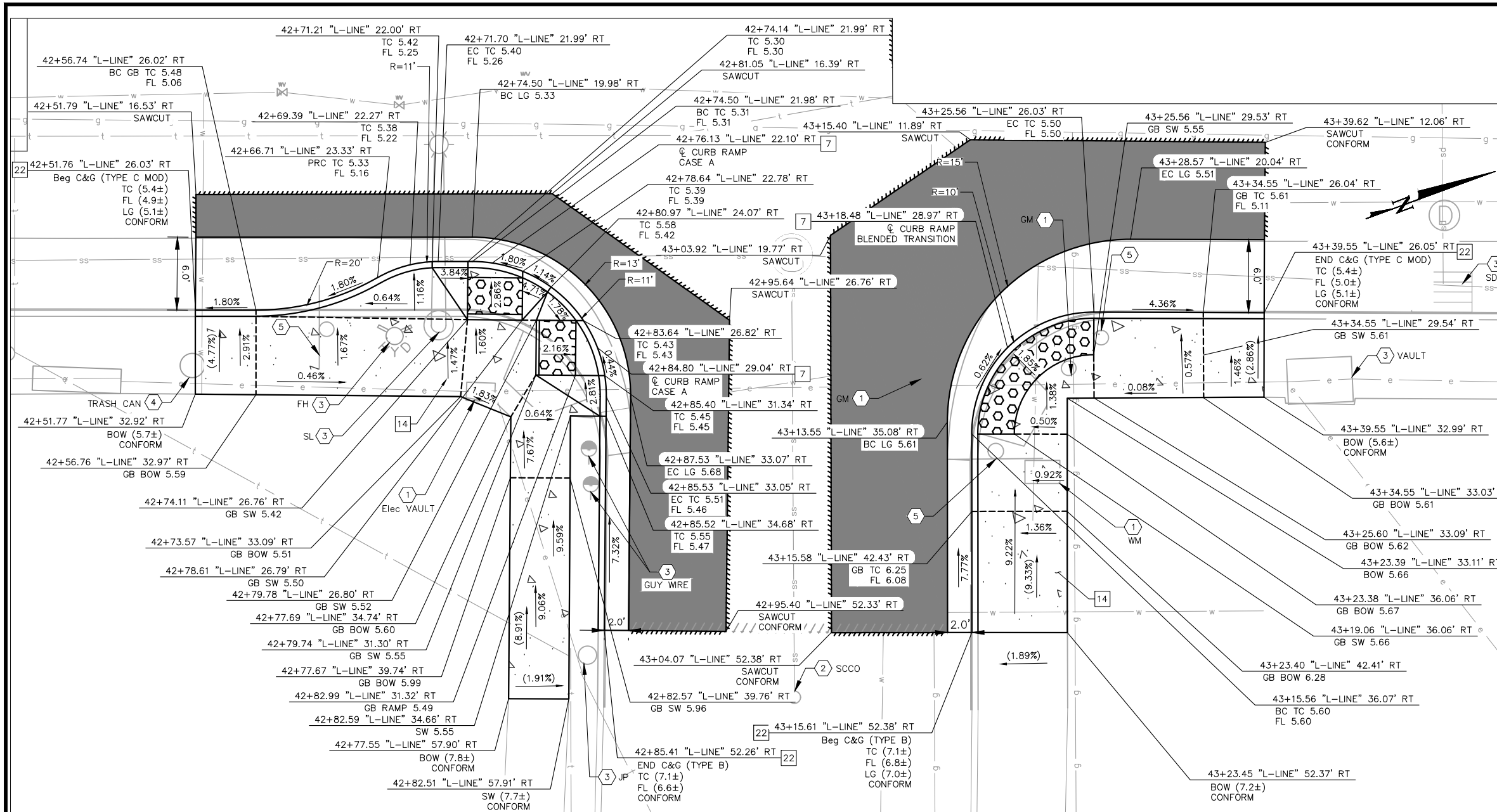
No.	DATE	BY	REFERENCE

**CONSTRUCTION DETAILS
CURB RAMP**

PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. CD-09
DATE: 12/11/2024 36 OF 83

DRAWING NAME: K:\OAK\RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-06.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
- TRENCH GRATE (SEE DD-01)
- GRADE BREAK
- SAWCUT LIMITS
- Exist SLOPE
- PROPOSED SLOPE

SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

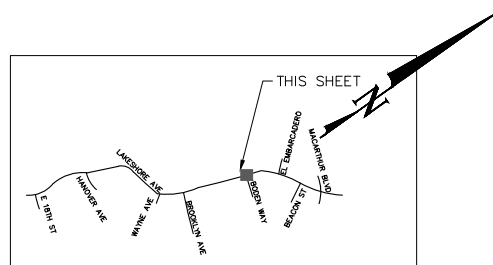
- INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
- INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

- ADJUST TO GRADE.
- EXISTING TO REMAIN.
- PROTECT IN PLACE.
- REMOVE AND RESET IN PLACE.
- REMOVE AND RESET SIGN AND POST. SEE SS SHEETS FOR MORE DETAILS.

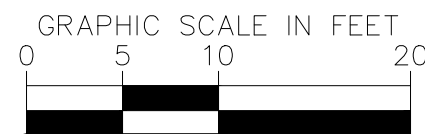
**LAKESHORE AVE & BODEN WAY
CURB RAMP DETAILS (SOUTHWEST AND SOUTHEAST CORNERS)**

SEE SHEET L-06
SCALE 1"=10'



KEY MAP

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley-Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
CURB RAMP**

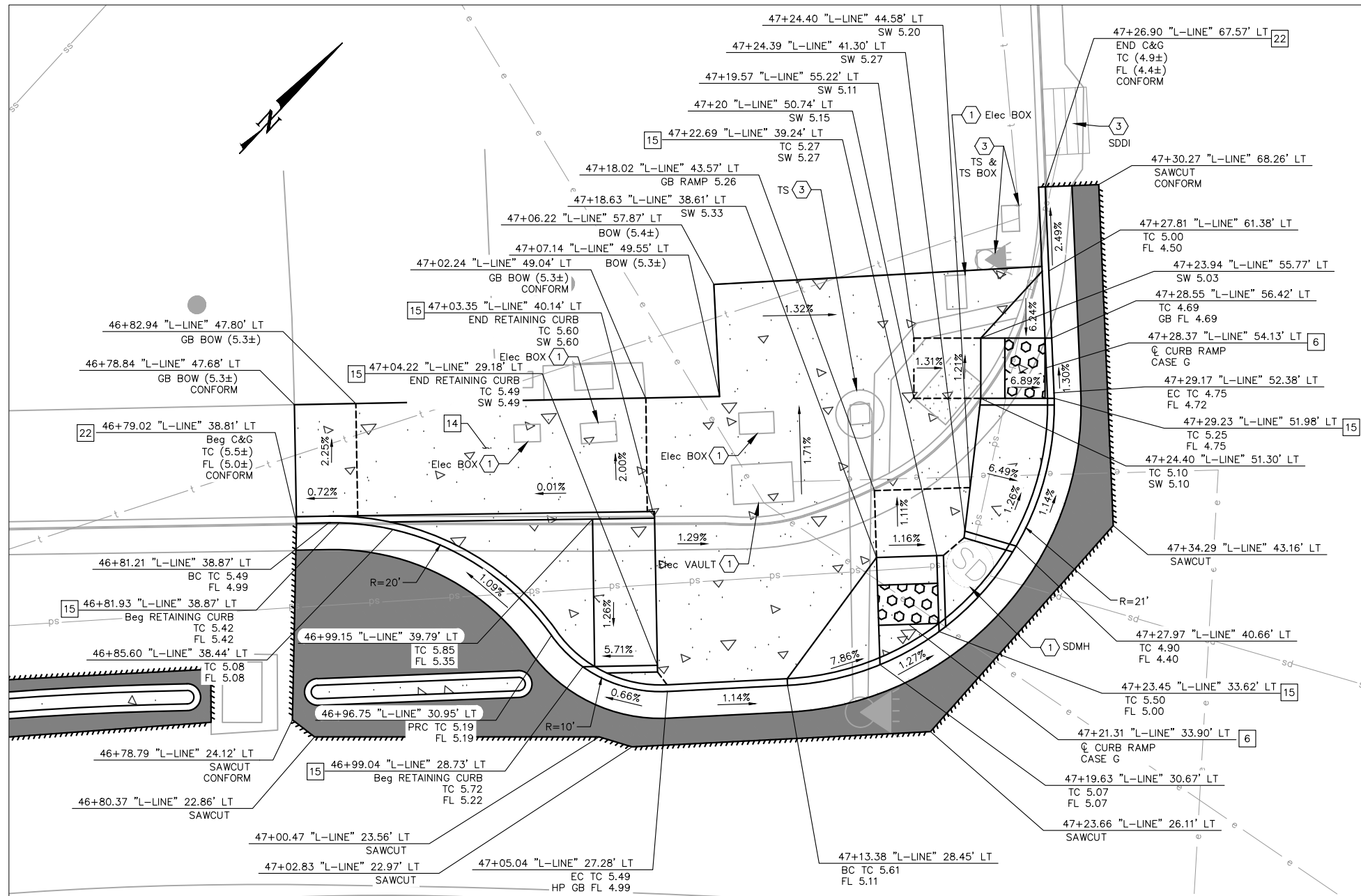
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-10
37 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-06.dwg
PLOT DATE: 12/11/24
PLOT BY: Priya Pallivathucal



LEGEND

- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
-

SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

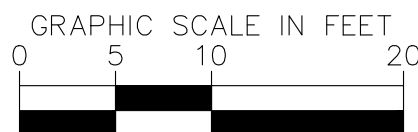
- 6** INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 14** INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 15** INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
- 22** INSTALL CURB AND GUTTER (TYPE B) PER CITY OF OAKLAND STD DWG S-1.

UTILITY AND DEMOLITION NOTES

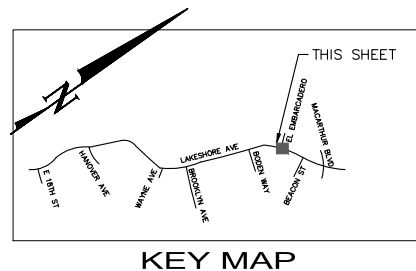
- 1** ADJUST TO GRADE.
- 3** PROTECT IN PLACE.

LAKESHORE AVE & EL EMBARCADERO CURB RAMP DETAILS (NORTHWEST CORNER)

SEE SHEET L-08
SCALE 1"=10'



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



100% SUBMITTAL NOT FOR BID OR CONSTRUCTION

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BOULEVARD

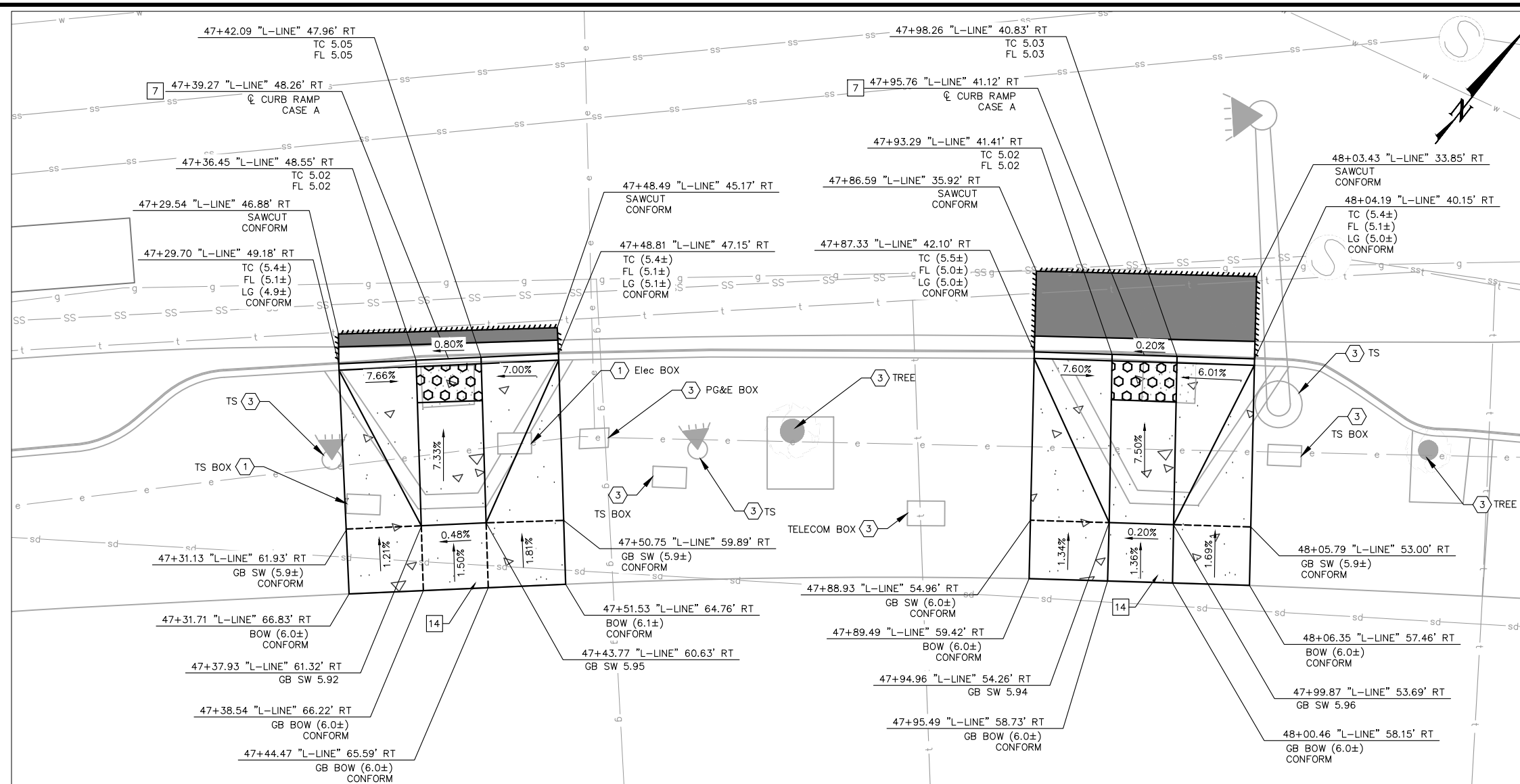
No.	DATE	BY	REFERENCE

CONSTRUCTION DETAILS CURB RAMP

PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. CD-11
DATE: 12/11/2024 **38** OF **83**

DRAWING NAME: K:\OAK_RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-11.dwg
PLOT DATE: 12/11/24
PLOT BY: Priya Pallivathucal



- ### LEGEND
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 -

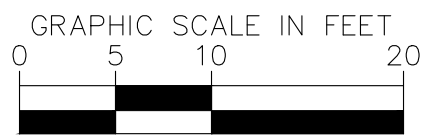
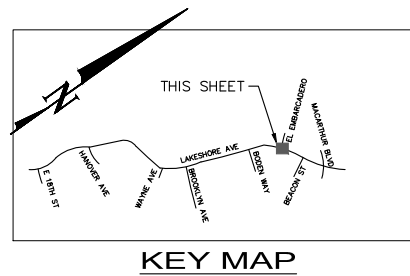
SEE GENERAL NOTES ON SHEET CD-01

- ### CONSTRUCTION NOTES
- 7** INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
 - 14** INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.

- ### UTILITY AND DEMOLITION NOTES
- 1** ADJUST TO GRADE.
 - 3** PROTECT IN PLACE.

**LAKESHORE AVE & EL EMBARCADERO
CURB RAMP DETAILS (SOUTHWEST AND SOUTHEAST CORNERS)**

SEE SHEET L-08
SCALE 1"=10'



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
CURB RAMP**

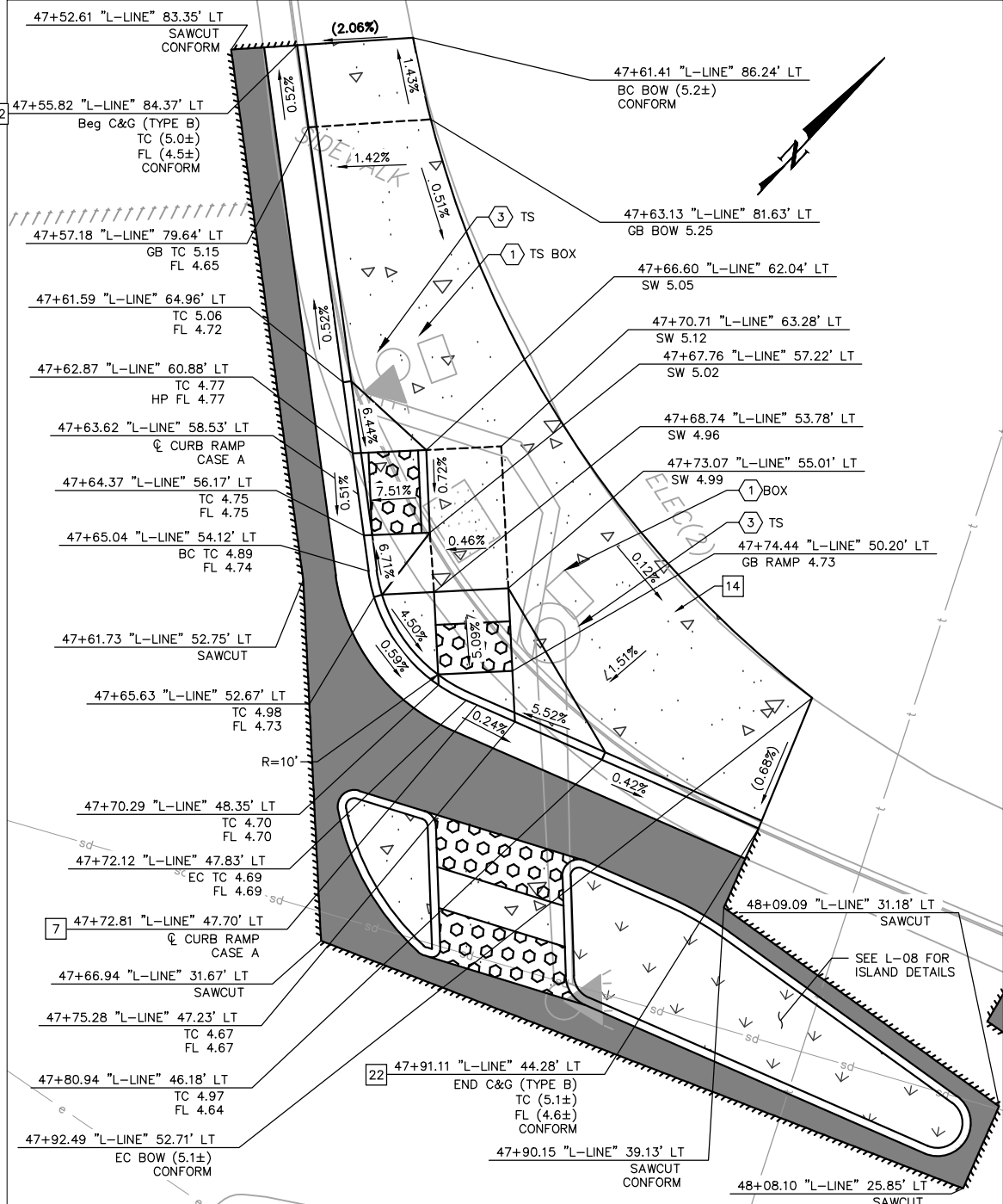
PROJECT NO.
1007299

SCALE:
AS SHOWN

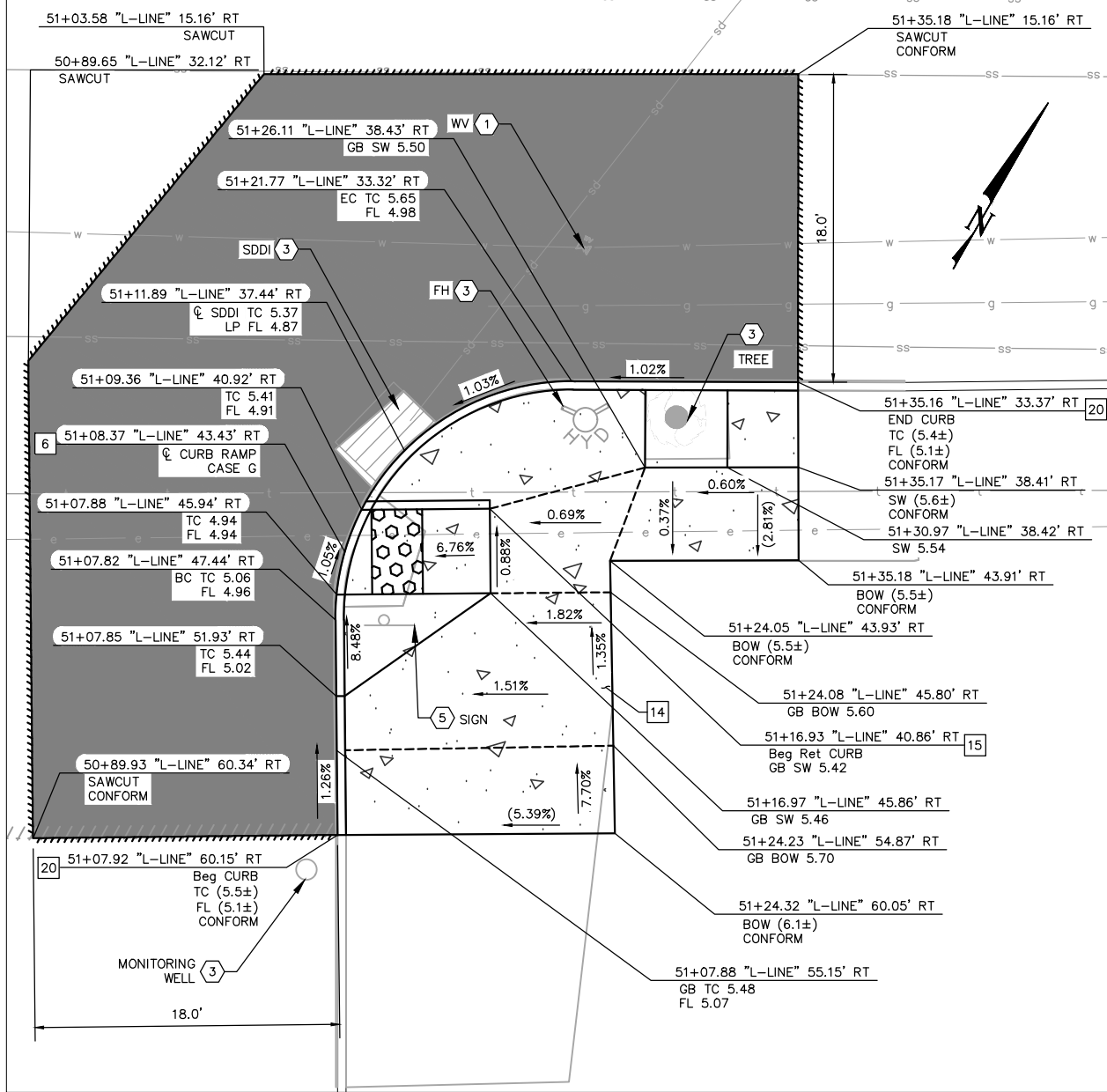
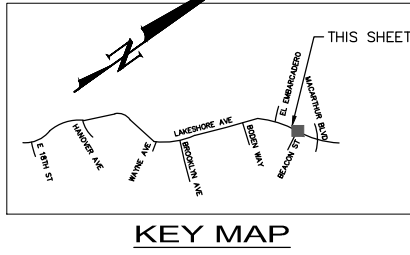
SHEET NO.
CD-12
39 OF **83**

DATE: 12/11/2024

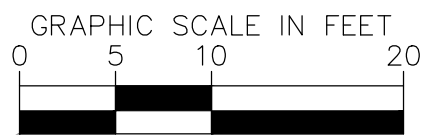
DRAWING NAME: K:\OAK\RDWY\02062024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-11.dwg
PLOT DATE: 12/11/24
PLOT BY: Priya Pallivathucal



DETAIL A
LAKESHORE AVE & EL EMBARCADERO
CURB RAMP DETAILS (NORTHEAST CORNER)
 SEE SHEET L-08
 SCALE 1"=10'



DETAIL B
LAKESHORE AVE & BEACON ST
CURB RAMP DETAILS (SOUTHEAST CORNER)
 SEE SHEET L-08
 SCALE 1"=10'



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

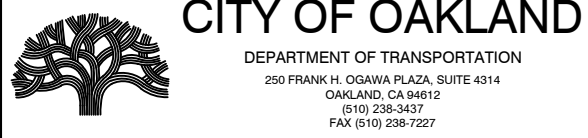
- LEGEND**
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 - BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
 - HMA PAVEMENT (12" DEPTH)
 - PROPOSED LANDSCAPE AREA
 - RAISED ISLAND WITH DOWEL CURB
 - RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
 - CHANNEL DRAIN (SEE DD-01)
 - GRADE BREAK
 - SAWCUT LIMITS
 - Exist SLOPE
 - PROPOSED SLOPE

SEE GENERAL NOTES ON SHEET CD-01

- CONSTRUCTION NOTES**
- 6** INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
 - 7** INSTALL CURB RAMP PER CITY OF OAKLAND STANDARD DWG S-3. CASE NOTED PER PLANS.
 - 14** INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
 - 15** INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
 - 20** INSTALL CURB (TYPE A1-6 MOD) PER DETAIL F ON CD-20. CURB HEIGHT AS SHOWN ON PLANS.
 - 22** INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

- UTILITY AND DEMOLITION NOTES**
- 1** ADJUST TO GRADE.
 - 3** PROTECT IN PLACE.
 - 5** REMOVE AND RESET SIGN AND POST. SEE SS SHEETS FOR MORE DETAILS.

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION



LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

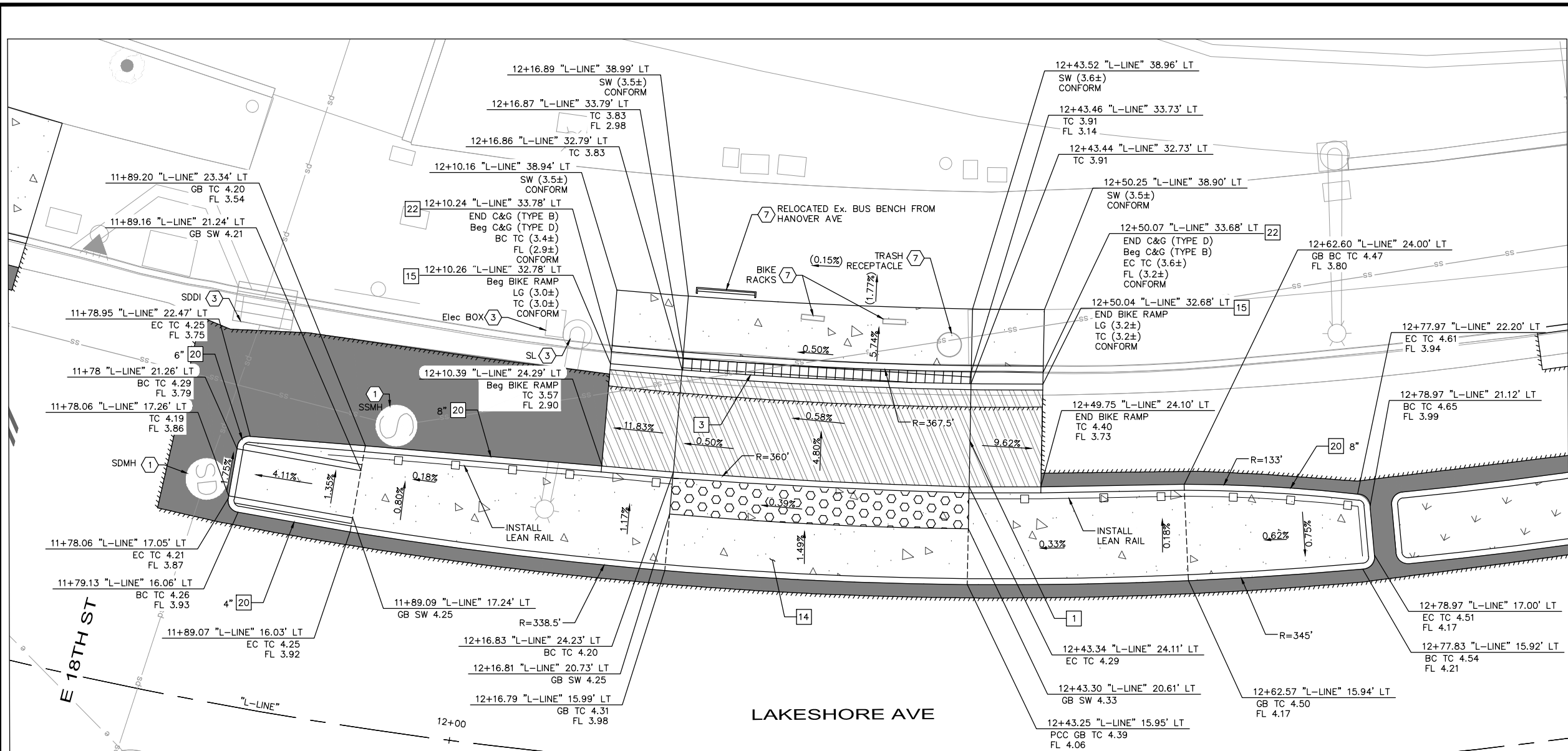
Kimley-Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CONSTRUCTION DETAILS CURB RAMP	PROJECT NO. 1007299
	SCALE: AS SHOWN
	SHEET NO. CD-13
	40 OF 83
	DATE: 12/11/2024

DRAWING NAME: K:\OAK_BDWY\02082024 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-11.dwg
 PLOT DATE: 12-13-24
 PLOTTED BY: Priya Pallivathucal



- ### LEGEND
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 -

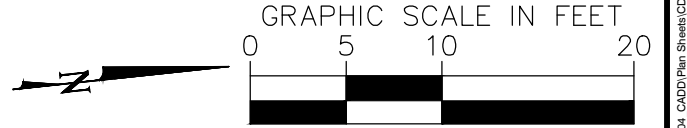
SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

- 1 INSTALL RAISED CROSSING FLUSH WITH BUS BOARDING ISLAND. SEE CD-21 FOR ADDITIONAL DETAILS.
- 3 INSTALL TRENCH GRATE PER DETAILS A AND B ON SHEET DD-01.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 15 INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
- 20 INSTALL CURB (TYPE A1-6 MOD) PER DETAIL F ON CD-20. CURB HEIGHT AS SHOWN ON PLANS.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

- 1 ADJUST TO GRADE.
- 3 PROTECT IN PLACE.
- 7 REMOVE AND RELOCATE. CONTRACTOR TO INSTALL AND COORDINATE FINAL LOCATION WITH ENGINEER.



LAKESHORE AVE & E. 18TH ST RAISED TRANSIT ISLAND

SEE SHEET L-01
SEE SHEET DM-01
SCALE 1"=10'

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
BUS BOARDING
ISLAND**

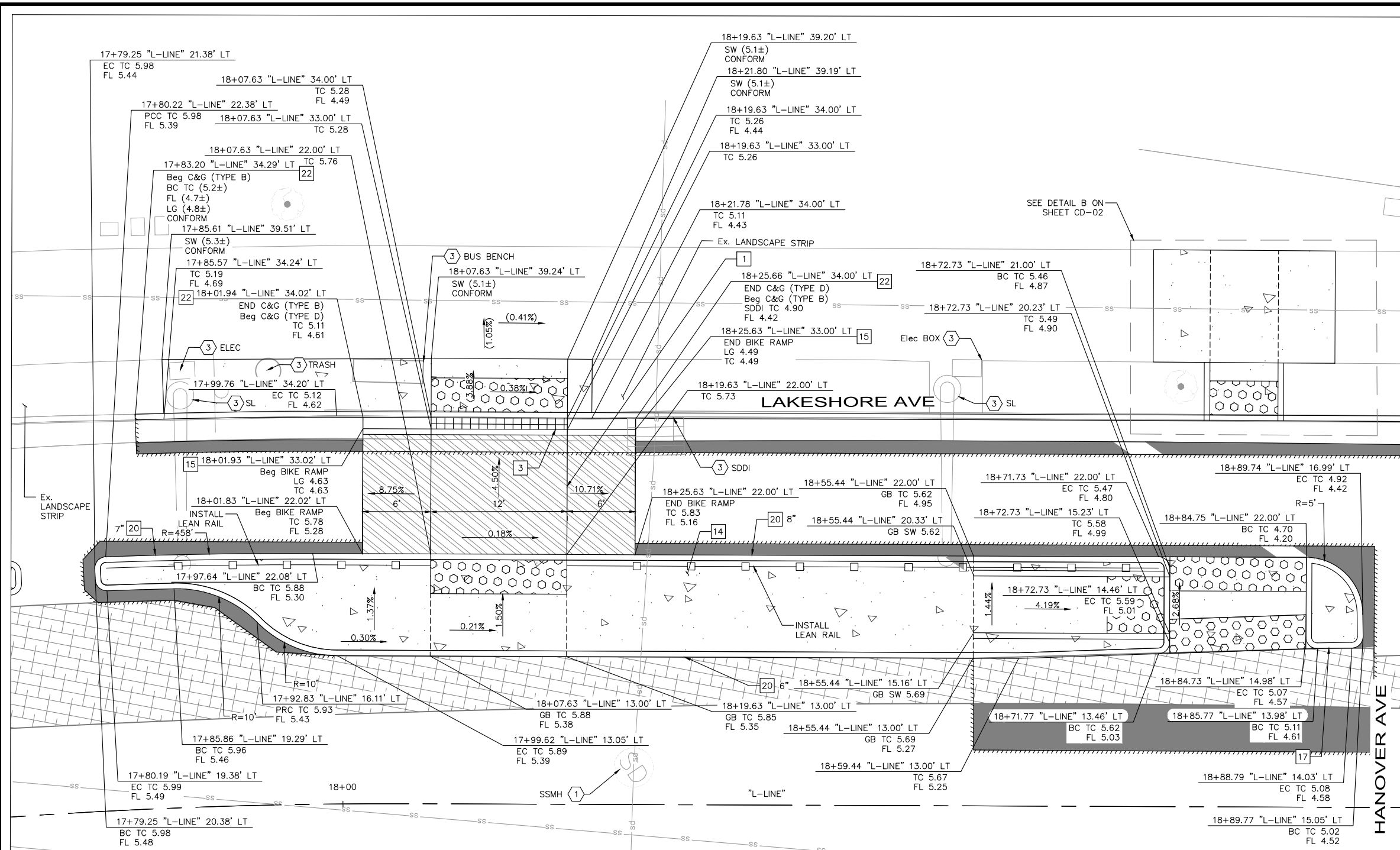
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-14
41 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\CD-15.dwg
PLOT DATE: 12-13-24
PLOTTED BY: Priya Pallivathucal



LEGEND

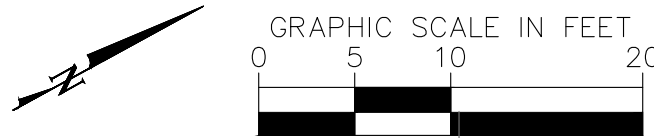
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
- TRENCH GRATE (SEE DD-01)
- Exist SLOPE (X.XX%)
- PROPOSED SLOPE (X.XX%)
- GRADE BREAK
- SAWCUT LIMITS
- LEAN RAIL

SEE GENERAL NOTES ON SHEET CD-10

- ### CONSTRUCTION NOTES
- 1 INSTALL RAISED CROSSING FLUSH WITH BUS BOARDING ISLAND. SEE CD-21 FOR ADDITIONAL DETAILS.
 - 3 INSTALL TRENCH GRATE PER DETAILS A AND B ON SHEET DD-01.
 - 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
 - 15 INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
 - 17 INSTALL CURB (TYPE A) PER CITY OF OAKLAND STD DWG S-1.
 - 20 INSTALL CURB (TYPE A1-6 MOD) PER DETAIL F ON CD-20. CURB HEIGHT AS SHOWN ON PLANS.
 - 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

- ① ADJUST TO GRADE.
- ③ PROTECT IN PLACE.



LAKESHORE AVE & HANOVER AVE RAISED TRANSIT ISLAND

SEE SHEET L-02
SEE SHEET DM-02
SCALE 1"=10'

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
BUS BOARDING
ISLAND**

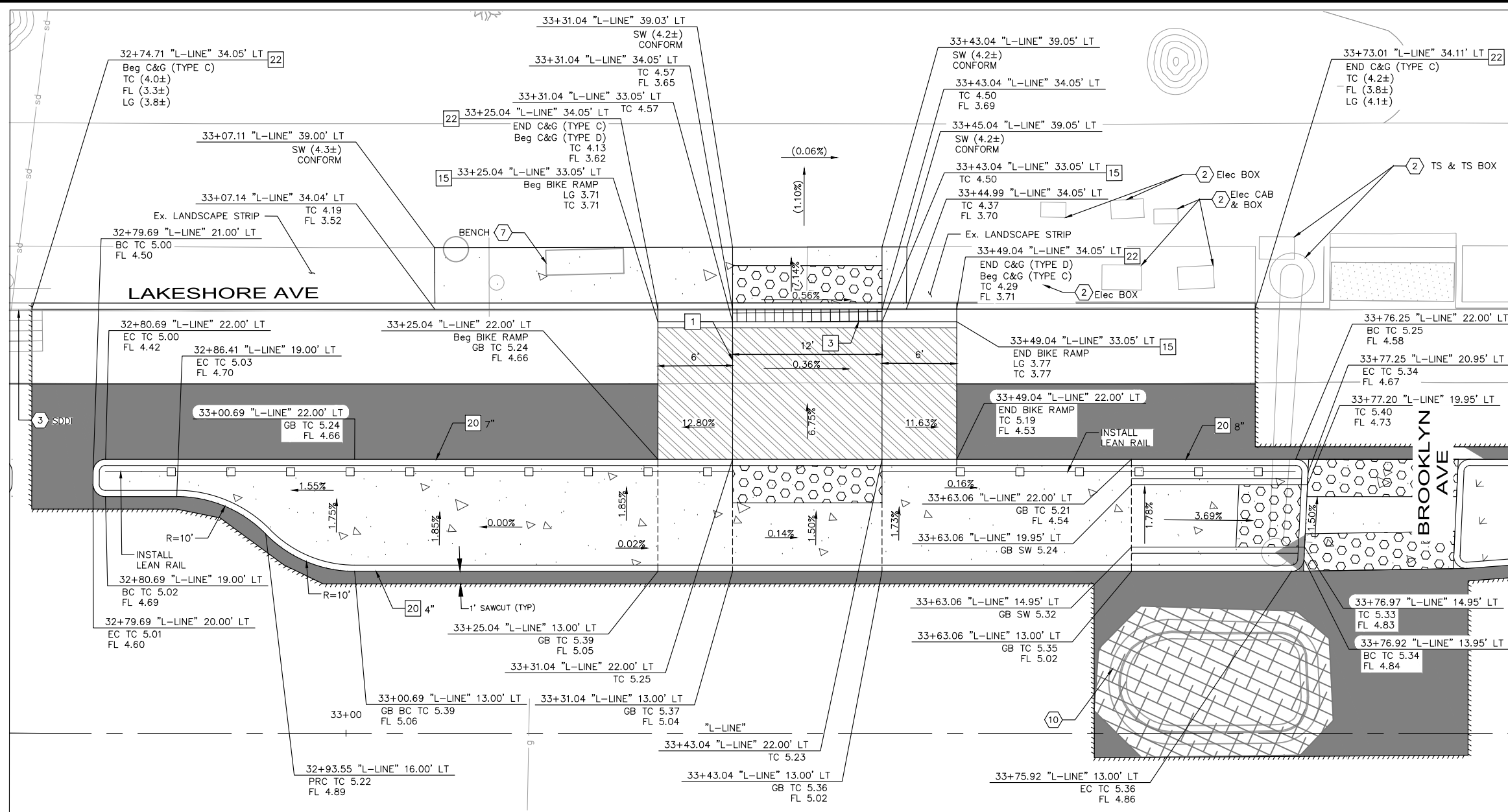
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-15
42 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\1007299\1007299-04 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\CD-15.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



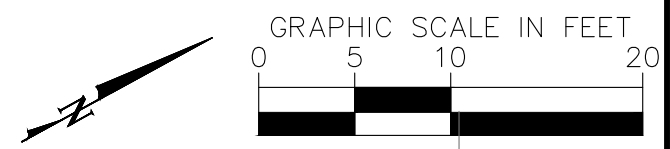
- ### LEGEND
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 -

SEE GENERAL NOTES ON SHEET CD-01

- ### CONSTRUCTION NOTES
- 1 INSTALL RAISED CROSSING FLUSH WITH BUS BOARDING ISLAND. SEE CD-21 FOR ADDITIONAL DETAILS.
 - 2 INSTALL BICYCLE RAMP. MAX 16% SLOPE PER CITY OF OAKLAND STD DWG BP-6.
 - 3 INSTALL TRENCH GRATE PER DETAILS A AND B ON SHEET DD-01.
 - 15 INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
 - 20 INSTALL CURB (TYPE A1-6 MOD) PER DETAIL F ON CD-20. CURB HEIGHT AS SHOWN ON PLANS.
 - 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

- ### UTILITY AND DEMOLITION NOTES
- 2 EXISTING TO REMAIN.
 - 3 PROTECT IN PLACE.
 - 7 REMOVE AND RELOCATE. CONTRACTOR TO INSTALL AND COORDINATE FINAL LOCATION WITH ENGINEER.
 - 10 REMOVE. SEE DEMOLITION PLANS FOR DETAILS.

**LAKESHORE AVE & BROOKLYN AVE
RAISED TRANSIT ISLAND**
SEE SHEET L-05
SEE SHEET DM-05
SCALE 1"=10'



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION
DETAILS
BUS BOARDING
ISLAND**

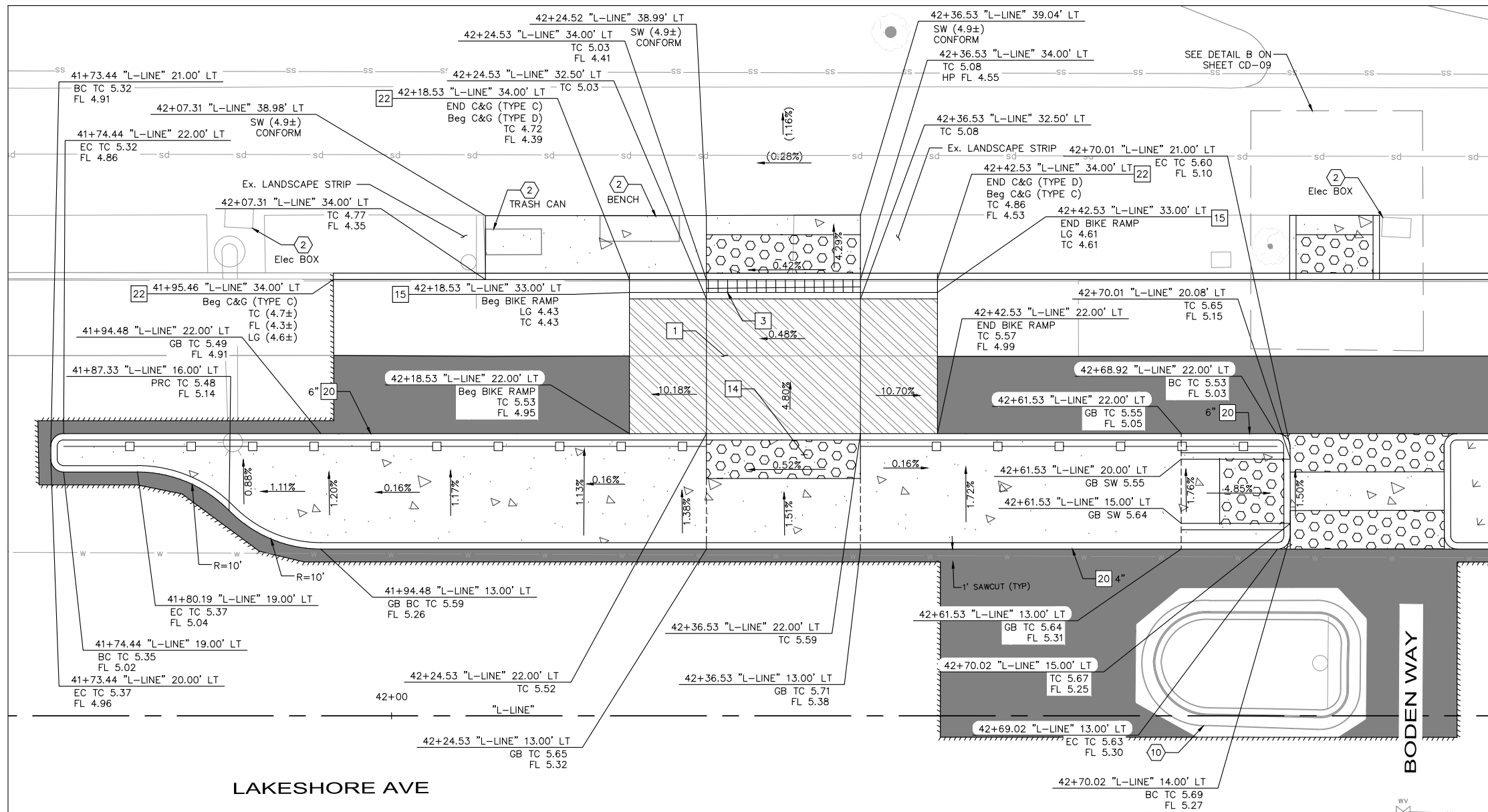
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-16
43 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\CD-16.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



- ### LEGEND
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 - BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
 - HMA PAVEMENT (12" DEPTH)
 - PROPOSED LANDSCAPE AREA
 - RAISED ISLAND WITH DOWEL CURB
 - RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
 - TRENCH GRATE (SEE DD-01)
 - Exist SLOPE
 - PROPOSED SLOPE
 - GRADE BREAK
 - SAWCUT LIMITS
 - LEAN RAIL

SEE GENERAL NOTES ON SHEET CD-01

CONSTRUCTION NOTES

- 1 INSTALL RAISED CROSSING FLUSH WITH BUS BOARDING ISLAND. SEE CD-21 FOR ADDITIONAL DETAILS.
- 2 INSTALL BICYCLE RAMP. MAX 16% SLOPE PER CITY OF OAKLAND STD DWG BP-6.
- 3 INSTALL TRENCH GRATE PER DETAILS A AND B ON SHEET DD-01.
- 14 INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 15 INSTALL RETAINING CURB PER DETAIL E ON SHEET CD-20.
- 20 INSTALL CURB (TYPE A1-6 MOD) PER DETAIL F ON CD-20. CURB HEIGHT AS SHOWN ON PLANS.
- 22 INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

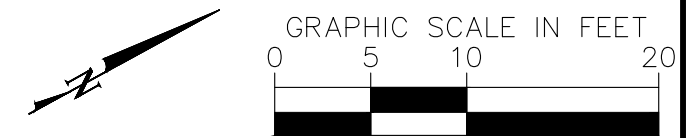
UTILITY AND DEMOLITION NOTES

- EXISTING TO REMAIN.
- REMOVE. SEE DEMOLITION PLANS FOR DETAILS.

LAKESHORE AVE

LAKESHORE AVE & BODEN WAY RAISED TRANSIT ISLAND

SEE SHEET L-06
SEE SHEET DM-06
SCALE 1"=10'



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

CONSTRUCTION
DETAILS
BUS BOARDING
ISLAND

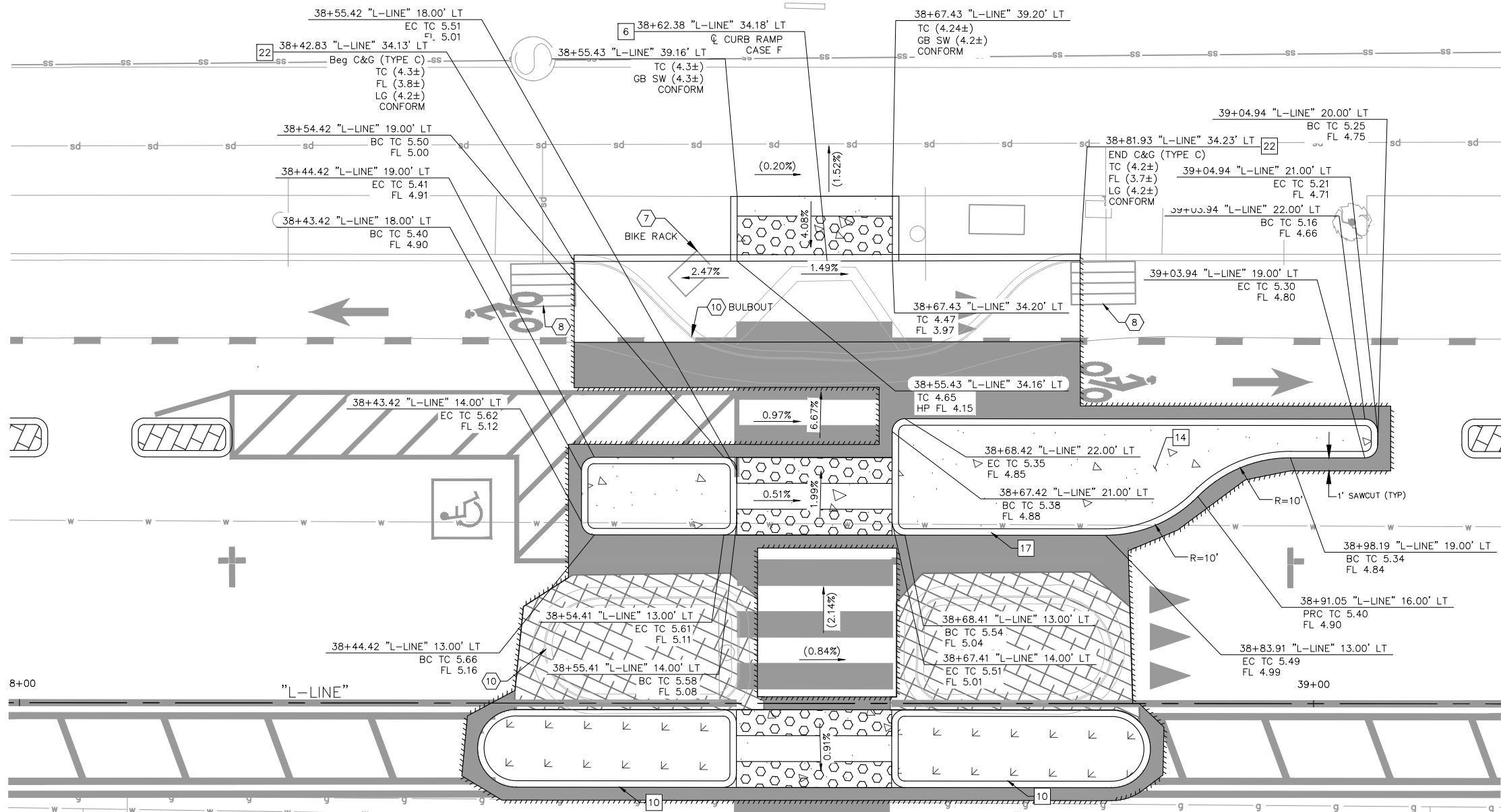
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
CD-17
44 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_BDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\CD-15.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



**LAKESHORE AVE
CURB RAMP DETAILS**
SEE SHEET L-06
SCALE 1"=10'

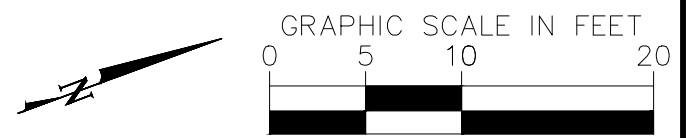
- LEGEND**
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
 - DETECTABLE WARNING SURFACE
 - BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
 - HMA PAVEMENT (12" DEPTH)
 - PROPOSED LANDSCAPE AREA
 - RAISED ISLAND WITH DOWEL CURB
 - RAISED CROSSWALK HAM (TYPE A) OVERLAY (6" MAX)
 - TRENCH GRATE (SEE DD-01)
 - GRADE BREAK
 - SAWCUT LIMITS
 - EXIST SLOPE
 - PROPOSED SLOPE
 - ADA LANDING ZONE (FOR REFERENCE ONLY)

**SEE GENERAL NOTES ON CD-01
CONSTRUCTION NOTES**

- 6** INSTALL CURB RAMP PER CALTRANS 2023 STD PLAN A88A. CASE NOTED PER PLANS.
- 10** INSTALL PEDESTRIAN REFUGE ISLAND PER DETAIL D ON SHEET CD-19. CONCRETE AND AGGREGATE BASE THICKNESS PER CITY OF OAKLAND SIDEWALK STD DWG S-1.
- 14** INSTALL SIDEWALK PER CITY OF OAKLAND STD DWG S-1.
- 17** INSTALL CURB (TYPE A) PER CITY OF OAKLAND STD DWG S-1.
- 22** INSTALL CURB AND GUTTER (C&G) PER CITY OF OAKLAND STD DWG S-1. C&G TYPE AS SPECIFIED ON PLANS.

UTILITY AND DEMOLITION NOTES

- 7** REMOVE AND RELOCATE. CONTRACTOR TO INSTALL AND COORDINATE FINAL LOCATION WITH ENGINEER.
- 8** REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.
- 10** REMOVE. SEE DEMOLITION PLANS FOR DETAILS.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

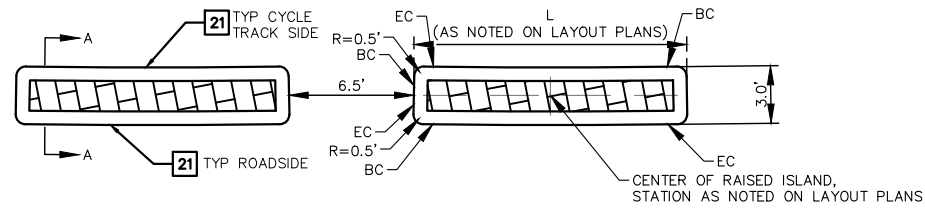
No.	DATE	BY	REFERENCE

CONSTRUCTION DETAILS

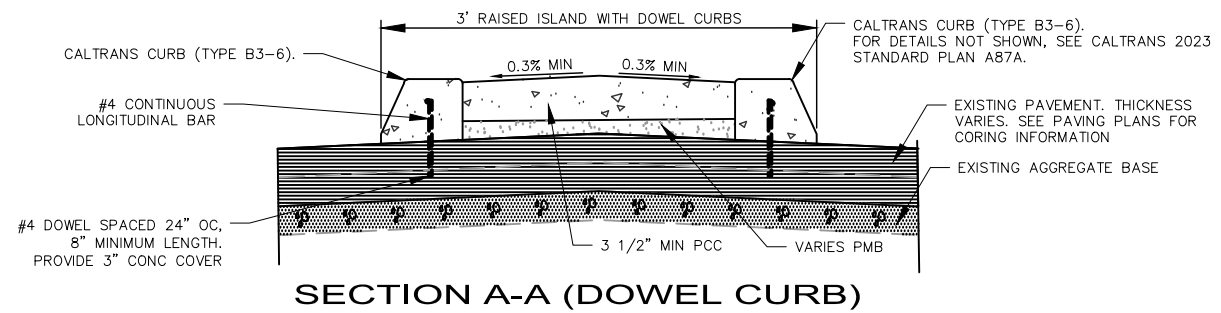
PROJECT NO. **1007299**

SCALE: AS SHOWN SHEET NO. CD-18
DATE: 12/11/2024 45 OF 83

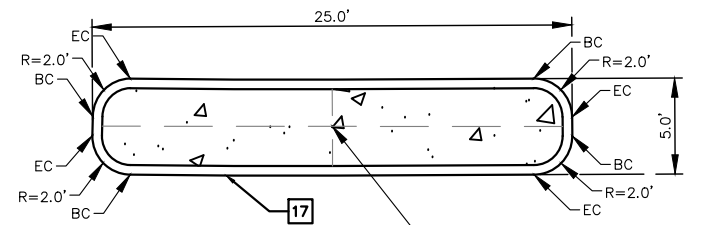
DRAWING NAME: K:\OAK_CD\00708204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-18.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



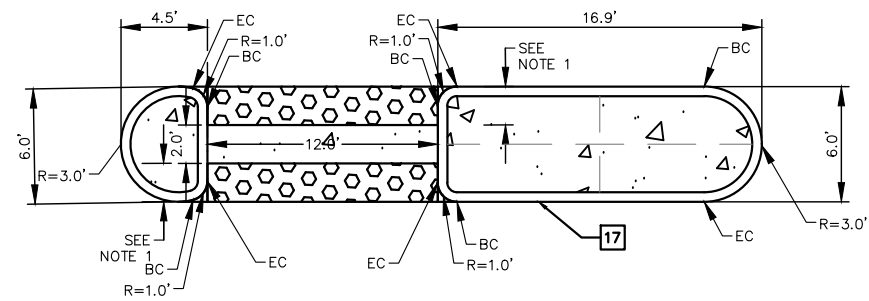
RAISED ISLAND WITH DOWEL CURB (3-FT WIDE) (A)



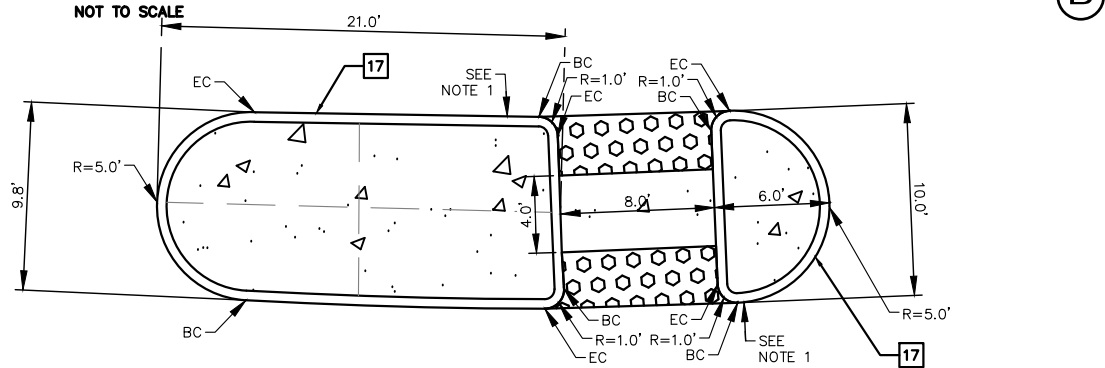
SECTION A-A (DOWEL CURB)



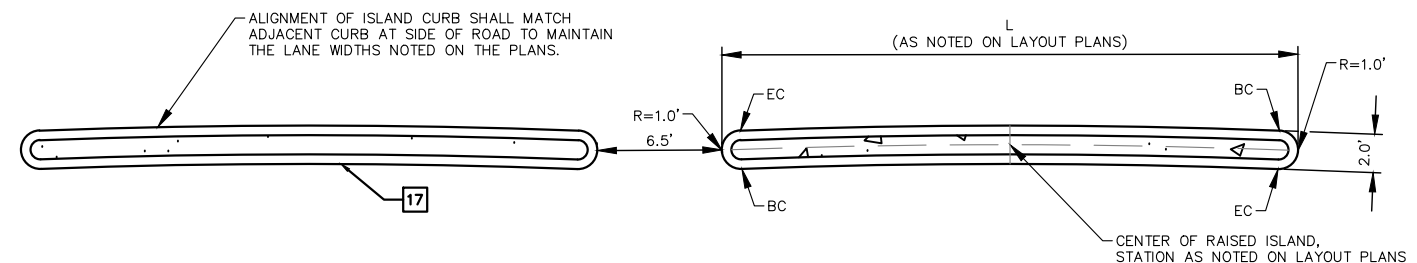
RAISED ISLAND DETAIL (5-FT WIDE) (B)



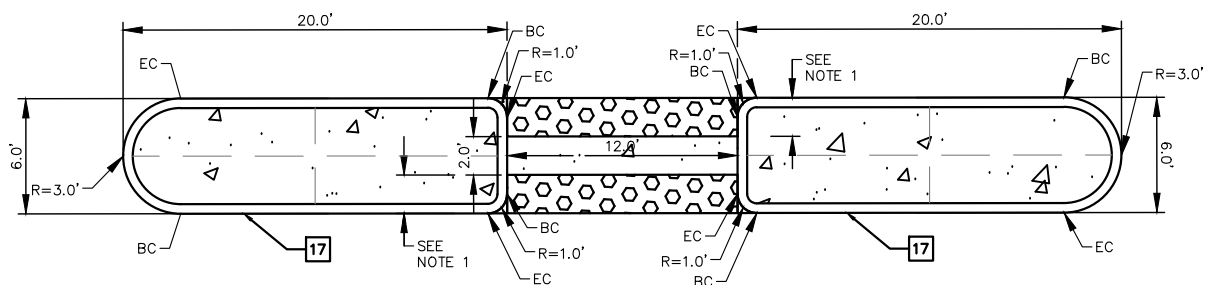
PEDESTRIAN REFUGE ISLAND DETAIL (E)



PEDESTRIAN REFUGE ISLAND DETAIL (C)



RAISED ISLAND DETAIL (2-FT WIDE) (F)



PEDESTRIAN REFUGE ISLAND DETAIL (D)

GENERAL NOTES:

- WHERE AN ISLAND PASSAGEWAY LENGTH IS GREATER THAN OR EQUAL TO 6'-0", BUT LESS THAN 8'-0", EACH DETECTABLE WARNING SURFACE SHALL EXTEND THE FULL WIDTH AND 2'-0" DEPTH OF THE PASSAGEWAY LENGTH. WHERE AN ISLAND PASSAGEWAY LENGTH IS GREATER THAN OR EQUAL TO 8'-0", EACH DETECTABLE WARNING SURFACE SHALL EXTEND THE FULL WIDTH AND 3'-0" DEPTH OF THE PASSAGEWAY LENGTH. DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL WIDTH OF THE ISLAND PASSAGEWAY EXCEPT A MAXIMUM GAP OF 1 INCH IS ALLOWED ON EACH SIDE OF THE PASSAGEWAY.

