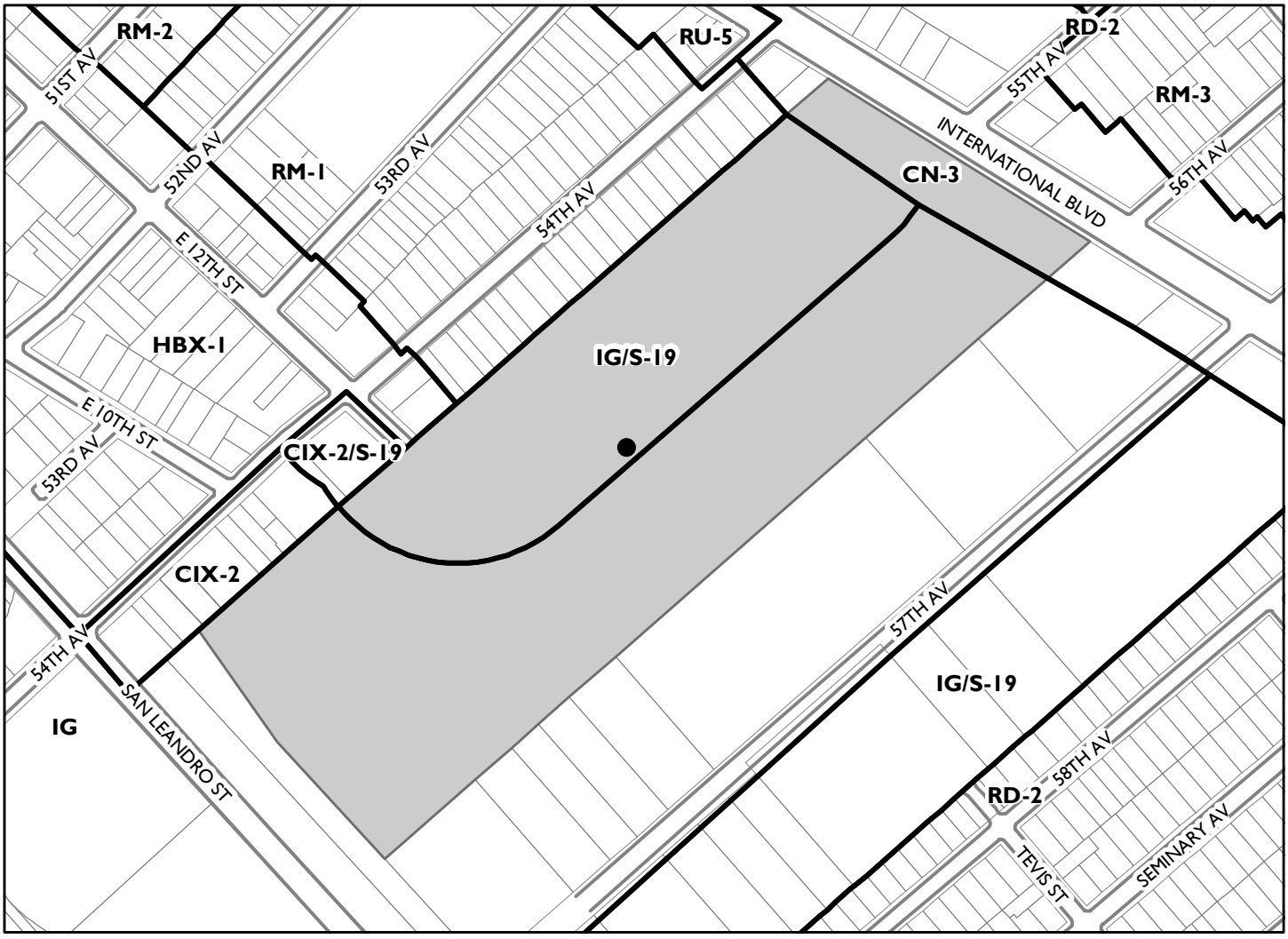
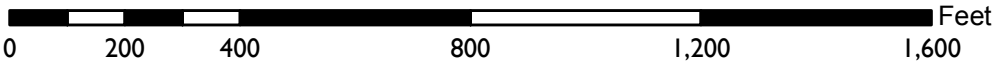
Location:	**5441 International Blvd.** **(APN: 041-3848-001-00) See map on the reverse**
Proposal:	Proposal to demolish eight existing contaminated buildings, remediate the site, and construct an approximately 540,000 square foot warehouse. The site is located within the historic 57th Avenue Industrial District Area of Primary Importance (API), and the proposed project includes the demolition of two contributors to the District. As part of the proposed project, the front "bulkhead" portion of Building #1 (i.e., the façade that contains early-20th century Classical Revival-inspired industrial architecture and a portion of the sides of the building) would be preserved and incorporated into the design of the new warehouse. A variant to the project that requires all project-related truck trips to access the site from San Leandro Street rather than International Boulevard has also been proposed.
Applicant:	Bridge Development Partners, LLC
Contact Person:	Brendan Kotler – (213) 805-6350
Owners:	Bridge Point Oakland, LLC
Planning Permits Required:	Regular Design Review for new construction including Category II Demolition Findings, and Major Conditional Use Permit to allow the industrial warehousing use that is permitted in the IG Zone that is partially located within the CN-3 Zoned portion of the project site.
General Plan:	General Industrial Neighborhood Center Mixed Use
Zoning:	IG/S-19, General Industrial/ Health and Safety Protection Zone CN-3, Neighborhood Commercial Zone - 3
Environmental Determination:	Draft Environmental Impact Report was published for a 45-day review period from December 20, 2019 to February 3, 2020. The FEIR/RTC was published on May 22, 2020.
Historic Status:	57th Avenue Industrial District (API) OCHS Ratings: Building 1, Rating A1+, API Anchor; Building 2: Dc1+; API contributor
City Council District:	5
Action to be Taken:	Consideration of Certification of the Environmental Impact Report and decision on the applications.
Staff Recommendation:	Adopt the CEQA findings, including Certification of the Environmental Impact Report and Statement of Overriding Considerations, and approve the project.
Finality of Decision:	Appealable to City Council within 10 days.
For further information:	Contact case planner **Pete Vollmann** at 510 238-6167 or by e-mail at pvollmann@oaklandca.gov .

CITY OF OAKLAND PLANNING COMMISSION





Case File: ER18-013/PLN19-076
Applicant: Bridge Development Partners, LLC
Address: 5441 International Blvd.
Zone: IG/S-19, CN-3

SUMMARY

In November 2018, Bridge Development Partners, LLC filed a request for environmental review application to begin review and consideration of a proposal to demolish all existing buildings on the subject property at 5441 International Boulevard (with the exception of the façade of Building #1), remediate the site, and construct a new industrial warehouse.

The City is the Lead Agency pursuant to the California Environmental Quality Act (CEQA) and has the responsibility to prepare the Environmental Impact Report (EIR) for the Project. Staff published a Notice of Preparation (NOP) of an EIR on December 21, 2018. A scoping session was held before the Landmarks Preservation Advisory Board on January 14, 2019, and the Oakland Planning Commission on January 16, 2019.

The Notice of Availability for the Draft EIR was prepared and released on December 20, 2019 beginning a 45-day public comment period. The public comment period ended on February 3, 2020.
Hearings on the DEIR were held before the Landmarks Board on January 13, 2020 and the Planning Commission on January 22, 2020.

The purpose of this meeting is to take any remaining public testimony concerning the Project and to consider the application submitted for the Project summarized in the Project Description section. Staff has prepared recommended actions for the Planning Commission to review and consider. These actions are listed below:

(1) Adoption of the enclosed CEQA findings, including Certification of the EIR, rejection of alternatives as infeasible and a Statement of Overriding Considerations.

(2) Approval of the Major Conditional Use Permit and Design Review for the Project as described in the Project Description section of this report subject to the conditions (including the Standard Conditions of Approval/Mitigation Monitoring and Reporting Program (SCAMMRP)), requirements, and findings contained in this staff report.

SITE DESCRIPTION

The project site consists of approximately 24 acres on International Boulevard, between 54th and 57th Avenues, and is located east of San Leandro Street and the Bay Area Rapid Transit (BART) tracks. The project site consists of approximately 24 acres formerly used as a manufacturing facility for General Electric. Today, eight buildings remain on the site (Buildings #1, #2, #4, #8, #17, #18, #20, and #21); these buildings were constructed between 1924 and 1975 with the exception of Building #21, which was constructed in the early 1980s to house remediation equipment.

The proposed project site is surrounded by a mix of commercial, residential and industrial uses (see Figure 3-2). There are residential uses, primarily single-family homes, directly north of the site, as well as northeast of the site, across International Boulevard. There are also a few commercial uses north of the site, along 54th Avenue, and northeast of the site, along International Boulevard. The commercial properties in this area are composed of retail establishments such as food uses and markets, automotive repair shops, and some manufacturing. Industrial uses and parking lots are located to the south and southeast of the site with additional manufacturing facilities towards the west and southwest of the property along the San Leandro Street corridor.

An unused Union Pacific right-of-way and railroad tracks, BART tracks, and San Leandro Street run directly along the southwest boundary of the project site. Across the railroad tracks on San Leandro Street are large, often cinderblock and metal or vinyl-sided buildings used for industrial and warehouse purposes.

Site Contamination

The Project site is included in the list of Hazardous Waste and Substances sites in the Department of Toxic Substances Control (DTSC) EnviroStor database, one of the lists meeting the "Cortese List" requirements. The buildings and site contain hazardous chemicals, including polychlorinated biphenyls (PCBs), in the soil, groundwater and building materials, and have been undergoing remediation and monitoring under the oversight of DTSC and the United States Environmental Protection Agency (USEPA).

Historical Resources

The subject property was previously evaluated by the Oakland Cultural Heritage Survey (OCHS), which identified the site as being located within a locally designated Area of Primary Importance (API), the 57th Avenue Industrial District. The proposed project site includes two contributors to the district: Building #1, which is a primary anchor to the district (OCHS rating of "A1+"), and Building #2 (OCHS rating of "Dc1+"). Building 1 was also evaluated as an individually significant resource.

57th Avenue Industrial District

The 57th Avenue Industrial district (API) is a visually distinctive industrial area of approximately 21 buildings (including buildings #1 and #2) on 22 parcels, all located on one city block in Central East Oakland, along both sides of a long cul-de-sac off of International Blvd. The buildings along 57th Avenue contain mostly zero setbacks from the street, with varying yards and driveways between buildings. The buildings in the district are generally similar in size, age, and design, most of which date from the 1920's to 1940's. The styles include early 20th century utilitarian, decorative brick, and Moderne industrial buildings. Typical buildings are one story with a long narrow plan, containing stepped parapets, truss roofs and vehicle doors. The exteriors are mainly pressed brick and common brick and glass, with stucco ornament, metal sash and three-dimensional brick work. According to the OCHS, the district appears eligible for listing to the National Register of Historic Places, and approximately 19 district properties (90% of the total) appear to contribute to the district's significance. Notable individual buildings are: the red brick General Electric plant at 5441 International Boulevard (the subject property); the tapestry brick Mutual Stores (Safeway) warehouse and tower at 5701 International Boulevard; and the Ferro Enamel plant at 1101 57th Avenue.

Significant Buildings

Building #1 located at 5441 International Boulevard is a very good example of an early 20th century utilitarian-Georgian Revival factory building. It was built in 1922, designed by the General Electric Plant Engineering Department (Schenectady, New York) and constructed by Foundation Company. A one story brick addition to the factory was made in 1927. Historically the building reflects industrial development in Oakland, and national businesses and industries in Oakland.

The original owner, developer, and occupant was General Electric Oakland Works. This was General Electric's second Oakland plant, the other being Mazda Lamp Works at 1600 Campbell Street which manufactured light bulbs. At the time General Electric bought the site, they had factories in 26 cities.

Building #1 at the site housed offices at the front section of the building and had a large factory and warehouse for the manufacturing of switchboards, transformers and motors.

The OCHS rates Building #1 as possessing "Highest Importance", particularly for its design quality and type/style and historical associations. It is a primary contributor to the 57th Avenue Industrial District (API). In addition to district contributor eligibility, this building also appears individually eligible for listing to the National Register of Historic Places in the context of masonry (industrial) buildings in Oakland 1850-1948.

Building #2 at 5441 International Blvd., located behind Building #1, is a representative example of an early 20th century utilitarian industrial building and reflects industrial development in Oakland and national businesses and industries in Oakland. The OCHS rates Building #2 as possessing "Minor Importance", with potential for "Secondary Importance" if restored. It is a contributor to the 57th Avenue Industrial District (API).

PROJECT DESCRIPTION

The project consists of demolition of the eight existing structures and associated equipment and foundations; remediation actions for contaminated materials and soils; and construction of the new warehouse building. The façade of Building #1 would be preserved, treated to contain any contaminated materials, and incorporated into the design of the new building. The redevelopment of the approximately 24-acre site includes the construction of an approximately 534,208 square foot industrial building, with 524,208 square feet of warehouse space, 10,000 square feet of ancillary office uses that includes a 5,000 square foot mezzanine. There would be 93,522 square feet of landscaping provided. The warehouse would have 85 dock doors and 219 parking stalls would be provided on the site for employees and visitors.

The primary site access is located off the existing International Blvd. frontage. However, the project proposal includes a project variant where the site would make use of the adjacent property under separate ownership through a lease agreement so that the primary truck access could be located off San Leandro Street.

GENERAL PLAN

Land Use and Transportation Element of the General Plan

The General Plan's Land Use and Transportation Element (LUTE) classifies the project site as located in the General Industrial and Transportation General Plan area with a small portion along the International Boulevard frontage within the Neighborhood Center Mixed Use area.

The General Industrial and Transportation land use classification is intended to recognize, preserve, and enhance areas of the City for a wide variety of businesses and related establishments that may have the potential to create off-site impacts such as noise, light/glare, truck traffic, and odor. These areas are characterized by sites with good freeway, rail, seaport, and/or airport access.

The Neighborhood Center land use classification is intended to identify, create, maintain, and enhance mixed use neighborhood commercial centers. These centers are typically characterized by a smaller scale pedestrian oriented, continuous street frontage with a mix of retail, housing, office, active open space, eating and drinking places, personal and business services, and smaller scale educational, cultural, or

entertainment uses. Future development within this classification should be commercial or mixed uses that are pedestrian oriented and serve nearby neighborhoods, or urban residential with ground floor commercial.

Among the General Plan Land Use and Transportation policies and objectives applicable to the proposed Project are the following:

Policy I/C 1.1 – Attracting New Businesses

Policy I/C 1.4 – Investing in Economically Distressed Areas of Oakland.

Policy I/C 2.1 – Pursuing Environmental Clean-Up

Policy I/C 4.2 – Minimizing Nuisances

The proposal is consistent with the LUTE by remediating and redeveloping the existing industrial site into a new industrial/warehousing activity that may accommodate a new business and would provide new job opportunities in the area while locating the truck related functions of the site as far away from the adjacent residential zone as possible. The portion of the site within the Neighborhood Center land use area will remain largely unchanged except that a new use will be introduced to the long vacant industrial site that will re-establish a job center on International Blvd. with good access to public transportation. Given the land use covenants on the site that restrict residential activities a mixed-use development would not be appropriate nor permitted along this project site frontage.

ZONING COMPLIANCE

The majority of the subject property is located within the IG/S-19 zone (General Industrial Zone/ Health and Safety Combining Zone) with the approximately 100 feet of the site fronting onto International Blvd located within the CN-3 zone (Neighborhood Commercial Zone -3).

The IG Zone is intended to create, preserve and enhance areas of the City that are appropriate for a wide variety of businesses and related commercial and industrial establishments that may have the potential to generate off-site impacts such as noise, light/glare, odor, and traffic. This zone allows heavy industrial and manufacturing uses, transportation facilities, warehousing and distribution, and similar and related supporting uses. Uses that may inhibit such uses, or the expansion thereof, are prohibited. This district is applied to areas with good freeway, rail, seaport, and/or airport access. The site is located within 300 feet of a residential zone which limits the FAR for the site to 1.0.

The intent of the CN-3 Zone is to create, improve, and enhance areas neighborhood commercial centers that have a compact, vibrant pedestrian environment. The portion of the site within the CN-3 Zone is located within the 60-foot height area, which allows for a maximum non-residential FAR of 3.0.

The approximately 540,000 square feet of development on the 24-acre site is well below an FAR of 1.0.

Conditional Use Permit

Pursuant to Planning Code Section 17.33.030, uses that are prohibited within the CN-3 Zone but permitted in adjacent zones, a Conditional Use Permit may be granted to allow such use within the CN-3 Zone. As described earlier in this report, the subject site is located almost entirely within the IG Zone, which permits the proposed warehousing activity. However, a portion of the re-use of the existing historic building and small portions of the proposed warehouse encroach into the CN-3 Zone. Therefore, a Conditional Use Permit is required to allow the proposed industrial use within this portion of the site.

The encroachment of the new industrial use into the CN-3 Zone is appropriate given that this site has long been an industrial site, as well as the fact that many other uses that would be typically desired such as mixed-use buildings would not be allowed on the subject property given the land use restrictions due to the on-site contamination.

Design Review

The proposed development is subject to Design Review pursuant to Planning Code Section 17.136, including the Category II Demolition Findings in Section 17.136.075 of the Planning Code.

As described earlier in this staff report, the proposal would retain the front "bulkhead" portion of Building #1 to incorporate into the development proposal, which is the most visible portion of the property as it fronts International Boulevard, and will act as a corner element to the new building. The new warehouse building would be constructed to the side and rear of the existing façade and occupy a large percentage of the project site. For the most part the façade of the new building is utilitarian and minimalist in design with a two-tone cement plaster façade and windows along the street fronting façade that relate to the historic window pattern of the existing Building #1 façade. The corners of the new building have been designed as modern interpretations of the Building #1 bulkhead with a brick veneer. The side building elevations are broken up by loading dock doors on the south elevation, and color patterns similar to the loading dock doors on the north elevation, both of which would contain clerestory windows to allow light into the building.

The proposed design has appeared before the Landmarks Board, which was supportive of the design concept and in agreement with the Demolition findings submittal that demonstrated the need to demolish the buildings on-site due to elevated levels of contamination of PCB's in the brick walls and concrete floors of the building, which has made the rehabilitation of the historic structures on site infeasible. This is except for the front "bulkhead" of Building #1 as it was previously used for office purposes and requires less remediation than other parts of the building that were part of the factory operations.

ENVIRONMENTAL REVIEW PROCESS

The City is the Lead Agency pursuant to CEQA and has the responsibility to prepare the EIR for the Project, under the requirements of CEQA, pursuant to Public Resources Code Section 21000 *et. seq.* An Initial Study was not prepared for the Project, as authorized under Section 15060(d) of the CEQA Guidelines.

Publication and Distribution of the DEIR

As stated earlier in this report, the City published the NOP December 21, 2018. A scoping session was held before the Landmarks Preservation Advisory Board on January 14, 2019, and the Oakland Planning Commission on January 16, 2019. Chapter 6 of the Draft EIR, *Other CEQA Considerations,* provides a

brief discussion of the following environmental topics that during scoping were determined to have less than significant impacts with implementation of the City's Standard Conditions of Approval: Land Use and Planning; Mineral Resources; Population and Housing; Public Services; Recreation; and Utilities. The following environmental topics were addressed in detail in the Draft EIR:

A. Cultural and Tribal Cultural Resources
B. Hazards and Hazardous Materials
C. Transportation and Circulation
D. Air Quality
E. Greenhouse Gas Emissions and Energy
F. Noise and Groundborne Vibration
G. Geology, Soils and Seismicity
H. Hydrology and Water Quality

Potentially Significant Impacts Identified in the Draft EIR

All impacts, City Standard Conditions of Approval and Mitigation Measures identified in the Draft EIR were summarized in Table 2-1 at the end of Chapter 2 (Summary) of the Draft EIR. Table 2-1 also identifies the level of significance of the impact after City Standard Conditions of Approval and recommended Mitigation Measures are implemented. Other than the impacts discussed below, all of the environmental effects of the Project can be reduced to less than significant levels through implementation of Standard Conditions of Approval or recommended Mitigation Measures.

The Draft EIR identified the following **Significant and Unavoidable** environmental impacts related to Cultural Resources and Greenhouse Gas Emissions:

- **Impact CULT - 1:** Demolition of buildings on the project site would adversely affect two historical buildings and an Area of Primary Importance that qualify as historical resources under CEQA.

- **Impact CULT – 2:** Demolition of buildings on the project site would adversely affect two historical buildings and an Area of Primary Importance that qualify as historical resources under CEQA and would contribute to a significant cumulative impact to historical resources in Oakland.

- **Impact GHG – 1:** Project construction and operation would generate GHG emissions that would exceed the City's target threshold and result in a significant and unavoidable impact.

- **Impact GHG – 2:** Project operations could conflict with applicable GHG plans, policies, or regulations.

- **Impact GHG – 3:** Project construction and operation would generate GHG emissions and would contribute to a significant and unavoidable cumulative impact.

The following is a summary of Mitigations that are proposed to respond to the impacts listed above but do not reduce the impacts to Less than Significant (these Mitigations are provided in more detail in Chapter 4.1 and 4.5 in the Draft EIR):

- **CULT-1a:** Historical Context Report. The project applicant shall retain a qualified cultural resources consultant to prepare a historical context report and photo-documentation of the historic buildings on the project site and the 57th Avenue Industrial District API.

- **CULT-1b:** Contribution to Façade Improvement Program. The project applicant shall contribute to the City's Façade Improvement Program in the amount of $684,000. The Façade Improvement Program contribution required hereunder shall be payable upon issuance of the first demolition permit for the project. Funds collected under this mitigation shall be designated for the repair or improvement of façades within the historic 57th Avenue Industrial District API for a two-year period. After that time, all remaining funds shall be eligible for citywide Façade Improvement Program expenditures.

- **CULT-1c:** Installation of a Commemorative Marker. To reduce the significant and unavoidable impact of the adverse effect on Building #1 and loss of Building #2 and the substantial adverse change in the historic significance of the 57th Avenue Industrial District API, the project applicant shall, prior to the issuance of the demolition permit for the project, install a commemorative marker or plaque on the project site. The marker or plaque shall be made of high quality, durable, all-weather materials, and describe the history of the project site and the 57th Avenue Industrial District; examples may be taken from the Bay Trail Series concerning historic industrial buildings.

- **CULT-1d:** Preparation of a Historic Property Treatment Plan. The project applicant shall prepare a Historic Property Treatment Plan for the retained portion of Building #1, in coordination with the City and OCHS staff and prior to the issuance of the demolition permit for the project.

- **CULT-2:** Implementation of CULT-1 (same mitigations apply to the cumulative impact).

- **GHG-1:** GHG Reduction Plan Required. The project applicant shall retain a qualified air quality consultant to develop a GHG Reduction Plan for City review and shall implement the approved GHG Reduction Plan.

- **GHG-2:** Implement Mitigation Measure GHG-1 (same mitigation applies to the plan level impact).

- **GHG-3:** Implement Mitigation Measure GHG-1 (same mitigation applies to the cumulative impact).

Project Alternatives

Chapter 5 of the Draft EIR included the analysis of three alternatives, beyond the *"No Project Alternative"*, to the Proposed Project that meet the requirements of CEQA, which include a reasonable range of alternatives to the Project that would feasibly attain most of the Project's basic objectives, and avoid or substantially lessen many of the Project's significant environmental effects. The CEQA alternatives analyzed in Chapter 5 include:

- *Approved Remedy Alternative* – The Approved Remedy Alternative would be consistent with the DTSC and USEPA approved 2011 remedial action plan ("RAP") risk-based clean up and would involve demolition of all the buildings on the site, and installation of an asphalt overlay around the building locations and over slabs that would remain on the site. After capping the site, it would remain vacant. While groundwater monitoring would continue, no additional remediation or reuse of the site would occur under this alternative.

- *No Reuse Alternative* – The No Reuse alternative includes two variants. Under Variant A, all of Building #1 and Building #2 would be protected in place, but not further used or occupied. Under

Variant B, only Building #1 would be protected in place and Building #2 would be demolished and the pad capped with asphalt. Under either variant, neither building would be remediated or restored. Repairs would be made so that further building deterioration would not occur, and neither building would be occupied. This alternative further assumes the demolition of all other buildings on the site, capping of the site with an asphalt pad, and no remediation or new construction for future industrial use. Only remediation and monitoring activities currently required by DTSC and EPA would continue.

- *Preservation and Reuse Alternative* – The Preservation and Reuse alternative includes the remediation of the site for future industrial use, as described for the proposed project, and assumes that both Building #1 and Building #2 could be remediated and rehabilitated for industrial use in conformance with the Secretary of the Interior's Standards for the Treatment of Historic Properties, requirements of the City of Oakland, and USEPA and DTSC requirements, assumed to be similar to the requirements in the RDIP Addendum to allow reuse of the bulkhead portion of Building #1. All other structures on the site would be demolished. After remediation, the remainder of the site would be developed with buildings or a building to support industrial uses.

The DEIR concluded that Variant A of the No Reuse alternative is the environmentally superior alternative. Under this alternative, repairs would be made so that Buildings #1 and #2 would not continue to deteriorate, would be protected in place, and would remain vacant. The buildings would not be restored or remediated for contamination to allow for reuse. This alternative assumes demolition and capping of pads for all other buildings on the site. Variant A would reduce the blighting influence on the surrounding neighborhood, retain the historic resources, reduce risks associated with hazardous materials and avoid impacts associated with greenhouse gas emissions as the buildings and site would remain vacant.

Response to Comments Document

The Response to Comments Document (which together with the DEIR make up the Final EIR) was published on May 22, 2020. The Response to Comments Document includes written responses to all comments received during the public review period on the DEIR and at the public hearings on the DEIR held by the Planning Commission. The FEIR was provided under separate cover for review and consideration by the Planning Commission, and notice of availability was sent to all who commented and is available to the public on the City of Oakland website.

CONCLUSION

The project site is a large vacant industrial site that has been shuttered due to high contamination levels. The proposed project will remediate the site to lessen the risk to surrounding properties and allow the site to be put back into an active economic use. The proposal, while demolishing historic resources due to contamination levels, will be preserving and incorporating the most prominent architectural element of the site by incorporating the Building #1 "bulkhead" into the new building thus preserving some element of the historic property. The proposal will address a long time public nuisance and put the property back into use as an industrial warehousing facility that will create jobs in the area with good access to public transit with the AC Transit BRT line running directly in front of the site along International Blvd. Staff recommends that the Planning Commission support the proposed development with the attached Conditions and Mitigations.

RECOMMENDATION

1) Adopt the attached CEQA findings, including Certification of the EIR, rejection of alternatives as infeasible and, Statement of Overriding Considerations, and Standard Conditions of Approval/Mitigation Monitoring and Reporting Program (SCAMMRP).

2) Approve the Design Review and Major Conditional Use Permit as described in this report subject to the conditions (including the Standard Conditions of Approval/Mitigation Monitoring and Reporting Program (SCAMMRP)), requirements, and findings contained in this staff report.

Prepared by:

Peterson Z. Vollmann, Planner IV

Reviewed by:

Catherine Payne

Catherine Payne, Acting Development Planning Manager
Bureau of Planning

Approved for forwarding to the Planning Commission:

Ed Manasse, Deputy Director
Department of Planning and Building

Attachments:
A. Findings, including CEQA Findings
B. Conditions of Approval
C. SCAMMRP
D. Project Plans

Note: The FEIR was provided under separate cover for review and consideration by the Planning Commission, and is available to the public on the City website at:
https://www.oaklandca.gov/documents/ge-site-remediation-and-redevelopment-project-draft-environmental-impact-report-deir-5441-international-blvd-case-file-number-er18013

ATTACHMENT A

FINDINGS

This proposal meets all the required findings under **Sections 17.136.050B and 17.136.075C (Design Review/Category II Demolition Findings) and Sections 17.134.050, 17.33.030(L4) and 17.102.110 (Conditional Use Permit Criteria)** of the Oakland Planning Code (OMC Title 17) as set forth below and which are required to approve your application. Required findings are shown in **bold** type; reasons your proposal satisfies them are shown in normal type. In addition, findings have been developed pursuant to the California Environmental Quality Act (Pub. Res. Code, § 21000 et seq.; "CEQA") and the CEQA Guidelines (Cal. Code Regs. Title 14, § 15000 et seq.). The basis to approve the Project and related permits are not limited to the findings contained herein, but also includes the information contained in the April 15, 2020 Staff Report to the Planning Commission, the conditions of approval and the Standard Conditions of Approval/Mitigation Monitoring and Reporting Program (SCAMMRP), the EIR prepared for the Project, and the entire administrative record, hereby incorporated by reference.

SECTION 17.136.050.B – NON-RESIDENTIAL DESIGN REVIEW CRITERIA

1. **That the proposal will help achieve or maintain a group of facilities which are well related to one another and which, when taken together, will result in a well-composed design, with consideration given to site, landscape, bulk, height, arrangement, texture, materials, colors, and appurtenances; the relation of these factors to other facilities in the vicinity; and the relation of the proposal to the total setting as seen from key points in the surrounding area. Only elements of design which have some significant relationship to outside appearance shall be considered, except as otherwise provided in Section 17.136.060;**

   The proposal will retain the existing front "bulkhead" portion of the historic Building #1 on the site, and construct a new warehousing facility to the rear and side. The new building will be a utilitarian warehousing facility that will include corner design elements that relate to the historic "bulkhead" that is being retainined, by incorporating a brick veneer and window patterns that reflect those on the historic building. The height and bulk of the structure is similar to that interms of site coverage as other industrial buildings in the area. The site will include landscape and fencing buffers around the site in particular to the north along the property line that abuts residential properties.

2. **That the proposed design will be of a quality and character which harmonizes with, and serves to protect the value of, private and public investments in the area.**

   The proposal would construct a new industrial facility that would retain a prominent portion of the historic building that fronts onto International Blvd. The new structure will largely be a utilitarian industrial building, but will incorporate corner elements to the building that will relate to the historic "bulkhead" that is being retainined and incorporated into the building. The new facility will put the site back into an active use and will include new landscaping improvements that will enhance the visual quality of the site that will help to protect the value of investments in the area.

3. **That the proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district plan, or development control.**

   The proposal is consistent with the LUTE by remediating and redeveloping the existing industrial site into a new industrial/warehousing activity that may accommodate a new business and would provide new job opportunities in the area while locating the truck related functions of the site as far away from the adjacent residential zone as possible. The portion of the site within the Neighborhood Center land use area will remain largely unchanged except that a new use will be introduced to the long vacant industrial site that will re-establish a job center on International Blvd. with good access to public transportation. Given the land use covenants on the site that restrict residential activities a mixed-use development would not be appropriate nor permitted along this project site frontage.

SECTION 17.136.075.C – CATEGORY II DEMOLITION FINDINGS:

1. **For the demolition of structures in the CIX-1A Zone; or contributors to an S-7 Zone, S-20 Zone, or API:**

   a. **The applicant demonstrates that: i) the existing property has no reasonable use or cannot generate a reasonable economic return and that the development replacing it will provide such use or generates such return, or ii) the applicant demonstrates that the structure constitutes a hazard and is economically infeasible to rehabilitate on its present site. For this criterion, a hazard constitutes a threat to health and safety that is not immediate; and**

      The applicant has submitted documentation on the extent of hazardous material contamination that is present in the concrete flooring and brick walls of the structure. The cost to remediate the building would exceed any expected economic resturns and is therefore infeasible.

   b. **It is economically, functionally, architecturally, or structurally infeasible to incorporate the historic structure, or existing structure in the CIX-1A Zone, into the proposed development.**

      While it is not economically feasible to incorporate the majority of either Building #1 or Building #2, the proposal will include the retention of the front "bulkhead" portion of Building #1, which is the most architecturally prominent element of the building as seen from International Blvd., and was historically used as an office area and is thus more economically viable to remediate due to the small area and reduced levels of contamination.

2. **For the demolition of noncontributors to an S-7 Zone, S-20 Zone, or API: The existing structure is either: i) seriously deteriorated or a hazard; or ii) the existing design is undistinguished and does not warrant retention. For this finding, a hazard constitutes a threat to health and safety that is not immediate;**

   Other than Buildgins #1 & #2, the other structures on the site prposed for demolition are non-contributors to the API. All of these additional structures were later additions to the site that consist of plain concrete clock and pre-fabricated metal buildings that were added between the

1960's to 1980's. All of these structures are undistringuished buildings of no particular architectural or historic significance that do not warrant retention.

3. For the demolition of any structure in an S-7 Zone, S-20 Zone, or API:

a. The design quality of the replacement structure is equal/superior to that of the existing structure; and

The proposed design will incorporate the "bulkhead" portion of the existing historic Building #1, which is the most prominent architectural feature of the site as seen from the preojct frontage on International Blvd. The remaining portions of the historic Building #1 and Building #2 were very utilitarian industrial buildings for their time period of construction. The new building will include elements that will be modern interpretations of the retainined "bulkhead" located at the corners of the building which will be similar in design quality, while other portions of the building that do not face out on to the street will be more utilitarian in design, consistent with the design context of the historic buildings.

b. The design of the replacement project is compatible with the character of the district, and there is no erosion of design quality at the replacement project site and in the surrounding area. This includes, but is not necessarily limited to, the following additional findings:

i. The replacement project is compatible with the district in terms of massing, siting, rhythm, composition, patterns of openings, quality of material, and intensity of detailing;

The proposed project is compatible with the 57th Avenue API by its low massing, brick- and glass clad exterior, utilitarian appearance, with symmetrical layout and rhythmic placement of fenestration and door openings. Although the new construction would be larger than most of the contributing elements of the 57th Avenue API, its scale is proportionate to the larger site at 5441 International Blvd being on of the larger sites within the district and acting as an anchor to the northern end of the district. The street facing façade references, but does not replicate, the retained "bulkhead" portion of GE Building #1's Classical Revival-styled façade, the symmetrical layout of fenestration and use of masonry wall cladding at the far left side is representative of the materials and detailing found in other contributing elements within the 57th Avenue API.

ii. New street frontage includes forms that reflect the widths and rhythm of the facades on the street and entrances that reflect the patterns on the street;

The street frontage of the proposal will retain the existing "bulkhead" of Building #1, and incorporate new elements that refererence this historic façade with a modern interpretation, which picks up on the materials and window opening patterns of the historic building.

iii. The replacement project provides high visual interest that either reflects the level and quality of visual interest of the district contributors or otherwise enhances the visual interest of the district;

The proposal will enhance the visual interest of the district by making building and site improvements to the site, which has long been vcacnt and a declared public

nuisance. The proposal will retain the most architecturallu significant element of Building #1, by retaining the front "bulkhead" of the building and establishing a new façade that relates to the historic character of the building with a modern interpretation.

iv. If the design contrasts the new to the historic character, the replacement project enriches the historic character of the district;

As stated previously, the proposed design will incorporate the historic "bulkhead" portion of Building #1, and the new street fronting elevation will contain corner elements that are designed to act as modern interpretations of the building with similar materiality and opening patterns. The portions of the building that will differ are the non-street fronting elevations, which will be much more utililitarian in design, which is appropriate for an industrial building and district.

v. The replacement project is consistent with the visual cohesiveness of the district. For the purpose of this item, visual cohesiveness is the architectural character, the sum of all visual aspects, features, and materials that defines the district. A new structure contributes to the visual cohesiveness of a district if it relates to the design characteristics of a historic district. New construction may do so by drawing upon some basic building features, such as the way in which a building is located on its site, the manner in which it relates to the street, its basic mass, form, direction or orientation (horizontal vs. vertical), recesses and projections, quality of materials, patterns of openings and level of detailing. When a combination of some of these design variables are arranged in a new building to relate to those seen traditionally in the area, but integral to the design and character of the proposed new construction, visual cohesiveness results; and

The proposed project is consistent with the visual cohesiveness of the 57th Avenue API because the new construction would properly reference the architectural character and massing of the retainied "bulkhead" of Building #1 and with the general unadorned industrial aesthetics found on the remaining contributing elements of the 57th Avenue API. The new construction design would be cohesive with the Building #1 and the API by its uniform height and cornice line, use of the symmetrical façade layout, and infill construction of rhythmic alternating pattern of solid and fenestrated walls.

vi. The replacement project will not cause the district to lose its current historic status.

The proposed project would remove two of the 18 remaining contributing elements to the 57th Avenue API. These two elements (the rear portion of GE Building #1 and all of GE Building #2) are located along the northern edge of the API. The loss of the two contributors represents an 11% reduction in the number of remaining contributing elements to the API. This reduction, along with the retention of the front "bulkhead" portion of Building #1 would not diminish the API's overall integrity of location, setting, design, materials, workmanship, feeling, and association. The 57th Avenue Industrial District API would retain those characteristics as an assemblage of 1920s-1950s industrial properties that qualify it as a NRHP-quality district. When completed, the 57th Avenue Industrial District API will retain its eligibility as an NRHP-quality district. The EIR for the project found that proposed project would result in a less than significant impact upon the API.

SECTION 17.134.050 –CONDITIONAL USE PERMIT FINDINGS:

1. **That the location, size, design, and operating characteristics of the proposed development will be compatible with, and will not adversely affect, the livability or appropriate development of abutting properties and the surrounding neighborhood, with consideration to be given to harmony in scale, bulk, coverage, and density; to the availability of civic facilities and utilities; to harmful effect, if any upon desirable neighborhood character; to the generation of traffic and the capacity of surrounding streets; and to any other relevant impact of the development.**

   The proposal will reutilize a long time vacant site that has historically been an industrial use providing jobs to the surrounding area. The proposal will demolish the existing contaminated buildings and construct a new industrial facility that will fit in with the scale, bulk and site coverage of the surrounding industrial facilities within the area, and the proposal will provide new landscape buffers to the residential properties to the north which had not previously exisited. The traffic impact study demonstrated that the proposal will not create a trip generation that would exceed the capacity of the existing surrounding streets.

2. **That the location, design, and site planning of the proposed development will provide a convenient and functional living, working, shopping, or civic environment, and will be as attractive as the nature of the use and its location and setting warrant.**

   The proposal will reutilize a long time vacant contaminated property and put the site back to an active use that will create new jobs for the area and create a functional working environment by being located on International Blvd. with excellent acces to the public transit. The proposed design will create an industrial facility that will fit in with other industrial facilities in the area and will retain the historic bulkhead of Building #, visible from Internaitonal Blvd., to preserve some of the historic significance of the site.

3. **That the proposed development will enhance the successful operation of the surrounding area in its basic community functions, or will provide an essential service to the community or region.**

   The General Industiral (IG Zone) zoning designation of the site is intended to create areas for more intensive industrial uses and act as job centers for the area. The proposal fits into this intent by reutilizing a long time vacant and contaminated site and constructing new facilities that will provide job opportunities and take advantage of the close proximity to the Port of Oakland.

4. That the proposal conforms to all applicable design review criteria set forth in the DESIGN REVIEW PROCEDURE of Chapter 17.136 of the Oakland Planning Code.

See Design Review findings above.

5. That the proposal conforms in all significant respects with the Oakland General Plan and with any other applicable plan or development control map which has been adopted by the City Council.

The proposal is consistent with the LUTE by remediating and redeveloping the existing industrial site into a new industrial/warehousing activity that may accommodate a new business and would provide new job opportunities in the area while locating the truck related functions of the site as far away from the adjacent residential zone as possible. The portion of the site within the Neighborhood Center land use area will remain largely unchanged except that a new use will be introduced to the long vacant industrial site that will re-establish a job center on International Blvd. with good access to public transportation. Given the land use covenants on the site that restrict residential activities a mixed-use development would not be appropriate nor permitted along this project site frontage.

17.102.110: CONDITIONAL USE PERMIT CRITERIA – EXPANSION OF USES INTO ADJACENT ZONE

1. That the location, size, design, and other characteristics of the entire use as proposed will substantially improve or provide superior environmental relationships among all uses in the immediate vicinity.

The allowance of the industrial use and facility to encroach into the CN-3 Zone would not alter any of the environmental relationships among all uses in the immediate vicinity. A portion of the historic Building #1 that is to be retainied on-site currently encroaches into the CN-3 Zone, and by allowing the new facility to be constructed within a small portion of the CN-3 Zone the overall project design is enhanced to provide for a more prominent street facing façade for International Blvd. The majority of the CN-3 zoned portion of the site has historically been used as a parking lot and landscaped area, and under the proposal this would continue to be the case, which is preferred in order to preserve the historic setting of the site.

2. That the design and site planning of all buildings, open areas, parking, service areas, paths, stairways, accessways, corridors, and balconies will be so designed as to not adversely affect the privacy, safety, or environmental amenities of adjacent properties.

The site planning of the CN-3 Zoned portion of the property will remain nearly unchanged from the historic setting, in that the area will be used as a parking lot and landscaped area. The portion of the building that would be near residential and commercial properties will remain unchanged, except that the project will provide for new enhanced landscaping to improve the appearance of the site and provide a better buffer to the adjacent properties.

3. That within the expansion area every reasonable effort will be undertaken to preserve natural grades, topographic features, watercourses, and significant landscape features.

The project site is relatively flat and no topographic or landscape feature currently exist.

SECTION 17.33.030 (L4) – CN ZONE CONDITIONAL USE PERMIT CRITERIA

1. That the proposal will not detract from the character desired for the area;

The proposal is to reutilize the existing vacant and contaminated industrial site with a new facility and active uses. While the desired character of the CN zones is typically to have active ground floor shops adjacent to the sidewalk, the existing historic setting does not allow for this type of development, both in that there are historic structures that will be preserved that are setback from the street in their original historic setting and that the site is contaminated and mixed commercial/residential uses would be prohibited on the property under land use covenants. What the proposal will do is return a job center to the area that will help to provide a customer base to the other commercial areas within the CN zones near the site.

2. That the proposal will not impair a generally continuous wall of building facades;

The existing setting of this stretch and frontage of International Blvd. is and has historically been an industrial setting. The proposal will preserve the existing setting and will not detract from a continuous wall of building facades as no such condition currently exists.

3. That the proposal will not weaken the concentration and continuity of retail facilities at ground level, and will not impair the retention or creation of an important shopping frontage;

The existing setting of the block frontage is primarily industrial and no concentration of retail facilities currently exists, and the creation of which would be challenging as noted in Finding #1 above.

4. That the proposal will not interfere with the movement of people along an important pedestrian street; and

The proposal will include the reestablishment of active driveways along International Blvd., which have historically existed and are necessary due to the limited access from other streets to the site. The proposal will include a variant that could potentially provide the majority of access through an easement off San Leandro. However, that is only through an easement that may be revocable at any time and thus the driveways on International are necessary. The reactivation of these driveways will not interfere with the movement of people along International as there will still be a large sidewalk with more than adequate site distance for pedestrians to see oncoming vehicles.

5. That the proposal will conform in all significant respects with any applicable district plan which has been adopted by the City Council.

The proposal is consistent with the LUTE by remediating and redeveloping the existing industrial site into a new industrial/warehousing activity that may accommodate a new business and would provide new job opportunities in the area while locating the truck related functions of the site as far away from the adjacent residential zone as possible. The portion of the site within the Neighborhood Center land use area will remain largely unchanged except that a new use will be introduced to the long vacant industrial site that will re-establish a job center on International Blvd. with good access to public transportation. Given the land use covenants on the site that restrict residential activities a mixed-use development would not be appropriate nor permitted along this project site frontage.

CEQA FINDINGS, INCLUDING CERTIFICATION OF THE EIR, REJECTION OF ALTERNATIVES AS INFEASIBLE AND ADOPTION OF A STATEMENT OF OVERRIDING CONSIDERATIONS

I. INTRODUCTION

1. These findings are made pursuant to the California Environmental Quality Act (Pub. Res. Code section 21000 et seq (CEQA)) and the CEQA Guidelines (Cal. Code Regs. title 14, section 15000 et seq.) by the City of Oakland Planning Commission in connection with the Environmental Impact Report (EIR) prepared for the General Electric (GE) Site Remediation and Redevelopment Project at 5441 International Blvd. in Oakland, California (Project, Case Numbers PLN19-076/ER18 013), SCH# 2018122043.

2. These CEQA findings are included as part of Attachment A and attached and incorporated by reference into each and every staff report, resolution and ordinance associated with approval of the Project. Attachment B contains conditions of approval, which includes as reference Attachment C, the Standard Conditions of Approval and Mitigation Monitoring and Reporting Program (SCAMMRP). All Attachments are incorporated by reference into each other and into the ordinance or resolution to which the Attachment is attached.

3. These findings are based on substantial evidence in the entire administrative record, and references to specific reports and specific pages of documents are not intended to identify those sources as the exclusive basis for the findings.

II. PROJECT DESCRIPTION

The The GE Project site consists of approximately 24 acres on International Blvd. (State Route 185), between 54th and 57th Avenues, and is located east of San Leandro Street and the Bay Area Rapid Transit (BART) tracks in the Melrose neighborhood of Oakland, CA. The eight existing buildings on the site are vacant and were formerly used for manufacturing, although a portion of Building #1 fronting International Blvd. contained accessory office uses. The Project site is included in the list of Hazardous Waste and Substances sites in the Department of Toxic Substances Control (DTSC) EnviroStor database, one of the lists meeting the "Cortese List" requirements. The buildings and site contain hazardous chemicals (including polychlorinated biphenyls (PCBs)) in the soil, groundwater and building materials, and have been undergoing remediation and monitoring under the oversight of DTSC and the United States Environmental Protection Agency (USEPA). In 1993 a deed restriction was imposed on the property by DTSC and only commercial or industrial uses are allowed and all other types of uses are prohibited.

The Oakland Cultural Heritage Survey (OCHS) assigned a property rating of "A1+" to Building #1, also listed on the California Register of Historical Resources on the site and "Dc1+" to Building #2, indicating that Building #1 is of "Highest Importance" and that both Building #1 and Building #2 are contributing elements to the 57th Avenue Industrial District Area of Primary Importance (API), and are therefore CEQA historic resources.

The remediation and redevelopment Project would include demolition of the eight existing structures, foundations and associated equipment, including Building #2 and the majority (approximately 94 percent) of Building #1. The front "bulkhead" portion (i.e., the portion of the building that expresses early-20th century utilitarian Classical Revival-inspired industrial architecture and a portion of the sides of the building) would be preserved, treated or encapsulated to contain any contaminated materials, and incorporated into the design of the new building. The site would be sufficiently remediated to permit its reuse. The demolition, abatement, remediation and ongoing monitoring activities would be conducted

with regulatory agency oversight by the USEPA and DTSC. After demolition and remediation, an approximately 534,208-square-foot industrial building would be constructed, with 524,208 square feet of warehouse space, 5,000 square feet of accessory office uses, and 5,000 square feet of accessory mezzanine office. There would be 93,522 square feet of landscaping provided. The warehouse would have 85 dock doors and 219 parking stalls would be provided on the site. Building construction would include soil vapor barriers, clean utility corridors and other protections for construction workers and employees of the new facility and will be overseen by the USEPA and DTSC. New connections would be made to existing utility systems.

For the proposed project, automobile and heavy truck access to and from the site would occur on International Blvd. via new access points. As part of the project, the signal and striping at the intersection with 55th Avenue would be modified to allow for left in and left out vehicle access movements. A variant to the project, referred to as the San Leandro Street variant or access variant, is also being considered in this EIR. The access variant would include the same remediation and warehouse development as the project, but would expand the Project site to include leased Union Pacific right-of-way along the southwestern site boundary sufficient to allow all project-related truck traffic to access the site to and from San Leandro Street via 54th Avenue. All project-related automobile and light trucks would continue to use the International Blvd. access as described above.
EIR.

III. ENVIRONMENTAL REVIEW OF THE PROJECT

4. Pursuant to CEQA and the CEQA Guidelines, a Notice of Preparation ("NOP") of an EIR was published on December 21, 2018. The City has adopted uniformly applied development policies and/or standards (the "Standard Conditions of Approval" (SCA)) that apply to all projects in the City and substantially mitigate or eliminate environmental impacts. Factors studied in the EIR with less-than-significant impacts because of the requirements contained in the City's Standard Conditions of Approval include: Aesthetics, Transportation and Circulation, Noise and Groundborne Vibration, Soils, Geology and Seismicity, Hydrology and Water Quality and Utilities. The following topics were found to be less than significant without the implementation of any mitigation measures or Standard Conditions of Approval: Shadow, Wind, Agricultural and Forestry Resources, Biological Resources, Land Use, Mineral Resources, Population and Housing, Public Services, Tribal Cultural Resources, and Wildfire. The topics studied in the EIR with less than-significant impacts after the implementation of mitigation measures and Standard Conditions of Approval include: Hazards and Hazardous Materials and Air Quality. The topics studied in the EIR with significant and unavoidable impacts, even with all feasible mitigation measures include: Cultural Resources, and Greenhouse Gas Emissions. The NOP was distributed to state and local agencies, posted at the Project site, and mailed to property owners within 300 feet of the Project site. Additionally, the NOP was sent to the State Clearinghouse. Scoping sessions were held for the Project on January 14, 2019 and January 16, 2019 before the Landmarks Preservation Advisory Board and Planning Commission, respectively, concerning the scope of the EIR. The public comment period on the NOP ended on January 22, 2019.

5. A draft of the EIR was prepared for the Project (DEIR) to analyze its environmental impacts. Pursuant to CEQA and the CEQA Guidelines, a Notice of Availability/Notice of Release and the DEIR were published on December 20, 2019. The Notice of Availability/Notice of Release of the DEIR was distributed to appropriate state and local agencies, posted at the Project site, mailed to property owners within 300 feet of the Project site, and mailed to individuals who have requested to specifically be notified of official City actions on the Project. Copies of the DEIR were also distributed to appropriate federal, state and local agencies, City officials including the Planning Commission, and made available for public review at the City of Oakland's Department of Planning and Building, Planning and Zoning Division (250 Frank H. Ogawa Plaza, Suite 2214) and on the City's website. Two duly noticed Public

Hearings on the DEIR were held before the City of Oakland Planning Commission and Landmarks Preservation on January 13, 2020 and January 22, 2020, respectively. The DEIR was properly circulated for a 45-day public review period ending on February 3, 2020.

6. The City received written and oral comments on the DEIR. The City prepared responses to comments on environmental issues and made clarifying changes to the DEIR. The responses to comments, changes to the DEIR, and additional information were published in a Final EIR ("FEIR") on May 22, 2020. The DEIR, the FEIR and all appendices thereto constitute the "EIR" referenced in these findings. The FEIR was made available for public review on May 22, 2020, 13 days prior to the duly noticed June 3, 2020 Planning Commission public hearing. Notice of and access to the FEIR was provided to those state and local agencies who commented on the NOP and DEIR, submitted electronically to the State Clearinghouse CEQAnet web portal, posted on the Project site, mailed to property owners within 300 feet of the Project site, and mailed to individuals who have requested to specifically be notified of official City actions on the Project. Notice of and access to the FEIR was also provided to City officials including the Planning Commission and Landmarks Preservation Advisory Board, and made available for public review on the City's website. Pursuant to CEQA Guidelines, responses to public agency comments on the Draft EIR have been published and made available to all commenting agencies at least 10 days prior to the final certification hearing. The Planning Commission has had an opportunity to review all comments and responses thereto prior to consideration of certification of the EIR and prior to taking any action on the proposed Project.

IV. THE ADMINISTRATIVE RECORD

7. The record, upon which all findings and determinations related to the approval of the Project are based, includes the following:

a. The EIR and all documents referenced in or relied upon by the EIR.

b. All information (including written evidence and testimony) provided by City staff to the Landmarks Preservation Advisory Board and Planning Commission relating to the EIR, the approvals, and the Project.

c. All information (including written evidence and testimony) presented to the Landmarks Preservation Advisory Board and Planning Commission by the environmental consultant and sub-consultants who prepared the EIR or incorporated into reports presented to the Planning Commission.

d. All information (including written evidence and testimony) presented to the City from other public agencies relating to the Project or the EIR.

e. All final applications, letters, testimony, reports, studies, memoranda, maps, and presentations presented by the Project sponsor and its consultants to the City in connection with the Project.

f. All final information (including written evidence and testimony) presented at any City public hearing or City workshop related to the Project and the EIR.

g. For documentary and information purposes, all City-adopted land use plans and ordinances, including without limitation general plans, specific plans and ordinances, together with environmental review documents, all documents referenced in and relied upon in such environmental review documents, findings, mitigation monitoring programs and other documentation relevant to planned growth in the area.

h. The Standard Conditions of Approval for the Project and Mitigation Monitoring and Reporting Program for the Project (the Standard Conditions of Approval and Mitigation Monitoring and Reporting Program (SCAMMRP)).

i. All other documents composing the record pursuant to Public Resources Code section 21167.6(e).

8. The City has relied on all of the documents listed above in reaching its decisions on the proposed Project even if not every document was formally presented to City decision-making bodies or City Staff as part of the City files generated in connection with the Project. Without exception, any documents set forth above not found in the Project files fall into one of two categories. Many of them reflect prior planning or legislative decisions of which the City decision-making bodies were aware in approving the Project. (See City of Santa Cruz v. Local Agency Formation Commission (1978) 76 Cal.App.3d 381, 391-392; Dominey v. Department of Personnel Administration (1988) 205 Cal.App.3d 729, 738, fn. 6.) Other documents influenced the expert advice provided to City Staff or consultants, who then provided advice to the City decision-making bodies for the Project. For that reason, such documents form part of the underlying factual basis for the City's decisions relating to approval of the Project. (See Pub. Resources Code, § 21167.6, subd. (e)(10); Browning-Ferris Industries v. City Council of City of San Jose (1986) 181 Cal.App.3d 852, 866.).

9. The custodian of the documents and other materials that constitute the record of the proceedings upon which the City's decisions are based is the Director of City Planning, Department of Planning and Building, Bureau of Planning, or his/her designee. Such documents and other materials are located at 250 Frank H. Ogawa Plaza, Suite 2214, Oakland, California, 94612.

V. CERTIFICATION OF THE EIR

10. In accordance with CEQA, the Planning Commission certifies that the EIR has been completed in compliance with CEQA. The Planning Commission has independently reviewed the record and the EIR prior to certifying the EIR and approving the Project. By these findings, the Planning Commission confirms, ratifies, and adopts the findings and conclusions of the EIR as supplemented and modified by these findings. The EIR and these findings represent the independent judgment and analysis of the City and the Planning Commission.

11. The Planning Commission recognizes that the EIR may contain clerical errors. The Planning Commission reviewed the entirety of the EIR and bases its determination on the substance of the information it contains.

12. The Planning Commission certifies that the EIR is adequate to support all actions in connection with the approval of the Project and all other actions and recommendations as described in the June 3, 2020 Planning Commission staff report. The Planning Commission certifies that the EIR is adequate to support approval of the Project or the San Leandro access variant described in the EIR, and any minor modifications to the Project or to the San Leandro Street access variant described in the EIR.

VI. ABSENCE OF SIGNIFICANT NEW INFORMATION

13. The Planning Commission recognizes that the FEIR incorporates information obtained and produced after the DEIR was completed, and that the FEIR contains additions, clarifications, and modifications. The Planning Commission has reviewed and considered the FEIR and all of this information. The new information added in the FEIR merely clarifies and makes insignificant changes to an adequate DEIR, and does not add significant new information to the DEIR that would require recirculation of the EIR under CEQA. The new information added to the EIR does not involve a new significant environmental impact, a substantial increase in the severity of a previously identified significant environmental impact, or a

feasible mitigation measure or alternative considerably different from others previously analyzed that the Project sponsor declines to adopt and that would clearly lessen the significant environmental impacts of the Project. No information indicates that the DEIR was inadequate or conclusory or that the public was deprived of a meaningful opportunity to review and comment on the DEIR. Thus, recirculation of the EIR is not required.

14. The Planning Commission finds that the changes and modifications made to the EIR after the DEIR was circulated for public review and comment do not individually or collectively constitute significant new information within the meaning of Public Resources Code section 21092.1 or CEQA Guidelines section 15088.5.

VII. STANDARD CONDITIONS OF APPROVAL AND MITIGATION MONITORING AND REPORTING PROGRAM

15. Public Resources Code section 21081.6 and CEQA Guidelines section 15097 require the City to adopt a monitoring and reporting program to ensure that the mitigation measures and revisions to the Project identified in the EIR are implemented. The SCAMMRP is attached and incorporated by reference into the June 3, 2020 Planning Commission staff report prepared for the approval of the Project, is included in the conditions of approval for the Project, and is adopted by the Planning Commission. The SCAMMRP satisfies the requirements of CEQA.

16. The standard conditions of approval ("SCA") and mitigation measures set forth in the SCAMMRP are specific and enforceable and are capable of being fully implemented by the efforts of the City of Oakland, the applicant, and/or other identified public agencies of responsibility. As appropriate, some standard conditions of approval and mitigation measures define performance standards to ensure no significant environmental impacts will result. The SCAMMRP adequately describes implementation procedures and monitoring responsibility in order to ensure that the Project complies with the adopted standard conditions of approval and mitigation measures.

17. The Planning Commission will adopt and impose the feasible standard conditions of approval and mitigation measures as set forth in the SCAMMRP as enforceable conditions of approval. The City has adopted measures to substantially lessen or eliminate all significant effects where feasible.

18. The standard conditions of approval and mitigation measures incorporated into and imposed upon the Project approval will not themselves have new significant environmental impacts or cause a substantial increase in the severity of a previously identified significant environmental impact that were not analyzed in the EIR. In the event a standard condition of approval or mitigation measure recommended in the EIR has been inadvertently omitted from the conditions of approval or the SCAMMRP, that standard condition of approval or mitigation measure is adopted and incorporated from the EIR into the SCAMMRP by reference and adopted as a condition of approval.

VIII. FINDINGS REGARDING IMPACTS

19. In accordance with Public Resources Code section 21081 and CEQA Guidelines sections 15091 and 15092, the Planning Commission adopts the findings and conclusions regarding impacts, standard conditions of approval and mitigation measures that are set forth in the EIR and summarized in the SCAMMRP. These findings do not repeat the full discussions of environmental impacts, mitigation measures, standard conditions of approval, and related explanations contained in the EIR. The Planning Commission ratifies, adopts, and incorporates, as though fully set forth herein, the analysis, explanations, findings, responses to comments and conclusions of the EIR. The Planning Commission adopts the reasoning of the EIR, staff reports, and presentations provided by the staff and the Project sponsor as may be modified by these findings.

20. The Planning Commission recognizes that the environmental analysis of the Project raises controversial environmental issues, and that a range of technical and scientific opinion exists with respect to those issues. The Planning Commission acknowledges that there are differing and potentially conflicting expert and other opinions regarding the Project. The Planning Commission has, through review of the evidence and analysis presented in the record, acquired a better understanding of the breadth of this technical and scientific opinion and of the full scope of the environmental issues presented. In turn, this understanding has enabled the Planning Commission to make fully informed, thoroughly considered decisions after taking account of the various viewpoints on these important issues and reviewing the record. These findings are based on a full appraisal of all viewpoints expressed in the EIR and in the record, as well as other relevant information in the record of the proceedings for the Project.

IX. POTENTIALLY SIGNIFICANT BUT MITIGABLE IMPACTS

21. Under Public Resources Code section 21081(a)(1) and CEQA Guidelines sections 15091(a)(1) and 15092(b), and to the extent reflected in the EIR, the SCAMMRP, mitigation measures and the City's Standard Conditions of Approval, the Planning Commission finds that changes or alterations have been required in, or incorporated into, the components of the Project that mitigate to a less than significant level or avoid the Project's potentially significant effects on the environment as identified in the EIR, except where expressly stated in Section X below. These changes and/or alterations required in, or incorporated into, the components of the Project are discussed below in Sections IX.A through G.

22. The following potentially significant impacts of the Project will be reduced to a less-than-significant level through the implementation of Project mitigation measures, or where indicated, through the implementation of the City's Standard Conditions of Approval, referenced in the EIR (which are an integral part of the SCAMMRP); some of the Standard Conditions of Approval are not CEQA-related but are nevertheless included for convenience and additional information provided to the decision-makers:

A. CULTURAL RESOURCES AND TRIBAL CULTURAL RESOURCES

23. Archeological and Paleontological Resources and Human Remains: Background research indicated that there are no prehistoric or historical archaeological deposits recorded within the Project site. However, the potential for associated intact deposits to be present beneath buildings, paved surfaces, and fill material cannot be entirely ruled out. Subsurface archaeological deposits that may be affected by Project activities include black-gray soils containing marine shell and bone artifacts and subsistence debris, culturally flaked stone artifacts and debris (i.e., obsidian and chert), heat/fire-affected rock, grinding implements (e.g., mortars and pestles), and human remains. In addition, there are no recorded paleontological resources (fossils) within the Project site, nor does the Project site contain a unique geological feature. The site is underlain by Holocene-age landforms, which are too recent to contain significant fossils. Underlying these Holocene deposits at an unknown depth are older Quaternary (i.e., Pleistocene) deposits, which have a potential to contain significant fossils, including bison, mammoths, ground sloths, saber-toothed cats, dire wolves, cave bears, rodents, birds, reptiles, and amphibians. The proposed Project includes the removal of contaminated soils, however, not to a depth that would disturb Pleistocene deposits. However, the potential to encounter previously undisturbed archaeological and paleontological resources and human remains at the Project site cannot be discounted. Implementation of SCA-CULT-1: Archaeological and Paleontological Resources – Discovery During Construction (#33) and SCA-CULT-2: Archaeologically Sensitive Areas – Pre-Construction Measures (#34), as set forth in the EIR and SCAMMRP, will reduce this impact to a less-than-significant level. Implementation of SCA-CULT-1 requires that in the event that any historic or prehistoric subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and the Project applicant shall notify the City and consult with a qualified archaeologist or paleontologist, as applicable, to assess the significance of the find. If a discovered cultural resource is found to be

significant, the Project applicant shall submit an Archaeological Research Design and Treatment Plan (ARDTP) prepared by a qualified archaeologist for review and approval by the City. Implementation of SCA-CULT-2 would require the Project applicant to implement either an intensive pre-construction survey, which would identify, prior to ground disturbing activities, the potential presence of history-period archaeological resources on the Project site or a construction ALERT sheet, which will contain, at a minimum, visuals that depict each type of artifact that could be encountered on the Project site. Implementation of SCA-CULT-3: Human Remains – Discover During Construction (#35) would require all work to be halted and the City and County Coroner be contacted, and if the remains are Native American, the City shall contact the California Native Heritage Commission to determine appropriate notifications and actions. Implementation of these SCAs would ensure that this impact is reduced to a less-than-significant level.

B. HAZARDS AND HAZARDOUS MATERIALS

24. Routine Transport, Use, or Disposal of Hazardous Materials. The proposed Project and variant would involve the excavation, transportation and disposal of soil impacted with hazardous materials including PCBs and volatile organic compounds (VOCs), and the transportation and disposal of hazardous building materials that contain PCBs, heavy metals, and asbestos. Hazardous materials (e.g., fuel, oils, and paints) would be routinely transported, stored, and used at the Project site during construction activities. Removal, relocation, handling, or transportation of hazardous materials could result in accidental releases or spills and associated health risks to workers, the public, and environment, and thus would result in a potentially significant impact. Implementation of SCA-HAZ-1: Hazardous Materials Related to Construction (#43), as set forth in the EIR and SCAMMRP, requires preparation and implementation of a Stormwater Pollution and Prevention Plan (SWPPP) and Best Management Practices (BMPs) during construction; SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44) requires a building and site assessment prior to demolition or construction and preparation of a Health and Safety Plan to protect Project construction workers from risks associated with hazardous materials; and SCA-HAZ-3: Hazardous Materials Business Plan (#45) requires the preparation and implementation of a Hazardous Materials Business Plan to ensure workers are adequately trained should emergency response be required. Implementation of these SCAs would ensure that this impact is reduced to a less-than-significant level.

25. Accidental Release of Hazardous Materials. Construction and operation of the proposed Project could result in the accidental release of hazardous materials that could result in exposure of workers, the public, and/or the environment to hazardous materials. Construction of the proposed Project or variant would be subject to the requirements of the Construction General Permit, SCA-HAZ-1: Hazardous Materials Related to Construction (#43) and SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44), as set forth in the EIR and SCAMMRP, which require preparation and implementation of a SWPPP and BMPs to reduce the risk of spills or leaks from reaching the environment, including procedures to address minor spills of hazardous materials. To reduce fugitive dust emissions, Mitigation Measure HAZ-1, as set forth in the EIR and SCAMMRP, would require the Project sponsor to temporarily cap the entire site as soon as possible during construction and to monitor dust emissions at the site until the permanent cap is installed. The Project sponsor would also be required to comply with the measures identified in the Remedial Design and Implementation Plan (RDIP) Addendum and its appendices, as set forth in the EIR, that would be overseen by the USEPA and the State DTSC. During construction, protective measures in the RDIP Addendum include, but are not limited to: implementation of a Transportation Plan Addendum (Appendix H) that describes the proposed truck routes, approved disposal facilities, loading and transportation procedures for materials leaving the site during implementation of remedial activities, documentation protocol, and emergency response procedures; implementation of a Decontamination Plan (Appendix I) which specifies procedures for removal, collection, and containment of soil and other potentially contaminated material from equipment and

transportation vehicles, guidelines for the construction of a decontamination pad, decontamination of personnel and tools, and methods for temporary storage, characterization, and off-site disposal/on-site reuse of decontamination wastes generated during decontamination activities; implementation of protective measures and requirements for tenting certain excavation areas (Appendix B); implementation of decontamination measures (Appendix I); implementation of dust, odor and vapor control measures (Appendix J) and air monitoring measures (Appendix K); and implementation of a groundwater extraction and treatment system (GETS) (Appendix F). During operation, protective measures in the RDIP Addendum include, but are not limited to: preparation of a soil management plan after remediation and redevelopment is complete and construction of a vapor intrusion mitigation system (Appendix E) to be included as part of the building design. Compliance with SCA-HAZ-3: Hazardous Materials Business Plan (HMBP) (#45), which requires an HMBP to be submitted for review and approval by the City, and implementation of the approved HMBP would also reduce significant impacts related to the storage or use of acutely hazardous materials during operation. Implementation of a SWPPP, compliance with SCA-HAZ-1: Hazardous Materials Related to Construction (#43), SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44), implementation of the RDIP Addendum with oversight of DTSC and USEPA, and implementation of Mitigation Measure HAZ-1 would reduce the potential release of hazardous materials during demolition, remediation, construction and operation to a less-than-significant level.

26. Hazardous Emissions near Schools. Bridges Academy, a public elementary school, is located at 1325 53rd Avenue, approximately 450 feet northwest of the Project site. The proposed Project or variant would include the handling of hazardous materials during construction and implementation of a SWPPP, compliance with SCA-HAZ-1: Hazardous Materials Related to Construction (#43), SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44), SCA-HAZ-3: Hazardous Materials Business Plan (#45), implementation of the RDIP Addendum with oversight of DTSC and USEPA, and implementation of Mitigation Measure HAZ-1, as set forth in the SCAMMRP and EIR, would ensure that impacts related to potential releases of hazardous materials near a school during construction and operation of the Project are reduced to a less-than-significant level.

27. Exposure to Hazardous Materials. While tenants of the proposed industrial/warehouse building have not been identified, if storage of acutely hazardous materials in excess of threshold planning quantities did occur during Project operation, the Project would be required to comply with SCA-HAZ-3: Hazardous Materials Business Plan (#45), which requires an HMBP to be submitted for review and approval by the City, and implementation of the approved HMBP. The Project would also be required to comply with existing hazardous materials regulations including preparation of a Risk Management Plan (RMP) under the California Accidental Release Program, as enforced by Alameda County Department of Environmental Health (ACDEH). The RMP is implemented to prevent or mitigate releases of regulated substances that could have off-site consequences through hazard identification, planning, source reduction, maintenance, training, and engineering controls. Compliance with existing hazardous materials regulations enforced by ACDEH and SCA-HAZ-3: Hazardous Materials Business Plan (#45) would ensure that this impact is reduced to less-than-significant levels.

C. TRANSPORTATION AND CIRCULATION

28. Vehicle Miles Travelled. Because the Project would increase the number of workers and truck trips at the Project site, it would have a potential increase in vehicle miles travelled (VMT). A detailed VMT evaluation included in the EIR estimated that the proposed Project would have about 37 percent less VMT per worker than the average VMT in the Project's traffic analysis zone, based on the Metropolitan Transportation Commission Model. Therefore, the estimated VMT per worker for the proposed Project would be below the significance threshold (regional average minus 15 percent) by about 14 percent in 2020 and 20 percent in 2040. Implementation of SCA-TRA-5: Transportation and Parking Demand

Management (#79), as set forth in the SCAMMRP and EIR, requires preparation of a Transportation Demand Management (TDM) plan for the Project that would further reduce the VMT associated with the Project by at least another 10 percent. Implementation of these SCAs would ensure that impacts related to VMT would be is reduced to a less-than-significant level.

29. Consistency with Plans and Safety of the Circulation System. The Project site is adjacent to railroad tracks owned and operated by the Union Pacific Railroad Company. While the tracks are not currently in use, there is one at-grade crossings on 54th Avenue, just east of San Leandro Street. The Project's access variant would increase truck traffic at the at-grade crossing on 54th Avenue, and the railroad tracks may be used in the future. Therefore, the Project must comply with SCA-TRA-7: Railroad Crossings (#82), as set forth in the SCAMMRP and EIR, that requires the preparation of a Diagnostic Review to ensure safety at the at-grade crossing. Implementation of this SCA would ensure that this impact is reduced to a less-than-significant level.

E. AIR QUALITY

30. Criteria Air Pollutant Emissions. Project construction activities, including demolition and remediation, and Project operation could generate criteria air pollutant emissions that could affect regional air quality. The primary pollutant emissions of concern during Project construction would be ROG, NOx, PM_{10}, and $PM_{2.5}$ from the exhaust of off-road construction equipment and on-road vehicles related to worker vehicles, vendor trucks, and haul trucks. In addition, fugitive dust emissions of PM_{10} and $PM_{2.5}$ would be generated by soil disturbance and demolition activities, and fugitive ROG emissions would result from paving and architectural coatings. Implementation of the enhanced dust-control measures described under SCA-AIR-1: Dust Controls – Construction Related (#21) would satisfy the Bay Area Air Quality Management District's (BAAQMD") requirement for BMPs during construction. Additional project-specific measures to reduce dust emissions include implementation of Mitigation Measure HAZ-1, as set forth in the EIR and SCAMMRP, that requires capping of the site as soon as possible during construction and additional monitoring of dust emissions, as well measures identified in the RDIP Addendum and its appendices including implementation of protective measures and requirements for tenting certain excavation areas (Appendix B); implementation of decontamination measures (Appendix I); implementation of dust, odor and vapor control measures (Appendix J); and implementation of air monitoring measures (Appendix K). During operation, protective measures in the RDIP Addendum include, but are not limited to: preparation of a soil management plan after remediation and redevelopment is complete and construction of a vapor intrusion mitigation system (Appendix E) to be included as part of the building design. Compliance with SCA-AIR-1: Dust Controls – Construction Related (#21), Mitigation Measure HAZ-1, SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44), and implementation of the RDIP Addendum with oversight of DTSC and USEPA would reduce the impact on local air quality during Project construction and operation to a less-than-significant level.