CONSTRUCTION NOTES

- 17 INSTALL CURB (TYPE A) PER CITY OF OAKLAND STD DWG S-1.
- 21 INSTALL CURB (TYPE B3-6) PER CALTRANS 2023 STANDARD PLAN A87A.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION DETAILS
RAISED ISLANDS**

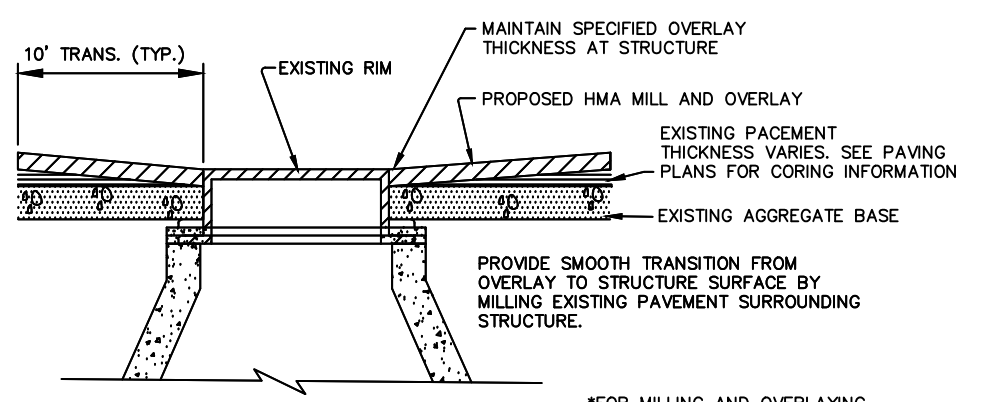
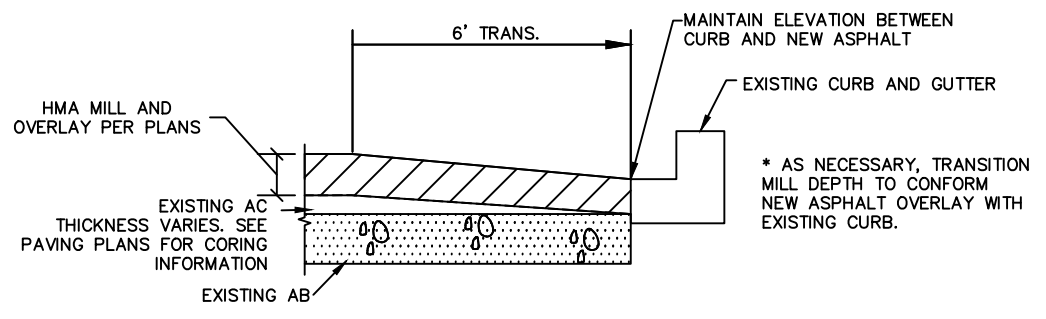
PROJECT NO.
1007299

SCALE:
AS SHOWN

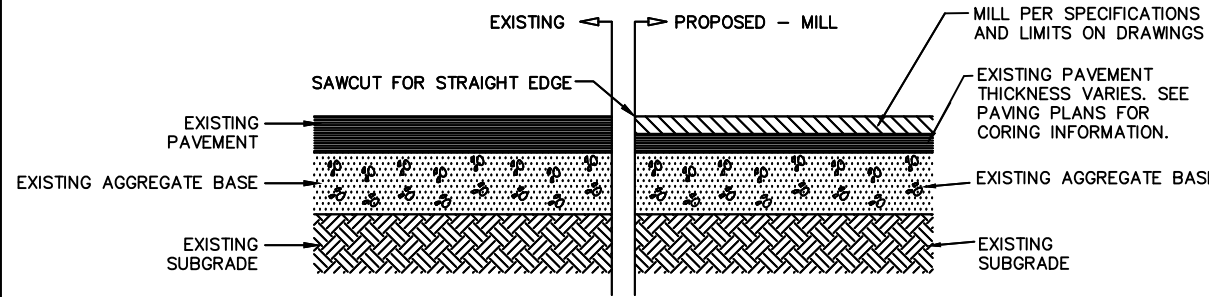
SHEET NO.
CD-19
46 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RD\W\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\CD-19.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

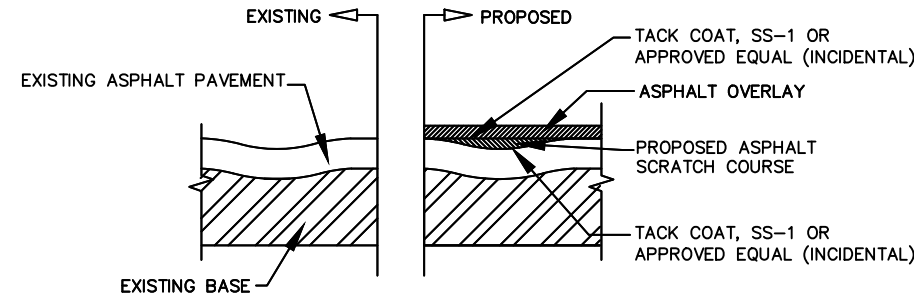


NOTE: PLANS DO NOT NECESSARILY INCLUDE ALL STRUCTURE LOCATIONS. CONTRACTOR SHALL VERIFY THE NUMBER AND LOCATION OF ALL STRUCTURES.



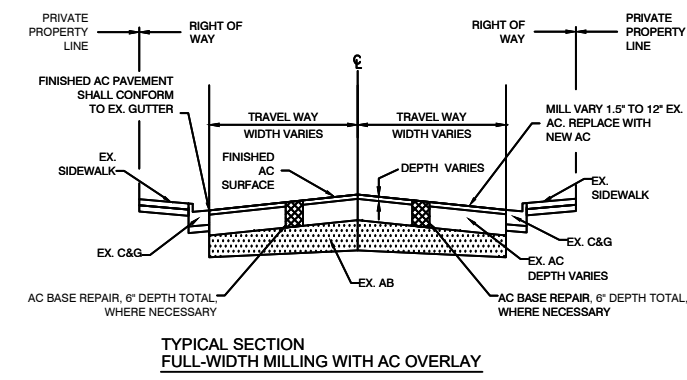
NOTE:
 1. AFTER MILL, ENTIRE AREA SHALL BE SWEEPED & CLEANED OF DEBRIS
 2. IT MAY BE REQUIRED TO RAISE MILL CUT WHERE THINNER PAVEMENTS ARE ENCOUNTERED. REMOVE AGGREGATE BASE AS NECESSARY TO PROVIDE SPECIFIED ASPHALT OVERLAY THICKNESS.

MILL ASPHALT
NOT TO SCALE

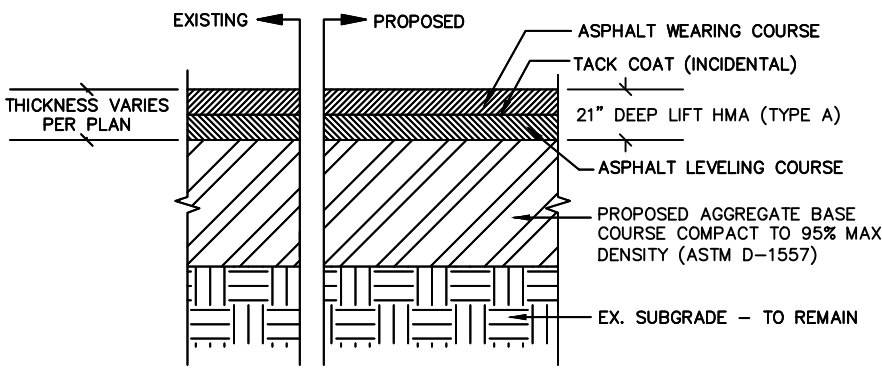


NOTES:
 1. INSTALL SCRATCH COURSE TO FILL LOW POINTS, SHRINKAGE CRACKS AND SMALL TO MEDIUM POTHOLES. THIS COURSE IS TO PROVIDE A SMOOTH AND LEVEL SURFACE AS CALLED FOR ON THE PLANS.
 2. AFTER SURFACES HAVE BEEN PREPARED TO THE SATISFACTION OF THE ENGINEER AND PRIOR TO OVERLAY, ENTIRE AREA SHALL BE SWEEPED AND CLEANED OF DEBRIS. PRIOR TO OVERLAY APPLY TACK COAT AT THE RATE OF 0.20 GAL/SYD. TRAFFIC WILL NOT BE PERMITTED AFTER TACK COAT INSTALLATION.
 3. MIN. LIFT THICKNESS SHALL BE THREE TIMES THE MAX. AGGREGATE SIZE.
 4. ISOLATED SCRATCH COURSE TO BE USED WHEN THE ENGINEER OR OWNER DETERMINE THAT LOW SPOTS EXIST THROUGHOUT PAVEMENT SURFACE AREAS AND NEED TO BE LEVELED PRIOR TO ASPHALT OVERLAY.

ASPHALT OVERLAY
NOT TO SCALE

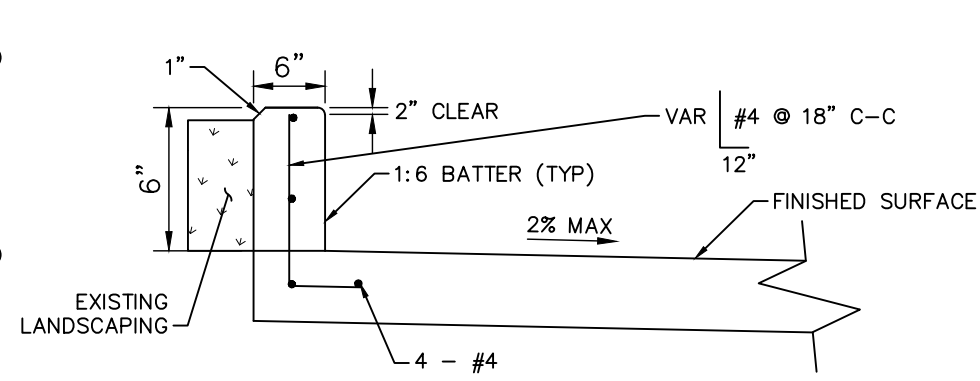


MILL AND OVERLAY
NOT TO SCALE

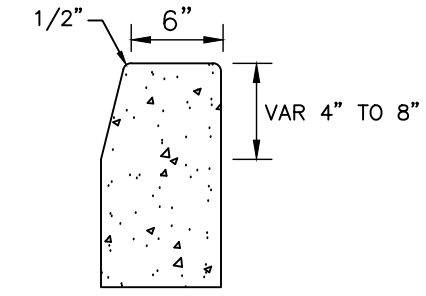


NOTE:
 MIN. LIFT THICKNESS SHALL BE AT LEAST THREE TIMES THE MAX. AGGREGATE SIZE. AS NECESSARY, REMOVE AGGREGATE BASE TO PROVIDE SPECIFIED ASPHALT THICKNESS.

AC BASE REPAIR
NOT TO SCALE



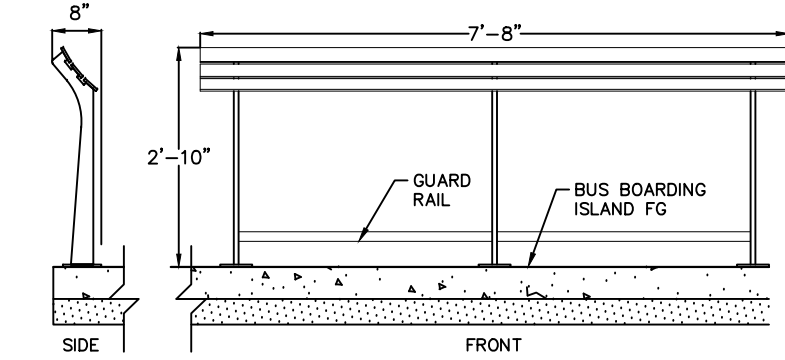
6" RETAINING CURB
NOT TO SCALE



NOTES:
 1. FOR DETAILS NOT SHOWN, SEE CALTRANS Std PLAN A87A.
 2. FOR CURB HEIGHTS, SEE CONSTRUCTION DETAIL SHEETS.

CURB (TYPE A1-6 MOD)
NOT TO SCALE

LEANING RAIL:
 MFR: KEYSTONE RIDGE DESIGNS, OR APPROVED EQUAL
 MODEL: CARSON CSL-8, OR APPROVED EQUAL
 FINISH/COLOR: CHROMITE



NOTE: INSTALL PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS

LEANING RAIL AT BUS ISLAND
NOT TO SCALE

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley-Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

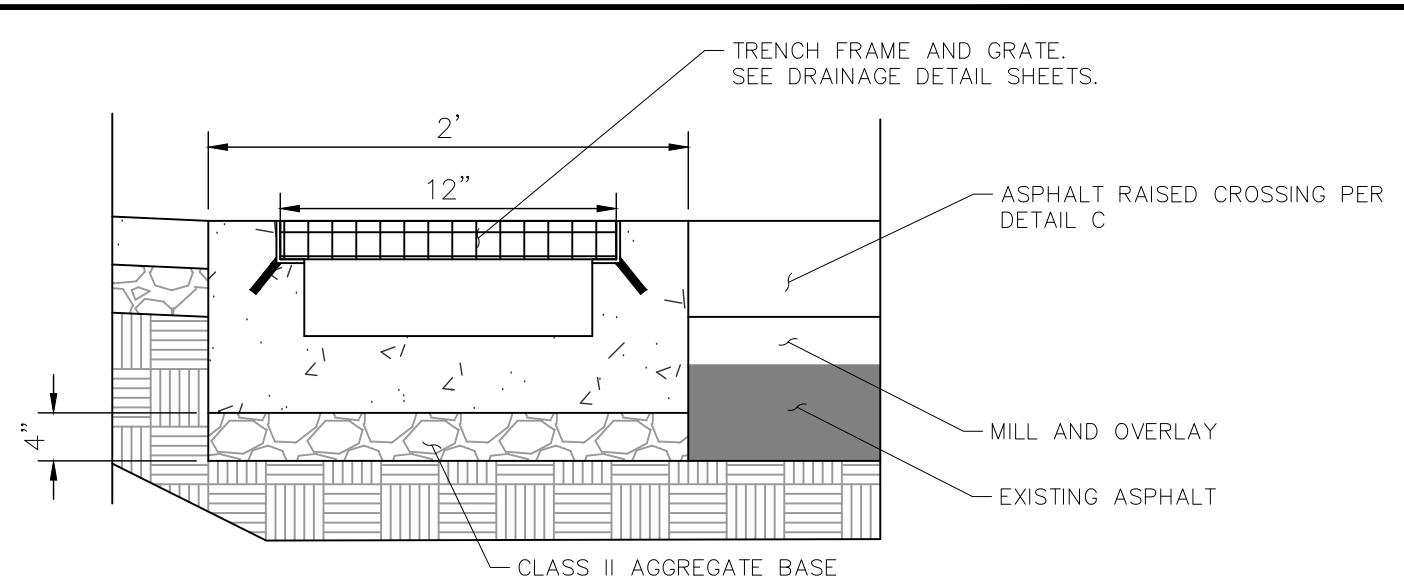
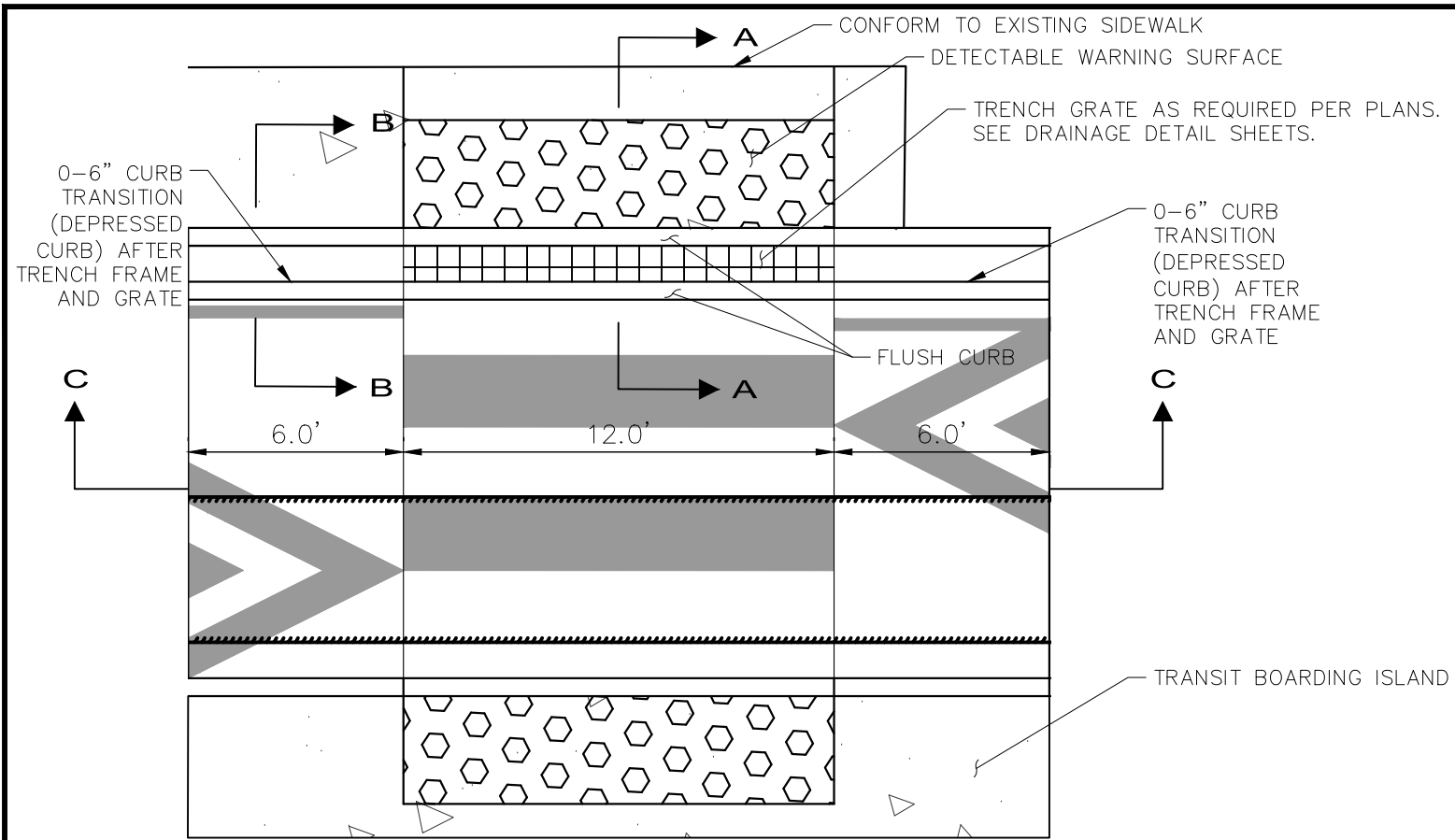
**CONSTRUCTION DETAILS
 MISCELLANEOUS**

PROJECT NO.
1007299

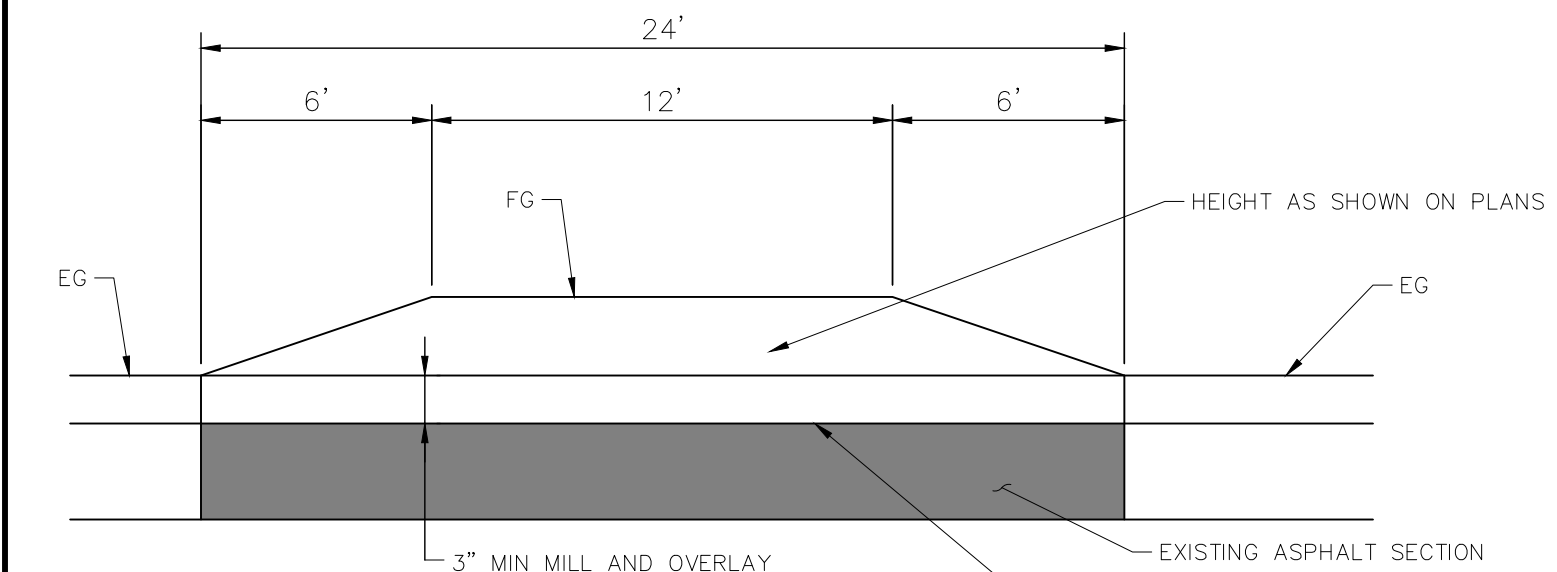
SCALE:
AS SHOWN

SHEET NO.
CD-20
47 OF 83

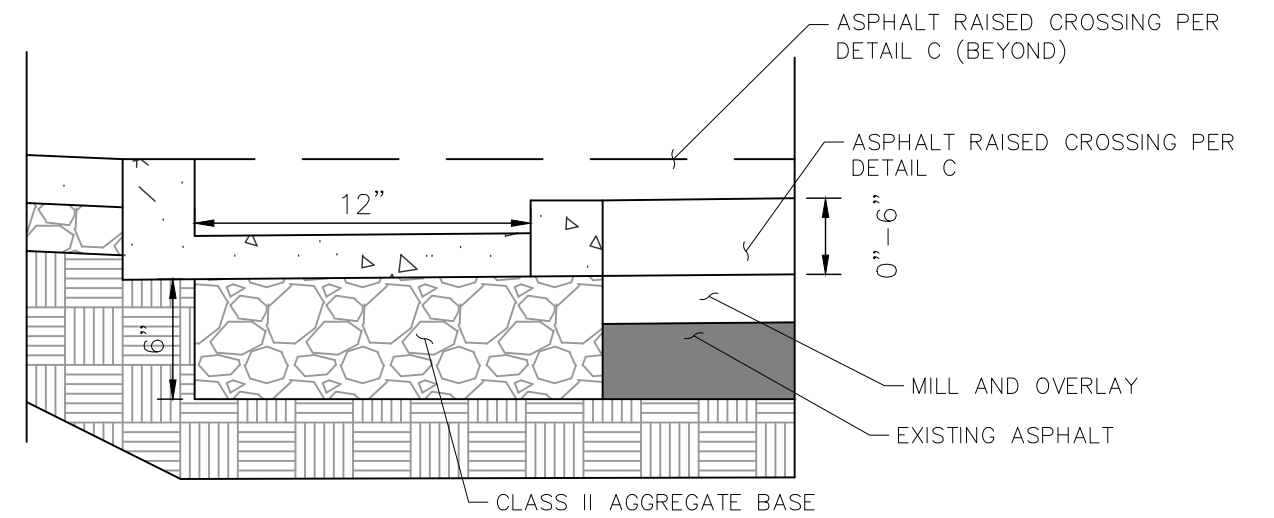
DATE: 12/11/2024



A-A RAISED CROSSWALK WITH TRENCH GRATE CROSS SECTION (A)
NOT TO SCALE



C-C RAISED CROSSING (C)
NOT TO SCALE



B-B RAISED CROSSWALK SECTION (B)
NOT TO SCALE

CLEAN EXISTING ASPHALT PAVEMENT AND APPLY TACK COAT LAYER BETWEEN NEW ASPHALT RAISED CROSSING AND EXISTING ASPHALT

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**CONSTRUCTION DETAILS
RAISED CROSSING**

PROJECT NO.
1007299

SCALE:
AS SHOWN

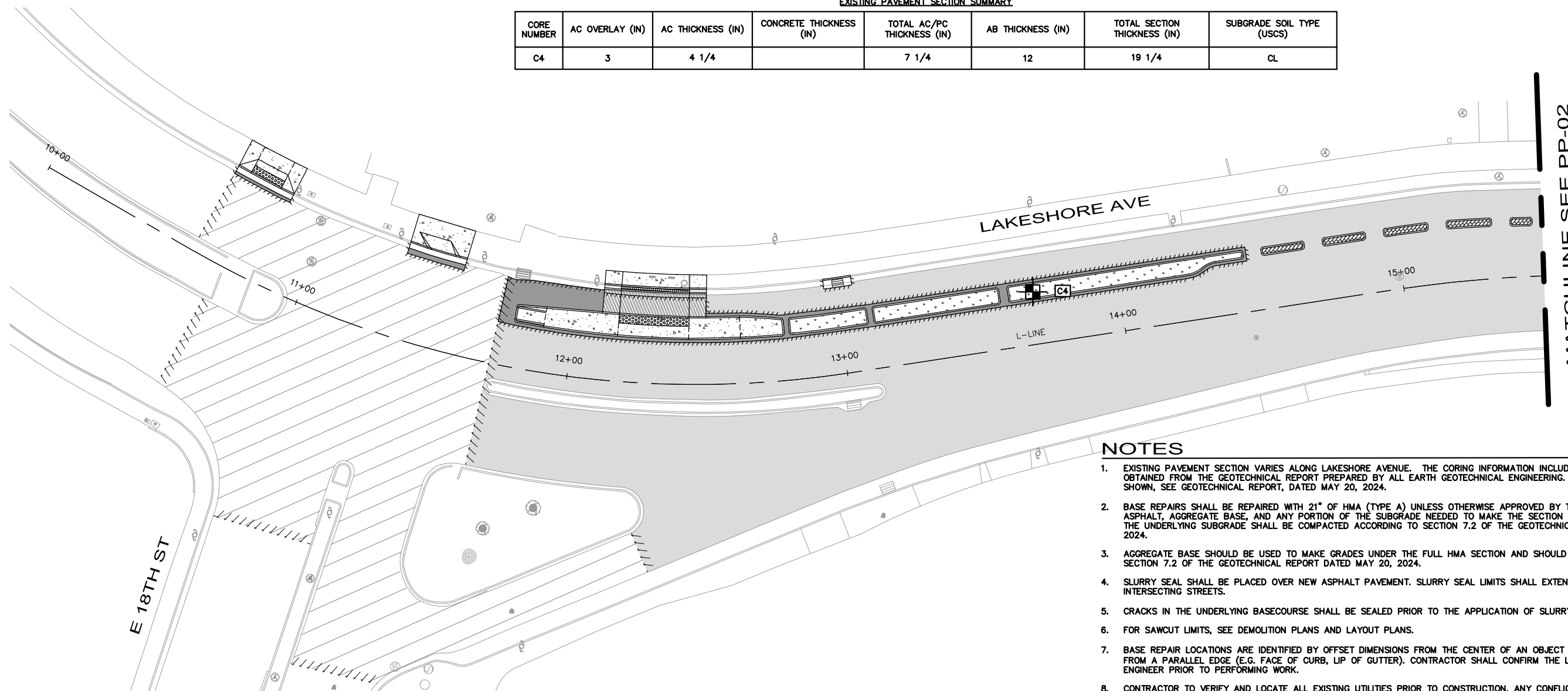
DATE: 12/11/2024

SHEET NO.
CD-21
48 OF 83

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\CD-14.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal

EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C4	3	4 1/4		7 1/4	12	19 1/4	CL

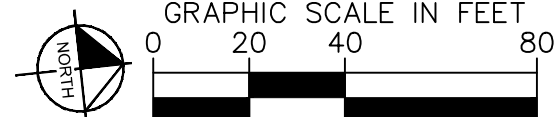


NOTES

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS. OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

	SLURRY SEAL		3" HMA (TYPE A) INLAY		ELECTRICAL TRENCHING, SEE ELECTRICAL SHEET
	TRENCH GRATE (SEE DD-01)		HMA PAVEMENT (12" DEPTH)		PAVEMENT TREATMENT LIMITS
	BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.		PROPOSED LANDSCAPE AREA		SAWCUT LIMITS
	PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND		RAISED ISLAND WITH DOWEL CURB		CORING LOCATION
	DETECTABLE WARNING SURFACE		RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)		



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

PAVING PLANS

PROJECT NO.
1007299

SCALE:
AS SHOWN

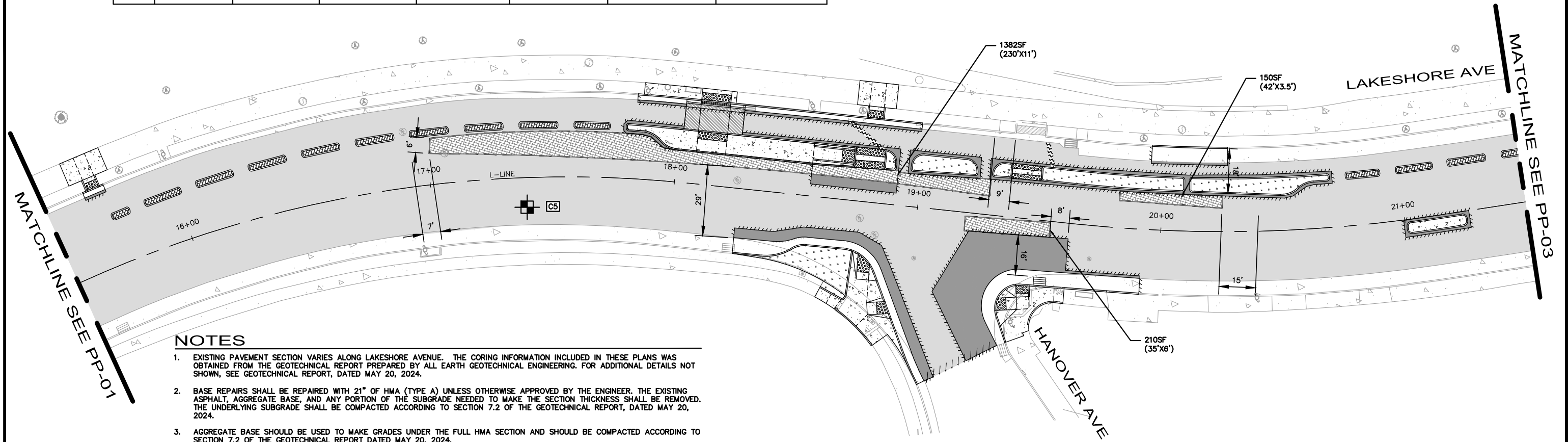
SHEET NO.
PP-01
49 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Tracks\04_CADD\Plan Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C5	2 1/2	2 1/2		5	11	16	CL



NOTES

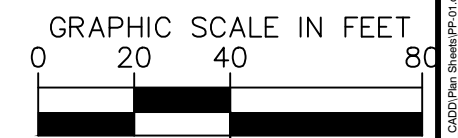
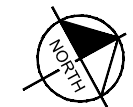
- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS. OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

	SLURRY SEAL		3" HMA (TYPE A) INLAY		ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
	TRENCH GRATE (SEE DD-01)		HMA PAVEMENT (12" DEPTH)		PAVEMENT TREATMENT LIMITS
	BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.		PROPOSED LANDSCAPE AREA		SAWCUT LIMITS
	PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND		RAISED ISLAND WITH DOWEL CURB		CORING LOCATION
	DETECTABLE WARNING SURFACE		RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)		

ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS, IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 © 2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

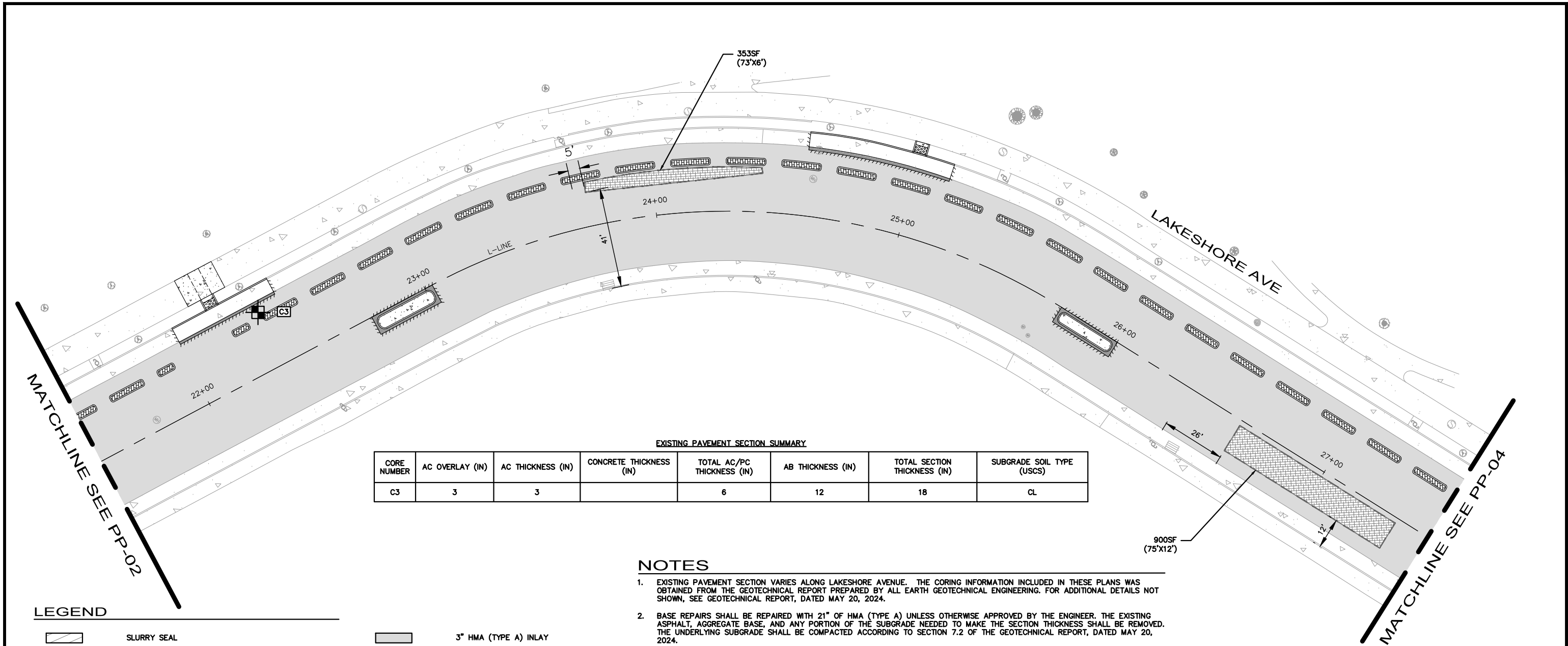
No.	DATE	BY	REFERENCE

PROJECT NO.
1007299

PAVING PLANS

SCALE: AS SHOWN SHEET NO. PP-02
DATE: 12/11/2024 50 OF 83

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal



EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C3	3	3		6	12	18	CL

NOTES

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

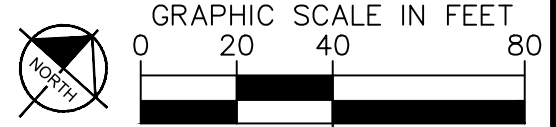
- SLURRY SEAL
- TRENCH GRATE (SEE DD-01)
- BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.
- PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND
- DETECTABLE WARNING SURFACE
- ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
- PAVEMENT TREATMENT LIMITS
- 3" HMA (TYPE A) INLAY
- HMA PAVEMENT (12" DEPTH)
- PROPOSED LANDSCAPE AREA
- RAISED ISLAND WITH DOWEL CURB
- RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)
- SAWCUT LIMITS
- CORING LOCATION

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

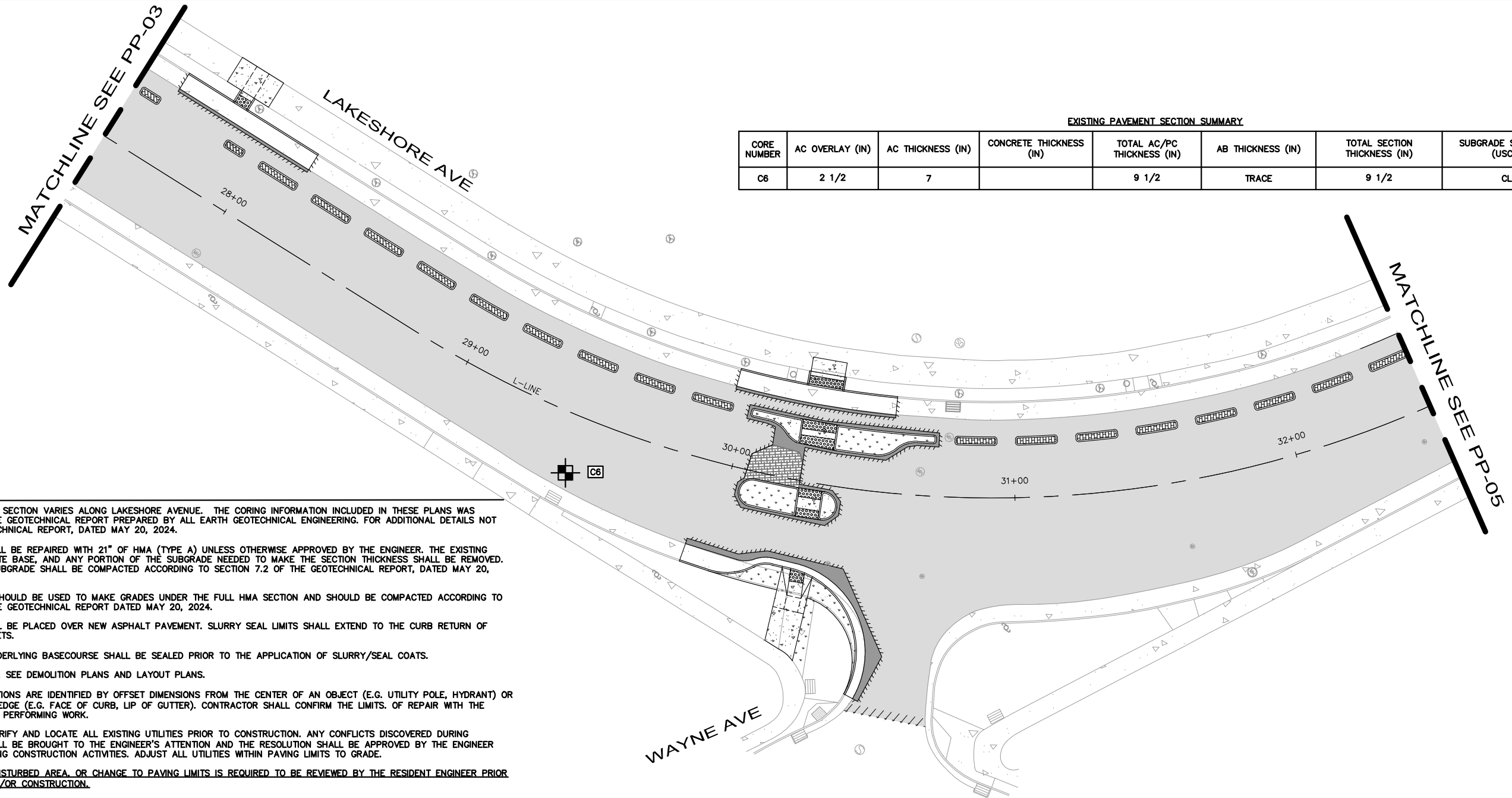
PAVING PLANS

SCALE: AS SHOWN
DATE: 12/11/2024

PROJECT NO.
1007299

SHEET NO.
PP-03
51 OF 83

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Trac\04_CADD\Plan_Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C6	2 1/2	7		9 1/2	TRACE	9 1/2	CL

NOTES

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS. OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

	SLURRY SEAL		3" HMA (TYPE A) INLAY		ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
	TRENCH GRATE (SEE DD-01)		HMA PAVEMENT (12" DEPTH)		PAVEMENT TREATMENT LIMITS
	BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.		PROPOSED LANDSCAPE AREA		SAWCUT LIMITS
	PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND		RAISED ISLAND WITH DOWEL CURB		CORING LOCATION
	DETECTABLE WARNING SURFACE		RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)		

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

PAVING PLANS

PROJECT NO.
1007299

SCALE:
AS SHOWN

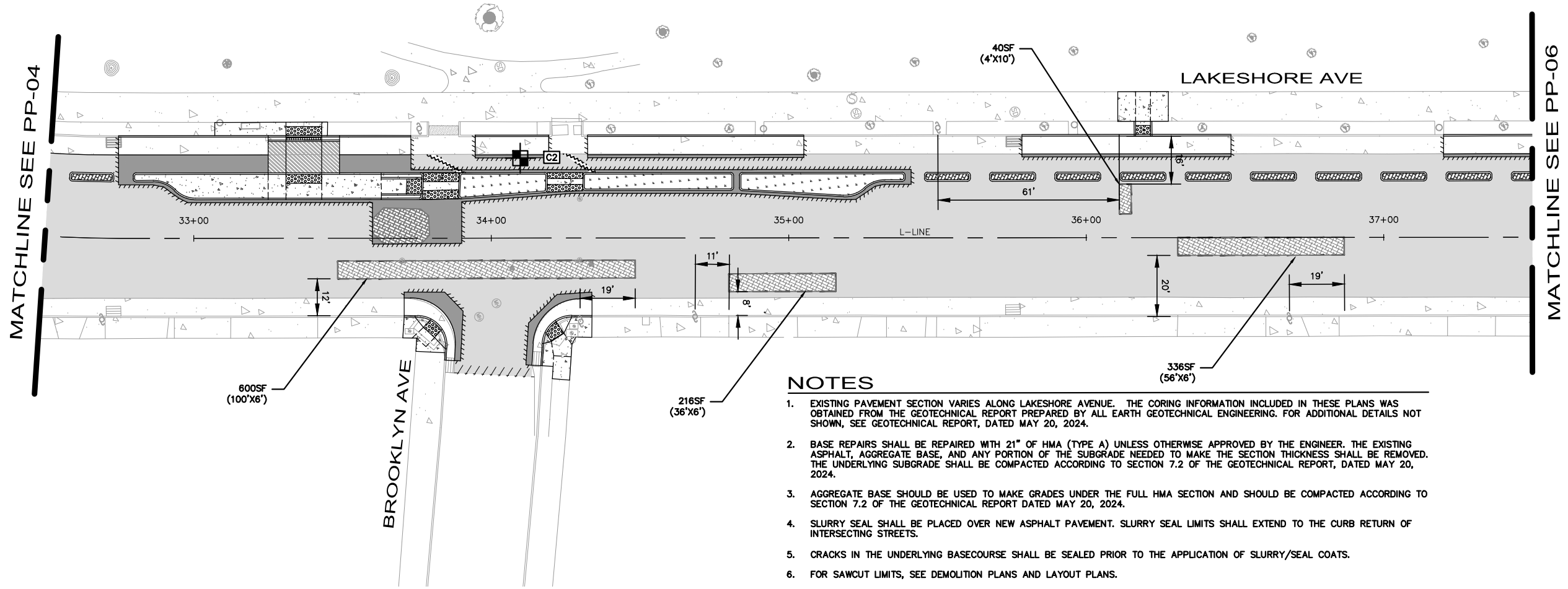
SHEET NO.
PP-04
52 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal

EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C2	4 1/2	2		6 1/2	4 1/2	11	CL



NOTES

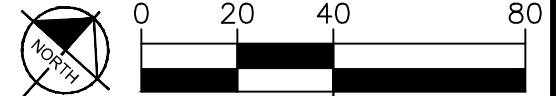
- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS. OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

	SLURRY SEAL		3" HMA (TYPE A) INLAY		ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
	TRENCH GRATE (SEE DD-01)		HMA PAVEMENT (12" DEPTH)		PAVEMENT TREATMENT LIMITS
	BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.		PROPOSED LANDSCAPE AREA		SAWCUT LIMITS
	PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND		RAISED ISLAND WITH DOWEL CURB		CORING LOCATION
	DETECTABLE WARNING SURFACE		RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)		

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

PROJECT NO.
1007299

PAVING PLANS

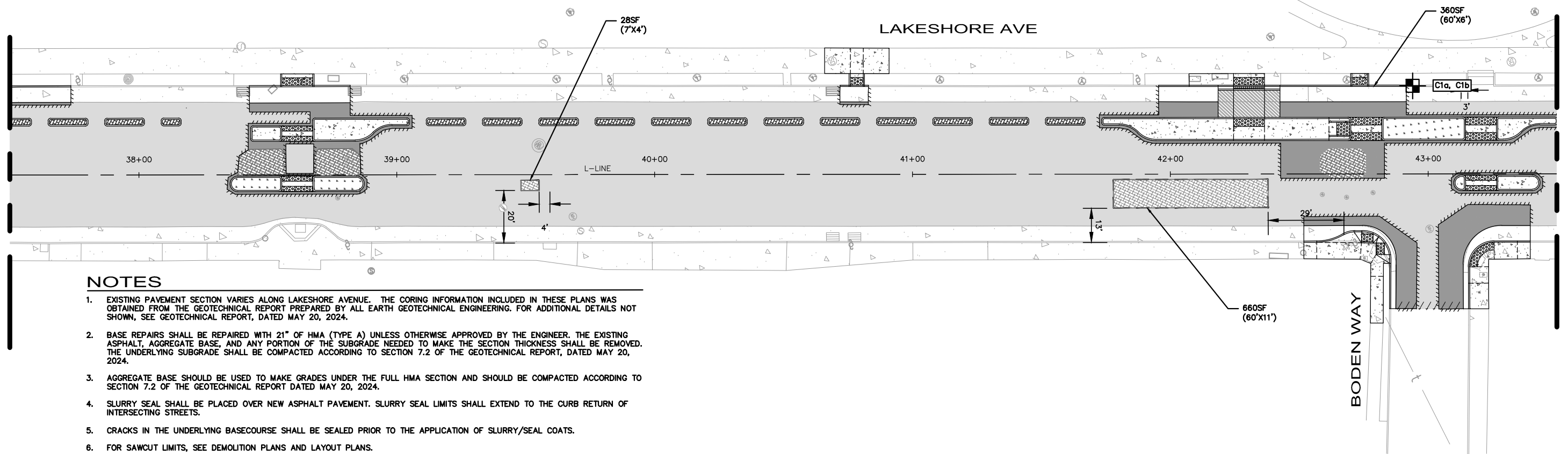
SCALE: AS SHOWN SHEET NO. PP-05
DATE: 12/11/2024 53 OF 83

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C1a			6 1/4	6 1/4	0	6 1/4	CL
C1b	2	3		5	6	11	CL

MATCHLINE SEE PP-05



MATCHLINE SEE PP-07

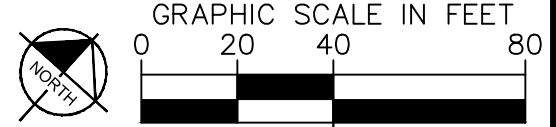
NOTES

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

	SLURRY SEAL		3" HMA (TYPE A) INLAY		ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
	TRENCH GRATE (SEE DD-01)		HMA PAVEMENT (12" DEPTH)		PAVEMENT TREATMENT LIMITS
	BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.		PROPOSED LANDSCAPE AREA		SAWCUT LIMITS
	PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND		RAISED ISLAND WITH DOWEL CURB		CORING LOCATION
	DETECTABLE WARNING SURFACE		RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)		

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

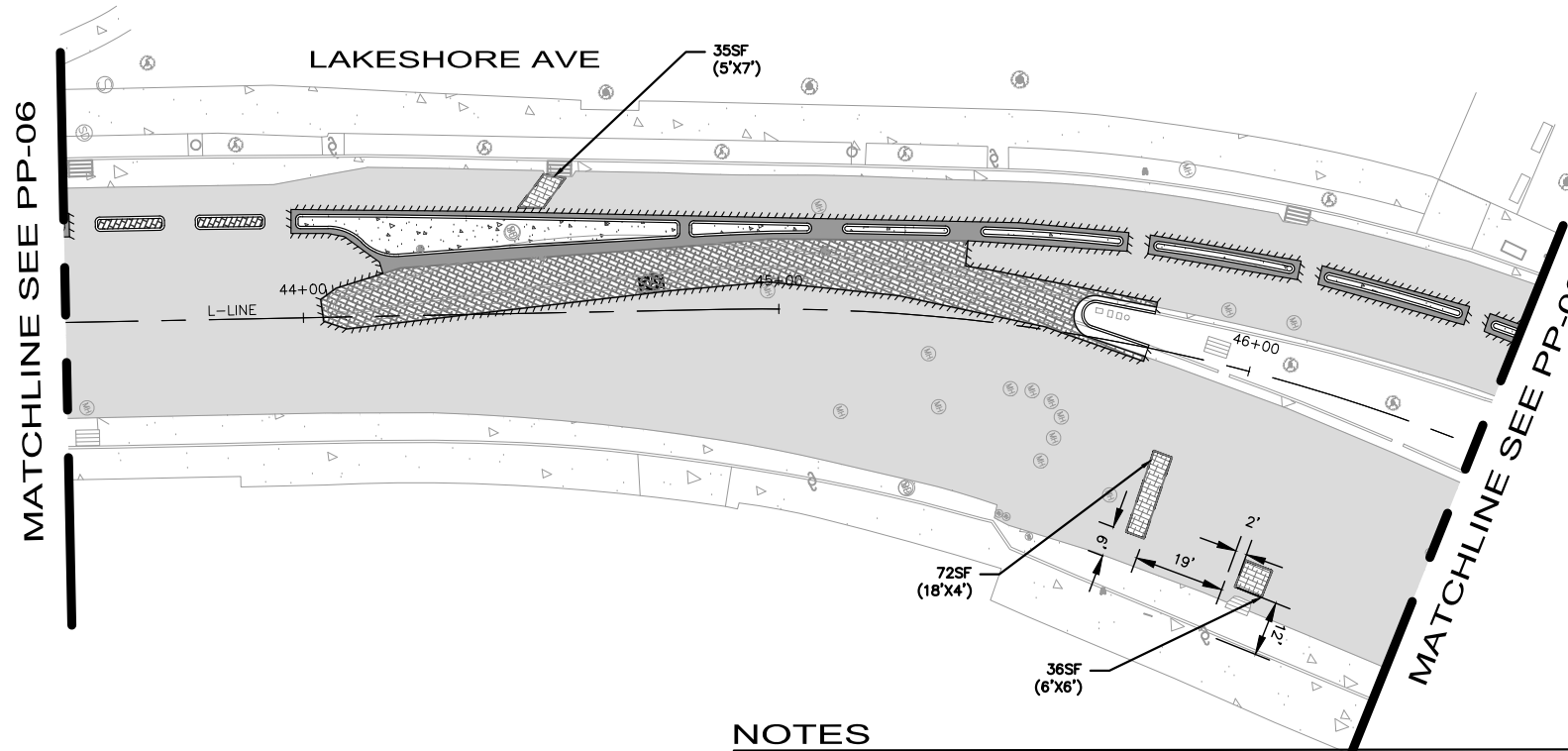
PAVING PLANS

PROJECT NO.
1007299

SCALE: AS SHOWN
DATE: 12/11/2024

SHEET NO.
PP-06
54 OF 83

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



LEGEND

- | | | | |
|--|--|--|--|
| | SLURRY SEAL | | 3" HMA (TYPE A) INLAY |
| | TRENCH GRATE (SEE DD-01) | | HMA PAVEMENT (12" DEPTH) |
| | BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19. | | PROPOSED LANDSCAPE AREA |
| | PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND | | RAISED ISLAND WITH DOWEL CURB |
| | DETECTABLE WARNING SURFACE | | RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX) |
| | ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS. | | SAWCUT LIMITS |
| | PAVEMENT TREATMENT LIMITS | | CORING LOCATION |

NOTES

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS. OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

PAVING PLANS

PROJECT NO.
1007299

SCALE:
AS SHOWN

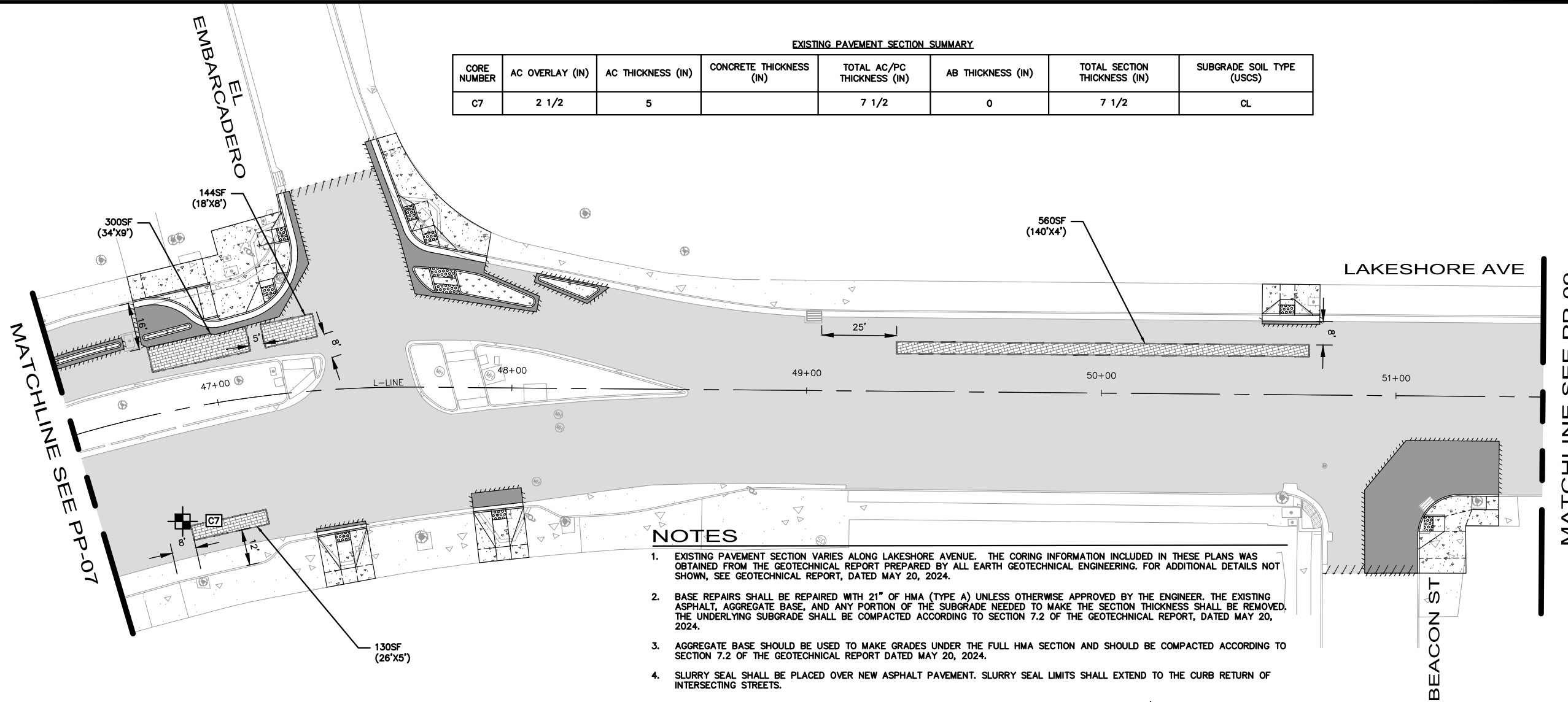
SHEET NO.
PP-07
55 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\PP-07.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya Pallivathucal

EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
C7	2 1/2	5		7 1/2	0	7 1/2	CL



NOTES

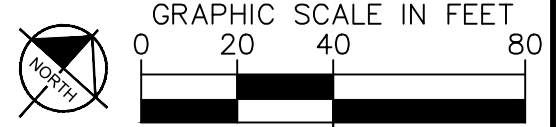
- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

LEGEND

	SLURRY SEAL		3" HMA (TYPE A) INLAY		ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS.
	TRENCH GRATE (SEE DD-01)		HMA PAVEMENT (12" DEPTH)		PAVEMENT TREATMENT LIMITS
	BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19.		PROPOSED LANDSCAPE AREA		SAWCUT LIMITS
	PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND		RAISED ISLAND WITH DOWEL CURB		CORING LOCATION
	DETECTABLE WARNING SURFACE		RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX)		

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

PAVING PLANS

PROJECT NO.
1007299

SCALE: AS SHOWN

SHEET NO.
PP-08
56 OF 83

DATE: 12/11/2024

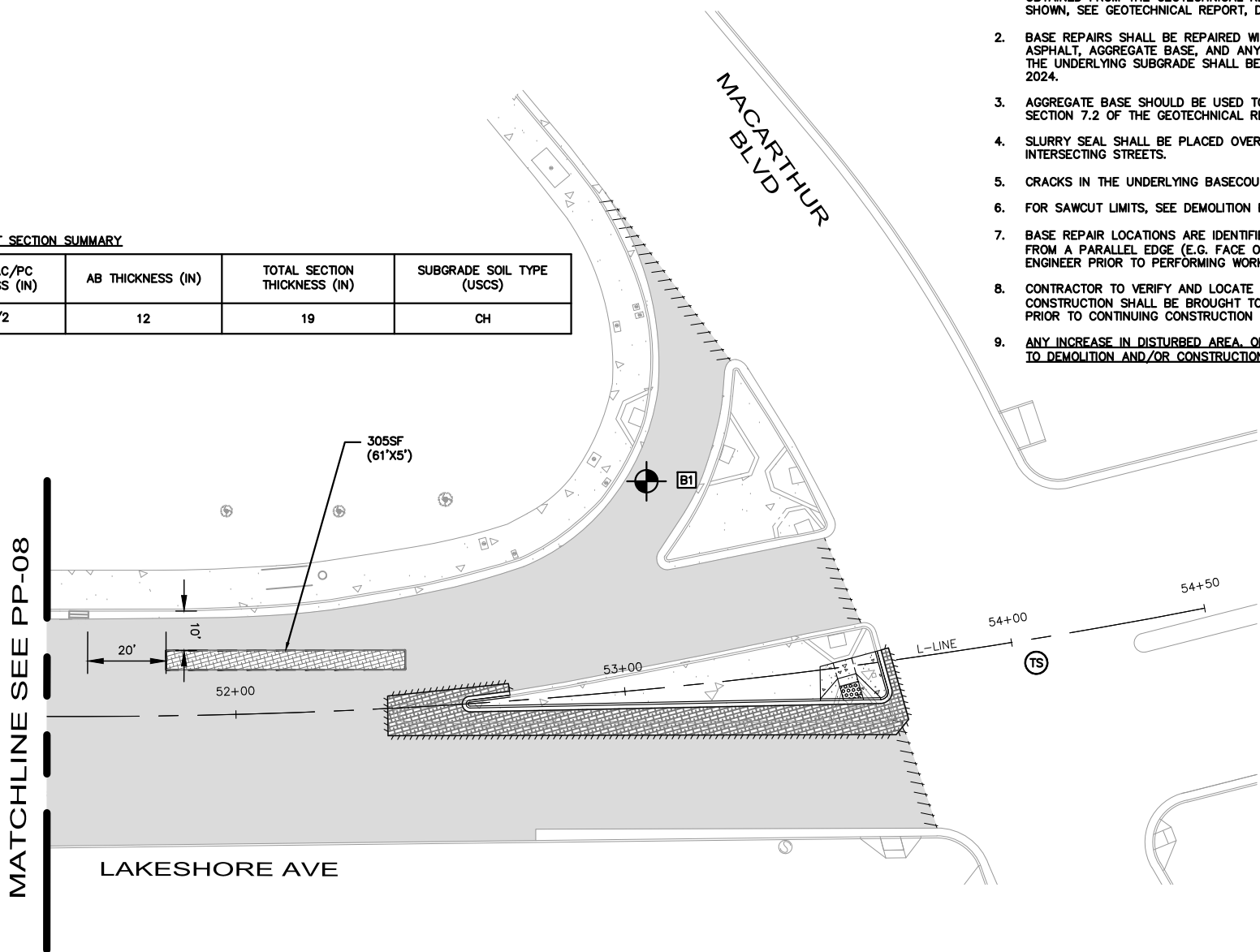
DRAWING NAME: K:\OAK_RD\WY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\PP-01.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal

NOTES

- EXISTING PAVEMENT SECTION VARIES ALONG LAKESHORE AVENUE. THE CORING INFORMATION INCLUDED IN THESE PLANS WAS OBTAINED FROM THE GEOTECHNICAL REPORT PREPARED BY ALL EARTH GEOTECHNICAL ENGINEERING. FOR ADDITIONAL DETAILS NOT SHOWN, SEE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- BASE REPAIRS SHALL BE REPAIRED WITH 21" OF HMA (TYPE A) UNLESS OTHERWISE APPROVED BY THE ENGINEER. THE EXISTING ASPHALT, AGGREGATE BASE, AND ANY PORTION OF THE SUBGRADE NEEDED TO MAKE THE SECTION THICKNESS SHALL BE REMOVED. THE UNDERLYING SUBGRADE SHALL BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT, DATED MAY 20, 2024.
- AGGREGATE BASE SHOULD BE USED TO MAKE GRADES UNDER THE FULL HMA SECTION AND SHOULD BE COMPACTED ACCORDING TO SECTION 7.2 OF THE GEOTECHNICAL REPORT DATED MAY 20, 2024.
- SLURRY SEAL SHALL BE PLACED OVER NEW ASPHALT PAVEMENT. SLURRY SEAL LIMITS SHALL EXTEND TO THE CURB RETURN OF INTERSECTING STREETS.
- CRACKS IN THE UNDERLYING BASECOURSE SHALL BE SEALED PRIOR TO THE APPLICATION OF SLURRY/SEAL COATS.
- FOR SAWCUT LIMITS, SEE DEMOLITION PLANS AND LAYOUT PLANS.
- BASE REPAIR LOCATIONS ARE IDENTIFIED BY OFFSET DIMENSIONS FROM THE CENTER OF AN OBJECT (E.G. UTILITY POLE, HYDRANT) OR FROM A PARALLEL EDGE (E.G. FACE OF CURB, LIP OF GUTTER). CONTRACTOR SHALL CONFIRM THE LIMITS OF REPAIR WITH THE ENGINEER PRIOR TO PERFORMING WORK.
- CONTRACTOR TO VERIFY AND LOCATE ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
- ANY INCREASE IN DISTURBED AREA, OR CHANGE TO PAVING LIMITS IS REQUIRED TO BE REVIEWED BY THE RESIDENT ENGINEER PRIOR TO DEMOLITION AND/OR CONSTRUCTION.