31. New Toxic Air Contaminants. The disturbance of soil and demolition of structures on the Project site could potentially generate dust emissions containing toxic air contaminants (TACs), such as asbestos or PCBs. Compliance with SCA-AIR-6: Asbestos in Structures (#27) requires the Project to comply with all applicable laws and regulations regarding demolition of existing structures that could contain asbestos materials and to provide evidence of compliance to the City upon request. Furthermore, the Project must comply with SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44), which requires a comprehensive building materials assessment to document the presence of asbestos-containing materials (ACMs), lead-based paint, PCBs, and any other hazardous materials at the Project site, and would require the stabilization and/or removal of the identified hazardous materials in accordance with recommendations from a qualified environmental professional to comply with all applicable laws and regulations. The RDIP Addendum also includes measures for the management and/or removal of

hazardous materials including a Dust Control Plan (Appendix J) and a perimeter Air Monitoring Plan (AMP) (Appendix K). The Dust Control Plan requires that dust-generating activities (e.g., demolition and excavation) in areas where soil or building materials contain PCBs exceeding 1,000 milligrams per kilogram be performed within an enclosure. Other recommended dust-control measures in the Dust Control Plan are generally consistent with or more conservative than the City's dust-control measures required under SCA-AIR-1: Dust Controls – Construction Related (#21).The AMP would be implemented during demolition, concrete crushing, cap removal, and all other excavation/soil handling activities. The AMP includes action levels developed for airborne concentrations of dust, volatile organic compounds, and other potential contaminants of concern based on regulatory ambient air quality standards and available regulatory guidance. Specifically, action levels are developed to ensure the total cancer risk from contaminated dust and vapor emissions not to exceed 1 in a million, and the total chronic hazard index (HI) not to exceed 1.0, in accordance with the target risk thresholds established by the USEPA. Additionally, implementation of Mitigation Measure HAZ-1 requires installation of a temporary cap to reduce dust and additional testing and monitoring of excessive dust emissions should a public complaint be made. Compliance with SCA-AIR-1: Dust Controls – Construction Related (#21), SCA-AIR-6: Asbestos in Structures (#27), SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44); implementation of the RDIP Addendum (including the protective measures discussed above), and implementation of Mitigation Measure HAZ-1, as required by Mitigation Measure AIR-1, would ensure that potential air quality impacts related to contaminated dust and vapor emissions during demolition, soil handling, and redevelopment activities would be less than significant. During operation, compliance with SCA-AIR-5: Truck-Related Risk Reduction Measures (Toxic Air Contaminants) (#79) would ensure that, during operation, the truck loading docks will be located on the southeast side of the proposed warehouse building, farthest away from the sensitive receptors to the northwest of the Project site. SCA-AIR-5 also requires the Project applicant to comply with all applicable California Air Resources Board (CARB) requirements to control emissions from diesel engines and demonstrate compliance to the satisfaction of the City. Compliance with SCA-AIR-5 would ensure that DPM and $PM_{2.5}$ emissions generated at the project's truck loading docks would have a less-than-significant impact on nearby sensitive receptors. During operation, VOCs that may contain TACs, such as trichloroethylene and other chlorinated VOCs, would continue to be emitted from the proposed relocated groundwater extraction and treatment system (GETS) and vapor intrusion mitigation system (VIMS). Operation of the relocated GETS and VIMS would be subject to BAAQMD permitting requirements for soil vapor extraction operations (BAAQMD Regulation 8 Rule 47) and requirements of SCA-AIR-4: Stationary Sources of Air Pollution (Toxic Air Contaminants((#25). Compliance with BAAQMD regulations and implementation of SCA-AIR-5: Truck-Related Risk Reduction Measures (Toxic Air Contaminants) (#79) and SCA-AIR-4: Stationary Sources of Air Pollution (Toxic Air Contaminants) (#25) would ensure that potential air quality impacts related to TACs during operation of the Project would be less than significant.

E. NOISE AND GROUNDBORNE VIBRATION

32. Noise During Construction. Construction of the proposed Project and the San Leandro Street variant would involve demolition (includes demolition of existing buildings, pavement, landscaping, walls and fencing), remediation (includes removal and remediation of contaminated materials), grading, building construction, additional site improvements (paving, utility connections, landscaping), and construction of the vapor intrusion mitigation system. Construction is expected to occur over a period of 22 months and would temporarily increase noise levels in the vicinity of the Project site. Construction noise levels would vary from day to day, depending on the quantity and condition of the equipment being used, the types and duration of activity being performed, the distance between the noise source and the receptor, and the presence or absence of barriers, if any, between the noise source and receptor. Demolition, excavation/grading, and foundation work are typically the noisiest phases of construction, and would occur during the first phases of construction. The later phases of construction include activities that are typically quieter and that occur within the building under construction, thereby providing a barrier for

noise between the construction activity and any nearby receptors. All construction phases would generate exterior noise levels above the 65 dBA long-term construction noise standard at the nearest residences and the church, and above the 70 dBA long-term construction noise standard at the nearest commercial and industrial land uses. Construction noise levels also have the potential to exceed 90 dBA at the adjacent residences to the northwest of the Project site. Measures included as part of the Project that would reduce noise include an approximately 16-foot tall construction fence that would be installed along the northern boundary of the Project site, and, as noted in the Dust Control Plan (Appendix J of the RDIP Addendum), tent structures would be utilized primarily for dust control to cover some of the work areas closet to the neighboring residences during excavation activities, which would help reduce noise levels during excavation. Without implementation of SCAs, the Project could result in excess construction noise, and thus would result in a potentially significant impact. Changes or alterations, described above, have been incorporated into the Project that substantially lessen these potentially significant impacts as identified in the EIR, so that environmental effects after SCAs are implemented are reduced to a less than significant level. Implementation of SCA-NOI 1: Construction Days/Hours (#62), SCA-NOI-2: Construction Noise (#63), SCA-NOI-3: Extreme Construction Noise (#64), and SCA-NOI-4: Construction Noise Complaints (#66), as set forth in the EIR and SCAMMRP, will reduce this impact to a less-than-significant level. SCA-NOI-1: Construction Days/Hours (#62) includes limits on the days and hours of construction to avoid generating noise when it would be most objectionable to neighboring residences. These limitations, which specify that construction activities would be limited to between 7:00 a.m. and 7:00 p.m. Monday through Friday (among other restrictions), would prevent the disturbance of sleep for a majority of residents located near the Project site. This SCA also requires that if the construction contractor wants to extend these work hours, the request must be approved in advance by the City and requires property owners and occupants within 300 feet of the Project site to be notified of such an extension. SCA-NOI-2: Construction Noise (#63) requires all construction projects to implement basic noise reduction measures during construction. Because the construction of the proposed Project and the San Leandro Street variant could generate noise levels greater than 90 dBA at the nearest receptors, SCA-NOI-3: Extreme Construction Noise (#64) would be triggered and require that the Project applicant prepare and implement a Construction Noise Management Plan that contains site-specific noise attenuation measures to reduce construction impacts associated with extreme noise generating activities as well as notification and communication measures for potentially affected receptors. Implementation of SCA-NOI-4: Construction Noise Complaints (#66) provides additional measures to respond to and track construction noise complaints during construction to allow sources of potentially disruptive construction noise to be quickly controlled or eliminated. Implementation of these SCAs would ensure that this impact is reduced to a less-than-significant level.

33. Project Operation Noise. Operation of the Project could expose nearby receptors to noise associated with the operation of the vapor intrusion mitigation system and the operation of the groundwater extraction and treatment system. Construction of the VIMS would involve installation of a blower system in the southwestern corner of the site to maximize the distance between the blower system and the noise-sensitive receptors to the northwest. The proposed Project and the San Leandro Street variant would relocate the GETS, which is an existing condition, slightly to the west of its existing location along the northwestern site boundary, which would not expose more noise-sensitive receptors to the operation of the system. In addition, the proposed Project and the San Leandro Street variant would be subject to SCA-NOI-6: Operational Noise (#68), as set forth in the EIR and SCAMMRP, that requires all operational noise to comply with the performance standards of chapter 17.120 of the Oakland Planning Code and Section 8.18 of the Oakland Municipal Code. Additionally, since ambient noise levels at the Project site range up to 73 dBA Ldn, the proposed Project and the San Leandro Street variant would be subject to SCA-NOI-5: Exposure to Community Noise (#67), as set forth in the EIR and SCAMMRP, that requires noise reduction to be incorporated into building design based upon the recommendations of a qualified acoustical engineer. Noise reduction measures must reduce interior noise levels to 65 dBA Ldn for

industrial activities. Implementation of these SCAs would ensure that operation period noise impacts are reduced to a less-than-significant level.

34. Groundborne Vibration Disturbance. Project and variant construction could expose persons to or generate groundborne vibration that could cause disturbance to nearby residents and/or damage to off-site buildings. Construction activities can result in varying degrees of groundborne vibration, depending on the equipment, activity, and relative proximity to sensitive receptors. Residential buildings to the northwest of the Project site are located within 63 feet from the Project site boundary, and there is the potential for the use of construction equipment to cause vibration that temporarily disturbs those residential receptors. Additionally, vibration from a large bulldozer (a version larger than a D5 dozer) and a hoe ram (for concrete breaking) could have the potential to damage adjacent residential and commercial buildings if it is operated within 11 feet from the off-site buildings; a loaded heavy truck could cause damage if operated within 10 feet of the buildings; and a small bulldozer could cause damage if operated within 1 foot of the buildings. A drill rig would be used during well destruction and protection at the central part of the Project site, and therefore would not be operated within 11 feet from the off-site buildings. SCA-NOI-1: Construction Days/Hours (#62), as set forth in the EIR and SCAMMRP, limits construction activities to the hours between 7:00 a.m. and 7:00 p.m. Monday through Friday, and limits construction with the potential to generate extreme noise (which is often correlated with the potential to generate high vibration) to the hours between 8:00 a.m. and 4:00 p.m. Therefore, severe vibration would be restricted to normal daytime hours, and would reduce the likelihood of disturbing residents (i.e., through interfering with sleep). Construction of the proposed Project and the San Leandro Street variant would be subject to SCA-NOI-7: Vibration Impacts on Adjacent Structures or Vibration-Sensitive Activities (#70), as set forth in the EIR and SCAMMRP. SCA-NOI-7 requires design means and methods of construction to be included in a Vibration Analysis that shall be utilized in order to not exceed the thresholds. Potential means and methods would include restrictions to reduce potential vibration impacts to the adjacent buildings to the northwest of the Project site along 54th Avenue located between San Leandro Street and International Blvd., including prohibitions on the use of larger bulldozers, loaded trucks and the loudest construction activities (e.g., concrete breaking) within 1 to 11 feet from off-site buildings. Implementation of these SCAs would reduce impacts associated with construction generated vibration to a less-than-significant level.

F. GEOLOGY, SOILS AND SEISMICITY

35. Seismic Hazards and Ground Shaking. The Project is located in a seismically active region and the site and proposed building could experience strong seismic ground shaking during a severe earthquake, and the Geotechnical Investigation indicated that unsaturated granular soil at the Project site could experience about ⅓-inch of total settlement following strong seismic shaking. To reduce potential impacts, the proposed Project is required to comply with SCA-GEO-1: Construction-Related Permit(s) (#37), as set forth in the EIR and SCAMMRP, which requires that the Project be designed and constructed in accordance with the 2016 CBC (Title 24, California Code of Regulations) as amended by the City of Oakland and the Oakland Grading Regulations. The Project must also comply with SCA-GEO-2: Seismic Hazards Zone (Landslide/Liquefaction) (#40), as set forth in the EIR and SCAMMRP, which requires that the Project be designed and constructed in accordance with seismic design recommendations presented in the Geotechnical Investigation. Compliance with these SCAs would ensure that potential impacts associated with seismic and geologic hazards are less than significant.

36. Liquefaction and Unstable Soil Conditions. The Project site is located within a Seismic Hazard Zone for liquefaction, and the Geotechnical Investigation found that several subsurface layers could experience liquefaction that could result in settlement at the ground surface. Additionally, the Geotechnical Investigation found highly expansive soils near the surface of the Project site within the non-engineered fill materials, and that the Project site is generally blanketed with up to 5 to 6 feet of non-engineered fill

material, and deeper non-engineered fill was encountered to a depth of about 18 feet in one area in the southern portion of the Project site. The Geotechnical Investigation indicated that the non-engineered fills are expected to vary in thickness, density, and consistency across the Project site, and indicated that additional areas of deep non-engineered fill are anticipated to be present at the Project site due to past development and environmental remediation of the Project site. To address potential liquefaction, highly expansive soils, and non-engineered fill, the Geotechnical Investigation recommended that (1) foundations be designed to tolerate the anticipated total and differential settlements, and (2) that non-engineered fill be mitigated through removal of non-engineered fill and replacement with compacted engineered fill. The Geotechnical Investigation also included recommendations for localized ground improvement (e.g., stone columns, rammed aggregate, grouted displacement columns, or similar densification techniques) that could be performed if removal of deeper non-engineered fill and replacement with engineered fill is determined not to be feasible, and recommended that additional geotechnical exploration be performed to further evaluate the lateral limits and depth of deeper fill areas and fill quality if ground improvement would be performed. Implementation of SCA-GEO-2: Seismic Hazards Zone (Landslide/Liquefaction) (#40), as set forth in the EIR and SCAMMRP, requires that the Project be designed and constructed in accordance with the recommendations of the Geotechnical Investigation to account for and withstand potential liquefaction, undocumented fill and soil-related impacts. Compliance with this SCA would ensure that potential impacts associated with this impact would be less than significant.

G. HYDROLOGY AND WATER QUALITYT

37. Construction Period Water Quality: The Project would involve construction activities that would disturb over 1 acre of land, and therefore would be required to comply with the Construction General Permit issued by the California State Water Resources Control Board (SWRCB) under Order 2009-0009-DWQ. On-site construction activities subject to the Construction General Permit include clearing, grading, excavation, and stockpiling. The Construction General Permit and SCA-HYD-1: Erosion and Sedimentation Control Plan for Construction (#45) also requires the development of a SWPPP and erosion and sedimentation control plan by a certified Qualified SWPPP Developer, which identifies all potential pollutants and their sources, including erosion, sediments, and construction materials, and includes a list of BMPs to reduce discharges of construction-related stormwater pollutants. Additionally, SCA-HAZ-1: Hazardous Materials Related to Construction (#43), as set forth in the EIR and SCAMMRP, requires preparation and implementation of a SWPPP and BMPs during construction, and SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44) requires a building and site assessment prior to demolition or construction and preparation of a Health and Safety Plan to protect Project construction workers from risks associated with hazardous materials, including potentially contaminated stormwater. In addition, dewatering could be performed during construction of excavations and deep trenches. Dewatering effluent could have high turbidity and could contain contaminants. Turbid/contaminated groundwater could cause degradation of the receiving water quality if discharged directly to storm drains without treatment. Any groundwater dewatering would be limited in duration, and the discharge of dewatering effluent would be subject to permits from the East Bay Municipal Utility District (if discharged to the sanitary sewer system) or the Regional Water Quality Control Board (if discharged to the storm sewer system). Additionally, the RDIP Addendum, overseen by USEPA and DTSC, includes measures and requirements to reduce water quality impacts associated with Project construction including, a Preliminary Erosion Control Plan (Appendix D of the RDIP Addendum) that requires implementation of erosion and sedimentation control measures (e.g., a stabilized construction entrance, placement of straw wattles around the perimeter of the Project site and adjacent to existing curb inlets, placement of sand bags and inlet filter fabric at existing curb inlets, and placement of rock filter bags around storm drain drop inlets that would be installed at the Project site). A preliminary Construction SWPPP is also included in Appendix M of the RDIP Addendum and indicates that during demolition and remediation excavation activities, the existing sediment control measures (fiber rolls and

filter fabric for upstream storm drain inlet protection, stormwater treatment via bag filters at manhole MH-23, and inlet filtration at manhole MH-24) must remain in place. The SWPPP identifies additional BMPs to address sediment in stormwater runoff and include protection of off-site storm drain inlets with gravel bag berms or wattles filled with granular activated carbon and filter fabric, decontamination of truck tires as outlined in the Decontamination Plan (Appendix I of the RDIP Addendum), dust control measures including street sweeping as outlined in the Dust Control Plan (Appendix J of the RDIP Addendum), and additional BMPs such as silt fences, straw bales, sand dikes, and sediment traps, if necessary, based on inspection and evaluation of the Project site's BMPs and stormwater monitoring. The Project applicant must also submit Construction General Permit requirements to the City, in accordance with SCA-HYD-2: State Construction General Permit (# 46), as set forth in the EIR and SCAMMRP. The Project site is impacted by PCBs in building materials and soil and PCB-impacted sediment has been identified in storm drains in and near the Project site in the past. The SWPPP (Appendix M) indicates that sampling and analysis or PCBs of stormwater discharges would be performed during construction in accordance with the requirements of the Construction General Permit and USEPA while the storm drain leading off-site (downstream of manhole MH-24) is not plugged. Sampling for PCBs would be performed as part of the non-visible pollutant monitoring program required by the Construction General Permit, and would be conducted at manhole MH-24 during qualifying storm events. In addition, pH monitoring of stormwater runoff may also be conducted if there are sources of pH altering materials present at the Project site (i.e., crushed concrete). Non-visible pollutant monitoring in stormwater would continue until the storm drain line is plugged and there is no off-site discharge. The concrete curb around the perimeter of the Project site would be left in place after the cap is removed and the storm drain at manhole MH-24 is plugged to prevent stormwater from leaving the Project site, and stormwater would infiltrate into the ground or be removed if needed. If stormwater would need to be removed, it would be stored in above ground tanks, tested, and treated, if needed, prior to on-site re-use for dust control or discharge to the sanitary sewer or storm drain. USEPA would be notified within 3 days if PCBs are detected in stormwater runoff or storm drain sediment samples. The SWPPP also indicates that the condition of the 24-inch diameter storm drain located immediately downstream of manhole MH-24 would be inspected and documented with video inspection equipment prior to the start of construction and immediately after the completion of cap installation at the Project site to evaluate potential sediment accumulation. If PCBs are detected during stormwater or sediment sampling, and if post redevelopment inspection of the 24-inch diameter storm drain shows that sediment is present, USEPA would require the applicant to perform cleaning of the 24-inch diameter storm drain. Compliance with USEPA requirements, existing State regulations, RDIP Addendum measures, SCA-HYD-1: Erosion and Sedimentation Control Plan for Construction (#45), SCA-HYD-2: State Construction General Permit (# 46), SCA-HAZ-1: Hazardous Materials Related to Construction (#43), and SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44) would ensure that potential impacts to water quality during construction of the Project or variant would be less than significant.

38. Operation Period Water Quality. During the operational phase of the proposed project, pollutants associated with vehicles (e.g., fuel, oil/lubricants, brake dust, and fallout from exhaust) would be deposited on the surface of parking areas and driveways which would contribute petroleum hydrocarbons, heavy metals, and sediment to the pollutant load in runoff being transported to receiving waters. Debris and particulates that gather on impervious surfaces such roofs can also add heavy metals and sediment to the pollutant load in runoff. In addition, landscape maintenance activities may involve the use of chemicals such as pesticides/herbicides and fertilizers which could also impact the quality of stormwater runoff. Additionally, stormwater from the Project site that infiltrates into the ground could mobilize remaining contaminants in the soil. Because the proposed Project or variant would create over 10,000 square feet of new impervious surfaces, the proposed Project would be required to comply with Provision C.3 of the San Francisco Bay Region Municipal Regional Stormwater National Pollutant Discharge Elimination System (NPDES) Permit, Order No. R2-2015-0049, NPDES Permit No. CAS612008, adopted October 14, 2009 (MRP). Additionally, because the proposed Project would include alteration of

over 50 percent of the impervious surface of the previously existing development that was not subject to Provision C.3 of the MRP, stormwater treatment systems at the Project site must be designed and sized to treat stormwater runoff from the entire site. Under the MRP, the preparation of a Stormwater Control Plan would be required for the proposed Project which would present the design elements and implementation measures that would be used to meet MRP requirements. The proposed Project or variant would be required to comply with SCA-HYD-3: NPDES C.3 Stormwater Requirements for Regulated Projects (#50), as set forth in the EIR and SCAMMRP, which requires compliance with provision C.3 of the MRP, and the preparation and implementation of a Post-Construction Stormwater Management Plan, which would include and identify stormwater control and treatment systems. Compliance with SCA-HYD-3: NPDES C.3 Stormwater Requirements for Regulated Projects (#50) also requires the Project applicant to enter into a maintenance agreement with the City, to ensure adequate installation/construction, operation, maintenance, inspection, and reporting of any on-site stormwater treatment measures. Additionally, the RDIP Addendum, overseen by USEPA and DTSC, includes measures and requirements to reduce water quality impacts associated with Project operation including compliance with the Preliminary Erosion Control Plan (Appendix D of the RDIP Addendum) and preparation of an operation-period industrial facility SWPPP for inclusion in the Operations, Maintenance, and Monitoring Plan Addendum to document the stormwater controls on the Project site and their long-term maintenance. The SWPPP would also describe wet season stormwater and sediment sampling requirements for PCBs requested by USEPA. Compliance with SCA-HYD-3, the MRP, RDIP Addendum measures and USEPA requirements for monitoring of PCBs in stormwater runoff would ensure that this impact is reduced to a less-than-significant level.

X. SIGNIFICANT AND UNAVOIDABLE IMPACTS

39. Under Public Resources Code sections 21081(a)(3) and 21081(b), and CEQA Guidelines sections 15091, 15092, and 15093, and to the extent reflected in the EIR and the SCAMMRP, the Planning Commission finds that the following impacts of the Project remain significant and unavoidable, notwithstanding the imposition of all feasible Standard Conditions of Approval and mitigation measures:

40. Historic Resources: As discussed in the EIR (DEIR Section 4.1), the proposed Project would result in two significant and unavoidable historic resources impacts:

> Impact CULT-1: Demolition of buildings on the project site would adversely affect two historical buildings and an Area of Primary Importance that qualify as historical resources under CEQA. Implementation of Mitigation Measures CULT-1a through CULT-1d, as set forth in the EIR and SCAMMRP, will reduce this significant project impact, but would not avoid or substantially lessen this impact to a less than significant level. Mitigation Measures CULT-1a through CULT-1d is summarized as follows: CULT-1a requires that prior to demolition of Building #1 and Building #2, the project applicant would undertake documentation of the subject buildings to be done according to Historic American Building Survey/Historic American Engineering Record guidelines and prepare a historical context report; CULT-1b requires the project applicant to contribute $684,000 to the City of Oakland's Façade Improvement Program; CULT-1c requires the project sponsor to prepare and install a commemorative marker at the project site; and CULT-1c requires the project sponsor to prepare a historic property treatment plan for the retained portion of Building #1. No other feasible mitigation measures have been identified that would avoid or substantially lessen this impact to a less than significant level. Accordingly, even with implementation of Mitigation Measures CULT-1a through CULT-1d, Impact CULT-1 would remain a significant unavoidable impact.

Impact CULT-2: Demolition of buildings on the project site would adversely affect two historical buildings and an Area of Primary Importance that qualify as historical resources under CEQA and would contribute to a significant cumulative impact to historical resources in Oakland. As described for Impact CULT-1, implementation of Mitigation Measures CULT-1a through CULT-1d, as set forth in the EIR and SCAMMRP, will reduce this significant project impact, but would not avoid or substantially lessen this impact to a less than significant level. No other feasible mitigation measures have been identified that would avoid or substantially lessen this impact to a less than significant level. Accordingly, even with implementation of Mitigation Measures CULT-1a through CULT-1d, cumulative Impact CULT-2 would remain a significant unavoidable impact.

41. Greenhouse Gas Emissions: As discussed in the EIR (DEIR Section 4.5), the proposed Project would result in three significant and unavoidable greenhouse gas emissions impacts:

Impact GHG-1: Project construction and operation would generate GHG emissions that would exceed the City's target threshold and result in a significant and unavoidable impact. Implementation of Mitigation Measure GHG-1, as set forth in the EIR and SCAMMRP, will reduce this significant project impact, but would not avoid or substantially lessen this impact to a less than significant level. Mitigation Measure GHG-1 is summarized as follows: The project sponsor is required to prepare a GHG Reduction Plan that includes a detailed GHG emissions inventory, ongoing monitoring and reporting requirements, and reduction measures during construction and operation. Implementation of Mitigation Measure GHG-1, as set forth in the EIR and SCAMMRP, will reduce this significant project impact, but would not avoid or substantially lessen this impact to a less than significant level. No other feasible mitigation measures have been identified that would avoid or substantially lessen this impact to a less than significant level. Accordingly, even with implementation of Mitigation Measure GHG-1, Impact GHG-1 would remain a significant unavoidable impact.

Impact GHG-2: Project operations could conflict with applicable GHG plans, policies, or regulations. As described for Impact GHG-1, implementation of Mitigation Measure GHG-1, as set forth in the EIR and SCAMMRP, will reduce this significant project impact, but would not avoid or substantially lessen this impact to a less than significant level. No other feasible mitigation measures have been identified that would avoid or substantially lessen this impact to a less than significant level. Accordingly, even with implementation of Mitigation Measure GHG-1, Impact GHG-1 would remain a significant unavoidable impact.

Impact GHG-3: Project construction and operation would generate GHG emissions and would contribute to a significant and unavoidable cumulative impact. In addition to implementation of Mitigation Measure GHG-1, as set forth in the EIR and SCAMMRP, the DEIR also requires the project sponsor to comply with SCA-GHG-1 that requires compliance with green building standards; and SCA-AIR-5 that requires implementation of truck-related risk reduction measures. However, implementation of Mitigation Measure GHG-1, SCA-GHG-1 and SCA-AIR-5, would not avoid or substantially lessen this impact to a less than significant level. No other feasible mitigation measures have been identified that would avoid or substantially lessen this impact to a less than significant level. Accordingly, even with implementation of Mitigation Measure GHG-1, SCA-GHG-1 and SCA-AIR-5 cumulative Impact GHG-3 would remain a significant unavoidable impact.

XI. FINDINGS REGARDING ALTERNATIVES

42. The Planning Commission finds that specific economic, social, environmental, technological, legal or other considerations make infeasible the alternatives to the Project described in the EIR for the reasons stated below, and that despite the remaining significant unavoidable impacts, the Project should nevertheless be approved, as more fully set forth in Section XII below, Statement of Overriding Considerations.

43. The EIR evaluated a reasonable range of alternatives to the Project that were described in the EIR (DEIR Chapter 5.0) which are hereby incorporated by reference. The four alternatives analyzed in detail in the EIR represent a reasonable range of potentially feasible alternatives that reduce one or more significant impacts of the Project and/or provide decision makers with additional information about Project alternatives that would include preservation of Building #1, with an assigned a property rating of "A1+" and listing on the California Register of Historical Resources, and Building #2, with a rating of "Dc1+". Both buildings are contributing elements to the 57th Avenue Industrial District Area of Primary Importance (API), and are therefore CEQA historic resources. The Project alternatives include: (a) No Project Alternative, (b) Approved Remedy Alternative (assumes implementation of the DTSC and USEPA approved 2011 remedial action plan risk-based clean up that involves demolition of all the buildings on the site, installation of an asphalt overlay around the building locations and over slabs that would remain on the site, and no reuse of the site), (c) No Reuse Alternative (assumes under Variant A that all of Building #1 and Building #2 would be protected in place, but would remain vacant due to the hazardous materials on the site and in the building materials. Under Variant B, only Building #1 would be protected in place and Building #2 would be demolished and the pad capped with asphalt), and (d) Preservation and Reuse Alternative (assumes remediation of the site for future industrial use per the RDIP Addendum, and remediation and rehabilitation of both Building #1 and Building #2 for industrial use). The EIR also identified an environmentally superior alternative that was considered to have the least number of environmental impacts if implemented. Variant A of the No Reuse Alternative was identified as the environmentally superior alternative.

44. The Planning Commission certifies that it has independently reviewed and considered the information on the alternatives provided in the EIR and in the record. The EIR reflects the Planning Commission's independent judgment as to alternatives. The Planning Commission finds that the Project provides the best balance between the Project sponsor's objectives, the City's goals and objectives, and the Project's benefits as described in the Staff Report and in the Statement of Overriding Considerations below. While the Project may cause some significant and unavoidable environmental impacts, mitigation measures and the City's SCAs identified in the EIR mitigate these impacts to the extent feasible. The alternatives and environmentally superior alternative proposed and evaluated in the EIR are rejected for the following reasons. Each individual reason presented below constitutes a separate and independent basis to reject the Project alternative as being infeasible, and, when the reasons are viewed collectively, provide an overall basis for rejecting the alternative as being infeasible.

45. No Project Alternative: The No Project Alternative assumes that the buildings on the site would remain in their current condition and would not be subject to demolition. Neither building would be remediated, restored or brought up to current building codes, would remain vacant and would continue to deteriorate. Under this alternative, The No Project Alternative would comply with all the requirements of the Cleanup and Abatement Order (CAO No. 80-011) for the Project site that requires the abatement of PCB discharges, as overseen by DTSC and USEPA. This Alternative may result in significant impacts related to hazardous materials and their entrainment and release into the environment through dust or runoff. This Alternative is rejected as infeasible because (a) it would not accomplish any of the objectives for the Project; (b) it would not remove hazardous materials at the site or in the buildings; (c) it would not return the site to productive use or provide new active uses along that portion of International Blvd.; (d) it would not comply with the City's building codes and Declaration of Public Nuisance; (e) it would not comply with the regulatory agencies (i.e., DTSC, USEPA, BAAQMD) requirements and conditions; and

(f) it would not remediate and conserve the bulkhead portion of Building #1, that is the two-story International Blvd.-facing façade and office portion that is an example of early-20th century utilitarian Classical Revival inspired industrial architecture built in 1922, and is the northernmost contributing building in the API and provides it's visual anchor, and the bulkhead portion of Building #1 would continue to deteriorate.

46. Approved Remedy Alternative: The Approved Remedy Alternative assumes implementation of the DTSC and USEPA approved 2011 RAP risk-based clean up that involves demolition of all the buildings on the site, installation of an asphalt overlay around the building locations and over slabs that would remain on the site, and no reuse of the site. The Approved Remedy alternative also would comply with the requirements of the Cleanup and Abatement Order (CAO No. 80-011) for the Project site that requires the abatement of PCB discharges, as overseen by DTSC and USEPA. City-required SCAs would also apply to this alternative. This Alternative would result in significant unavoidable impacts related to cultural resources due to the demolition of Buildings #1 and #2 and effect on the 57th Avenue Industrial District API. The Approved Remedy Alternative is rejected because it would not achieve the Project objectives to the same extent as the proposed Project. This alternative would result in a vacant, asphalt covered site and would not meet the basic objective of returning the site to productive use as an employment center. This alternative would not contribute a well-designed building and active uses for the portion of International Blvd. fronting the site and may contribute to blight. While the requirements of the regulatory agencies would be met, this Alternative would not include remediation activities and measures to protect human health and the environment to the same extent as proposed for the Project through mitigation measures and defined in the RDIP Addendum. This alternative also would not remediate and conserve any portion of Building #1.

47. No Reuse Alternative: The No Reuse Alternative assumes under Variant A that all of Building #1 and Building #2 would be protected in place, but would remain vacant due to the hazardous materials on the site and in the building materials. Under Variant B, only Building #1 would be protected in place and Building #2 would be demolished, the pad capped with asphalt and the building and site would remain vacant. Hazardous materials such as PCBs, lead, and asbestos would remain in Buildings #1 and #2. The No Reuse alternative would be consistent with the DTSC and USEPA approved 2011 RAP risk-based clean up, the DTSC approved 2013 RDIP for the Project site and the USEPA conditionally approved Toxic Substances Control Act (TSCA) Application for Risk Based PCB Cleanup. The No Reuse alternative also would comply with all the requirements of the Cleanup and Abatement Order (CAO No. 80-011) for the Project site that requires the abatement of PCB discharges, as overseen by DTSC and USEPA. Additional plans and protective measures may be required to protect worker health and safety during building repair. Under Variant B (Building #2 demolished), this alternative would result in a direct significant and unavoidable impact to a historic resource and the API. The No Reuse Alternative is rejected because it would not achieve the Project objectives to the same extent as the proposed Project. Under either variant, this alternative would result in a vacant, mostly asphalt covered site, and the alternative would not meet the basic objective of returning the site to productive use as an employment center. Under either variant, Building #1 would be protected in place, but would remain vacant and would not assist in activating that portion of International Blvd. While the requirements of the regulatory agencies would be met, this Alternative would not include remediation activities and measures to protect human health and the environment to the same extent as proposed for the Project through mitigation measures and defined in the RDIP Addendum.

48. Preservation and Reuse Alternative: The Preservation and Reuse Alternative assumes remediation of the site for future industrial use per the RDIP Addendum, and remediation and rehabilitation of both Building #1 and Building #2 for industrial use in conformance with the Secretary of the Interior's Standards for the Treatment of Historic Properties, requirements of the City of Oakland, and USEPA and DTSC requirements. All other structures on the site would be demolished under this alternative, and after

remediation, the remainder of the site would be developed with buildings or a building to support industrial uses. The Preservation and Reuse Alternative could result in significant unavoidable impacts related to greenhouse gas emissions and significant impacts related to hazardous materials and seismic hazards. This Alternative is rejected as infeasible because (a) due to the high levels of PCB and lead contamination in the building materials and the size of the buildings (Building #1 is 75,200 square feet and Building #2 is 45,200 square feet), it is unknown whether the buildings could ever be remediated and risks to public health and safety reduced sufficiently for reuse; (b) due to the size and dilapidation of the building, the cost and technical feasibility is unknown in regards to whether the buildings could be structurally improved to meet current building codes and allow reuse, and (c) the extremely high cost of additional investigation of PCB contamination and determination of TSCA compliance options approved by DTSC, USEPA and the City in addition to the cost of remediation and seismic stability structural reinforcement, if it technically could be done, related to the potential return from use of the buildings renders the Project economically infeasible. Additionally, this alternative would not maximize the site's development potential and would have a less robust economic impact on the City as it would generate lower new revenue streams in terms of property taxes, job creation, gross receipts taxes, and new office worker population that would activate and support International Blvd. businesses.

49. Environmentally Superior Alternative: The EIR identified an environmentally superior alternative that was considered to have the least number of environmental impacts if implemented. Variant A of the No Reuse Alternative was identified as the environmentally superior alternative as it would preserve the entirety of Building #1 and Building #2, unlike the Project that only preserves the bulkhead portion of Building #1. As explained in the DEIR, Variant A of the No Reuse Alternative would have a less-than-significant impact on the two buildings that are CEQA cultural resources (Buildings #1 and #2), and on the 57th Avenue Industrial District Area of Primary Importance, to which they contribute. By contrast, the Project would have a significant and unavoidable impact on the historic resources and the API and would result in a significant and unavoidable cumulative impact. Variant A of the No Reuse Alternative would also avoid the Project's significant and unavoidable cumulative impact related to greenhouse gas emissions, as the buildings would remain vacant under this alternative. As stated above, Variant A of the No Reuse Alternative is rejected because it would not achieve the Project objectives to the same extent as the proposed Project. Under Variant A, this alternative would result in a vacant, mostly asphalt covered site, and the alternative would not meet the basic objective of returning the site to productive use as an employment center. Under Variant A, Building #1 would be protected in place, but would remain vacant and would not assist in activating International Blvd. with a productive use.

XII. STATEMENT OF OVERRIDING CONSIDERATIONS

50. This Planning Commission adopts and makes this statement of overriding considerations concerning the Project's significant impacts to explain why the Project's benefits override and outweigh its unavoidable impacts. Having (i) adopted all feasible mitigation measures, (ii) rejected as infeasible alternatives to the proposed Project discussed above, (iii) recognized all significant, unavoidable impacts, and (iv) balanced the benefits of the proposed Project against the proposed Project's significant and unavoidable impacts, the Planning Commission hereby finds that the benefits outweigh and override the significant unavoidable impacts for the reasons stated below.

51. Pursuant to Public Resources Code Section 21081 and CEQA Guidelines sections 15091 et. seq. and after extensive review of the entire administrative record, including the Draft and Final EIR, the staff reports, and the oral and written testimony, and the evidence provided, this Planning Commission finds that the Project's significant unmitigated impacts are outweighed by the Project's overriding benefits. The below stated reasons summarize the benefits, goals, and objectives of the proposed Project and provide the rationale for the benefits of the proposed Project. Each benefit set forth below constitutes an

overriding consideration warranting approval of the Project, independent of the other benefits, despite each and every unavoidable impact.

52. The Project will remediate hazardous conditions at the site and will replace vacant, contaminated, seismically unsafe and dilapidated buildings with a high-quality, 540,000 square foot industrial/warehouse building that remediates, preserves and incorporates the bulkhead portion of Building #1, the two-story International Blvd.-facing façade and office portion, built in 1922, that is an example of early-20th century utilitarian Classical Revival inspired industrial architecture and provides the visual anchor for the northernmost portion of the 57th Street Industrial District API. In addition to conserving the two-story office portion of Building #1, the Project would concentrate active office uses on International Blvd. and provide landscaping and streetscape elements designed to improve and activate the street level environment and implement many of the City-wide General Plan goals, objectives, and policies.

53. The Project conforms with the City of Oakland's General Plan and Zoning designations for the site. The majority of the project site is designated as General Industry and Transportation, which allows manufacturing and distribution uses, and the proposed industrial/warehouse building would be consistent with that designation. The northwestern portion of the site has a zoning designation of IG/S-19 General Industrial/Health and Safety Protection Overlay due to the residential and commercial uses adjacent to the site boundary. In conformance to the IG/S-19 designation, the Project includes substantial site remediation and monitoring measures and a permanent cap on the site to ensure that the public health and safety is protected during construction and operation of the Project. An area of the site within approximately 100 feet from International Blvd. is within the Neighborhood Center Mixed Use General Plan designation and is zoned CN-3, Neighborhood Center Commercial Zone the intent of which is to create, preserve and enhance mixed-use neighborhood commercial centers. By preserving and incorporating the historic two-story office portion of Building #1, locating the office portion of the Project on International Blvd. and providing streetscape improvements, the Project would comply with the CN-3 designation.

54. The Project could add approximately 200 permanent jobs. Employment from the Project would provide job opportunities for Oakland and East Bay residents to work closer to home and avoid/reduce commutes, opportunities for new jobs nearby to advance skills and experience, and opportunities to become employed and gain experience for residents not now employed or seeking a new career. The Project is located in a transit accessible area on International Blvd. with a bus rapid transit (BRT) stop within .25 miles and would therefore allow employees to use transit to access the site thereby decreasing vehicle miles traveled.

55. The Project conforms to the requirements, orders, and oversight of federal, state, regional and local agencies, including but not limited to USEPA, DTSC, San Francisco Bay Regional Water Quality Control Board, and BAAQMD, that allow for protection of the public's health and safety and environment. The Project also meets the requirements of the 1993 deed restriction covenant that restricts the use of the property commercial and industrial uses that preserve the integrity of the cap containment of the hazardous substances in soil, and the Declaration of Public Nuisance – Substandard (Declaration) prepared by the City on May 21, 2010 (Complaint #1001777).

56. The Project will contribute to the City's Facade Improvement Program, which would benefit other businesses in the City, improve and preserve historic facades, and upgrade the urban environment for residents, employees, and visitors in Oakland.

57. The Project will promote sustainability by meeting the contemporary energy and green building objectives of the City and the State by ensuring that the new building meets mandatory performance

standards of CALGreen, LEED Gold requirements and the requirements of the approved greenhouse gas reduction plan.

58. The Project will revitalize and activate a large industrial site on International Blvd. that has been underutilized for approximately 30 years.

ATTACHMENT B

Conditions of Approval

General Administrative Conditions

1. Approved Use

The project shall be constructed and operated in accordance with the authorized use as described in the approved application materials, staff report and the approved plans **dated 4/2/20**, as amended by the following conditions of approval and mitigation measures, if applicable ("Conditions of Approval" or "Conditions").

2. Effective Date, Expiration, Extensions and Extinguishment

This Approval shall become effective immediately, unless the Approval is appealable, in which case the Approval shall become effective in ten (10) calendar days unless an appeal is filed. Unless a different termination date is prescribed, this Approval shall expire **two years** from the Approval date, or from the date of the final decision in the event of an appeal, unless within such period a complete building permit application has been filed with the Bureau of Building and diligently pursued towards completion, or the authorized activities have commenced in the case of a permit not involving construction or alteration. Upon written request and payment of appropriate fees submitted no later than the expiration date of this Approval, the Director of City Planning or designee may grant a one-year extension of this date, with additional extensions subject to approval by the approving body. Expiration of any necessary building permit or other construction-related permit for this project may invalidate this Approval if said Approval has also expired. If litigation is filed challenging this Approval, or its implementation, then the time period stated above for obtaining necessary permits for construction or alteration and/or commencement of authorized activities is automatically extended for the duration of the litigation.

3. Compliance with Other Requirements

The project applicant shall comply with all other applicable federal, state, regional, and local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City's Bureau of Building, Fire Marshal, Department of Transportation, and Public Works Department. Compliance with other applicable requirements may require changes to the approved use and/or plans. These changes shall be processed in accordance with the procedures contained in Condition #4.

4. Minor and Major Changes

a. Minor changes to the approved project, plans, Conditions, facilities, or use may be approved administratively by the Director of City Planning

b. Major changes to the approved project, plans, Conditions, facilities, or use shall be reviewed by the Director of City Planning to determine whether such changes require submittal and approval of a revision to the Approval by the original approving body or a new independent permit/approval. Major revisions shall be reviewed in accordance with the procedures required for the original permit/approval. A new independent permit/approval shall be reviewed in accordance with the procedures required for the new permit/approval.

5. Compliance with Conditions of Approval

a. The project applicant and property owner, including successors, (collectively referred to hereafter as the "project applicant" or "applicant") shall be responsible for compliance with all the Conditions of Approval and any recommendations contained in any submitted and approved technical report at his/her sole cost and expense, subject to review and approval by the City of Oakland.

b. The City of Oakland reserves the right at any time during construction to require certification by a licensed professional at the project applicant's expense that the as-built project conforms to all applicable requirements, including but not limited to, approved maximum heights and minimum setbacks. Failure to construct the project in accordance with the Approval may result in remedial reconstruction, permit revocation, permit modification, stop work, permit suspension, or other corrective action.

c. Violation of any term, Condition, or project description relating to the Approval is unlawful, prohibited, and a violation of the Oakland Municipal Code. The City of Oakland reserves the right to initiate civil and/or criminal enforcement and/or abatement proceedings, or after notice and public hearing, to revoke the Approval or alter these Conditions if it is found that there is violation of any of the Conditions or the provisions of the Planning Code or Municipal Code, or the project operates as or causes a public nuisance. This provision is not intended to, nor does it, limit in any manner whatsoever the ability of the City to take appropriate enforcement actions. The project applicant shall be responsible for paying fees in accordance with the City's Master Fee Schedule for inspections conducted by the City or a City-designated third-party to investigate alleged violations of the Approval or Conditions.

6. Signed Copy of the Approval/Conditions

A copy of the Approval letter and Conditions shall be signed by the project applicant, attached to each set of permit plans submitted to the appropriate City agency for the project, and made available for review at the project job site at all times.

7. Blight/Nuisances

The project site shall be kept in a blight/nuisance-free condition. Any existing blight or nuisance shall be abated within sixty (60) days of approval, unless an earlier date is specified elsewhere.

8. Indemnification

a. To the maximum extent permitted by law, the project applicant shall defend (with counsel acceptable to the City), indemnify, and hold harmless the City of Oakland, the Oakland City Council, the Oakland Redevelopment Successor Agency, the Oakland City Planning Commission, and their respective agents, officers, employees, and volunteers (hereafter collectively called "City") from any liability, damages, claim, judgment, loss (direct or indirect), action, causes of action, or proceeding (including legal costs, attorneys' fees, expert witness or consultant fees, City Attorney or staff time, expenses or costs) (collectively called "Action") against the City to attack, set aside, void or annul this Approval or implementation of this Approval. The City may elect, in its sole discretion, to participate in the defense of said Action and the project applicant shall reimburse the City for its reasonable legal costs and attorneys' fees.

b. Within ten (10) calendar days of the filing of any Action as specified in subsection (a) above, the project applicant shall execute a Joint Defense Letter of Agreement with the City, acceptable to the Office of the City Attorney, which memorializes the above obligations. These obligations and the Joint Defense Letter of Agreement shall survive termination, extinguishment, or invalidation of the Approval. Failure to timely execute the Letter of Agreement does not relieve the project applicant of

any of the obligations contained in this Condition or other requirements or Conditions of Approval that may be imposed by the City.

9. Severability

The Approval would not have been granted but for the applicability and validity of each and every one of the specified Conditions, and if one or more of such Conditions is found to be invalid by a court of competent jurisdiction this Approval would not have been granted without requiring other valid Conditions consistent with achieving the same purpose and intent of such Approval.

10. Special Inspector/Inspections, Independent Technical Review, Project Coordination and Monitoring

The project applicant may be required to cover the full costs of independent third-party technical review and City monitoring and inspection, including without limitation, special inspector(s)/inspection(s) during times of extensive or specialized plan-check review or construction, and inspections of potential violations of the Conditions of Approval. The project applicant shall establish a deposit with Engineering Services and/or the Bureau of Building, if directed by the Director of Public Works, Building Official, Director of City Planning, Director of Transportation, or designee, prior to the issuance of a construction-related permit and on an ongoing as-needed basis.

11. Public Improvements

The project applicant shall obtain all necessary permits/approvals, such as encroachment permits, obstruction permits, curb/gutter/sidewalk permits, and public improvement ("p-job") permits from the City for work in the public right-of-way, including but not limited to, streets, curbs, gutters, sidewalks, utilities, and fire hydrants. Prior to any work in the public right-of-way, the applicant shall submit plans for review and approval by the Bureau of Planning, the Bureau of Building, Engineering Services, Department of Transportation, and other City departments as required. Public improvements shall be designed and installed to the satisfaction of the City.

12. Compliance Matrix

The project applicant shall submit a Compliance Matrix, in both written and electronic form, for review and approval by the Bureau of Planning and the Bureau of Building that lists each Condition of Approval (including each mitigation measure if applicable) in a sortable spreadsheet. The Compliance Matrix shall contain, at a minimum, each required Condition of Approval, when compliance with the Condition is required, and the status of compliance with each Condition. For multi-phased projects, the Compliance Matrix shall indicate which Condition applies to each phase. The project applicant shall submit the initial Compliance Matrix prior to the issuance of the first construction-related permit and shall submit an updated matrix upon request by the City.

13. Construction Management Plan

Prior to the issuance of the first construction-related permit, the project applicant and his/her general contractor shall submit a Construction Management Plan (CMP) for review and approval by the Bureau of Planning, Bureau of Building, and other relevant City departments such as the Fire Department, Department of Transportation, and the Public Works Department as directed. The CMP shall contain measures to minimize potential construction impacts including measures to comply with all construction-related Conditions of Approval (and mitigation measures if applicable) such as dust control, construction emissions, hazardous materials, construction days/hours, construction traffic control, waste reduction and recycling, stormwater pollution prevention, noise control, complaint management, and cultural resource management (see applicable Conditions below). The CMP shall provide project-specific information including descriptive procedures, approval documentation, and drawings (such as a site logistics plan, fire safety plan, construction phasing plan, proposed truck routes, traffic control plan, complaint management

plan, construction worker parking plan, and litter/debris clean-up plan) that specify how potential construction impacts will be minimized and how each construction-related requirement will be satisfied throughout construction of the project.

14. Standard Conditions of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP)

a. All mitigation measures identified in the **GE Site Remediation and Redevelopment Project EIR** are included in the Standard Condition of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP) which is included in these Conditions of Approval and are incorporated herein by reference, as **Attachment C**, as Conditions of Approval of the project. The Standard Conditions of Approval identified in the **GE Site Remediation and Redevelopment Project EIR** are also included in the SCAMMRP, and are, therefore, incorporated into these Conditions by reference but are not repeated in these Conditions. To the extent that there is any inconsistency between the SCAMMRP and these Conditions, the more restrictive Conditions shall govern. In the event a Standard Condition of Approval or mitigation measure recommended in the **GE Site Remediation and Redevelopment Project EIR** has been inadvertently omitted from the SCAMMRP, that Standard Condition of Approval or mitigation measure is adopted and incorporated from the **GE Site Remediation and Redevelopment Project EIR** into the SCAMMRP by reference, and adopted as a Condition of Approval. The project applicant and property owner shall be responsible for compliance with the requirements of any submitted and approved technical reports, all applicable mitigation measures adopted, and with all Conditions of Approval set forth herein at his/her sole cost and expense, unless otherwise expressly provided in a specific mitigation measure or Condition of Approval, and subject to the review and approval by the City of Oakland. The SCAMMRP identifies the timeframe and responsible party for implementation and monitoring for each Standard Condition of Approval and mitigation measure. Unless otherwise specified, monitoring of compliance with the Standard Conditions of Approval and mitigation measures will be the responsibility of the Bureau of Planning, with overall authority concerning compliance residing with the Environmental Review Officer. Adoption of the SCAMMRP will constitute fulfillment of the CEQA monitoring and/or reporting requirement set forth in section 21081.6 of CEQA.

b. Prior to the issuance of the first construction-related permit, the project applicant shall pay the applicable mitigation and monitoring fee to the City in accordance with the City's Master Fee Schedule.

Other Standard Conditions

15. Employee Rights

Requirement: The project applicant and business owners in the project shall comply with all state and federal laws regarding employees' right to organize and bargain collectively with employers and shall comply with the City of Oakland Minimum Wage Ordinance (chapter 5.92 of the Oakland Municipal Code).

When Required: Ongoing

Initial Approval: N/A

Monitoring/Inspection: N/A

16. Public Art for Private Development

Requirement: The project is subject to the City's Public Art Requirements for Private Development, adopted by Ordinance No. 13275 C.M.S. ("Ordinance"). The public art

contribution requirements are equivalent to one-half percent (0.5%) for the "residential" building development costs, and one percent (1.0%) for the "non-residential" building development costs.

The contribution requirement can be met through: 1) the installation of freely accessible art at the site; 2) the installation of freely accessible art within one-quarter mile of the site; or 3) satisfaction of alternative compliance methods described in the Ordinance, including, but not limited to, payment of an in-lieu fee contribution. The applicant shall provide proof of full payment of the in-lieu contribution and/or provide plans, for review and approval by the Planning Director, showing the installation or improvements required by the Ordinance prior to issuance of a building permit.

Proof of installation of artwork, or other alternative requirement, is required prior to the City's issuance of a final certificate of occupancy for each phase of a project unless a separate, legal binding instrument is executed ensuring compliance within a timely manner subject to City approval.

When Required: Payment of in-lieu fees and/or plans showing fulfillment of public art requirement – Prior to Issuance of Building permit

Installation of art/cultural space – Prior to Issuance of a Certificate of Occupancy.

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

Project Specific Conditions

17. Exterior Finishes/ Final Design Details

Requirement: The final building permit plan set shall contain detailed information on all proposed exterior finishes and elevations for approval by the Director of Planning. If requested, sample materials shall be provided and/or materials mock ups constructed on-site.

When Required: Prior to issuance of a Building Permit
Initial Approval: Bureau of Planning
Monitoring/Inspection: Bureau of Planning

18. Transportation Improvement Measures

The following improvements shall be submitted as part of a p-job application for review and approval by the Department of Transportation (DOT). If approved they shall be implemented.

Requirement #1: Explore the feasibility and if determined feasible by City of Oakland staff, consider implementing or contributing to the Class 3B Bicycle Boulevard Neighborhood Bike Route proposed along 54th Avenue between San Leandro Street and International Boulevard.

Requirement #2: Explore the feasibility and if determined feasible by City of Oakland staff, consider implementing or contributing to the segment of the East Bay Greenway Class I path proposed just west of the project adjacent to San Leandro Street.

Requirement #3: If the existing railroad tracks adjacent to the west of the project are abandoned, consider providing direct pedestrian/bicycle connection between the project site and the proposed East Bay Greenway.

Requirement #4: Upgrade the pedestrian amenities on International Boulevard adjacent to the project, including the installation of amenities such as lighting; pedestrian-oriented green infrastructure, trees, or other greening landscape; and trash receptacles per the Pedestrian Master Plan and any applicable streetscape plan.

When Required: Prior to issuance of a building permit

Initial Approval: Bureau of Planning / DOT

Monitoring/Inspection: N/A

ATTACHMENT C

STANDARD CONDITIONS OF APPROVAL AND MITIGATION MONITORING AND REPORTING PROGRAM

This Standard Conditions of Approval and Mitigation Monitoring and Reporting Program (SCAMMRP) was formulated based on the findings of the Final Environmental Impact Report (EIR) prepared for the General Electric (GE) Site Remediation and Redevelopment Project at 5441 International Boulevard in the City of Oakland, California (proposed project, Case Numbers PLN19-076/ER18 013). This SCAMMRP complies with Section 15097 of the CEQA Guidelines, which requires that the Lead Agency "adopt a program for monitoring or reporting on the revisions which it has required in the project and the measures it has imposed to mitigate or avoid significant environmental effects." The SCAMMRP lists Standard Conditions of Approval (SCAs) and mitigation measures recommended in the EIR and identifies mitigation monitoring requirements.

The SCAMMRP table below presents the SCAs and mitigation measures identified in the GE Site Remediation and Redevelopment Project EIR necessary to mitigate potentially significant impacts. Each mitigation measure is numbered according to the topical section to which it pertains in the EIR. As an example, Mitigation Measure CULT-1a is the first mitigation measure identified in the EIR for the GE Site Remediation and Redevelopment Project in Section 4.1, Cultural Resources and Tribal Cultural Resources. The SCAs are numbered consistent with the City's SCAs in place at the time of preparation of the Draft EIR.

The first column of the SCAMMRP table identifies the Mitigation Measure or SCA. The second column identifies implementation action and responsibility, the third column identifies the monitoring schedule or timing, and the fourth column names the party responsible for monitoring and the required monitoring action. The fifth column provides a place to record compliance with monitor dates and initials. This last column will be used by the City to ensure that individual mitigation measures are monitored.