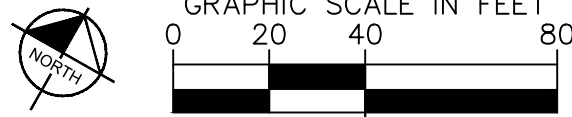
EXISTING PAVEMENT SECTION SUMMARY

CORE NUMBER	AC OVERLAY (IN)	AC THICKNESS (IN)	CONCRETE THICKNESS (IN)	TOTAL AC/PC THICKNESS (IN)	AB THICKNESS (IN)	TOTAL SECTION THICKNESS (IN)	SUBGRADE SOIL TYPE (USCS)
B1	3/1/2004	3 3/4		7 1/2	12	19	CH



LEGEND

- | | | | | | |
|--|--|--|--|--|--|
| | SLURRY SEAL | | 3" HMA (TYPE A) INLAY | | ELECTRICAL TRENCHING, SEE ELECTRICAL SHEETS. |
| | TRENCH GRATE (SEE DD-01) | | HMA PAVEMENT (12" DEPTH) | | PAVEMENT TREATMENT LIMITS |
| | BASE REPAIR (21" HMA - TYPE A) (SEE NOTE 7). FOR ADDITIONAL DETAILS, SEE AC BASE REPAIR DETAIL ON SHEET CD-19. | | PROPOSED LANDSCAPE AREA | | SAWCUT LIMITS |
| | PROPOSED CONCRETE SIDEWALK, BUS BOARDING ISLAND, RAISED ISLAND, AND PEDESTRIAN REFUGE ISLAND | | RAISED ISLAND WITH DOWEL CURB | | CORING LOCATION |
| | DETECTABLE WARNING SURFACE | | RAISED CROSSWALK HMA (TYPE A) OVERLAY (6" MAX) | | |



Kimley»Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

PAVING PLANS

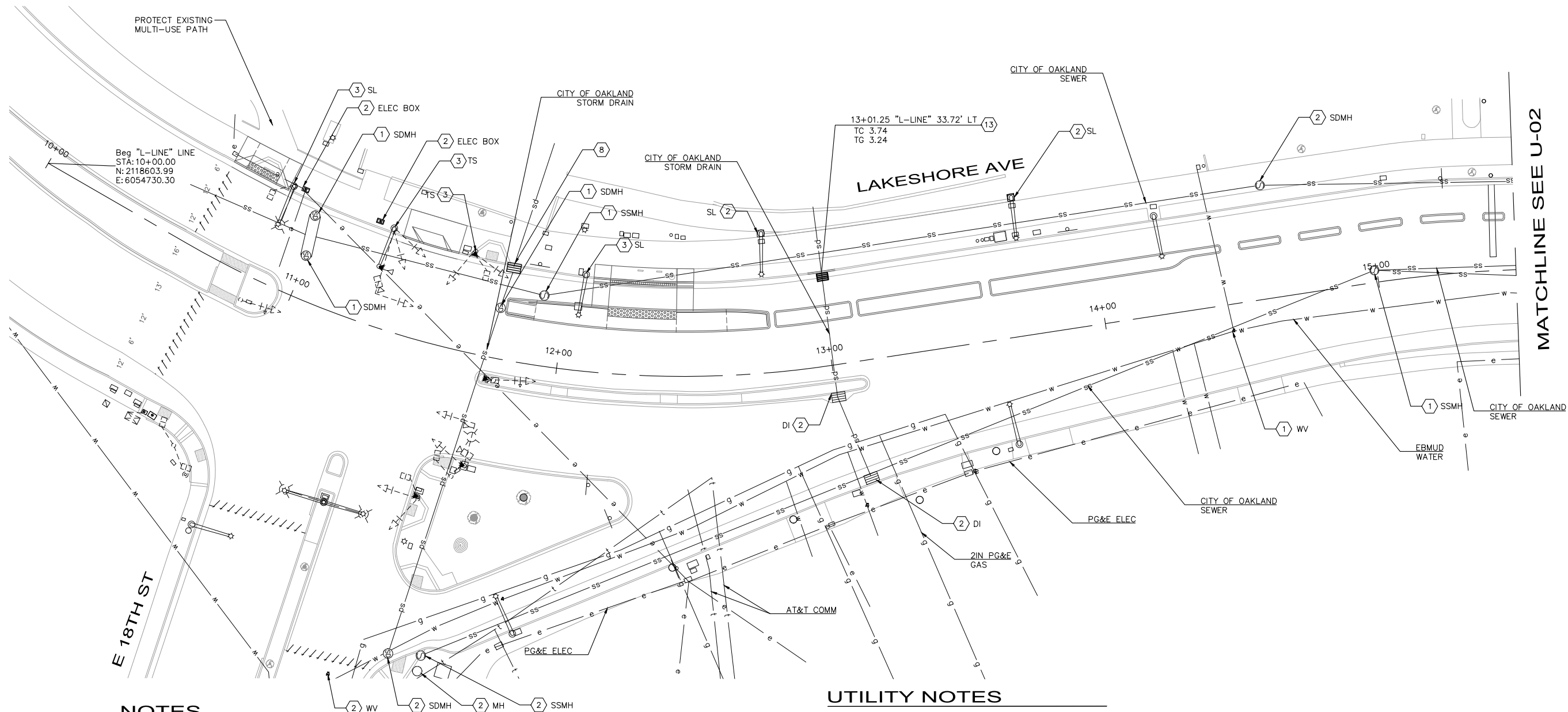
PROJECT NO.
1007299

SCALE:
 AS SHOWN

SHEET NO.
PP-09
 57 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan Sheets\PP-01.dwg
 PLOT DATE: 12-11-24
 PLOTTED BY: Priya Pallivathucal



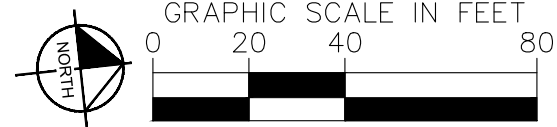
MATCHLINE SEE U-02

NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ③ PROTECT IN PLACE.
- ⑧ REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.
- ⑬ INSTALL DRAINAGE INLET WITH HEEL PROOF AND ADA COMPLIANT GRATE.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
PLANS**

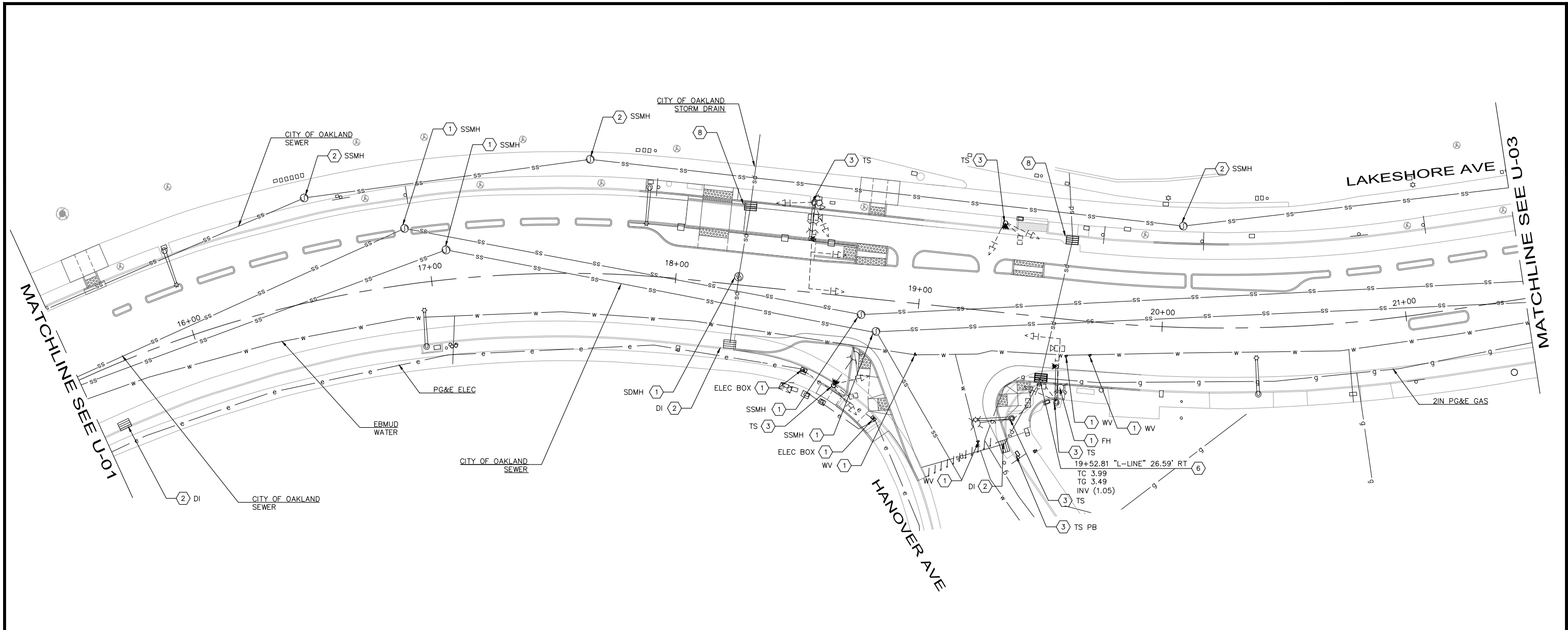
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
U-01
58 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\U-XX.dwg
PLOT DATE: 12/12/24
PLOT BY: Priya Pallivathucal



MATCHLINE SEE U-01

MATCHLINE SEE U-03

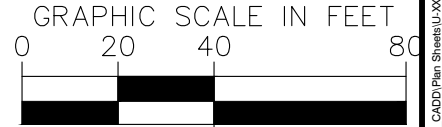
NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ③ PROTECT IN PLACE.
- ⑥ REMOVE AND REPLACE STORM DRAIN INLET.
- ⑧ REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
PLANS**

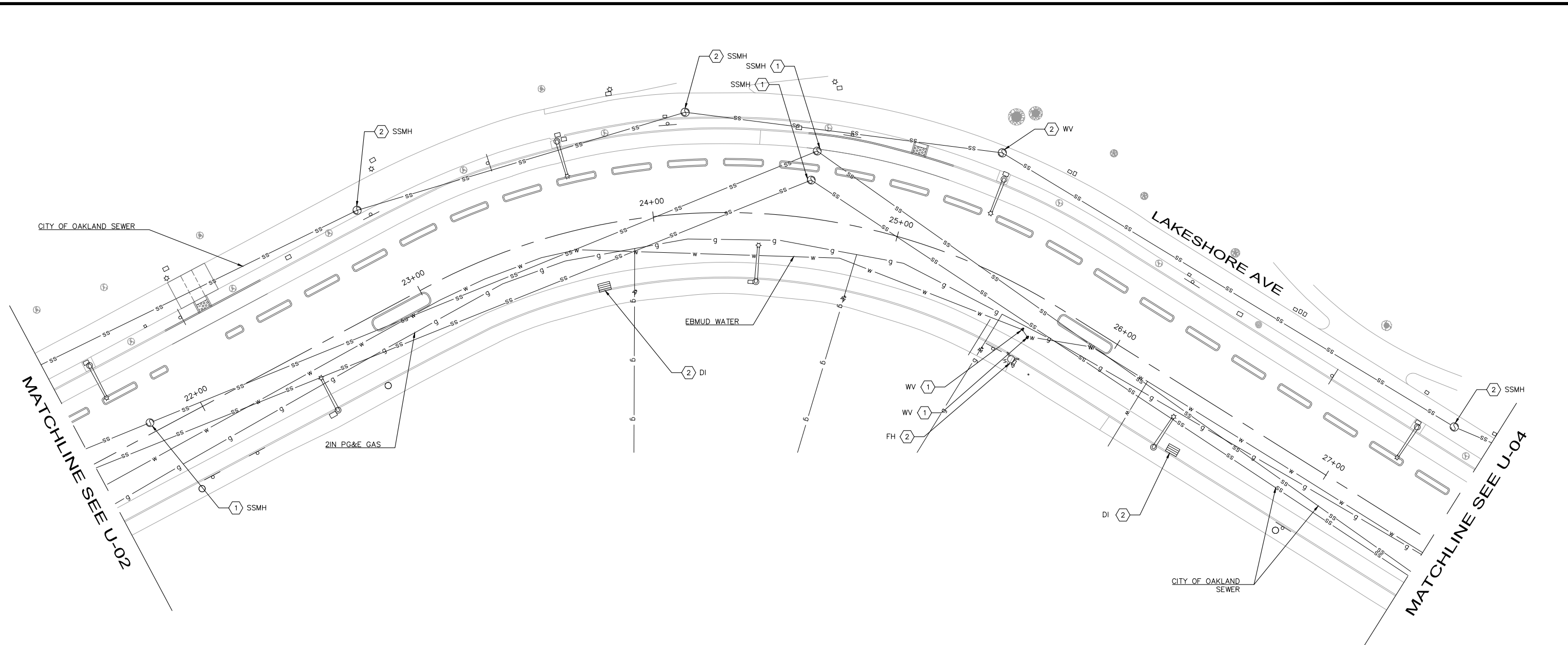
PROJECT NO.
1007299

SCALE:
AS SHOWN

SHEET NO.
U-02
59 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_BDW\100709299 - Lakeshore Ave Cycle Trace\04_CADD\Plan_Sheets\U-XX.dwg
PLOT DATE: 12/12/24
PLOTTED BY: Priya Pallivathucal



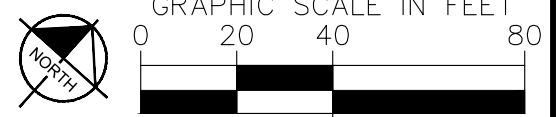
NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
PLANS**

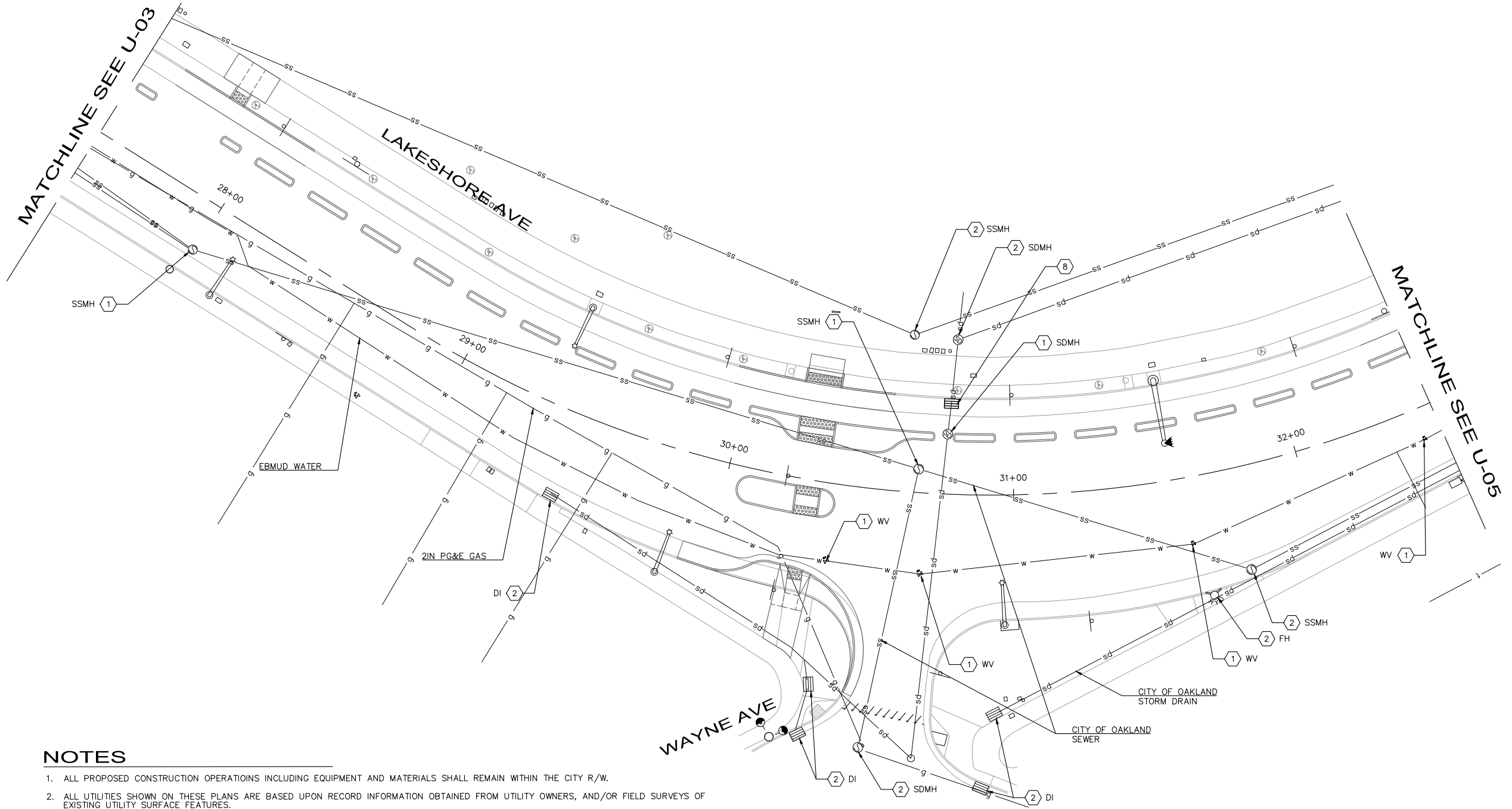
PROJECT NO.
1007299

SCALE:
AS SHOWN

DATE: 12/11/2024

SHEET NO.
U-03
60 OF 83

DRAWING NAME: K:\OAK_RD\W\100709299 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\U-XX.dwg
PLOT DATE: 12/12/24
PLOTTED BY: Priya Pallivathucal

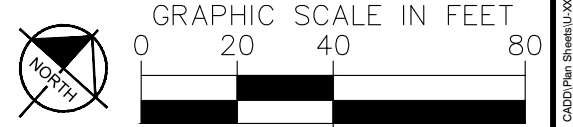


NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ⑧ REPLACE DRAINAGE INLET WITH HEEL PROOF AND ADA COMPLIANT GRATE.



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

Kimley»Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
 PLANS**

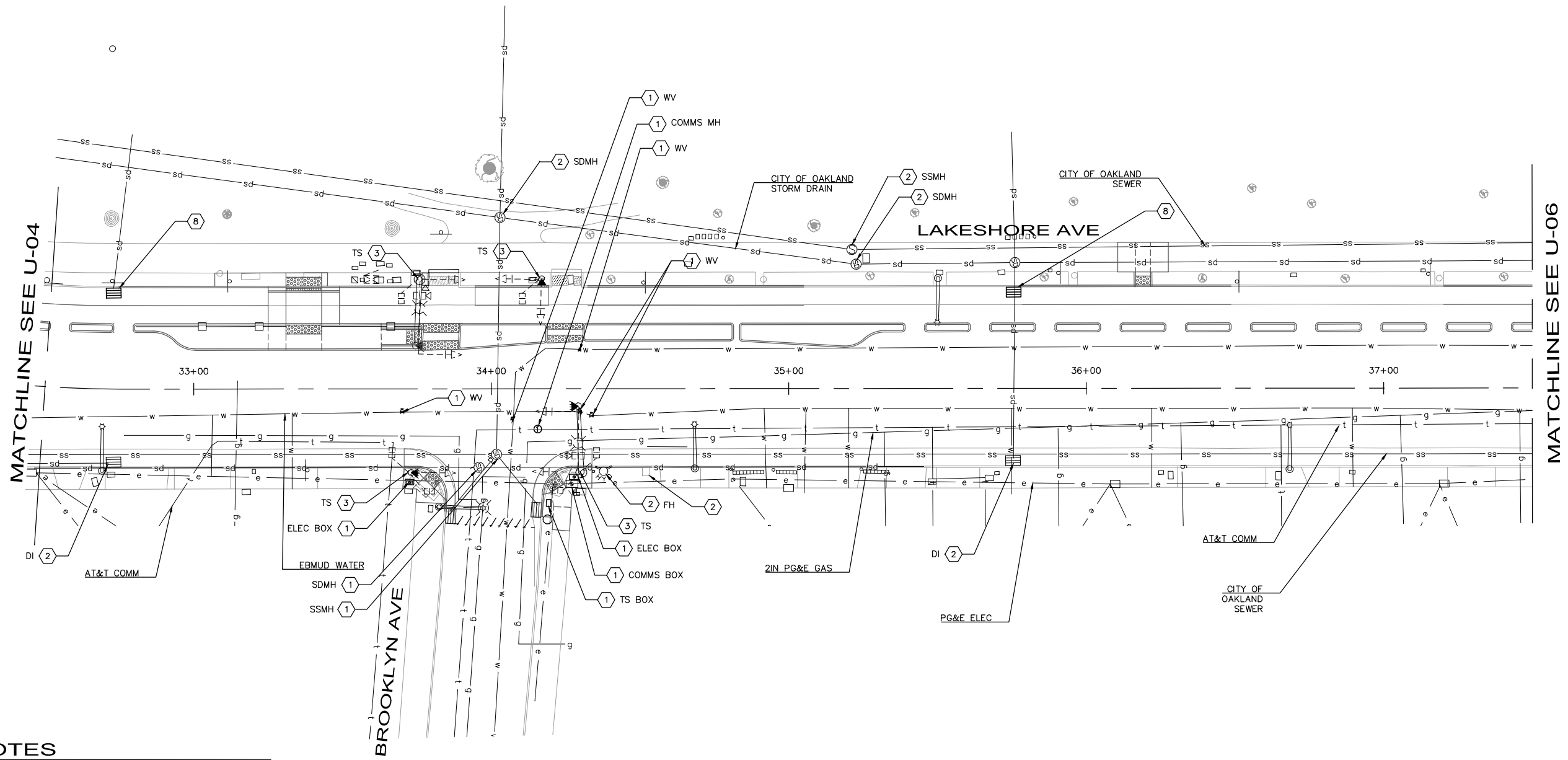
PROJECT NO.
1007299

SCALE:
 AS SHOWN

DATE: 12/11/2024

SHEET NO.
 U-04
 61 OF 83

DRAWING NAME: K:\OAK_RDW\1007299\02- Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\U-XX.dwg
 PLOT DATE: 12/12/24
 PLOTTED BY: Priya Pallivathucal

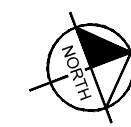


NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ③ PROTECT IN PLACE.
- ⑧ REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
PLANS**

PROJECT NO.
1007299

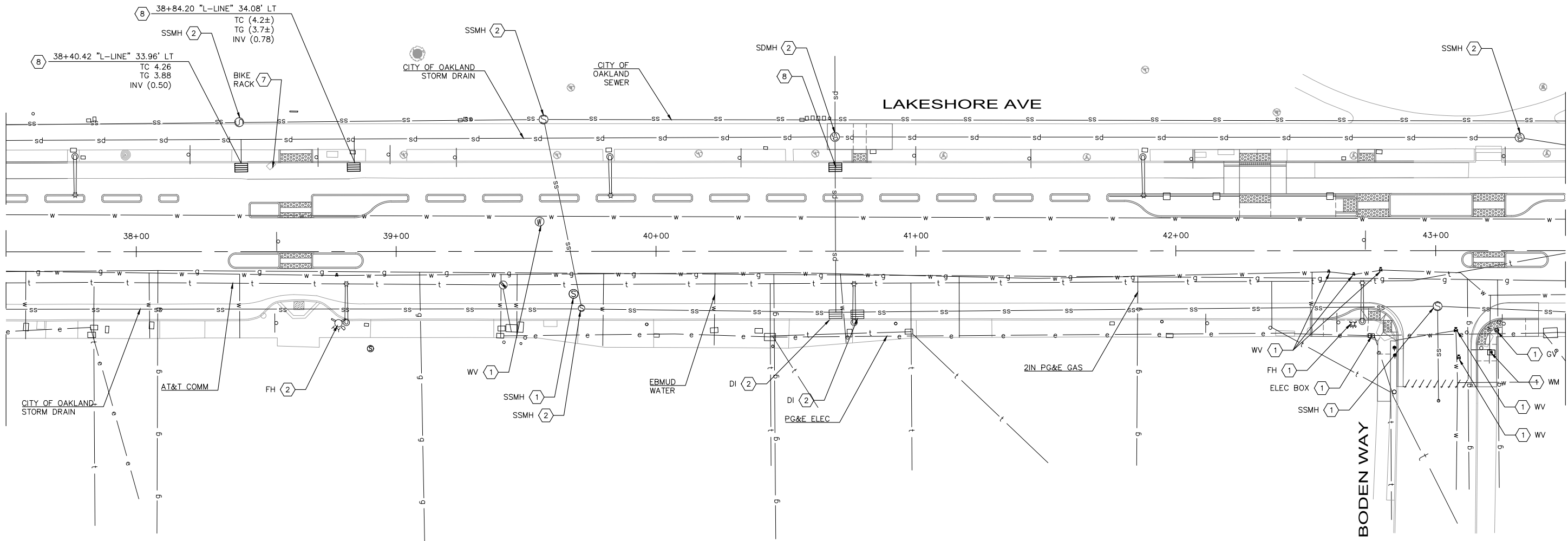
SCALE: AS SHOWN
DATE: 12/11/2024

SHEET NO.
U-05
62 OF 83

DRAWING NAME: K:\OAK_RDWY\09708204 - Lakeshore Ave Cycle Trace\04_CADD\Plan Sheets\U-XX.dwg
PLOT DATE: 12/13/24
PLOTTED BY: kay@gh.khandelwal

MATCHLINE SEE U-05

MATCHLINE SEE U-07



NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- (1) ADJUST TO GRADE.
- (2) EXISTING TO REMAIN.
- (7) REMOVE AND RELOCATE. CONTRACTOR TO INSTALL AND COORDINATE FINAL LOCATION WITH ENGINEER.
- (8) REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
PLANS**

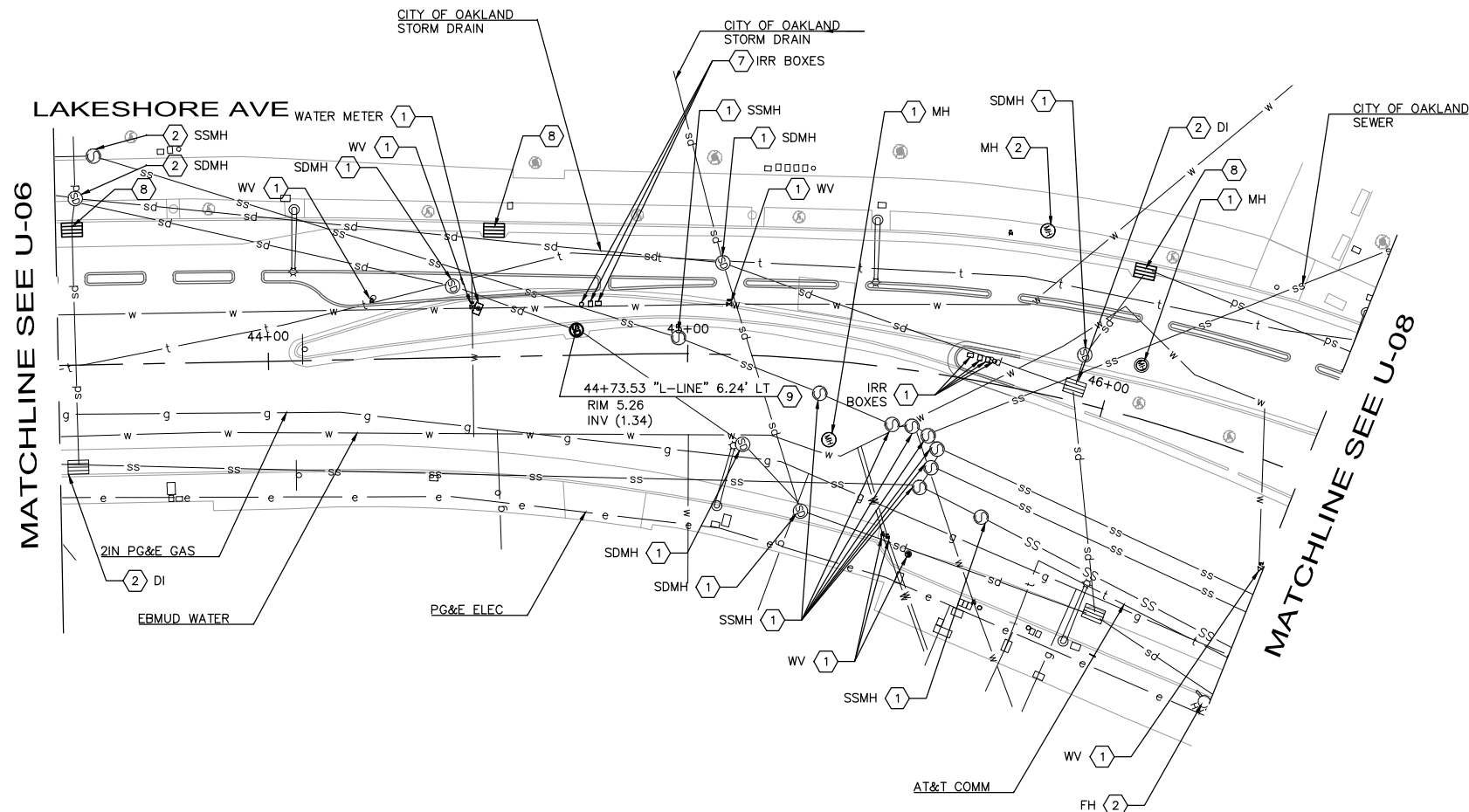
PROJECT NO.
1007299

SCALE:
AS SHOWN

DATE: 12/11/2024

SHEET NO.
U-06
63 OF **83**

DRAWING NAME: K:\OAK_RD\W\100709299 - Lakeshore Ave Cycle Trace\04_CADD\Plan_Sheets\U-XX.dwg
PLOT DATE: 12/12/24
PLOTTED BY: Priya Pallivathucal



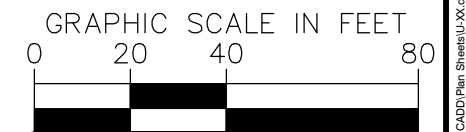
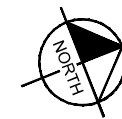
NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ⑦ REMOVE AND RELOCATE. CONTRACTOR TO INSTALL AND COORDINATE FINAL LOCATION WITH ENGINEER.
- ⑧ REPLACE DRAINAGE INLET GRATE WITH HEEL PROOF AND ADA COMPLIANT GRATE.
- ⑨ MODIFY STORM DRAIN INLET TO STORM DRAIN MAINTENANCE HOLE. SEE DETAIL F ON SHEET DD-02.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

Kimley Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
 PLANS**

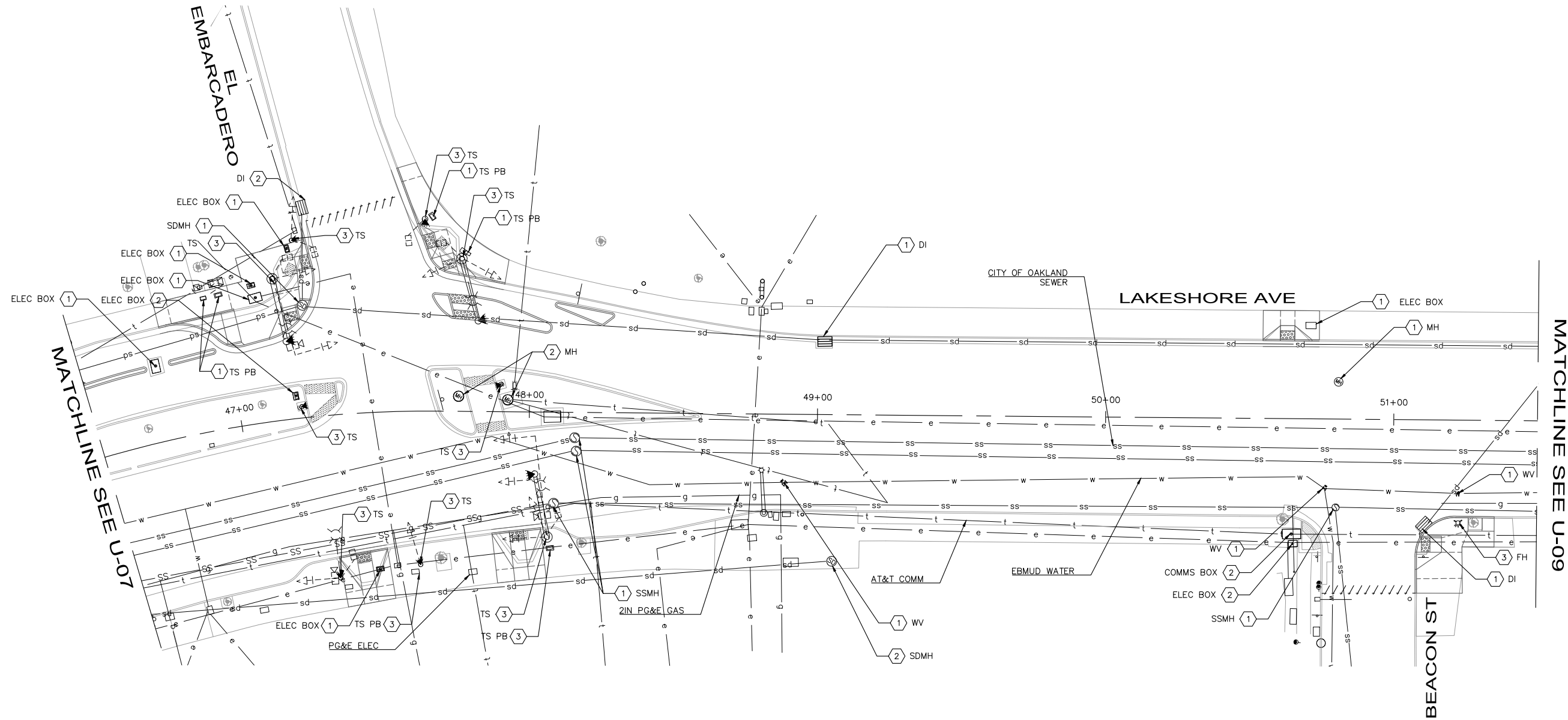
PROJECT NO.
1007299

SCALE:
 AS SHOWN

SHEET NO.
 U-07
 64 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Trace\04_CADD\Plan_Sheets\U-XX.dwg
 PLOT DATE: 12/12/24
 PLOTTED BY: Priya Pallivathucal



NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ③ PROTECT IN PLACE.



PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley-Horn
 1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
 Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
 DESIGNED BY JORGE MORALES
 DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



100% SUBMITTAL
 NOT FOR BID OR
 CONSTRUCTION

CITY OF OAKLAND
 DEPARTMENT OF TRANSPORTATION
 250 FRANK H. OGAWA PLAZA, SUITE 4314
 OAKLAND, CA 94612
 (510) 238-3437
 FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
 FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
 PLANS**

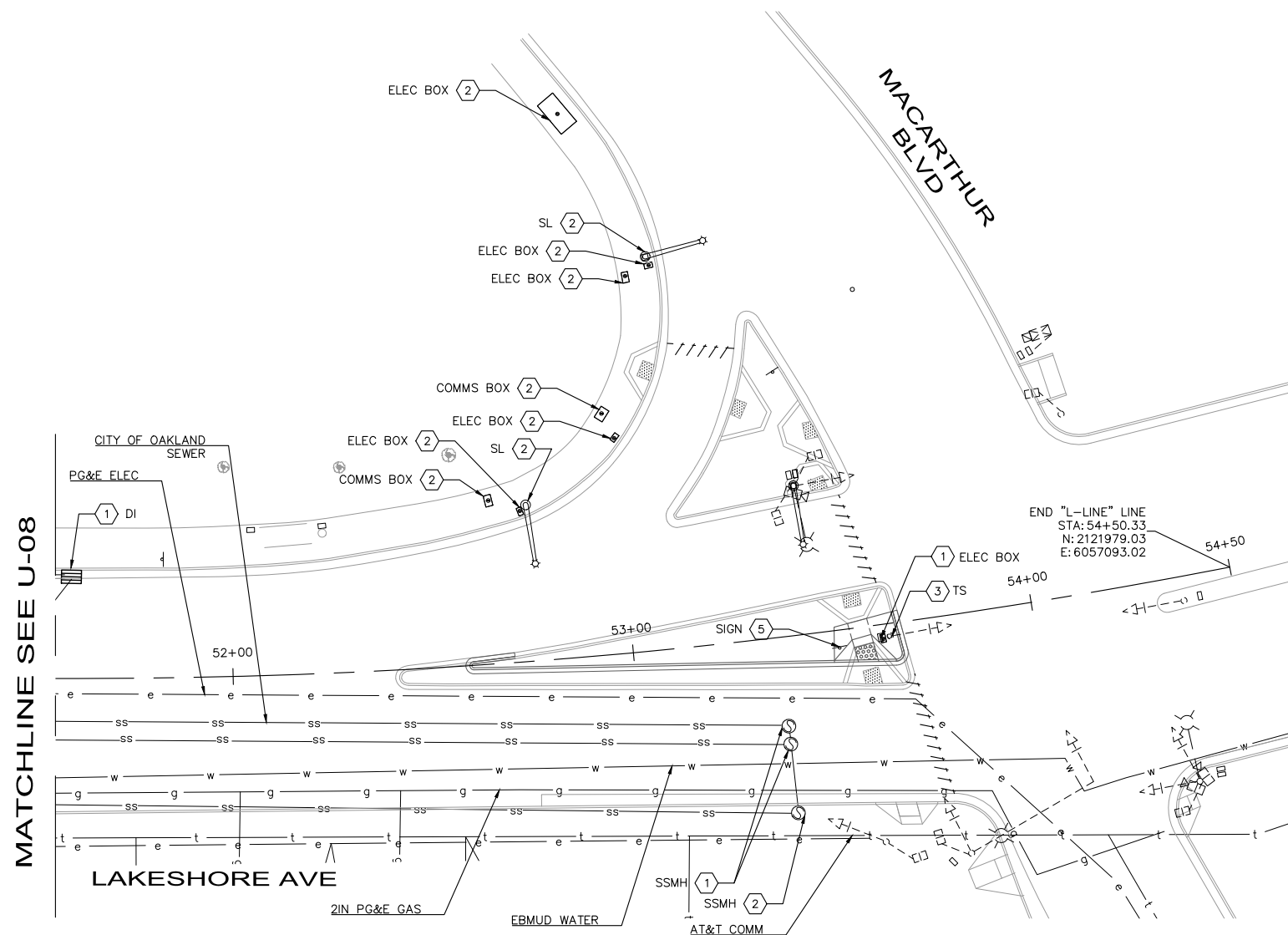
PROJECT NO.
1007299

SCALE:
 AS SHOWN

SHEET NO.
 U-08
 65 OF 83

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Trac\04_CADD\Plan_Sheets\U-X.dwg
 PLOT DATE: 12/12/24
 PLOTTED BY: Priya Pallivathucal

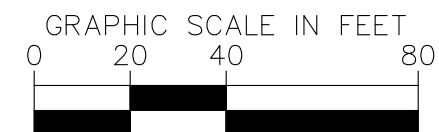


NOTES

1. ALL PROPOSED CONSTRUCTION OPERATIONS INCLUDING EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE CITY R/W.
2. ALL UTILITIES SHOWN ON THESE PLANS ARE BASED UPON RECORD INFORMATION OBTAINED FROM UTILITY OWNERS, AND/OR FIELD SURVEYS OF EXISTING UTILITY SURFACE FEATURES.
3. LOCATION OF UTILITY FACILITIES SHOWN ON THESE PLANS ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHALL SCHEDULE CONSTRUCTION OPERATIONS SO THAT UTILITIES ARE KEPT IN SERVICE AT ALL TIMES.
4. CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO WORK AND SHALL BE HELD LIABLE FOR ALL DAMAGES INCURRED. CALL SERVICE ALERT (U.S.A) AT (800) 227-2600 48 HOURS PRIOR TO EXCAVATION. ANY CONFLICTS DISCOVERED DURING CONSTRUCTION SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AND THE RESOLUTION SHALL BE APPROVED BY BOTH THE CITY AND THE ENGINEER PRIOR TO CONTINUING CONSTRUCTION ACTIVITIES. ADJUST ALL UTILITIES WITHIN PAVING LIMITS TO GRADE.
5. CONTRACTOR TO MAINTAIN NECESSARY CLEARANCE TO OVERHEAD LINES AT ALL TIMES.
6. CONTRACTOR TO RAISE DRAINAGE INLET GRATE TO NEW GRADES AS SHOWN. PROTECT IN PLACE EXISTING SD LINE AND STRUCTURE.
7. CONTRACTOR SHALL REPLACE ALL DRAINAGE INLET GRATES WITH HEEL PROOF AND ADA COMPLIANT GRATES.
8. FOR CHANNEL DRAIN DETAIL, SEE DRAINAGE DETAILS.
9. FOR TRAFFIC SIGNAL IMPROVEMENTS, SEE ELECTRICAL PLANS.

UTILITY NOTES

- ① ADJUST TO GRADE.
- ② EXISTING TO REMAIN.
- ③ PROTECT IN PLACE.
- ⑤ REMOVE AND RESET SIGN AND POST. SEE SS SHEETS FOR MORE DETAILS.



100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

**DRAINAGE AND UTILITY
PLANS**

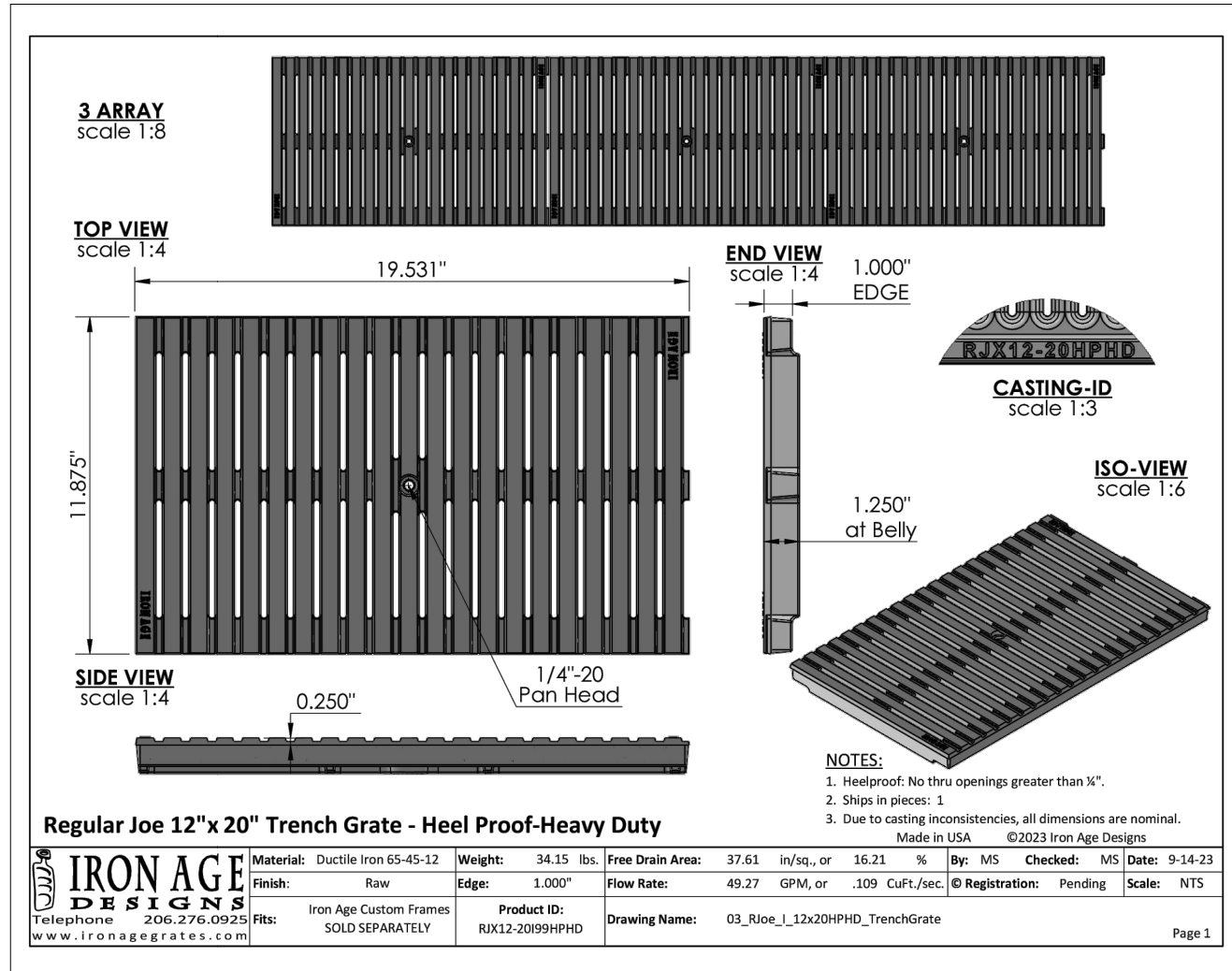
PROJECT NO.
1007299

SCALE:
AS SHOWN

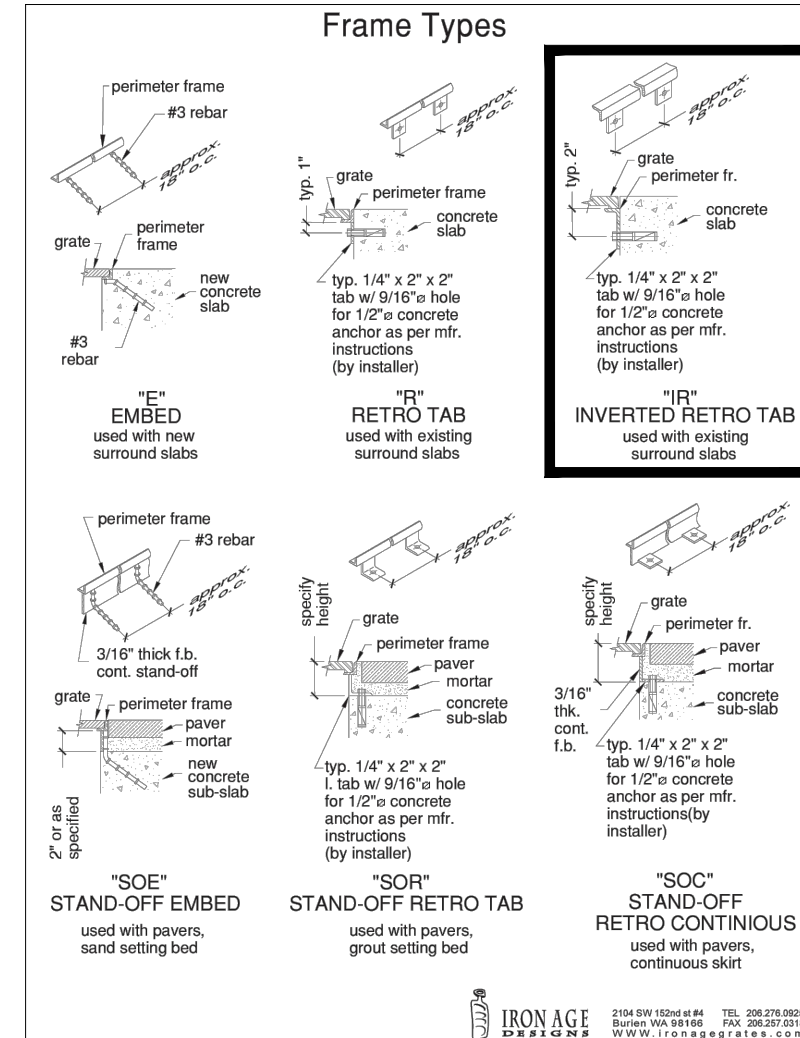
SHEET NO.
U-09
66 OF **83**

DATE: 12/11/2024

DRAWING NAME: K:\OAK_RDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\U-XX.dwg
PLOT DATE: 12/12/24
PLOTTED BY: Priya Pallivathucal



DETAIL A:
TRENCH GRATE DETAIL



DETAIL B:
TRENCH GRATE FRAME DETAIL

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

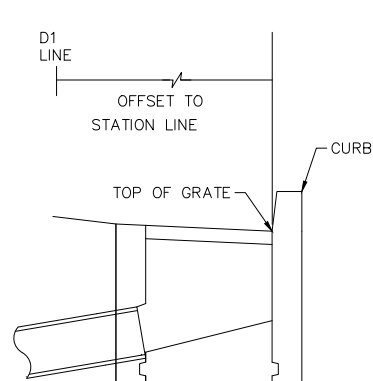
DRAINAGE DETAILS

PROJECT NO.
1007299

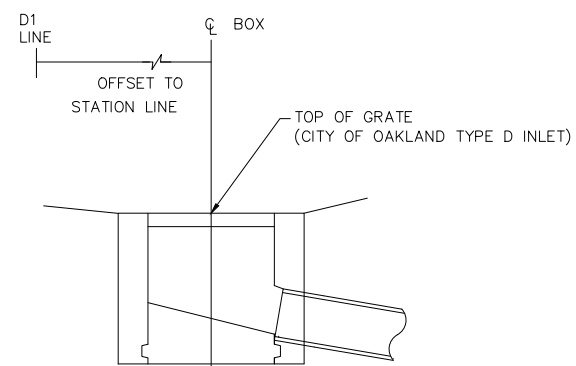
SCALE: NTS
DATE: 12/11/2024

SHEET NO.
DD-01
67 OF 83

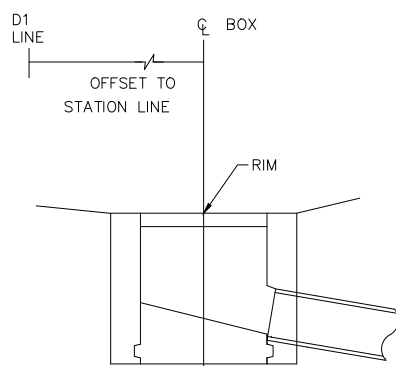
DRAWING NAME: K:\OAK_RDW\100709299 - Lakeshore Ave Cycle Trench\04_CADD\Plan_Sheets\DD-XK.dwg
PLOT DATE: 12-11-24
PLOTTED BY: Priya.Pallivathucal



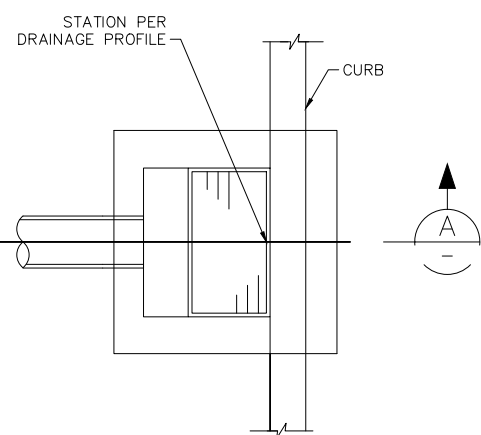
**SECTION A-A
DRAINAGE INLET STATION
AND OFFSET DETAIL**
NTS



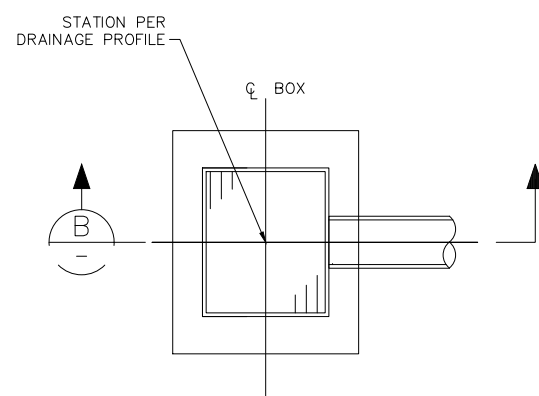
**SECTION B-B
FIELD DRAINAGE INLET STATION
AND OFFSET DETAIL**
NTS



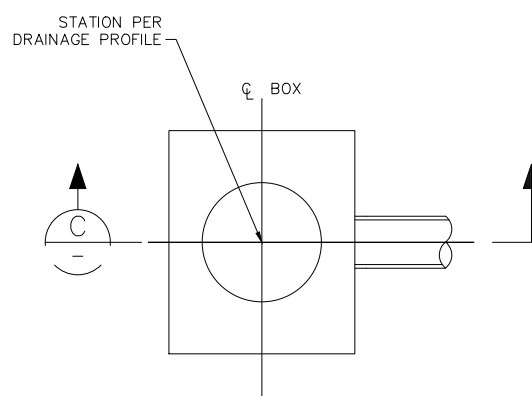
**SECTION C-C
MANHOLE STATION
AND OFFSET DETAIL**
NTS



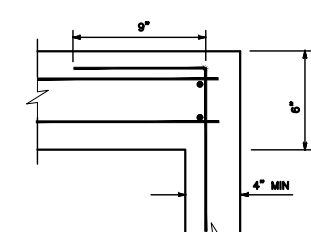
CITY OF OAKLAND TYPE "A" INLET (A)
NOT TO SCALE



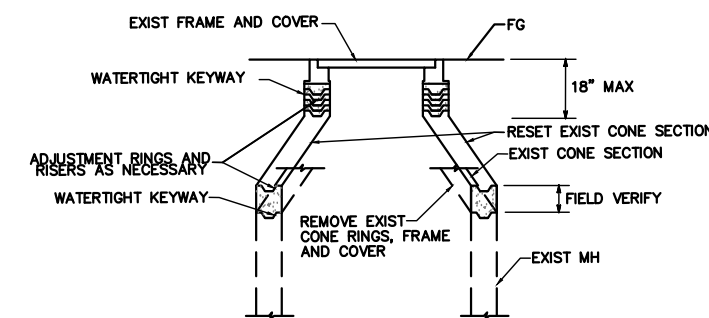
CITY OF OAKLAND TYPE "D" INLET (B)
NOT TO SCALE



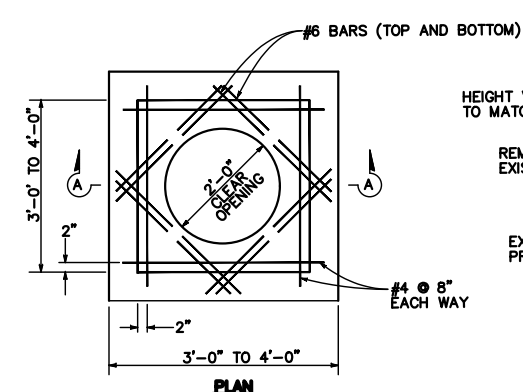
MAINTENANCE HOLE (C)
NOT TO SCALE



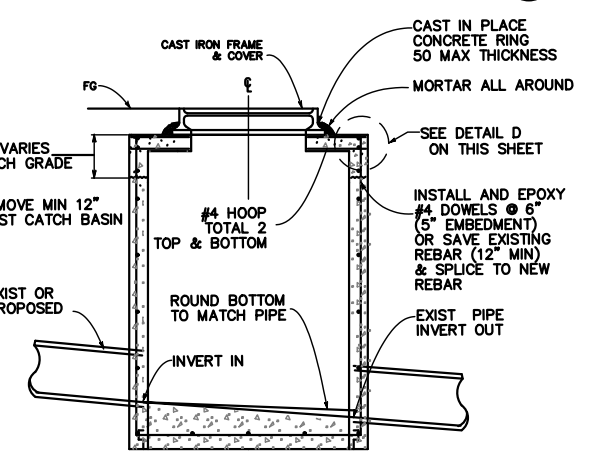
REBAR DETAILS (D)
NOT TO SCALE



ADJUST MH COVER TO GRADE (E)
NOT TO SCALE



MODIFY INLET TO MAINTENANCE HOLE (F)
NOT TO SCALE



**DETAIL A: DRAINAGE
STRUCTURES
AND OFFSET DETAIL**
NTS

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON
ANSI B (11" X 17") SHEETS. REFER TO
GRAPHIC SCALE FOR SHEETS OF
DIFFERENT SIZES.

Kimley»Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY JORGE MORALES
DRAWN BY TESSA KEMPER/PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

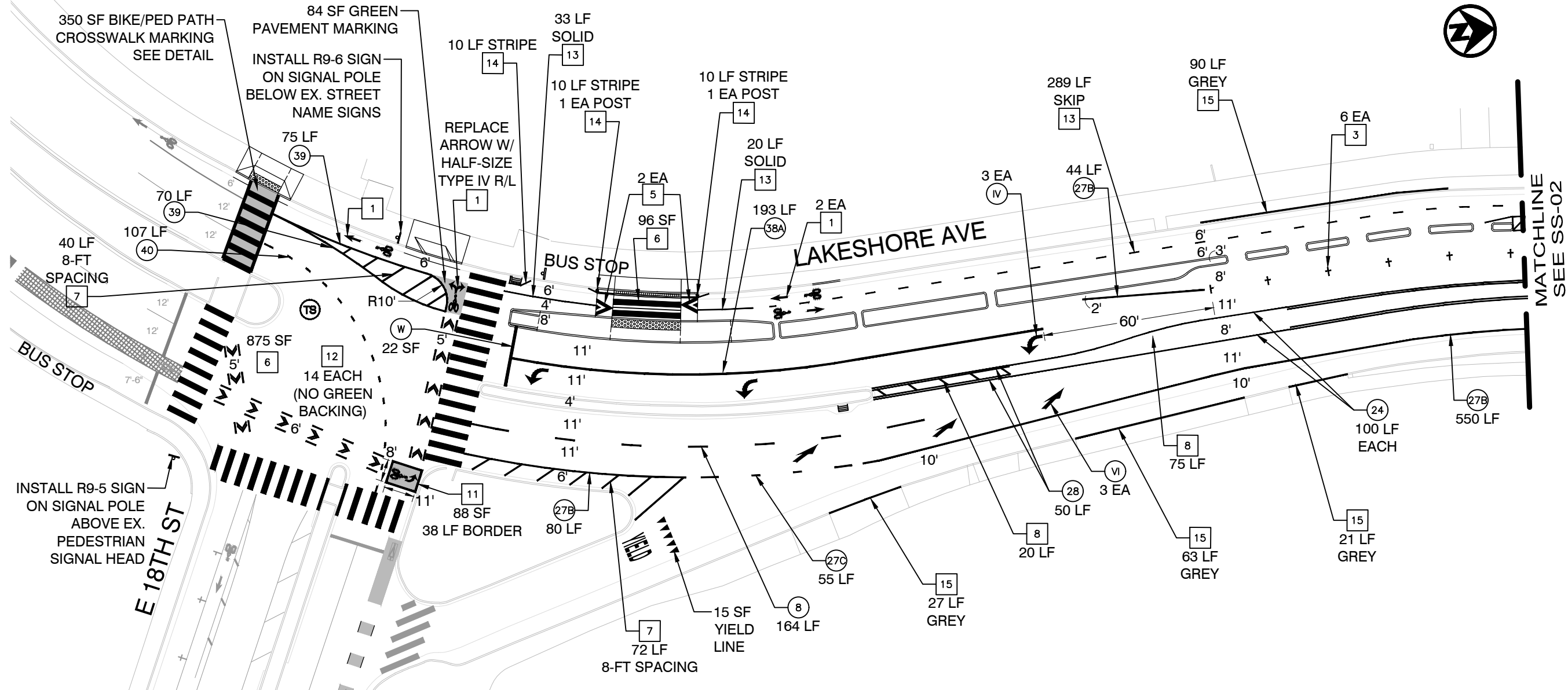
DRAINAGE DETAILS

SCALE: NTS
DATE: 12/11/2024

PROJECT NO.
1007299

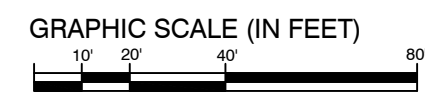
SHEET NO.
DD-02
68 OF 83

DRAWING NAME: K:\OAK_BDWY\08708204 - Lakeshore Ave Cycle Track\04_CADD\Plan_Sheets\DD-XK.dwg
PLOT DATE: 12-11-24
PLOT BY: Priya Pallivathucal



LEGEND		CONSTRUCTION NOTES	
#	STRIPING CONSTRUCTION NOTE NUMBER	1	BIKE LANE SYMBOL & ARROW Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.
f	DETAIL NUMBER PER CALTRANS STD PLANS Traffic lines as cardinal number Arrows as roman numeral	2	SHARROW Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.
TS	TRAFFIC SIGNAL	3	PARKING TEE Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.
W	CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)	4	BIKE DETECTOR SYMBOL Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.
STOP	PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown	5	RAISED BIKEWAY CROSSING MARKING Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.
EXISTING SIGN	EXISTING SIGN ON EXISTING POST	6	CONTINENTAL CROSSWALK Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.
FIRE HYDRANT	FIRE HYDRANT	7	WHITE BUFFER STRIPING Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.
BLUE FIRE HYDRANT MARKER	BLUE FIRE HYDRANT MARKER	8	YELLOW BUFFER STRIPING Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.
EX	EXISTING TO REMAIN rendered in grey	9	REMOVE CONFLICTING STRIPING
LF	LINEAR FEET	10	REMOVE CONFLICTING MARKINGS
SF	SQUARE FEET	11	BIKE BOX OR 2-STAGE LEFT TURN BOX Install bike box or 2-stage left turn box as noted on plan. See details.
CR	CURB RETURN	12	BIKE INTERSECTION MARKINGS Install bike intersection markings as noted. See detail.
FC	FACE OF CURB	13	SEPARATED BIKE LANE CENTERLINE STRIPING Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
NEW SIGN	NEW SIGN ON NEW POST	14	DRAINAGE FEATURE EDGELINE STRIPING Install 4 inch white stripe and/or channelizing posts as noted. See details.
NEW SIGN	NEW SIGN ON EXISTING POST	15	CURB PAINT Install curb paint. Color as noted.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	DESIGNED BY	DRAWN BY	No.	DATE	BY	REFERENCE
JASON PATTON	DEP	DEP				

SIGNAGE AND STRIPING PLANS

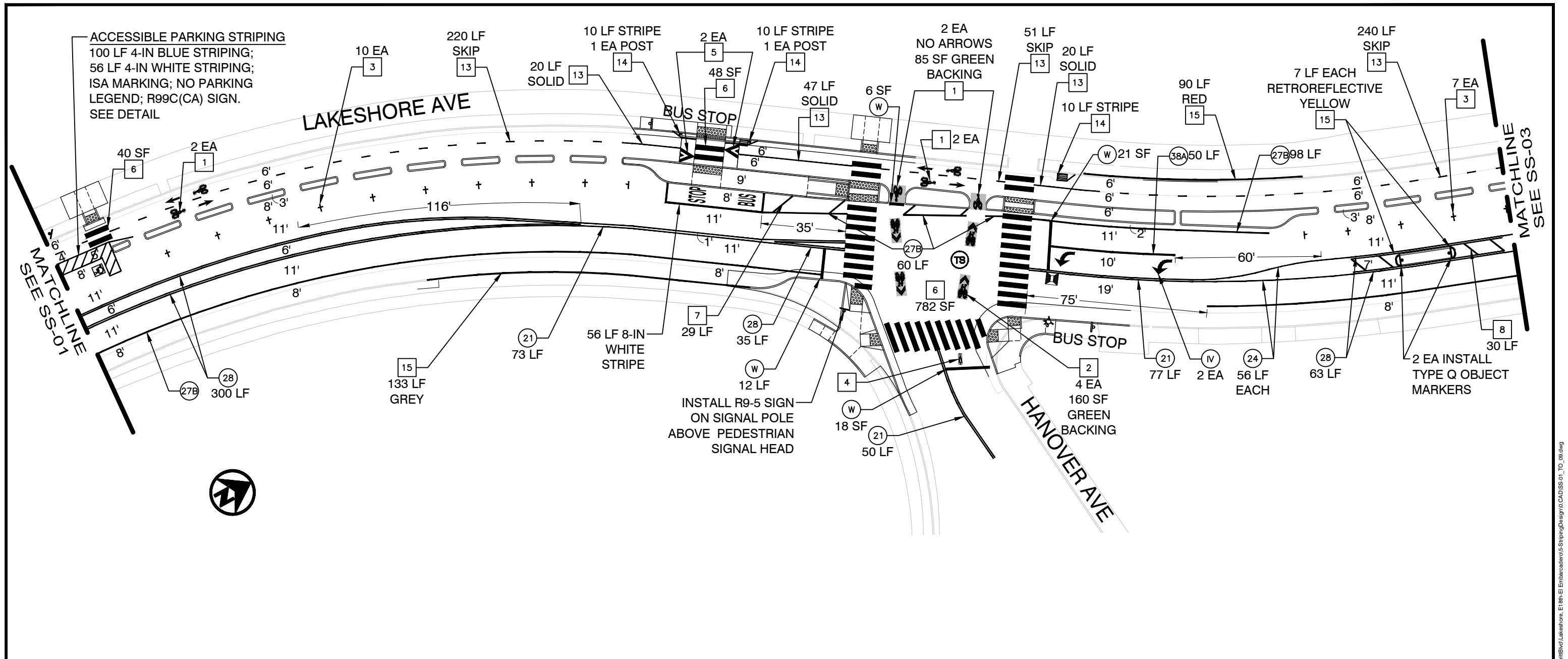
PROJECT NO.
1007299

SCALE:
1" = 40'

SHEET NO.
SS-01
69 OF 83

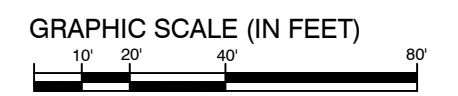
DATE: 12/12/2024

DRAWING NAME: \work\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-striping\LakeshoreAve-LakeMerritt\Bld\Lakeshore_E18thEI-Embarcadero\5-StripingDesign\0.CADD\SS-01_TO_06.dwg
PLOT DATE: 12/12/24
PLOTTED BY: pwa04



LEGEND		CONSTRUCTION NOTES	
	STRIPING CONSTRUCTION NOTE NUMBER		1 BIKE LANE SYMBOL & ARROW Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.
	DETAIL NUMBER PER CALTRANS STD PLANS Traffic lines as cardinal number Arrows as roman numeral		2 SHARROW Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.
	TRAFFIC SIGNAL		3 PARKING TEE Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.
	CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)		4 BIKE DETECTOR SYMBOL Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.
	PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown		5 RAISED BIKEWAY CROSSING MARKING Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.
	EXISTING SIGN ON EXISTING POST		6 CONTINENTAL CROSSWALK Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.
	FIRE HYDRANT		7 WHITE BUFFER STRIPING Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.
	BLUE FIRE HYDRANT MARKER		8 YELLOW BUFFER STRIPING Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.
	EX EXISTING TO REMAIN rendered in grey		9 REMOVE CONFLICTING STRIPING
	LF LINEAR FEET		10 REMOVE CONFLICTING MARKINGS
	SF SQUARE FEET		11 BIKE BOX OR 2-STAGE LEFT TURN BOX Install bike box or 2-stage left turn box as noted on plan. See details.
	CR CURB RETURN		12 BIKE INTERSECTION MARKINGS Install bike intersection markings as noted. See detail.
	FC FACE OF CURB		13 SEPARATED BIKE LANE CENTERLINE STRIPING Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
	NEW SIGN ON NEW POST		14 DRAINAGE FEATURE EDGELINE STRIPING Install 4 inch white stripe and/or channelizing posts as noted. See details.
	NEW SIGN ON EXISTING POST		15 CURB PAINT Install curb paint. Color as noted.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	DESIGNED BY	DRAWN BY	No.	DATE	BY	REFERENCE
JASON PATTON	DEP	DEP				

SIGNAGE AND STRIPING PLANS

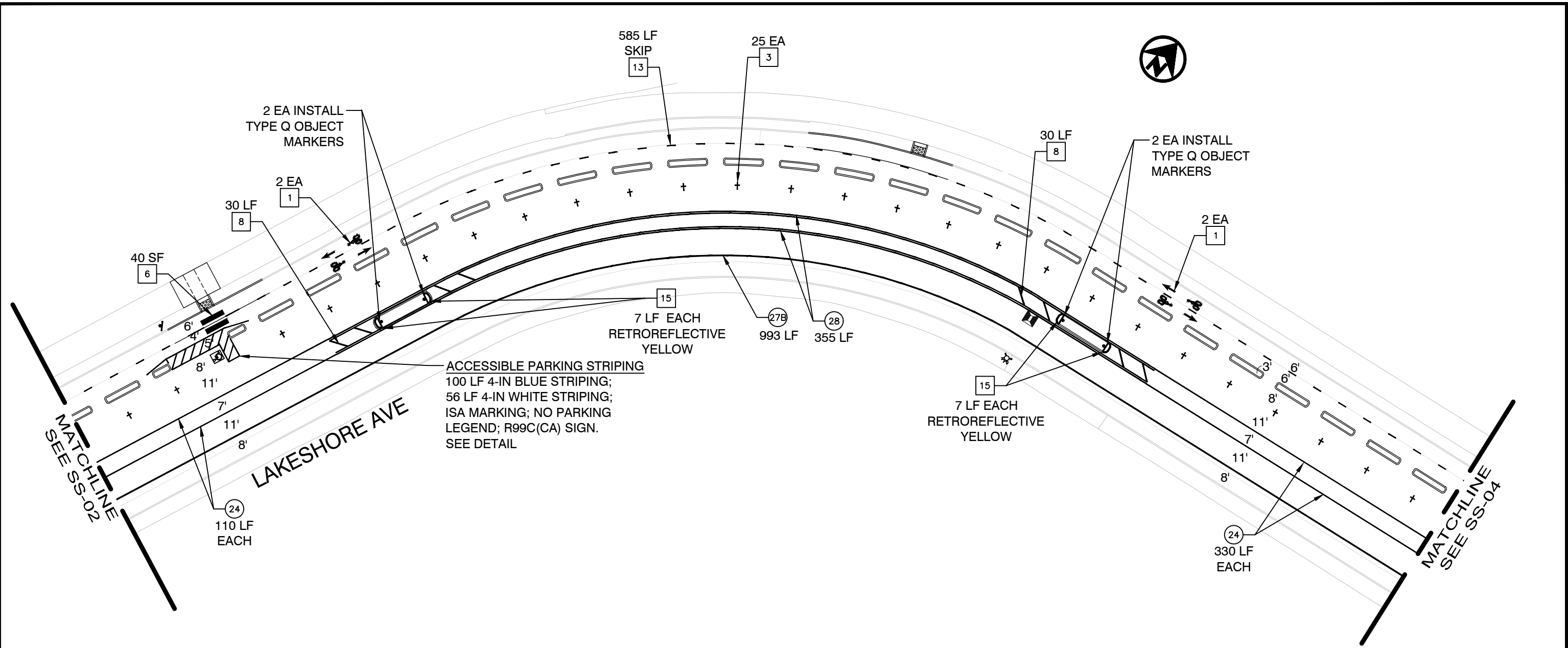
PROJECT NO.
1007299

SCALE:
1" = 40'

SHEET NO.
SS-02
70 OF **83**

DATE: 12/12/2024

DRAWING NAME: \\oakland\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-striping\LakeshoreAve-LakeMerritt\Bld\Lakeshore_E18thEI\Embarcadero\5-Striping\Design\0.CADD\SS-01_TO_08.dwg
PLOT DATE: 12/12/24
PLOTTED BY: pene04

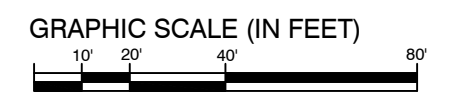


- LEGEND**
- # STRIPING CONSTRUCTION NOTE NUMBER
 - Ⓜ DETAIL NUMBER PER CALTRANS STD PLANS
Traffic lines as cardinal number
Arrows as roman numeral
 - Ⓣs TRAFFIC SIGNAL
 - Ⓜ CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)
 - STOP PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown
 - EXISTING SIGN ON EXISTING POST
 - Ⓜ FIRE HYDRANT
 - Ⓜ BLUE FIRE HYDRANT MARKER
 - EX EXISTING TO REMAIN rendered in grey
 - LF LINEAR FEET
 - SF SQUARE FEET
 - CR CURB RETURN
 - FC FACE OF CURB
 - NEW SIGN ON NEW POST
 - NEW SIGN ON EXISTING POST

- CONSTRUCTION NOTES**
- 1 BIKE LANE SYMBOL & ARROW
Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.
 - 2 SHARROW
Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.
 - 3 PARKING TEE
Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.
 - 4 BIKE DETECTOR SYMBOL
Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.