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
4.1 Cultural Resources and Tribal Cultural Resources				
Mitigation Measure CULT-1a: Historical Context Report. Prior to approval of demolition and construction-related permits, the project applicant shall retain a qualified cultural resources consultant to prepare a historical context report and photo-documentation of the historic buildings on the project site and the 57th Avenue Industrial District API. The report shall include a description of the resources' historical significance within the context of Oakland's historical industrial development during the early-20th century as well their historical architectural significance within the context of utilitarian, unreinforced masonry buildings in Oakland. The report shall also include a discussion of the project site's historical association with the former KGO radio station. Oral histories of those who worked at the GE plant, or those who otherwise have knowledge of the project site's history, shall be sought out and, if located, findings incorporated into the historical context report, as appropriate. Recordings of the oral histories that result from this mitigation shall also be made available to the public by the City or a local historical archive as a digital file (e.g., mp3). Photo-documentation of the project site buildings and the API shall be included in the report to provide additional descriptive data and a permanent visual record of the resources. The photo-documentation shall be done according to Historic American Building Survey/Historic American Engineering Record (HABS/HAER) guidelines. Based on the curation requirements of the receiving institution, either hard copies and/or electronic copies of the report and photo-documentation shall be offered to the Oakland Heritage Alliance, the Oakland Cultural Heritage Survey, the Oakland Public Library, the Environmental Design Library, Archives, and Visual Resource Center	Project sponsor: □ Prepare the historical context report. □ Make report available to entities identified in Mitigation Measure CULT-1a.	□ Prior to the Issuance of a demolition permit for Buildings #1 and #2.	City of Oakland, Planning and Zoning Division: □ Review and approve the historical context report that includes requirements identified by Mitigation Measure CULT-1a. □ Confirm receipt of HABS documentation and that the report and photo-documentation is available to the general public.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
at the University of California, Berkeley, and the Northwest Information Center. The applicant shall also be responsible for ensuring that the report and photo-documentation are available to the general public via the internet.				
Mitigation Measure CULT-1b: Contribution to Façade Improvement Program. Prior to approval of demolition permits, the project applicant shall contribute to the City's Façade Improvement Program. Funds collected should be reserved for buildings within the 57th Avenue Industrial District API for a period of two years. The use of Façade Improvement Program funds for use in the API is appropriate given the location, visibility and contribution of Building #1 and Building #2 within the 57th Avenue Industrial District API. By directing that the funds be used in the 57th Avenue Industrial District API, the mitigation will have a direct effect on the remaining historic resources in the District, including the remaining portion of Building #1, as well as the District itself. The mitigation measure is devised to reflect this and provide more specificity regarding the process for use of the funds. In accordance with the City's Façade Improvement Program, the amount of the contribution required to be paid by the project applicant under this mitigation measure (based upon the calculation for obtaining façade improvement grants) shall be based on the following: $10,000 for the first 25 feet of two façades of each building and $2,500 per 10 additional linear feet of the same two façades beyond the first 25 feet. There shall be a 20 percent increase added for each building designated as a Historical Resource under CEQA.	Project sponsor: □ Make contribution to the façade improvement program.	□ Payable upon issuance of the first demolition permit for the project.	City of Oakland, Planning and Zoning Division: □ Verify proof of payment to Oakland Cultural Heritage Survey for implementation of funds for façade improvement program. □ Review and approve façade improvement program based on its ability to enhance, promote and preserve the integrity of the historic district.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
The total for each building shall be multiplied by 2 for being located within an API. For purposes of this mitigation, the length of the main, International Boulevard-facing façade of Building #1 is 135 feet minus 129 feet, which is the length to be retained, for a total of 6 feet. The length of the secondary, southeast-facing façade of Building #1 is 585 feet minus 33 feet, which is the portion to be retained, for a total of 552 feet. As 25 feet from two facades will not be removed by the project, the $10,000 would not apply to the front façade of Building #1; however, it would apply to the removal of 552 feet of the southeast-facing façade. For Building #2, the length of the main, International Boulevard-facing façade is 110 feet, and the length of the secondary, southeast-facing façade is 450 feet. The following calculation results in a total contribution of $684,000. *5441 International Boulevard - Building #1:* Secondary façade: $10,000 + ($2,500 x 552 feet)/10 feet $138,000 *5441 International Boulevard - Building #2:* Main façade: $10,000 + ($2,500 x 85 feet)/10 feet $31,250				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Secondary façade: $10,000 + ($2,500 x 425 feet)/10 feet $116,250 $147,000 *Building #1 total ($138,000) + Building #2 total ($147,500) $285,000* CEQA Historical Resources – increase by 20%: $335,000 x 1.2 $342,000 Located in an API – increase by two times $684,000 The Façade Improvement Program contribution required hereunder shall be payable upon issuance of the first demolition permit for the project. Funds collected under this mitigation shall be designated for the repair or improvement of façades within the historic 57th Avenue Industrial District API for a two-year period. After that time, all remaining funds shall be eligible for citywide Façade Improvement Program expenditures. All rehabilitation efforts or façade improvements under this Program shall be undertaken using the Secretary of the Interior's Standards for the Treatment of Historic Properties. Administration of this Program shall be overseen by OCHS staff.				
Mitigation Measure CULT-1c: Installation of a Commemorative Marker. To reduce the significant and unavoidable impact of the adverse effect on Building #1 and loss of Building #2 and the substantial adverse change in the historic significance of the 57th Avenue Industrial District API, the project applicant shall, prior to the issuance of the demolition permit for the	Project sponsor: □ Install marker or plaque and prepare a spare.	□ Prior to the issuance of a demolition permit for the	City of Oakland, Planning and Zoning Division: □ Confirm installation	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
project, install a commemorative marker or plaque on the project site. The marker or plaque shall be made of high quality, durable, all-weather materials, and describe the history of the project site and the 57th Avenue Industrial District; examples may be taken from the Bay Trail Series concerning historic industrial buildings. The marker or plaque shall be installed on the project site to allow for high public visibility from International Boulevard. The content, materials, and appearance of the commemorative marker or plaque shall be developed by a consultant experienced in urban architectural interpretive displays, and shall be done in consultation with OCHS staff. At the time of installation, the project applicant shall provide the City with a plan that details ongoing review and maintenance of the marker or plaque. Additionally, for use in case of damage to the first marker or plaque, the applicant shall have prepared and stored a spare to be produced at the same time the first marker is manufactured.		project.	of commemorative marker and location of spare marker.	
Mitigation Measure CULT-1d: Preparation of a Historic Property Treatment Plan. The project applicant shall prepare a Historic Property Treatment Plan for the retained portion of Building #1, in coordination with the City and OCHS staff and prior to the issuance of the demolition permit for the project. At a minimum, the Historic Property Treatment Plan shall identify the portion of the building to be preserved, plans for maintaining and protecting that portion of the building during demolition and construction, and rehabilitation plans. The plan shall be prepared with oversight by a Preservation Architect who meets or exceeds the Secretary of the Interior's Professional Qualifications Standards for Architectural History and Historic Architecture (48 CFR 44738-9). The City shall be responsible for ensuring that Mitigation Measures CULT-	Project sponsor: □ Prepare Historic Property Treatment Plan.	□ Prior to the issuance of a demolition permit for the project.	City of Oakland, Planning and Zoning Division: □ Review and approve the historic property treatment plan.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
1a, CULT-1b, CULT-1c and CULT-1d are completed as a condition of the demolition permit. The applicant shall be responsible for funding the mitigation measures identified herein. Implementation of Mitigation Measures CULT-1a, CULT-1b, CULT-1c, and CULT-1d will mitigate the cultural resources impact to a degree, but not to a level that is less than significant and the impact would remain significant and unavoidable.				
Mitigation Measure CULT-2: Implementation of Mitigation Measures CULT-1a, CULT-1b, and CULT-1d will mitigate this cumulative impact to a degree, but not to a level that is less than significant and this cumulative impact would remain significant and unavoidable.	Project sponsor: □ Implement Mitigation Measures CULT-1a, CULT-1b, and CULT-1d identified above.	□ Prior to the issuance of a demolition permit for the project.	City of Oakland, Planning and Zoning Division: □ See monitoring actions for Mitigation Measures CULT-1a, CULT-1b, and CULT-1d identified above.	
SCA-CULT-1: Archaeological and Paleontological Resources – Discovery During Construction (#33) Requirement: Pursuant to CEQA Guidelines section 15064.5(f), in the event that any historic or prehistoric subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and the project applicant shall notify the City and consult with a qualified archaeologist or paleontologist, as applicable, to	Project sponsor: □ In the event that any historic or prehistoric subsurface cultural resources are discovered, ensure all work within 50 feet of the resources is	□ Prior to issuance of the final building permit. □ Ongoing throughout demolition,	City of Oakland, Building Services Division and Planning and Zoning Division – Historic Preservation Staff: □ In the event that any	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
assess the significance of the find. In the case of discovery of paleontological resources, the assessment shall be done in accordance with the Society of Vertebrate Paleontology standards. If any find is determined to be significant, appropriate avoidance measures recommended by the consultant and approved by the City must be followed unless avoidance is determined unnecessary or infeasible by the City. Feasibility of avoidance shall be determined with consideration of factors such as the nature of the find, project design, costs, and other considerations. If avoidance is unnecessary or infeasible, other appropriate measures (e.g., data recovery, excavation) shall be instituted. Work may proceed on other parts of the project site while measures for the cultural resources are implemented. In the event of data recovery of archaeological resources, the project applicant shall submit an Archaeological Research Design and Treatment Plan (ARDTP) prepared by a qualified archaeologist for review and approval by the City. The ARDTP is required to identify how the proposed data recovery program would preserve the significant information the archaeological resource is expected to contain. The ARDTP shall identify the scientific/historic research questions applicable to the expected resource, the data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. The ARDTP shall include the analysis and specify the curation and storage methods. Data recovery, in general, shall be limited to the portions of the archaeological resource that could be impacted by the proposed project. Destructive data recovery methods shall not be applied to portions of the archaeological resources if nondestructive methods are practicable. Because the intent of the ARDTP is to save as much of the archaeological resource as possible, including moving the resource, if	halted and ensure the project applicant and/or City consult with a qualified archaeologist or paleontologist, as applicable, to assess the significance of the find.	grading and construction.	historic or prehistoric subsurface cultural resources are discovered, ensure adherence to measures.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
feasible, preparation and implementation of the ARDTP would reduce the potential adverse impact to less than significant. The project applicant shall implement the ARDTP at his/her expense. In the event of excavation of paleontological resources, the project applicant shall submit an excavation plan prepared by a qualified paleontologist to the City for review and approval. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and/or a report prepared by a qualified paleontologist, as appropriate, according to current professional standards and at the expense of the project applicant. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-CULT-2: Archaeologically Sensitive Areas – Pre-Construction Measures (#34) Requirement: The project applicant shall implement either Provision A (Intensive Pre- Construction Study) or Provision B (Construction ALERT Sheet) concerning archaeological resources. Provision A: Intensive Pre-Construction Study. Prior to approval of construction-related permits, the project applicant shall retain a qualified archaeologist to conduct a site-specific, intensive archaeological resources study for review and approval by the City prior to soil-disturbing activities	Project sponsor: □ Implement either Provision A (Intensive Pre-Construction Study) or Provision B (Construction ALERT Sheet) as described in SCA-CULT-2.	□ Prior to the issuance of a demolition, grading, or building permit. □ Ongoing throughout demolition, grading and	City of Oakland, Building Services Division and Planning and Zoning Division – Historic Preservation Staff: □ Approve the implementation of Provision A by review and approval of the	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
occurring on the project site. The purpose of the site-specific, intensive archaeological resources study is to identify early the potential presence of history-period archaeological resources on the project site. At a minimum, the study shall include: a. Subsurface presence/absence studies of the project site. Field studies may include, but are not limited to, auguring and other common methods used to identify the presence of archaeological resources. b. A report disseminating the results of this research. c. Recommendations for any additional measures that could be necessary to mitigate any adverse impacts to recorded and/or inadvertently discovered cultural resources. If the results of the study indicate a high potential presence of historic-period archaeological resources on the project site, or a potential resource is discovered, the project applicant shall hire a qualified archaeologist to monitor any ground disturbing activities on the project site during construction and prepare an ALERT sheet pursuant to Provision B below that details what could potentially be found at the project site. Archaeological monitoring would include briefing construction personnel about the type of artifacts that may be present (as referenced in the ALERT sheet, required per Provision B below) and the procedures to follow if any artifacts are encountered, field recording and sampling in accordance with the Secretary of Interior's Standards and Guidelines for Archaeological Documentation, notifying the appropriate officials if human remains or cultural resources are discovered, and preparing a report to document negative findings after construction is completed if no archaeological		construction.	Intensive Pre-Construction Study, or □ Approve the implementation of Provision B by review and approval of the Construction ALERT Sheet and the associated measures. □ Monitor during demolition and grading.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
resources are discovered during construction. Provision B: Construction ALERT Sheet. Prior to and during ground disturbing activities, the project applicant shall prepare a construction "ALERT" sheet developed by a qualified archaeologist for review and approval by the City prior to soil-disturbing activities occurring on the project site. The ALERT sheet shall contain, at a minimum, visuals that depict each type of artifact that could be encountered on the project site. Training by the qualified archaeologist shall be provided to the project's prime contractor, any project subcontractor firms (including demolition, excavation, grading, foundation, and pile driving), and utility firms involved in soil- disturbing activities within the project site. The ALERT sheet shall state, in addition to the basic archaeological resource protection measures contained in other standard conditions of approval, all work must stop and the City's Environmental Review Officer contacted in the event of discovery of the following cultural materials: concentrations of shellfish remains; evidence of fire (ashes, charcoal, burnt earth, fire-cracked rocks); concentrations of bones; recognizable Native American artifacts (arrowheads, shell beads, stone mortars [bowls], humanly shaped rock); building foundation remains; trash pits, privies (outhouse holes); floor remains; wells; concentrations of bottles, broken dishes, shoes, buttons, cut animal bones, hardware, household items, barrels, etc.; thick layers of burned building debris (charcoal, nails, fused glass, burned plaster, burned dishes); wood structural remains (building, ship, wharf); clay roof/floor tiles; stone walls or footings; or gravestones. Prior to any soil-disturbing activities, each contractor shall be responsible for ensuring that the ALERT sheet is circulated to all field personnel, including machine operators, field crew, pile drivers, and supervisory				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
personnel. The ALERT sheet shall also be posted in a visible location at the project site. When Required: Prior to approval of construction-related permit; during construction Initial Approval: Bureau of Building; Bureau of Planning Monitoring/Inspection: Bureau of Building				
SCA-CULT-3: Human Remains – Discovery During Construction (#35) Requirement: Pursuant to CEQA Guidelines section 15064.5(e)(1), in the event that human skeletal remains are uncovered at the project site during construction activities, all work shall immediately halt and the project applicant shall notify the City and the Alameda County Coroner. If the County Coroner determines that an investigation of the cause of death is required or that the remains are Native American, all work shall cease within 50 feet of the remains until appropriate arrangements are made. In the event that the remains are Native American, the City shall contact the California Native American Heritage Commission (NAHC), pursuant to subdivision (c) of section 7050.5 of the California Health and Safety Code. If the agencies determine that avoidance is not feasible, then an alternative plan shall be prepared with specific steps and timeframe required to resume construction activities. Monitoring, data recovery, determination of significance, and avoidance measures (if applicable) shall be completed expeditiously and at the expense of the project applicant.	Project sponsor: □ In the event that human skeletal remains are uncovered, ensure that all work is immediately halted and the Alameda County Coroner is contacted to evaluate the remains following the procedures and protocols pursuant to CEQA Guidelines section 15064.5(e)(1).	□ Ongoing throughout demolition, grading and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ In the event that human skeletal remains are uncovered, and the County Coroner determines that the remains are Native American, ensure adherence to the identified measures.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
4.2 Hazards and Hazardous Materials				
Mitigation Measure HAZ-1: Following the completion of grading activities, the entire site shall be temporarily capped as soon as possible using recycled crushed concrete sourced from the project site that has been tested and determined to be available for this use and imported aggregate base material, as needed, to ensure that large areas of exposed soil are not present on the project site for the remainder of project construction. If recycled crushed concrete that is impacted by PCBs is used for construction of the temporary cap, dust monitoring shall be performed until the permanent cap is installed at the project site. In the event that a complaint is made by the public regarding visible dust emissions from the project site after working hours, real time dust monitoring equipment shall operate 24 hours per day and 7 days a week to monitor and log dust levels after normal working hours to evaluate whether excessive dust emissions occur after working hours. The real time air monitoring equipment shall be equipped with a remote telemetry alarm system that can notify the applicant's environmental consultant and contractor at any time if air monitoring trigger levels for dust are being exceeded after normal working hours. In the event that air monitoring trigger levels are exceeded after normal working hours or if a complaint is made by the public regarding visible dust emissions from the project site after working hours, contractor personnel shall arrive at the project site within one hour of the trigger level	Project sponsor: □ Apply a temporary cap as soon as possible after grading. □ Perform dust monitoring until the permanent cap is installed. □ In the event a complaint is made by the public regarding visible or deposited dust emissions, notify the City and conduct real time dust monitoring, testing and cleanup, if necessary, per the identified measures and notification and direction by	□ Ongoing throughout demolition, and grading prior to the installation of the permanent cap.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve demolition and grading plan and schedule in regards to placement of a temporary cap. □ Review and approve the dust monitoring plan. □ Confirm that all applicable measures are being implemented if a complaint is made and per the approved	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
exceedance or public complaint, and shall implement dust control measures. Air monitoring outside of working hours can be discontinued, with approval from DTSC and the City, following demonstration that dust emissions after working hours are being adequately controlled. If a public complaint is made to a site representative regarding visible dust from project site demolition and remediation activities being deposited on surfaces of neighboring properties, DTSC and USEPA shall be notified. The applicant shall be responsible for testing and cleanup of dust on neighboring properties if necessary and as directed by DTSC or USEPA.	DTSC and USEPA.		plan.	
SCA-HAZ-1: Hazardous Materials Related to Construction (#43) Requirement: The project applicant shall ensure that Best Management Practices (BMPs) are implemented by the contractor during construction to minimize potential negative effects on groundwater, soils, and human health. These shall include, at a minimum, the following: a. Follow manufacture's recommendations for use, storage, and disposal of chemical products used in construction; b. Avoid overtopping construction equipment fuel gas tanks; c. During routine maintenance of construction equipment, properly contain and remove grease and oils; d. Properly dispose of discarded containers of fuels and other chemicals; e. Implement lead-safe work practices and comply with all local, regional, state, and federal requirements concerning lead (for more information refer to the Alameda County Lead Poisoning Prevention Program); and f. If soil, groundwater, or other environmental medium with suspected contamination is encountered unexpectedly during construction activities (e.g., identified by odor or visual staining, or if any	Project sponsor: □ Implement Best Management Practices (BMPs) listed in SCA-HAZ-1.	□ Prior to and during demolition, grading, and construction activities.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Verify that construction BMPs are implemented.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
underground storage tanks, abandoned drums or other hazardous materials or wastes are encountered), the project applicant shall cease work in the vicinity of the suspect material, the area shall be secured as necessary, and the applicant shall take all appropriate measures to protect human health and the environment. Appropriate measures shall include notifying the City and applicable regulatory agency(ies) and implementation of the actions described in the City's Standard Conditions of Approval, as necessary, to identify the nature and extent of contamination. Work shall not resume in the area(s) affected until the measures have been implemented under the oversight of the City or regulatory agency, as appropriate. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-HAZ-2: Hazardous Building Materials and Site Contamination (#44) *a. Hazardous Building Materials Assessment* Requirement: The project applicant shall submit a comprehensive assessment report to the Bureau of Building, signed by a qualified environmental professional, documenting the presence or lack thereof of asbestos-containing materials (ACMs), lead-based paint, polychlorinated biphenyls (PCBs), and any other building materials or stored materials classified as hazardous materials by State or federal law. If lead-based paint, ACMs, PCBs, or any other building materials or stored materials classified as hazardous materials are present, the project applicant shall	Project sponsor: □ Submit a comprehensive assessment report and a Phase I Environmental Site Assessment report, and Phase II Environmental Site Assessment report. □ Submit specifications for the stabilization and/or removal of the identified hazardous materials in	□ Prior to issuance of demolition, grading, or building permits. □ Ongoing during demolition, grading and construction	City of Oakland, Building Services Division and Planning and Zoning Division and Oakland Fire Department: □ Review and approve comprehensive assessment report, Phase 1 and Phase 2 Environmental Site	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
submit specifications prepared and signed by a qualified environmental professional, for the stabilization and/or removal of the identified hazardous materials in accordance with all applicable laws and regulations. The project applicant shall implement the approved recommendations and submit to the City evidence of approval for any proposed remedial action and required clearances by the applicable local, state, or federal regulatory agency. When Required: Prior to approval of demolition, grading, or building permits Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building *b. Environmental Site Assessment Required* Requirement: The project applicant shall submit a Phase I Environmental Site Assessment report, and Phase II Environmental Site Assessment report if warranted by the Phase I report, for the project site for review and approval by the City. The report(s) shall be prepared by a qualified environmental assessment professional and include recommendations for remedial action, as appropriate, for hazardous materials. The project applicant shall implement the approved recommendations and submit to the City evidence of approval for any proposed remedial action and required clearances by the applicable local, state, or federal regulatory agency. When Required: Prior to approval of construction-related permit	accordance with all applicable laws and regulations. □ Implement the approved recommendations, the Health and Safety Plan and BMPs and submit evidence to the City of approval for remedial actions and required clearances.	activities.	Assessment reports, and Health and Safety Plan. □ Review and verify evidence of approval for the proposed remedial actions and required clearances by oversight authorities. □ Confirm that all applicable measures are being implemented or complied with pursuant to the regulatory agencies, the Health and Safety Plan and BMPs.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
Initial Approval: Applicable regulatory agency with jurisdiction Monitoring/Inspection: Applicable regulatory agency with jurisdiction *c. Health and Safety Plan Required* Requirement: The project applicant shall submit a Health and Safety Plan for the review and approval by the City in order to protect project construction workers from risks associated with hazardous materials. The project applicant shall implement the approved Plan. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building *d. Best Management Practices (BMPs) Required for Contaminated Sites* Requirement: The project applicant shall ensure that BMPs are implemented by the contractor during construction to minimize potential soil and groundwater hazards. These shall include the following: i. Soil generated by construction activities shall be stockpiled on-site in a secure and safe manner. All contaminated soils determined to be hazardous or non-hazardous waste must be adequately profiled (sampled) prior to acceptable reuse or disposal at an appropriate off-site facility. Specific sampling and handling and transport procedures for reuse or disposal shall be in accordance with applicable local, state,				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
and federal requirements. ii. Groundwater pumped from the subsurface shall be contained on-site in a secure and safe manner, prior to treatment and disposal, to ensure environmental and health issues are resolved pursuant to applicable laws and policies. Engineering controls shall be utilized, which include impermeable barriers to prohibit groundwater and vapor intrusion into the building. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-HAZ-3: Hazardous Materials Business Plan (#45) Requirement: The project applicant shall submit a Hazardous Materials Business Plan for review and approval by the City, and shall implement the approved Plan. The approved Plan shall be kept on file with the City and the project applicant shall update the Plan as applicable. The purpose of the Hazardous Materials Business Plan is to ensure that employees are adequately trained to handle hazardous materials and provides information to the Fire Department should emergency response be required. Hazardous materials shall be handled in accordance with all applicable local, state, and federal requirements. The Hazardous Materials Business Plan shall include the following: a. The types of hazardous materials or chemicals stored and/or used on-	Project sponsor: □ Submit a Hazardous Materials Business Plan and ensure that the plan is implemented.	□ Prior to issuance of demolition, grading, or building permits.	City of Oakland, Building Services Division and Planning and Zoning Division, Oakland Fire Department: □ Review and approve the Hazardous Materials Business Plan. □ Visit the site to verify that the measures	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
site, such as petroleum fuel products, lubricants, solvents, and cleaning fluids. b. The location of such hazardous materials. c. An emergency response plan including employee training information. d. A plan that describes the manner in which these materials are handled, transported, and disposed. When Required: Prior to building permit final Initial Approval: Oakland Fire Department Monitoring/Inspection: Oakland Fire Department			included in the plan are being implemented.	
4.3 Transportation and Circulation				
SCA-TRA-1: Construction Management Plan (#13) Requirement. Prior to the issuance of the first construction-related permit, the project applicant and his/her general contractor shall submit a Construction Management Plan (CMP) for review and approval by the Bureau of Planning, Bureau of Building, and other relevant City departments such as the Fire Department, Department of Transportation, and the Public Works Department as directed. The CMP shall contain measures to minimize potential construction impacts including measures to comply with all construction-related Conditions of Approval (and mitigation measures if applicable) such as dust control, construction emissions,	Project sponsor: □ Submit a Construction Management Plan (CMP) and ensure that plan measures are implemented.	□ Prior to issuance of demolition, grading, or building permits.	City of Oakland, Building Services Division, Planning and Zoning Division, Oakland Fire Department, Public Works Agency, Transportation Services Division:	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
hazardous materials, construction days/hours, construction traffic control, waste reduction and recycling, stormwater pollution prevention, noise control, complaint management, and cultural resource management (see applicable Conditions below). The CMP shall provide project-specific information including descriptive procedures, approval documentation, and drawings (such as a site logistics plan, fire safety plan, construction phasing plan, proposed truck routes, traffic control plan, complaint management plan, construction worker parking plan, and litter/debris clean-up plan) that specify how potential construction impacts will be minimized and how each construction-related requirement will be satisfied throughout construction of the project. When Required: Prior to approval of construction-related permit Initial Approval: Department of Transportation Monitoring/Inspection: Department of Transportation			□ Review and approve the Construction Management Plan. □ Verify that each construction-related requirement is satisfied.	
SCA-TRA-2: Construction Activity in the Public Right-of-Way (#76) *a. Obstruction Permit Required* Requirement: The project applicant shall obtain an obstruction permit from the City prior to placing any temporary construction-related obstruction in the public right-of-way, including City streets, sidewalks, bicycle facilities, and bus stops. When Required: Prior to approval of construction-related permit	Project sponsor: □ Obtain an obstruction permit. □ Submit a Traffic Control Plan. □ Implement measures in the Traffic Control Plan.	□ Prior to issuance of demolition, grading, or building permits. □ Ongoing during grading and construction.	City of Oakland, Building Services Division, Planning and Zoning Division, Public Works Agency, Oakland Fire Department, Transportation Services Division:	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Initial Approval: Department of Transportation Monitoring/Inspection: Department of Transportation *b. Traffic Control Plan Required* Requirement: In the event of obstructions to vehicle or bicycle travel lanes, bus stops, or sidewalks, the project applicant shall submit a Traffic Control Plan to the City for review and approval prior to obtaining an obstruction permit. The project applicant shall submit evidence of City approval of the Traffic Control Plan with the application for an obstruction permit. The Traffic Control Plan shall contain a set of comprehensive traffic control measures for auto, transit, bicycle, and pedestrian accommodations (or detours, if accommodations are not feasible), including detour signs if required, lane closure procedures, signs, cones for drivers, and designated construction access routes. The Traffic Control Plan shall be in conformance with the City's Supplemental Design Guidance for Accommodating Pedestrians, Bicyclists, and Bus Facilities in Construction Zones. The project applicant shall implement the approved Plan during construction. When Required: Prior to demolition permit Initial Approval: Department of Transportation Monitoring/Inspection: Department of Transportation *c. Repair of City Streets*	□ Repair any damage to the public right-of-way.		□ Review and approve the Traffic Control Plan prior to issuance of the obstruction permit. □ Verify that the measures in the traffic control plan are implemented and that damage to the public right-of-way is repaired.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Requirement: The project applicant shall repair any damage to the public right-of way, including streets and sidewalks, caused by project construction at his/her expense within one week of the occurrence of the damage (or excessive wear), unless further damage/excessive wear may continue; in such case, repair shall occur prior to approval of the final inspection of the construction-related permit. All damage that is a threat to public health or safety shall be repaired immediately. When Required: Prior to building permit final Initial Approval: N/A Monitoring/Inspection: Department of Transportation				
SCA-TRA-3: Bicycle Parking (#77) Requirement: The project applicant shall comply with the City of Oakland Bicycle Parking Requirements (chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall demonstrate compliance with the requirements. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building	Project sponsor: □ Comply with Bicycle Parking Requirements.	□ Prior to issuance of demolition, grading, or building permits.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review project drawings to ensure compliance with Bicycle Parking Requirements.	
SCA-TRA-4: Transportation Improvements (#78)	Project sponsor:	□ Plans submitted prior	City of Oakland, Building Services	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Requirement: The project applicant shall implement the recommended on- and off-site transportation-related improvements contained within the Transportation Impact Review for the project (e.g., signal timing adjustments, restriping, signalization, traffic control devices, roadway reconfigurations, transportation demand management measures, and transit, pedestrian, and bicyclist amenities). The project applicant is responsible for funding and installing the improvements, and shall obtain all necessary permits and approvals from the City and/or other applicable regulatory agencies such as, but not limited to, Caltrans (for improvements related to Caltrans facilities) and the California Public Utilities Commission (for improvements related to railroad crossings), prior to installing the improvements. To implement this measure for intersection modifications, the project applicant shall submit Plans, Specifications, and Estimates (PS&E) to the City for review and approval. All elements shall be designed to applicable City standards in effect at the time of construction and all new or upgraded signals shall include these enhancements as required by the City. All other facilities supporting vehicle travel and alternative modes through the intersection shall be brought up to both City standards and ADA standards (according to Federal and State Access Board guidelines) at the time of construction. Current City Standards call for, among other items, the elements listed below: a. 2070L Type Controller with cabinet accessory b. GPS communication (clock) c. Accessible pedestrian crosswalks according to Federal and State Access Board guidelines with signals (audible and tactile) d. Countdown pedestrian head module switch out e. City Standard ADA wheelchair ramps f. Video detection on existing (or new, if required)	□ Implement the recommended transportation improvements on International Boulevard and at the 55th Avenue and International Boulevard intersection. □ Submit Plans, Specifications and Estimates (PS&E) to modify the 55th Avenue and International Boulevard intersection in accordance with City standards in effect at the time of construction. □ Fund, prepare, and install the approved plans and improvements.	to issuance of demolition, grading, or building permits. □ Installation complete prior to Certificate of Occupancy.	Division, Planning and Zoning Division, Public Works Agency, Oakland Fire Department, Transportation Services Division: □ Review and approve the PS&E for compliance with City standards and ADA standards in effect at the time of construction. □ Confirm public improvements are implemented.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
g. Mast arm poles, full activation (where applicable) h. Polara Push buttons (full activation) i. Bicycle detection (full activation) j. Pull boxes k. Signal interconnect and communication with trenching (where applicable), or through existing conduit (where applicable), 600 feet maximum l. Conduit replacement contingency m. Fiber switch n. PTZ camera (where applicable) o. Transit Signal Priority (TSP) equipment consistent with other signals along corridor p. Signal timing plans for the signals in the coordination group q. Bi-directional curb ramps (where feasible, and if project is on a street corner) r. Upgrade ramps on receiving curb (where feasible, and if project is on a street corner) When Required: Prior to building permit final or as otherwise specified Initial Approval: Bureau of Building; Department of Transportation Monitoring/Inspection: Bureau of Building				
SCA-TRA-5: Transportation and Parking Demand Management (#79) *a. Transportation and Parking Demand Management (TDM) Plan Required* Requirement: The project applicant shall submit a Transportation and Parking Demand Management (TDM) Plan for review and approval by the	Project sponsor: □ Submit for review and approval a Transportation and Parking Demand Management (TDM) Plan	□ Prior to issuance of a final inspection of the building permit.	City of Oakland, Planning and Zoning Division, Transportation Services Division:	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
City. i. The goals of the TDM Plan shall be the following: • Reduce vehicle traffic and parking demand generated by the project to the maximum extent practicable. • Achieve the following project vehicle trip reductions (VTR): o Projects generating 50-99 net new a.m. or p.m. peak hour vehicle trips: 10 percent VTR. o Projects generating 100 or more net new a.m. or p.m. peak hour vehicle trips: 20 percent VTR. • Increase pedestrian, bicycle, transit, and carpool/vanpool modes of travel. All four modes of travel shall be considered, as appropriate. • Enhance the City's transportation system, consistent with City policies and programs. ii. The TDM Plan should include the following: • Baseline existing conditions of parking and curbside regulations within the surrounding neighborhood that could affect the effectiveness of TDM strategies, including inventory of parking spaces and occupancy if applicable. • Proposed TDM strategies to achieve VTR goals (see below). i. For employers with 100 or more employees at the subject site, the TDM Plan shall also comply with the requirements of Oakland Municipal Code Chapter 10.68 Employer-Based Trip Reduction Program. ii. The following TDM strategies **must** be incorporated into a TDM Plan based on a project location or other characteristics. When required, these mandatory strategies should be identified as a credit toward a project's VTR. *(See SCA -5 Improvement, Table 5: Required by code or when… on page*	containing strategies to reduce vehicle traffic and parking demand generated by the project to the maximum extent practicable. □ Implement strategies contained in the plan.	□ Ongoing throughout demolition, grading and construction. □ During project operation.	□ Verify that the TDM Plan has been prepared in accordance with the stipulations outlined in the SCA and approve if found acceptable. □ Monitor and enfore compliance with ongoing VTR strategies during project operation per conditions in SCA-TRA-5.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
4.3-14 of the Draft EIR) iii. Other TDM strategies to consider include, but are not limited to, the following: • Inclusion of additional long-term and short-term bicycle parking that meets the design standards set forth in chapter five of the Bicycle Master Plan and the Bicycle Parking Ordinance (chapter 17.117 of the Oakland Planning Code), and shower and locker facilities in commercial developments that exceed the requirement. • Construction of and/or access to bikeways per the Bicycle Master Plan; construction of priority bikeways, on-site signage and bike lane striping. • Installation of safety elements per the Pedestrian Master Plan (such as crosswalk striping, curb ramps, count down signals, bulb outs, etc.) to encourage convenient and safe crossing at arterials, in addition to safety elements required to address safety impacts of the project. • Installation of amenities such as lighting, street trees, and trash receptacles per the Pedestrian Master Plan, the Master Street Tree List and Tree Planting Guidelines (which can be viewed at http://www2.oaklandnet.com/oakca1/groups/pwa/documents/report/oak042662.pdf and http://www2.oaklandnet.com/oakca1/groups/pwa/documents/form/oak025595.pdf, respectively) and any applicable streetscape plan. • Construction and development of transit stops/shelters, pedestrian access, way finding signage, and lighting around transit stops per transit agency plans or negotiated improvements. • Direct on-site sales of transit passes purchased and sold at a bulk				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
group rate (through programs such as AC Transit Easy Pass or a similar program through another transit agency). • Provision of a transit subsidy to employees or residents, determined by the project applicant and subject to review by the City, if employees or residents use transit or commute by other alternative modes. • Provision of an ongoing contribution to transit service to the area between the project and nearest mass transit station prioritized as follows: 1) Contribution to AC Transit bus service; 2) Contribution to an existing area shuttle service; and 3) Establishment of new shuttle service. The amount of contribution (for any of the above scenarios) would be based upon the cost of establishing new shuttle service (Scenario 3). • Guaranteed ride home program for employees, either through 511.org or through separate program. • Pre-tax commuter benefits (commuter checks) for employees. • Free designated parking spaces for on-site car-sharing program (such as City Car Share, Zip Car, etc.) and/or car-share membership for employees or tenants. • On-site carpooling and/or vanpool program that includes preferential (discounted or free) parking for carpools and vanpools. • Distribution of information concerning alternative transportation options. • Parking spaces sold/leased separately for residential units. Charge employees for parking, or provide a cash incentive or transit pass alternative to a free parking space in commercial properties. • Parking management strategies including attendant/valet parking and shared parking spaces. • Requiring tenants to provide opportunities and the ability to work off-				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
site. • Allow employees or residents to adjust their work schedule in order to complete the basic work requirement of five eight-hour workdays by adjusting their schedule to reduce vehicle trips to the worksite (e.g., working four, ten-hour days; allowing employees to work from home two days per week). • Provide or require tenants to provide employees with staggered work hours involving a shift in the set work hours of all employees at the workplace or flexible work hours involving individually determined work hours. The TDM Plan shall indicate the estimated VTR for each strategy, based on published research or guidelines where feasible. For TDM Plans containing ongoing operational VTR strategies, the Plan shall include an ongoing monitoring and enforcement program to ensure the Plan is implemented on an ongoing basis during project operation. If an annual compliance report is required, as explained below, the TDM Plan shall also specify the topics to be addressed in the annual report. When Required: Prior to approval of planning application. Initial Approval: Bureau of Planning Monitoring/Inspection: N/A *b. TDM Implementation – Physical Improvements* Requirement: For VTR strategies involving physical improvements, the project applicant shall obtain the necessary permits/approvals from the City				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
and install the improvements prior to the completion of the project. When Required: Prior to building permit final Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building *c. TDM Implementation – Operational Strategies* Requirement: For projects that generate 100 or more net new a.m. or p.m. peak hour vehicle trips and contain ongoing operational VTR strategies, the project applicant shall submit an annual compliance report for the first five years following completion of the project (or completion of each phase for phased projects) for review and approval by the City. The annual report shall document the status and effectiveness of the TDM program, including the actual VTR achieved by the project during operation. If deemed necessary, the City may elect to have a peer review consultant, paid for by the project applicant, review the annual report. If timely reports are not submitted and/or the annual reports indicate that the project applicant has failed to implement the TDM Plan, the project will be considered in violation of the Conditions of Approval and the City may initiate enforcement action as provided for in these Conditions of Approval. The project shall not be considered in violation of this Condition if the TDM Plan is implemented but the VTR goal is not achieved. When Required: Ongoing				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Initial Approval: Department of Transportation Monitoring/Inspection: Department of Transportation				
SCA-TRA-6: Transportation Impact Fee (#80) Requirement: The project applicant shall comply with the requirements of the City of Oakland Transportation Impact Fee Ordinance (chapter 15.74 of the Oakland Municipal Code). When Required: Prior to issuance of building permit Initial Approval: Bureau of Building Monitoring/Inspection: N/A	Project sponsor: □ Comply with Transportation Impact Fee Ordinance.	□ Prior to issuance of demolition, grading, or building permits.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Ensure compliance with Transportation Impact Fee Ordinance requirements.	
SCA-TRA-7: Railroad Crossings (#82) Requirement: The project applicant shall submit for City review and approval a Diagnostic Review to evaluate potential impacts to at-grade railroad crossings resulting from project-related traffic. In general, the major types of impacts to consider are collisions between trains and vehicles, trains and pedestrians, and trains and bicyclists. The Diagnostic Review shall include specific traffic elements, such as roadway and rail description, accident history, traffic volumes (all modes, including pedestrian and bicyclist crossing movements), train volumes, vehicular speeds, train speeds, and existing rail and traffic control. Where the Diagnostic Review identifies potentially substantially dangerous	Project sponsor: □ Submit for review and approval a Diagnostic Review that evaluates potential impacts to at-grade railroad crossings resulting from project-related traffic. □ If potentially substantially dangerous crossing conditions caused by the	□ Submit Diagnostic Review for approval prior to issuance of demolition, grading, or building permits. □ Implement measures during operation	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve Diagnostic Review. □ If necessary, approve and verify implementation of measures identified to reduce substantially	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
crossing conditions at at-grade railroad crossings caused by the project, measures relative to the project's traffic contribution to the crossings shall be applied through project redesign and/or incorporation of the appropriate measures to reduce potential adverse impacts at the crossings. These measures may include, without limitation, the following: a. Installation of grade separations at crossings, i.e., physically separating roads and railroad tracks by constructing overpasses or underpasses b. Improvements to warning devices at existing highway rail crossings that are impacted by project traffic c. Installation of additional warning signage d. Improvements to traffic signaling at intersections adjacent to crossings, e.g., signal preemption e. Installation of median separation to prevent vehicles from driving around railroad crossing gates f. Where sound walls, landscaping, buildings, etc. would be installed near crossings, maintaining the visibility of warning devices and approaching trains g. Prohibition of parking within 100 feet of the crossings to improve the visibility of warning devices and approaching trains h. Construction of pull-out lanes for buses and vehicles transporting hazardous materials i. Installation of vandal-resistant fencing or walls to limit the access of pedestrians onto the railroad right-of-way j. Elimination of driveways near crossings k. Increased enforcement of traffic laws at crossings l. Rail safety awareness programs to educate the public about the hazards of highway-rail grade crossings	project are identified, implement measures relative to the project's traffic contribution to the crossings to reduce potential adverse impacts at the crossings. □ As necessary, coordinate with California Public Utility Commission (CPUC) and affected railroads.	of the project.	dangerous crossing conditions caused by the project at at-grade railroad crossings. □ As necessary, verify coordination with the California Public Utility Commission (CPUC) and affected railroads.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Any proposed improvements must be coordinated with California Public Utility Commission (CPUC) and affected railroads and all necessary permits/approvals obtained, including a GO 88-B Request (Authorization to Alter Highway Rail Crossings). The project applicant shall implement the approved measures during construction of the project. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building				
SCA-TRA-8: Plug-In Electric Vehicle (PEV) Charging Infrastructure (#83) *a. PEV-Ready Parking Spaces* Requirement: The applicant shall submit, for review and approval of the Building Official and the Zoning Manager, plans that show the location of parking spaces equipped with full electrical circuits designated for future PEV charging (i.e. "PEV-Ready) per the requirements of Chapter 15.04 of the Oakland Municipal Code. Building electrical plans shall indicate sufficient electrical capacity to supply the required PEV-Ready parking spaces. When Required: Prior to Issuance of Building Permit Initial Approval: Bureau of Building	Project sponsor: □ Submit for review and approval plans showing location and charging infrastructure for PEV and ADA-accessible parking spaces.	□ Prior to issuance of building permits.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve PEV and ADA plans.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Monitoring/Inspection: Bureau of Building *b. PEV-Capable Parking Spaces* Requirement: The applicant shall submit, for review and approval of the Building Official, plans that show the location of inaccessible conduit to supply PEV-capable parking spaces per the requirements of Chapter 15.04 of the Oakland Municipal Code. Building electrical plans shall indicate sufficient electrical capacity to supply the required PEV-capable parking spaces. When Required: Prior to Issuance of Building Permit Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building *c. ADA-Accessible Spaces* Requirement: The applicant shall submit, for review and approval of the Building Official, plans that show the location of future accessible EV parking spaces as required under Title 24 Chapter 11B Table 11B-228.3.2.1, and specify plans to construct all future accessible EV parking spaces with appropriate grade, vertical clearance, and accessible path of travel to allow installation of accessible EV charging station(s). When Required: Prior to Issuance of Building Permit				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building				
4.4 Air Quality				
Mitigation Measure AIR-1: Implement Mitigation Measure HAZ-1. *See Mitigation Measure HAZ-1 above.*	*See Mitigation Measure HAZ-1 above.*	*See Mitigation Measure HAZ-1 above.*	*See Mitigation Measure HAZ-1 above.*	
SCA-AIR-1: Dust Controls – Construction Related (#21) Requirement: The project applicant shall implement all of the following applicable dust control measures during construction of the project: a. Water all exposed surfaces of active construction areas at least twice daily. Watering should be sufficient to prevent airborne dust from leaving the site. Increased watering frequency may be necessary whenever wind speeds exceed 15 miles per hour. Reclaimed water should be used whenever feasible. b. Cover all trucks hauling soil, sand, and other loose materials or require all trucks to maintain at least two feet of freeboard (i.e., the minimum required space between the top of the load and the top of the trailer). c. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited. d. Limit vehicle speeds on unpaved roads to 15 miles per hour.	Project sponsor: □ Require construction manager to implement all the dust control measures identified in SCA-AIR-1 during construction.	□ Ongoing and throughout demolition, grading, and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Make regular visits to the project site to ensure that all dust and pollution control measures are being implemented and followed. □ Verify that a designated project complaint manager is on-call during the	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
e. All demolition activities (if any) shall be suspended when average wind speeds exceed 20 mph. f. All trucks and equipment, including tires, shall be washed off prior to leaving the site. g. Site accesses to a distance of 100 feet from the paved road shall be treated with a 6 to 12 inch compacted layer of wood chips, mulch, or gravel. h. Apply and maintain vegetative ground cover (e.g., hydroseed) or non-toxic soil stabilizers to disturbed areas of soil that will be inactive for more than one month. Enclose, cover, water twice daily, or apply (non-toxic) soil stabilizers to exposed stockpiles (dirt, sand, etc.). i. Designate a person or persons to monitor the dust control program and to order increased watering, as necessary, to prevent transport of dust offsite. Their duties shall include holidays and weekend periods when work may not be in progress. j. When working at a site, install appropriate wind breaks (e.g., trees, fences) on the windward side(s) of the site, to minimize wind-blown dust. Windbreaks must have a maximum 50 percent air porosity. k. Post a publicly visible large on-site sign that includes the contact name and phone number for the project complaint manager responsible for responding to dust complaints and the telephone numbers of the City's Code Enforcement unit and the Bay Area Air Quality Management District. When contacted, the project complaint manager shall respond and take corrective action within 48 hours. l. All exposed surfaces shall be watered at a frequency adequate to maintain minimum soil moisture of 12 percent. Moisture content can be verified by lab samples or moisture probe. When Required: During construction			construction period. □ Ensure all other measures in SCA-AIR-1 are implemented as applicable.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-AIR-2: Criteria Air Pollutant Controls - Construction Related (#22) Requirement: The project applicant shall implement all of the following applicable basic control measures for criteria air pollutants during construction of the project as applicable: a. Idling times on all diesel-fueled commercial vehicles over 10,000 lbs. shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to two minutes (as required by the California airborne toxics control measure Title 13, Section 2485, of the California Code of Regulations). Clear signage to this effect shall be provided for construction workers at all access points. b. Idling times on all diesel-fueled off-road vehicles over 25 horsepower shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to two minutes and fleet operators must develop a written policy as required by Title 23, Section 2449, of the California Code of Regulations (“California Air Resources Board Off- Road Diesel Regulations”). c. All construction equipment shall be maintained and properly tuned in accordance with the manufacturer’s specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. Equipment check documentation should be kept at the construction site and be available for review by the City and the Bay Area Air Quality District as needed. d. Portable equipment shall be powered by grid electricity if available. If	Project sponsor: □ Implement all basic control measures to reduce criteria air pollutants during construction identified in SCA-AIR-2.	□ Ongoing and throughout demolition, grading, and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Make regular visits to the project site to ensure that all criteria air pollutant reduction measures in SCA-AIR-2 are being implemented.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
electricity is not available, propane or natural gas generators shall be used if feasible. Diesel engines shall only be used if grid electricity is not available and propane or natural gas generators cannot meet the electrical demand. e. Low VOC (i.e., ROG) coatings shall be used that comply with BAAQMD Regulation 8, Rule 3: Architectural Coatings. f. All equipment to be used on the construction site shall comply with the requirements of Title 13, Section 2449, of the California Code of Regulations ("California Air Resources Board Off-Road Diesel Regulations") and upon request by the City (and the Air District if specifically requested), the project applicant shall provide written documentation that fleet requirements have been met. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-AIR-3: Diesel Particulate Matter Controls-Construction Related (#23) *a) Diesel Particulate Matter Reduction Measures* Requirement: The project applicant shall implement appropriate measures during construction to reduce potential health risks to sensitive receptors due to exposure to diesel particulate matter (DPM) from construction emissions. The project applicant shall choose one of the following methods: The project applicant shall retain a qualified air quality consultant to	Project sponsor: □ Choose one of the methods identified in SCA-AIR-3 and submit to the City for approval. □ Implement diesel particulate matter (DPM) reduction measures.	□ Submit measures for approval prior to issuance of demolition, grading, or building permits. □ Implement measures during	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review the Health Risk Assessment (HRA) and Construction Emissions Minimization Plan, or	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
prepare a Health Risk Assessment (HRA) in accordance with current guidance from the California Air Resources Board (CARB) and Office of Environmental Health and Hazard Assessment to determine the health risk to sensitive receptors exposed to DPM from project construction emissions. The HRA shall be submitted to the City (and the Air District if specifically requested) for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then DPM reduction measures are not required. If the HRA concludes that the health risk exceeds acceptable levels, DPM reduction measures shall be identified to reduce the health risk to acceptable levels as set forth under subsection b below. Identified DPM reduction measures shall be submitted to the City for review and approval prior to the issuance of building permits and the approved DPM reduction measures shall be implemented during construction. -or- All off-road diesel equipment shall be equipped with the most effective Verified Diesel Emission Control Strategies (VDECS) available for the engine type (Tier 4 engines automatically meet this requirement) as certified by CARB. The equipment shall be properly maintained and tuned in accordance with manufacturer specifications. This shall be verified through an equipment inventory submittal and Certification Statement that the Contractor agrees to compliance and acknowledges that a significant violation of this requirement shall constitute a material breach of contract. When Required: Prior to issuance of a construction related permit (i), during construction (ii)		construction of the project.	Equipment Inventory and Certification Statement, as necessary. □ Verify implementation of any diesel particulate matter reduction measures as described in SCA-AIR-3 or the Emissions Minimization Plan are being implemented.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building *b) Construction Emissions Minimization Plan (if required by a above)* Requirement: The project applicant shall prepare a Construction Emissions Minimization Plan (Emissions Plan) for all identified DPM reduction measures (if any). The Emissions Plan shall be submitted to the City (and the Bay Area Air Quality District if specifically requested) for review and approval prior to the issuance of building permits. The Emissions Plan shall include the following: i. An equipment inventory summarizing the type of off-road equipment required for each phase of construction, including the equipment manufacturer, equipment identification number, engine model year, engine certification (tier rating), horsepower, and engine serial number. For all VDECS, the equipment inventory shall also include the technology type, serial number, make, model, manufacturer, CARB verification number level, and installation date. ii. A Certification Statement that the Contractor agrees to comply fully with the Emissions Plan and acknowledges that a significant violation of the Emissions Plan shall constitute a material breach of contract. When Required: Prior to issuance of a construction related permit Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
SCA-AIR-4: Stationary Sources of Air Pollution (Toxic Air Contaminants) (#25) Requirement: The project applicant shall incorporate appropriate measures into the project design in order to reduce the potential health risk due to on-site stationary sources of toxic air contaminants (TACs). The project applicant shall choose one of the following methods: a. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with California Air Resources Board (CARB) and Office of Environmental Health and Hazard Assessment requirements to determine the health risk associated with proposed stationary sources of pollution in the project. The HRA shall be submitted to the City for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then health risk reduction measures are not required. If the HRA concludes the health risk exceeds acceptable levels, health risk reduction measures shall be identified to reduce the health risk to acceptable levels. Identified risk reduction measures shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City. - or - b. The project applicant shall incorporate the following health risk reduction measures into the project. These features shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City:	Project sponsor: □ Incorporate appropriate measures into the project design to reduce potential health risks due to on-site stationary sources of toxic air contaminants (TACs). □ Choose one of the methods identified in SCA-AIR-4 and submit to City for approval. □ Implement TACs reduction measures.	□ Submit measures for approval prior to issuance of demolition, grading, or building permits. □ Implement measures during construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve the Health Risk Assessment (HRA) and risk reduction measures, as necessary. □ Verify implementation of any TACs reduction measures as described in SCA-AIR-4.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
i. Installation of non-diesel fueled generators, if feasible, or; ii. Installation of diesel generators with an EPA-certified Tier 4 engine or engines that are retrofitted with a CARB Level 3 Verified Diesel Emissions Control Strategy, if feasible. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building				
SCA-AIR-5: Truck-Related Risk Reduction Measures (Toxic Air Contaminants) (#26) *a. Truck Loading Docks* Requirement: The project applicant shall locate proposed truck loading docks as far from nearby sensitive receptors as feasible. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building *b. Truck Fleet Emission Standards* Requirement: The project applicant shall comply with all applicable California Air Resources Board (CARB) requirements to control emissions from diesel engines and demonstrate compliance to the satisfaction of the City. Methods to comply include, but are not limited to, new clean diesel trucks, higher-tier diesel engine trucks with added Particulate Matter (PM)	Project sponsor: □ Provide final plans showing location of truck loading docks. □ Comply with and document compliance for City approval of CARB requirements to control emissions from diesel engines during project operation.	□ Submit final plans for approval showing location of truck loading docks and document-tation showing compliance with CARB's Procedures for In-Use Strategies to Control Emissions from Diesel Engines prior to issuance of construction	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve final plans showing loading dock location. □ Verify compliance with CARB's Procedures for In-Use Strategies to Control Emissions from Diesel Engines.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
filters, hybrid trucks, alternative energy trucks, or other methods that achieve the applicable CARB emission standard. Compliance with this requirement shall be verified through CARB's Verification Procedures for In-Use Strategies to Control Emissions from Diesel Engines. When Required: Prior to building permit final; ongoing Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building		and building permits.		
SCA-AIR-6: Asbestos in Structures (#27) Requirement: The project applicant shall comply with all applicable laws and regulations regarding demolition and renovation of Asbestos Containing Materials (ACM), including but not limited to California Code of Regulations, Title 8; California Business and Professions Code, Division 3; California Health and Safety Code sections 25915-25919.7; and Bay Area Air Quality Management District, Regulation 11, Rule 2, as may be amended. Evidence of compliance shall be submitted to the City upon request. When Required: Prior to approval of construction-related permit Initial Approval: Applicable regulatory agency with jurisdiction Monitoring/Inspection: Applicable regulatory agency with jurisdiction	Project sponsor: □ Comply with laws and regulations regarding demolition and renovation of asbestos containing materials. □ Submit evidence of compliance to City.	□ Prior to issuance of building occupancy permit.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review evidence of compliance with applicable laws and regulations regarding demolition and renovation of asbestos containing materials.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
4.5 Greenhouse Gas Emissions and Energy				
Mitigation Measure GHG-1: **a. GHG Reduction Plan Required.** The project applicant shall retain a qualified air quality consultant to develop a GHG Reduction Plan for City review and shall implement the approved GHG Reduction Plan. The GHG Reduction Plan shall demonstrate compliance with at least one of the following GHG goals prior to approval of a construction-related permit: 1) Consistency with a certified Qualified Climate Action Plan (if available); or 2) GHG emissions from non-transportation sources below the 2030 GHG efficiency threshold of 0.61 MT CO2e/SP. The GHG Reduction Plan shall include, at a minimum: (a) a detailed GHG emissions inventory for the project, taking into consideration energy efficiencies included as part of the project (including the City's Standard Conditions of Approval, project design features, and other City requirements) and additional GHG reduction measures available to further reduce GHG emissions, and (b) requirements for ongoing monitoring and reporting to demonstrate that the additional GHG reduction measures are being implemented. If the project is to be constructed in phases, the GHG Reduction Plan shall provide GHG emission scenarios by phase. Such GHG reduction measures may include, but are not limited to, the following:	Project sponsor: □ Submit GHG Reduction Plan for approval. □ Implement GHG reduction measures during construction. □ Implement GHG reduction measures during operation and provide monitoring reports to comply with measures identified in GHG-1.	□ Submit GHG Reduction Plan prior to issuance of demolition, grading, or building permits. □ Implement measures during construction and operation of the project.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve GHG Reduction Plan. □ Verify compliance with GHG Reduction Plan measures during construction. □ Review and approve Annual Report and implement any corrective procedures, as necessary.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Carbon-Free Energy. Address 100 percent of the project's electricity need through carbon-free sources (e.g., renewable, and hydroelectric) and/or carbon offset projects. **Alternative Fuels for Diesel-Powered Construction Equipment.** Use renewable diesel fuel for diesel-powered construction equipment that meets California's Low Carbon Fuel Standards and is certified by CARB Executive Officer. **Outdoor Electrical Receptacles.** Include electrical receptacles on the exterior of walls of the building that are accessible for the purposes of charging or powering electric landscaping equipment and providing an alternative to using fossil fuel-powered generators. **Electric Forklifts and Associated Charging Stations.** Include a dedicated charging station for electric forklifts at all loading docks and truck loading areas. Other potential GHG reduction measures to be considered include, but are not be limited to, measures recommended in BAAQMD's latest CEQA Guidelines, the California Air Resources Board Scoping Plan (December 2008, as may be revised), the California Air Pollution Control Officers Association (CAPCOA) Quantifying Greenhouse Gas Mitigation Measures (August 2010, as may be revised), the California Attorney General's website, and Reference Guides on Leadership in Energy and Environmental Design (LEED) published by the U.S. Green Building Council. The types of allowable GHG reduction measures include the following				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
(listed in order of City preference): (1) physical design features, listed above; (2) operational features; and (3) the payment of fees to fund GHG-reducing programs (i.e., the purchase of "carbon credits") as explained below. For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be included on the drawings submitted for construction-related permits. If, after exhaustion of feasible physical design features and operational features specific to the project, the project's GHG emissions would still fail to meet the requirements of GHG goal 1 or GHG goal 2, the project applicant shall purchase carbon credits to further reduce GHG emissions. The preference for purchasing carbon credits by location shall be applied as follows: (1) within the City of Oakland; (2) within the San Francisco Bay Area Air Basin; (3) within the State of California; then (4) elsewhere in the United States. The cost of carbon credit purchases shall be based on current market value at the time purchased and shall be based on the project's operational emissions estimated in the GHG Reduction Plan or subsequent approved emissions inventory, which may result in emissions that are higher or lower than those estimated in the GHG Reduction Plan. For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be included on the drawings submitted for construction-related permits. **b. GHG Reduction Plan Implementation during Construction.** The project applicant shall implement the GHG Reduction Plan during construction of the project. For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be implemented during construction. For physical GHG reduction measures to be incorporated into off-site projects, the project applicant shall obtain all				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
necessary permits/approvals and the measures shall be included on drawings and submitted to the City Planning Director or his/her designee for review and approval. These off-site improvements shall be installed prior to completion of the subject project (or prior to completion of the project phase for phased projects). For GHG reduction measures involving the purchase of carbon credits, evidence of the payment/purchase shall be submitted to the City for review and approval prior to completion of the project (or prior to completion of the project phase, for phased projects). **c. GHG Reduction Plan Implementation after Construction.** The project applicant shall implement the GHG Reduction Plan after construction of the project (or at the completion of the project phase for phased projects). For operational GHG reduction measures to be incorporated into the project or off-site projects, the measures shall be implemented on an indefinite and ongoing basis. The project applicant shall satisfy the following requirements for ongoing monitoring and reporting to demonstrate that the additional GHG reduction measures are being implemented. The GHG Reduction Plan requires regular periodic evaluation over the life of the project (generally estimated to be at least 40 years) to determine how the Plan is achieving required GHG emissions reductions over time, as well as the efficacy of the specific additional GHG reduction measures identified in the Plan. **d. Annual Report.** Implementation of the GHG reduction measures and related requirements shall be ensured through compliance with Conditions of Approval adopted for the project. Generally, starting two years after the City issues the first Certificate of Occupancy for the project, the project applicant shall prepare each year of the useful life of the project an Annual				

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Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
GHG Emissions Reduction Report ("Annual Report"), for review and approval by the City Planning Director or his/her designee. The Annual Report shall be submitted to an independent reviewer of the City's choosing to be paid for by the project applicant. The Annual Report shall summarize the project's implementation of GHG reduction measures over the preceding year, intended upcoming changes, compliance with the conditions of the Plan, and include a brief summary of the previous year's Annual Report results (starting the second year). The Annual Report shall include a comparison of annual project emissions to the baseline emissions reported in the GHG Plan. The GHG Reduction Plan shall be considered fully attained when the project demonstrates consistency with a Qualified Climate Action Plan or when the project reduces non-transportation GHG emissions below the 2030 GHG efficiency threshold of 0.61 MT CO2e/SP, as confirmed by the City through an established monitoring program. Monitoring and reporting activities will continue at the City's discretion, as discussed below. **e. Corrective Procedure.** If the third Annual Report, or any report thereafter, indicates that, in spite of the implementation of the GHG Reduction Plan, the project is not achieving the GHG reduction goal, the project applicant shall prepare a report for City review and approval, which proposes additional or revised GHG measures to better achieve the GHG emissions reduction goals, including without limitation, a discussion on the feasibility and effectiveness of the menu of other additional measures ("Corrective GHG Action Plan"). The project applicant shall then implement the approved Corrective GHG Action Plan.				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
If, one year after the Corrective GHG Action Plan is implemented, the required GHG emissions reduction target is still not being achieved, or if the project applicant fails to submit a report at the times described above, or if the reports do not meet City requirements outlined above, the City may, in addition to its other remedies: (a) assess the project applicant a financial penalty based upon actual percentage reduction in GHG emissions as compared to the percent reduction in GHG emissions established in the GHG Reduction Plan; or (b) refer the matter to the City Planning Commission for scheduling of a compliance hearing to determine whether the project's approvals should be revoked, altered or additional conditions of approval imposed. The penalty as described in (a) above shall be determined by the City Planning Director or his/her designee and be commensurate with the percentage GHG emissions reduction not achieved (compared to the applicable numeric significance thresholds) or required percentage reduction from the "adjusted" baseline. In determining whether a financial penalty or other remedy is appropriate, the City shall not impose a penalty if the project applicant has made a good faith effort to comply with the GHG Reduction Plan. The City would only have the ability to impose a monetary penalty after a reasonable cure period and in accordance with the enforcement process outlined in Planning Code Chapter 17.152. If a financial penalty is imposed, such penalty sums shall be used by the City solely toward the implementation of the GHG Reduction Plan. **f. Timeline Discretion and Summary.** The City shall have the discretion				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
to reasonably modify the timing of reporting, with reasonable notice and opportunity to comment by the applicant, to coincide with other related monitoring and reporting required for the project.				
Mitigation Measure GHG-2: Implement Mitigation Measure GHG-1. While implementation of Mitigation Measure GHG-1 would reduce the potential conflicts with policies, this impact would still be considered significant and unavoidable. *See Mitigation Measure GHG-1 above.*	*See Mitigation Measure GHG-1 above.*	*See Mitigation Measure GHG-1 above.*	*See Mitigation Measure GHG-1 above.*	
Mitigation Measure GHG-3: Implement Mitigation Measure GHG-1. While implementation of this mitigation measure would reduce the potential conflicts with policies, this cumulative impact would still be considered significant and unavoidable. *See Mitigation Measure GHG-1 above.*	*See Mitigation Measure GHG-1 above.*	*See Mitigation Measure GHG-1 above.*	*See Mitigation Measure GHG-1 above.*	
SCA-GHG-1: Green Building Requirements (#87) *a. Compliance with Green Building Requirements During Plan-Check* Requirement: The project applicant shall comply with the requirements of the California Green Building Standards (CALGreen) mandatory measures and the applicable requirements of the City of Oakland Green Building Ordinance (chapter 18.02 of the Oakland Municipal Code). i. The following information shall be submitted to the City for review and approval with the application for a building permit: Documentation showing compliance with Title 24 of the current version of the California Building Energy Efficiency Standards.	Project sponsor: □ Comply with the requirements of the California Green Building Standards (CALGreen) and implement mandatory measures and the applicable requirements of the Green Building Ordinance (chapter 18.02 of the Oakland Municipal Code).	□ Submit measures for approval prior to issuance of demolition, grading, or building permits.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve required documentation described in SCA-GHG-1.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Completed copy of the final green building checklist approved during the review of the Planning and Zoning permit. Copy of the Unreasonable Hardship Exemption, if granted, during the review of the Planning and Zoning permit. Permit plans that show, in general notes, detailed design drawings, and specifications as necessary, compliance with the items listed in subsection (ii) below. Copy of the signed statement by the Green Building Certifier approved during the review of the Planning and Zoning permit that the project complied with the requirements of the Green Building Ordinance. Signed statement by the Green Building Certifier that the project still complies with the requirements of the Green Building Ordinance, unless an Unreasonable Hardship Exemption was granted during the review of the Planning and Zoning permit. Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance. ii. The set of plans in subsection (i) shall demonstrate compliance with the following: CALGreen mandatory measures. New Construction of Residential or Non- residential projects that remove a Historic Resource (as defined by the Green Building				

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Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Ordinance) the point level certification requirement is 53 points for residential and LEED Gold for non-residential)] per the appropriate checklist approved during the Planning entitlement process. All green building points identified on the checklist approved during review of the Planning and Zoning permit, unless a Request for Revision Plan-check application is submitted and approved by the Bureau of Planning that shows the previously approved points that will be eliminated or substituted. The required green building point minimums in the appropriate credit categories. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building Monitoring/Inspection: N/A *b. Compliance with Green Building Requirements During Construction* Requirement: The project applicant shall comply with the applicable requirements of CALGreen and the Oakland Green Building Ordinance during construction of the project. The following information shall be submitted to the City for review and approval: i. Completed copies of the green building checklists approved during the review of the Planning and Zoning permit and during the review of the				

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
building permit. ii. Signed statement(s) by the Green Building Certifier during all relevant phases of construction that the project complies with the requirements of the Green Building Ordinance. iii. Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building *c. Compliance with Green Building Requirements After Construction* Requirement: Prior to issuance of the final Building Permit, the Green Building Certifier shall submit the appropriate documentation to City staff and attain the minimum required point level. When Required: Prior to Final Approval Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building				
4.6 Noise and Groundborne Vibration				
SCA-NOI-1: Construction Days/Hours (#62)	Project sponsor:	□ Implement measures	City of Oakland, Building Services	

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Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Requirement: The project applicant shall comply with the following restrictions concerning construction days and hours: a. Construction activities are limited to between 7:00 a.m. and 7:00 p.m. Monday through Friday, except that pier drilling and/or other extreme noise generating activities greater than 90 dBA shall be limited to between 8:00 a.m. and 4:00 p.m. b. Construction activities are limited to between 9:00 a.m. and 5:00 p.m. on Saturday. In residential zones and within 300 feet of a residential zone, construction activities are allowed from 9:00 a.m. to 5:00 p.m. only within the interior of the building with the doors and windows closed. No pier drilling or other extreme noise generating activities greater than 90 dBA are allowed on Saturday. c. No construction is allowed on Sunday or federal holidays. Construction activities include, but are not limited to, truck idling, moving equipment (including trucks, elevators, etc.) or materials, deliveries, and construction meetings held on-site in a non-enclosed area. Any construction activity proposed outside of the above days and hours for special activities (such as concrete pouring which may require more continuous amounts of time) shall be evaluated on a case-by-case basis by the City, with criteria including the urgency/emergency nature of the work, the proximity of residential or other sensitive uses, and a consideration of nearby residents'/occupants' preferences. The project applicant shall notify property owners and occupants located within 300 feet at least 14 calendar days prior to construction activity proposed outside of the above	□ Ensure that construction contractors comply with the construction days and hours restrictions identified in SCA-NOI-1.	during demolition, grading and construction.	Division and Planning and Zoning Division: □ Ensure that construction limitations are appropriately implemented.	

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Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
days/hours. When submitting a request to the City to allow construction activity outside of the above days/hours, the project applicant shall submit information concerning the type and duration of proposed construction activity and the draft public notice for City review and approval prior to distribution of the public notice. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-NOI-2: Construction Noise (#63) Requirement: The project applicant shall implement noise reduction measures to reduce noise impacts due to construction. Noise reduction measures include, but are not limited to, the following: a. Equipment and trucks used for project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically-attenuating shields or shrouds) wherever feasible. b. Except as provided herein, impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for project construction shall be hydraulically or electrically powered to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used; this muffler can lower noise	Project sponsor: □ Ensure that construction contractors implement noise control measures identified in SCA-NOI-2.	□ Implement measures during demolition, grading and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Make regular visits to the project site to ensure that noise reduction measures are appropriately implemented.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
levels from the exhaust by up to about 10 dBA. External jackets on the tools themselves shall be used, if such jackets are commercially available, and this could achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than impact equipment, whenever such procedures are available and consistent with construction procedures. c. Applicant shall use temporary power poles instead of generators where feasible. d. Stationary noise sources shall be located as far from adjacent properties as possible, and they shall be muffled and enclosed within temporary sheds, incorporate insulation barriers, or use other measures as determined by the City to provide equivalent noise reduction. e. The noisiest phases of construction shall be limited to less than 10 days at a time. Exceptions may be allowed if the City determines an extension is necessary and all available noise reduction controls are implemented. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-NOI-3: Extreme Construction Noise (#64)	Project sponsor:	□ Submit Construction	City of Oakland, Building Services	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
a. Construction Noise Management Plan Required Requirement: Prior to any extreme noise generating construction activities (e.g., pier drilling, pile driving and other activities generating greater than 90dBA), the project applicant shall submit a Construction Noise Management Plan prepared by a qualified acoustical consultant for City review and approval that contains a set of site-specific noise attenuation measures to further reduce construction impacts associated with extreme noise generating activities. The project applicant shall implement the approved Plan during construction. Potential attenuation measures include, but are not limited to, the following: i. Erect temporary plywood noise barriers around the construction site, particularly along on sites adjacent to residential buildings; ii. Implement "quiet" pile driving technology (such as pre-drilling of piles, the use of more than one pile driver to shorten the total pile driving duration), where feasible, in consideration of geotechnical and structural requirements and conditions; iii. Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site; iv. Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings by the use of sound blankets for example and implement such measure if such measures are feasible and would noticeably reduce noise impacts; and	□ Submit Construction Noise Management Plan for approval. □ Implement noise reduction measures contained in the plan during construction. □ Submit for approval extreme noise generating activities and the proposed public notice.	Noise Management Plan for approval prior to issuance of demolition, grading, or building permits. □ Implement measures during demolition, grading and construction.	Division and Planning and Zoning Division: □ Review and approve Construction Noise Management Plan and extreme noise generating activities and public notice prior to posting. □ Make regular visits to the project site to ensure that noise reduction measures identified in SCA-NOI-3 are appropriately implemented.	

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Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
v. Monitor the effectiveness of noise attenuation measures by taking noise measurements. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building *b. Public Notification Required* Requirement: The project applicant shall notify property owners and occupants located within 300 feet of the construction activities at least 14 calendar days prior to commencing extreme noise generating activities. Prior to providing the notice, the project applicant shall submit to the City for review and approval the proposed type and duration of extreme noise generating activities and the proposed public notice. The public notice shall provide the estimated start and end dates of the extreme noise generating activities and describe noise attenuation measures to be implemented. When Required: During construction Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building				

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Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
SCA-NOI-4: Construction Noise Complaints (#66) Requirement: The project applicant shall submit to the City for review and approval a set of procedures for responding to and tracking complaints received pertaining to construction noise, and shall implement the procedures during construction. At a minimum, the procedures shall include: a. Designation of an on-site construction complaint and enforcement manager for the project; b. A large on-site sign near the public right-of-way containing permitted construction days/hours, complaint procedures, and phone numbers for the project complaint manager and City Code Enforcement unit; c. Protocols for receiving, responding to, and tracking received complaints; and d. Maintenance of a complaint log that records received complaints and how complaints were addressed, which shall be submitted to the City for review upon the City's request. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building	Project sponsor: □ Submit for approval the Construction Noise Complaints procedures as identified in SCA-NOI-4.	□ Submit procedures for approval prior to issuance of demolition, grading, or building permits. □ Implement measures during demolition, grading and construction.	City of Oakland, Building Services Division: □ Review and approve the noise complaint procedures to ensure they include the measures outlined in SCA-NOI-4.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
SCA-NOI-5: Exposure to Community Noise (#67) Requirement: The project applicant shall submit a Noise Reduction Plan prepared by a qualified acoustical engineer for City review and approval that contains noise reduction measures (e.g., sound-rated window, wall, and door assemblies) to achieve an acceptable interior noise level in accordance with the land use compatibility guidelines of the Noise Element of the Oakland General Plan. The applicant shall implement the approved Plan during construction. To the maximum extent practicable, interior noise levels shall not exceed the following: a. 45 dBA: Residential activities, civic activities, hotels b. 50 dBA: Administrative offices; group assembly activities c. 55 dBA: Commercial activities d. 65 dBA: Industrial activities When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Planning Monitoring/Inspection: Bureau of Building	Project sponsor: □ Submit a Noise Reduction Plan for approval. □ Implement the noise reduction measures during construction.	□ Submit Noise Reduction Plan prior to issuance of demolition, grading, or building permit. □ Implement measures during demolition, grading and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve Noise Reduction Plan. □ If deemed necessary, ensure that noise reduction measures outlined in the plan are implemented.	
SCA-NOI-6: Operational Noise (#68) Applicable To: All projects.	Project sponsor: □ Ensure noise levels from the activity, property, or any	□ Ongoing.	City of Oakland, Building Services Division and Planning	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
Requirement: Noise levels from the project site after completion of the project (i.e., during project operation) shall comply with the performance standards of chapter 17.120 of the Oakland Planning Code and chapter 8.18 of the Oakland Municipal Code. If noise levels exceed these standards, the activity causing the noise shall be abated until appropriate noise reduction measures have been installed and compliance verified by the City. When Required: Ongoing Initial Approval: N/A Monitoring/Inspection: Bureau of Building	mechanical equipment on site shall comply with the performance standards of chapter 17.120 of the Oakland Planning Code and chapter 8.18 of the Oakland Municipal Code.		and Zoning Division: □ If noise levels exceed the standards identified in SCA-NOI-6, verify compliance after action is taken by the project sponsor to regain compliance as described in SCA-NOI-6.	
SCA-NOI-7: Vibration Impacts on Adjacent Structures or Vibration-Sensitive Activities (#70) Requirement: The project applicant shall submit a Vibration Analysis prepared by an acoustical and/or structural engineer or other appropriate qualified professional for City review and approval that establishes pre-construction baseline conditions and threshold levels of vibration that could damage the structure and/or substantially interfere with activities at the nearest residential and commercial buildings to the northwest of the project site along 54th Avenue located between San Leandro Street and International Boulevard. The Vibration Analysis shall identify design means and methods of construction that shall be utilized in order to not exceed the thresholds. The applicant shall implement the recommendations during construction.	Project sponsor: □ Submit a Vibration Analysis for approval. □ Implement the design means and methods of construction to avoid exceeding the thresholds.	□ Submit Vibration Analysis prior to issuance of demolition, grading, or building permit. □ Implement measures during demolition, grading and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve the Vibration Analysis and pre-construction existing conditions study prior to issuance of a grading permit. □ Verify recommended measures are being	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
When Required: Prior to construction Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building			implemented during construction. □ Review findings upon completion of the project to ensure the proposed project did not cause any damage to the nearest residential and commercial buildings located northwest of the project site along 54th Avenue.	
4.7 Geology, Soils and Seismicity				
SCA-GEO-1: Construction-Related Permit(s) (#37) Requirement: The project applicant shall obtain all required construction-related permits/approvals from the City. The project shall comply with all standards, requirements and conditions contained in construction-related codes, including but not limited to the Oakland Building Code and the Oakland Grading Regulations, to ensure structural integrity and safe	Project sponsor: □ Obtain all required construction-related permits and approvals from the City.	□ Prior to approval of Building #1 demolition permit and construction-related permits.	City of Oakland, Building Services Division and Planning and Zoning Division – Historic Preservation Staff: □ Review and approve	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
construction. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building			design-level construction and building reports related to the techniques and methods for remediating and stabilizing the bulkhead portion of Building #1 and incorporation into the new building.	
SCA-GEO-2: Seismic Hazards Zone (Landslide/Liquefaction) (#40) Requirement: The project applicant shall submit a site-specific geotechnical report, consistent with California Geological Survey Special Publication 117 (as amended), prepared by a registered geotechnical engineer for City review and approval containing at a minimum a description of the geological and geotechnical conditions at the site, an evaluation of site-specific seismic hazards based on geological and geotechnical conditions, and recommended measures to reduce potential impacts related to liquefaction and/or slope stability hazards. The project applicant shall implement the recommendations contained in the approved report during project design and construction. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building	Project sponsor: □ Submit a site-specific geotechnical report to the City. □ Implement the recommendations contained in the approved report.	□ Prior to issuance of construction-related permits. □ During project grading and construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve the site-specific geotechnical report that meet the requirements of SCA-GEO-2.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program				
Standard Conditions of Approval/Mitigation Measures	**Implementation Responsibility/ Action**	**Timing**	**Monitoring Responsibility/Action**	**Date Completed/ Signature**
Monitoring/Inspection: Bureau of Building				
4.8 Hydrology and Water Quality				
SCA-HYD-1: Erosion and Sedimentation Control Plan for Construction (#45) *a. Erosion and Sedimentation Control Plan Required* Requirement: The project applicant shall submit an Erosion and Sedimentation Control Plan to the City for review and approval. The Erosion and Sedimentation Control Plan shall include all necessary measures to be taken to prevent excessive stormwater runoff or carrying by stormwater runoff of solid materials on to lands of adjacent property owners, public streets, or to creeks as a result of conditions created by grading and/or construction operations. The Plan shall include, but not be limited to, such measures as short-term erosion control planting, waterproof slope covering, check dams, interceptor ditches, benches, storm drains, dissipation structures, diversion dikes, retarding berms and barriers, devices to trap, store and filter out sediment, and stormwater retention basins. Off-site work by the project applicant may be necessary. The project applicant shall obtain permission or easements necessary for off-site work. There shall be a clear notation that the plan is subject to changes as changing conditions occur. Calculations of anticipated stormwater runoff and sediment volumes shall be included, if required by the City. The Plan shall specify that, after construction is complete, the project applicant shall ensure that the storm drain system shall be inspected and that the project applicant shall clear the system of any debris	Project sponsor: □ Submit a site-specific erosion and sedimentation control plan to the City in conformance with the measures outlined in SCA-HYD-1. □ Obtain permission or easements necessary for off-site work from the City and Caltrans. □ Implement approved plan during construction.	□ Prior to issuance of construction-related permits. □ Ongoing throughout demolition, grading, and/or construction activities.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve the site-specific erosion and sedimentation control plan in conformance with the measures outlined in SCA-HYD-1. □ Confirm issuance of permission or easements necessary for off-site work. □ Confirm that all applicable measures are being implemented or complied with per the approved plan.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
or sediment. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Building Monitoring/Inspection: N/A *b. Erosion and Sedimentation Control During Construction* Requirement: The project applicant shall implement the approved Erosion and Sedimentation Control Plan. No grading shall occur during the wet weather season (October 15 through April 15) unless specifically authorized in writing by the Bureau of Building. When Required: During construction Initial Approval: N/A Monitoring/Inspection: Bureau of Building				
SCA-HYD-2: State Construction General Permit (#46) Requirement: The project applicant shall comply with the requirements of the Construction General Permit issued by the State Water Resources Control Board (SWRCB). The project applicant shall submit a Notice of Intent (NOI), Stormwater Pollution Prevention Plan (SWPPP), and other required Permit Registration Documents to SWRCB. The project applicant	Project sponsor: □ Comply with the requirements of the Construction General Permit issued by the State Water Resources Control	□ Prior to issuance of demolition, grading, and building permits. □ Ongoing throughout	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve evidence of compliance with permit	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
shall submit evidence of compliance with Permit requirements to the City. When Required: Prior to approval of construction-related permit Initial Approval: State Water Resources Control Board; evidence of compliance submitted to Bureau of Building Monitoring/Inspection: State Water Resources Control Board	Board (SWRCB). □ Submit a Notice of Intent (NOI), Stormwater Pollution Prevention Plan (SWPPP), and other required Permit Registration Documents to SWRCB.	construction	requirements, including the NOI, SWPPP, and permit registration documents.	
SCA-HYD-3: NPDES C.3 Stormwater Requirements for Regulated Projects (#50) *a. Post-Construction Stormwater Management Plan Required* Requirement: The project applicant shall comply with the requirements of Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES). The project applicant shall submit a Post-Construction Stormwater Management Plan to the City for review and approval with the project drawings submitted for site improvements, and shall implement the approved Plan during construction. The Post-Construction Stormwater Management Plan shall include and identify the following: i. Location and size of new and replaced impervious surface; ii. Directional surface flow of stormwater runoff;	Project sponsor: □ Submit a Post-Construction Stormwater Management Plan and project drawings to the City for review and approval. □ Implement the approved plan during and after construction.	□ Prior to issuance of demolition, grading, and building permits. □ Ongoing throughout construction.	City of Oakland, Building Services Division and Planning and Zoning Division: □ Review and approve project drawings and confirm that the drainage plan reduces post-construction volume and velocity of stormwater runoff, as required by SCA-HYD-3. □ Review and approve post-construction stormwater pollution	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
iii. Location of proposed on-site storm drain lines; iv. Site design measures to reduce the amount of impervious surface area; v. Source control measures to limit stormwater pollution; vi. Stormwater treatment measures to remove pollutants from stormwater runoff, including the method used to hydraulically size the treatment measures; and vii. Hydromodification management measures, if required by Provision C.3, so that post-project stormwater runoff flow and duration match pre-project runoff. When Required: Prior to approval of construction-related permit Initial Approval: Bureau of Planning; Bureau of Building Monitoring/Inspection: Bureau of Building *b. Maintenance Agreement Required* Requirement: The project applicant shall enter into a maintenance agreement with the City, based on the Standard City of Oakland Stormwater Treatment Measures Maintenance Agreement, in accordance with Provision C.3, which provides, in part, for the following: i. The project applicant accepting responsibility for the adequate installation/construction, operation, maintenance, inspection, and			management plan, maintenance agreement and compliance with Provision C.3 Requirements of NPDES permit.	

Table 1: Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

Standard Conditions of Approval/Mitigation Measures	Implementation Responsibility/ Action	Timing	Monitoring Responsibility/Action	Date Completed/ Signature
reporting of any on-site stormwater treatment measures being incorporated into the project until the responsibility is legally transferred to another entity; and ii. Legal access to the on-site stormwater treatment measures for representatives of the City, the local vector control district, and staff of the Regional Water Board, San Francisco Region, for the purpose of verifying the implementation, operation, and maintenance of the on-site stormwater treatment measures and to take corrective action if necessary. The maintenance agreement shall be recorded at the County Recorder's Office at the applicant's expense. When Required: Prior to building permit final Initial Approval: Bureau of Building Monitoring/Inspection: Bureau of Building				

Source: Baseline Environmental Consulting, 2020.

ATTACHMENT D

HERDMAN
ARCHITECTURE + DESIGN

5441 INTERNATIONAL BLVD
OAKLAND, CA

04.02.2020
H-A+D JOB NO: A17-2096

HERDMAN
ARCHITECTURE + DESIGN

5441 INTERNATIONAL BLVD
OAKLAND, CA

04.02.2020
H-A+D JOB NO: A17-2096

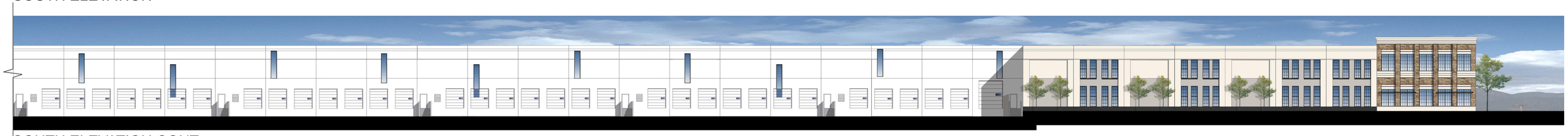
EAST ELEVATION

WEST ELEVATION

NORTH ELEVATION

NORTH ELEVATION CONT.

SOUTH ELEVATION



SOUTH ELEVATION CONT.

HERDMAN
ARCHITECTURE + DESIGN

BRIDGE

BRIDGE DEVELOPMENT
INTERNATIONAL BLVD - OAKLAND, CA

04.08.202
H-A+D JOB NO: A17-2096

ENLARGED VIEW @ MAIN OFFICE ENTRY

A. SW 6071:
POPULAR GRAY

B. SW 7642:
PAVESTONE

C. TAUPE BRICK

D. SOLARBAN 60
INSULATED CLEAR &
CLEAR GLAZING

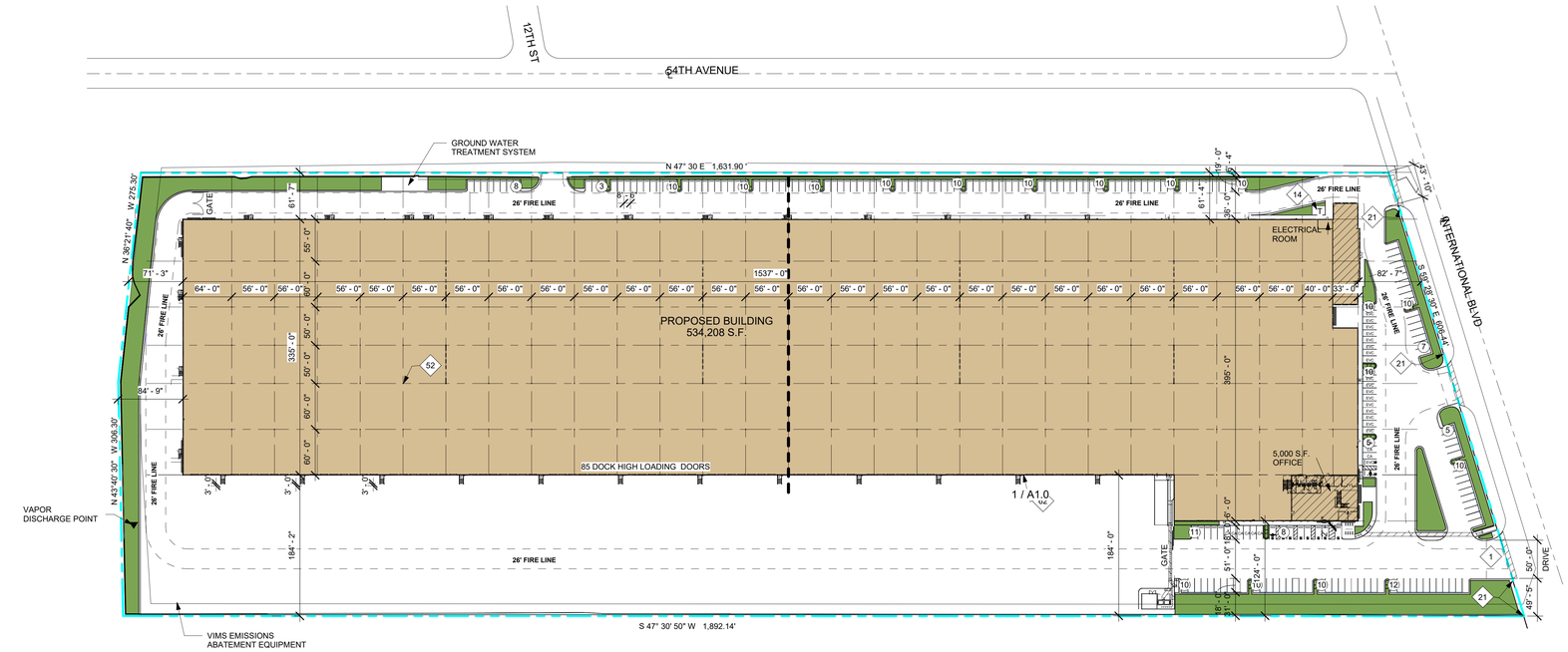
E. DECORATIVE
BREAK METAL

HERDMAN
ARCHITECTURE + DESIGN

BRIDGE

BRIDGE DEVELOPMENT
INTERNATIONAL BLVD - OAKLAND, CA

04.02.2020
H-AD JOB NO: A17-2096



2 PROPOSED SITE PLAN
1" = 80'-0"

DEVELOPER/OWNER

BRIDGE DEVELOPMENT PARTNERS, LLC.
1600 E. FRANKLIN AVE STE. D
EL SEGUNDO, CA 90245

CONTACT: BRENDAN KOTLER
PHONE: 818-674-6770
EMAIL: BKOTLER@BRIDGEDEV.COM

APPLICANT'S REPRESENTATIVE/ARCHITECT

HERDMAN ARCHITECTURE & DESIGN, INC.
16201 SCIENTIFIC WAY
IRVINE, CA 92618
CONTACT: BRIDGET HERDMAN
PHONE: 714.389.2800
EMAIL: BRIDGET@HERDMAN-AD.COM

LEGAL DESCRIPTION

ALL OF THAT LAND DESCRIBED AS PARCELS

NOTE:

ASSESSOR'S PARCEL NO: 041-3848-001-00

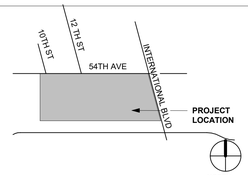
UTILITY PROVIDERS

WATER - CITY OF OAKLAND

SEWER - CITY OF OAKLAND

ELECTRIC - PG & E

VICINITY MAP



SITE LEGEND

- LANDSCAPE AREA
- CONCRETE PAVING
- STANDARD PARKING STALL PER CITY REQUIREMENT
- TYP. ACCESSIBLE PARKING STALL WITH 5' ACCESS AISLE PER CBC 2016 11B 502.2
- TYP. VAN ACCESSIBLE PARKING STALL WITH 8' ACCESS AISLE PER CBC 2016 11B 502.2
- CLEAN AIR PARKING STALL: PAINT, IN THE PAINT USED FOR STALL STRIPING, THE FOLLOWING CHARACTERS SUCH THAT THE LOWER EDGE OF THE LAST WORD ALIGNS WITH THE END OF STALL STRIPING AND IS VISIBLE BENEATH A PARKED VEHICLE:

  CLEAN AIR/
  VANPOOL/ EV

  PER CALGREEN 5.106.5.2.1
- ELECTRIC VEHICLE CHARGING STATION: EVCS STALL SHALL PROVIDE SURFACE MARKING STATING' EV CHARGING ONLY" IN LEFTTER 12" HIGH MINIMUM. THE LOWER EDGE OF THE LAST WORD ALIGNS WITH THE END OF STALL STRIPING AND IS VISIBLE BENEATH A PARKED VEHICLE

  PER CBC 2016 11B-812.9
- FIRE HYDRANT
- STREET LIGHT
- PATH OF TRAVEL
- PROPERTY LINE
- DOCK HIGH DOOR
- DRIVE THRU. DOOR

PROJECT INFORMATION

PROJECT INFORMATION			12.18.2019
GROSS SITE AREA	23.20 AC	**1,010,747 SF**	
CLEAR HEIGHT: 36' @ 6" INSIDE FIRST GRID LINE			
TOTAL BUILDING AREA		**534,208 SF**	
1ST FLOOR		529,208 SF	
WAREHOUSE		524,208 SF	
OFFICE		5,000 SF	
MEZZANINE LEVEL		5,000 SF	
OFFICE		5,000	
NET COVERAGE		**52.85%**	
MAX FAR		0.00%	
LANDSCAPE PROVIDED		**88,316 SF**	**8.74%**
LANDSCAPE REQUIRED		5.00%	
PARKING REQUIRED			
WAREHOUSE @ 1/3500		150	
1st FLOOR OFFICE @ 1/600		9	
MEZZANINE OFFICE @ 1/1,000		5	
TOTAL REQUIRED		**164**	**STALLS**
PARKING PROVIDED		**216**	**STALLS**
	STANDARD	189	AUTO
	ADA	7	AUTO
	EV	12	
	EV ADA	1	
	EV VAN ACCESSIBLE	1	
	CLEAN AIR	6	
	BICYCLE RACKS	16	
	TRAILER STALLS	0	TRAILER

KEYNOTES

1 NEW DRIVE CUT, PER CITY OR COUNTY STANDARDS. REFER TO LANDSCAPE DRAWINGS FOR ENHANCED PAVING DESIGN IF APPLICABLE. REFER TO CIVIL DRAWINGS FOR ADDITIONAL INFO.
14 PROPOSED TRANSFORMER LOCATION, PROVIDE BOLLARDS PER UTILITY COMPANY REQUIREMENTS. SEE ELECTRICAL DRAWINGS FOR ADDITIONAL INFORMATION.
21 METAL TUBE STEEL FENCE, 6' HIGH
52 STRUCTURAL BUILDING COLUMN.
62 AIR INTAKE LOUVER. PAINT TO MATCH BUILDING WALL, TYP. SIZE HORIZONTAL 4'X 8', PROVIDE BIRD SCREEN, FILTER AND BURGLAR BARS.