- 5 RAISED BIKEWAY CROSSING MARKING
Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.
- 6 CONTINENTAL CROSSWALK
Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.
- 7 WHITE BUFFER STRIPING
Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.
- 8 YELLOW BUFFER STRIPING
Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.
- 9 REMOVE CONFLICTING STRIPING
- 10 REMOVE CONFLICTING MARKINGS
- 11 BIKE BOX OR 2-STAGE LEFT TURN BOX
Install bike box or 2-stage left turn box as noted on plan. See details.
- 12 BIKE INTERSECTION MARKINGS
Install bike intersection markings as noted. See detail.
- 13 SEPARATED BIKE LANE CENTERLINE STRIPING
Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
- 14 DRAINAGE FEATURE EDGELINE STRIPING
Install 4 inch white stripe and/or channelizing posts as noted. See details.
- 15 CURB PAINT
Install curb paint. Color as noted.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

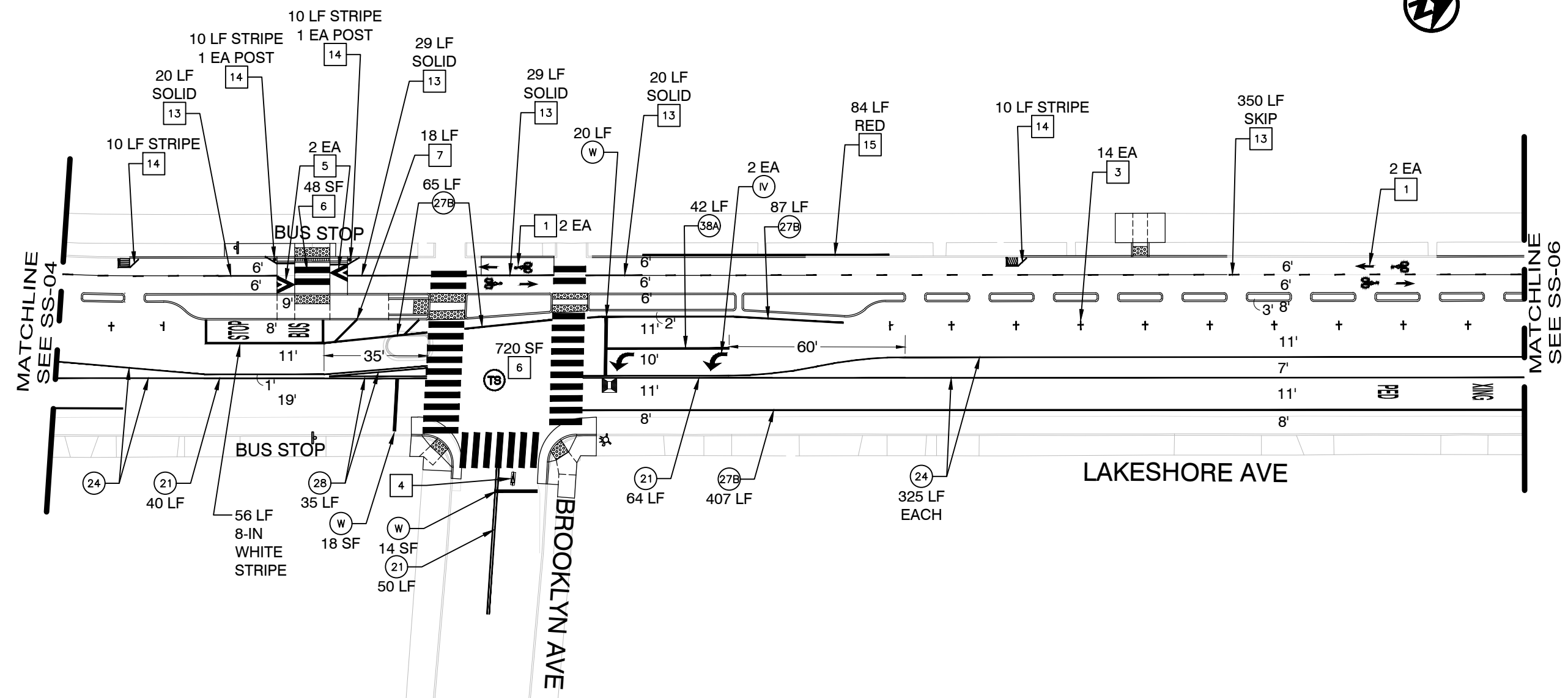
DRAFT NOT FOR CONSTRUCTION

CHECKED BY	No.	DATE	BY	REFERENCE
JASON PATTON				
DESIGNED BY				
DEP				
DRAWN BY				
DEP				

SIGNAGE AND STRIPING PLANS

PROJECT NO. 1007299	
SCALE: 1" = 40'	SHEET NO. SS-03 71 OF 83
DATE: 12/12/2024	

DRAWING NAME: \\oakland\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-striping\LakeshoreAve-LakeMerrittBvd\Lakeshore_E18thE1EmbarcaderoS-StripingDesign\0.CADD\SS-01_TO_08.dwg
PLOT DATE: 12/12/24
PLOTTED BY: pwa04



- LEGEND**
- # STRIPING CONSTRUCTION NOTE NUMBER
 - f DETAIL NUMBER PER CALTRANS STD PLANS
Traffic lines as cardinal number
Arrows as roman numeral
 - TS TRAFFIC SIGNAL
 - W CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)
 - STOP PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown
 - EXISTING SIGN ON EXISTING POST
 - EX EXISTING TO REMAIN rendered in grey
 - LF LINEAR FEET
 - SF SQUARE FEET
 - CR CURB RETURN
 - FC FACE OF CURB
 - NEW SIGN ON NEW POST
 - NEW SIGN ON EXISTING POST

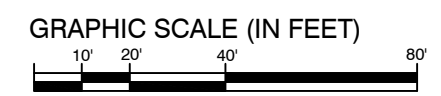
- FIRE HYDRANT
- BLUE FIRE HYDRANT MARKER

- CONSTRUCTION NOTES**
- 1 BIKE LANE SYMBOL & ARROW
Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.
 - 2 SHARROW
Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.
 - 3 PARKING TEE
Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.
 - BIKE DETECTOR SYMBOL
4 Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.

- 5 RAISED BIKEWAY CROSSING MARKING
Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.
- 6 CONTINENTAL CROSSWALK
Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.
- 7 WHITE BUFFER STRIPING
Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.
- 8 YELLOW BUFFER STRIPING
Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.
- 9 REMOVE CONFLICTING STRIPING

- 10 REMOVE CONFLICTING MARKINGS
- 11 BIKE BOX OR 2-STAGE LEFT TURN BOX
Install bike box or 2-stage left turn box as noted on plan. See details.
- 12 BIKE INTERSECTION MARKINGS
Install bike intersection markings as noted. See detail.
- 13 SEPARATED BIKE LANE CENTERLINE STRIPING
Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
- 14 DRAINAGE FEATURE EDGELINE STRIPING
Install 4 inch white stripe and/or channelizing posts as noted. See details.
- 15 CURB PAINT
Install curb paint. Color as noted.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

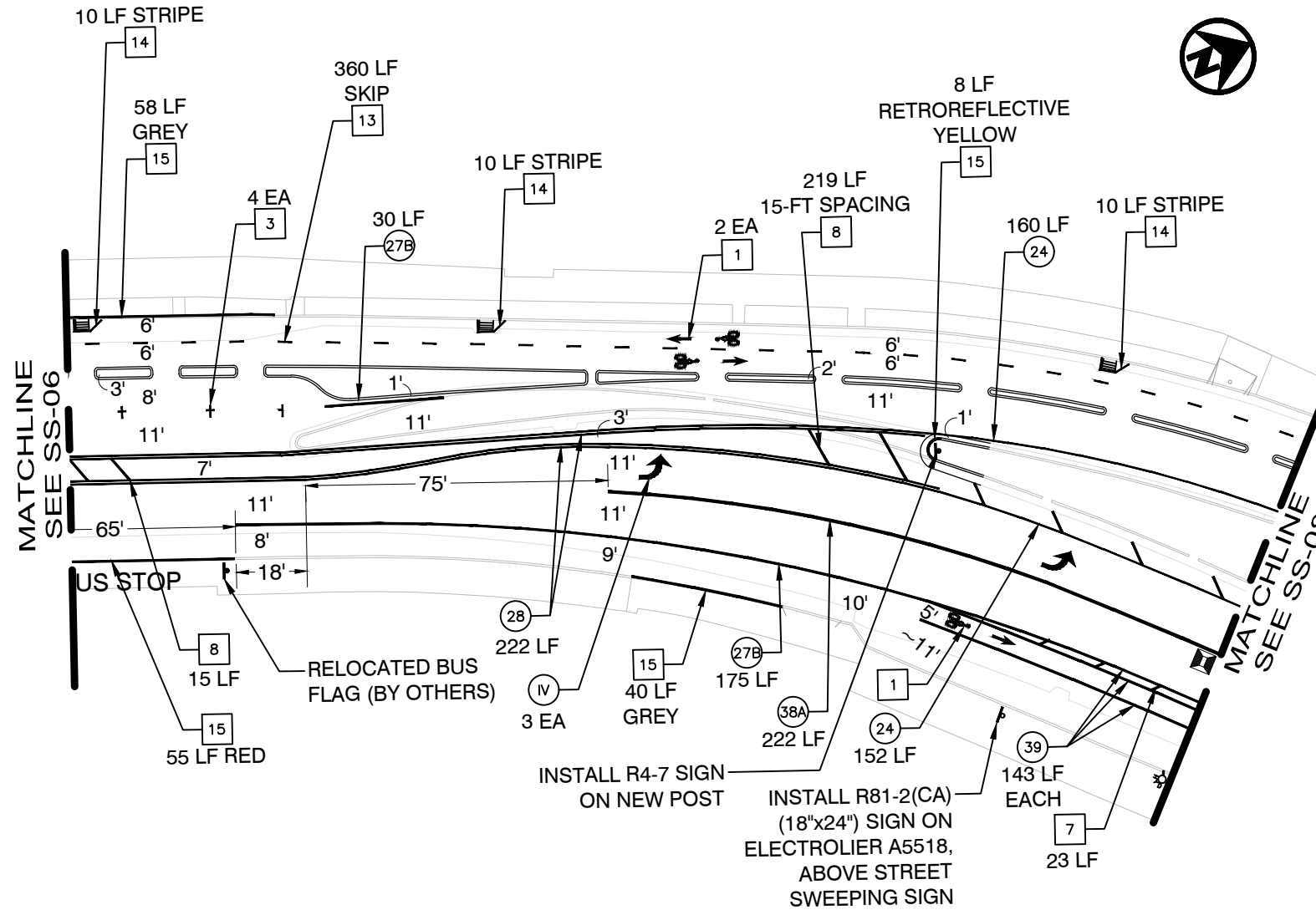
DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON	No.	DATE	BY	REFERENCE
DESIGNED BY	DEP				
DRAWN BY	DEP				

SIGNAGE AND STRIPING PLANS

PROJECT NO. 1007299	
SCALE: 1" = 40'	SHEET NO. SS-05 73 OF 83
DATE: 12/12/2024	

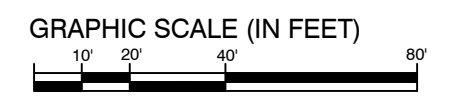
DRAWING NAME: \\oakland\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-striping\LakeshoreAve-LakeMerritt\Bld\Lakeshore_E18th-EI-Embarcadero-5-StripingDesign\0.CADD\SS-01_TO_06.dwg
PLOT DATE: 12/12/24
PLOT BY: pene04



LEGEND

# STRIPING CONSTRUCTION NOTE NUMBER	🔥 FIRE HYDRANT	CONSTRUCTION NOTES	5 RAISED BIKEWAY CROSSING MARKING Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.	10 REMOVE CONFLICTING MARKINGS
Ⓜ DETAIL NUMBER PER CALTRANS STD PLANS Traffic lines as cardinal number Arrows as roman numeral	EX EXISTING TO REMAIN rendered in grey	1 BIKE LANE SYMBOL & ARROW Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.	6 CONTINENTAL CROSSWALK Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.	11 BIKE BOX OR 2-STAGE LEFT TURN BOX Install bike box or 2-stage left turn box as noted on plan. See details.
Ⓢ TRAFFIC SIGNAL	LF LINEAR FEET	2 SHARROW Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.	7 WHITE BUFFER STRIPING Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.	12 BIKE INTERSECTION MARKINGS Install bike intersection markings as noted. See detail.
Ⓢ CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)	SF SQUARE FEET	3 PARKING TEE Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.	8 YELLOW BUFFER STRIPING Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.	13 SEPARATED BIKE LANE CENTERLINE STRIPING Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
STOP PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown	CR CURB RETURN	4 BIKE DETECTOR SYMBOL Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.	9 REMOVE CONFLICTING STRIPING	14 DRAINAGE FEATURE EDGELINE STRIPING Install 4 inch white stripe and/or channelizing posts as noted. See details.
EXISTING SIGN ON EXISTING POST	FC FACE OF CURB			15 CURB PAINT Install curb paint. Color as noted.
	NEW SIGN ON NEW POST			
	NEW SIGN ON EXISTING POST			

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON	No.	DATE	BY	REFERENCE
DESIGNED BY	DEP				
DRAWN BY	DEP				

SIGNAGE AND STRIPING PLANS

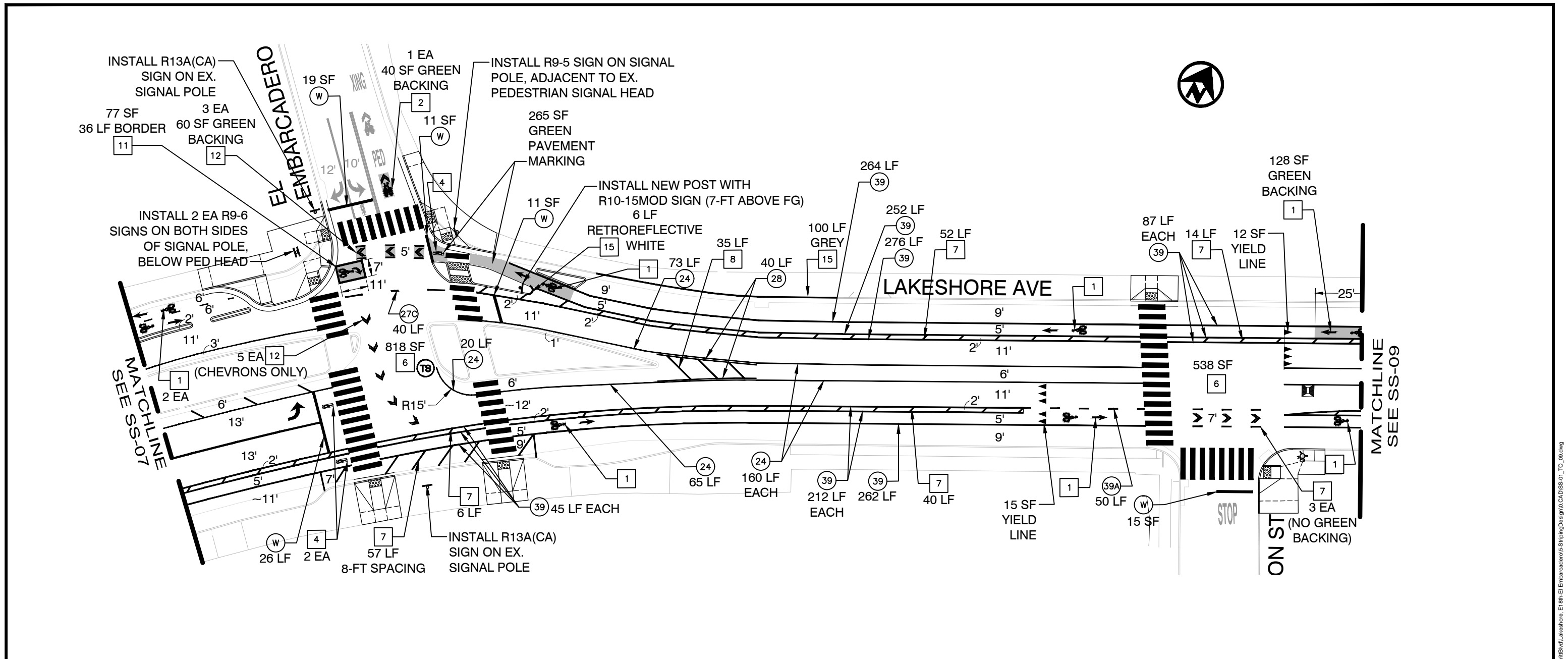
PROJECT NO.
1007299

SCALE: 1" = 40'

SHEET NO. SS-07
75 OF 83

DATE: 12/12/2024

DRAWING NAME: \\oakland\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-striping\LakeshoreAve-LakeMerritt\Bld\LakeShore_E18th-EI\Embarcadero5-StripingDesign\0.CAD\SS-01_TO_08.dwg
PLOT DATE: 12/12/24
PLOTTED BY: pene04



- LEGEND**
- # STRIPING CONSTRUCTION NOTE NUMBER
 - Ⓜ DETAIL NUMBER PER CALTRANS STD PLANS
Traffic lines as cardinal numeral
Arrows as roman numeral
 - Ⓣ TS TRAFFIC SIGNAL
 - Ⓜ CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)
 - STOP PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown
 - EXISTING SIGN ON EXISTING POST

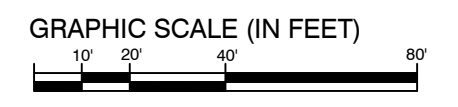
- Ⓜ FIRE HYDRANT
- Ⓜ BLUE FIRE HYDRANT MARKER
- EX EXISTING TO REMAIN rendered in grey
- LF LINEAR FEET
- SF SQUARE FEET
- CR CURB RETURN
- FC FACE OF CURB
- NEW SIGN ON NEW POST
- NEW SIGN ON EXISTING POST

- CONSTRUCTION NOTES**
- 1 BIKE LANE SYMBOL & ARROW
Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.
 - 2 SHARROW
Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.
 - 3 PARKING TEE
Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.
 - BIKE DETECTOR SYMBOL
Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.

- 5 RAISED BIKEWAY CROSSING MARKING
Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.
- 6 CONTINENTAL CROSSWALK
Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.
- 7 WHITE BUFFER STRIPING
Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.
- 8 YELLOW BUFFER STRIPING
Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.
- 9 REMOVE CONFLICTING STRIPING

- 10 REMOVE CONFLICTING MARKINGS
- 11 BIKE BOX OR 2-STAGE LEFT TURN BOX
Install bike box or 2-stage left turn box as noted on plan. See details.
- 12 BIKE INTERSECTION MARKINGS
Install bike intersection markings as noted. See detail.
- 13 SEPARATED BIKE LANE CENTERLINE STRIPING
Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
- 14 DRAINAGE FEATURE EDGELINE STRIPING
Install 4 inch white stripe and/or channelizing posts as noted. See details.
- 15 CURB PAINT
Install curb paint. Color as noted.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON	No.	DATE	BY	REFERENCE
DESIGNED BY	DEP				
DRAWN BY	DEP				

SIGNAGE AND STRIPING PLANS

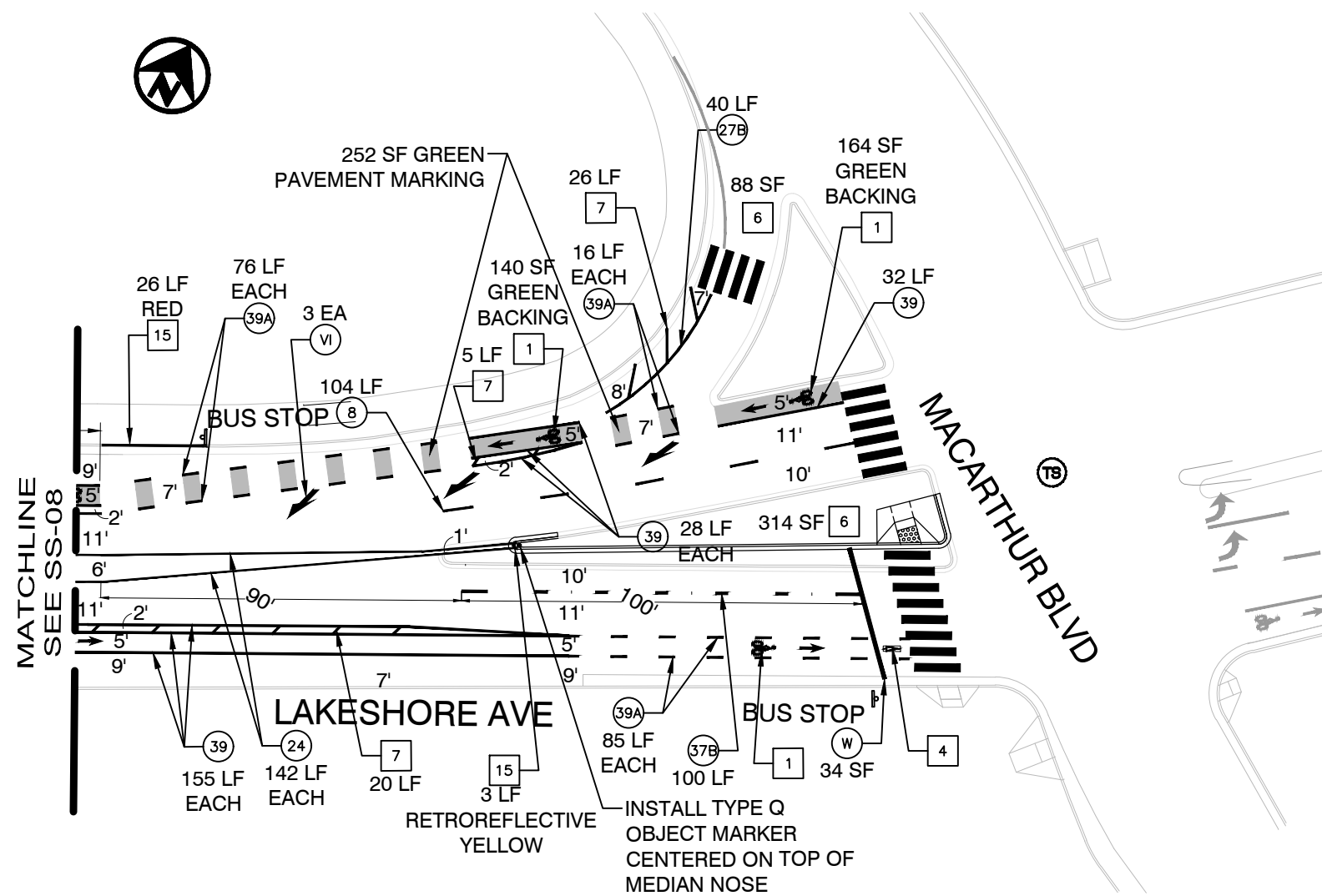
PROJECT NO.
1007299

SCALE:
1" = 40'

SHEET NO.
SS-08
76 OF 83

DATE: 12/12/2024

DRAWING NAME: \work\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-Striping\LakeshoreAve-LakeMerritt\Bld\Lakeshore_E18th\ElEmbarcadero\5-StripingDesign\CADD\SS-01_TO_08.dwg
PLOT DATE: 12/12/24
PLOT BY: pene04



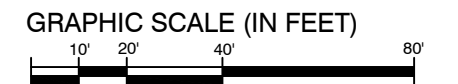
LEGEND

- # STRIPING CONSTRUCTION NOTE NUMBER
- Ⓢ DETAIL NUMBER PER CALTRANS STD PLANS
Traffic lines as cardinal number
Arrows as roman numeral
- Ⓣ TS TRAFFIC SIGNAL
- Ⓦ W CROSSWALK/LIMIT LINE (SOLID ONE FOOT WHITE LINE)
- PAVEMENT MARKING WORDS PER CALTRANS STD PLANS as shown
- EXISTING SIGN ON EXISTING POST
- ⚡ FIRE HYDRANT
- Ⓜ BLUE FIRE HYDRANT MARKER
- EX EXISTING TO REMAIN rendered in grey
- LF LINEAR FEET
- SF SQUARE FEET
- CR CURB RETURN
- FC FACE OF CURB
- NEW SIGN ON NEW POST
- NEW SIGN ON EXISTING POST

CONSTRUCTION NOTES

- 1 BIKE LANE SYMBOL & ARROW
Install bike lane symbol and arrow centered in bike lane, per CA MUTCD Figure 9C-3-B, as shown on plans.
- 2 SHARROW
Install shared roadway bicycle marking, per Caltrans Standard Plan A24C, as shown on plans.
- 3 PARKING TEE
Install short stem toward curb, distance measured from center of cross. Spacing between tees to be determined by engineer. See Detail.
- BIKE DETECTOR SYMBOL
4 Install bike detector pavement marking, per CA MUTCD Figure 9C-7.1 foot back from limit line and centered in lane, or as noted.
- 5 RAISED BIKEWAY CROSSING MARKING
Install speed table marking, centered in bike lane on near-side ramp, per CA MUTCD Figure 3B-30, Option A.
- 6 CONTINENTAL CROSSWALK
Install 2 foot stripes aligned with roadway centerline, spaced 4 foot on-center, white unless noted otherwise.
- 7 WHITE BUFFER STRIPING
Install 6 inch white striping every 15 feet at 45 degree angle, or as noted.
- 8 YELLOW BUFFER STRIPING
Install 6 inch yellow striping every 8 feet at 45 degree angle, or as noted.
- 9 REMOVE CONFLICTING STRIPING
- 10 REMOVE CONFLICTING MARKINGS
- 11 BIKE BOX OR 2-STAGE LEFT TURN BOX
Install bike box or 2-stage left turn box as noted on plan. See details.
- 12 BIKE INTERSECTION MARKINGS
Install bike intersection markings as noted. See detail.
- 13 SEPARATED BIKE LANE CENTERLINE STRIPING
Install 4 inch yellow striping per CA MUTCD Figure 9C-2, solid or skip pattern (9 foot gap, 3 foot stripe) as noted.
- 14 DRAINAGE FEATURE EDGELINE STRIPING
Install 4 inch white stripe and/or channelizing posts as noted. See details.
- 15 CURB PAINT
Install curb paint. Color as noted.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON	No.	DATE	BY	REFERENCE
DESIGNED BY	DEP				
DRAWN BY	DEP				

SIGNAGE AND STRIPING PLANS

PROJECT NO.
1007299

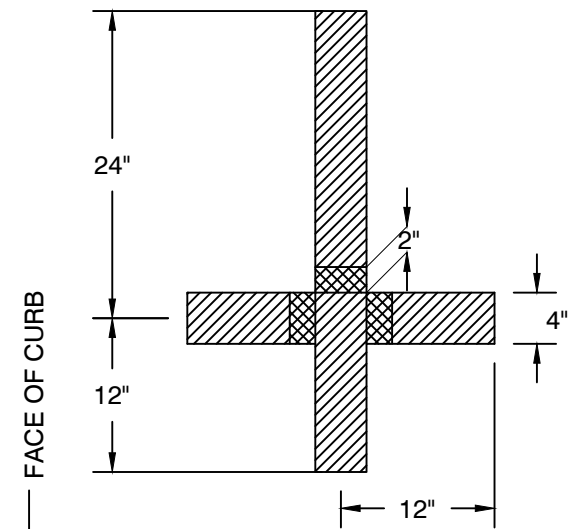
SCALE:
1" = 40'

SHEET NO.
SS-09
77 OF 83

DATE: 12/12/2024

DRAWING NAME: \\oakland\pwa\Transportation\Bike-Ped_Program\Projects\Bikeways-Striping\LakeshoreAve-LakeMerrittBld\Lakeshore_E18thEILakeshoreAve-StripingDesign\0.CADD\SS-01_TO_09.dwg
PLOT DATE: 12/12/24
PLOT BY: pene04

PARKING TEE DETAIL



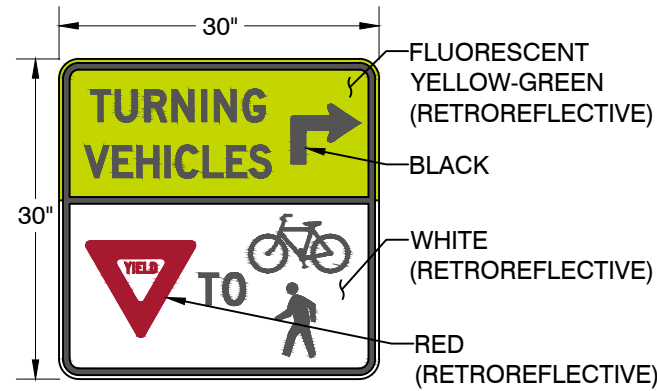
OPTIONAL 2" GAPS FOR STENCIL-BASED APPLICATIONS

20' (TYPICAL) 18' (MINIMUM) 22' (TYPICAL) 8' (TYPICAL) 7' (MINIMUM)

RED CURB

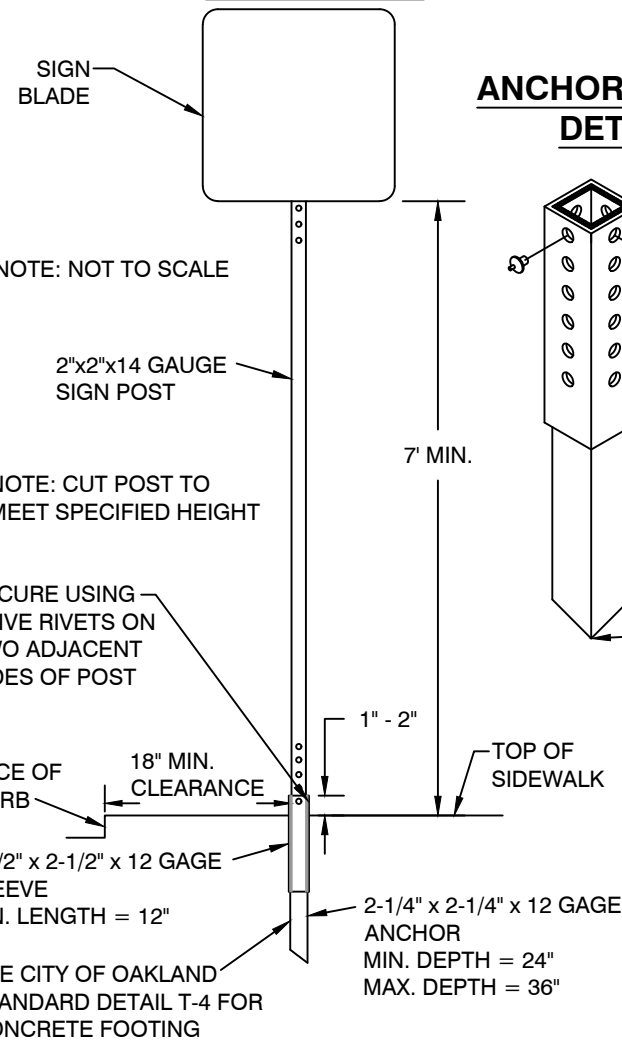
FACE OF CURB

R10-15MOD SIGN DETAIL



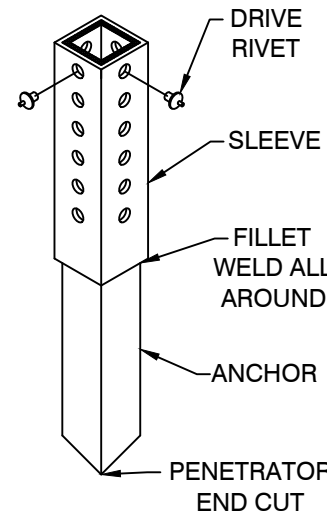
FLUORESCENT YELLOW-GREEN (RETROREFLECTIVE)
BLACK
WHITE (RETROREFLECTIVE)
RED (RETROREFLECTIVE)

STANDARD SIGN POST DETAIL



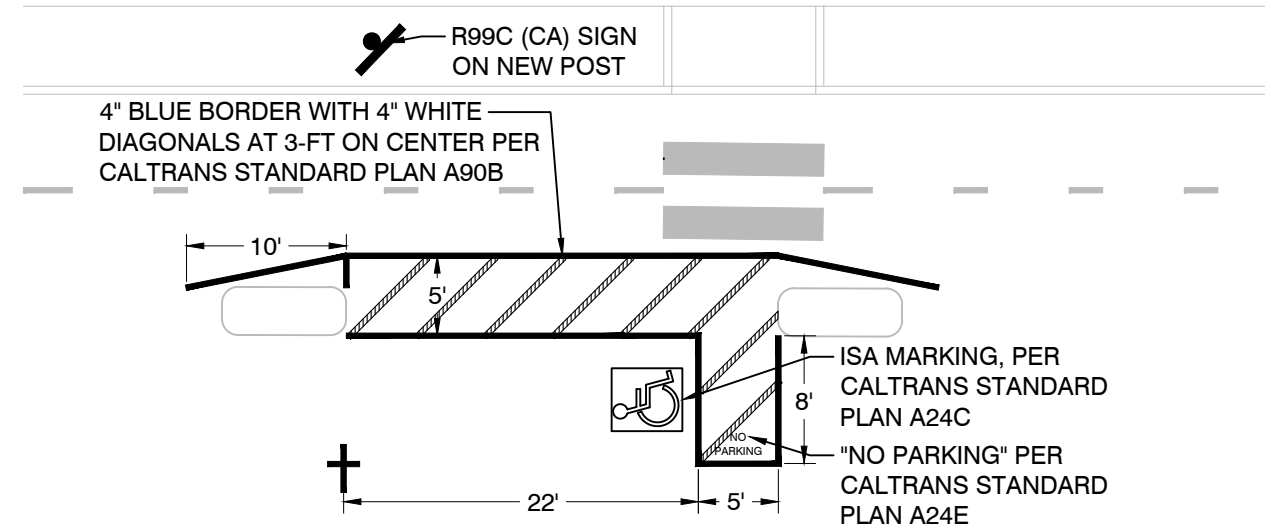
SIGN BLADE
NOTE: NOT TO SCALE
2"x2"x14 GAUGE SIGN POST
NOTE: CUT POST TO MEET SPECIFIED HEIGHT
7' MIN.
SECURE USING DRIVE RIVETS ON TWO ADJACENT SIDES OF POST
FACE OF CURB 18" MIN. CLEARANCE
2-1/2" x 2-1/2" x 12 GAGE SLEEVE MIN. LENGTH = 12"
SEE CITY OF OAKLAND STANDARD DETAIL T-4 FOR CONCRETE FOOTING
1" - 2"
TOP OF SIDEWALK
2-1/4" x 2-1/4" x 12 GAGE ANCHOR MIN. DEPTH = 24" MAX. DEPTH = 36"

ANCHOR/SLEEVE DETAIL



DRIVE RIVET
SLEEVE
FILLET WELD ALL AROUND
ANCHOR
PENETRATOR END CUT

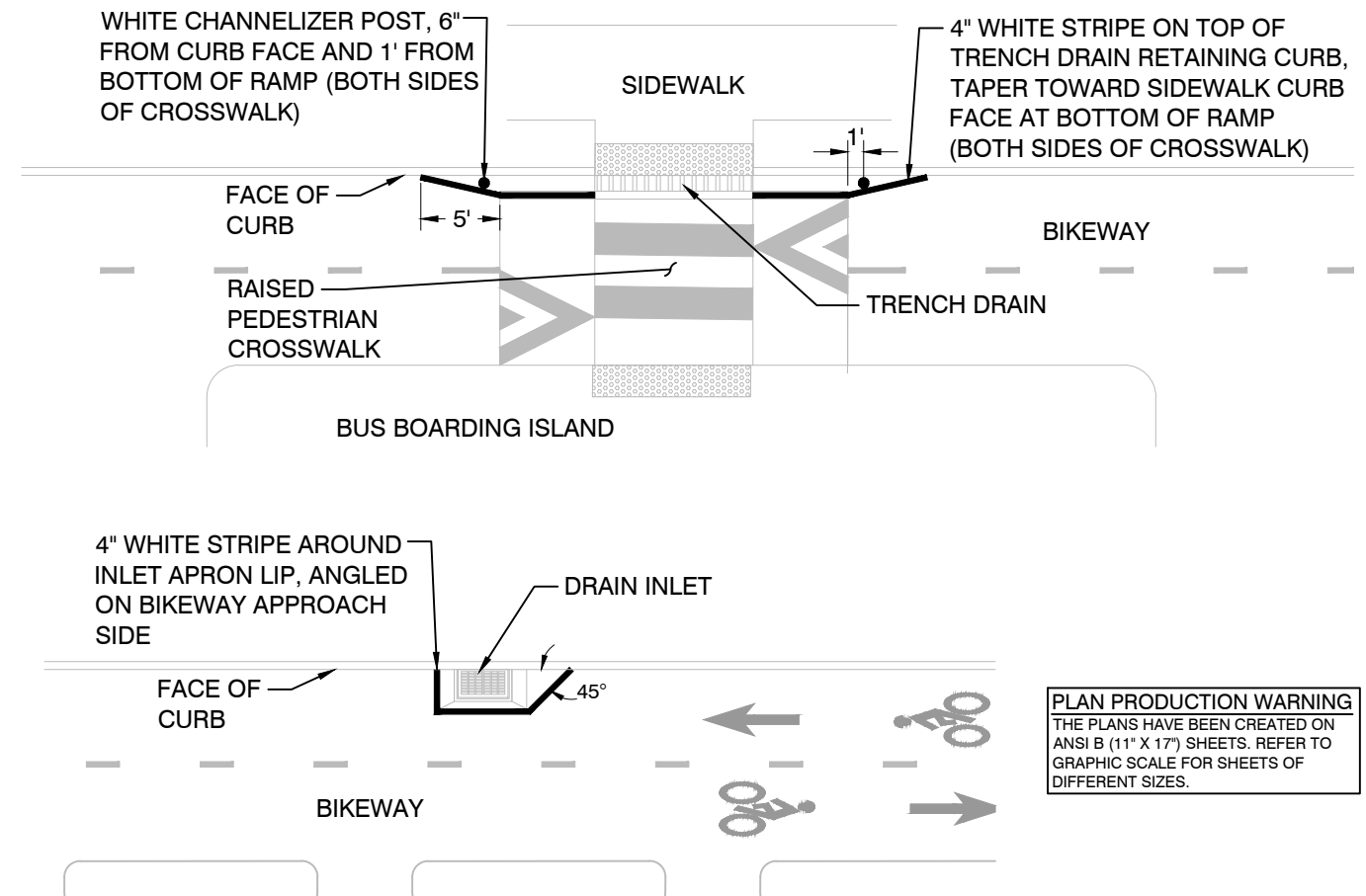
ACCESSIBLE PARKING STALL DETAIL



R99C (CA) SIGN ON NEW POST
4" BLUE BORDER WITH 4" WHITE DIAGONALS AT 3-FT ON CENTER PER CALTRANS STANDARD PLAN A90B

ISA MARKING, PER CALTRANS STANDARD PLAN A24C
"NO PARKING" PER CALTRANS STANDARD PLAN A24E

DRAINAGE FEATURE EDGE-LINE DETAILS



WHITE CHANNELIZER POST, 6" FROM CURB FACE AND 1' FROM BOTTOM OF RAMP (BOTH SIDES OF CROSSWALK)

4" WHITE STRIPE ON TOP OF TRENCH DRAIN RETAINING CURB, TAPER TOWARD SIDEWALK CURB FACE AT BOTTOM OF RAMP (BOTH SIDES OF CROSSWALK)

FACE OF CURB

SIDEWALK

BIKEWAY

RAISED PEDESTRIAN CROSSWALK

TRENCH DRAIN

BUS BOARDING ISLAND

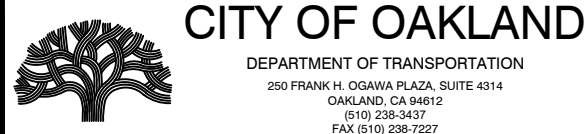
4" WHITE STRIPE AROUND INLET APRON LIP, ANGLED ON BIKEWAY APPROACH SIDE

DRAIN INLET

FACE OF CURB

BIKEWAY

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON
DESIGNED BY	DEP
DRAWN BY	DEP

No.	DATE	BY	REFERENCE

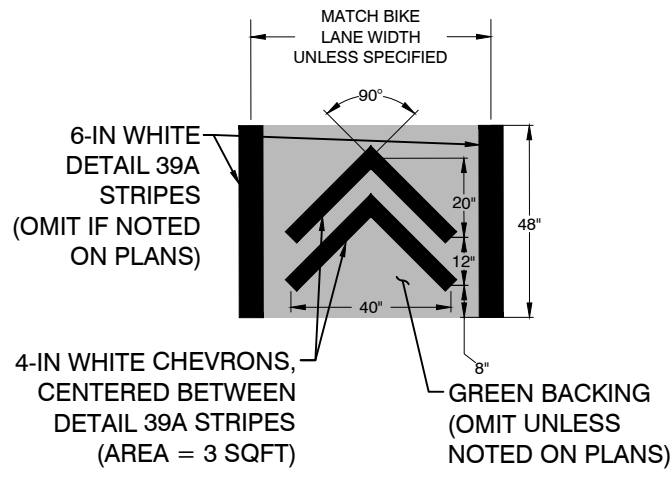
SIGNAGE AND STRIPING DETAILS

PROJECT NO.
1007299

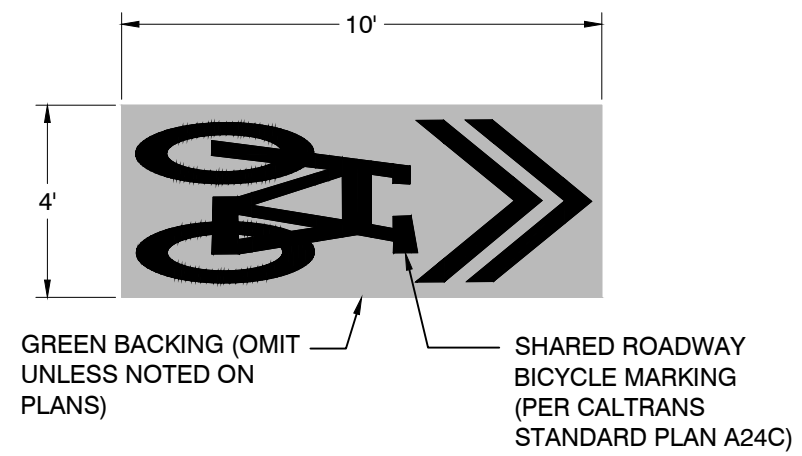
SCALE: NTS
DATE: 11/27/2024
SHEET NO. SSD-01
78 OF 83

DRAWING NAME: Z:\Bldg_Progam\Projects\Bikeways-striping\LakeshoreAve-LakeMerrittBldg\Lakeshore, E 18th-E Embarcadero\StripingDesign\10_CAD\LakeshoreAve_StripingDetails.dwg
PLOT DATE: 12-01-24
PLOT BY: Penah

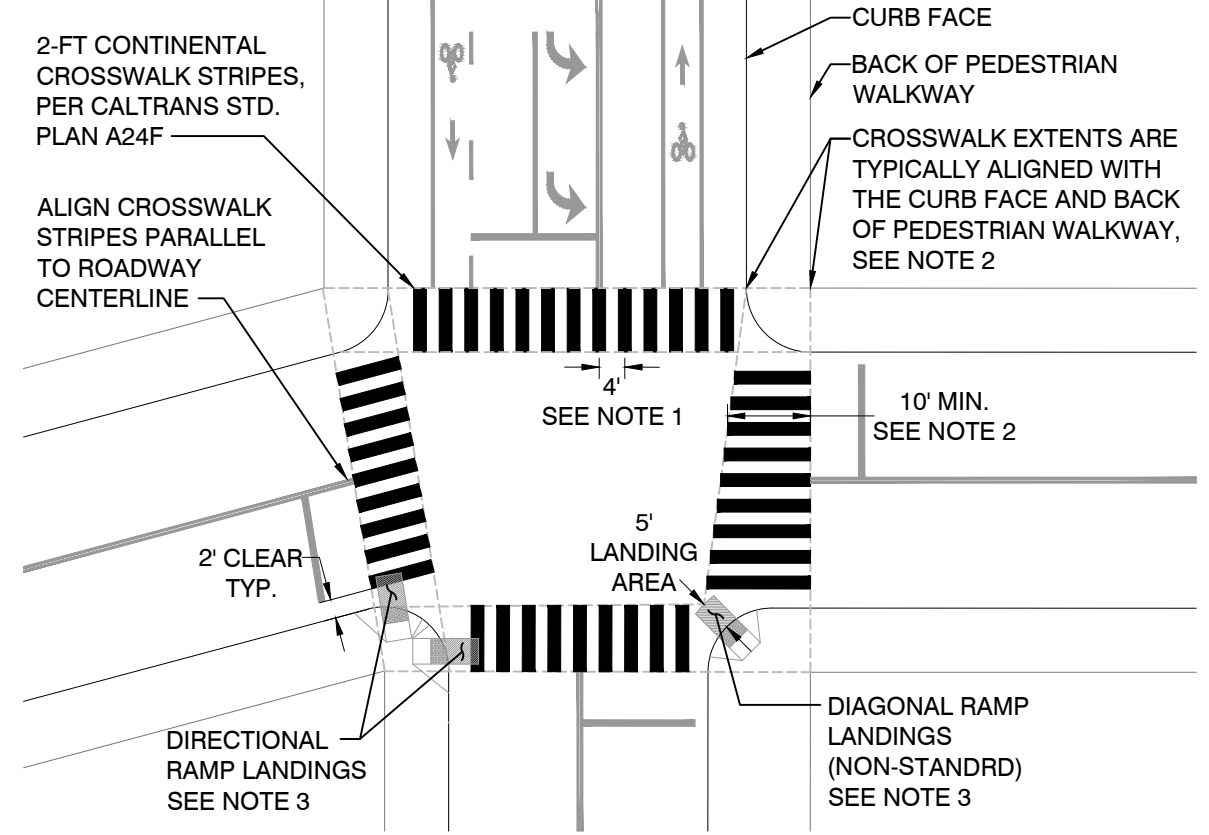
BIKE LANE INTERSECTION MARKING DETAILS



SHARROW SYMBOL DETAIL



CONTINENTAL CROSSWALK DETAIL

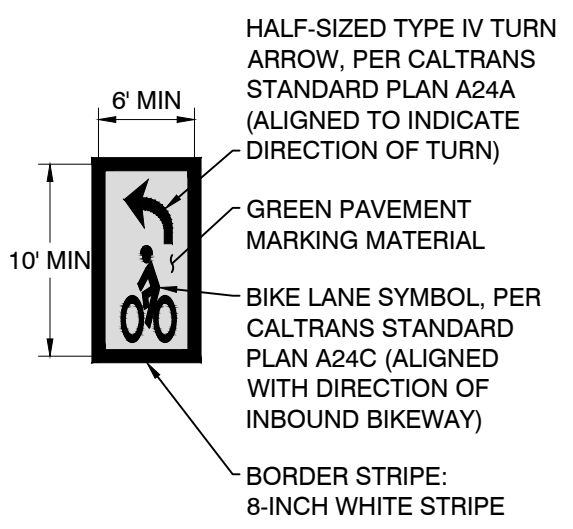


NOTES

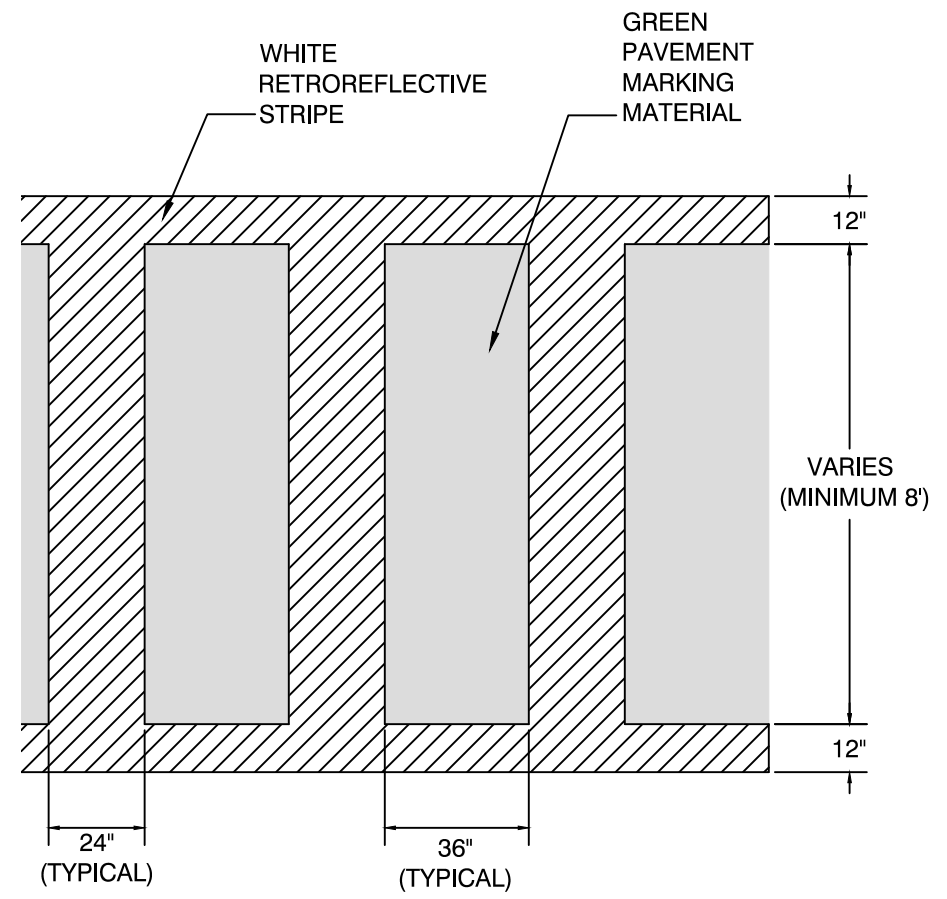
1. Continental crosswalk stripe spacing is 4 ft, on-center, unless otherwise specified. Align first crosswalk stripe with centerline of roadway and continue pattern toward edge of roadway, maintaining 2 ft clear from face of curb (if present).
2. Crosswalks should encompass the pedestrian walkways they connect. Unless otherwise specified, align the front of the crosswalk with the curb face at the corresponding curb returns. Align the back of the crosswalk with the back of the pedestrian walkway but no less than 10 ft back from the front of the crosswalk.
3. Crosswalks must encompass the curb ramp landing areas they connect. Directional ramps are the preferred standard, but where a single diagonal ramp serves both crosswalks at a corner, the front of the crosswalks may need to be shifted toward the intersection to encompass the entire 5' landing area at the bottom of the curb ramp.
4. See Detail RM-4A for supplemental crosswalk layout guidance.
5. At controlled approaches, install advanced limit lines with continental crosswalks, per Marking Detail RM-5.

PLAN PRODUCTION WARNING
 THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

TWO-STAGE TURN BOX DETAIL



BIKE/PED PATH CROSSWALK DETAIL



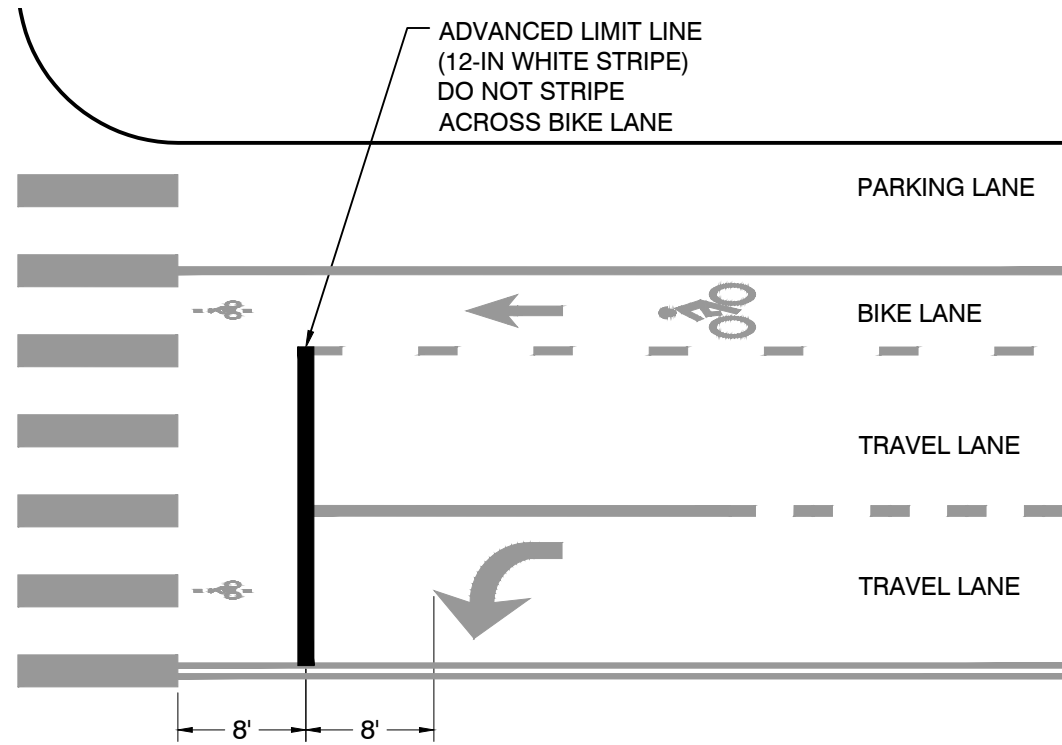
DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON
DESIGNED BY	DEP
DRAWN BY	DEP

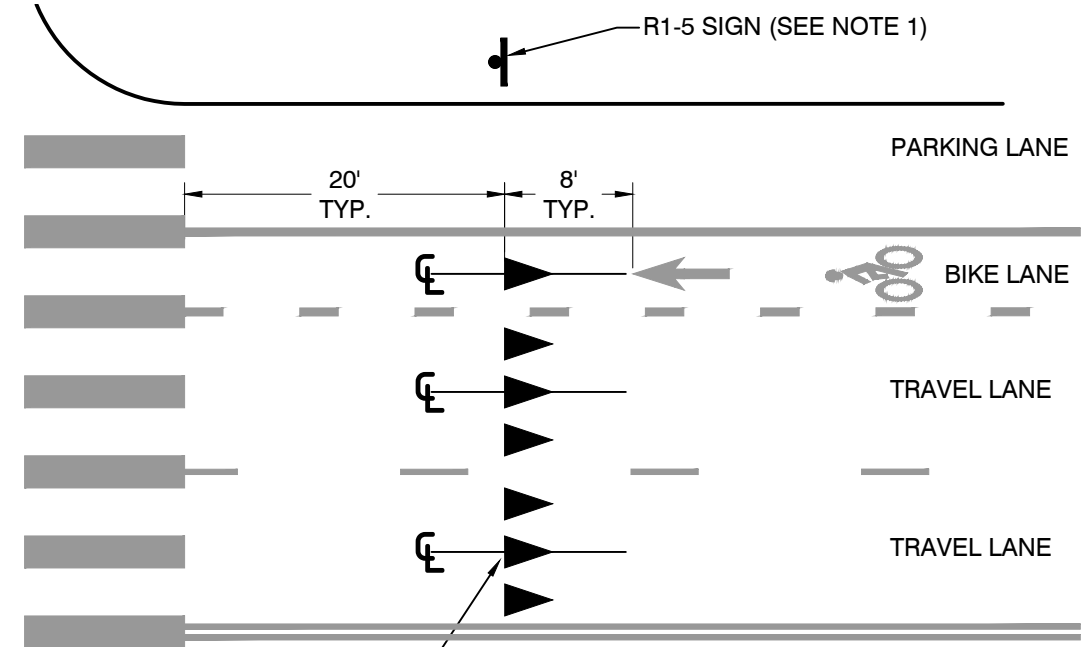
No.	DATE	BY	REFERENCE

DRAWING NAME: Z:\Bike-Ped_Program\Projects\Bikeways-striping\LakeShoreAve-LakeMerrittBldg\Lakeshore, E 18th\ET\Embarcadero\5-Striping\Design\10_CAD\LakeshoreAve_StripingDetails.dwg
 PLOT DATE: 12-01-24
 PLOTTED BY: Pen@oh

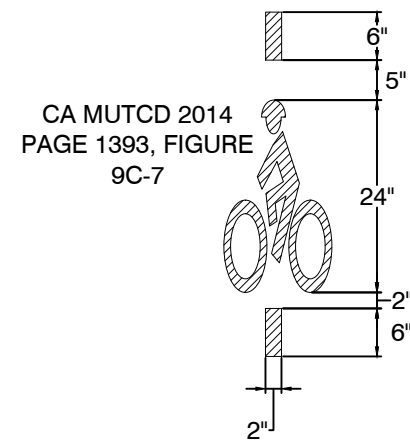
ADVANCED LIMIT LINE DETAIL



ADVANCED YIELD LINE DETAIL



BIKE DETECTOR SYMBOL DETAIL



INSTALL BIKE DETECTOR MARKING 1 FOOT BACK FROM CROSSWALK AND CENTERED IN LANE, OR AS NOTED

NOTES

1. INSTALL MUTCD R1-5 SIGN WITH ADVANCED YIELD LINE IF NOTED ON PLANS (AT UNCONTROLLED MULTILANE CROSSWALK APPROACHES)
2. ADVANCED YIELD LINES MAY ALSO BE INSTALLED (AS NOTED ON PLANS) AT UNCONTROLLED SINGLE LANE CROSSWALK APPROACHES.

PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT FROM E 18TH STREET TO MACARTHUR BLVD

DRAFT NOT FOR CONSTRUCTION

CHECKED BY	JASON PATTON	No.	DATE	BY	REFERENCE
DESIGNED BY	DEP				
DRAWN BY	DEP				

SIGNAGE AND STRIPING DETAILS

PROJECT NO.
1007299
SCALE: NTS
DATE: 11/27/2024
SHEET NO.
SSD-03
80 OF 83

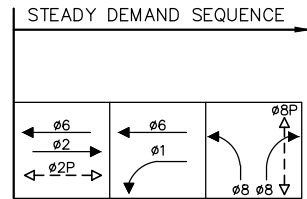
DRAWING NAME: Z:\Bldg-Ped_Program\Projects\Bikeways-striping\LakeshoreAve-LakeMerrittBldg\Lakeshore, E 18th St\Embarcadero\5-Striping\Design\10_CAD\LakeshoreAve_StripingDetails.dwg
PLOT DATE: 12-01-24
PLOTTED BY: Penobscot

GENERAL NOTES

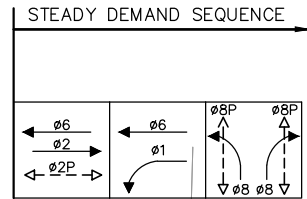
1. NOT ALL EXISTING EQUIPMENT, CONDUITS, AND PULL BOXES ARE SHOWN ON PLANS. PLANS ONLY SHOW TRAFFIC SIGNAL EQUIPMENT WHEN IMPACTED. EXISTING SIGNAL EQUIPMENT, CONDUITS, AND CONDUCTORS TO REMAIN, UNLESS OTHERWISE NOTED.
2. EXISTING SIGNAL PHASING AND SIGNAL CABLE CONDUCTOR PHASE ASSIGNMENTS DEVELOPED USING CITY PROVIDED AS-BUILTS. CONTRACTOR SHALL FIELD VERIFY EXISTING WIRING PRIOR TO CONSTRUCTION.
3. PROPOSED PULL BOXES SHALL BE CALTRANS NO. 5 UNLESS OTHERWISE NOTED ON PLANS.
4. CENTER OF PEDESTRIAN PUSH BUTTONS SHALL BE MOUNTED 3'-6" FROM SIDEWALK GRADE (TO CENTER OF BUTTON). MAXIMUM HORIZONTAL REACH FROM EDGE OF NEAREST SIDEWALK TO PUSH BUTTON SHALL BE 10". INSTALL APS ON EXTENSION BRACKET AS REQUIRED TO MEET 10" REACH REQUIREMENT.