SCOPE OF WORK

NEW SPECULATIVE CONCRETE TILT-UP BUILDING FOR WAREHOUSE & OFFICE USE. INCLUDES SITE IMPROVEMENTS AS SHOWN.

SHEET INDEX

SITE PLAN GENERAL NOTES

1. SITE PLAN SHALL MEET ALL ENGINEERING & NPDES REQUIREMENTS.
2. GENERAL CONTRACTOR TO REVIEW SOILS REPORT PREPARED BY_____ DATED_____AND ANY SUBSEQUENT AMENDMENTS. G.C. TO CONFIRM COMPLIANCE.
3. REFER TO CIVIL DRAWINGS FOR ADDITIONAL UTILITY INFORMATION INCLUDING POINTS OF CONNECTION TO OFFSITE UTILITIES AND BUILDING POINTS OF CONNECTION.
4. GENERAL CONTRACTOR TO COORDINATE ALL POINTS OF CONNECTION BETWEEN OFFSITES, CIVIL, M,E,P, & FP DRAWINGS.
5. GRADES SURROUNDING BUILDING TO PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDING.
6. REFER TO CIVIL DRAWINGS FOR FINISH GRADE ELEVATIONS AND PERCENTAGE SLOPES.
7. GENERAL CONTRACTOR TO CONFIRM AND COMPLY WITH ALL BUILDING, FIRE, AND ENGINEERING DEPARTMENT REGULATIONS DURING CONSTRUCTION INCLUDING ANY TEMPORARY FACILITIES REQUIRED.
8. ALL PAVED AND LANDSCAPED AREAS TO BE BOUND BY 6" MIN. CONCRETE CURB TYPICAL UNLESS SPECIFICALLY NOTED OTHERWISE.
9. ALL ADA PATHS OF TRAVEL NOTED ON PLANS TO MEET THE FOLLOWING MINIMUM REQUIREMENTS: NO ABRUPT CHANGES IN ELEVATION ALLOWABLE ALONG THE PATH OF TRAVEL. THE SLOPE AND CROSS-SLOPE SHALL NOT EXCEED 5% AND 2% RESPECTIVELY UNLESS AN ADA COMPLIANT RAMP OR CURB RAMP IS DESIGNED BY THE CIVIL ENGINEER. IF A WALK CROSSES OR ADJOINS A VEHICLE WAY, AND THE WALKING SURFACES ARE NOT SEPARATED BY CURBS, RAILING OR OTHER ELEMENTS BETWEEN THE PEDESTRIAN AREAS AND VEHICULAR AREAS; THE BOUNDARY BETWEEN THE AREAS SHALL BE DEFINED BY A 4' DEEP DETECTABLE WARNING WHICH IS 36" WIDE COMPLYING WITH CBC SECTION 11B-705.1.2.5
10. ALL SPECIFICATIONS ON DRAWINGS ARE MINIMUM REQUIREMENTS ONLY. GENERAL CONTRACTOR TO NOTIFY ARCHITECT IN WRITING OF ANY CONFLICTS IN DRAWINGS AND SPECIFICATIONS VIA 'RFI'.
11. GENERAL CONTRACTOR TO REFER TO ARCHITECTURAL DETAIL SHEETS FOR TYPICAL MINIMUM SITE IMPROVEMENT STANDARDS.
12. CONCRETE MOW STRIP PER ARCHITECTURAL DETAILS TO BE PROVIDED AT ALL GLAZING/STOREFRONT LOCATIONS WHERE ADJACENT TO LANDSCAPING.
13. CONCRETE SPLASH BLOCK PER ARCHITECTURAL DETAILS TO BE PROVIDED AT ALL ROOF DRAIN/DOWN SPOUT TERMINATIONS AT NON-CONCRETE AREAS.
14. BRASS LAMB'S TONGUE TO BE PROVIDED AT ALL ROOF DRAIN OVERFLOWS THAT DAYLIGHT AT FACE OF BUILDING WALL.
15. GATES, FENCES, AND WALLS MAY BE SUBJECT TO DEFERRED SUBMITTAL REQUIREMENTS. GENERAL CONTRACTOR TO CONFIRM WITH CITY AND MUST SUBMIT SHOP DRAWINGS TO THE ARCHITECT FOR APPROVAL.

PROJECT
BRIDGE DEV. - 5441 INTERNATIONAL BLVD
OAKLAND, CA

3RD ENTITLEMENT SUBMITTAL

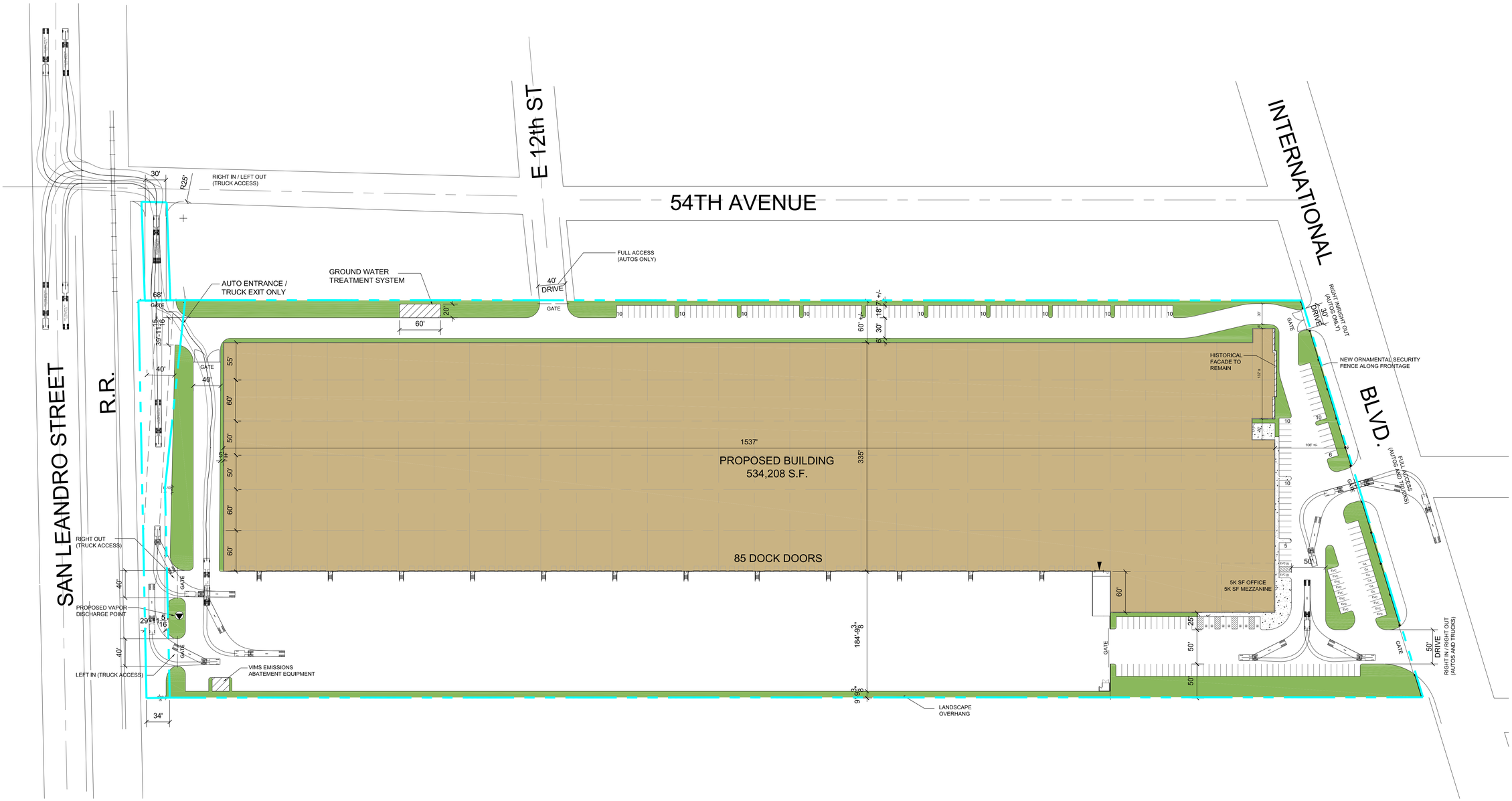
HERDMAN
ARCHITECTURE + DESIGN

A17-2096
04.08.2020

SITE PLAN

A1

4/7/2020 6:16:08 PM



A CONCEPTUAL SITE PLAN
SCALE: 1" = 60'

PROJECT INFORMATION			10.23.2019
GROSS SITE AREA	23.88 AC	**1,040,200 SF**	
CLEAR HEIGHT: 36' @ 6" INSIDE FIRST GRID LINE			
TOTAL BUILDING AREA		**534,208 SF**	
1ST FLOOR		529,208 SF	
WAREHOUSE		524,208 SF	
OFFICE		5,000 SF	
MEZZANINE LEVEL		5,000 SF	
OFFICE		5,000	
NET COVERAGE		**51.36%**	
MAX FAR		*0.00%*	
LANDSCAPE PROVIDED		**80,313 SF**	**7.72%**
LANDSCAPE REQUIRED		*5.00%*	
PARKING REQUIRED			
WAREHOUSE @ 1/3500		150	
1st FLOOR OFFICE @ 1/600		9	
MEZZANINE OFFICE @ 1/1,000		5	
TOTAL REQUIRED		**164**	**STALLS**
PARKING PROVIDED		**219**	**STALLS**
	STANDARD	192	AUTO
	ADA	7	AUTO
	EV	12	
	EV ADA	2	
	CLEAN AIR	6	
	TRAILER STALLS	0	TRAILER

PROJECT BRIDGE DEVELOPMENT
5441 INTERNATIONAL BOULEVARD, CA
SCHEME 3Q
SITE PLAN

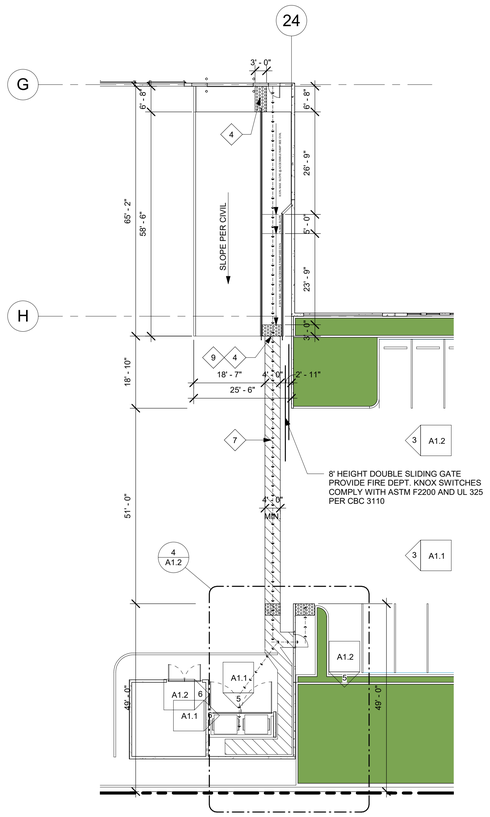
BRIDGE

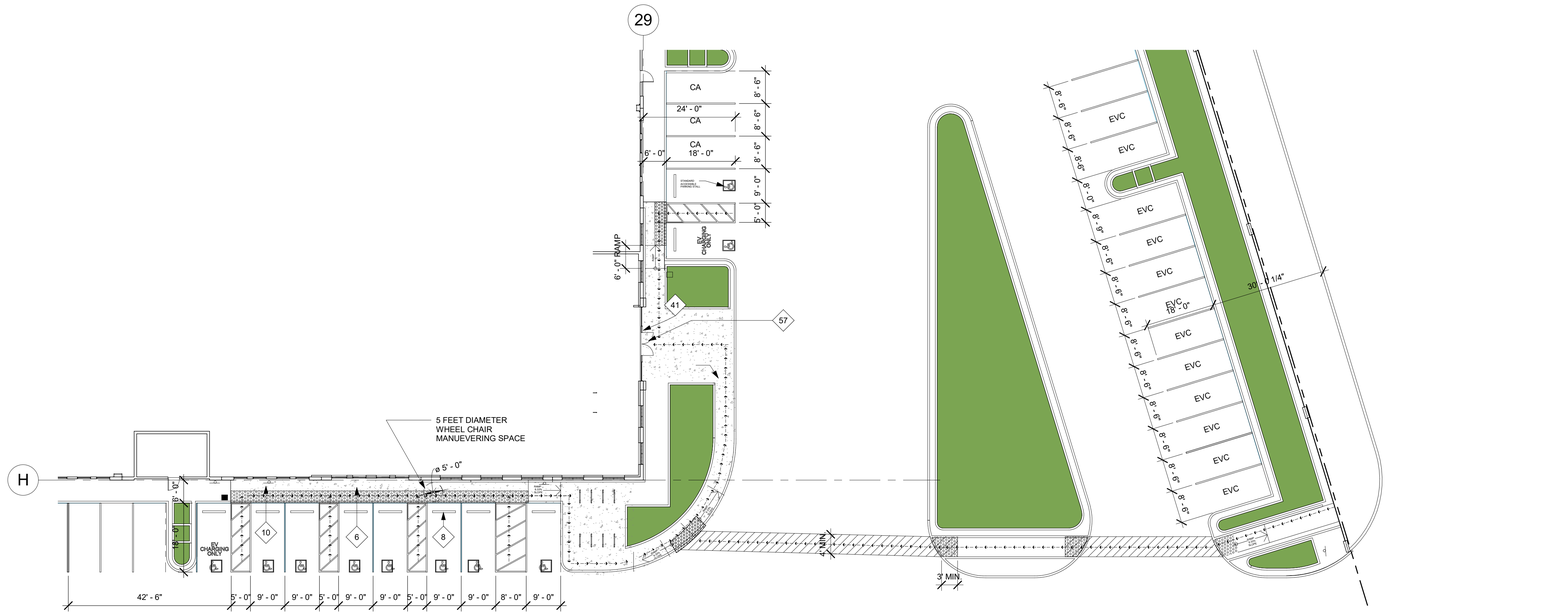
HERDMAN
ARCHITECTURE + DESIGN
16201 Scientific Way
Irvine, CA 92618
www.Herdman-AD.com
714.389.2800
info@Herdman-AD.com

A17-2096
10.23.2019

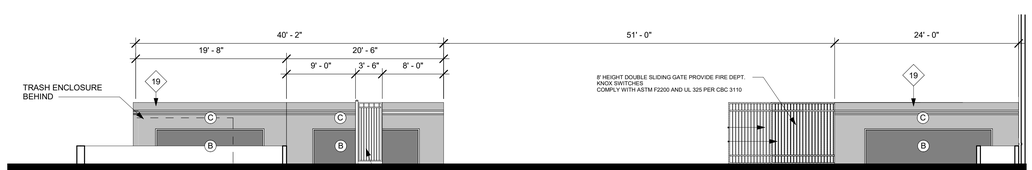
NORTH

CONCEPTUAL SITE PLAN

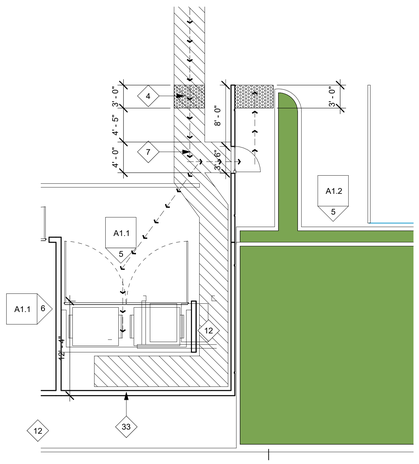




1 PROPOSED ENLARGED ACCESSIBLE PARKING
1/16" = 1'-0"

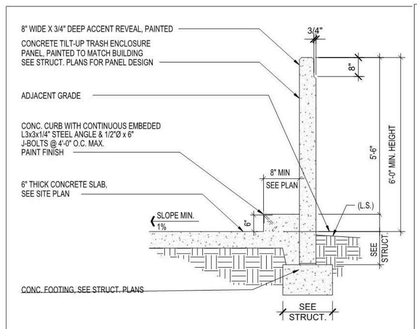


2 PRPOPOSED ENLARGED LOADING DOCK ACCESSIBLE RAMP
1/16" = 1'-0"

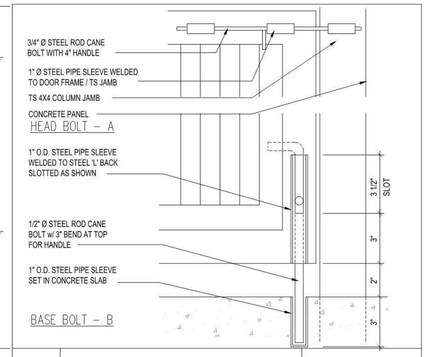


4 PROPOSED ENLARGED TRASH ENCLOSURE
1/8" = 1'-0"

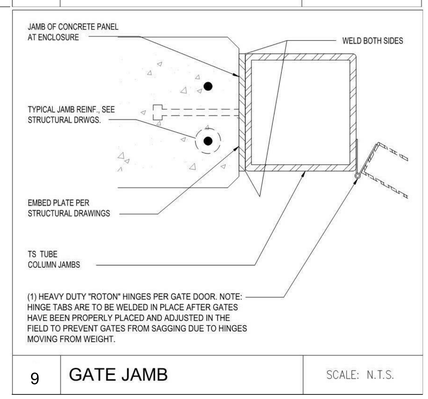
3 PROPOSED SITE WALL/ GATE EAST ELEVATION
1/8" = 1'-0"



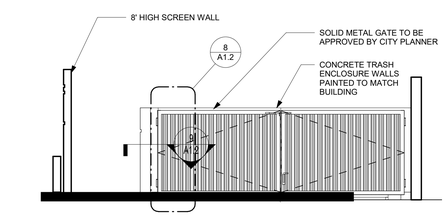
7 TRASH ENCLOSURE WALL SECTION — SCALE: N.T.S.



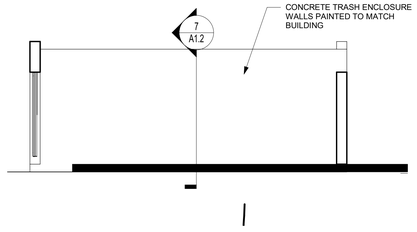
8 GATE CANE BOLT — SCALE: N.T.S.



9 GATE JAMB — SCALE: N.T.S.



5 PROPOSED TRASH ENCLOSURE FRONT ELEVATION
1/4" = 1'-0"



6 PROPOSED TRASH ENCLOSURE WEST ELEVATION
1/4" = 1'-0"

KEYNOTES

4	TRUNCATED DOMES
6	ADA PARKING STALL SIGN PER CODE, TYP. PROVIDE AT ALL ADA STALLS.
7	ADA PATH OF TRAVEL
8	PRECAST CONCRETE WHEEL STOP
9	ZERO CURB FACE.
10	CONCRETE WALK, SEE SITE PLAN FOR ADA PATH OF TRAVEL. 4" MIN THICKNESS, SCORE CONCRETE @ 5' O.C., PROVIDE A LIGHT BROOM FINISH. REFER TO LANDSCAPE DRAWINGS FOR SPECIALTY CONCRETE FINISHING, TYP. REFER TO SOILS REPORT FOR ADDITIONAL MIN. REQ.
12	LANDSCAPE AREA - SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION
19	CONCRETE SCREEN WALL.
33	CONCRETE TRASH ENCLOSURE PER CITY REQUIREMENTS.
41	AFFIX THE INTERNATIONAL ACCESSIBILITY SYMBOL AT ALL ACCESSIBLE ENTRANCES.
57	EXTERIOR STOREFRONT DOOR, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.

EXTERIOR COLOR SCHEDULE

- (A) WHITE EXTERIOR PAINT — COLOR: SW 7063 NEBULOUS WHITE
- (B) LIGHT GREY EXTERIOR PAINT — COLOR: SW7067 CITYSCAPE
- (C) DARK GREY EXTERIOR PAINT — COLOR: SW 7674 PEPPERCORN
- (D) ELDORADO TUNDRA BRICK — COLOR: IRONSIDE
- (E) STOREFRONT — SOLARBAN 60 INSULATED CLEAR GLAZING
- (F) DECORATIVE BREAK METAL TO MATCH MULLIONS

TYP PAINT NOTES:
PAINT MAN DOORS, GUARD WALLS, RAMP WALLS, STAIR WALLS, GUARD RAILS, ROOF DRAINS, AND LOUVERS TO MATCH ADJACENT BUILDING WALL U.N.O.

TRUCK DOORS TO BE PRE-FINISHED BY MANUFACTURER IN WHITE FINISH

SITE LEGEND

- LANDSCAPE AREA
- CONCRETE PAVING
- STANDARD PARKING STALL PER CITY REQUIREMENT (19', 9')
- TYP. ACCESSIBLE PARKING STALL WITH 5' ACCESS AISLE PER CBC 2016 11B 502.2 (20', 18' MIN, 9', 5')
- TYP. VAN ACCESSIBLE PARKING STALL WITH 8' ACCESS AISLE PER CBC 2016 11B 502.2 (20', 18' MIN, 8', 19', 8')
- CLEAN AIR PARKING STALL: PAINT, IN THE PAINT USED FOR STALL STRIPING, THE FOLLOWING CHARACTERS SUCH THAT THE LOWER EDGE OF THE LAST WORD ALIGNS WITH THE END OF STALL STRIPING AND IS VISIBLE BENEATH A PARKED VEHICLE:
  CLEAN AIR/
  VANPOOL/ EV
  PER CALGREEN 5.106.5.2.1
- ELECTRIC VEHICLE CHARGING STATION: EVCS STALL SHALL PROVIDE SURFACE MARKING STATING" EV CHARGING ONLY" IN LEFTTER 12" HIGH MINIMUM. THE LOWER EDGE OF THE LAST WORD ALIGNS WITH THE END OF STALL STRIPING AND IS VISIBLE BENEATH A PARKED VEHICLE (8' MIN, 5' MIN, 8' @ VAN PARKING, 12', 18" MIN.)
  PER CBC 2016 11B-812.9
- FIRE LANE
- FIRE HYDRANT
- STREET LIGHT
- PATH OF TRAVEL
- PROPERTY LINE
- DOCK HIGH DOOR
- DRIVE THRU. DOOR

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SITE DETAILS

A1.2

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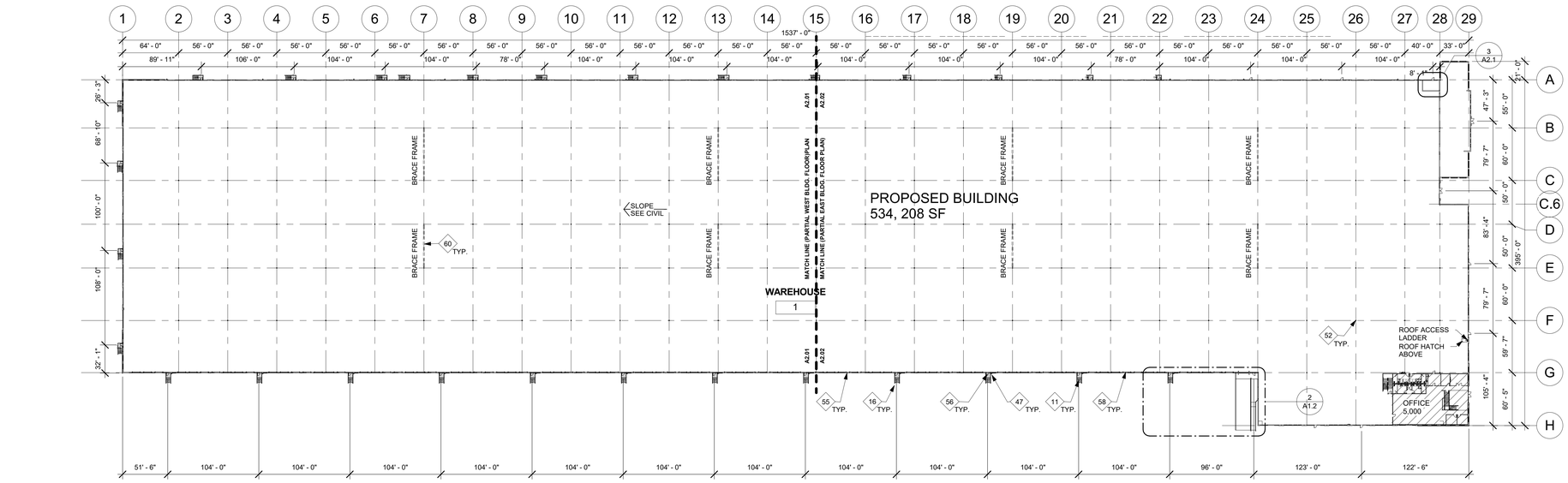
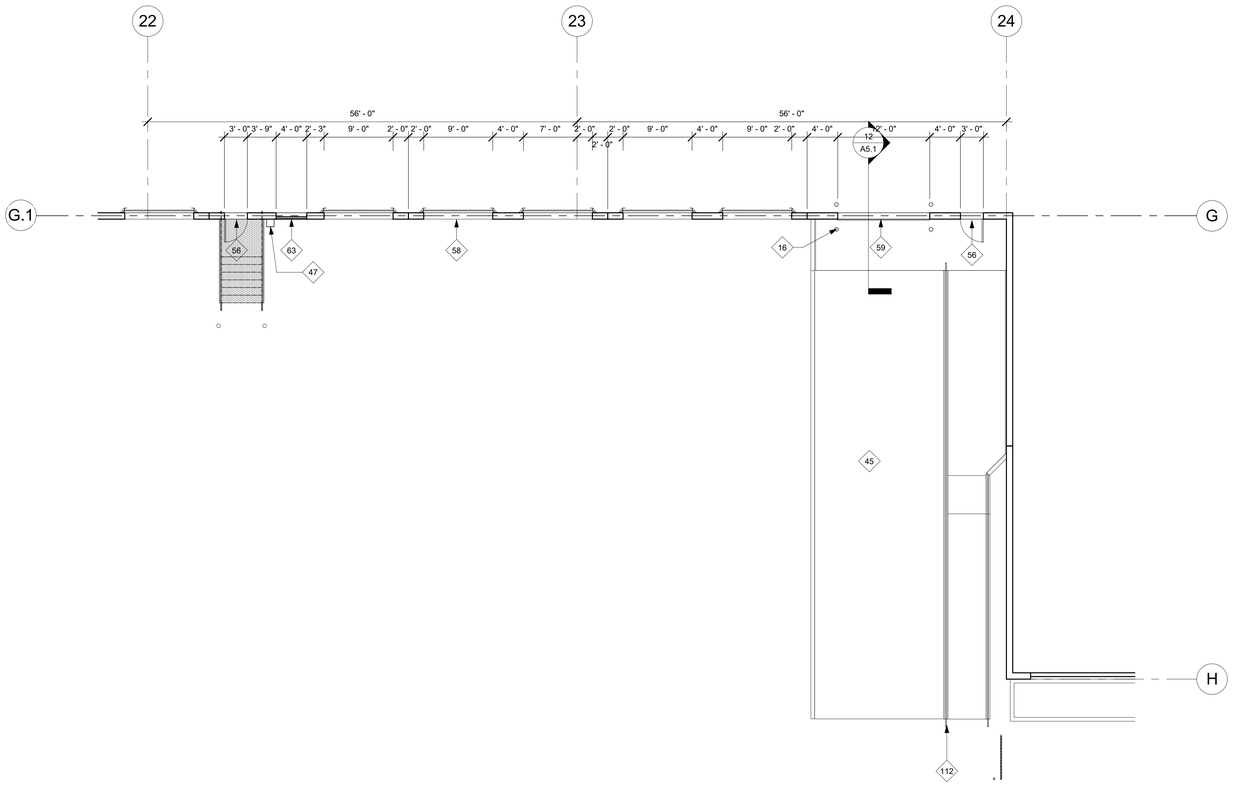


1 PROPOSED FLOOR PLAN
1" = 60'-0"



2 TYPCAL DOCK DOOR SPACING
1/8" = 1'-0"

KEYNOTES

11	EXTERIOR STEEL FRAMED EXIT STAIRS. REFER TO CIVIL AND STRUCTURAL DRAWINGS
16	PROTECTIVE METAL BOLLARDS, CONCRETE FILLED, PAINTED, TYP.
45	CONCRETE TRUCK RAMP WITH 42" HIGH CONC. TILT UP GUARD WALLS PAINTED TO MATCH BUILDING, SEE ELEVATIONS.
47	EXTERIOR METAL DOWNSPOUT AND OVERFLOW SCUPPERS PAINTED TO MATCH BUILDING. REFER TO PLUMBING PLANS FOR MINIMUM SCUPPER OPENINGS ALLOWABLE PER CODE.
52	STRUCTURAL BUILDING COLUMN.
55	CONCRETE TILT-UP PANEL, TYP. PAINTED, SEE EXTERIOR COLOR SCHEDULE. REFER TO ELEVATIONS AND "S" DRAWINGS FOR ADDITIONAL INFORMATION.
56	EXTERIOR MAN DOOR 3'X7', HOLLOW METAL, PAINTED, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.
58	DOCK-HI LOADING DOOR, 9'X10', WITH VISION GLAZING PRE FINISHED BY MANUFACTURER PER COLOR SCHEDULE.
59	DRIVE THRU LOADING DOOR 12'X14' WITH VISION GLAZING, PRE FINISHED BY MANUFACTURER PER COLOR SCHEDULE.
60	STRUCTURAL BRACE FRAME. SEE "S" DRAWINGS.
63	AIR INTAKE LOUVER. PAINT TO MATCH BUILDING WALL, TYP. SIZE VERTICAL 4'X 8'. PROVIDE BIRD SCREEN, FILTER AND BURGLAR BARS.
112	METAL HANDRAIL PAINTED PER COLOR SCHEDULE

FLOOR PLAN GENERAL NOTES

1. FINISH FLOOR SLAB SLOPES. REFER TO CIVIL DRAWINGS FOR ELEVATIONS AND ADDITIONAL INFORMATION.
2. PROVIDE STEGO WRAP 15MIL BARRIER BELOW SLAB PER MANUFACTURERS INSTRUCTIONS AND PER SOILS REPORT IN LOCATIONS FOR PROPOSED OFFICE AREAS. SEE FLOOR PLAN LEGEND FOR HATCHED AREAS.
3. REFER TO STRUCTURAL DRAWINGS FOR DESIGN OF FOUNDATION.
4. POUR STRIP TO BE SLOPED TO EXTERIOR DOORS 1/2".
5. PROVIDE FIRE EXTINGUISHERS AS REQUIRED BY FIRE DEPARTMENT AND CBC/CFC.
6. PROVIDE ILLUMINATED EXIT SIGNS AT ALL EXTERIOR EXIT DOORS, DOORS EXITING FROM TENANT SPACES, DOORS INTO EXIT ENCLOSURES, AND ANY ADDITIONAL LOCATIONS NOTED ON PLANS. SEE "E" DRAWINGS FOR ADDITIONAL REQUIREMENTS.SIGN TO BE CONTINUOUSLY ILLUMINATED FOR DURATION OF 90 MIN IN CASE OF PRIMARY POWER LOSS.
7. ALL FIRE RATED PARTITIONS TO EXTEND TO DECK ABOVE, AND PENETRATIONS TO BE SEALED.
8. DO NOT USE CURING COMPOUND OR RELEASE AGENTS TO CURE SLAB.
9. CRANES, CONCRETE TRUCKS, AND SIMILAR HEAVY EQUIPMENT PROHIBITED ON SLAB.
10. FLY-ASH PROHIBITED IN CONCRETE SLAB MIX.
11. FLOOR SLAB TO BE CLASS V PER ACI 302-IR-89
12. FLOOR COMPACTION TO BE 95% MIN
13. TRENCH COMPACTION TO BE 90% MIN
14. SLAB FINISH TO BE STEEL FLOAT HARD TROWEL BURNISHED FINISH
15. DIMENSIONS ARE TO FACE OF CONCRETE PANEL, FINISH FACE OF DRYWALL, FINISH OPENING, TYPICAL UNLESS NOTED OTHERWISE.
16. PROVIDE EXIT SIGNS INCLUDING TACTILE SIGN REQUIRED BY SECTION 1011 OF 2016 CBC. SIGN TO BE CONTINUOUSLY ILLUMINATED FOR DURATION OF 90 MIN IN CASE OF PRIMARY POWER LOSS.
17. ALL MAN DOORS, OVERHEAD DOORS, AND ROLL-UP DOORS TO BE DESIGNED FOR WIND LOAD AND EXPOSURE DETERMINED BY BUILDING CODE AND LOCAL JURISDICTION.
18. ALL STOREFRONT SYSTEMS TO BE DESIGNED FOR WIND LOAD AND EXPOSURE DETERMINED BY THE BUILDING CODE AND LOCAL JURISDICTION.STOREFRONT SYSTEMS TO BE DESIGN BUILD.G.C. TO PROVIDE SHOP DRAWINGS FOR ARCHITECT'S REVIEW
19. REFER TO CIVIL DRAWINGS FOR ALL POINT OF CONNECTIONS FOR UTILITIES CONTRACTOR TO VERIFY LOCATIONS.
20. PROVIDE STEEL BOLLARDS FILLED WITH CONCRETE AND PAINTED PER FINISH SCHEDULE AT FIRE RISERS, PIVS, TRANSFORMERS, AND OTHER LOCATIONS AS REQUIRED.
21. CONTRACTOR TO MAINTAIN A CLEAN FLOOR SLAB, ALL TRUCKS AND EQUIPMENT TO BE DIAPERED.
22. NO ACCESS HARDWARE ON THE EXTERIOR SIDE OF THE NON-ENTRY DOORS
23. FOR TYPICAL DOOR LANDING CLEARANCES, REFER 2/A0.2.2 FOR MORE INFORMATION
24. NO SMOKING WITHIN 25' OF BUILDING ENTRIES, ACCORDING TO GREEN BUILDING STANDARD CODE DIVISION 5.504.7