CONSTRUCTION NOTES:

- 1 REMOVE EXISTING #2 PPB PEDESTRIAN PUSH BUTTON.
- 2 REPLACE EXISTING WITH NO. 6 PULL BOX.
- 3 REMOVE EXISTING #8 PPB.
- 4 REPROGRAM VIDEO DETECTION ZONES FOR NEW BIKE AND VEHICLE LANE CONFIGURATION.
- 5 INSTALL R9-5 SIGN ON POLE.



EXISTING PHASE DIAGRAM



PROPOSED PHASE DIAGRAM

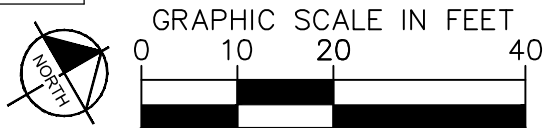
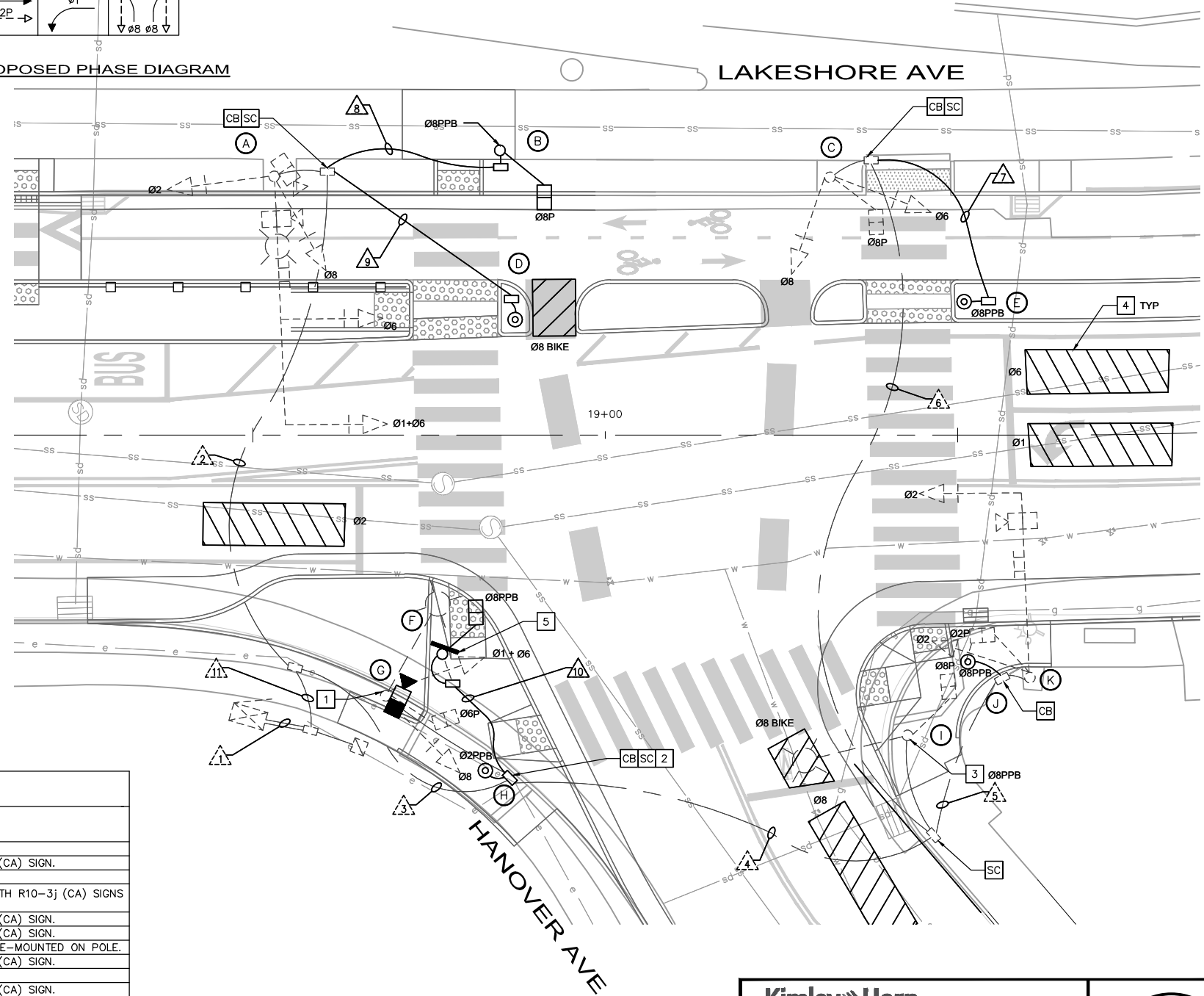
AWG	CONDUCTOR DESIGNATION	CONDUCTOR SCHEDULE										
		NUMBER OF CONDUCTORS										
		RUN NUMBER										
SIGNAL CABLE 12C/14TS	CABLE 'A'	1	1	1	1	1	1	1	1	1	1	1
	CABLE 'B'	1	1	1	1	1	1	1	1	1	1	1
	CABLE 'C'	1	1	1	1	1	1	1	1	1	1	1
	TOTAL SIGNAL CABLE	3	2	3	3	1	2		(1)		(1)	3
PPB	3C/#14 - #2P	2		2	1							2
	3C/#14 - #8P	(1)	(1)	1	1	1	1	(1)	(1)	(1)	(1)	(1)
	TOTAL PPB CABLE	4	(1)	3	2	1	1	(1)	(1)	(1)	(1)	4
VIDEO DETECTION	#1 & #6	1	1									1
	#8 (BIKE)	(1)	(1)	1	1	1						1
	TOTAL CABLES	3	2	1	1	1						3
NO. 8	240V LIGHTING	2	2	2								2
	GROUND	1	1	1	1	1	1	(1)	(1)	(1)	(1)	1
	TOTAL NO. 8	3	3	3	1	1	1	(1)	(1)	(1)	(1)	3
NO. 10	IISNS(120V)	2	2	2	2							2
	TOTAL NO. 10	2	2	2	2							2
NO. 6	FEEDER TO CONTROLLER (120V)	3										3
	TOTAL NO. 6	3										3
PERCENT FILL (%)		10%	17%	25%	23%	9%	12%	5%	8%	5%	3%	14%
CONDUIT SIZE		2-3"	3"	3"	3"	3"	3"	(2")	(3")	(2")	(3")	3"

() - DENOTES NEW

POLE AND EQUIPMENT SCHEDULE							
LOCATION	TYPE	STANDARD LUMINAIRE	MAST ARM	LED LUM WATTAGE	PED SIGNAL MOUNTING	APS ARROW	SPECIAL REQUIREMENTS
Ae	EX.	EX.	EX.	EX.	-	-	-
B	1-B (7')	-	-	-	TP-1-T	↔	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
Ce	EX.	-	-	-	-	↔	-
D	TYPE 2 (4'6")	-	-	-	-	↔	INSTALL DOUBLE-SIDED TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNALS (APS) WITH R10-3j (CA) SIGNS FOR USE BY CYCLISTS AND PEDESTRIANS
E	TYPE 2 (4'6")	-	-	-	-	↔	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
F	1-B (7')	-	-	-	TP-1-T	↔	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
Ge	EX.	EX.	EX.	EX.	-	-	REMOVE EXISTING APS FROM POLE G. INSTALL A VIDEO DETECTION CAMERA SIDE-MOUNTED ON POLE.
H	TYPE 2 (4'6")	-	-	-	-	↔	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
Ie	EX.	-	-	-	-	↔	REMOVE EXISTING #8 APS FROM POLE I. #2 APS TO REMAIN.
J	TYPE 2 (4'6")	-	-	-	-	↔	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
Ke	EX.	-	-	-	-	-	-

E - DENOTES EXISTING

100% SUBMITTAL NOT FOR BID OR CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 © 2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY TESSA KEMPER
DRAWN BY PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD

No.	DATE	BY	REFERENCE

ELECTRICAL PLANS

PROJECT NO. **1007299**
SCALE: AS SHOWN
SHEET NO. **E-01**
DATE: 12/11/2024
81 OF **83**

DRAWING NAME: K:\OAK_BDW\100708204 - Lakeshore Ave Cycle Trac\04_CADD Plan_Sheets\EXX.dwg
PLOT DATE: 12/12/24
PLOT BY: Priya Pallivathucal

GENERAL NOTES

1. NOT ALL EXISTING EQUIPMENT, CONDUITS, AND PULL BOXES ARE SHOWN ON PLANS. PLANS ONLY SHOW TRAFFIC SIGNAL EQUIPMENT WHEN IMPACTED. EXISTING SIGNAL EQUIPMENT, CONDUITS, AND CONDUCTORS TO REMAIN, UNLESS OTHERWISE NOTED.
2. EXISTING SIGNAL PHASING AND SIGNAL CABLE CONDUCTOR PHASE ASSIGNMENTS DEVELOPED USING CITY PROVIDED AS-BUILTS. CONTRACTOR SHALL FIELD VERIFY EXISTING WIRING PRIOR TO CONSTRUCTION.
3. PROPOSED PULL BOXES SHALL BE CALTRANS NO. 5 UNLESS OTHERWISE NOTED ON PLANS.
4. CENTER OF PEDESTRIAN PUSH BUTTONS SHALL BE MOUNTED 3'-6" FROM SIDEWALK GRADE (TO CENTER OF BUTTON). MAXIMUM HORIZONTAL REACH FROM EDGE OF NEAREST SIDEWALK TO PUSH BUTTON SHALL BE 10". INSTALL APS ON EXTENSION BRACKET AS REQUIRED TO MEET 10" REACH REQUIREMENT.

CONSTRUCTION NOTES:

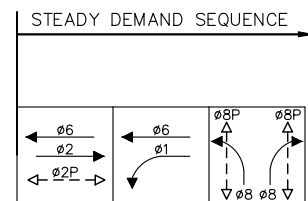
- 4 REPROGRAM VIDEO DETECTION ZONES FOR NEW BIKE AND VEHICLE LANE CONFIGURATION.
- 6 INSTALL APS ON NEW EXTENSION BRACKET SO BUTTON IS LESS THAN 10" FROM EDGE OF CURB RAMP.

AWG	CONDUCTOR SCHEDULE						
	CONDUCTOR DESIGNATION	NUMBER OF CONDUCTORS					
		RUN NUMBER					
		1	2	3	4	5	6
SIGNAL CABLE 12C/14TS	CABLE 'A'	1	1	1	1		
	CABLE 'B'	1	1	1	1		
	CABLE 'C'	1	1	1	1		
	TOTAL SIGNAL CABLE	3	1	2	1		
PPB	2C/#14 W/ GROUND - Ø8P	1	1	1	1	(1)	(1)
	TOTAL 2C/#14 CABLE	1	1	1	1	(1)	(1)
AUDIBLE PED. SIGNAL	3C/#14 W/ GROUND - AUDIBLE PED. SIGNAL	1	1	1	1		
	TOTAL 3C/#14 CABLE	1	1	1	1		
VIDEO DETECTION NO. 8	Ø1 & 6	1					
	Ø2	1	1	1	1		
	Ø8	1					
	TOTAL CABLES	3		1	1		
	240V LIGHTING	2	2	2	2		
	GROUND	1	1	1	1	(1)	(1)
	TOTAL NO. 8	3	3	3	3	(1)	(1)
	PERCENT FILL (%)	12%	9%	16%	16%	5%	5%
	CONDUIT SIZE	2-3"	3"	3"	3"	(2")	(2")

() - DENOTES NEW

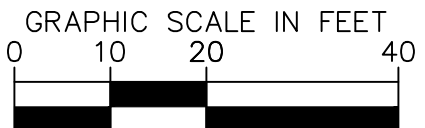
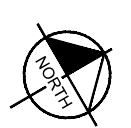
LOCATION	STANDARD				APS ARROW	SPECIAL REQUIREMENTS
	TYPE	LUMINAIRE	MAST ARM	LED LUMINAIRE WATTAGE		
A _e	EX.		EX.	EX.	←	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
B _e	EX.				←	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
C	TYPE 2 (4'6")				←	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
D	TYPE 2 (4'6")				←	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
E _e	EX.				←	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.
F _e	EX.		EX.	EX.	←	INSTALL NEW TOUCHLESS ACCESSIBLE PEDESTRIAN SIGNAL (APS) WITH R10-3j (CA) SIGN.

E - DENOTES EXISTING

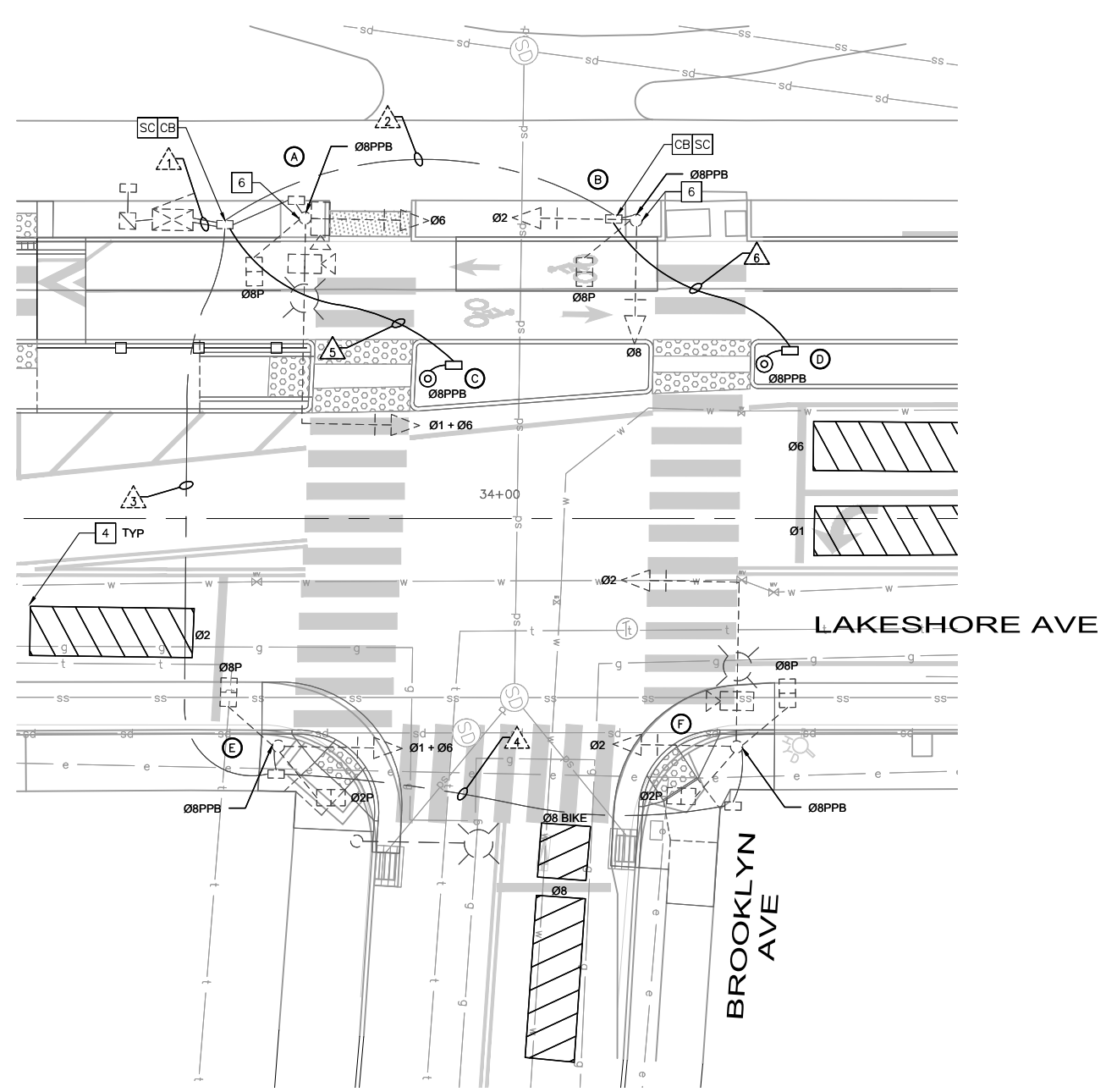


EXISTING PHASE DIAGRAM (TO REMAIN)

100% SUBMITTAL
NOT FOR BID OR
CONSTRUCTION



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.



Kimley-Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY TESSA KEMPER
DRAWN BY PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

ELECTRICAL PLANS

PROJECT NO.
1007299

SCALE: AS SHOWN
DATE: 12/11/2024

SHEET NO.
E-02
82 OF 83

DRAWING NAME: K:\OAK_R\DWG\100708204 - Lakeshore Ave Cycle Trac\04_CADD\Plan_Sheets\E-XX.dwg
PLOT DATE: 12/12/24
PLOTTED BY: Priya Pallivathucal

GENERAL NOTES

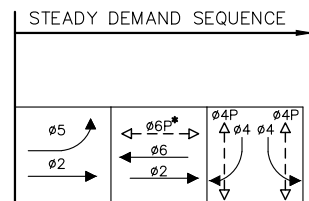
- NOT ALL EXISTING EQUIPMENT, CONDUITS, AND PULL BOXES ARE SHOWN ON PLANS. PLANS ONLY SHOW TRAFFIC SIGNAL EQUIPMENT WHEN IMPACTED. EXISTING SIGNAL EQUIPMENT, CONDUITS, AND CONDUCTORS TO REMAIN, UNLESS OTHERWISE NOTED.
- EXISTING SIGNAL PHASING AND SIGNAL CABLE CONDUCTOR PHASE ASSIGNMENTS DEVELOPED USING CITY PROVIDED AS-BUILTS. CONTRACTOR SHALL FIELD VERIFY EXISTING WIRING PRIOR TO CONSTRUCTION.
- PROPOSED PULL BOXES SHALL BE CALTRANS NO. 5 UNLESS OTHERWISE NOTED ON PLANS.
- CENTER OF PEDESTRIAN PUSH BUTTONS SHALL BE MOUNTED 3'-6" FROM SIDEWALK GRADE (TO CENTER OF BUTTON). MAXIMUM HORIZONTAL REACH FROM EDGE OF NEAREST SIDEWALK TO PUSH BUTTON SHALL BE 10". INSTALL APS ON EXTENSION BRACKET AS REQUIRED TO MEET 10" REACH REQUIREMENT.

CONSTRUCTION NOTES:

- REPROGRAM VIDEO DETECTION ZONES FOR NEW BIKE AND VEHICLE LANE CONFIGURATION.
- FURNISH AND INSTALL VIDEO DETECTION CAMERA ON EXISTING LUMINAIRE MAST ARM.
- REMOVE AND RELOCATE EXISTING APS ONTO POLE J.
- REMOVE AND RELOCATE EXISTING APS ONTO POLE K.

AWG	CONDUCTOR DESIGNATION	CONDUCTOR SCHEDULE																		
		NUMBER OF CONDUCTORS																		
		1	2	3	4	5	6	7	8	9	10									
SIGNAL CABLE 12C/14TS	CABLE 'A'	1	1	1	1															
	CABLE 'B'			1	1															
	CABLE 'C'					1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
	CABLE 'D'																			
	TOTAL SIGNAL CABLE	1	1	2	3	1	1	2	4	1										
PPB	3C/#14 - Ø6P	1	1	1	1	1	1	1	2	1										
	3C/#14 - Ø4P	1	1	2	2	1	1	1	3	1										
	TOTAL PPB CABLE	1	1	1	1	1	1	1	1	1										
VIDEO DETECTION	Ø2	1	1	1	1															
	Ø4					1	1	1	1											
	Ø6 & Ø1	1	1	1	1															
	Ø6 BIKE			(1)	(1)															
	TOTAL CABLES	2	2	3	3	2	2	2	6											
NO. 8	240V LIGHTING	2	2	2	2															
	GROUND	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
	TOTAL NO. 8	3	3	3	3	1	1	3	1	3	1	3	1	3	1	3	1	3	1	3
NO. 10	IISNS(120V)	2	2	2	2															
	TOTAL NO. 10	2	2	2	2															
NO. 6	FEEDER TO CONTROLLER (120V)																			2
	TOTAL NO. 6																			2
PERCENT FILL (%)		11%	11%	19%	18%	8%	9%	14%	12%	2%	1%									
CONDUIT SIZE		3"	3"	3"	3"	3"	3"	3"	2-3"	3"	3"									

() - DENOTES NEW

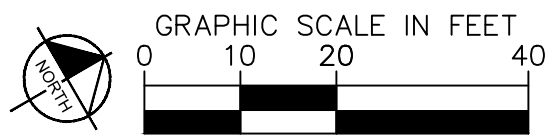
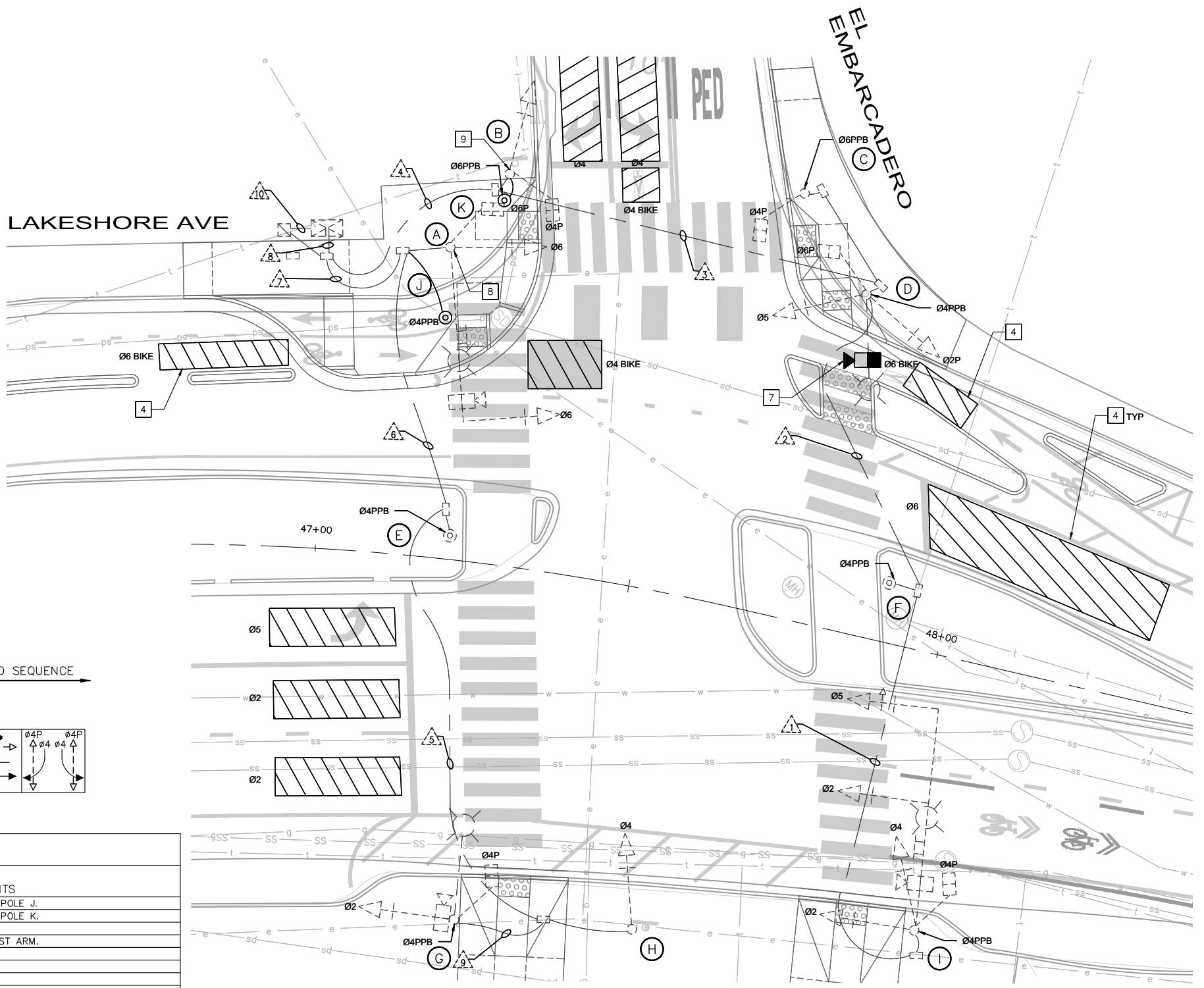


POLE AND EQUIPMENT SCHEDULE					
LOCATION	TYPE	STANDARD	LED LUMINAIRE WATTAGE	APS	SPECIAL REQUIREMENTS
A _e	EX.	EX.	EX.	-	REMOVE EX. APS FROM EX. POLE AND RELOCATE TO POLE J.
B _e	EX.	-	-	-	REMOVE EX. APS FROM EX. POLE AND RELOCATE TO POLE K.
C _e	EX.	-	-	-	-
D _e	EX.	EX.	EX.	→	INSTALL VIDEO DETECTION CAMERA ON LUMINAIRE MAST ARM.
E _e	EX.	-	-	→	-
F _e	EX.	-	-	→	-
G _e	EX.	EX.	EX.	→	-
H _e	EX.	-	-	→	-
I _e	EX.	EX.	EX.	→	-
J	TYPE 2 (4'6")	-	-	→	INSTALL RELOCATED EXISTING APS FROM POLE A.
K	TYPE 2 (4'6")	-	-	→	INSTALL RELOCATED EXISTING APS FROM POLE B.

E - DENOTES EXISTING

100% SUBMITTAL NOT FOR BID OR CONSTRUCTION

EXISTING PHASE DIAGRAM (TO REMAIN)
*Ø6 BIKE SHALL ACTIVATE Ø6



PLAN PRODUCTION WARNING
THE PLANS HAVE BEEN CREATED ON ANSI B (11" X 17") SHEETS. REFER TO GRAPHIC SCALE FOR SHEETS OF DIFFERENT SIZES.

Kimley-Horn
1300 Clay Street, Suite 900 Tel. No. (510)625-0712 ©2024
Oakland, California 94612 Fax No. (916)608-0885

CHECKED BY RYAN DOLE, P.E.
DESIGNED BY TESSA KEMPER
DRAWN BY PRIYA PALLIVATHUCAL



CITY OF OAKLAND
DEPARTMENT OF TRANSPORTATION
250 FRANK H. OGAWA PLAZA, SUITE 4314
OAKLAND, CA 94612
(510) 238-3437
FAX (510) 238-7227

**LAKESHORE AVENUE SEPARATED BIKE LANES PROJECT
FROM E 18TH STREET TO MACARTHUR BOULEVARD**

No.	DATE	BY	REFERENCE

ELECTRICAL PLANS

PROJECT NO.
1007299

SCALE: AS SHOWN
SHEET NO. E-03
83 OF 83
DATE: 12/11/2024

CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission
FROM: Frederick Morris III, Sports Coordinator, OPRYD
DATE: February 3, 2025
SUBJECT: REQUEST FOR THE PARKS AND RECREATION ADVISORY COMMISSION TO SUPPORT A GIFT-IN-PLACE DONATION IN THE AMOUNT NOT TO EXCEED TWENTY-NINE THOUSAND DOLLARS (\$29,000) TO RENOVATE LINDEN PARK BASKETBALL COURTS BY HOOP BUS INC. AND MISTER F.A.B. INC.

SUMMARY

This proposal outlines a scope of work, costs, and deliverables for Hoop Bus Incorporated, a 501(c)(3) nonprofit organization, to renovate and resurface the outdoor basketball court at Linden Park (4398 Linden Street.) in partnership with the City of Oakland Recreation and Youth Development (OPRYD) and Mister F.A.B. This court refurbishment and the courts' inauguration event are proposed to take place during Hoop Bus Show Bay Love initiation, scheduled for February 15, 2025, during the NBA's All-Star Weekend in the Bay Area. The proposed scope of work consists of structural and cosmetic aspects, similar to the project OPRYD presented at PRAC for the Rainbow Recreation outside basketball courts. This proposal is pivoting away from All-Star weekend due to logistical realities.

BACKGROUND / LEGISLATIVE HISTORY

It all began at Venice Beach, at the courts known as Hoopers Paradise, the home of West Coast Streetball. Countless tourists, players, and global citizens from around the world were captured by the experience of the Venice Basketball League, and the most common question became, "How can I bring this atmosphere to my city!" After a lifetime dedication to renovating courts and building hoops from any and everything, the founders of Venice Ball and more than 200 crowd-funders scraped together enough to purchase a 40' yellow school bus, put some hoops on it, and began to share it with communities across the country!

From a nationwide tour amplifying the Black Lives Matter movement to social justice campaigns with the Michelle Obama Foundation to climate protests, the Hoop Bus has remained dedicated to translating differences through the simple connective powers of a communal basketball rim.

As of November 2024, a fleet of four Hoop Buses is spread out across the country. They are continuing to bring basketball to where it's needed most and reach unexpected places to spread the love of the game. Hoop Bus originated in 2019 as a vehicle for change and a powerful tool for advocacy. We've since grown into an international non-profit with a fleet of Hoop Buses serving the United States and Mexico City, with additional programming coming to France and the Philippines in 2025.

Hoop Bus Incorporated's mission is to build community and spread love through the game of basketball. Hoop Bus is committed to bringing the game where it's needed most. In 2023, Hoop Buses' impact was measured through four (4) renovated basketball courts, and youth programs run in

more than. Hoop Bus Incorporated's goals are to break down social barriers to stimulate the community, to inspire kids and adults of all ages to get outside and play basketball, and to educate the next generation through targeted programs that teach life skills through the lens of basketball.

Hoop Bus Incorporated's impact is measured through implementing Hoop Bus programs, which serve our mission in three impact pillars.

Youth Programming: Hosting events, assemblies, and developmental programs that inspire and educate youth nationwide.

- Programs: PE Takeover, Hoop Bus University, Best Day in the Life, Community Pop-Ups

Area Beautification: Providing clean, safe spaces to play basketball is essential to providing underserved kids access to healthy and free developmental recreational experiences.

- Program: Court Refurbishments

Resource Distribution: Mobilizing our partners, volunteers, and ambassadors to provide essential resources, including basketballs, sports equipment, sneakers, and meals for in-need communities

- Programs: Serve the Block, The City, The Neighborhood, The World Hoop Sleigh.

OPRYD has consulted with Zoning and Planning to see if this project triggers a minor Conditional Use Permit (CUP). Since this is a repave of the existing court/surface, it will not require a CUP or any other permit.

PROJECT / PROGRAM DESCRIPTION

Linden Park Basketball Court is located at 4398 Linden Street in the North Oakland district of the City of Oakland. It will be renovated by the group's Hoop Bus and Mister F.A.B. Inc. Residents and park users developed a plan to bring positive needed activity space into their neighborhood and accommodate community events. The project scope consists of:

- Resurfacing and repairing cracks on the court.
- Replacing plexiglass backboards.
- Installing backboard padding.

FISCAL IMPACT

There is no fiscal impact on the City of Oakland for this project. The Hoop Bus Inc., Mister F.A.B Inc., and its vendor will finance and provide court resurfacing for the project. This includes all costs for cleaning litter, paint, and supplies required for the project. OPRYD does not anticipate extra maintenance costs for the upgraded Linden Basketball court.

PUBLIC OUTREACH / INTEREST

Mister F.A.B. hosts annual Christmas toy drives, back-to-school supplies drives, Easter celebrations, and turkey drives. Through these programs, Mr. F.A.B. has fed, clothed, and supplied over 20,000 youth and adults in this community. The Warriors and Steph Curry have attended the celebration and basketball games at Linden Park for the past four years. At these various community events over the years, the community expressed interest in resurfacing this court. This opportunity will meet the community's needs by providing a safe space for future generations.

COORDINATION

OPRYD has contacted Oakland Public Works to get a response to this proposal. The response is as follows:

- City of Oakland Public Works- Facilities Services Division supports the process for the resurfacing.
- The City of Oakland Bureau of Planning considers court repaving a repair and does not trigger a Conditional Use Permit (CUP).

RECOMMENDATION

Staff recommends that the Park and Recreation Advisory Commission (PRAC) support the gift-in-place donation of \$29,000 to repair the Linden Park Basketball court renovation and replace two basketball backboards.

Respectfully Submitted,

Frederick Morris

Frederick Morris (Feb 6, 2025 20:48 PST)

Prepared by:

Frederick Morris III

Sports Coordinator, OPRYD

Justice Bolden

Justice Bolden (Feb 6, 2025 22:47 PST)

Reviewed by:

Justice Bolden

Recreation General Supervisor, OPRYD

Fred Kelley

Fred Kelley (Feb 7, 2025 06:03 PST)

Approved by:

Fred Kelley

Director, OPRYD

Identification of Support Documents:

Attachments: Exhibit A – *Current Condition of Linden Park Basketball Court*
Exhibit B – *Proposed HOOP Bus Basketball Court Scope of Work*
Exhibit C – *Proposed Hoop Bus Basketball Court Design Concept*
Exhibit D – *Appendix*

EXHIBIT A – Current Conditions of Linden Park Basketball Court

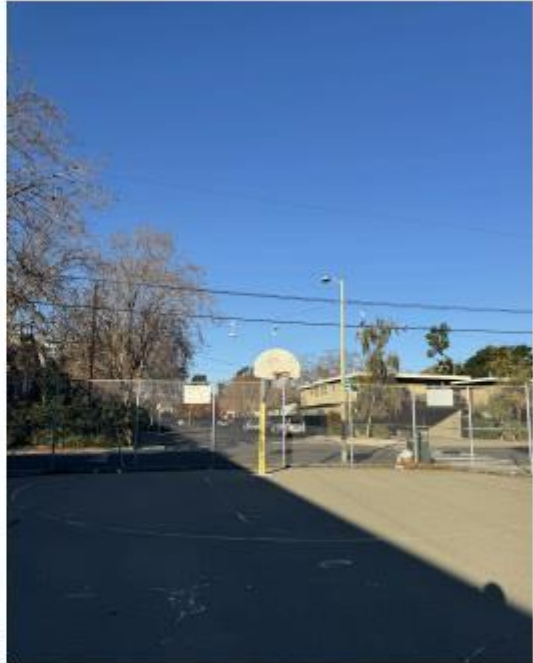


EXHIBIT B – Proposed HOOP Bus Basketball Court Scope of Work

Resurfacing

- Refers to the structural process of making the court more playable, primarily through applying crack fillers and layers of prime coat over the court surface.

Muraling

- Refers to the cosmetic process of creating a basketball court as community art, including working with a local artist who represents the community to design and lead a team of installers to finalize the art on the court.

Equipment Upgrades

- At the base level, the rims will be upgraded to four-way flex breakaway rims, budget-permitting, the backboards can be upgraded with True Bounce structures.
- Court Inauguration Community Event
- An inclusive community event to celebrate the grand opening of the court.

Estimated Costs

Resurfacing - \$6,000

- Crack repair of existing court, resurfacing, and prime coat application.

Paint, supplies and materials - \$8,000

- Street bond asphalt coating in specific project colors
- Color coating for art enhancement
- Additional materials (brushes, rollers, etc.) necessary for installation
- New break-away rims

Artist commission & design - \$3,000

- An artist hired to work with Hoop Bus and the city of Oakland to develop and install a unique artist direction for the space.
- Graphic design of concepts
- Leadership of labor and installation of creative
- See below for proposed artists and design concepts

Labor - \$7,500

- Facilitate the proper application of the decided creative to the surface

Travel & lodging - \$2,500

- Flights, hotels, and food per diem for artists and project manage

Project management - \$2,000

- Participate in meetings, calls, and other communication to facilitate the installation.
- Identify and work with an artist to create a unique work of art for the space.
- On-site oversight, ensuring the work is completed to specifications.

Plexiglass backboards - \$5,000 (optional)

- True Bounce backboards are the best in the industry. They are plexiglass with holes that release the ball's energy impact, resulting in a better bounce and often leading to truer rebounding and scoring.
- Backboards feature many properties that enable them to withstand wear and tear from weather, graffiti, and heavy use in an outdoor setting.
- Installation included

Grand Opening- \$3,500 (optional)

- Programming including
- DJ and sound
- T-shirts and jerseys (x150)
- Street shutdown for Hoopbus
- Bounce House
- Refs + Staff

Annual Programming: \$10,000 (optional)

- An investment in an annual program led by Hoop Bus to ensure a self-functioning youth league to serve the community
- Inclusive of maintenance of the court

Summary

Line	Cost
Resurfacing	\$6,000
Paint, supplies, and materials	\$8,000
Artist commission and design	\$3,000
Labor	\$7,500



Travel & lodging	\$2,500
Project management	\$2,000
Grand Total	29,000
Grand Opening	\$3,500
True Bounce Backboards (optional)	\$5,000
Annual Programming: Youth League	\$10,000
Optional Additional Costs	\$18,500

Exhibit C - Proposed Hoop Bus Basketball Court Design Concept

See below for the following concept of court renovations to show court art's vibrancy and creative directions.



EXHIBIT D – Appendix

Hoop Bus Impact

Our impact is measured by implementing Hoop Bus programs, which serve our mission in three impact pillars.

1. **Youth Programming:** Hosting events, assemblies, and developmental programs that inspire and educate youth nationwide.
Programs: PE Takeover, Hoop Bus University, Best Day in the Life, Community Pop-Ups
2. **Area Beautification:** Providing clean, safe spaces to play basketball is essential to providing underserved kids access to healthy and free developmental recreational experiences.
Program: Court Refurbishments
3. **Resource Distribution:** Mobilize our partners, volunteers, and ambassadors to provide essential resources, including basketballs, sports equipment, sneakers, and meals for in-need communities.

History

Hoop Bus originated in 2019 as a vehicle for change and a powerful tool for advocacy. We've since grown into an international non-profit with a fleet of Hoop Buses serving the United States and Mexico City, with additional programming coming to France and the Philippines in 2025. We bring the game of basketball to the communities that need it the most to inspire the next generation, teach life skills through basketball, promote physical activity, and create unity in a world divided. In 2023, our impact was measured through four (4) renovated basketball courts; youth programs run in more than fifty (50) cities reaching more than one hundred (100) schools, and by engaging over 320,000 people in person and more than 100 million online through social media reach.





CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission
FROM: Khalilha Haynes, Planner III, Strategic Planning Division, Planning and Building Department
DATE: February 6, 2025
SUBJECT: REQUEST FOR PARKS AND RECREATION ADVISORY COMMISSION TO RECEIVE AN INFORMATIONAL REPORT ON THE CITY OF OAKLAND'S GENERAL PLAN UPDATE PROCESS.

SUMMARY

The City of Oakland Planning and Building Department (PBD), in coordination with other City departments, is leading a citywide process to update the Oakland General Plan in two phases.

Phase 2 of the General Plan Update (GPU) launched in September 2024, and as part of the GPU process thus far, staff conducted a survey that asked community members to share how they use their neighborhood parks and what improvements they would like to see at parks throughout the city.

This report provides an overview of Phase 2 of the General Plan Update and a high-level summary of the GPU Park Survey findings.

BACKGROUND / LEGISLATIVE HISTORY

The City of Oakland is [updating its General Plan in two phases](#). Phase 1 of the GPU began in November 2021 and included development of the Housing Element (adopted January 2023) and the Environmental Justice (EJ) and Safety Elements (adopted in September 2023). PBD staff presented and discussed the GPU Phase 1 with the PRAC in February 2022. Phase 2 began in September 2024 and focuses on the Land Use and Transportation Element (LUTE), Open Space, Conservation, and Recreation (OSCAR) Element, Noise Element, and the Infrastructure and Capital Facilities Element.

The GPU is an exciting opportunity for the Oakland community to help plan the city's future. Through the General Plan, we aim to create livable and connected neighborhoods where people have access to everything they need, such as grocery stores, entertainment, stores, libraries, parks, and safe transportation.

PROJECT / PROGRAM DESCRIPTION

In August 2024, staff launched the GPU Park Survey, which sought to understand better how community members are using their local parks. The information gained from the GPU Park Survey will be used to inform the development of the land use, open space, and transportation alternatives that will then guide the update to the OSCAR Element. The survey was published in English, Spanish, and Chinese and stayed open for one month, receiving 597 responses. The survey included 13 questions related to 1) park accessibility, 2) park amenities and usage, and 3) community perspectives on parks and visions for Oakland parks system. There were eight demographic questions related to age, race and ethnicity, gender, language, zip code, and connection to Oakland.

The [GPU Park Survey Report](#) (*see Exhibit A*) details the findings of the survey and provides an overview of the methodology and next steps. Some key takeaways from the survey are:

1. People are visiting parks at least once per week, with many going one to three times per week.
2. While residents mainly visit parks via car, there is desire for alternative transportation options via transportation methods and public transit.
3. The most used amenities at parks are 1) walkways and trails, 2) basketball courts, and 3) flexible green space.
4. The biggest desired improvements to Oakland parks are 1) improved maintenance, 2) improved access to restrooms, and 3) increased clean-up of litter and illegal dumping.
5. People see parks as spaces of community connection and belonging and as spaces that promote community health through outdoor recreation and access to green space.
6. People recognize unequal access to green space across the city.
7. There are new and emerging community desires that require new infrastructure, two key examples were a roller rink at DeFremery Park and pickleball courts at Bushrod Park.

As a complement to the survey, City staff conducted site visits at 22 parks across the city between July and August of 2024, focusing on parks in Environmental Justice communities. The goal of the site visits was to gain a better understanding of the state of park amenities and assess their use by park users to inform the development of the OSCAR. Findings from the site visits are available on the General Plan Update's [OSCAR webpage](#).

FISCAL IMPACT

Since this report is informational only, no fiscal impacts are included.

PUBLIC OUTREACH / INTEREST

The survey was shared using a targeted distribution approach. It was directly shared with the directors or programmatic staff at each [Recreation Center in the City of Oakland](#). Additionally, the survey was shared at community events, with community partners, and with the Oakland Parks and Recreation Foundation, which included it in their monthly newsletter.

COORDINATION

Planning staff coordinated with staff in the Oakland Parks, Recreation, and Youth Development (OPRYD) and Oakland Public Works (OPW) staff to develop the questions in the survey and the scope of the site visits. Findings from the survey and site visit were shared with OPW and OPRYD staff to help inform OPW's parks assessment project. Staff from OPRYD, OPW, and other relevant departments meet regularly to discuss progress on the GPU Phase 2.

RECOMMENDATION

Staff recommends that the Park and Recreation Advisory Commission receive this informational Report and provide feedback on their priorities related to the major topics of the GPU Phase 2 (see questions in the presentation in *Exhibit B*).

Respectfully Submitted,



Prepared by:

Khalilha Haynes

Planner III, Strategic Planning Division, Planning and Building Department

Lakshmi Rajagopalan

Lakshmi Rajagopalan (Feb 6, 2025 09:33 PST)

Reviewed by:

Lakshmi Rajagopalan

Planner IV, Strategic Planning Division, Planning and Building Department

Laura B. Kaminski

Laura Kaminski (Feb 6, 2025 09:38 PST)

Approved by:

Laura Kaminski

Strategic Planning Manager, Strategic Planning Division, Planning and Building Department

Identification of Support Documents:

Attachments: Exhibit A – *GPU Park Survey Report*

Exhibit B – *GPU Phase 2 Presentation*

EXHIBIT A

General Plan Update Phase 2 – Oakland Park Survey Report

January 21, 2025

Authors: Bebe LeGardeur, Khalilha Haynes, Mariah Padilla

I. Introduction

The City of Oakland is currently working on [updating its General Plan](#). As part of the General Plan Update process, the City wanted to better understand how people use Oakland's parks. We created a survey that asked community members to share how they use their neighborhood parks and what improvements they would like to see at parks throughout the city. The information gathered from this survey will inform the development of the General Plan's land use, open space, and transportation alternatives, and eventually an update to the Open Space, Conservation, and Recreation (OSCAR) Element. As a complement to the survey, City staff conducted site visits at 22 parks across the city between July and August of 2024, focusing on parks in Environmental Justice communities. The goal of the site visits was to gain a better understanding of the state of park amenities and assess their use by park users to inform the development of the OSCAR. Findings from the site visits are available on the General Plan Update's [OSCAR webpage](#).

The survey was open from August 20, 2024, to September 20, 2024, and was published in English, Spanish, and Chinese. We received 597 responses total, 571 in English, 22 in Chinese, and three in Spanish. The survey included 13 questions related to 1) park accessibility, 2) park amenities and usage, and 3) community perspectives on parks and visions for Oakland parks system. There were eight demographic questions related to age, race and ethnicity, gender, language, zip code, and connection to Oakland. This report outlines the methodology process, analyzes the survey results, describes main themes that emerged from the survey's long-form questions, and concludes with next steps.

II. Methodology

The survey was shared using a targeted distribution approach. It was directly shared with the directors or programmatic staff at each [Recreation Center in the City of Oakland](#). Additionally, the survey was shared with the Oakland Parks and Recreation Foundation, which included it in their monthly newsletter. Through the targeted outreach, individuals were asked to share it with their networks and were provided a link to the digital survey, copies of the physical survey to print, and a QR code to share broadly. All survey materials were provided in English, Spanish, and Chinese to help ensure accessibility and a wide range of responses. The survey was also distributed at in-person events in tandem with the General Plan Update Phase 2 community engagement process.

The survey questions were analyzed using both qualitative and quantitative methods. Qualitative analysis was conducted on questions that encouraged survey respondents to provide short responses sharing their perspectives and lived experience. In addition, quantitative analysis was conducted on multiple choice questions in which respondents had to select an answer from a pre-determined list of answer options.

From the qualitative analysis, there emerged several recurring themes reflecting the community relation to and perception of parks – parks as spaces of community connection and belonging, outdoor recreation and community health, uneven access to green space, park maintenance, improvement, and safety, and evolving community desires that require new infrastructure.

III. Survey Results

At the beginning of the survey, respondents were asked to name the park that they wanted to focus on throughout the survey. There were 54 unique parks mentioned, and the most popular ones were:

- 1) Joaquin Miller Park (168 responses)
- 2) DeFremery Park (129 responses)
- 3) Bushrod Park (61 responses)
- 4) Lake Merritt (40 responses)
- 5) Mosswood Park (22 responses)

There were 22 responses where the park name or intersection entered was unclear.

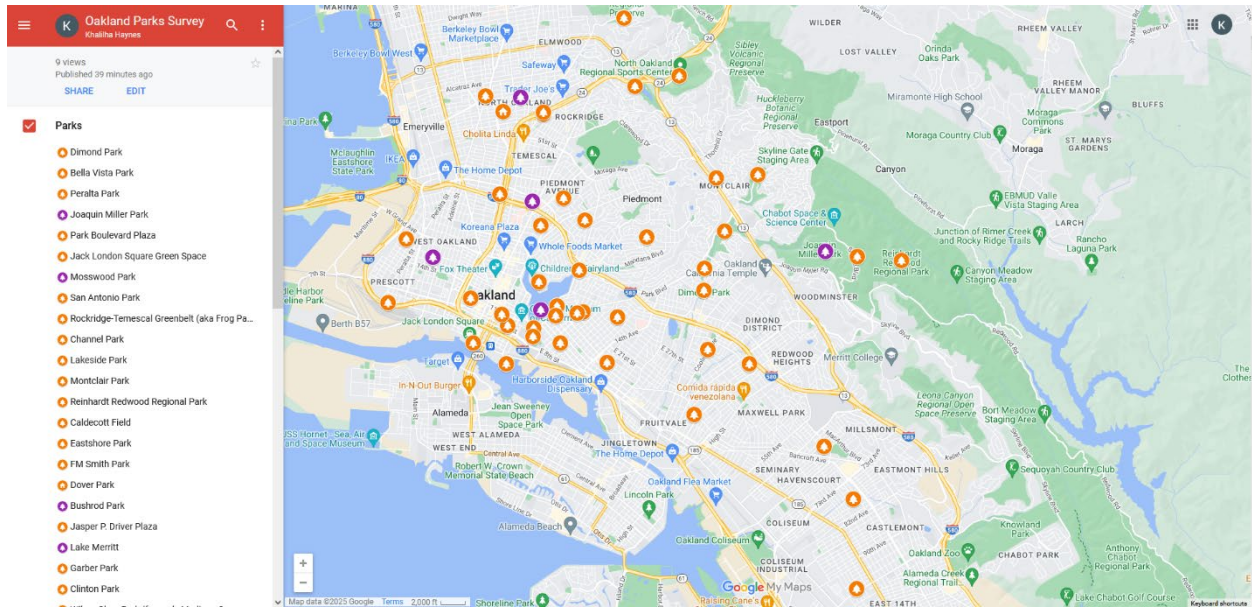


Figure 1: Map of Parks Mentioned. Top 5 visited parks shown in purple. View full map: https://www.google.com/maps/d/edit?mid=1DWm-PFcg_8MM0OF3ezNiU9qOftUGyXU&usp=sharing

Table 1: Full list of Parks Mentioned

Parks Referenced	Count
Joaquin Miller Park	168
DeFremery Park	129
Bushrod Park	61
Lake Merritt	40
Mosswood Park	22

Unknown	22
Dimond Park	17
Montclair Park	16
Dover Park	11
Bella Vista Park	9
Lincoln Square Park	8
Eastshore Park	7
San Antonio Park	7
Wilma Chan Park	7
Rockridge-Temescal Greenbelt (aka Frog Park)	6
Snow Park	6
Reinhardt Redwood Regional Park	5
FM Smith Park	4
Lakeside Park	4
South Prescott Park	4
Arroyo Viejo Park	3
Jasper P. Driver Plaza	3
Sausal Creek	3
Temescal Regional Recreation Area	3
All Parks	2
Burckhalter Park	2
Garber Park	2
Hardy Dog Park	2
Lafayette Square	2
Morcom Rose Garden	2
Oakland Davie Tennis Stadium	2
Peralta Park	2
Roberts Regional Recreation Area	2
Allendale Recreation Center	1
Athol Plaza	1
Brooklyn Basin	1
Caldecott Field	1

Cesar Chavez Park	1
Channel Park	1
Clinton Park	1
Concordia Park	1
Dimond Canyon	1
Elmhurst Park	1
Estuary Park	1
Glen Echo Park	1
Grove Shafter Park	1
Gwen Jackson Park	1
Jack London Square Green Space	1
Jefferson Square Park	1
Montclair Railroad Trail	1
Oak Park	1
Park Boulevard Plaza	1
Pine Knoll Park	1
Raimondi Park	1

A. Park Accessibility

There were three questions related to park accessibility that sought to understand how people get to their local parks.

1. How long does it take you to get to this park?
2. How do you usually get to the park?
3. How would you prefer to get to the park?

Question 1: How long does it take you to get to this park?

Response Options:

- a. *Less than 5 minutes*
- b. *5-10 minutes*
- c. *10-15 minutes*
- d. *15-20 minutes*
- e. *20-25 minutes*
- f. *25-30 minutes*
- g. *More than 30 minutes*

We asked respondents how long it takes them to get to the park they identified. Over 50% of people said it took them 10 minutes or less to get to their local park, with most respondents, 27%, taking between five and 10 minutes to get to their park.

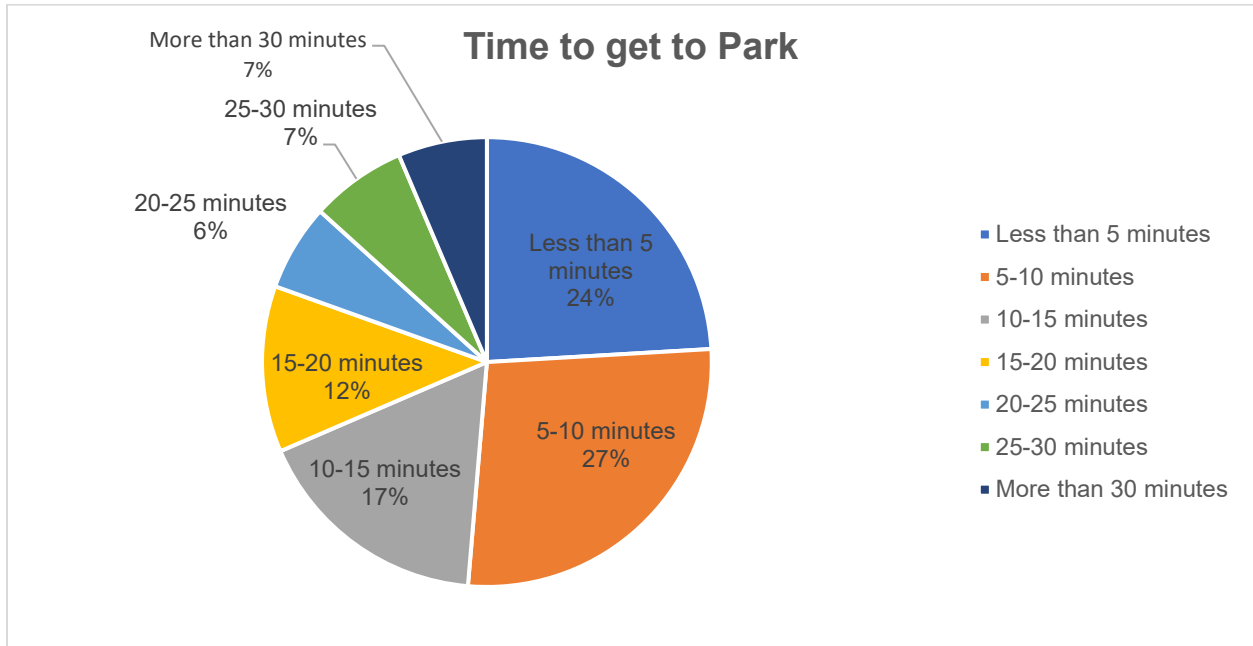


Figure 2: How long does it take you to get to this park?

Question 2: How do you usually get to the park?

Response Options:

- a. Car
- b. Carpool
- c. Walk
- d. Run
- e. Bike
- f. E-Scooter
- g. Transit (Bus, BART, etc.)
- h. Wheelchair
- i. Skateboard
- j. Other

The second question asked respondents to identify the way they usually get to their park, whether walking, biking, driving, taking public transit or some other method. Most respondents drive, with walking and biking being the second and third most common methods.

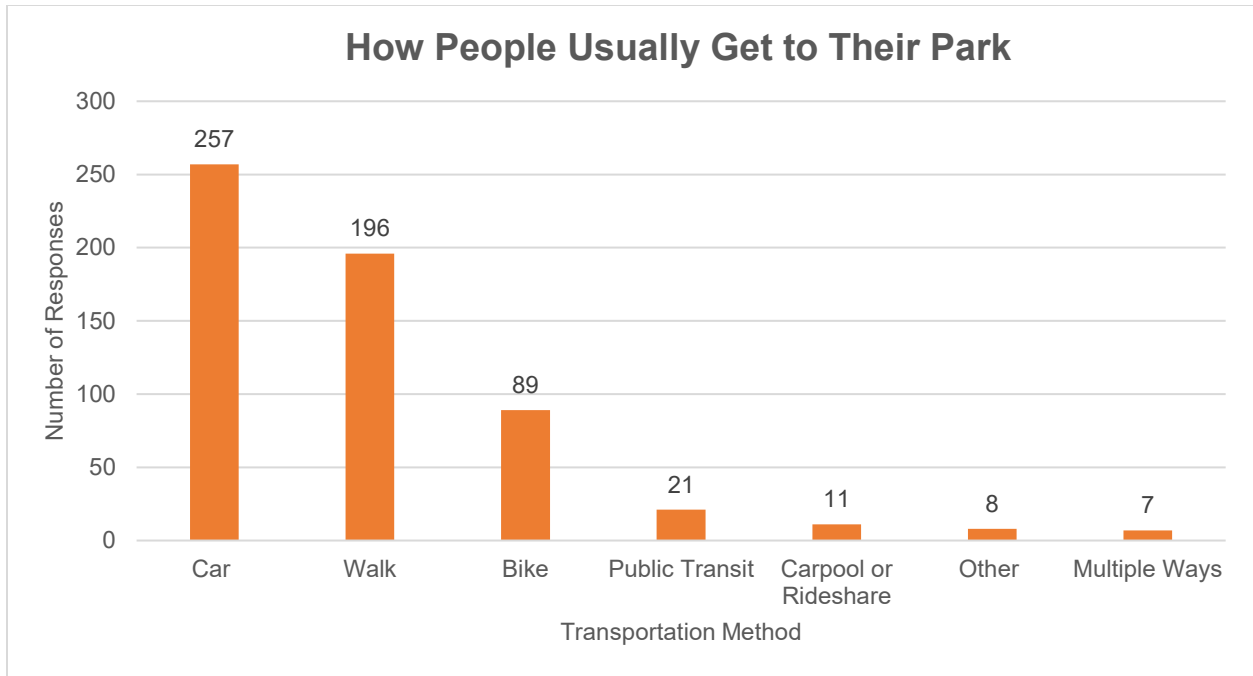


Figure 3: How do you usually get to the park?

Question 3: How would you prefer to get to this park?

Response Options:

- a. My current method of transportation to the park is ideal.
- b. Other

In response to the question “How would you prefer to get to this park?”, most people (87%) responded that their current way of getting to their park is ideal. Approximately five percent of respondents stated that they would like to get to their park with reliable public transit, whether bus, train, or shuttle and four percent wanted to get to their park via bicycle, skateboard, or roller skates. There were eight respondents who selected “Other,” but did not elaborate. Only one respondent wanted to get to their park by driving, which results in the “0%” sliver shown on the pie chart in Figure 4.

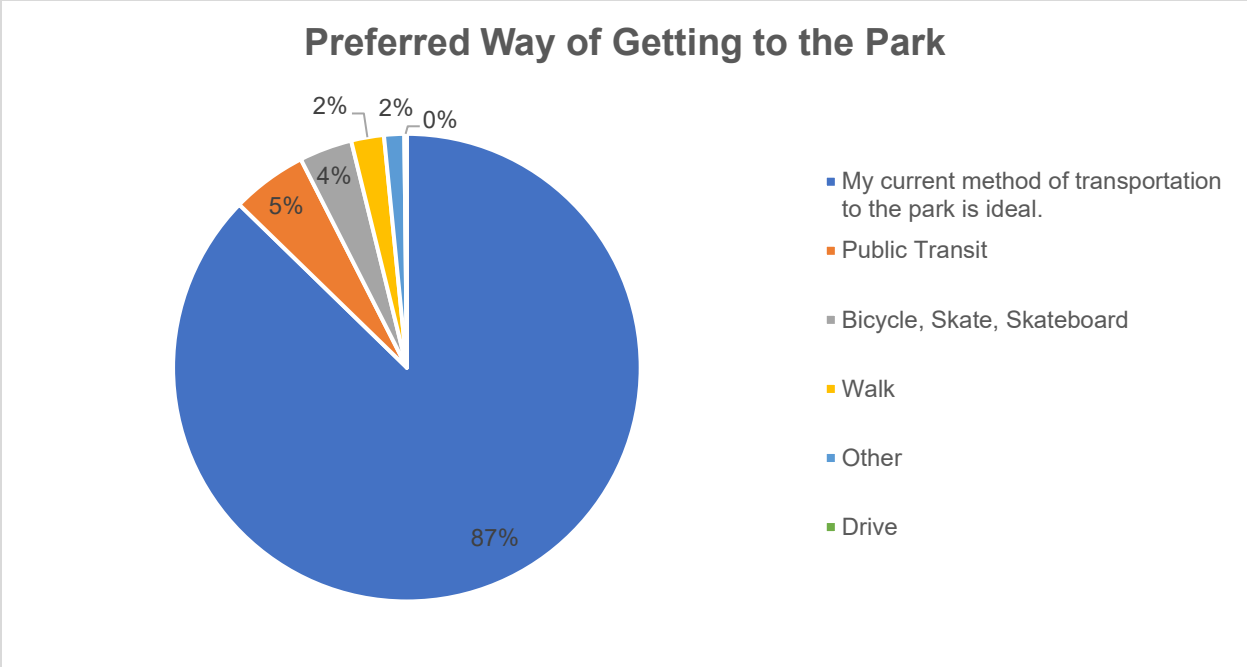


Figure 4: How would you prefer to get to the park?

B. Park Amenities and Usage

There were six questions related to park amenities and usage that sought to understand how people use their neighborhood parks – what they do when they are there, what they would like to see improved, and the types of events that draw them to the park. We specifically asked about restroom access, which has been a recurring theme in community feedback throughout the General Plan Update process.

4. How often do you go to the park?
5. What amenities do you use the most at this park? (Select top 3 in order of use)
6. What are things you would like to see improved about this park?
7. Have you been able to use the restroom at this park when you need to?
8. What other amenities would you like to see at this park?
9. Does this park host community and/or cultural events? If yes, what types of events do you attend?

Question 4: How often do you go to the park?

Response Options:

- a. Once per week
- b. 1-3 times per week
- c. 1-3 times per month
- d. 3-5 times per month
- e. 1-3 times per year
- f. 3-5 times per year

g. Every day

This question assessed how often respondents use the park they indicated. The goal was to understand how different parks are used and the role they fill in residents' daily, weekly, or monthly recreation habits. Parks in Oakland play an important role in respondents' recreation habits. Over 65% of respondents indicated that they visit the park at least once a week while another 25% indicated that they visit the park at least once a month.

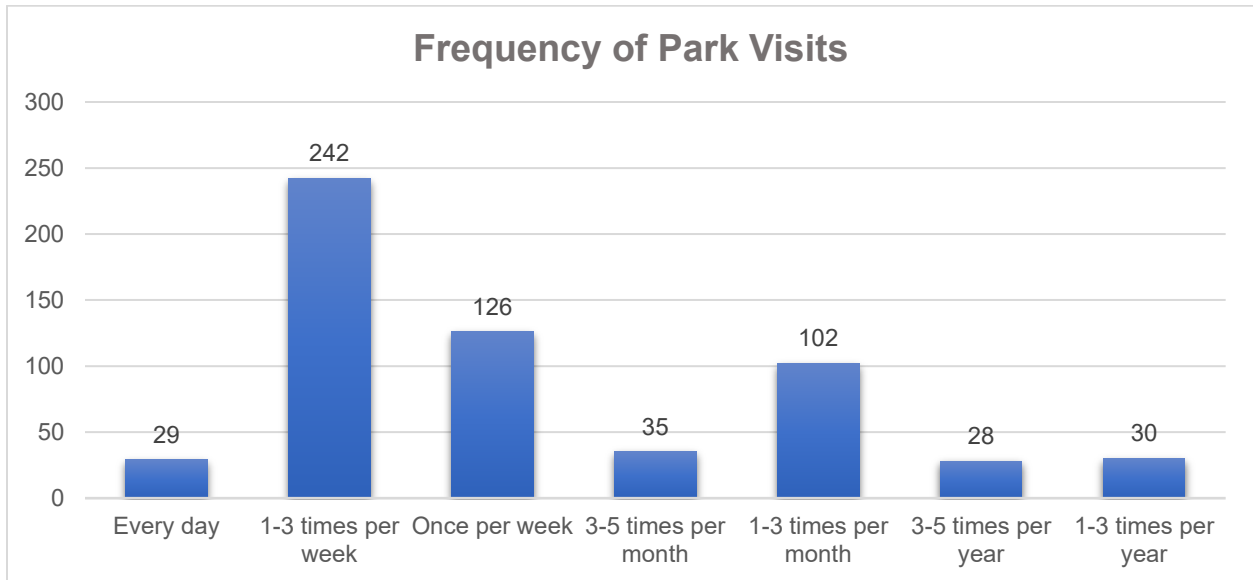


Figure 5: How often do you go to the park?

Question 5: What amenities do you use most at this park (Select top 3 in order of use).

Response options:

- a. Benches
- b. Basketball Courts
- c. Tennis Courts
- d. Walkway/Trail
- e. Picnic Tables
- f. Barbeques
- g. Tot Lot
- h. Playground
- i. Trashcans
- j. Recreation Centers
- k. Dog Parks
- l. Athletic Fields
- m. Flexible Grass Space
- n. Other _____

The purpose of this question was to get a better understanding of how people use the amenities in their local parks. This understanding can be used to better target resource allocation to maintain or enhance the most used amenities at each park. We asked respondents to rank their top 3 amenities from a list developed through a review of amenities listed on the Oakland Parks, Recreation, and Youth Development Department [website](#), and on the Oakland Parks and Recreation Foundation’s “[Parks Guides](#),” and through observation of parks facilities. However, the responses options for the question included only official uses that have facilities in Oakland Parks. For instance, though pickleball is an observed activity at many parks, only “Tennis Courts” was included as a response option because there are no official pickleball courts at this time.

The “Walkway/Trail” option was ranked 679 times and was by far the most used amenity indicated by respondents. The second most used amenity was “Basketball Courts” (ranked 277 times), followed by “Flexible Grass Space” (ranked 105 times). These responses reflect a preference for both active (basketball courts, tennis courts, playgrounds) and passive uses (walkways/trails, flexible lawns, benches), suggesting a need for both types of programming in future planning efforts.

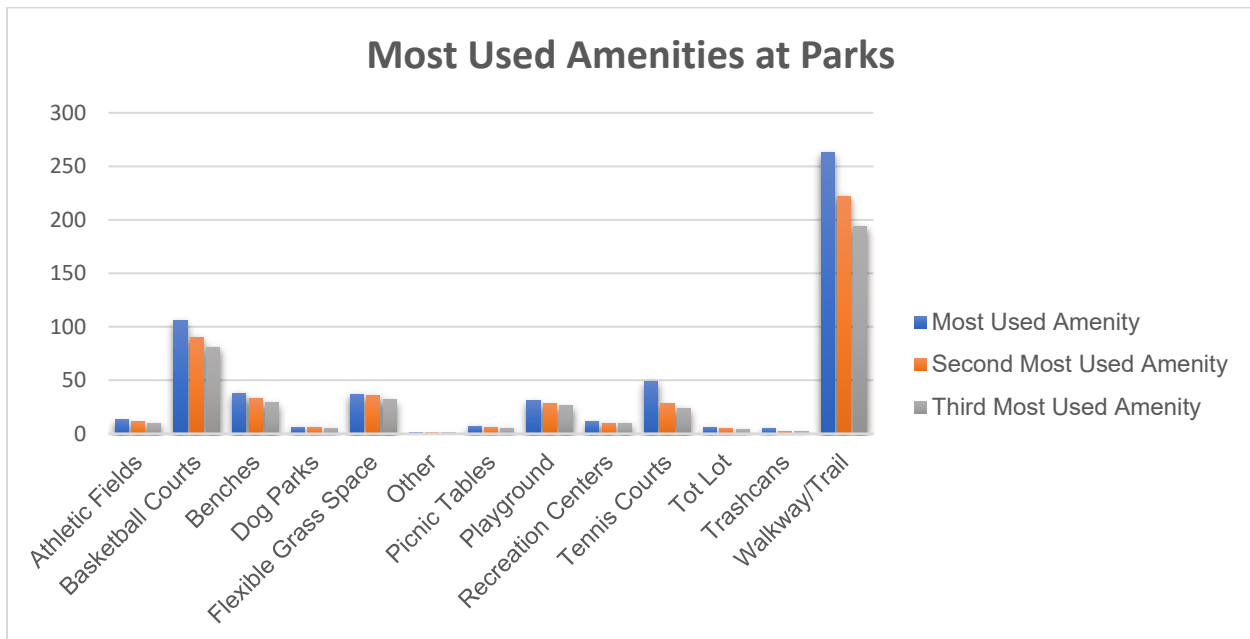


Figure 6: What amenities do you use most at this park?

Question 6: What are things you would like to see improved about this park?

Response options:

- a. Amenities
- b. Tree maintenance
- c. Landscaping/lawn maintenance
- d. Litter

- e. Restroom maintenance
- f. Restroom access
- g. Programming
- h. Other _____

This question asked respondents to reflect on what they would like to see improved at the park they selected. This question provided an opportunity for respondents to share what would improve their experience of Oakland parks and provide feedback on specific interventions they would like to see. Respondents were able to select multiple options, as well as write-in specific improvements that they would like to see. We analyzed each respondent’s answer to develop an understanding of what improvements Oaklanders most want to see in their parks.

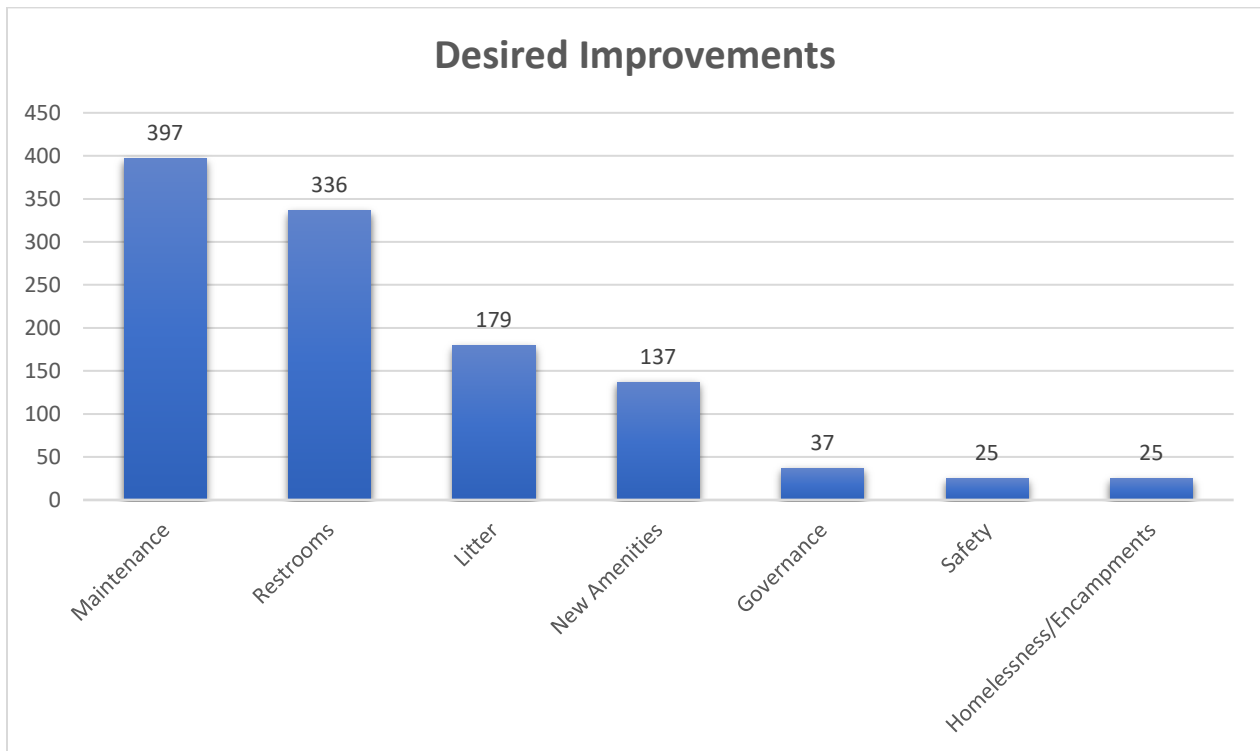


Figure 7: Improvements Desired at Parks

Key insights from answers to this question include:

1. **Quality and Maintenance of Existing Amenities (397 references):** Quality/maintenance of existing features was the most common concern among survey respondents. Answers in this category included concerns about tree maintenance, bathroom maintenance, trail maintenance, and playground maintenance. Respondents concerned about maintenance referenced Joaquin Miller Park and DeFremery Park more than all other parks combined.
2. **Access to Restrooms (336 references):** Respondents see access to restrooms as a key area for improvement, with over half of respondents indicating dissatisfaction with either the state of the restrooms or the availability of restrooms at their parks. Calls for

improved restroom access were focused on DeFremery Park, Joaquin Miller Park, Bushrod Park, Lake Merritt, and Mosswood Park.

3. **Litter (179 references):** Litter, illegal dumping, and insufficient supply of garbage cans at parks was cited often as an area for improvement.
4. **New Amenities that Support for Diverse Activities (137 references):** User groups, specifically roller skaters at DeFremery Park, pickleball players, and mountain bikers, called for improvements to parks to support these activities.
5. **Governance over Competing Uses and User Groups (37 references):** Governance was a clear concern at parks with competing user groups. Conflicts that appeared include those between mountain bikers and hikers; people with dogs and people who don't have dogs; and people using parks for loud gatherings and those using them for quiet reflection and recreation.

Question 7: Have you been able to use the restroom at this park when you need to?

Response Options:

- a. Yes
- b. No

In the Oakland Park and Recreation Foundation's [2018 Park Maintenance Survey Report](#), access to restrooms at parks emerged as a key issue, consistent with feedback received during engagement for Phase 1 of the General Plan Update. The 2018 survey found that a significant number of the freestanding restrooms at inspected parks were either locked or otherwise unusable. This question expanded on the previous survey by asking respondents if they were able to use the restroom at their park when they needed.

Over half of respondents (332, 56%) indicated that they were not able to use the restroom at the park when they needed to. Lack of access to restrooms represents a significant challenge when it comes to increasing park usership and maintaining parks. Lack of restrooms deters users, cuts trips to the park short, or encourages park users to use the park as their restroom, exacerbating the maintenance challenges faced by Oakland's Public Works Department.

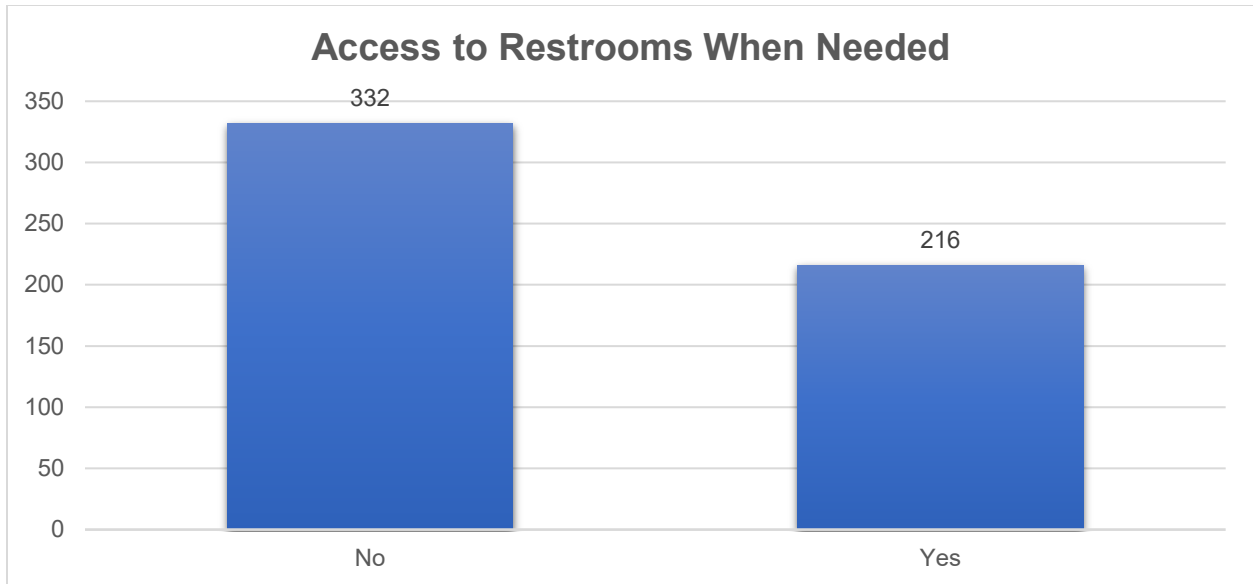


Figure 8: Have you been able to use the restroom at this park when you need to?

Question 8: What other amenities would you like to see at this park?

This open-ended question provided an opportunity for respondents to discuss additional amenities they would like to see at the park they indicated, or to provide more information on the answers they already provided.

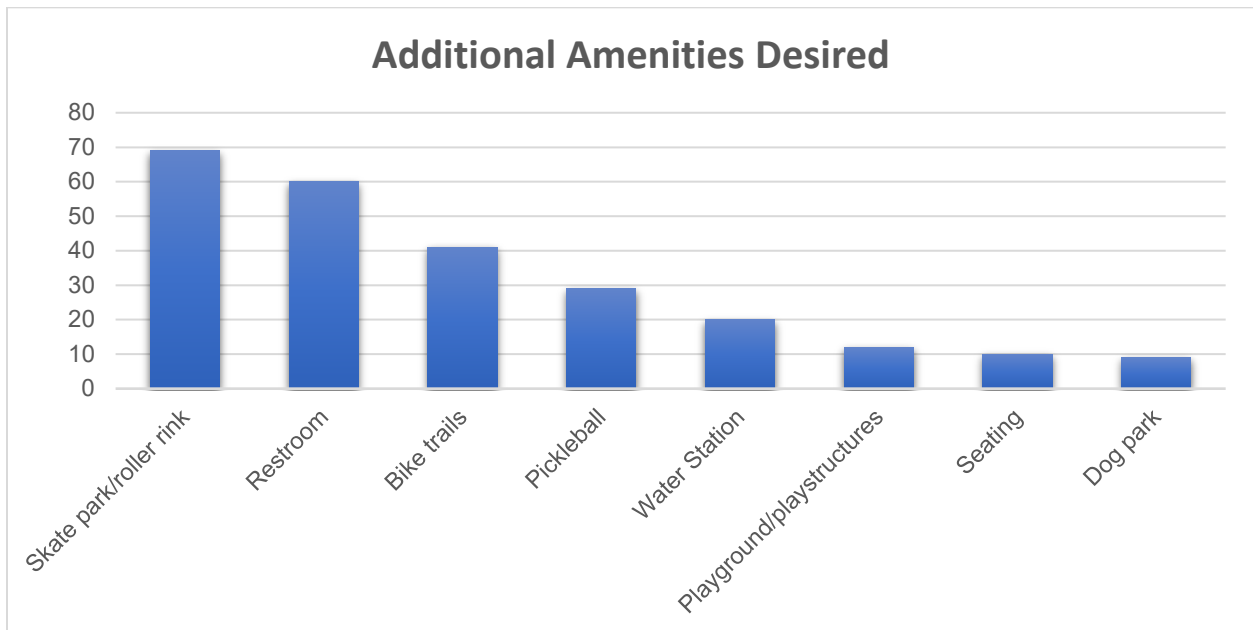


Figure 9: What other amenities would you like to see at this park?

The themes that emerged in the answers to this question were like those discussed in the analysis of Question 6. Responses mainly focused on specific enhancements people wanted to

see in their parks, maintenance concerns, and desired clarity about park operations and governance.

Park Improvements: 254 responses focused on desired improvements to the parks, ranging from the construction of new mountain biking trails to the installation of new water fountains. The most frequently recurring themes were calls for the creation of a roller-skating rink, new restrooms, new bike trails, and dedicated pickleball courts.

Park Maintenance: 71 respondents also pointed to maintenance and quality of life improvements they wanted to see at their park. Statements in this category largely fell into four categories: 1) improving or maintaining existing facilities or structures such as restrooms and playgrounds, as well as park assets like lighting and fencing; 2) providing more regular trash pick-up and more trashcans; 3) providing more regular trail work and landscaping; and 4) removing or mitigating the impact on recreational park users of encampments.

Governance: Respondents referenced governance issues as an area for improvement. Comments in this category included requests for clearer signage to set expectations around mountain biker/pedestrian access to trails, more transparency around park resources and hours of operation, and stronger enforcement of existing rules governing the use of parks.

Question 9: Does this park host community and/or cultural events? If yes, what types of events do you attend?

We asked this question to learn more about the types of programming organized by individual parks, volunteer groups, and individual park users, as well as the appeal of the provided programming to respondents. Through the write-in response option, we sought to provide insights into the activities that bring respondents to the park.

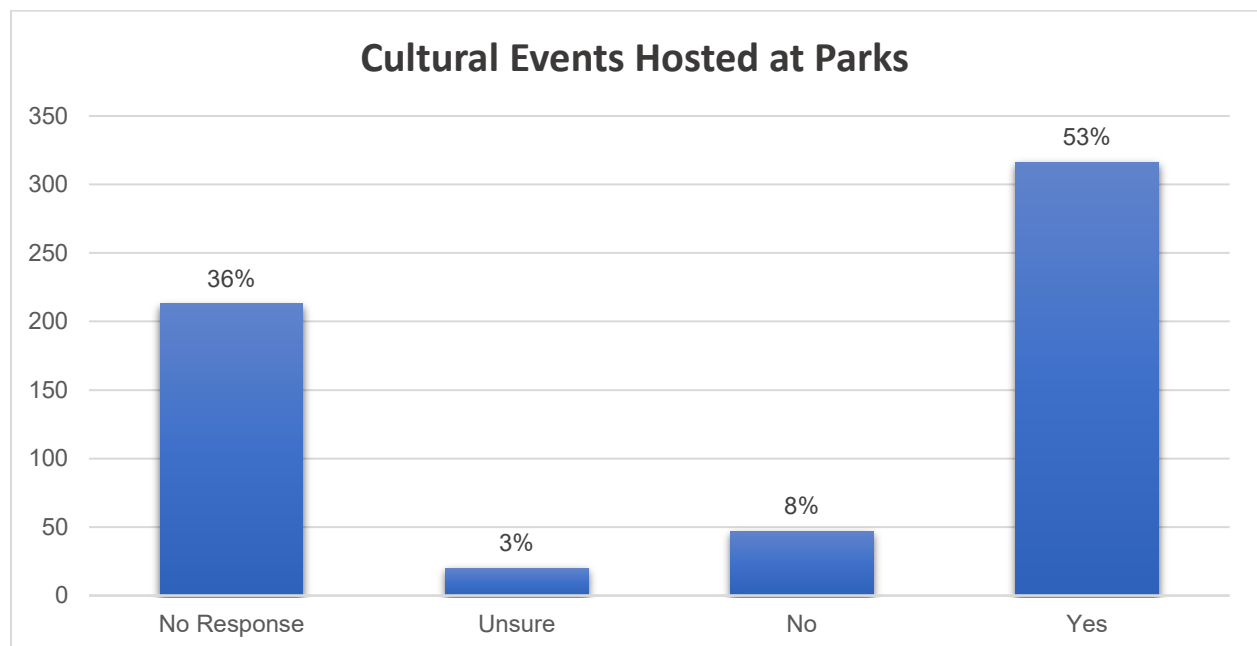


Figure 9: Presence of community and/or cultural events hosted at the park.

Answers to this question highlighted two key themes. The first is that most of the events referenced by respondents are not City-sponsored programming, but community-initiated meetups, classes, and celebrations. This theme speaks positively to the role that parks play in community life in Oakland because residents feel empowered to regularly activate parks to meet their needs. In contrast, the responses also indicate that the “unofficial” nature of some of these gatherings, and the potential for them to get out of hand, leads to alienation and feelings of unsafety among some residents. Five respondents stated that they used to attend events at parks but now feel unsafe doing so. Another respondent shared their frustration with not being able to find a calendar of events that would help them get involved in community activities. Respondents also stated that they would like to see more events in parks. This dynamic suggests that the City has a greater role to play in activating parks and using them as a space to celebrate Oakland’s culture. This also points to the importance of having open green space that can be used in several different ways and that designated athletic fields and/or courts are not necessarily needed for people to find ways to use a park.

The second theme to emerge was the connection between recreation and park use. Many respondents called out specific activities, such as roller-skating nights, mountain bike classes or group rides, and pickleball tournaments as the sole event that they take part in at their park. This highlights the role of organized activities with dedicated facilities or natural features as a driver of visitation to Oakland parks.

C. Perspective on Parks and Community Visioning

We asked respondents to share their perspective on the personal and community benefits of parks, as well as their ideas on how to create a safe and connected network of parks in Oakland. The four questions in this section are listed below.

10. Do you think that access to parks has a positive impact on your physical, social, and mental health?
11. Do you believe that this park benefits your community? Why or why not?
12. Is there another park in Oakland that you visit more often? If so, which park(s)?
13. What does a safe and connected Oakland Park system look like to you?

Question 10: Do you think that access to parks has a positive impact on your physical, social, and mental health?

Response Options:

- a. Yes
- b. No

To better understand the role of parks in promoting, physical, social, and mental health, we asked respondents to respond yes or no to the above statement. We found that 99% (584 out of 589 responses) of respondents believe that access to parks has a positive impact on their physical, social, and mental health.

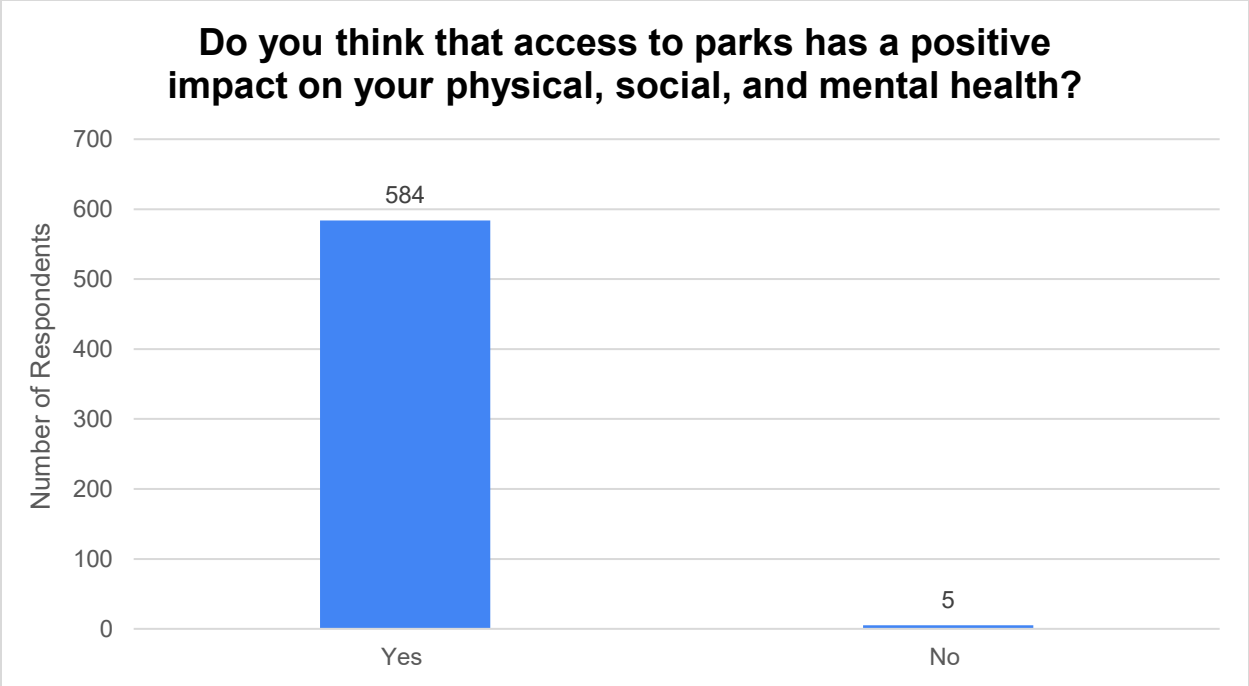


Figure 10: Parks impact on physical, social, and mental health.

Question 11: Do you believe this park benefits your community? Why or why not?

This question provided survey respondents the opportunity to share their perspective on the role of parks in their community. Through this question we sought to understand the shared benefits that park users received from local green spaces, as well as existing challenges or barriers that park users are facing that limit the positive impact of parks on local communities.

Respondents highlighted the role of parks in facilitating community connection, enhancing safety, improving community health, and providing access to nature and outdoor recreation. 202 survey responses focused on the role of parks in fostering community connectedness, 33 responses highlighted the role of parks in activating spaces and promoting community safety, 171 responses focused on the ability of parks to provide access to nature and outdoor recreation spaces, and 78 responses focused on positive health impacts.

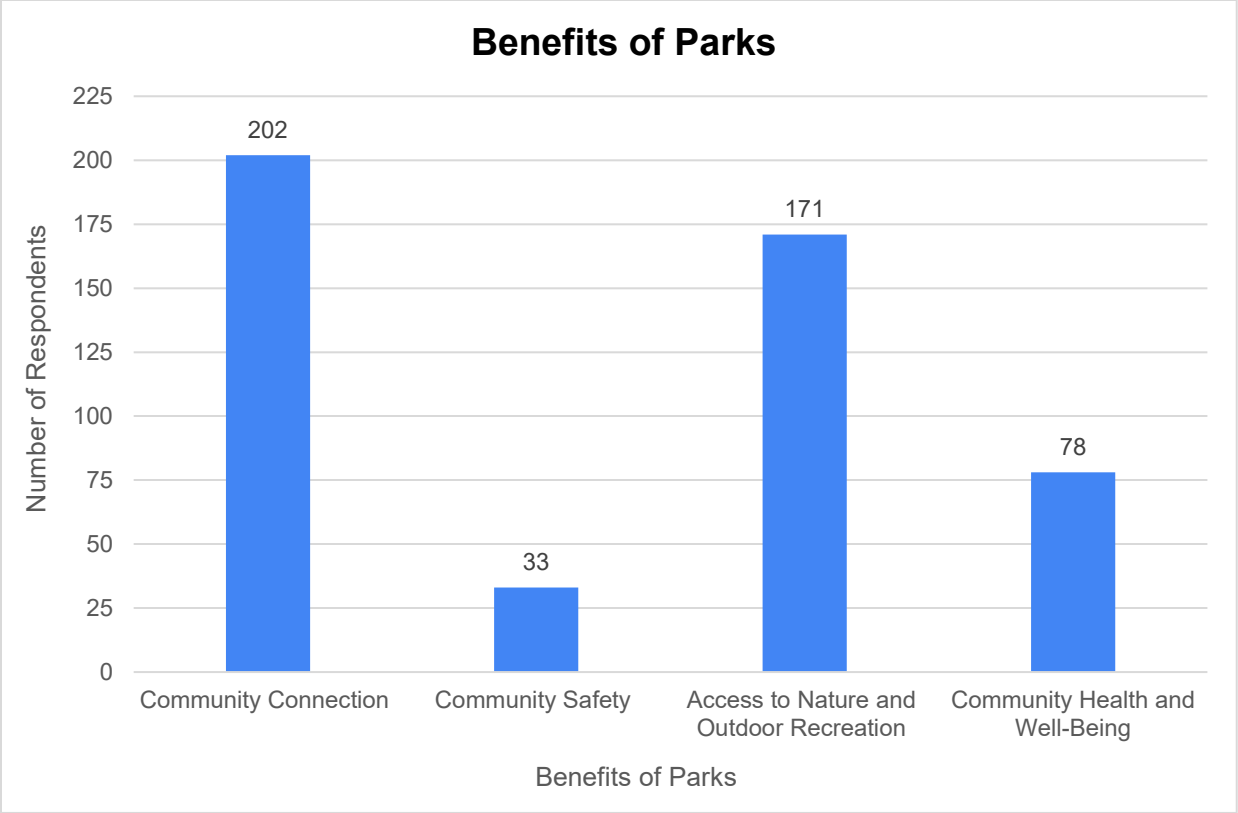


Figure 11: Community Benefits from Parks

Question 12: Is there another park you visit more often? If so, which parks?

We asked this question to provide survey respondents the opportunity to list other parks that they may enjoy visiting or frequent often, in addition to the park they provided feedback on.

Table 2: Full List of Other Parks Respondents Visit More Often

Park Referenced	Count
Lake Merritt	38
Dimond Park	25
Reinhardt Redwood Regional Park	23
Joaquin Miller Park	15
Montclair Park	12
Smith Park	9
Dover Park	8
Bushrod Park	7
Temescal Regional Recreation Area	7

Lincoln Square Park	7
Snow Park	6
Little Frog Park	5
Mosswood Park	5
Lakeside Park	5
Upper Frog Park	4
Sibley Volcanic Regional Reserve	4
Roberts Regional Recreation Park	4
Rose Garden	4
East Bay Regional Parks District	3
Township Commons	3
Martin Luther King Shoreline	3
Pine Knoll Park <i>(temporarily closed)</i>	3
Bella Vista Park	2
Tilden Regional Park	2
DeFremery Park	2
Oakland Hills	2
Wilma Chan Park	2
Bonsai Garden	1
Athol Plaza	1
Skyline Regional Park	1
Sausal Creek	1
San Antonio Park	1
Plum Patch Park	1
The Bay Trail	1
Lower Bobs Skatepark	1
Huckleberry Regional Botanic Reserve	1
Leona Heights Park	1
Splash Pad Park	1
Chabot Park	1
Brookdale Park	1
Knowland Park	1
Kings Estate Open Space Park	1

Linden Park	1
Astro Park	1
Barry Place Parklet	1
Cleveland Cascade	1
Lakeshore Park	1

Question 13: What does a safe and connected Oakland Park system look like to you?

Survey respondents reflected on their hopes for the future of the Oakland Park system, which were focused on enhancing public safety, increasing the accessibility, connectivity, and walkability of parks, and improving parks maintenance and services.

Safety: 146 responses focused on community concerns and hopes for a safer park experience in the future. To improve safety, respondents expressed a desire for improved lighting (especially at night), reducing encampment presence by identifying long-term solutions to support the unhoused community, increasing security or staff presence at parks, providing safe and clean restrooms, preventing drug and alcohol usage, as well as theft and car break-ins, and providing overall safe spaces for children to play.

Accessibility, Connectivity, and Walkability: 67 responses highlighted a desire for improved park accessibility, connectivity between parks, and the ability to easily walk or bike to and from parks. Comments in this category include a desire for improved wayfinding, such as through improved signage and maps, easy public transit connections, safe car and bike parking options, well-maintained hiking and bike trails, and safe pedestrian intersections between parks.

Improving Park Maintenance and Services/Programming: 200 respondents shared that a primary component of their vision for Oakland’s parks are improvements in park maintenance and the increased provision of park services and community programming. Respondents shared a desire for free or low-cost events and open and sufficiently staffed recreation centers that have appropriate activities for all ages.

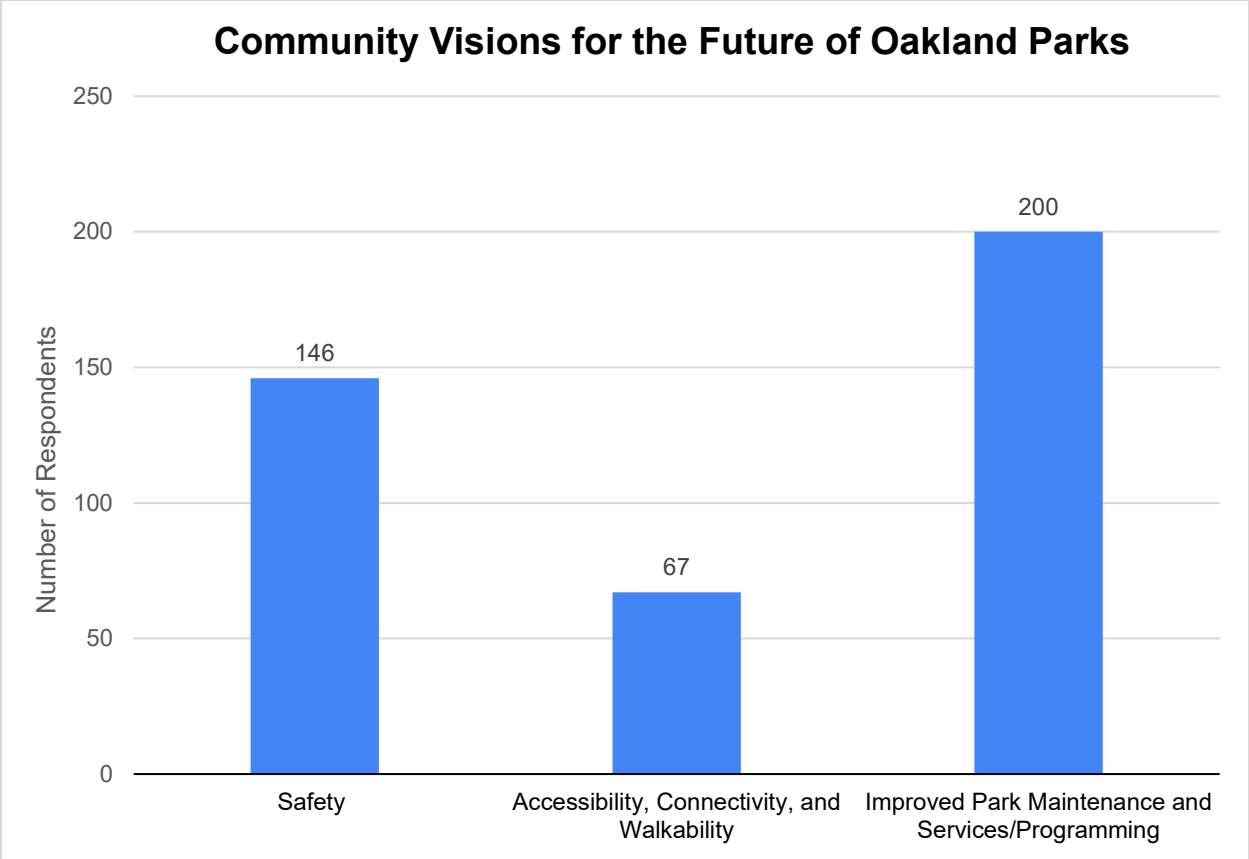


Figure 12: Priorities for a safe and connected Oakland parks system

Creating a Safe and Connected Oakland Parks System

We derived a few key insights from the responses to Question 13 which are presented below and supported by a few quotes from survey respondents.

Parks as Spaces of Community Connection and Belonging

Community members highlighted local parks as spaces that foster community connection, feelings of belonging and civic pride. Parks are one of the last few gathering places that are free, and they are critical spaces for fostering community cohesiveness. One respondent, whose local park is Lafayette Square, wrote about the role of parks as key to facilitating community connection across diverse groups, saying, "...Parks are important third spaces and bring peace, recreation, community, and engagement between neighbors, elected [officials], city officials, and other nonprofit groups." Similarly, a respondent whose local park is Lincoln Square Park shared that, parks "[allow] more people to go out of their homes and be willing to integrate with the community, have activities, communication, tolerance and unity, bring together different national cultures, and make the community cohesive." Another respondent from Lincoln Square Park described the park as the "heartbeat of the neighborhood," and that it helps people feel rooted and less lonely and isolated. These community statements show that residents value parks as spaces of critical connection, belonging, and community building.

Outdoor Recreation and Community Health

In addition to fostering belonging and connection, residents highlighted that parks play a large role in promoting community health and well-being by promoting outdoor recreation and the opportunity to connect with nature. Outdoor recreational activities identified by community members include hiking, bike riding, mountain biking, pickleball, visiting community and public gardens, exercise, basketball, picnics, dog walking, skateboarding, roller skating, and playing. One respondent, reflecting on Mosswood Park stated "...it's really important as a green space and a place for kids to play (and for adults to play too). For me it's all about the community garden where I have a plot, pretty much my favorite place to be."

Many survey respondents provided feedback on Joaquin Miller Park, naming it as a key outdoor space in Oakland. One park user wrote, "it provides a beautiful outdoor space for all types of social/family gatherings, solo/family/group hiking, trail bike riding, nature observation (birding for example)." A notable finding from the survey responses was recurring comments focused on Joaquin Miller Park as a prime mountain biking location in Oakland and in the larger Bay Area region. Bushrod Park was also cited as a popular recreation area for pickleball, which has gained popularity in recent years. Several respondents also shared that parks can be key to promoting exercise, socialization, and healthier lifestyles in specific populations, such as older adults or renters. A respondent from Cesar Chavez Park responded to the question of whether parks benefit their community with, "yes, there are many renters in this neighborhood in crowded housing. The park is key for kids and adults alike to hang out and be active." Similarly, a respondent from Bushrod Park shared that the park "provides a place for seniors to get exercise and socialize."

With the understanding of parks as key spaces for individuals, families, and communities to spend time outdoors and recreate, many survey respondents highlighted parks as fundamental to promoting physical health, mental health, and overall well-being. This understanding of the link between health and access to parks is seen in respondent statements such as, "Absolutely. We need contact with nature to restore our mental and physical health." Another survey respondent specifically highlighted the role of green space in combatting urban challenges related to climate change, stating "yes it [provides] essential amenities and reduces [the] heat island effect in West Oakland." The urban heat island effect refers to the phenomenon in which urban spaces are warmer than surrounding areas due to the concentration of heat-absorbing infrastructure, such as concrete and buildings, and the lack of greenspace. The concentration of heat can have large impacts on human health, especially for the elderly, individuals with chronic illnesses, the unhoused, and those who lack access to air conditioning or live in older housing.

Uneven Access to Green Space

Despite the many perceived benefits of parks, community responses also highlighted frustration associated with the inequitable access and distribution of green space in Oakland. Due to historical discriminatory land use policies and decisions about public resources, access to green space in Oakland is not equitably distributed. This is not unique to Oakland; many other cities across the country are dealing with the legacy of discriminatory housing, land use and resource allocation policies. Lack of access to green space has been linked to worse health outcomes, such as higher rates of stress, reduced physical activity, and higher rates of mental illness. The disparity in green space distribution is seen in statements such as "Bushrod is one of the few green spaces in North Oakland." Another survey respondent from San Antonio Park wrote "Yes, public space is where humans gather and meet people. We need many more of these types of

opportunities in Oakland.” Lastly, one community response highlighted that the uneven distribution of parks also results in longer commutes to reach parks. One resident put it thusly “Yes. There are no real parks in my neighborhood (the Fruitvale). I have to leave my neighborhood to enjoy the calm and beauty of nature.” This feedback from community members makes it clear that community members see parks as vital to individual and community health and that ensuring equitable access to parks should be a priority for the General Plan Update.

Park Maintenance, Improvement, and Safety

Survey responses also highlighted that two primary concerns of community members are park maintenance and safety. Common concerns raised by survey respondents include litter, illegal dumping, encampments, lack of supervision by City staff or police, park maintenance and cleanliness, bathroom access, lighting, and improved park signage. Survey respondents shared that due to lack of park maintenance and the deterioration of many local parks, the benefits that the parks provide to the community are diminished. One respondent shared a desire for routine park maintenance, stating, “pick up the trash more frequently. Maintain the infrastructure. Cut the grass more frequently. Add bathrooms so people don’t have to use disgusting port of potties.” Another survey respondent from Eastshore Park expressed feelings of frustration due to the inability of neighborhood residents to enjoy the park, saying “this park is a gathering place for so many nearby neighborhoods. But it is underutilized due to lack of amenities and cleanliness.”

Respondents also shared feeling unsafe and uncomfortable at parks due to the presence of encampments at some parks. One survey respondent shared “it is nice to have somewhere to go outside my house, but its value is significantly diminished by the amount of homeless encampments and trash in the park. I no longer feel interested in going there since tents popped up; I feel ambivalent - like I’m intruding on their living space but also like I may be unsafe.” Other responses similarly called for the City to implement long-term actions to support people living in encampments, “the city needs to open up public lots for the unhoused to safely encamp and invest in long term affordable housing for them. No one should have to live at a park. Sweeping encampments is obviously not a good long-term solution.”

Survey responses also highlighted examples of community members working together and pooling resources to improve their local park. For example, a respondent from Dover Park shared how the community worked together to improve the park by pooling money to fix broken swings and patch up holes in the ground.

Evolving Community Desires that Require New Infrastructure

In addition to park maintenance improvement, respondent feedback also highlighted a need for new infrastructure to meet ongoing and evolving community desires. From the comments received, evolving community needs include well-maintained, public, and permanent roller-skating rinks, public restrooms at all parks, more spaces for community gatherings, functional water fountains, pickleball courts, activity-specific trails, and improved infrastructure to make parks more accessible for all users.

The community desire for a permanent roller-skating rink has been shared by many respondents who currently participate in Panther Skate at DeFremery Park in West Oakland. Respondents shared their visions for a future roller-skating plaza in Oakland that is “clean, well attended and offers space to do activities that are popular with the local community. People love roller skating and there are few places that are conducive to roller skating. Smooth, flat, large

enough to skate like one would at a proper rink.” Additionally, another respondent provided thoughts on intermediary actions that can be taken to support the skating community in the meantime, writing “creating a dedicated skating plaza at DeFremery, and until that happens, allowing the lights to stay on past 10pm.” 76 responses highlighted a desire for a permanent roller-skating rink for the skating community.

Survey responses show that there is a growing pickleball community in Oakland that want dedicated pickleball courts. A respondent from Bushrod Park wrote “there are a couple thousand people who come to Bushrod Park to play pickleball. Even more would come if there were permanent courts and players didn’t need to know how to access temporary nets.”

Respondents also highlighted that pickleball has the potential to benefit specific populations, such as seniors and retired individuals, who often need additional support and programming to get exercise and socialize. One respondent shared “on any given day, there are 60 people at a single session with multiple sessions throughout the day. This is Pickleball. The age range is 25 to 85 and a provides a social network for so many people, especially those who are retired.”

These survey responses highlight the need for more pickleball infrastructure to support growing community desires. 64 survey responses highlighted the benefit of pickleball in the community and the need for more formal pickleball courts.

IV. Demographic Breakdown

Survey respondents had the option of answering the following demographic questions.

- What is your age?
- What gender do you identify with?
- Do you _____ in Oakland? Select all that apply.
- If you live in Oakland, which zip code do you live in?
- Which of the following best represents your race? Check all that apply.
- Which of the following best represents your ethnicity?
- Preferred Language

Age of Respondents

Most respondents, 43%, were between the ages of 21 and 44 and 35% were between the ages of 45 and 64. There were few responses from youth, one of our target demographics, which speaks to the need to bolster youth engagement, especially around park and open space priorities.

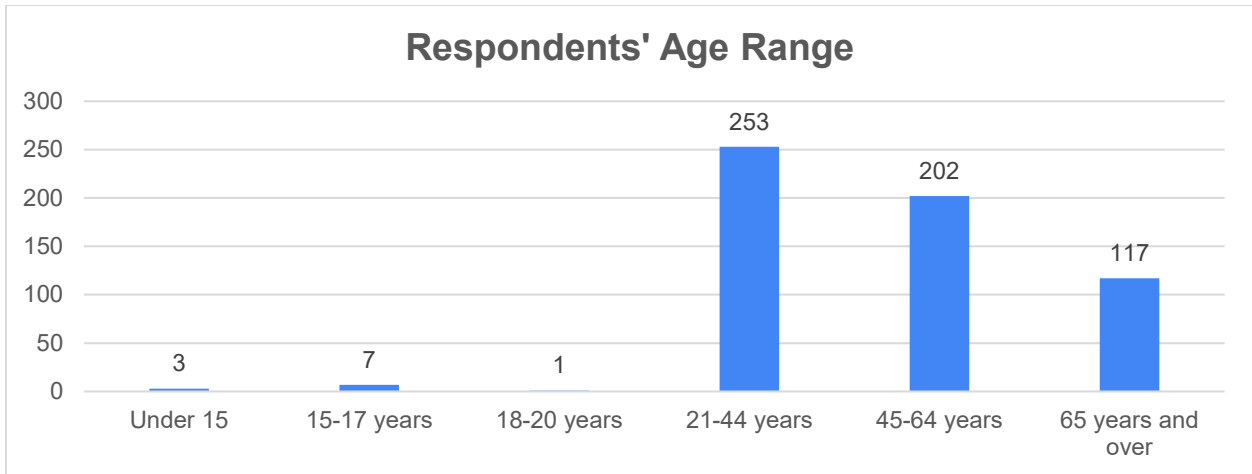


Figure 102: Age Range of Respondents

Gender of Respondents

Over half of respondents, 55%, identified as female and 41% as male. Less than five percent of respondents identified as Transgender or Other.

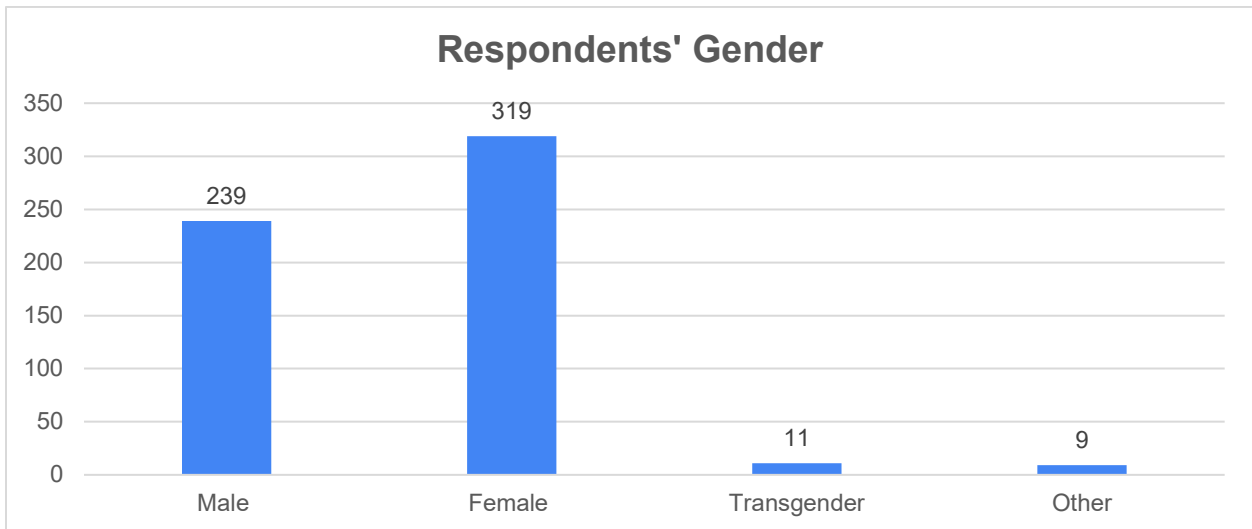


Figure 111: Gender of Respondents

Respondents' Relationship to Oakland

We asked survey respondents to identify whether they live, work, or spend time in Oakland. Respondents were able to select more than one option. Most respondents, 77%, were Oakland residents, and a quarter of respondents live, work, and spend time in Oakland.

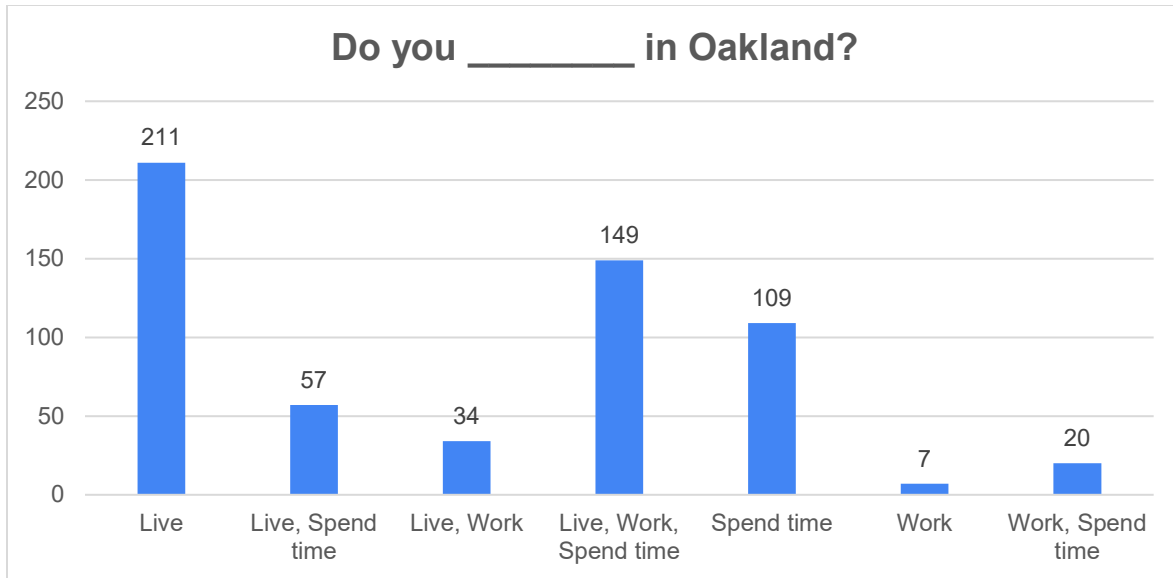


Figure 12: Do you _____ in Oakland?

Zip Codes of Oakland Residents

Most respondents live in three zip codes:

- 1) 94602 – 16%, which includes neighborhoods in North Oakland, such as Glenview and Lincoln Highlands,
- 2) 94610 – 15%, which includes neighborhoods immediately north/northeast of Lake Merritt, such as Lakeshore, Adams Point, and Crocker Highland.
- 3) 94609 – 13%, which includes neighborhoods in North Oakland, such as Telegraph, Temescal, and Rockridge.

There were few respondents living in zip codes representing West and East Oakland, which indicates the need for increased efforts targeting those communities to understand their priorities and concerns regarding parks and recreation.

Respondents' Race and Ethnicity

According to Census data for 2023, the racial composition in Oakland is approximately 41% White, 24% Black, 20% Asian, and 27% Some Other Race. Most survey respondents identified as White. Only 12% of survey respondents identified as Black and 16% identified as Asian. Approximately 30% of Oaklanders identify as Hispanic or Latino, regardless of race. However, over 80% of the survey respondents identified as Not Hispanic or Latine and only 11% as Hispanic/Latino/Latine. This breakdown is skewed compared to the realities of Oakland's populations and does not reflect the targeted populations of the General Plan Update, which include people of color and people who speak English as a second language.

Respondents' Preferred Language

Most respondents, nearly 95%, selected English as their preferred language. This breakdown does not reflect the targeted populations of the General Plan Update, which include people who speak English as a second language. There is an overrepresentation of English speakers amongst the survey responders. Approximately 61% of Oaklanders prefer to speak English predominantly, 21% prefer to speak Spanish and 6.4% prefer to speak Chinese.

V. Conclusion and Next Steps

Through the analysis of the park survey results, we have developed a broad overview of the opportunities and challenges that are facing Oakland's parks. The survey results will be used to inform the development of the General Plan Update, specifically the Open Space, Conservation, and Recreation (OSCAR) element. By developing a deeper understanding of park user priorities, experiences, and values, we aim to develop planning policies and actions that meet the needs of the community on the ground and help close current gaps in current park policies.

Additionally, the demographic analysis of survey respondents show that we need to make a concerted outreach to understand the park and recreation priorities of residents in East and West Oakland, as well as youth and people of color. As engagement on the General Plan Update ramps up in 2025, outreach will be focused on these communities to get their input into the development of the General Plan.

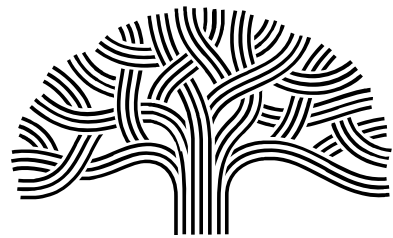
To complement the park survey findings, we will develop a Park Amenities matrix for the OSCAR that will serve as an accessible and easy to use tool for users to quickly receive key information on park amenities.

EXHIBIT B

General Plan Update (GPU) Phase 2 Overview

**Presentation to the Parks and
Recreation Advisory Commission**
February 12, 2025

Planning and Building Department
**Khalilha Haynes, Bebe LeGardeur, Joanna
Winter**



**CITY OF
OAKLAND**



Agenda

- General Plan Update (GPU) Background
- GPU Phase 2 Components
- Summary of Park Survey Report
- Discussion
- Next Steps

General Plan Background

City of Oakland Team

- **Joanna Winter, Phase 2 Project Manager** Conservation, and Recreation (OSCAR) Element
- Laura Kaminski, Strategic Planning Manager
- Tarisha Bal, Deputy City Attorney
- Lakshmi Rajagopalan and Michelle Matranga, Co-Leads, Land Use and Transportation Element (LUTE)
- Ruslan Filipau, Lead, Noise Element
- Daniel Findley, Lead, Infrastructure & Capital Facilities (ICF) Element
- Khalilha Haynes, Lead, Open Space, Conservation, and Recreation (OSCAR) Element
- Timothy Green, General Plan Advisory Committee Coordinator & LUTE
- Bebe LeGardeur, ICF and OSCAR
- Mariah Padilla, LUTE and OSCAR
- Aaron Lehmer, Technical Communications Specialist

What is a General Plan?



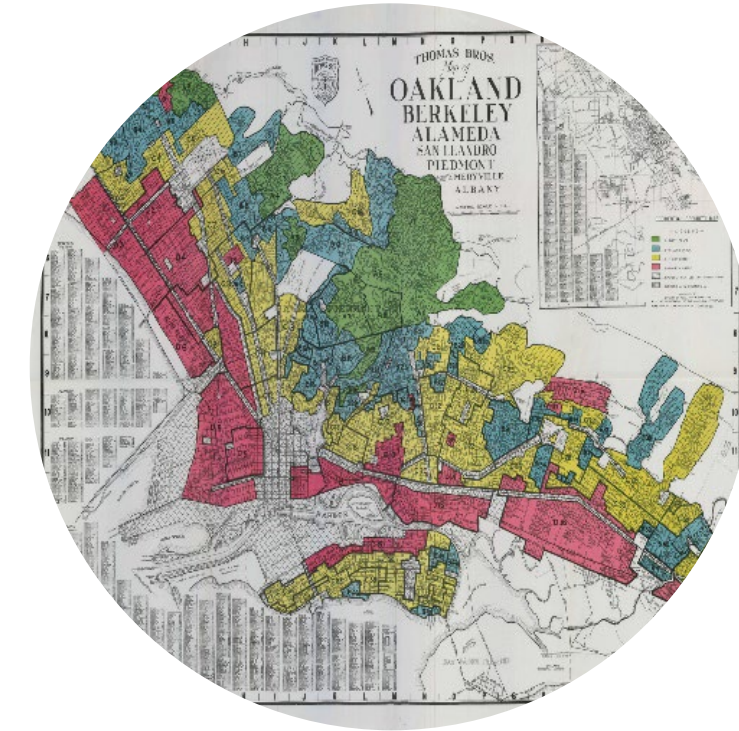
"Constitution" for development and conservation

Establishes citywide vision
and supporting goals,
policies, and
implementation measures



Identifies past challenges & accomplishments

Provides consistent
direction for future
development



Opportunity to Reduce Racial Inequities & Do Inclusive Engagement

Engage our community in
the planning and decision-
making process

How does the General Plan Affect Oakland?



Enhance parks and recreational spaces



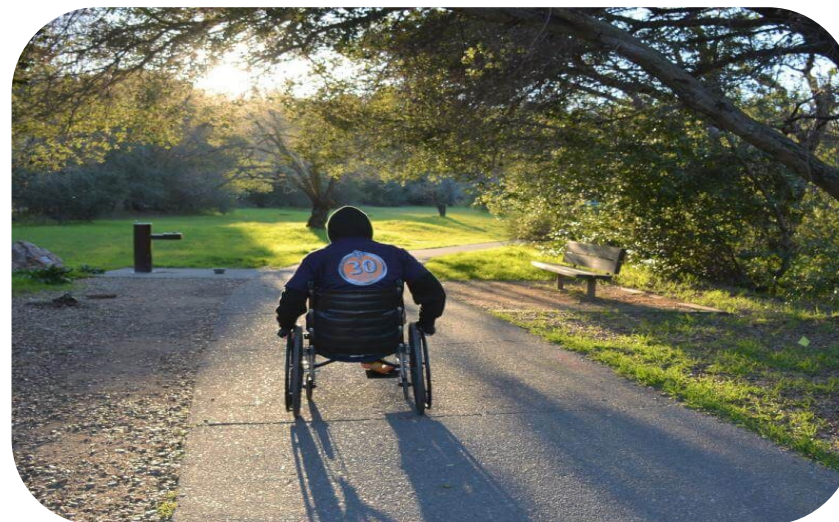
Improve access to jobs and career opportunities



Create more livable neighborhoods



Nurture cultural and natural resources



Improve how people get around



Improve city infrastructure

Phased Update

PHASE 1:
2020-2023

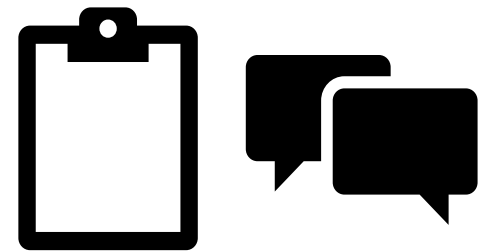


PHASE 2:
2023-2026



Phase 2 Timeline

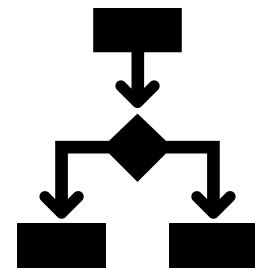
FALL 2024



Initial Engagement

Build on Phase 1 input and get thoughts on Phase 2 issues and strategies

WINTER 2024 –
SPRING 2025



Develop Alternatives

Evaluate different options, concepts, and tradeoffs to achieving community goals

SPRING 2025 –
SUMMMER 2025



Preferred Alternative

Combine preferred alternatives concepts into the Preferred Plan

FALL 2025 –
FALL 2026



Element Development

Using community input, develop elements for public review

WINTER 2026



Hearings and Adoption

Elements go to decisionmakers for consideration

**Community engagement opportunities and feedback loops (where the City shares how input has been incorporated) to occur at all steps*

Community Engagement



Image: Deeply rooted youth fellows and Unity council members at the Oakland General Plan table, Source: Karla Guerra



A. "Kariem" Khan, @ak2web3, and Jessica Lee, @jessicazlee, discuss their vision for Oakland's future @oaklandca.



Focus on Oakland's most impacted residents

- Working class communities
- Communities of color
- Unhoused
- Formerly incarcerated
- Youth
- Communities experiencing environmental injustices

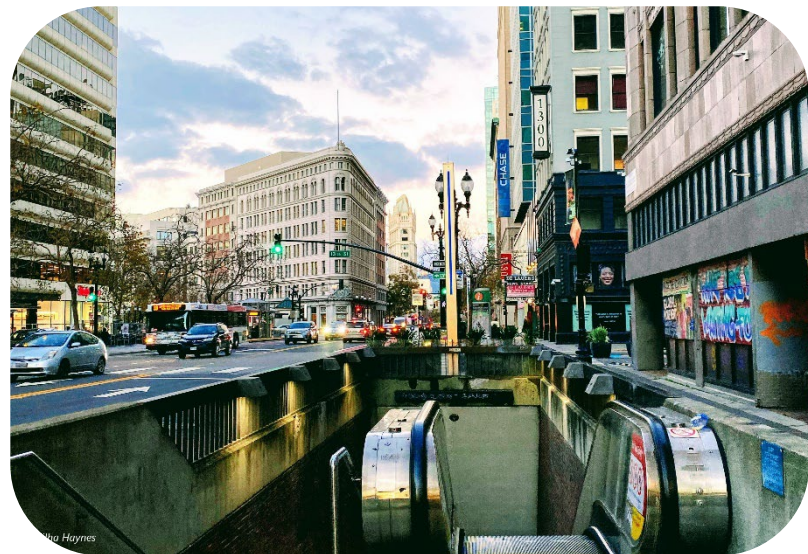
Community Events:

- Tabling at Existing Community Events
- Neighborhood and Townhall Workshops
- Discussion Groups
- Decisionmaker Meetings
- Online Engagement

Full Engagement Plan: <https://www.oaklandca.gov/documents/oakland-general-plan-update-gpu-phase-2-community-engagement-plan>

Phase 2 Components

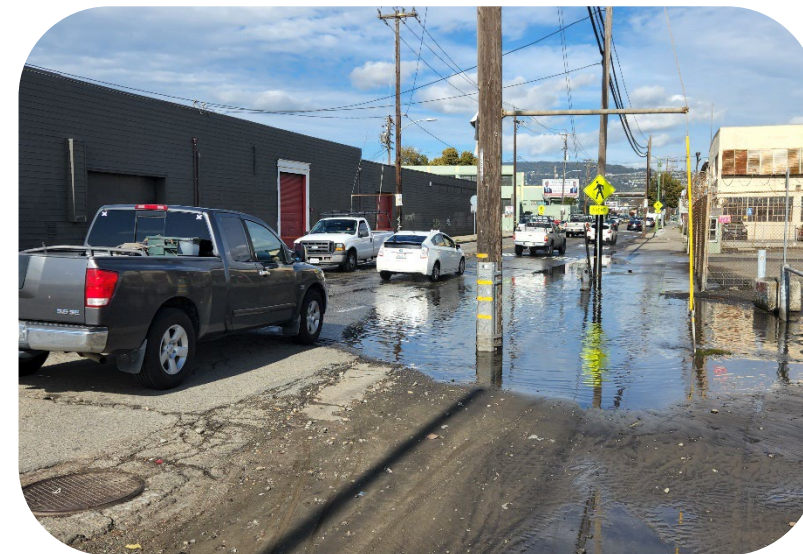
Phase 2 Elements



**Land Use and
Transportation
Element**



**Open Space,
Conservation, and
Recreation (OSCAR)**



**Infrastructure and
Facilities**



Noise

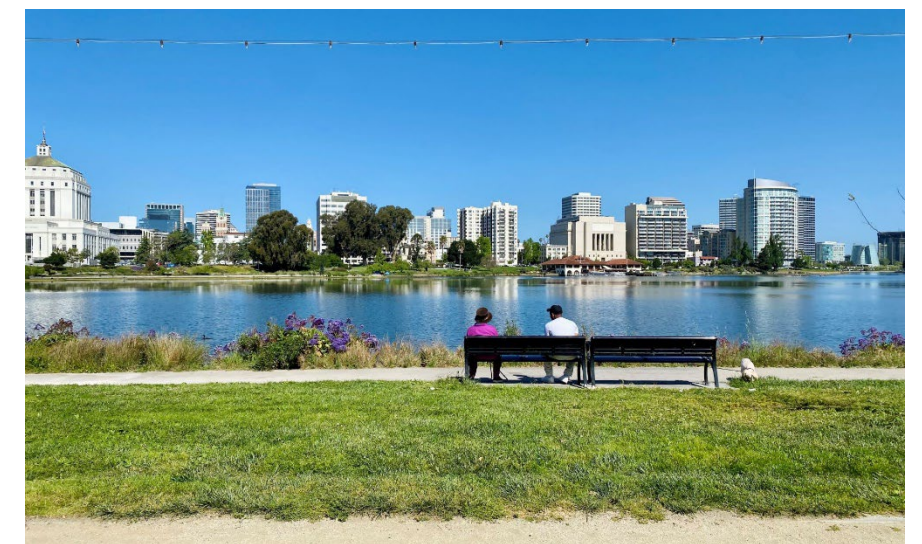
Open Space, Conservation, and Recreation (OSCAR) Element

Open Space and Conservation: Maintain, preserve, & strengthen Oakland's network of open spaces and conserve important natural resources Kinds, locations, and intensities of land uses

- Open space network connections and accessibility
- Habitat management

Recreation : Build an equitable, well-maintained, accessible network of parks and recreational facilities that support and reflect Oaklander's needs, identities and cultures

- Types of parks and distribution throughout city
- Parks maintenance, safety, and security
- Parks programming, events, arts & culture



Summary of Park Survey Report

Park Survey and Assessment Process

Park Survey

- **597 responses**
- Open August 20, 2024 - September 20, 2024
- Available in English, Spanish, and Chinese
- Shared through:
 - Directors and/or programmatic staff at each Recreation Center
 - Oakland Parks and Recreation Foundation newsletter
 - In-person events and with community partners

Park Site Visits

- **22 parks visited**
- July to August 2024
- Parks identified through conversations with relevant departments and reviews of relevant planning documents
- Visited parks in **Environmental Justice Communities** that were classified as Active Mini Park, Neighborhood Park, or Community Park

Themes from Park Survey

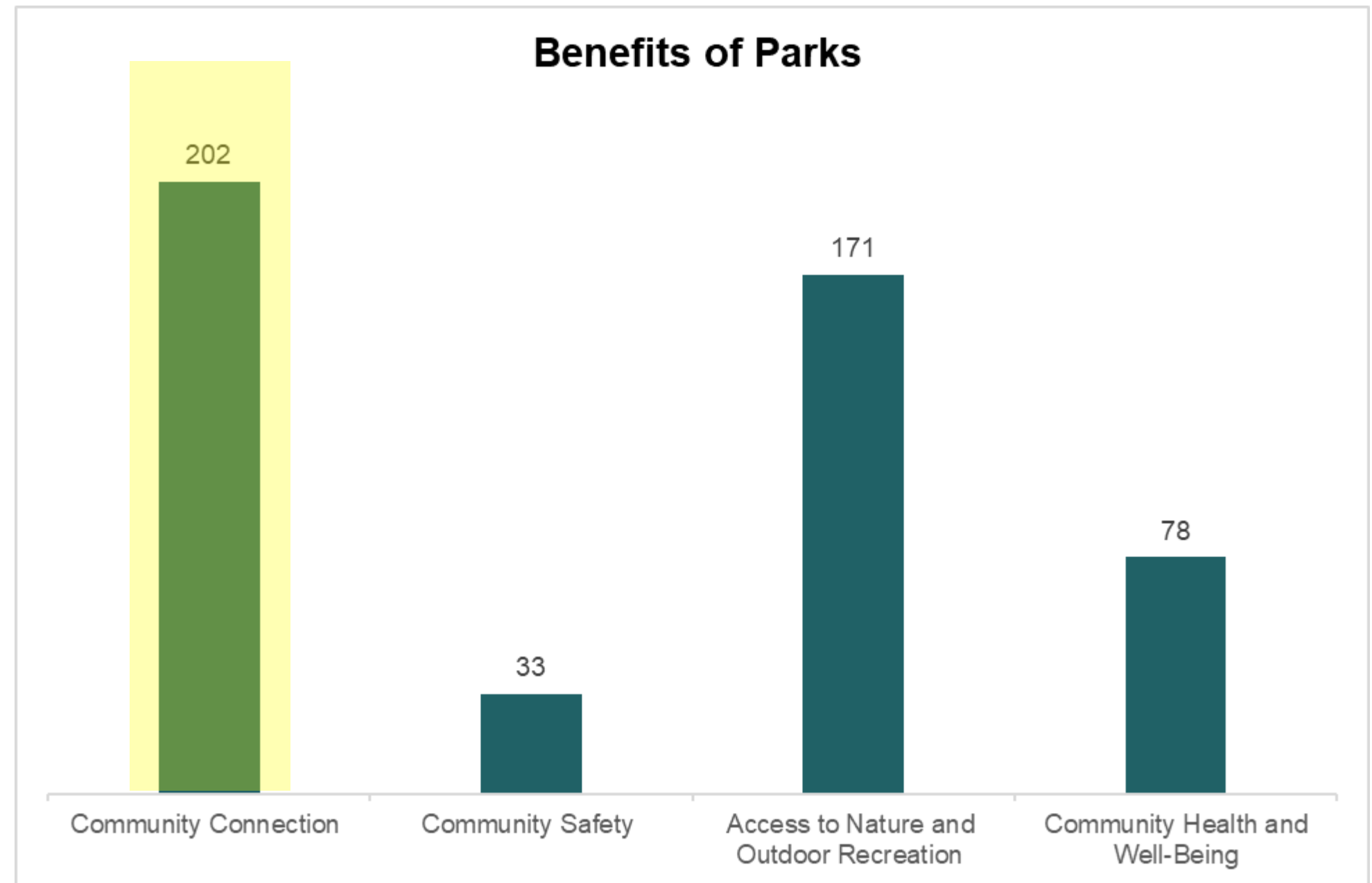
5 themes emerged from the park survey analysis:

- Parks are spaces of community connection and belonging
- Parks promote community health through outdoor recreation
- Uneven access to green space across the city
- Desire for increased park maintenance, improvement, and safety
- Many new community desires and interests require new infrastructure



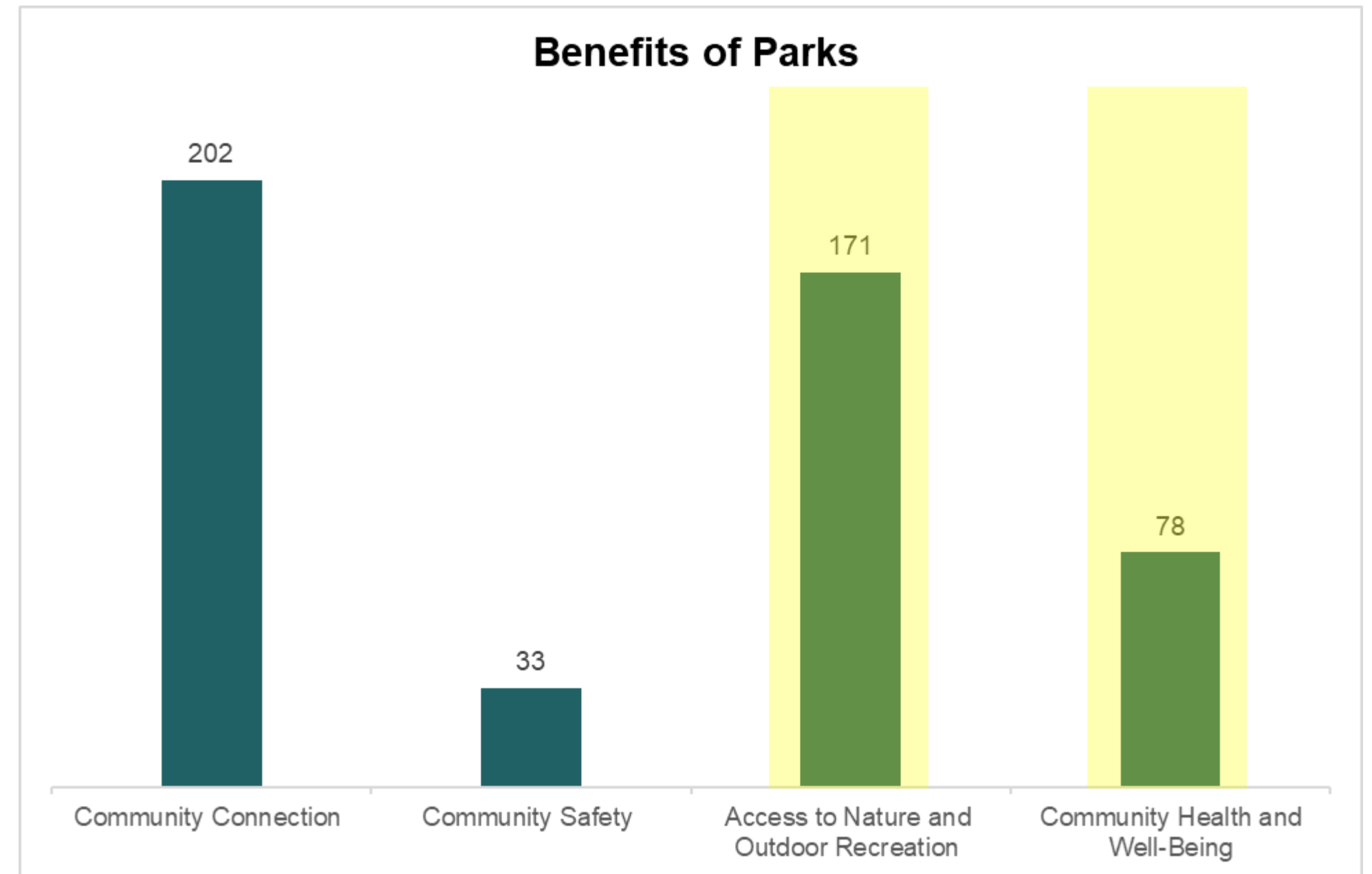
Parks as Spaces of Community Connection and Belonging

- Over 40% of responses highlighted the importance of parks as spaces that **foster community connection, feelings of belonging, and civic pride**
- Parks are one of the last free gathering places
- Parks help people feel rooted and lessen feelings of isolation



Outdoor Recreation and Community Health

- Over 50% of respondents shared that parks play a large role in promoting **community health and well-being** through outdoor recreation and the opportunity to connect with nature
- Outdoor recreational activities identified include: hiking, bike riding, mountain biking, pickleball, visiting community and public gardens, exercise, basketball, picnics, dog walking, skateboarding, roller skating, and playing



Uneven Access to Green Space

- Despite the many perceived benefits of parks, **community responses also highlighted frustration associated with the inequitable access and distribution of green space** in Oakland. Due to historical discriminatory land use policies and decisions about public resources, access to green space in Oakland is not equitably distributed.
- **Lack of access to green space can lead to poorer health outcomes**, such as higher rates of stress, reduced physical activity, and higher rates of mental illness

"Bushrod is one of the few green spaces in North Oakland."

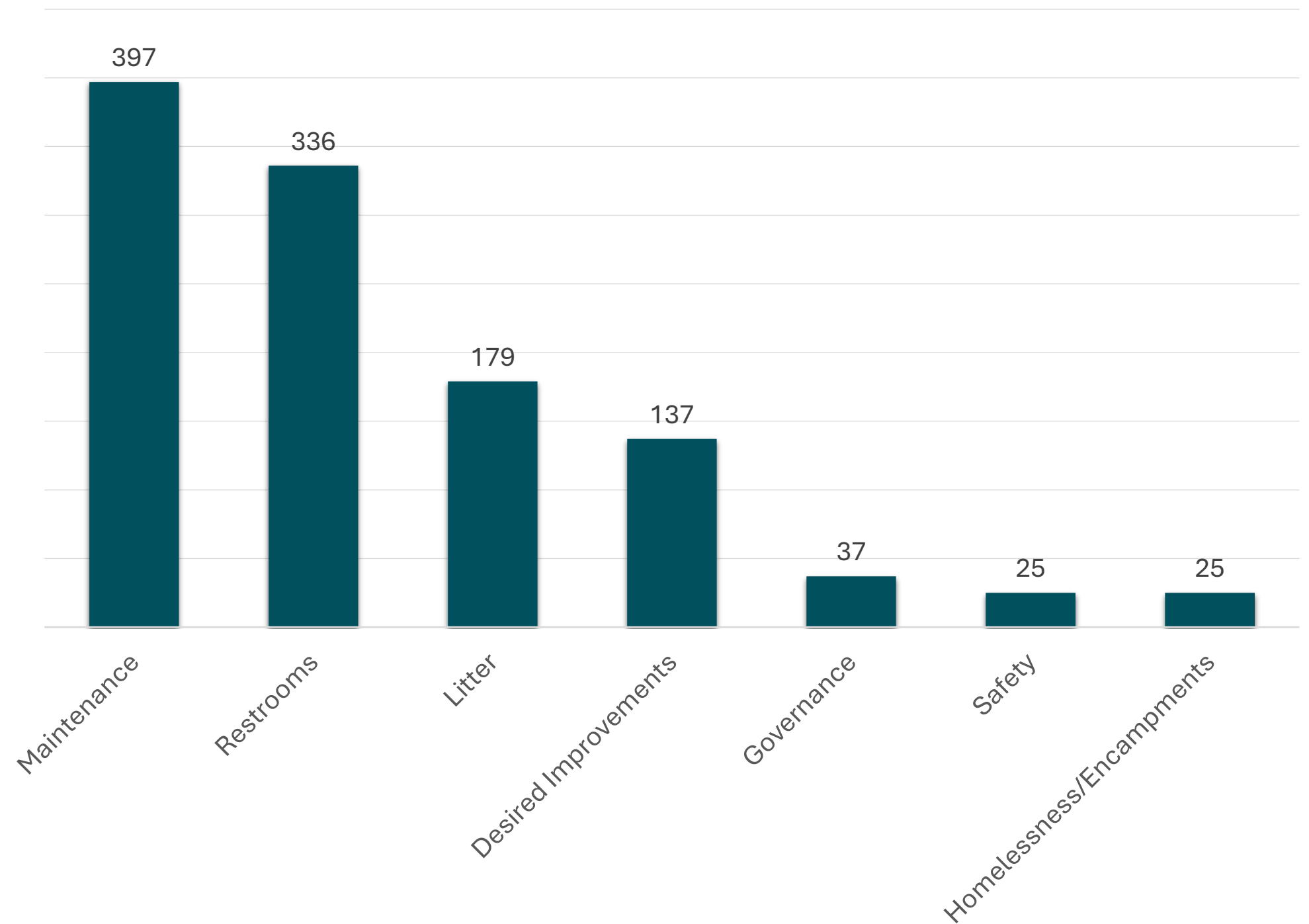
"Yes, public space is where humans gather and meet people. We need many more of these types of opportunities in Oakland."

"There are no real parks in my neighborhood (the Fruitvale). I have to leave my neighborhood to enjoy the calm and beauty of nature."

"[DeFremery Park is a] rare open green space in West Oakland."

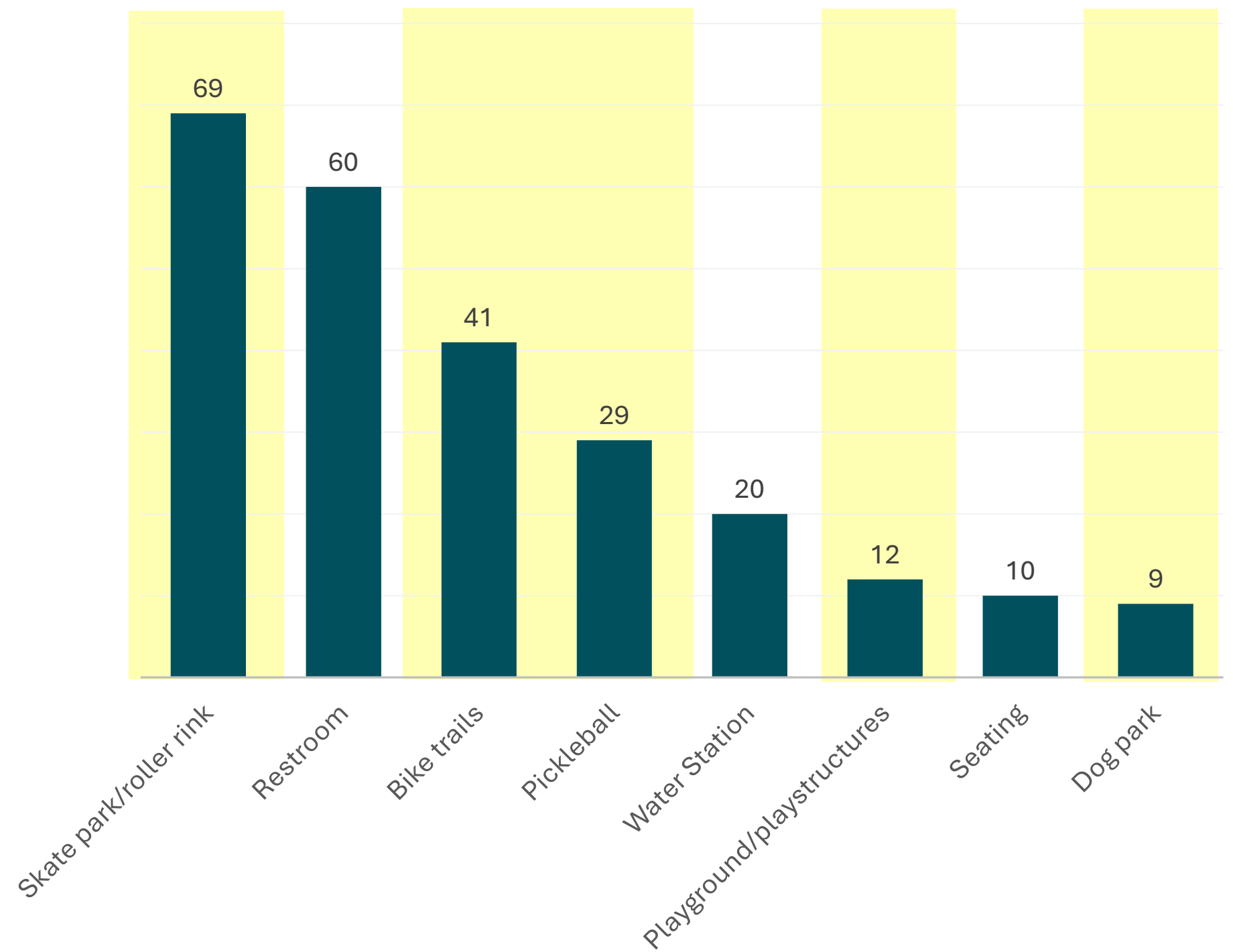
Park Maintenance, Improvement, and Safety

- **Park maintenance** was the top-cited improvement park-goers would like to see at their parks
- **Litter collection** and prevention/clean-up of **illegal dumping** were regularly referenced as a needed improvement
- There is strong support for **improved amenities** at parks, such as restrooms and increased athletic facilities



Evolving Community Desires that Require New Infrastructure

- Many respondents go to Oakland parks to participate in recreational activities that require specific facilities.
- This trend is reflected in the additional amenities respondents would like to see at their parks, including a **roller rink**, **dedicated bike trails**, **pickleball courts**, **updated play structures**, and **dog parks**.



Site Visit Analysis

- Data gathered from the site visits will augment information available online to develop a matrix of park amenities – **a current gap in the City's resources**
- When completed, the matrix will serve as a resource for understanding the distribution of park amenities throughout Oakland, which will inform the development of priorities and policies for Phase 2 of the General Plan Update



Next Steps

- Complete Park Amenities Matrix, **expected February 2025**
- Park Survey and Amenity Matrix will inform General Plan Phase 2 priorities and policies, **2025-2027**



Discussion

Discussion Questions

1. Considering fiscal shortfalls, how can the city fund and more efficiently address **parks maintenance and recreation programs**?
2. What are **issues** the Commission is working on that could be addressed through zoning, permitting, or policy changes?
3. How should the OSCAR prioritize **health and physical activity**?
4. How can the city better **support and work with volunteer park organizations**, (e.g., Friends of Joaquin Miller Park, Friends of San Antonio Park, etc.) in programming and habitat management?

Next Steps

- Visit the website and sign up for the mailing list:
oaklandca.gov/gpu
- Stay tuned for other community events and opportunities to engage



CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission
FROM: Quincy Williams, Assistant Capital Improvement Project Coordinator, OPRYD
DATE: February 5, 2025
SUBJECT: REQUEST FOR THE PARKS AND RECREATION ADVISORY COMMISSION (PRAC) TO SUPPORT A GIFT-IN-PLACE DONATION IN THE AMOUNT NOT TO EXCEED FORTY-THOUSAND FIVE HUNDRED DOLLARS (\$47,500) FROM FOOD OF GOD (FOG) TO COVER THE INSTALLATION OF NEW KITCHEN EQUIPMENT AT THE DEFREMERY RECREATION CENTER KITCHEN

SUMMARY

Food of God (FOG), a 501C3, is a local food organization committed to providing free groceries and a free hot meal every week to West Oakland community members experiencing malnutrition, homelessness, and food insecurity. FOG is a longtime agency partner in good standing with Alameda County Community Food Bank (ACCFB), a vital participant in the ACCFB Food Recovery Program (FRP) (SB 1383) and the StopWaste.org Food Recovery Network (ACFRN). FOG has chosen the DeFremery Recreation Center (DRC) and its kitchen as a future satellite hub for its food (preparation, distribution, and recycling) program.

FOG is a food program that serves mainly BIPOC communities across Oakland struggling to put food on the table. FOG's offer of using existing grant funding to refurbish the DRC kitchen will allow FOG limited use of the DeFremery kitchen to prepare a free vegan hot meal to serve in the tree-canopied spacious DeFremery public park. This combination (a facility in a public park) is a tremendous resource to FOG, which identifies it as a street (outdoor) ministry. A building permit must be completed by FOG and submitted to the City of Oakland Building Permit Department. The DRC welcomes FOG's desire to solidify a long-lasting partnership (see DRC Letter of Support).

An upgraded kitchen will help fulfill FOG's goal of being a valuable community partner in addressing hunger relief in West Oakland and helping to keep perfectly edible food out of the local landfills (recycling). FOG is an active member of the ACCFB Food Rescue Program (FRP) and StopWaste.org Food Rescue Network (ACFRN), retrieving several thousand pounds every month from local retailers and markets (Sprouts, Good Eggs, STAR Market, etc.) FOG's collective vision for a thriving food program at the DRC is to offer food assistance in this 'food desert' for healing and nourishment year-round. Other nonprofit food donors will be asked to join us as often as possible to donate excess produce and staples to our guests on the line (e.g., Dig Deep Farms, Punks With Lunch, etc.).

Currently, the DeFremery kitchen is in limited use by facility employees and the community due to its outdated condition. Approved upgrades to this OPRYD asset will reduce future repair costs. All upgrades will be to code. The collaboration between OPRYD and FOG directly serves the best interests of the West Oakland community in many ways. Their joint effort requires dedicated coordinators/volunteers to use a clean, safe, and reliable modern kitchen. In the future, modernized, other nonprofit food programs may be eligible to lease this upgraded facility at the discretion of the DRC director, Greg McClain.

BACKGROUND / LEGISLATIVE HISTORY

Having identified the DRC and its public park as ideal locations for our food program, FOG and the DRC director, Greg McClain, have collaborated for over two years with Marcos' help. Trujillo (ACCFB) to establish a collaborative, working partnership. We have had the kitchen inspected and have adhered to any/all (Alameda County Department of Environmental Health) ACDEH requirements. A budget has been established, and RESTRICTED funds have been received to cover the cost of the upgrades. Bids for the upgrade work were submitted, and a local contractor (Ned Gendell) was chosen. The FOG board has unanimously approved the upgrade project.

The DeFremery Recreation Center (DRC) is in an impoverished neighborhood. The collective vision for a thriving food program at the DRC is to offer food assistance in this 'food desert' for healing and nourishment year-round.

PROJECT / PROGRAM DESCRIPTION

In this proposal, FOG requests approval to perform upgrades to turn the outdated kitchen into a vibrant, modern, safe cooking center. Upgrades (all addressing and meeting the City's Zoning and Building codes and regulations) will include new electrical conduits, outlets to GFI, new commercial grade flooring, stainless steel cabinet countertops, new sinks (2 basin food prep, 3-basin wash unit, hand washing unit), new drains with grease trap, and a new HVAC make-up air system. The existing upper and lower cabinets will be refaced (painted with an approved epoxy coating).

Old gas lines serving the kitchen will be capped, and a new induction cooking system (green infrastructure) will be installed. The project also entails upgraded exit doors (self-closing for safety) and the installation of a proper open/close window unit in the ventilation space above the prep sink (healthier air). Our goal is to improve the air quality as best we can. The interior roll-up of the existing wooden shutter may be painted (if necessary).

A local general contractor has submitted a bid; FOG has approved a budget, and, if able, the City will assist in obtaining a permit for renovations. These upgrades will complement DeFremery's role in the future ECAP "Resiliency Hubs" designation outlined by OPRYD. Once nominated as an emergency disaster hub, DRC will play a pivotal role as a safe community center during climate change and environmental upheavals. A modern kitchen would be essential in serving the health and safety needs of the community.

New, energy-efficient appliances will be installed. These will include a new induction stove with a convection oven (replacing the defunct gas stove, helping to reduce air pollution), an ENERGY-STAR refrigerator, a freezer, and a lowboy (under the counter) refrigerated unit. FOG hopes to ask for a variance on the requirement to install a new, separate water heater as a high-efficiency unit (50 gal) was recently installed (2023) with the assistance of the Public Works Department. (Willdan Energy Solutions/PG&E grant program). If there is no variance, FOG will install a separate water heater for kitchen use only.

Plans include involving the West Oakland Environmental Indicators Project (WOEIP), which will work with us to give voice and engagement to this oppressed neighborhood. The DRC will be used as a safe space (a "resiliency hub") for residents during climate and natural disasters (earthquakes,

wildfires, heat waves, drought). FOG will participate in food distribution (if necessary) at these times.

Implementation plan (including timeline) Upon PRAC's acceptance of our DRC kitchen upgrade project, FOG will work with the Alameda County of Environmental Health (ACDEH). Once approval has been given, FOG will submit applications for the necessary permits from the Zoning Department and the Building and Planning Department. The contractor will begin work in the winter-spring of 2025. Approximately 60 days are scheduled for these upgrades—another 30 days for installation of appliances. FOG anticipates opening its hot meal and food redistribution program in the summer of 2025. FOG plans to serve 100 folks once a week, every week, all year long. If all these actions/requests are approved and completed, FOG and OPRYD would like to have a re-opening ceremony of the kitchen at DeFremery Park in the summer of 2025.

FISCAL IMPACT

The \$47,500 (forty-seven thousand five hundred dollars) gift-in-place donation will improve usage at the site. The only negative fiscal impact on the City of Oakland will be an increase in utility costs (electric and water usage) due to the improved space with new kitchen equipment, activating its increased usage. This can be offset by the increased rent charged to groups using the kitchen for events. FOG will apply to the Recycling Center, City of Oakland, CIP, StopWaste.org, and the Alameda County Community Food Bank, among others, for funding to cover the cost of the new appliances and sinks. Under our proposal, Public Works, as an agent (OPRYD is OPW's client), will oversee and recommend the maintenance standards for the proposed appliances. OPRYD, as asset owner, will oversee and approve all maintenance services to their assets (appliances). We intend to purchase top-quality cooling and cooking units serving the DRC for many years.

Once the DRC has an upgraded kitchen approved by required entities (PRAC, City Building Permits), it can lease this space to other food programs, providing a steady source of income for years to come.

Also, the proposal will not burden the City of Oakland/DeFremery staffing. Food of God will staff its own feeding and food distribution program. If possible, FOG would like to collaborate with the A.C.C.E.S.S., City of Oakland employment program, which offers employment to retired, low-income seniors with modest incomes. If our application is approved, a small stipend will be provided to prospective senior cooks, cleaners, etc., in the immediate neighborhood.

PUBLIC OUTREACH / INTEREST

In 2011, FOG's successful outdoor feeding and food distribution programs were dedicated to the underserved and the underprivileged: they would like to use the DeFremery kitchen to prepare their meals and to use the surrounding park area to distribute their meals and groceries. This opportunity to renovate and use a community center kitchen will allow FOG to stand as a beacon of hope and welcome to this vulnerable and disenfranchised population in West Oakland. FOG aims to upgrade to code the DRC kitchen for its hot meal program, re-establishing the free food distribution programs organized in the park in the past, e.g., the Black Panthers in the late 1960s. FOG aims to feed, serve, and 'give voice' to those who join us: to assuage the effects of hunger on people experiencing poverty and to reflect on the meaning of good health. FOG aims to improve this area by increasing access to fresh food and essential services.

COORDINATION

FOG, in collaboration with the City of Oakland Public Works, will work to review design upgrades. FOG has a general contractor’s kitchen remodel design available (see Exhibit A enclosed in this document), as well as the formal, itemized budget (See Exhibit B) and letter of support (Exhibit C) from DRC for review.

OPRYD also consulted with the City of Oakland Real Estate Department. Please note the following response from that department. Upon completion and approval of the kitchen upgrades by required entities, including PRAC and the City Building Permits Department, the space can be leased to other food programs. All work will be permitted by the City of Oakland's zoning, Planning, and Design Departments. Public Works (City of Oakland) has helped us access existing plans, designs, and permits.

RECOMMENDATION

OPRYD recommends approval for FOG’s proposal of \$47,500 (gift-in-place) installation of new kitchen equipment upgrades to the DeFremery Community Center kitchen.

Respectfully Submitted,


Quincy Williams (Feb 7, 2025 15:48 PST)

Prepared by:
Quincy Williams
Capital Improvement Project Coordinator, OPRYD


Harith Aleem (Feb 7, 2025 15:48 PST)

Reviewed by:
Harith Aleem
Assistant Director, OPRYD

Identification of Support Documents:

- Attachments: Exhibit A – *Current Pictures and Kitchen Design*
- Exhibit B – *Project Budget*
- Exhibit C – *DRC Letter of Support*

Exhibit A









KITCHEN REMODEL
1651 ADELINE ST.
OAKLAND, CA 94607
DeFremery Recreation Center
DeFremery Park

SCOPE OF WORK
Convert Existing Kitchen to commercial kitchen to serve meals to low income homeless folks.
Kitchen to be run by:
Food of God (FOG)
EIN: 27-5348142
424 Monte Vista Ave
Oakland, CA 94611
Barbara Collins
bardee1@yahoo.com
(510) 480-2521

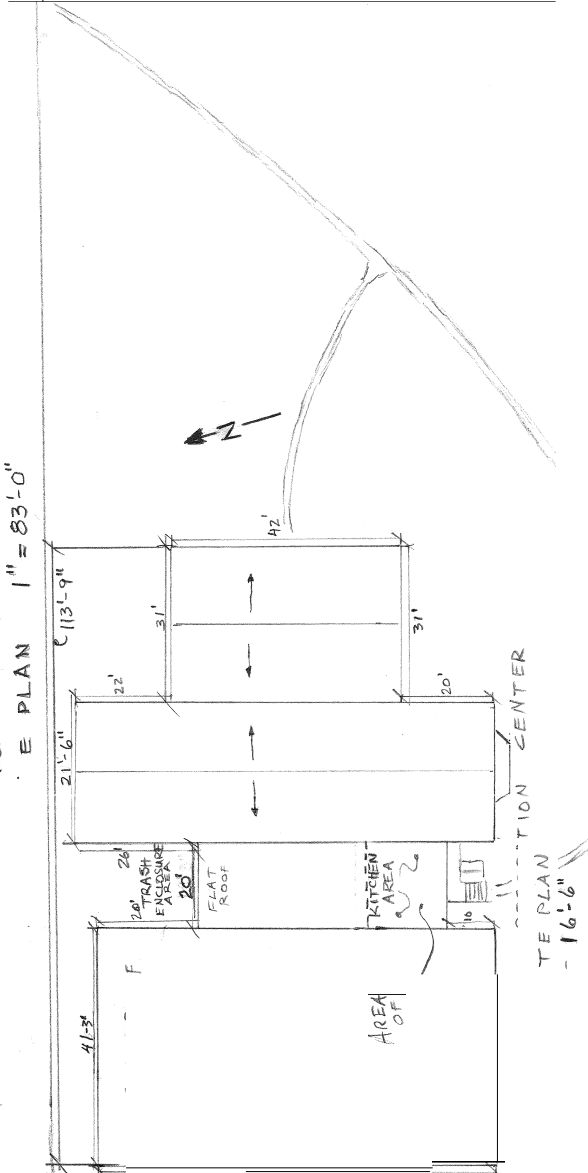
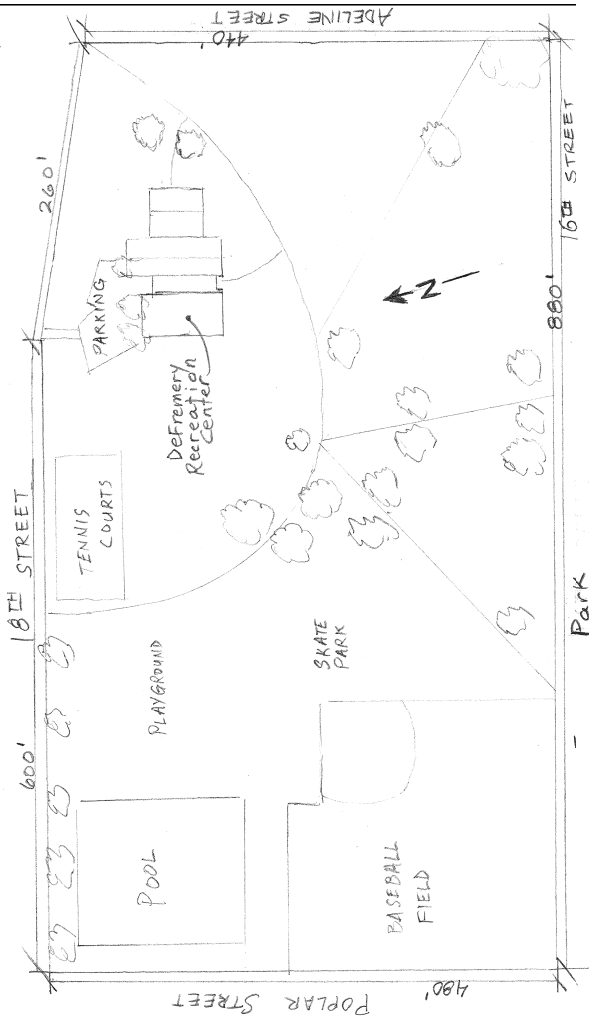
NO STRUCTURAL WORK INVOLVED

OWNER: CITY OF OAKLAND
CONTACT: EDWARD GENDALL
(510) 501-5448
BARBARA COLLINS
(510) 480-2521
1269 18TH ST.
SORT PARCEL
Defremery Contact
Person

ADDRESS: 1269 18TH ST.
REC. CTR. DIRECTOR: GREG MCCLINE (510) 238-3191

KITCHEN Job UPGRADE
BRING EXISTING KITCHEN UP TO COMMERCIAL STANDARDS FOR CHARITABLE OPERATIONS
OF
ELECTRICAL / PLUMBING / MECHANICAL / HYGIENE / FIRE / ENERGY

2022 CBC w/ AMENDMENTS
2022 CFC w/ AMENDMENTS
2022 CFC w/ AMENDMENTS
2022 CFC w/ AMENDMENTS
2022 CFC
2022 CFC
ALAMEDA COUNTY DEPARTMENT OF ENVIRONMENTAL HEALTH CODES
2021 - 114353 AND ALL APPLICABLE CODES



SHEET INDEX
TITLE SHEET / SITE PLAN
PLAN VIEWS
ELEVATIONS
KITCHEN PLAN VIEW/ELEVATIONS
UPPER REFLECTED LIGHT PLAN
HOOD & MAKE UP AIR DETAILS

TE PLAN
- 16'-6"

YEFREMERY RECREATION
CENTER
ELEVATIONS $\frac{3}{4}'' = 1'-0''$

CTR

ELEVATIONS

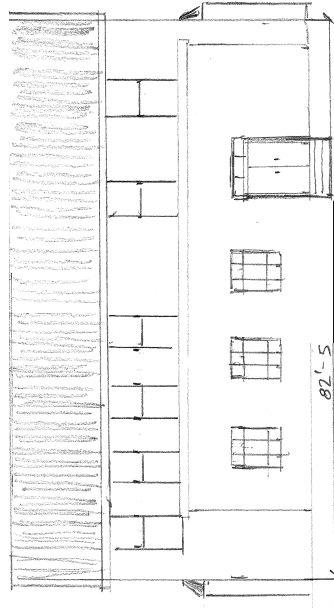


"
WEST ELEVATION

Gendell	
510	
3.01-5.440	

DE FREMERY RECREATION
KITCHEN REMODEL

Date	10/10/24
Scale	3/4" = 10'-0"

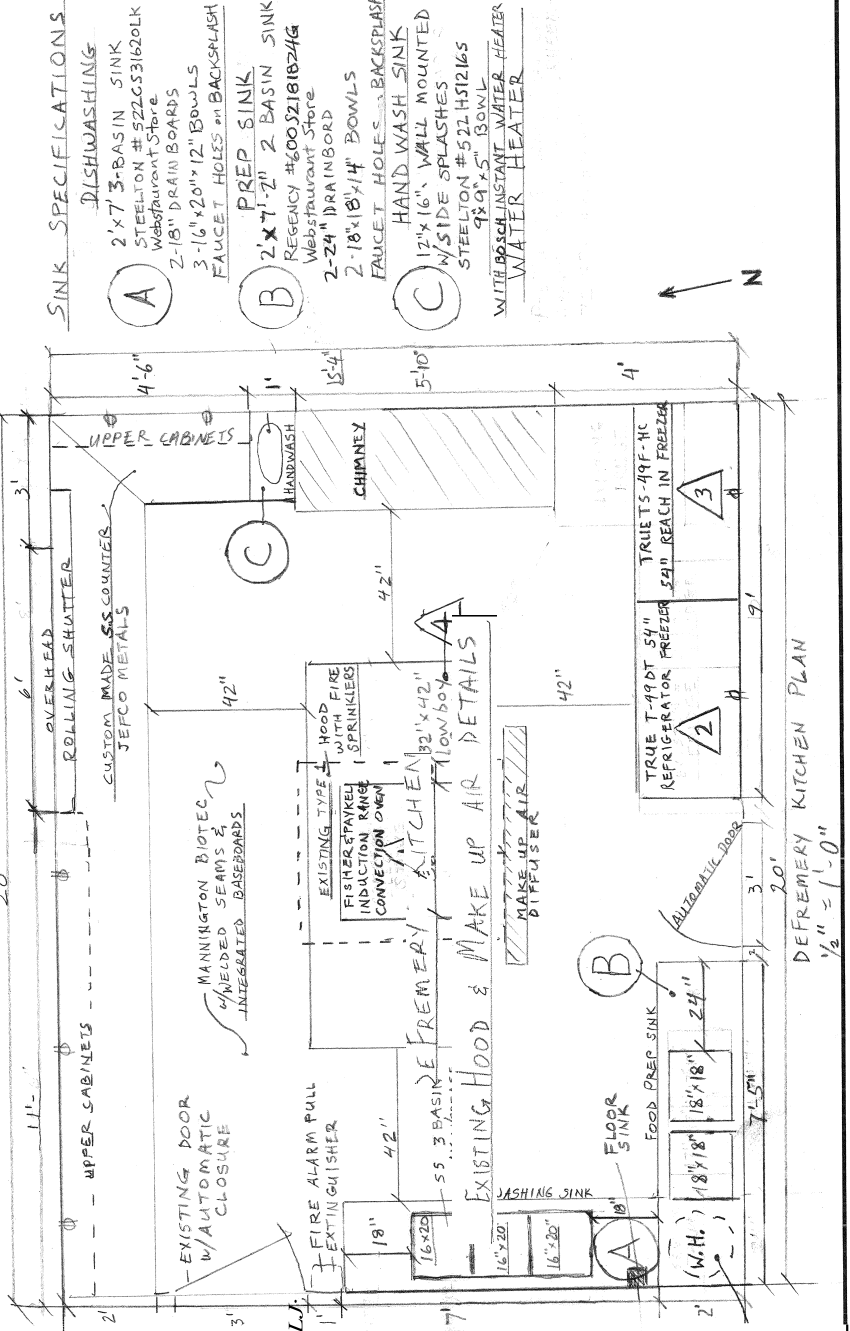
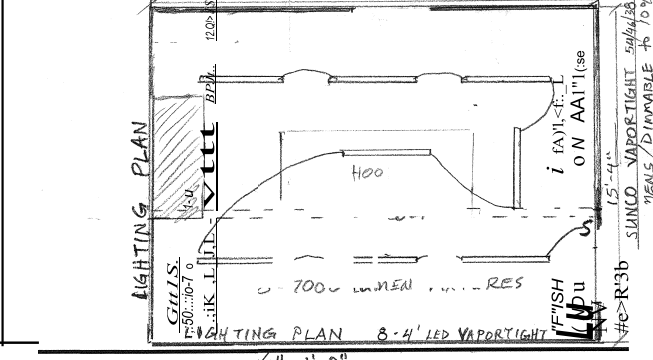
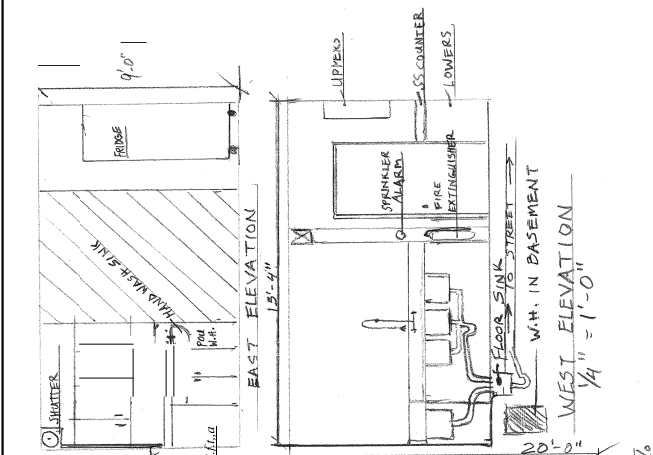
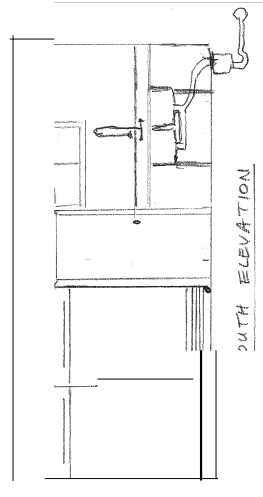
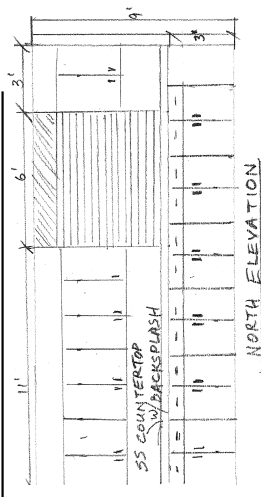


REVISIONS	BY

KITCHEN REMODEL
De Fremery Recreation
1651 Adeline St
Oakland, CA 94607

REVISIONS IV

Date: 10/11/17	Scale: 1/2" = 1'-0"
Drawn: _____	Job: _____
Sheet: A-1	25 of 75 Sheets



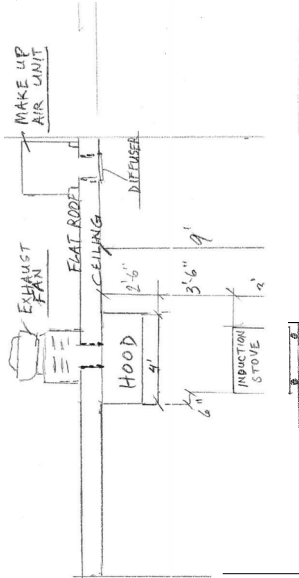
- KITCHEN REMODEL**
- LIGHT DUTY SPECIFICATIONS**
- CHANGE STOVE FROM GAS TO ELECTRIC
 - CFM HOOD FOR W/MAKE UP AIR
 - TYPE 1 HOOD - EXISTING
 - CFM MAKE UP AIR UNIT - NEW
 - ALL WOOD CABINETS & ROLLING SHUTTER TO BE PAINTED WITH COMMERCIAL GRADE EPOXY PAINT
 - FOR PANELING ON ALL WALLS
- WATER FLOORING - 1/2" TH-SEMEJ**
- APPLIANCES**
- 36" x 15.4" ECTION OVEN 40A 220 SC 16X1 25X36"
 - TRUE REFRIGERATOR/FREEZER T 49DT 54" x 29 1/2" x 23 5/16" w/CASTORS
 - TRUE REACH IN FREEZER TS-49F-HC 54" x 29 1/2" x 23 5/16" w/CASTORS
 - TRUE LOW-BOY REFRIGERATOR TUC-40-HC 40 5/8" x 30" x 29 3/4" HIGH

- SINK SPECIFICATIONS**
- (A) DISHWASHING**
2'x7' 3-BASIN SINK
STEELTON # 522CS31620LK
WebFaurant Store
2-18" DRAIN BOARDS
3-16" x 20" x 12" BOWLS
FAUCET HOLES IN BACKSPLASH
 - (B) PREP SINK**
2'x7'-2" 2 BASIN SINK
REGENCY #600SZ1818Z4G
WebFaurant Store
2-24" DRAINBOARD
2-18" x 18 1/4" BOWLS
FAUCET HOLES IN BACKSPLASH
 - (C) HAND WASH SINK**
12" x 16" WALL MOUNTED W/SIDE SPLASHES
STEELTON #522H52165
48" x 5" BOWL
WITH BOSCH INSTANT WATER HEATER
WATER HEATER

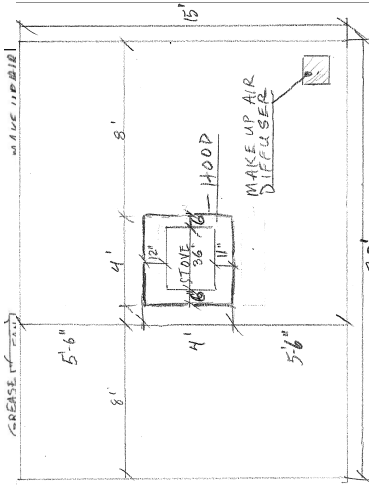


UN

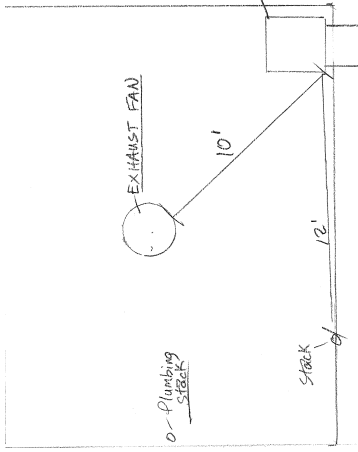
WEST ELEVATION
1/4" = 1'-0"



FRONT ELEVATION
1/4" = 1'-0"



HOOD PLAN INSIDE



HOOD & MAKE-UP AIR MAKE

MARK W Date
1/1 FL 1/11/11 Scale
Drawn by
Of

KITCHEN REMODEL
DETERMERY RECREATION CTR.
1651 ADELIN ST. OAKLAND
DETERMERY PARK
HOOD & MAKE-UP AIR PLAN.

Sheets

A

0/31/24
1/0 = 1'-0"
Edward
Lambert

1/4

Exhibit B

Ned Gendell Quote for DeFremery Kitchen Project

Project: DeFremery Kitchen Upgrades: Project Est. Start Date March 15, 2025 Date July 12, 2024: Planning Stage through December 2024

Operating Costs				
NIAC General Liability Insurance	1650.00	EST		FOG Liability Coverage
FOG	subtotal	1,650.00	1,650.00	n/a
Renovation Expenses				
Item	EST	ACTUAL	Description of Expense	
General Contractor				
Electrical	\$ 6,000.00	6000.00		
New conduit	3,500.00			Add five new 20A dedicated circuits.
GFI outlets and upgrades	2,500.00			Upgrade all outlets to commercial GFI in the kitchen.
NR wiring; 40A stove				Add a subpanel if necessary (see Makeup Air)
Ceiling lighting	300.00			Switch 4 ceiling fixtures to LED
Makeup air wiring				
		subtotal	\$ 6,300.00	
Plumbing	\$ 10,000.00			
Demo removal of cabinets and counter		1400	\$ 1,400.00	Removal of existing countertops and sinks; haul away Removal of upper cabinets; install wire shelving for drainage
Add new drains and vents. Grease trap installation				Install drains and vent; install a grease trap.
*3 Sink NSF Unit install				Install to code 16-18 gauge SS 3 basin sink unit w/ overhead spray faucet. Install SS 16-18
*Food Prep Sink NSF install				gauge two basin prep sink with drainboard with faucet.
*Handwashing Sink NSF				Install SS 18 gauge handwashing unit with mounted faucet (18 x 18 23 1/2)
				(18" sink with L splash guard unit)
\$ 7,000.00	\$ 7,000.00		\$ 8,300.00	
		subtotal	\$ 16,000.00	
		running subtotal	\$ 16,000.00	
HVAC New Exhaust Fan on roof 700 CFM				w Exhaust Fan on roof
HVAC Contractor: MakeUp Air System 640 CFM		5,000.00	7,700.00	chase and install a new 220v makeup air system to code The
		running subtotal	23,700.00	of-mounted 3500 CFM dedicated circuit.
Roof repair				Roof penetration and waterproofing
		subtotal	23,700.00	
General Work: Flooring				
New vinyl flooring Subfloor/Biotec Vinyl	3,500.00		\$ 6,800.00	Commercial Sheet vinyl with welded seams and coving to code
		subtotal	30,500.00	
Subcontractor				
SS 16 gauge L-shaped Countertop with backsplash	5000	5,000.00	\$ 7,200.00	Order and install 25' of SS countertop on N and E. Removal of old and Installation of new SS countertop \$200 an ft x 25 ft plus extra for corner plus 6 ft
		subtotal	24,610.00	
37,700.00				
Add fiberglass boards or tile on walls?				TBD place above existing tile for code if necessary
Additional backsplash behind sink units?				TBD place above 9.5 backsplash of new sink units
Sheetrock removal and repair				replacement and repair, mudding/priming/painting heavy
Refacing of kitchen cabinets				epoxy coating on doors
			2,800.00	
			1,400.00	
subtotal			41,900.00	
Installation of new water heater	TBD	5000	5,000.00	
Misc			600.00	
Self-closing doors				
install new glass window				
		subtotal	47,500.00	
Permits	TBD			
ACDEH			\$ 1,240.00	ACDEH Plan and Design Review
City of Oakland	Zoning	TBD		TBD Estimate for City electrical and plumbing

..

Exhibit C

Office of Parks, Recreation and Youth Development

250 Frank H. Ogawa Plaza, Suite
3330 Oakland, CA 94612
www.oaklandca.gov/departments



DeFremery Recreation Center

1651 Adeline Street
(510) 238-7739

Tam Tran
Chair, Parks and Recreation Advisory Commission Lakeside Park
Garden Center
666 Bellevue Avenue
Oakland, CA, 94610

December 16, 2024

Dear Commissioner Tran,

I am Greg McClain, Center Director at DeFremery Recreation Center ("DRC"). I am writing to support Food of God's ("FOG") request that PRAC review and approve their financial gift of \$47,500 (Gift-In-Place) to cover the installation of new equipment at the DRC kitchen and support future building permits.

FOG is a dynamic local non-profit organization committed to providing free hot meals to Oakland community members experiencing homelessness and food insecurity. Their effort requires hard-working volunteers and a clean, safe, and reliable kitchen to prepare and serve the meals. I became aware of FOG's mission when FOG's Director, Barbara Collins, approached me about using the kitchen at DRC to sustain FOG's operational viability after they were forced to leave a prior kitchen that served as their base.

As Director at DRC, I was excited about possibly assisting FOG. I believe DRC should serve as an asset that well-meaning groups can use to support vital community work that needs to occur in Oakland. As an incentive for my support, Barbara stated FOG would work to secure funds to renovate the kitchen so that DRC could benefit from partnering with FOG. The benefits of a kitchen renovation for DRC would include:

- The Renovation Addresses Safety Risks and Potential Health Hazards - The DRC kitchen is currently not fully up to code in many areas. There are no GFI outlets, the stove and overhead fan need repair, and the floors and cabinets are worn and outdated. DRC staff uses the kitchen to prepare snacks and other meals for youth participating in camps and after-school programs held at DRC. The community also uses the kitchen when repasts and birthday parties are held at the center. Significant improvement to the kitchen would ensure excellent safety for these users.
- The Renovation Creates Access to Healthy Living and Improved Public Health. When the DRC kitchen is updated per FOG's specifications, it will enable FOG to provide free, hot, and nutritious meals to community members. FOG's program will also increase food choices for individuals facing food insecurity. In addition to the food served, FOG provides guidance and support to community members through empathetic volunteers who take the time to interact with clients and form relationships with them. This combination of outstanding food service and compassionate social outreach directly promotes healthy living and public health.

- Renovation Improves and Expands the Quality and Number of Services DRC Can Provide the Community - With an improved, near commercial-grade kitchen, DRC could expand services by offering various cooking classes to teens, adults, and seniors. Food handling certification courses could also be offered, thus preparing people for entry into the food service industry. A kitchen renovation would allow DRC to raise revenue by contracting with caterers and small restaurant owners who need a prep kitchen for their events. Moreover, an improved kitchen would dramatically improve the overall interior aesthetic of the DRC facility

DRC fully supports FOG's desire to use the DRC kitchen to continue operating its food distribution and outreach program because the collaboration between the agencies will serve the best interests of the West Oakland community in several ways. First, DeFremery Park is an area that has a high degree of homelessness and food insecurity. FOG is dedicated to ending hunger in the community. By allowing FOG to renovate and use its kitchen to prepare the hot meals it serves, DRC will provide a tremendous resource to FOG and fulfill its goal of being a valuable "community center" asset in addressing hunger in Oakland.

Second, the FOG/DRC partnership "activates" DeFremery Park for a critical purpose. The Oakland Parks, Recreation, and Youth Development Department ('OPRYD') parks are often underutilized. The parks can be, but are rarely, used to satisfy more than the community's recreational needs. By establishing its cooking operations in the DRC kitchen and serving meals out in the park, FOG uses the park in a critical, non-recreational way that will attract community members to the park who might not visit otherwise. Our parks are intended to benefit the entire community, not just recreation enthusiasts. FOG's program at DeFremery Park helps achieve that aim, especially by catering to some of Oakland's most marginalized community members.

Third, the FOG/DRC collaboration offers the possibility of a positive intergenerational experience for the young people who attend the recreation center. FOG relies heavily on volunteer staff members who are seniors in the community. The seniors' presence, demonstrating the importance of a commitment to serving others, is a valuable act of community involvement for youth to witness. FOG's presence will give DRC youth the chance to interact with, learn from, and develop friendships with members of an older generation. Moreover, exposure to the FOG volunteers will show youth they can help alleviate hunger and perhaps other troubling societal issues.

DRC is prepared to enter an OPRYD facility contract with FOG to use the DRC kitchen. The facility contract will bind both parties and guarantee FOG's exclusive use of the DRC kitchen on a day and time they prefer during the week. Furthermore, FOG and DRC will create and sign a Memorandum of Understanding (MOU) that expresses the specific details of the FOG/DRC collaboration and clarifies the obligations of both entities regarding the use of the DRC kitchen. There is a strong willingness and a desire to solidify a long-lasting partnership between FOG and DRC. Homelessness is close to becoming an intractable problem in Oakland. As Director of DRC, I intend for the FOG/DRC partnership to continue indefinitely because the need to provide meals to unsheltered residents and other food-insecure people will continue for many years.

The FOG/DRC partnership will take time to develop fully, but when it does, both organizations expect it to be a longstanding and vital addition to the array of services DRC provides.

For the reasons stated above, on behalf of DRC, I wholeheartedly endorse the review and approval of FOG's financial gift of \$47,500 (Gift-In-Place) to cover the installation of new equipment in the DRC kitchen. FOG's gift to and forming a partnership with DRC will benefit DRC and serve the Oakland community for many years.

Thank you for the opportunity to share my thoughts on this matter.

Sincerely,

A handwritten signature in cursive script, appearing to read "Greg McClain", followed by a horizontal line.

Greg McClain,
Center Director

DeFremery Recreation Center
gmcclain@al.oaklandca.gov



CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission
FROM: Fred Morris, Sports Coordinator, OPRYD
DATE: February 5, 2025
SUBJECT: REQUEST FOR THE PARKS AND RECREATION ADVISORY COMMISSION TO APPROVE A GIFT-IN-PLACE DONATION IN THE AMOUNT NOT TO EXCEED FORTY-NINE THOUSAND FIVE HUNDRED DOLLARS (\$49,500) TO RENOVATE IRA JINKINS RECREATION CENTER GYMNASIUM

SUMMARY

This proposal outlines a scope of work, costs, and deliverables for the Damian Lillard Foundation and Adidas to renovate and resurface the gymnasium inside of Ira Jenkins Recreation Center (978 Edes Ave.) in partnership with the City of Oakland Recreation and Youth Development (OPRYD). The proposed scope of work consists of cosmetic, aesthetic, and functional repairs, similar to past basketball court and gymnasium refurbishments completed in OPRYD recreation centers.

Resurfacing the gymnasium floor at Ira Jenkins Recreation Center will allow for the replacement of existing basketball and volleyball court lines and the addition of pickleball court lines, transforming this space into a multipurpose floor that can be shared by Oakland youth, adults, and seniors. The existing wooden backboards on the side courts will be replaced with glass backboards, while the existing score clock, shot clocks, and wall padding will be replaced. OPRYD is requesting the Parks and Recreation Advisory Commission (PRAC) support the gift-in-place donation of \$49,500 for the resurfacing and renovations of the Ira Jenkins Recreation Center Gymnasium (D7).

BACKGROUND / LEGISLATIVE HISTORY

Damian Lillard is a product of Oakland Parks and Recreation. Lillard was a member of Ira Jenkins youth basketball programs and later graduated from Oakland High School. He would go on to play collegiate basketball at Weber State University before being selected with the 6th pick in the 2012 NBA Draft by the Portland Trailblazers. Damian sponsors one of the Bay Area's oldest AAU high school basketball programs, the Oakland Rebels. Every summer, these teams compete with other national top-ranked high school players seeking Damian's NBA dreams. Damian's dedication extends beyond basketball; he is profoundly invested in Oakland's youth's future.

He recognizes their potential and dedicates substantial time, energy, and resources to developing and sustaining programs that foster individual growth and community well-being. His commitment to the youth has been widely recognized. PBWA President Josh Robbins of The Athletic stated, "Damian Lillard's ongoing and steadfast dedication to the youth of Oakland and Oregon stands out. His efforts to inspire and uplift the next generation are commendable and a testament to his character." Through his actions, Damian exemplifies a strong sense of responsibility to his Oakland community to inspire future generations to reach their full potential. These efforts highlight his deep commitment to positively impacting the community.

PROJECT / PROGRAM DESCRIPTION

Ira Jinkins Recreation Center (D7) is located at 978 Edes Avenue. The Damian Lillard Foundation and Adidas will renovate the gymnasium jointly. This renovation will bring positive activity space into the neighborhood and accommodate youth sports, adult sports, and community events. The project scope consists of:

- Resurface the multipurpose gymnasium floor to include basketball, volleyball, and pickleball lines.
- Paint the gymnasium interior walls.
- Replace the existing side wooden backboards with glass backboards.
- Replace the wall padding.
- Replace the existing scoreboard and shot clocks.

FISCAL IMPACT

OPRYD does not anticipate any direct costs associated with renovating the Ira Jinkins Recreation Center Gymnasium. The Damian Lillard Foundation and Adidas will finance and provide a court resurfacing project, which includes all costs for cleaning litter, paint, and supplies.

PUBLIC OUTREACH / INTEREST

Damian Lillard has hosted back-to-school community events at Ira Jinkins Recreation Center for over eight years. The Damian Lillard Ira Jinkins Back-To-School event provides backpacks, shoes, clothes, school supplies, food, and local entertainment. At various youth, adult, and community practices, games, and events hosted at Ira Jinkins Recreation Center, the community has expressed interest in resurfacing the court based on the amount of heavy usage and foot traffic throughout the year. Most recently, in December of 2024, the Parks and Recreation Advisory Commission (PRAC) supported a Pickleball Pilot Program at select locations throughout the OPRYD's park system, including the Ira Jinkins Recreation Center gymnasium. Adding new pickleball activation at the Ira Jinkins Recreation Center Gymnasium is expected to increase morning and daytime usage amongst adults and seniors in East Oakland. Resurfacing the gymnasium floor at Ira Jinkins Recreation Center will allow for the replacement of existing basketball and volleyball court lines and the addition of pickleball court lines that can be shared by Oakland youth, adults, and seniors.

COORDINATION

OPRYD has contacted the city departments to get a response to this proposal. The response is as follows:

- City of Oakland Public Works (OPW) - Facilities Services Division supports the process of resurfacing and renovation.
- The City of Oakland Bureau of Planning considers court resurfacing and replacement of existing equipment a repair that does not trigger a Conditional Use Permit (CUP).

RECOMMENDATION

Staff recommends the Park and Recreation Advisory Commission (PRAC) support the gift-in-place donation of \$49,500 for the resurfacing and renovations of the Ira Jinkins Recreation Center Gymnasium (D7).

Respectfully Submitted,

Frederick Morris

[Frederick Morris \(Feb 7, 2025 09:13 PST\)](#)

Prepared by:

Frederick Morris III

Sports Coordinator, OPRYD

Justice Bolden

[Justice Bolden \(Feb 7, 2025 09:14 PST\)](#)

Reviewed by:

Justice Bolden

Recreation General Supervisor, OPRYD

Fred Kelley

[Fred Kelley \(Feb 7, 2025 09:37 PST\)](#)

Approved by:

Fred Kelley

Director, OPRYD

Identification of Support Documents:

Attachments: Exhibit A – *Current Condition of Ira Jenkins Gymnasium Court*
Exhibit B – *Proposed Ira Jenkins Gymnasium Court Resurfacing Scope of Work*
Exhibit C – *Ira Jenkins Gymnasium Court Design Concept*

EXHIBIT A – Current Condition of Ira Jenkins Gymnasium Court



EXHIBIT B – Proposed Ira Jenkins Gymnasium Court Resurfacing Scope of Work

- Resurfacing
 - Sand blast floor and remove paint
 - Paint Floors
 - Replace Basketball Court Lines
 - Replace Volleyball Court Lines
 - Install Pickleball Court Lines
 - Install Damian Lillard Adidas Logos and Graphics
- Replace Existing Scoreboard and Shot Clocks
 - Install Scoreboard
 - Install a pair of shot clocks
- Wall padding
 - Install Adidas padding
- Painting
 - Paint the upper gymnasium walls
- Replace existing four (4) wood backboards with glass backboards
 - Install Adidas glass backboards with padding

Estimated Costs

Resurfacing - \$30,000

- Sand blast floor and remove paint
- Paint Floors
- Replace Basketball Court Lines
- Replace Volleyball Court Lines
- Install Pickleball Court Lines
- Install Damian Lillard Adidas Logos and Graphics

Replace existing score clocks - \$12,000

- Replace the existing score clock
- Replace existing shot clocks

Replace Wood Backboards - \$5,100

- Replace existing wooden backboard with glass backboards

Painting - \$1,500

- Paint the upper gymnasium walls

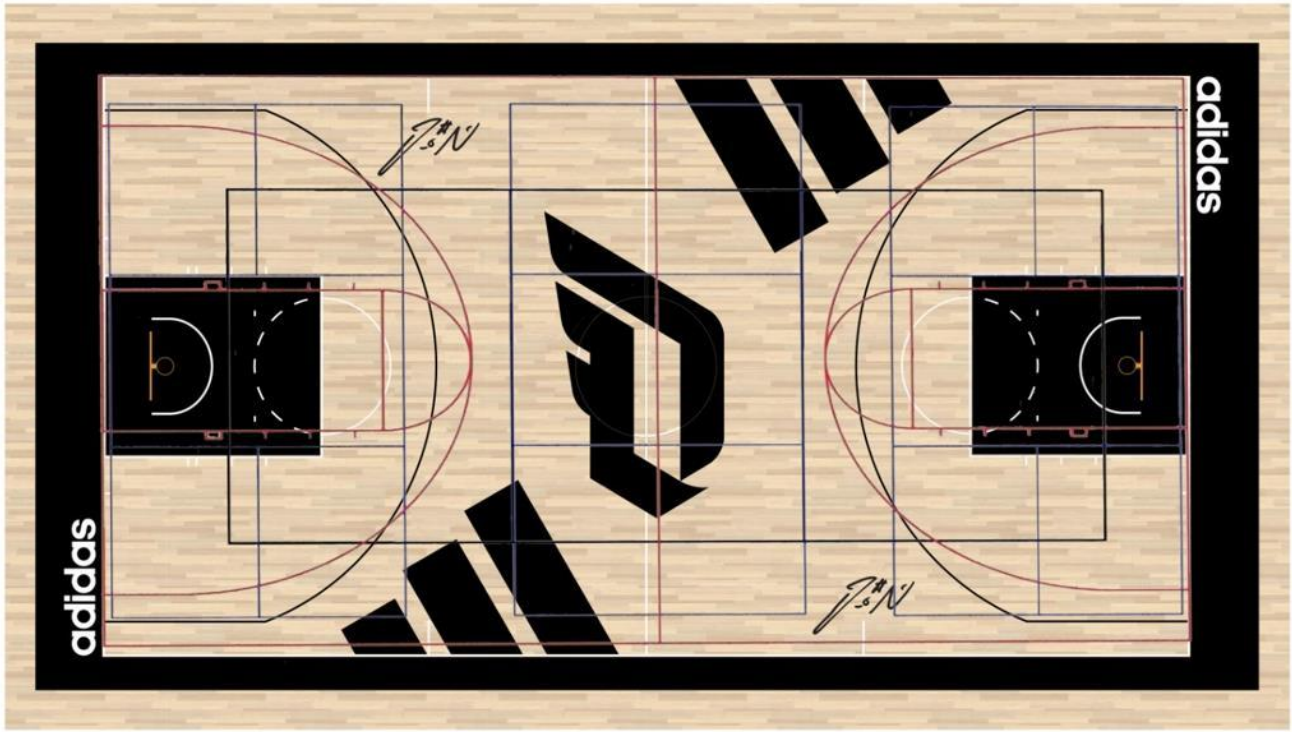
Wall Padding - \$900

- Replace existing wall padding with Adidas padding

Summary

Line	Cost
Resurfacing	\$30,000
Replacing existing score clocks and shot clocks	\$12,000
Replace wooden backboards with glass backboards	\$5,100
Painting	\$1,500
Replace existing wall padding	\$900
Grand Total	\$49,500

Exhibit C - Ira Jenkins Gymnasium Court Design Concept





CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Tam Tran, Chair, Parks and Recreation Advisory Commission
FROM: Felicia Gotthelf, OPRYD
DATE: February 12, 2025
SUBJECT: **REQUEST FOR THE PARKS AND RECREATION ADVISORY COMMISSION TO 1) MAKE A RECOMMENDATION TO THE BUREAU OF PLANNING TO APPROVE AN APPLICATION FILED BY THE OAKLAND BALLERS, A PROFESSIONAL BASEBALL TEAM, FOR IMPROVEMENTS AND UPGRADES TO RAIMONDI PARK, AND 2) PROVIDE FEEDBACK TO THE BUREAU OF PLANNING FOR APPROVAL OF A FUTURE APPLICATION BY THE OAKLAND BALLERS FOR ON-SITE ADVERTISING AT THE RAIMONDI PARK BASEBALL FIELD BASED ON THE PRELIMINARY SIGNAGE CONCEPT, AND 3) TO MAKE A RECOMMENDATION TO CITY COUNCIL TO APPROVE A TEN (10) YEAR LICENSE AGREEMENT FOR USE OF THE RAIMONDI PARK BASEBALL FIELD BY THE OAKLAND BALLERS.**

SUMMARY

This report seeks a recommendation from the PRAC to the Bureau of Planning on an application filed by the Oakland Ballers for improvements to the City-owned, OPRYD-operated Raimondi Park. The current application includes a request for a Minor Conditional Use Permit (“CUP”) for athletic fields, new accessory structures, new restroom facilities, and new concessions stands. The current application also includes a request for a Minor Variance for the height of the outfield fence along Campbell Street, of which 45 feet in height is allowed while 50 feet is proposed.

This report seeks a recommendation from the PRAC to City Council on a license agreement for the Raimondi Park Baseball Field (“Field”) by the Oakland Ballers for a 10-year term. As consideration for the license agreement, the Ballers would pay to the City 1) base rent, 2) a surcharge on ticket sales, 3) a share of (future) naming-rights revenue and 4) a share of (future) on-site advertising revenue. The amount of the base rent is TBD by appraisal, with a credit to the Ballers for up to 50% of base rent for the cost of capital improvements made by the Ballers to Raimondi Park and the Field. The ticket surcharge would begin at \$0.50 per paid admission, increasing by 3% annually. Additionally, the City would receive 50% of any future naming rights revenue and 20% of any future on-site advertising sign revenue, less reasonable third-party commissions, manufacturing and installation costs (this would be dependent on future approval of an application that has not yet been filed).

City staff from OPRYD, Real Estate Division and City Administrator’s Office have negotiated a term sheet with the Ballers for the license agreement between the Ballers and the City for their use of

the Field, attached as **Exhibit A**. **Exhibit B** provides plans and a draft rendering of the Oakland Ballers proposed improvements and renovations to the Field for their upcoming 2025 season. Once completed, these renovations will provide Oakland youth and the Oakland community at large a greatly enhanced neighborhood park.

In addition to the application for improvements currently filed with the Bureau of Planning, the Ballers are also exploring the possibility of including advertising signage at Raimondi Field. The preliminary signage concept is attached as **Exhibit C**. This would require the filing of a Major Conditional Use Permit for consideration by the Planning Commission. Staff expect the Ballers will file an application for advertising signs and, therefore, this report also seeks feedback from the PRAC on the initial signage concept plan, and if supported, requests a recommendation to the Bureau of Planning that may be used for consideration in processing such application.

FISCAL IMPACT

If approved, the Ballers License Agreement would generate four forms of payments to the City. These payments will provide needed support to OPRYD and the City.

1. *Base rent.* First, the Ballers would pay to the City a base rent per month in an amount to be determined by an appraisal that is currently underway.
2. *Ticket surcharge.* Second, the Ballers would apply a \$0.50 surcharge, subject to 3% annual escalations, on each ticket sold and pass that revenue on to the City.
3. *Naming rights revenue.* And third, the Ballers would share with the City 50% of any future naming rights revenue.
Signage revenue. Finally, the Ballers would share with the City 20% of any future on-site advertising sign revenue.

Signage and naming rights revenue would be less reasonable third-party commissions, manufacturing and installation costs.

The Ballers would provide a number of in-kind benefits to the City and community. This includes free clinics for Oakland residents, including MLB Play Ball Camp, Oakland Ballers Camp, Coaches Clinic and Little Ballers Program; a Field Maintenance Internship Program for City employees, providing needed skilled training opportunities; and a dedicated on-site office space for the City's use measuring at least 100 square feet.

Once the Ballers have a permanent concessions structure, they intend to allow its use by the City and other community partners. This provides a to-be-determined fundraising opportunity for Field users.

The minor CUP would result in repairs and improvements to the Park at a value still to be determined. The agreement additionally provides ongoing security, field and site maintenance and repair services that the City is financially unable to provide.

PROJECT / PROGRAM DESCRIPTION

Background.

Prior to 2024, the baseball field at Raimondi Park was unplayable. The Oakland Ballers Baseball Club, LLC, a Delaware limited liability company ("Ballers" or "Oakland Bs") provided ~\$1.6

million in improvements and repairs and began using the Field for their inaugural 2024 Pioneer league baseball season. The Ballers now wish to continue this partnership with the City; to continue funding and executing improvements and repairs to Raimondi Park; to enter into a 10-year agreement allowing their use of the Field for practice and “home” games; to maintain and secure the Field and provide community access when not in use by the Ballers; and to provide ongoing community benefits.

Raimondi Park’s “adoption” by the Oakland Ballers baseball team is providing West Oakland with an infusion of excitement, community spirit, financial benefit, and neighborhood improvements. Cofounded by Bryan Carmel and Paul Freedman as the first West Coast franchise of the historic Pioneer Baseball League, the Oakland B’s formed in 2023 with the intent of building a team that honors the legacy of Oakland’s past great players and that values uniting Oaklanders through the power of community sports and teamsmanship.

In preparation for their inaugural season, in early 2024, the City entered into a Right of Entry Agreement with the Oakland Ballers, allowing the Ballers to begin repairing the field and its public restrooms. This included general repairs to the existing baseball field facilities. Further improvements to the field were also heard by the PRAC as part of a Minor Conditional Use Permit application to the Bureau of Planning for the installation of bleachers, dugouts, scoreboard upgrades, netting, a batter’s eye and fences to conform with league standards.

The Ballers community-centered approach of partnering with the City of Oakland is evident in the rejuvenated Raimondi Park, which is now a safer and more welcoming environment with a professional-grade baseball field where all Oaklanders may come and play. Park improvements in this underserved neighborhood contribute to West Oakland pride, provide youth with a safe and modernized facility, make Oakland an attractive destination for athletic excellence, and offer the possibility for teams to represent Oakland, in Oakland, on a global stage.

Since first expressing interest in Raimondi Park, the Ballers have continued their committed and ongoing community engagement efforts. As part of their initial outreach effort, the Ballers went door-to-door, expressing their intent to build with community and uplift the Oakland community through the love of sports. They committed to holding bi-weekly community meetings until their first home game on June 4, 2024. Their belief in partnering to co-create the experience is manifest in working closely with fans and viewing everyone involved/affected as a stakeholder.

Community members continue to express overwhelming support for this project, which enhances the beauty, safety, and usability of Raimondi Park. Oakland has a long history of creating community excitement around sports teams, and the Oakland Ballers are no exception.

Renovations/Improvements Implementation plan.

As part of the current proposed License Agreement extension for the Ballers use of the field, they are looking to provide additional upgrades to the park and field that require a new application with the Bureau of Planning. These upgrades include permanent site perimeter fencing to replace the previously approved temporary fencing that surrounded the baseball field and license area. Upgrades to the baseball field include reconstructed outfield fencing and netting including a new taller wall and netting up to 50 feet in height at the eastern edge of the site along Campbell Street to prevent homerun balls from going into the street, new batting cages and other accessory structures to the

athletic fields for storage and other ancillary uses. Upgrades to the park will also include new concession and merchandise stands and new restroom facilities that would be located on the western side of the baseball field. The additional proposed improvements require a Minor Conditional Use permit as well as a Minor Variance for the outfield fence height as noted earlier in this report. This application has been filed with the Bureau of Planning (case file number PLN24175) and a recommendation on the application by the PRAC is required prior to a decision on the application by the Planning Director pursuant to Planning Code Sections 17.11.060 and 17.135.030.

The Ballers will also continue to operate under a Special Event Permit through the Economic & Workforce Development Department to govern their use of the Field during game days. This permit would continue to provide requirements to address issues and regulate activities such as crowd control, alcohol sales, parking, noise, trash, lighting, and transportation.

As noted earlier in this report, the Ballers are also exploring the possibility of including advertising signage at Raimondi Field. Any future application for such signage would require the filing of a Major Conditional Use Permit with the Bureau of Planning, which would require approval by the Planning Commission. Staff seeks feedback from the PRAC on the initial signage concept plan, and if supported, requests a recommendation to the Bureau of Planning that may be used for consideration in processing such application.

A project timeline is included here as **Exhibit D**.

BACKGROUND / LEGISLATIVE HISTORY

Raimondi Park was named for Oakland Oaks minor league baseball player Ernie Raimondi, who died in World War II. Baseball Hall of Fame outfielder Frank Robinson, National League All-Star center fielder Vada Pinson, and activist and Gold Glove winning center fielder Curt Flood are among the legendary athletes who played on this field as McClymonds High School students. Situated in West Oakland, the Park is in a historically underserved neighborhood. More than a decade ago, Raimondi Park was identified as an existing Oakland park in need of renovation and rehabilitation under the City's Fiscal Year 2003-2005 Capital Improvement Program budget. An overall concept plan was developed in 2003 and 2004 by the Oakland Parks and Recreation Foundation (OPRF, formerly Friends of Parks and Recreation), with input from the community and the City. The overall estimated cost for the entire project at the time was approximately \$7.2 million. Due to a widespread economic downturn, and lack of adequate funds to complete the entire park restoration at one time, a two-phase plan was developed (Phase 1 and 2). After Council approved the capital improvements in 2007, Phase 1 commenced and was completed in September 2008.

As a result of significant budget cuts, OPRYD initiated creative avenues to increase revenues for the capital improvement needs in Oakland's parks. By partnering with its users, community non-profits and businesses, OPRYD has been able to better sustain its parks and sports fields to ensure safe and community-oriented environments. In this vein, the initial use of the Field by the Ballers was discussed at PRAC on January 10 and March 13. The initial ordinance, authorizing entrance into a license agreement, was introduced by Councilmember Fife and City Administrator Johnson and approved at the April 16, 2024 City Council meeting ([Ordinance No. 13791 C.M.S.](#)). The initial license and Minor CUP allowed the Ballers to provide a gift of significant improvements to Raimondi Park; restore and utilize the Field as a professional baseball team's "home"; enable

Oakland youth teams to practice and play on a greatly improved Field; and provide year-round benefits to the Oakland community at large.

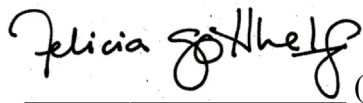
Concurrently, the City Council adopted [Resolution No. 90194](#) authorizing a professional services agreement for the Ballers to remove contaminated soil stockpiles from an undeveloped, City-owned lot at 1707 Wood Street and authorized a license agreement for the Ballers to utilize the same property as a parking area during home games.

The Ballers are now requesting that this mutually beneficial partnership be continued for a 10-year period as described herein.

RECOMMENDATION

Staff recommends the Parks and Recreation Advisory Commission 1) make a recommendation to the Bureau of Planning to approve an application filed by the Oakland Ballers, a professional baseball team, for improvements and upgrades to Raimondi Park, and 2) provide feedback to the Bureau of Planning for approval of a future application by the Oakland Ballers for on-site advertising at the Raimondi Park baseball field based on the preliminary signage concept, and 3) to make a recommendation to city council to approve a ten (10) year license agreement for use of the Raimondi Park baseball field by the Oakland Ballers.

Respectfully submitted,

 (Signature)

Prepared by:
Felicia Gotthelf
Management Assistant

 (Signature)

Approved by:
Fred Kelley
Director

Identification of Support Documents:

Attachments: Exhibit A – *License Agreement Term Sheet*
Exhibit B – *Property map and Proposed Plans Draft Rendering*
Exhibit C – *Signage Proposal*
Exhibit D – *Timeline*

Exhibit A
License Agreement Terms Sheet

Ballers Phase II Term Sheet
January 23, 2025

Form of agreement	License
Term	10 years
Base rent	TBD by appraisal Future capital improvements (not repairs or maintenance) to Raimondi Park will be credited up to 50% of Base Rent (not Additional Rent) Paid monthly Re-appraisal at year 5
Additional rent	50% of naming rights revenue and 20% of sign revenue, net of reasonable third-party commissions, manufacturing and installation costs, to be dedicated to park facilities and/or programming \$.50/paid admission increasing 3% annually, to be renegotiated at year 5 to a mutually agreeable rate Paid quarterly
Utilities	Ballers responsible (can recover third party utility usage from reservation fees)
Security	Ballers responsible 24x7x365
Repairs & maintenance	Ballers responsible, including for maintaining the field in playable condition throughout the year
City access	City can access license area at any time not reserved by Ballers, City has keys City can access license area at any time for emergencies City has exclusive use of on-site office space measuring at least 100 SF
Public access	Ballers and OPRYD staff meet at least 2x annually (approximately January and May) to coordinate the calendar Ballers have priority scheduling during season, including spring training and post-season ("Season"), to be blocked-off prior to Season Ballers provide and manage reservation system year-round

Ballers collect, keep reservation fee
 Ballers facilitate access to/use of locker room, restroom, scoreboard, public address system
 Reservations required except during designated open play times, minimum of 25 days and more give if sufficient demand
 Reservable for clinics, camps, OUSD or other youth baseball games and community events during the Season
 Ballers to submit annual report on attendance, public use and community benefits
 Ballers can charge the fees below for third party use

Proposed Fee Structure for Raimondi Baseball Stadium

Facilities are rented for a minimum of 2 hours
 Site Attendant is required at each event

Field Rental	Youth - Oakland Resident	Adult - Oakland Resident	Oakland Private / Charter Schools	FRV - Youth	FRV - Adult
Practice	\$20/Hr	\$30/Hr	\$40/Hr	\$85/Hr	\$170/Hr
Games	\$50/Hr	\$60/Hr	\$60/Hr	\$160/Hr	\$320/Hr
Field Prep	\$50/Hr	\$50/Hr	\$50/Hr	\$100/Hr	\$100/Hr
Lights	\$15/Hr	\$15/Hr	\$15/Hr	\$50/Hr	\$100/Hr
PA	\$20/Hr	\$20/Hr	\$20/Hr	\$50/Hr	\$100/Hr
Scoreboard	\$25/Hr	\$25/Hr	\$25/Hr	\$50/Hr	\$75/Hr
Locker Room	\$20/Hr	\$20/Hr	\$20/Hr	\$100/Hr	\$100/Hr
Locker Room Attendant	\$50/Hr	\$50/Hr	\$50/Hr	\$50/Hr	\$50/Hr
Deposit	\$300	\$300	\$300	\$300	\$500
Concession Stand Use	TBD	TBD	TBD	TBD	TBD

Community benefits

MLB Play Ball Camp, free for Oakland residents
 Oakland Ballers Camp, free for Oakland residents
 Coach’s Clinic, free for Oakland residents
 Little Ballers Program, free for Oakland residents
 Field maintenance internships for City employees
 Comp tickets for one game for OPRYD summer campers and staff
 2x/month community events (e.g., movie nights, symphony) throughout the year
 Open play hours

Priority hiring for displaced Coliseum workers and OUSD students
Priority scheduling for historical use organizations: McClymonds
(no fees), NOLL/SOLL, Babe Ruth

1707 Wood St

Concurrent license of 1707 Wood St for parking
Rent of \$500/month (status quo)
One year term with two (2) one-year options
City and Ballers each have 30-day right to terminate except within
30 days of the start of the season or during the season
Ballers to provide 24x7x365 security, maintenance, repair

**Exhibit B
Property Map**

Red line denotes boundary of license agreement

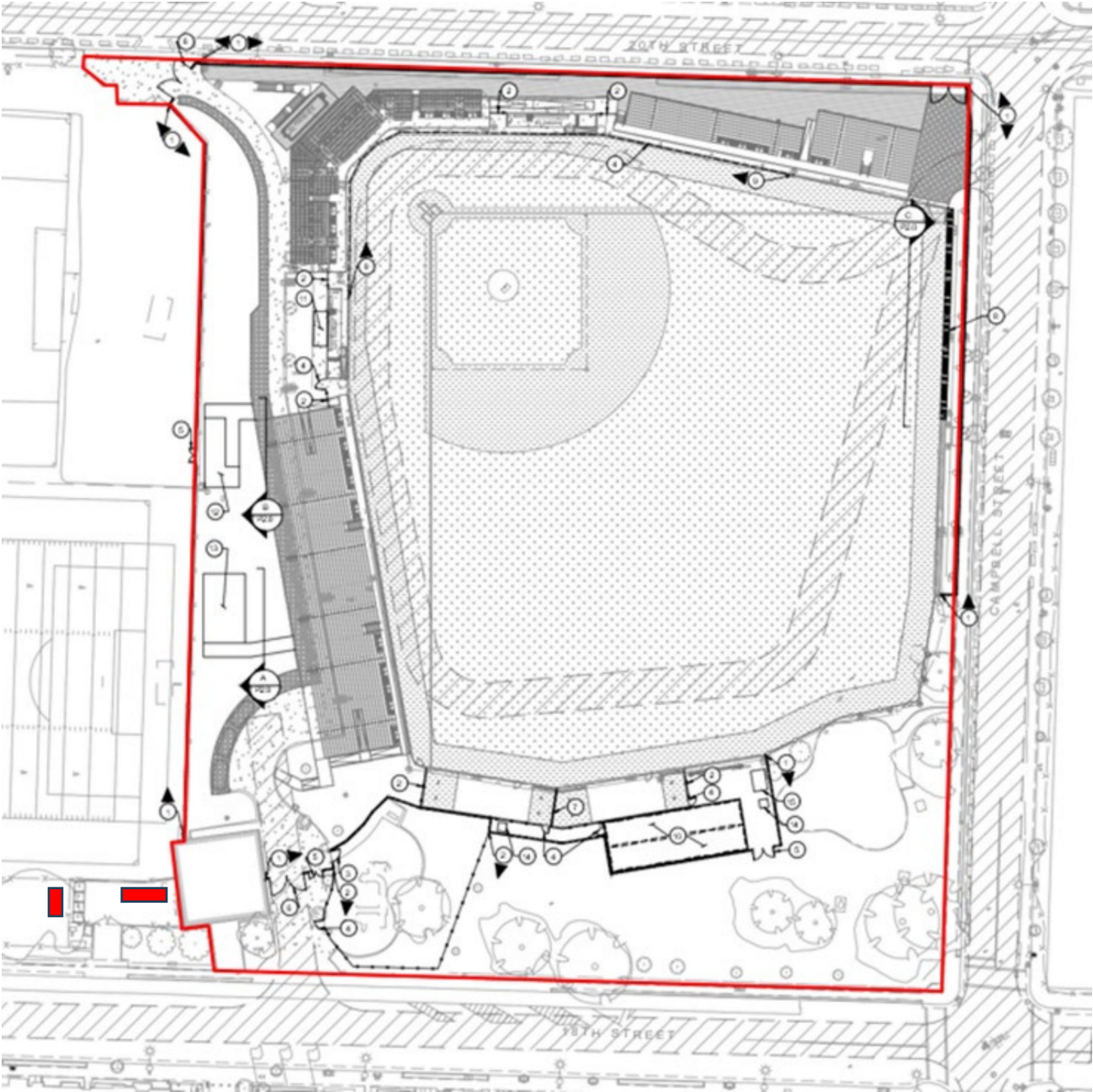
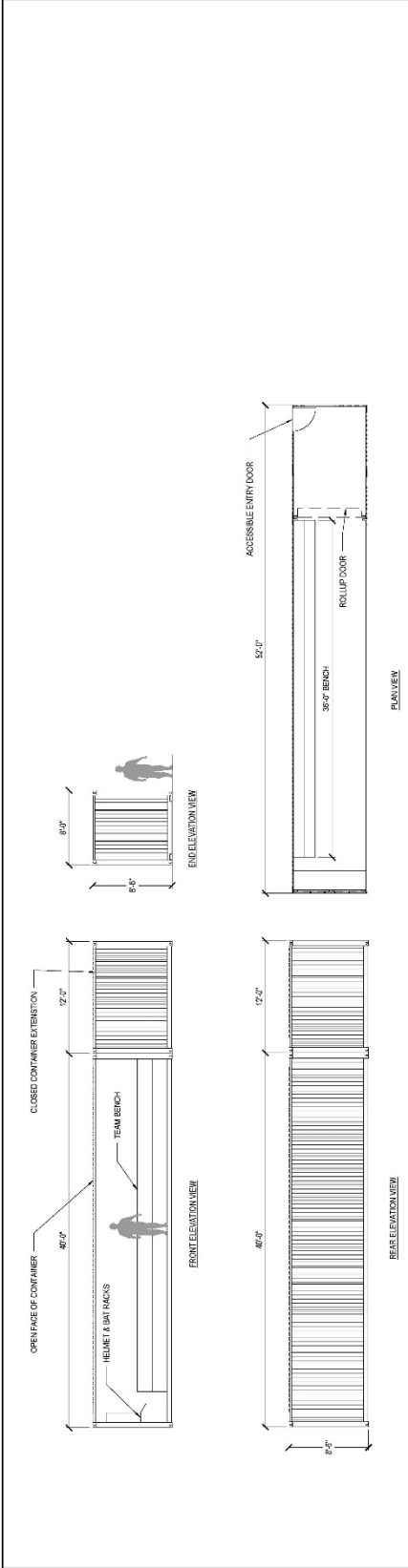


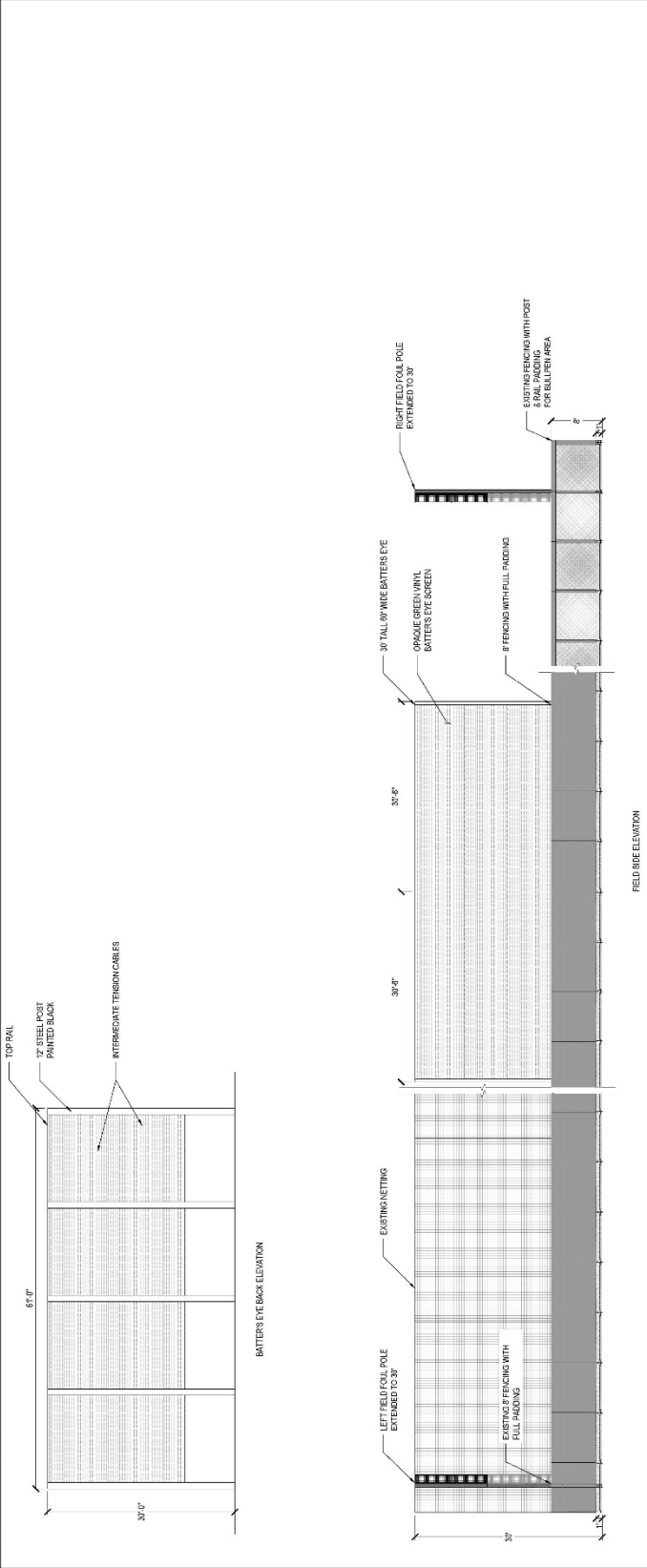
Exhibit B
Property Map



Exhibit B Plans and Draft Rendering



A SHIPPING CONTAINER DUGOUT ENLARGEMENTS
 SCALE: 3/16" = 1'-0"



B OUTFIELD FENCE AND BATTERS EYE ELEVATIONS
 SCALE: 1/8" = 1'-0"

PRELIMINARY
 MASTER
 PLAN
 3/11/24

PRELIMINARY NOT
 FOR CONSTRUCTION

NO. OF SHEETS	BL
DATE	MARCH 2024
SCALE	24:117

ELEVATIONS

OAKLAND



ATHLETICS

2025 RAIMONDI PARK SIGNAGE GUIDE

OAKLANDBALLERS.COM

 @OAKLANDBALLERS  @OAKLANDBALLERS  @OAKLANDBASEBALLERS

Exhibit C - Signage Proposal
PR&C Item 8a
02/12/25





Exhibit C - Signage Proposal
PRAC Item 8a
02/12/25

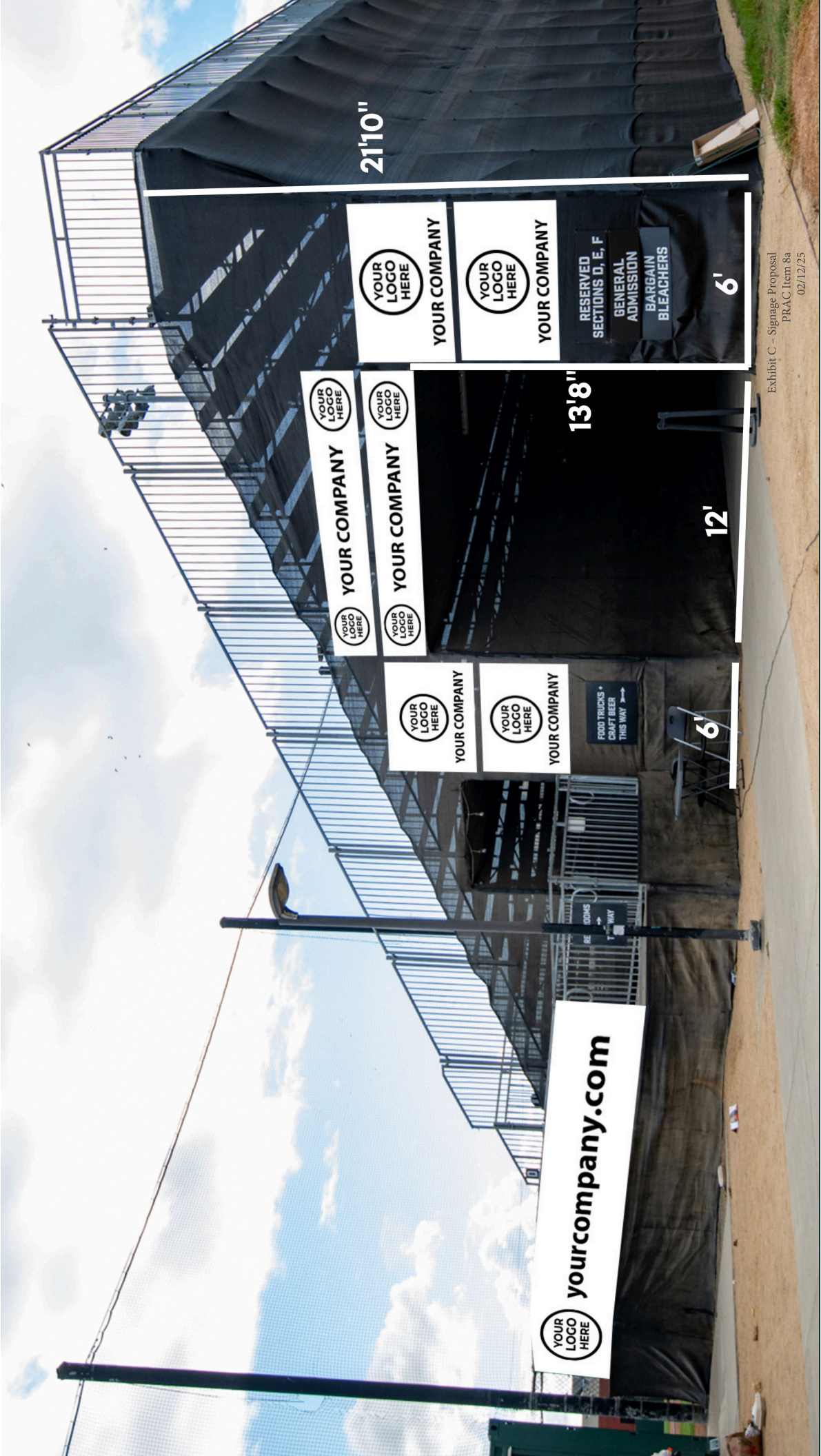


Exhibit C - Signage Proposal
PRAC Item 8a
02/12/25

yourcompany.com

YOUR LOGO HERE

YOUR COMPANY
YOUR LOGO HERE

YOUR COMPANY
YOUR LOGO HERE

YOUR COMPANY
YOUR LOGO HERE

YOUR COMPANY
YOUR LOGO HERE

YOUR COMPANY
YOUR LOGO HERE

YOUR COMPANY
YOUR LOGO HERE

RESERVED
SECTIONS D, E, F
GENERAL
ADMISSION
BARGAIN
BLEACHERS

13'8"

12'

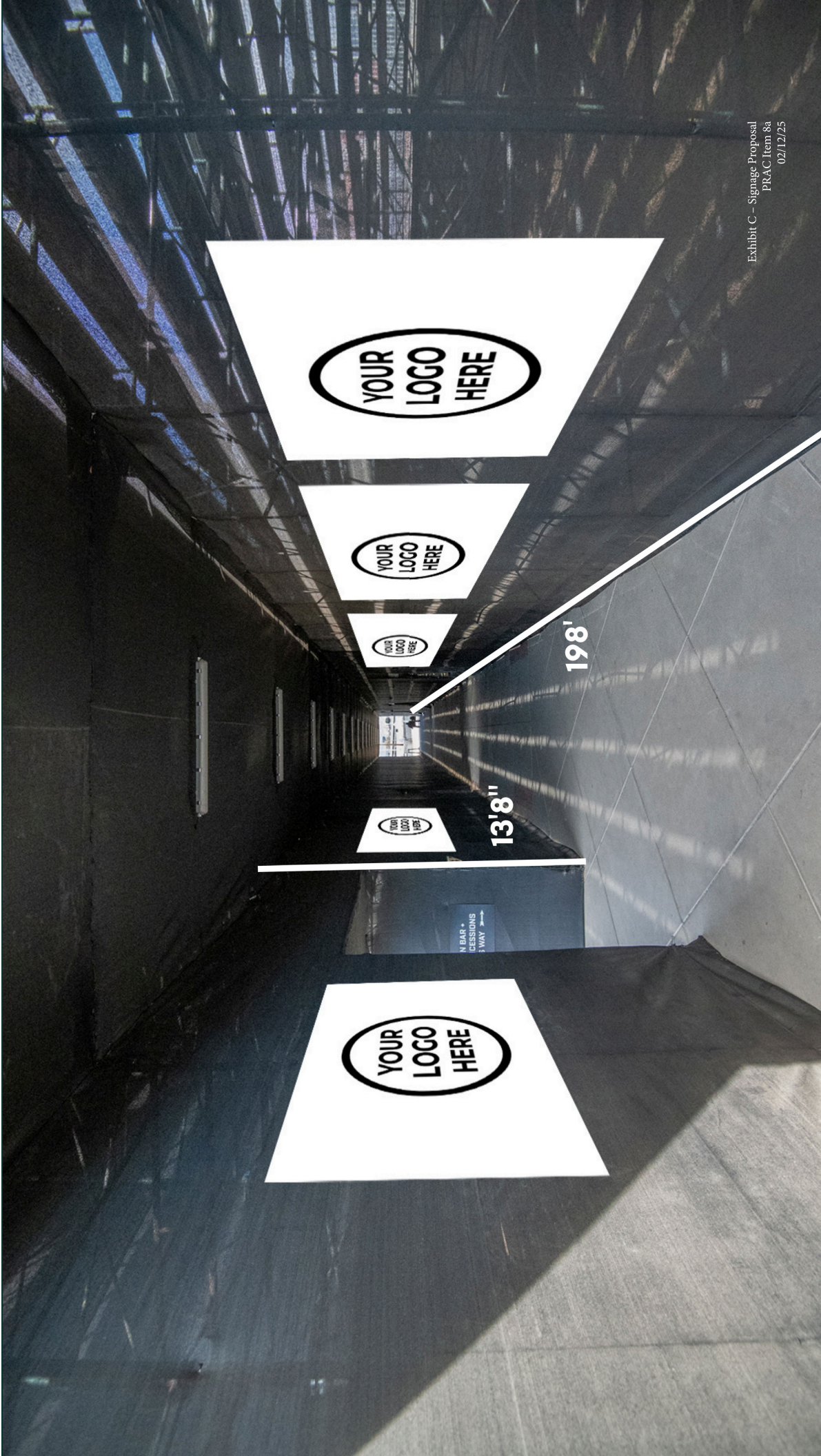
6'

6'

FOOD TRUCKS +
CRAFT BEER
THIS WAY →

RE-ENTRY
THIS WAY →

21'10"



YOUR
LOGO
HERE

YOUR
LOGO
HERE

YOUR
LOGO
HERE

YOUR
LOGO
HERE

YOUR
LOGO
HERE

N BAR +
SESSIONS
5 WAY →

13'8"

198'



8'

YOUR LOGO

yourcompany.com

YOUR LOGO HERE

12'

YOUR LOGO HERE

YOUR LOGO HERE

YOUR COMPANY

26'



YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

Your Company

YOUR LOGO HERE

198'

4'



yourcompany.com

YOUR LOGO HERE

yourcompany.com

YOUR LOGO HERE

yourcompany.com

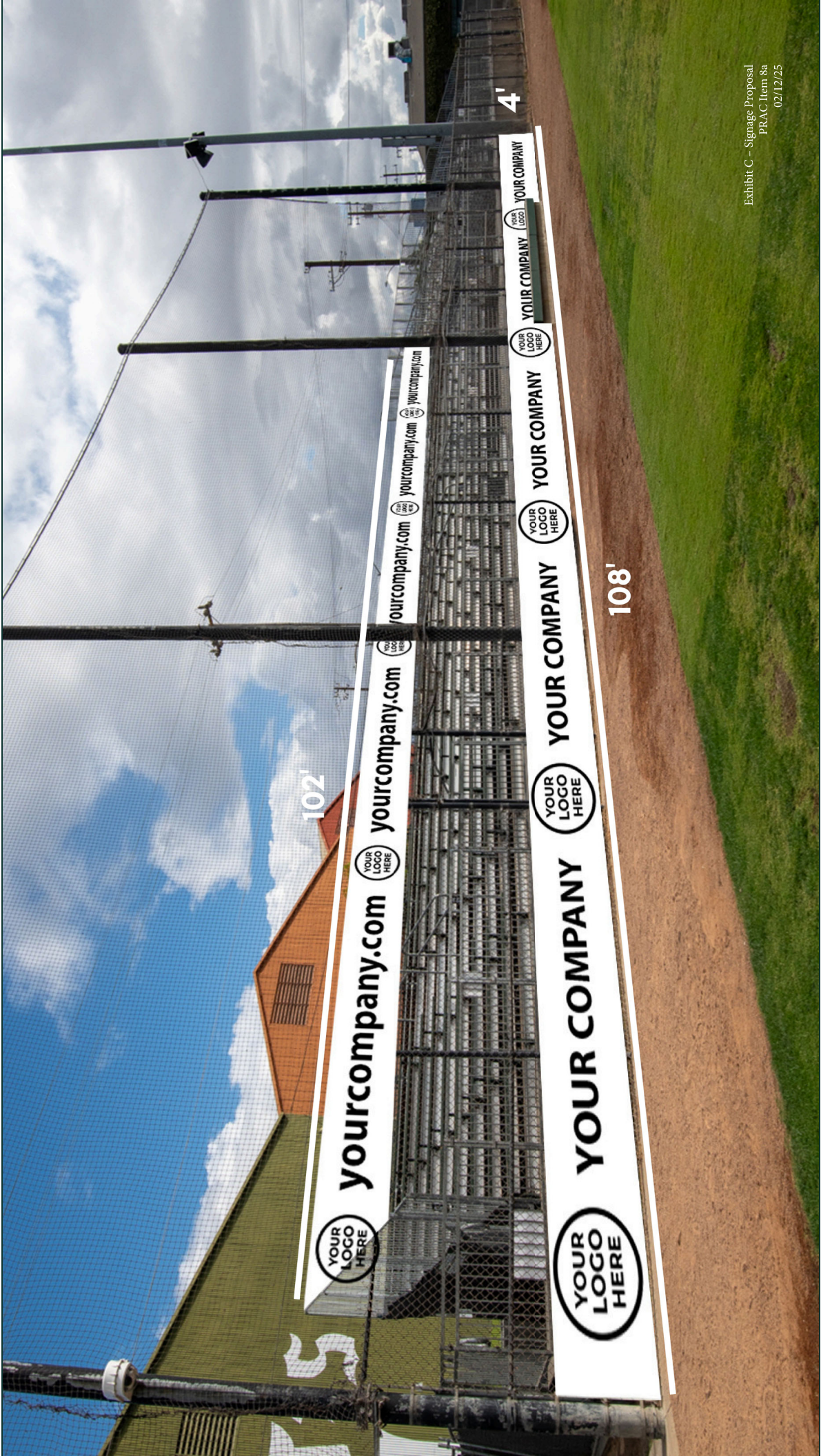
YOUR LOGO HERE

HOW COME I'M SO BLACK AND BLUE?
I'M THE ONLY ONE WHO'S EVER BEEN HERE
BRANDI PARK

BRANDI PARK
BAGGAIN BLEACHERS
GENERAL ADMISSION
RESERVED SECTIONS
PREMIUM SECTIONS
RESTROOMS
CONCESSIONS

198'

19'



102'

108'

4'

YOUR LOGO HERE

yourcompany.com

YOUR LOGO HERE

yourcompany.com

YOUR LOGO HERE

yourcompany.com

YOUR LOGO HERE

yourcompany.com

YOUR LOGO HERE

YOUR COMPANY YOUR COMPANY

YOUR LOGO HERE

YOUR LOGO HERE

YOUR COMPANY

YOUR LOGO HERE

YOUR COMPANY

YOUR LOGO HERE

YOUR COMPANY

YOUR LOGO HERE



398

YOUR
LOGO
HERE

7' X 4'

YOUR
LOGO
HERE

10' X 6'



OAKLAND



BALLERS

THANK YOU.



@OAKLANDBALLERS



@OAKLANDBALLERS



@OAKLANDBASEBALLERS

OAKLANDBALLERS.COM

Ballers Raimondi Park Phase II Project, City Approvals Timeline

As of February 4, 2025

<i>Dec 13</i>	<i>Submit Planning applications (minor CUP & variance)</i>
<i>Dec 20</i>	<i>Solicit appraisal bids</i>
<i>Jan 23</i>	<i>Initiate appraisal</i>
<i>Feb 7</i>	<i>Publish PRAC report</i>
<i>Feb 14</i>	<i>Finalize license negotiations (rent, signage, public access, comm. benefits)</i>
<i>Feb 21</i>	<i>Submit at-risk Building Permit application (fencing, batting cages)</i>
<i>Feb 24</i>	<i>Ballers commence repairs (existing structures only)</i>
<i>Feb 12</i>	<i>PRAC meeting (review last season, minor CUP & variance, license, signage)</i>
<i>Feb 14</i>	<i>Submit special event permit application</i>
<i>Feb 14</i>	<i>Issue 17-day public notice of minor CUP & variance application</i>
<i>Feb 24</i>	<i>City Council materials to CAO</i>
<i>Mar 3</i>	<i>Public comments due on minor CUP & variance</i>
<i>Mar 4*</i>	<i>Minor CUP & variance decision letter issued</i>
<i>Mar 13</i>	<i>Rules Committee (to schedule CED consideration)</i>
<i>Mar 14</i>	<i>10-day appeal of minor CUP & variance closes</i>
<i>Mar 14</i>	<i>Ballers commence improvements (fencing, batting cages)</i>
<i>Mar 14</i>	<i>City Council materials published</i>
<i>Mar 15</i>	<i>Construct phase II improvements (fence, restroom, concession, cage)</i>
<i>Mar 25</i>	<i>Community & Economic Development (CED) Committee mtg (license)</i>
<i>Mar 26**</i>	<i>Submit Planning application for Major CUP (signage)</i>
<i>Apr 15</i>	<i>City Council first reading (license approval)</i>
<i>April 18</i>	<i>Issue notice for Planning Commission hearing of Major CUP (signage)</i>
<i>May 6</i>	<i>City Council second reading (license approval)</i>
<i>May 7</i>	<i>Execute phase II license agreements (Raimondi & 1707 Wood St)</i>
<i>May 7</i>	<i>Building permits issued</i>
<i>May 7</i>	<i>Planning Commission hearing of Major CUP (signage)</i>
<i>May 12</i>	<i>Special Event Permit approval</i>
<i>May 15</i>	<i>Spring training begins</i>
<i>May 19</i>	<i>10-day appeal of Major CUP closes</i>
<i>May 20</i>	<i>First game</i>

* Date subject to change. Any public comments received will need to be addressed prior to issuance of the decision letter.

** Pending PRAC/CED feedback

CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission
FROM: Keary Brink, Park Supervisor II, Oakland Public Works (OPW)
DATE: January 21, 2025
SUBJECT: PUBLIC HEARING AND REQUEST FOR THE PARKS AND RECREATION ADVISORY COMMISSION (PRAC) TO MAKE A RECOMMENDATION TO GRANT A MINOR CONDITIONAL USE PERMIT FOR A GIFT-IN PLACE OF \$36,500 FOR RECONSTRUCTION OF THE VOLUNTEER TOOL SHED IN THE GARDENS AT LAKE MERRITT

SUMMARY

Park Services is recommending that PRAC review and support a donation by the Friends of the Gardens at Lake Merritt (FGLM) of the design and construction of a new tool shed and mulch bay. The structure is meant to replace the existing shed which is in poor repair and is too small to house all the tools used by the various volunteer groups who regularly maintain many of the specialty beds within the Gardens at Lake Merritt.

BACKGROUND / LEGISLATIVE HISTORY

The Gardens at Lake Merritt are made up of a wide variety of themed gardens and specialty plant collections, and the facility has benefited over the years from a strong partnership with numerous volunteers and volunteer groups who help to maintain many of these gardens and specialty collections. These volunteers use tools which have been stored for them in a small, aging shed near the ‘Bay-friendly’ garden by Gate 2. The FGLM, the umbrella organization of all these specialty volunteer groups, has advocated for some time for replacement of this shed. In addition, when mulch, compost and other bulk materials are acquired for use in the Gardens, they have often been stored for weeks at a time in open piles near Gate 3, the most convenient entrance for vehicle deliveries.

PROJECT / PROGRAM DESCRIPTION

The FGLM is proposing a gift-in-place donation which will not exceed \$50,000 and will enter a \$1 construction contract agreement with Swinerton as the general contractor to complete the relocation of the volunteer tool shed and construction of a delivery enclosure to house bulk materials used within the Gardens. The design and engineering services for the new shed have been donated by David Baker Architects and Verdant Structural Engineers. The shed’s location will be moved to a more central location within the garden, easily accessible but away from the public entrances at gates 1 and 4, and nearer the (typically closed) maintenance entrance at Gate 3. The enclosure for storage of bulk materials will be in this same general area.

FISCAL IMPACT

The proposed project is a gift-in-place donation in which the donor (the FGLM) and their selected contractor will enter into a \$1 construction contract agreement with the City. There will be no fiscal

impact to the City of Oakland and the project will not create unbudgeted costs. The newly constructed tool shed will address deferred maintenance of the existing structure.

PUBLIC INTEREST

While the Gardens themselves are for the enjoyment of the public at large, the near-daily presence and activities of the many volunteer groups who dedicate their time to care for the Gardens are a vibrant and ongoing demonstration of positive engagement with the city’s staff and its treasured public-facing facilities.

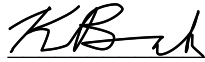
COORDINATION

The FGLM and the design team at David Baker Architects have been in ongoing meetings with staff in Park Services and Facilities Maintenance regarding materials, construction, placement and other key components of the project design. The placement of the structures and the decision to make them both durable and appealing to the eye are intended to have a neutral or even positive impact on the Garden’s function as an OPRYD rental facility.


RECOMMENDATION

Staff recommends that after review, the PRAC give its support to the project as proposed for the relocation / construction of these two important volunteer resources. The non-profit FGLM sees sufficient benefit in the project to donate the entire cost of the project, and the design should deliver two functional, durable and aesthetically appealing structures which should serve the Gardens for some time.

Respectfully Submitted,



Keary Brink
Park Supervisor II, Public Works



Approved by:
Clinton Pugh
Park Services Manager, Public Works

Supporting Documents:

Attachments: Exhibit A – Site description, design drawings.

EXHIBIT A

GENERAL

G-00	COVER SHEET
G-01	EXISTING
G-02	EXISTING
G-03	RENDERINGS
A100 PLANS	
A-00	SITE PLAN
A-10	FLOOR PLAN - LEVEL 1
A200 ELEVATIONS	
A-20	ELEVATION & SECTION
A300 SECTIONS	
A-300	SECTIONS



FISCAL IMPACT

THE PROPOSED PROJECT IS A GIFT-IN-PLACE DONATION IN WHICH THE DONOR AND THEIR SELECTED CONTRACTOR WILL ENTER A \$1 CONSTRUCTION CONTRACT AGREEMENT WITH THE CITY.

FGLM WILL BE DONATING CONSTRUCTION MATERIALS INCLUDING THE WEATHERED STEEL PANELS, DOOR HARDWARE, AND FOUNDATION MATERIALS.

PROJECT SCOPE

THE FRIENDS OF THE GARDEN AT LAKE MERRITT (FGLM), A NONPROFIT ORGANIZATION IS PROPOSING A GIFT-IN-PLACE DONATION IN THE AMOUNT NOT TO EXCEED \$50,000 AND ENTER A \$1 CONSTRUCTION CONTRACT AGREEMENT WITH SWINERTON AS THE GENERAL CONTRACTOR TO COMPLETE THE TOOL SHED RELOCATION AND DELIVERY ENCLOSURE AT THE GARDENS AT LAKE MERRITT.

APPROVAL OF THE PROPOSAL WILL RELOCATE THE EXISTING WOOD TOOL SHED TO A MORE ACCESSIBLE LOCATION NEAR THE MAINTENANCE ENTRANCE AT GATE 3.



PROJECT TEMPLATE

COVER SHEET

L2401

scale:
date: 2025-01-24

G-00

PRAC -2/12/25
Item 8B



EXISTING SITE

THE EXISTING SITE FOR THE SHED RELOCATION IS ADJACENT TO GATE 3. THIS AREA IS HIGHLY VISIBLE TO THE PUBLIC, BUT HAS THE APPEARANCE OF A VERY BACK OF HOUSE USE. WITH THE RELOCATED FENCE, THERE IS AN OPPORTUNITY TO MAKE BETTER USE AND IMPROVE THE APPEARANCE OF THIS AREA, IN ADDITION TO ENHANCING THE OAK TREE HABITAT. GATE 3 IS USED BY MAINTENANCE TRUCKS, AND THAT FUNCTION WILL BE PRESERVED WITH THE DESIGN WITH A 20' CLEAR WIDTH BETWEEN THE SHED AND MULCH ENCLOSURE. THE MULCH ENCLOSURE IS ORIENTED FOR EASY TRUCK ACCESS AND WILL BETTER CONTAIN AND VISUALLY SCREEN DELIVERIES.



VIEW OF THE RELOCATED TOOL SHED FROM THE BAY FRIENDLY GARDEN



VIEW OF THE RELOCATED TOOL SHED AND DELIVERY ENCLOSURE FROM GATE 3

GATE 2

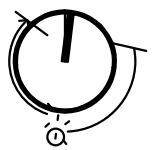
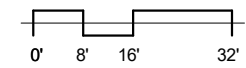
EXISTING TOOL SHED

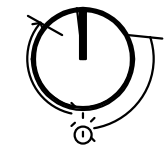
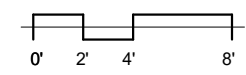
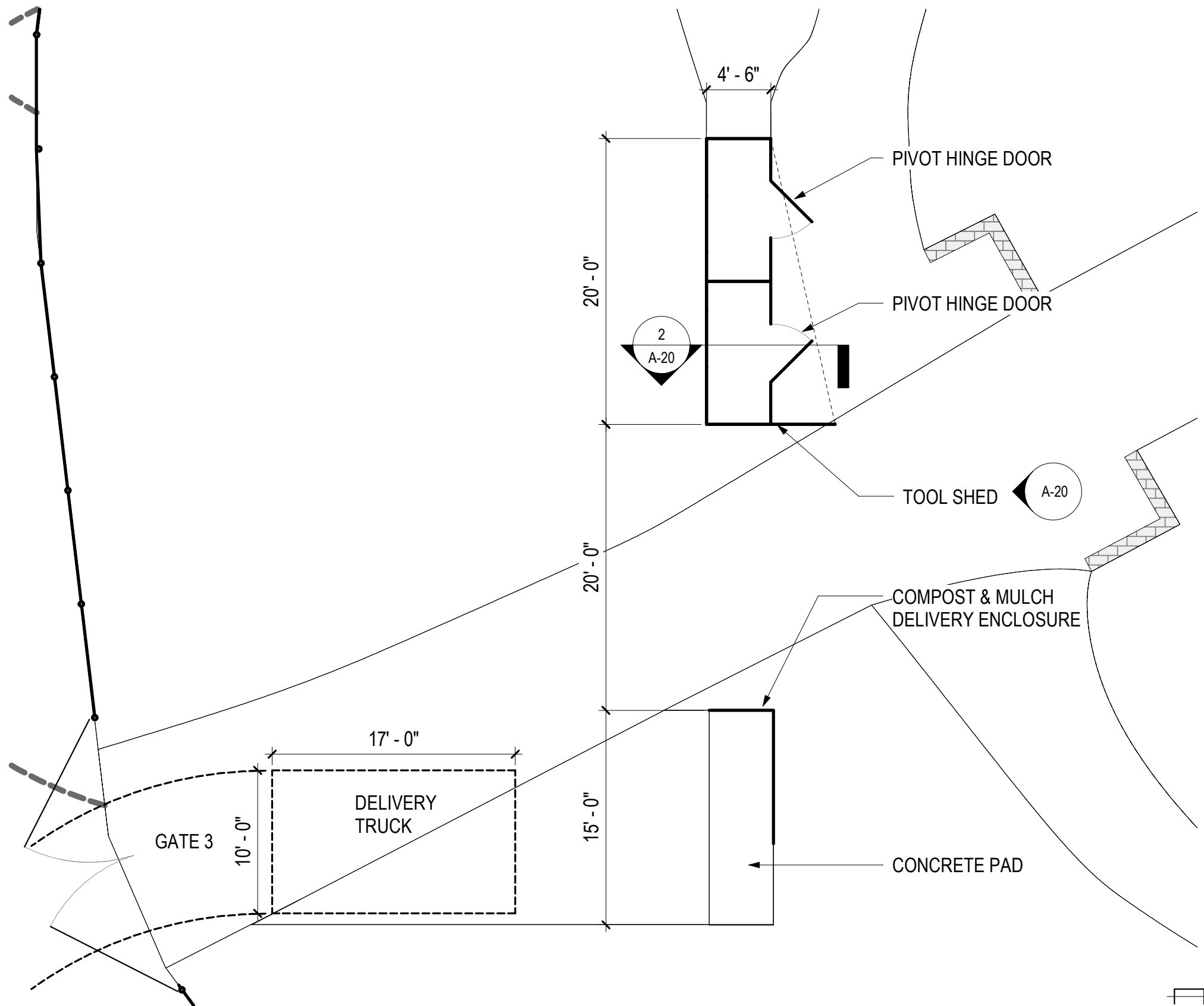
FENCE LINE

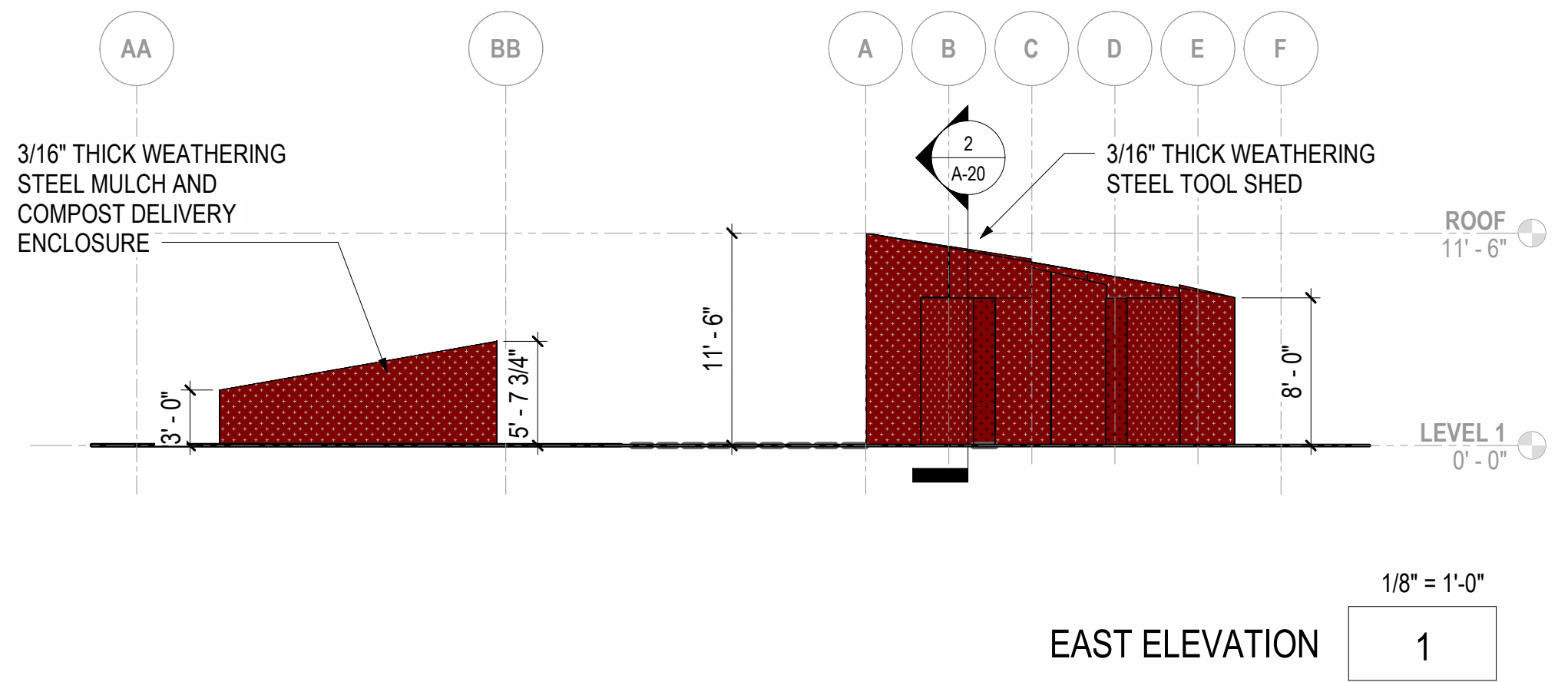
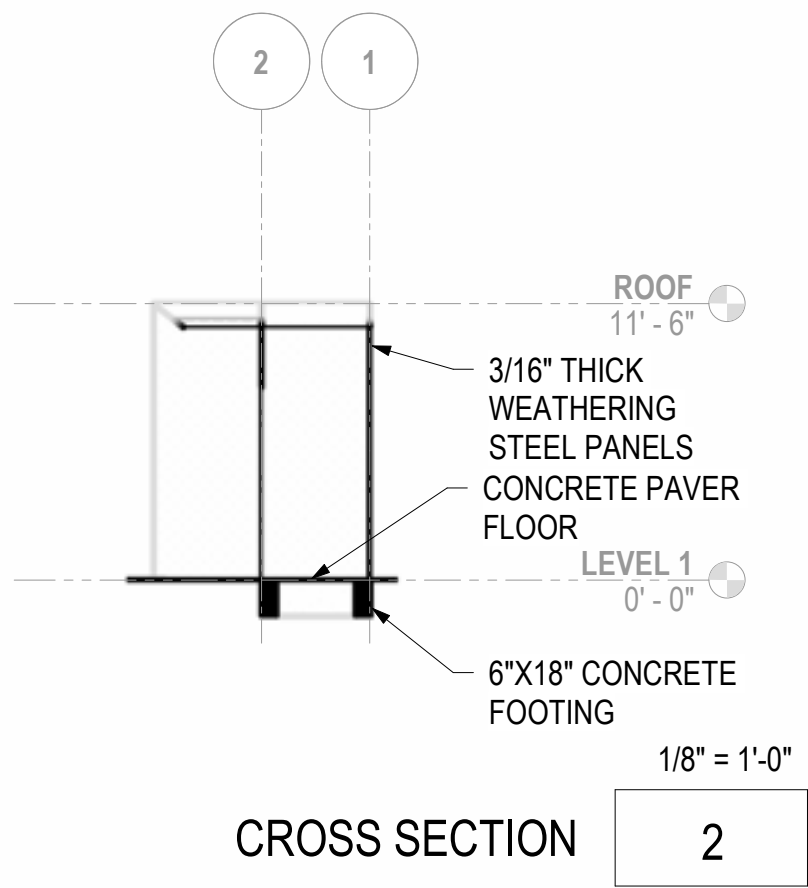
PROPOSED 90 SF
TOOL SHED

PROPOSED DELIVERY
ENCLOSURE

GATE 3







CITY OF OAKLAND
Oakland Parks, Recreation & Youth Development

TO: Marc Barach, Chair, Parks and Recreation Advisory Commission
FROM: Quincy Williams, Assistant Capital Improvement Coordinator, OPRYD
DATE: February 5, 2025
SUBJECT: REQUEST FOR THE PARKS AND RECREATION ADVISORY COMMISSION (PRAC) REVIEW AND SUPPORT A GIFT OF \$36,500 (GIFT-IN-PLACE) FOR IMPROVEMENTS AT CLINTON COMMUNITY CENTER FOR THE INSTALLATION OF A NEW EXTERIOR PROTECTIVE FENCE, ELECTRICAL CONDUIT, UPGRADES TO A BATHROOM, INSTALLATION OF FLOORING, AND TO MAKE A RECOMMENDATION TO GRANT A MINOR CONDITIONAL USE PERMIT

SUMMARY

The non-profit organization known as Trybe operates the City-owned Clinton Community Center located at 655 International Boulevard, Oakland, CA. As described in the License Agreement, it has with the City administers various community activities, including free flu and COVID shots, school backpack giveaways, and free food distribution to food-insecure recipients. Trybe has occupied the center since early 2024. Before its occupancy, Oakland Public Works (OPW) -Facilities Services Division, Oakland Parks, Recreation & Youth Development (OPRYD) and Trybe performed a walk-through inspection of the community center. At that time, it was noted that the building needed several repairs and improvements. Some of those repairs/improvements are listed below.

- Electrical conduit for added power for future increased power use.
- Removal of ceiling tiles and painting of multi-purpose room.
- Replacing floor tiles in the multipurpose room.
- Inner building courtyard landscaping and irrigation repair.
- Outdated roof replaced.

Since Trybe has occupied the center, it has completed some upgrades, repairs, and installation of items at the site that **did not adhere to City processes and/or codes. The following sections describe Trybe's improvements and how they relate to the License Agreement and actions needed to comply with the City of Oakland Building and Planning and Public Works Facilities Services Division requirements.**

BACKGROUND / LEGISLATIVE HISTORY

Trybe and the City entered into a License Agreement for Trybe to lease Clinton Community Center on May 8, 2024. The agreement includes Trybe's responsibilities as the Licensee and the City's as the Licensor. Trybe's duties as it pertains to maintenance of the center/site include the following:

- Licensee (Trybe) agrees to provide maintenance and keep and maintain all portions of the premises at its own cost and expense.
- The licensee shall be responsible for the maintenance and repair of all improvements to the property, including fencing, lighting, or other fixtures or equipment installed.

The agreement also states that the Licensee (Trybe) “shall not make any improvements/modifications to the Premises without prior written consent of Licensor “ (City). OPRYD and OPW have communicated with Trybe on several organizational maintenance and improvement inquiries/requests. Some requests were supported by OPRYD and OPW Facilities Services Division and did not require a minor conditional use or other permits. However, Trybe completed some improvements/installations that required permits. The process for these projects was incomplete/did not fully comply with City requirements. Trybe installed a fence on the east side of the building (see Exhibit A below) to protect the power meter, irrigation system from vandalism, and a community garden. Also, Trybe installed an electrical conduit to improve the facility's power. The following section describes the process for Trybe’s improvements to comply with City requirements.

PROJECT / PROGRAM DESCRIPTION

On December 19, 2024, City representatives from OPRYD and OPW met with Trybe at Clinton Community Center to review the improvements, discuss if these improvements trigger permits, and how future modifications must comply with City requirements. As mentioned in the Background section of this report, Trybe installed a fence. For this fence to comply with City requirements, Trybe must submit a minor conditional permit application to the City of Oakland Building and Planning Department. Upon completing this process and getting approval from Building and Planning, the fence will be considered satisfactory/compliant with city requirements. Trybe has contracted with a licensed electrician who installed an electrical conduit to increase the facility's power. Trybe has re-applied to Building and Planning for an electrical permit (which is needed to get the correct one). The installation will be considered compliant upon approval of the license.

At the site meeting, Trybe requested that the bathroom flooring in its multipurpose room be replaced and that a bathroom be converted to a laundry room with outlets for a washer and dryer. These projects will not require a minor conditional use permit or trigger any other Building and Planning requirements.

The City and Trybe discussed other proposed facility and site improvements. The protocol for these projects to be City compliant includes the following:

- Trybe has and will continue to submit Oak311 tickets for repairs. OPW has/will follow up on current and future requests.
- Trybe notifies OPRYD/OPW of proposed improvements to the facility/site. The city reviews and responds with appropriate actions.
- If the proposed project does not require a permit/PRAC report, then OPRYD/OPW will allow the project to be installed.
- If a proposed project requires/triggers a PRAC report, permits/other City requirements, then OPRYD/OPW will inform Trybe.

The points mentioned above are guidelines. Future proposed projects should adhere closely to this process/protocol.

FISCAL IMPACT

Trybe has installed the fence, electrical conduit, and washer/dryer outlets in a space converted to a laundry room. It has also purchased flooring to replace/upgrade the floor in the multipurpose room. As a result of these projects, Trybe will cover additional items, such as permit fees. The total

estimated cost for this gift-in-place donation is \$36,500. There will be no cost to the City for these improvements.

PUBLIC OUTREACH / INTEREST

No public outreach was utilized to install the fence or the electrical conduit.

COORDINATION

This proposed project requires continued coordination between OPRYD, OPW Facilities Services, the Landscaping Division, and Trybe.

RECOMMENDATION

OPRYD and OPW recommend that the Parks and Recreation Advisory Council support a gift-in-place donation from Trybe of \$36,500 for improvements at Clinton Community Center and support a retroactive minor conditional use and electrical permits for fence and electrical conduit already installed or underway.

Respectfully Submitted,


Quincy Williams (Feb 7, 2025 15:50 PST)

Prepared by:
Quincy Williams
Assistant Capital Improvement Project Coordinator, OPRYD


Harith Aleem (Feb 7, 2025 15:51 PST)

Reviewed by:
Harith Aleem
Assistant Director, OPRYD

Identification of Support Documents:

Attachments: Exhibit A – *Current (January 2025) Pictures*

Exhibit A