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FLOOR PLAN

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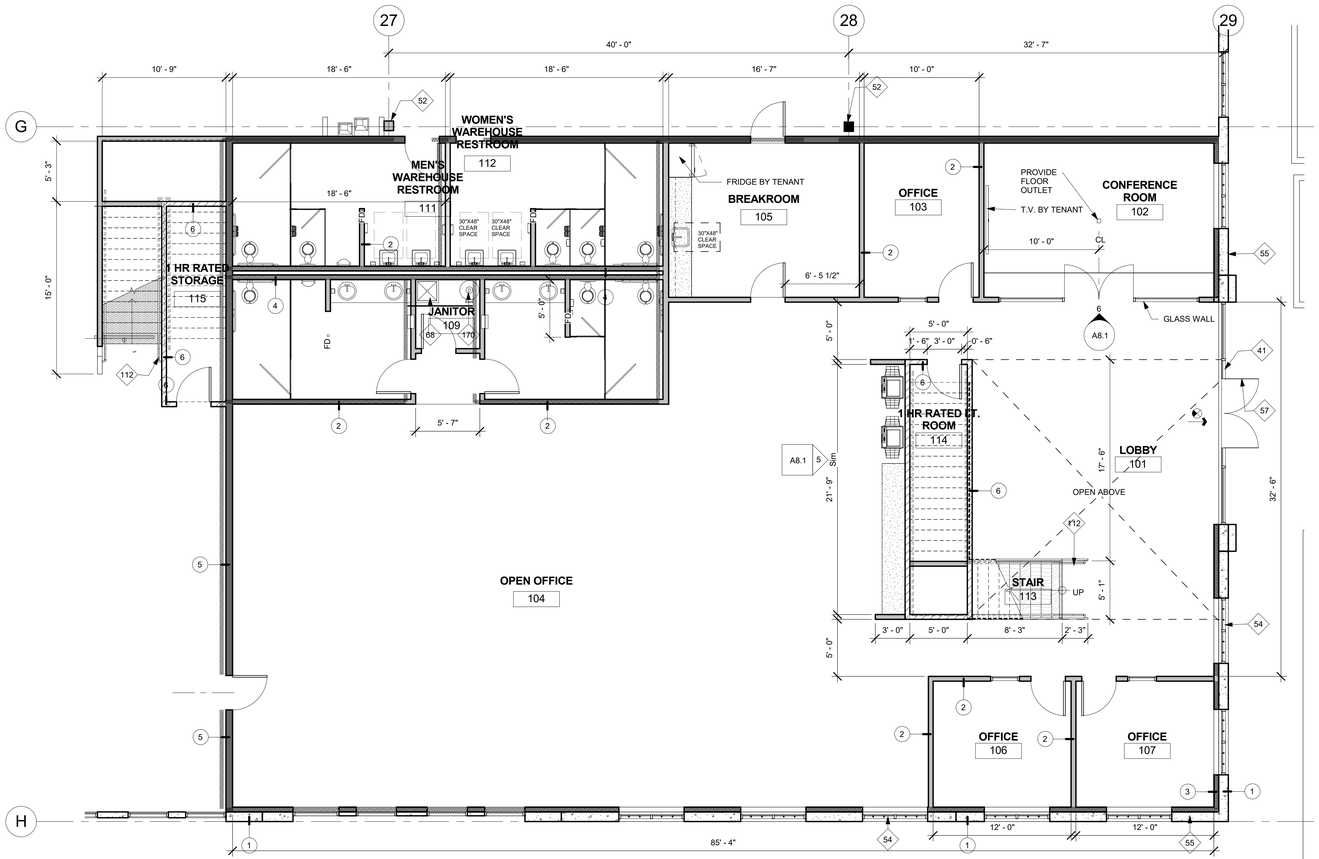
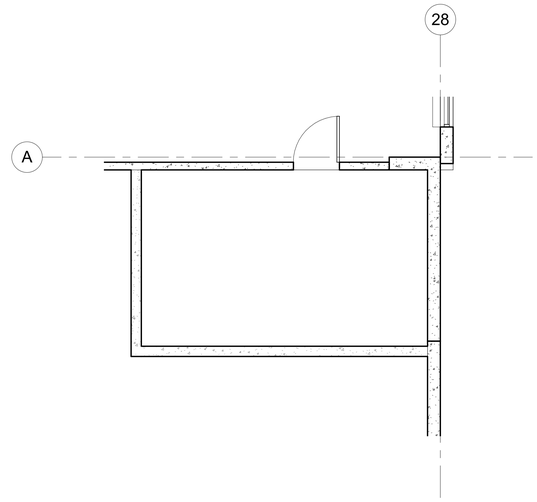
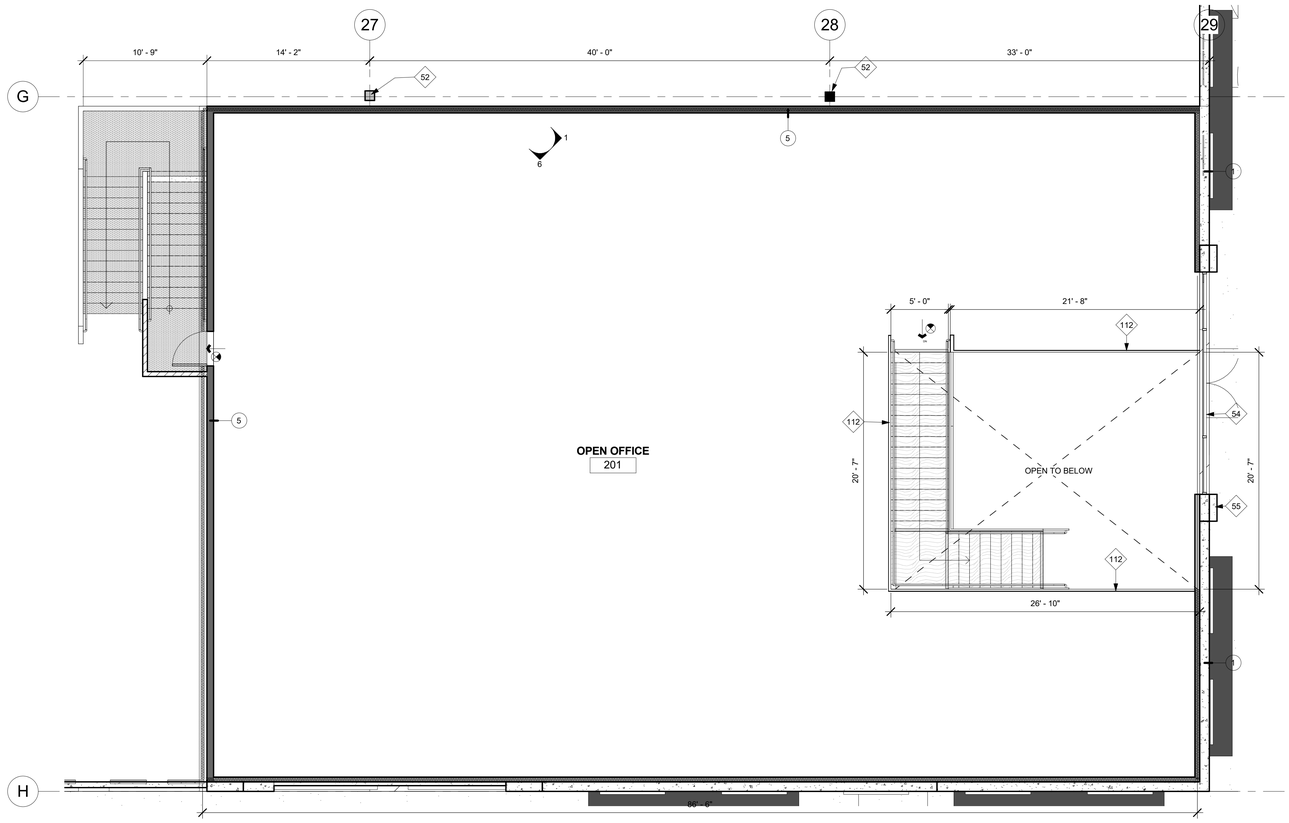


1 PROPOSED ENLARGED OFFICE PLAN
3/16" = 1'-0"



3 PROPOSED ENLARGED ELECTRICAL ROOM
1/4" = 1'-0"



2 PROPOSED ENLARGED MEZZANINE FLOOR PLAN
3/16" = 1'-0"

KEYNOTES

41	AFFIX THE INTERNATIONAL ACCESSIBILITY SYMBOL AT ALL ACCESSIBLE ENTRANCES.
52	STRUCTURAL BUILDING COLUMN.
54	STOREFRONT, SEE ELEVATIONS & EXTERIOR COLOR SCHEDULE. STORE FRONT TO BE DESIGNED TO RESIST WIND LOAD AS REQUIRED BY BUILDING CODES AND LOCAL JURISDICTION. DESIGN OF STOREFRONT FRAMING SYSTEM AND STRUCTURAL CALCULATIONS TO BE DESIGN BUILD BY G.C. AND UNDER DEFERRED SUBMITTAL.
55	CONCRETE TILT-UP PANEL, TYP. PAINTED, SEE EXTERIOR COLOR SCHEDULE. REFER TO ELEVATIONS AND "S" DRAWINGS FOR ADDITIONAL INFORMATION.
57	EXTERIOR STOREFRONT DOOR, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.
68	MOP SINK.
112	METAL HANDRAIL PAINTED PER COLOR SCHEDULE
170	WATER HEATER

FLOOR PLAN WALL LEGEND

- CONCRETE TILT UP WALL, SEE "S" DRAWINGS FOR ADDITIONAL INFORMATION. PROVIDE METAL STUD FURRING (SEE STUD SCHEDULE A0.2) AND FULL HEIGHT BATT INSULATION PER TITLE 24 REQUIREMENTS. SEE 3/A0.2 FOR CONNECTION DETAIL.
- STOREFRONT SYSTEM, UNDER DEFERRED SUBMITTAL. SEE ELEVATIONS FOR ARCHITECTURAL DETAILS
- METAL STUD WALL, SEE 13/A0.6 FOR STUD SIZE & DETALS
- METAL STUD FURRING. INSTALL INSULATION PER TITLE 24 REQUIREMENTS. PROVIDE 5/8" TYPE X GYP. BD. ON THE INTERIOR SIDE
- ONE HOUR FIRE RATED WALL. SEE WALL CALL OUTS AND STUD SCHEDULE FOR ADDITIONAL INFORMATION
- STRUCTURAL BUILDING COLUMNS
- ILLUMINATED EMERGENCY EXIT SIGN PER CBC AND FIRE DEPT. SEE "E" DRAWINGS FOR LOCATION. SIGN SHALL BE CONTINUOUSLY ILLUMINATED FOR DURATION OF 90 MIN. IN CASE OF PRIMARY POWER LOSS.

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ENLARGED OFFICE & MEZZ PLAN

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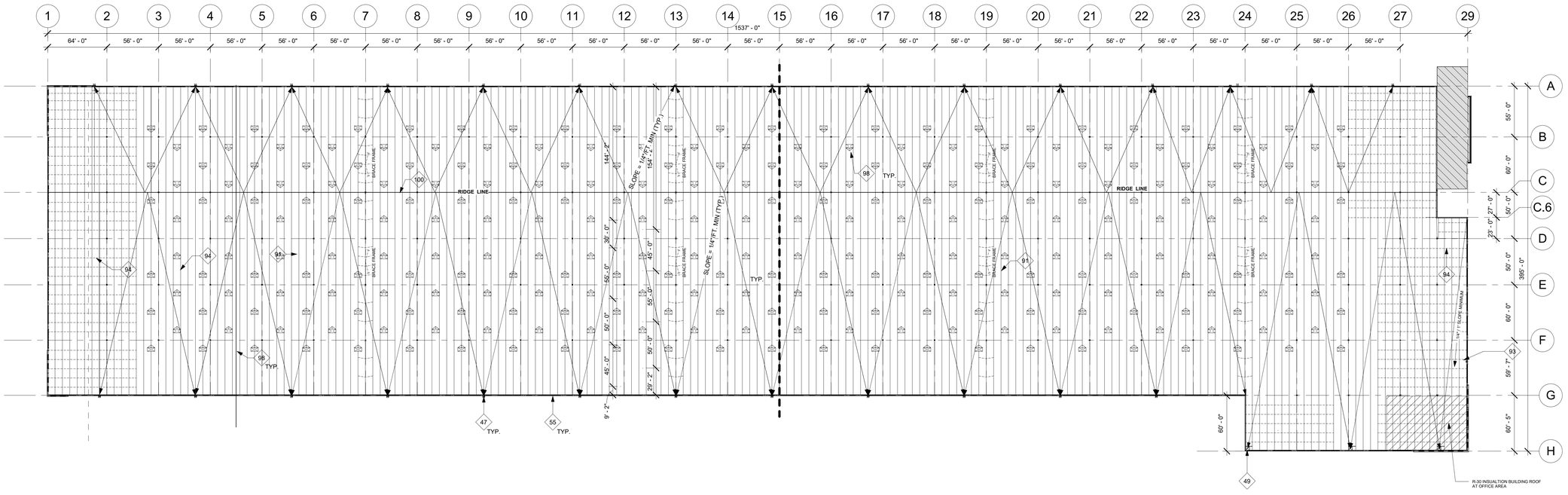

1 PROPOSED ROOF PLAN
1" = 50'-0"

ROOF PLAN LEGEND

- 4' X 8' SKYLIGHT
- 4' X 8' SMOKE HATCH
- 4' X 8' ROOF HATCH
- OFFICE AREA, LOCATION OF EQUIPMENT T.B.D.

SKYLIGHTS

PROPOSED BUILDING:
PER SECTION 110.10 - MANDATORY REQUIREMENTS FOR SOLAR READY BUILDINGS

AREA OF ROOF:	534,208 SF
AREA OF SKYLIGHTS REQUIRED 1.66%:	8,868 SF
AREA OF SKYLIGHTS PROVIDED:	9,024 SF

ORIENTATION: 110 DEGREES AND 270 DEGREES OF TRUE NORTH
SKYLIGHTS: BRISTOLITE ESR - 3177

KEYNOTES

47	EXTERIOR METAL DOWNSPOUT AND OVERFLOW SCUPPERS PAINTED TO MATCH BUILDING. REFER TO PLUMBING PLANS FOR MINIMUM SCUPPER OPENINGS ALLOWABLE PER CODE.
49	INTERIOR ROOF DRAIN AND OVERFLOW SCUPPERS PAINTED TO MATCH BUILDING. REFER TO PLUMBING PLANS FOR MINIMUM SCUPPER OPENINGS ALLOWABLE PER CODE.
55	CONCRETE TILT-UP PANEL, TYP. PAINTED, SEE EXTERIOR COLOR SCHEDULE. REFER TO ELEVATIONS AND "S" DRAWINGS FOR ADDITIONAL INFORMATION.
91	4-PLY BUILT UP ROOFING CLASS "A". REFER TO PROJECT SPECIFICATIONS FOR ADDITIONAL INFORMATION. VERIFY WARRANTY REQUIREMENTS WITH OWNER.
93	ROOF ACCESS HATCH.
94	ROOF FRAMING BELOW, TYP., REFER TO "S" DRAWINGS FOR SIZES AND ADDITIONAL INFORMATION.
98	4'X 8'SKYLIGHT.
100	RIDGE/HIGH POINT OF ROOF.

ROOF PLAN GENERAL NOTES

1. CONTRACTOR TO VERIFY POSITIVE ROOF DRAINAGE. ROOFING CONTRACTOR TO VERIFY PRIOR TO INSTALLING RIGID INSULATION OR ROOFING. SEE "S" DRAWINGS FOR CRICKETS, ETC.
2. BUILT UP ROOFING TO BE CLASS 1 UL LISTED ROOFING ASSEMBLY DESIGNED TO RESIST 90MPH OR AS REQUIRED.
3. SEE STRUCTURAL DRAWINGS FOR ROOF ELEVATIONS, TYP.
4. REFER TO DETAIL 1/AD.1 FOR TYPICAL ROOF SECTION.
5. PROVIDE CRICKETS ON (HIGH SIDE) OF ALL MECHANICAL UNITS AND ROOF EQUIPMENT AT SKYLIGHTS & SMOKE HATCHES. PROVIDE POSITIVE DRAINAGE AROUND UNITS AT 1/2" PER SLOPE MINIMUM.
6. CONTRACTOR TO COORDINATE ALL ROOF PENETRATIONS. SEE ROOF DETAIL SHEET FOR PENETRATIONS.
7. ALL SKYLIGHTS TO BE DESIGNED TO MEET WINDLOAD AS DETERMINED BY THE BUILDING CODE AND LOCAL JURISDICTION.
8. ALL MECHANICAL CONDENSATE DRAINS TO BE BELOW ROOF.
9. G.C. TO CONFIRM REQUIREMENT FOR ROOF WALK PADS WITH OWNER.
10. ROOFING CAP SHEET TO HAVE MINIMUM AGED SOLAR REFLECTANCE EQUAL TO OR GREATER THAN 0.63, AND AN SRI EQUAL TO OR GREATER THAN 72 PER 2014 COUNTY OF LOS ANGELES GREEN BUILDING STANDARDS CODE
11. ROOF ELEVATIONS TO BE VERIFIED WITH TABLE VERIFY # WITH STRUCTURAL DRAWINGS.
12. FOR ALL PIPE AND DUCT PENETRATIONS THRU ROOF, SEE DETAILS ON AD SHEETS
13. ALL CONDESATE LINES FROM HVAC UNITS MUST BE INSTALLED BELOW ROOF
14. ALL MECHANICAL EQUIPMENT WEIGHTS ARE OPERATING WEIGHTS.
15. PROVIDE A FULL TIME OSB MOISTURE INSPECTION AND GAP DISTANCE, BY A QUALIFIED ROOFING INSPECTION FIRM APPROVED BY THE OWNER AND THE OSB MANUFACTURER. INSPECTION FIRM TO BE ON SITE PRIOR TO THE START OF ANY BUILT UP ROOFING WORK.
16. ALL WOOD CURBS TO BE P.T.D.F.
17. ROOF EXHAUST FANS SHALL BE CENTERED DIRECTLY ABOVE A SPRINKLER HEAD. VERIFY WITH FIRE PROTECTION PLANS PRIOR TO INSTALLATION.
18. ALL SUB-PURLIN HANGERS HALL BE "Z-MAX" TRIPLE ZINC COATED AS MANUF. BY SIMPSON OR APPROVED EQUAL.
19. AUTOMATIC SPRINKLER SYSTEMS SERVING MORE THAN 100 SPRINKLER HEADS SHALL BE SUPERVISED BY AN APPROVED CENTRAL PROPRIETARY, OR REMOTE STATION SERVICE, OR A LOCAL ALARM WHICH WILL GIVE AN AUDIBLE SIGNAL AT CONSTANTLY ATTENDED LOCATION.

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ROOF PLAN

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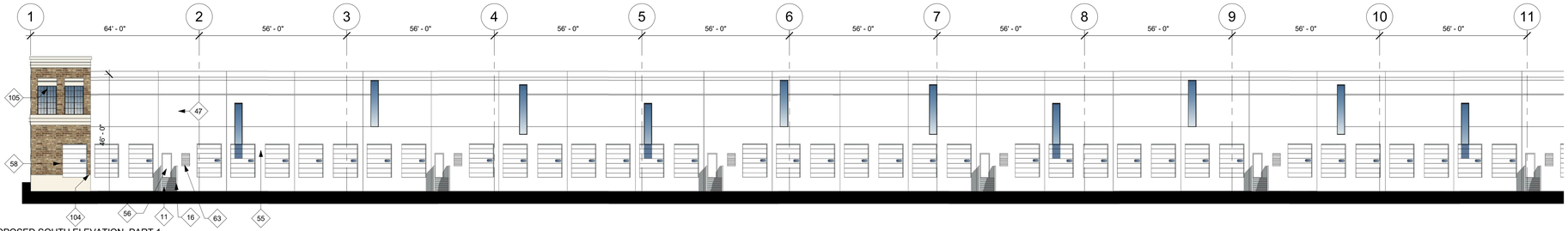
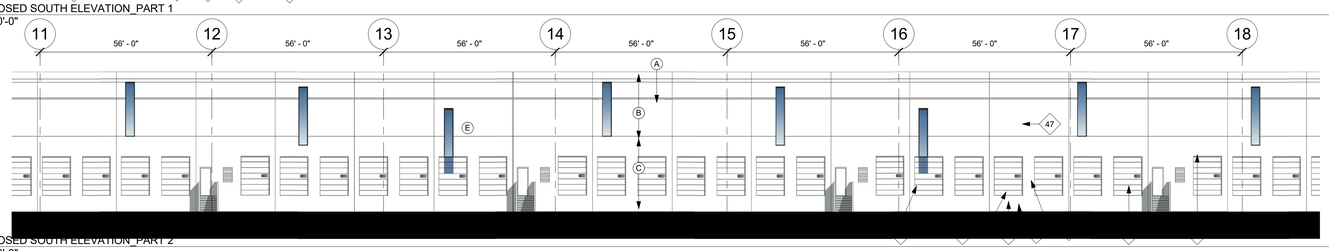
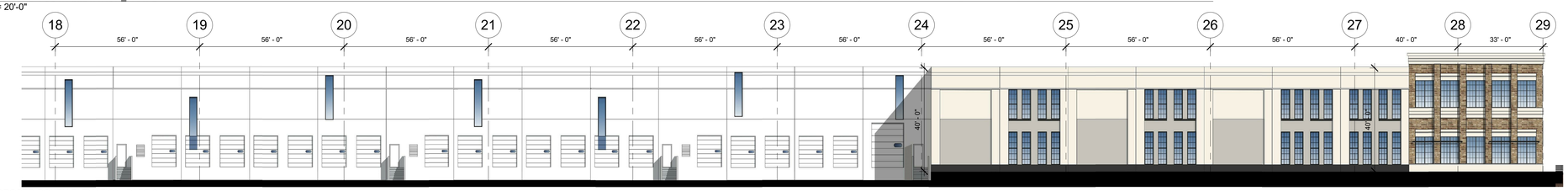


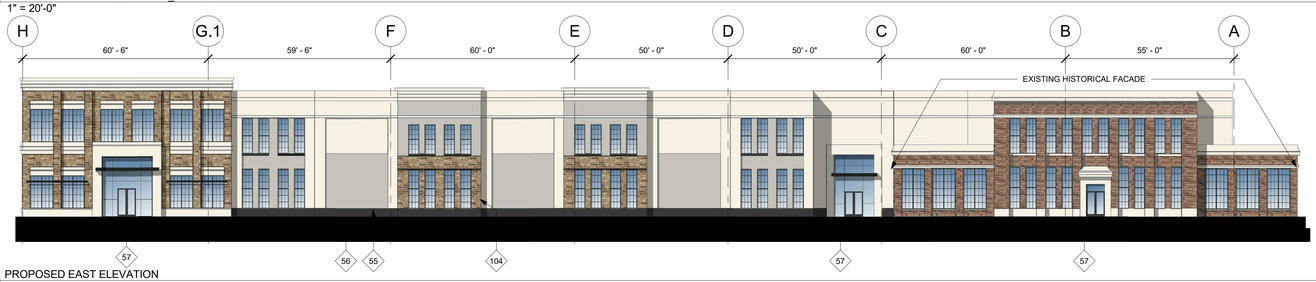
1 PROPOSED SOUTH ELEVATION_PART 1
1" = 20'-0"



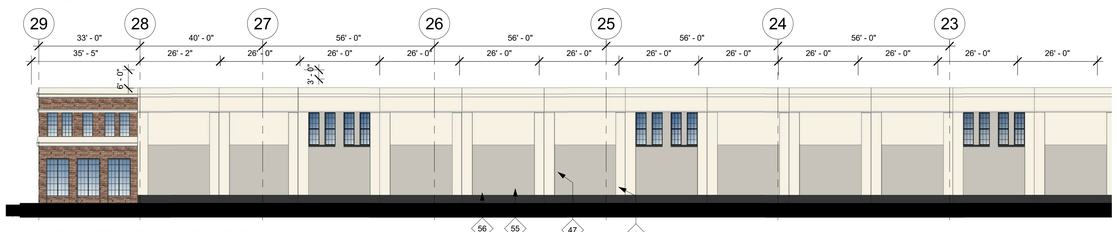
2 PROPOSED SOUTH ELEVATION_PART 2
1" = 20'-0"



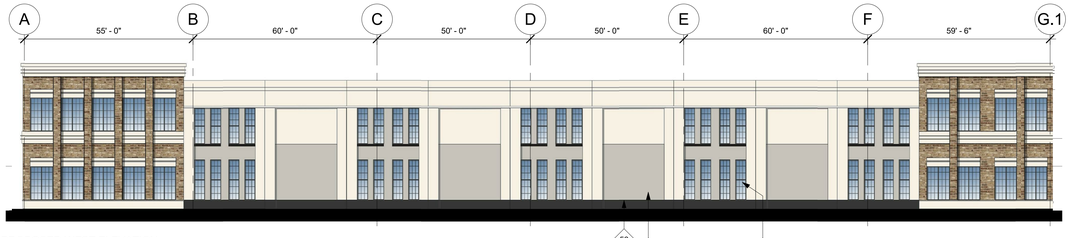
3 PROPOSED SOUTH ELEVATION_PART 3
1" = 20'-0"



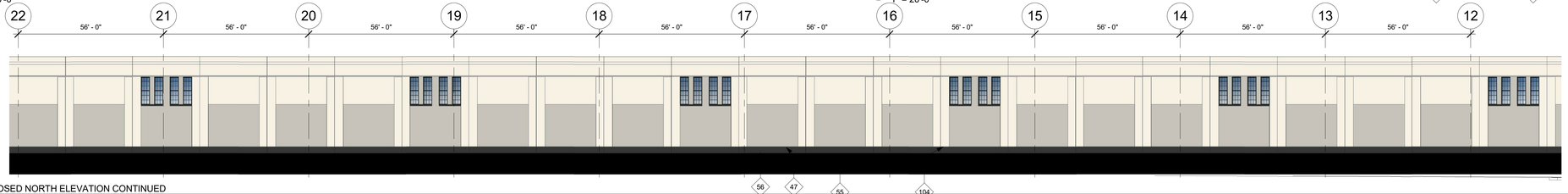
4 PROPOSED EAST ELEVATION
1" = 20'-0"



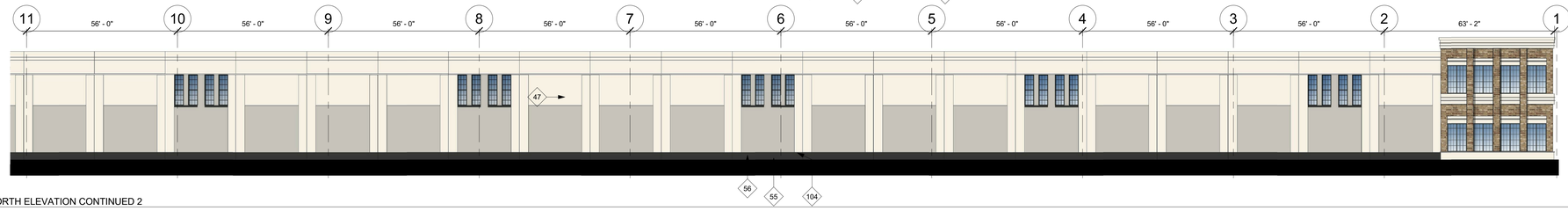
5 PROPOSED NORTH ELEVATION_PART 1
1" = 20'-0"



6 PROPOSED WEST ELEVATION
1" = 20'-0"



7 PROPOSED NORTH ELEVATION CONTINUED
1" = 20'-0"



8 PROPOSED NORTH ELEVATION CONTINUED 2
1" = 20'-0"

KEYNOTES

11	EXTERIOR STEEL FRAMED EXIT STAIRS. REFER TO CIVIL AND STRUCTURAL DRAWINGS
16	PROTECTIVE METAL BOLLARDS, CONCRETE FILLED, PAINTED, TYP.
47	EXTERIOR METAL DOWNSPOUT AND OVERFLOW SCUPPERS PAINTED TO MATCH BUILDING. REFER TO PLUMBING PLANS FOR MINIMUM SCUPPER OPENINGS ALLOWABLE PER CODE.
55	CONCRETE TILT-UP PANEL, TYP. PAINTED, SEE EXTERIOR COLOR SCHEDULE. REFER TO ELEVATIONS AND "S" DRAWINGS FOR ADDITIONAL INFORMATION.
56	EXTERIOR MAN DOOR 3'X7', HOLLOW METAL, PAINTED, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.
57	EXTERIOR STOREFRONT DOOR, SEE EXTERIOR COLOR SCHEDULE & DOOR SCHEDULE FOR ADDITIONAL INFO.
58	DOCK-HI LOADING DOOR, 9'X10', WITH VISION GLAZING PRE FINISHED BY MANUFACTURER PER COLOR SCHEDULE.
63	AIR INTAKE LOUVER. PAINT TO MATCH BUILDING WALL, TYP. SIZE VERTICAL 4'X 8', PROVIDE BIRD SCREEN, FILTER AND BURGLAR BARS.
104	PANEL JOINT, TYP.
105	2" DECORATIVE CONCRETE REVEAL WITH CHAMFERED EDGES, TYP.

GLAZING LEGEND

VISION GLAZING:

NON VISION GLAZING:

TEMPERED: T

NOTE:
REFER TO ELEVATIONS FOR TEMPERED GLAZING LOCATIONS.

NON VISION GLAZING NOTES:
1. SINGLE PANE GLAZING PAINT FACE OF CONCRETE PANEL BEHIND BLACK. NO COATING REQUIRED.
2. PROVIDE BREATHABLE MULLION SYSTEM @ NON-VISION GLAZING SECTIONS, NO HOLES REQUIRED IN CONCRETE.
3. PROVIDE SHADE CLOTH BEHIND GLASS IN AREAS INTENDED TO BE NON-VISION WHEN THERE IS NO SPANDREL CONCRETE:
TENCATE MIRAFI 140N 12.5' X 360' FILTER FABRIC

TEMPERED GLAZING NOTES:
1. IN OPERABLE DOORS, WINDOWS AND WITHIN 18" OF WALKING SURFACE TO BE TEMPERED.

EXTERIOR COLOR SCHEDULE

- (A) EXTERIOR PAINT COLOR: SW 7063 NEBULOUS WHITE
- (B) EXTERIOR PAINT COLOR: SW 6071 POPULAR GRAY
- (C) EXTERIOR PAINT COLOR: SW 7642 PAVESTONE
- (D) DARK GREY EXTERIOR PAINT COLOR: SW 7674 PEPPERCORN
- (E) ELDORADO TUNDRA BRICK COLOR: LATIGO
- (F) STOREFRONT SOLARBAN 60 INSULATED CLEAR GLAZING
- (G) DECORATIVE BREAK METAL TO MATCH MULLIONS

TYP PAINT NOTES:
PAINT MAN DOORS, GUARD WALLS, RAMP WALLS, STAIR WALLS, GUARD RAILS, ROOF DRAINS, AND LOUVERS TO MATCH ADJACENT BUILDING WALL U.N.O.

TRUCK DOORS TO BE PRE-FINISHED BY MANUFACTURER IN WHITE FINISH

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ARCHITECTURE + DESIGN

A17-2096
04.08.2020

EXTERIOR ELEVATIONS

A4

4/8/2020 11:01:14 PM

PRELIMINARY SITE IMPROVEMENT PLANS

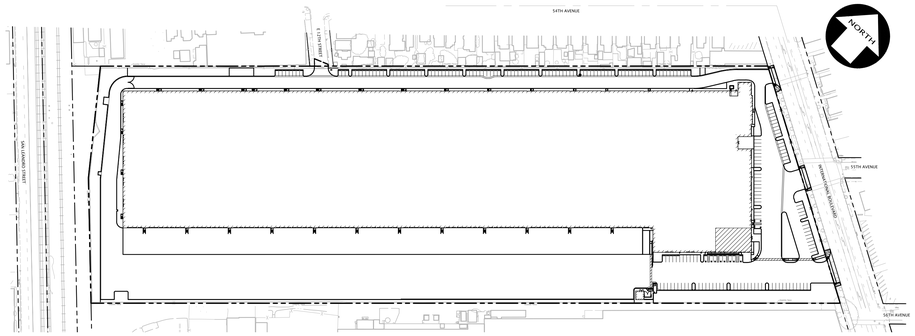
OF

5441 INTERNATIONAL BOULEVARD

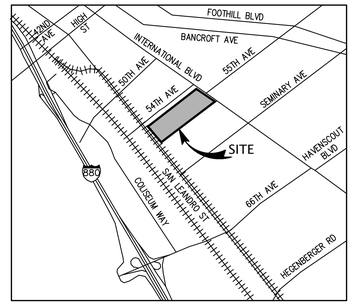
FOR

BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA



PROJECT SITE MAP



VICINITY MAP

NOT TO SCALE

SHEET INDEX

DEVELOPER

BRIDGE DEVELOPMENT PARTNERS
ATTN: BRENDAN KOTLER
1600 E. FRANKLIN AVE., SUITE D
EL SEGUNDO, CA 90245
PHONE: 213.805.6350

ARCHITECT

HERDMAN ARCHITECTURE + DESIGN
ATTN: BRIDGET HERDMAN, AIA
16201 SCIENTIFIC WAY,
IRVINE, CA 92618
PHONE: 714.389.2800

CIVIL ENGINEER

KIER & WRIGHT CIVIL ENGINEERS & SURVEYORS, INC.
ATTN: ADAM MAHONEY, P.E., L.S.
2850 COLLIER CANYON ROAD
LIVERMORE, CA 94551
PHONE: 925.245.8788

LANDSCAPE ARCHITECT

JETT LANDSCAPE ARCHITECTURE + DESIGN
ATTN: BRUCE JETT
ADDRESS: 2 THEATRE SQUARE, #218
ORINDA, CA 94563
PHONE: 925.254.5422

SOIL ENGINEER

CORNERSTONE EARTH GROUP, INC.
ATTN: DANH T. TRAN, P.E.
1259 OAKMEAD PKWY.,
SUNNYVALE, CA 94085
PHONE: 408.245.4600

Know what's **below.**
Call before you dig.

NO.	REVISION	BY

NO.	REVISION	BY
	12.06.2019 – ENTITLEMENT SUBMITTAL	
	04.17.2020 – ENTITLEMENT SUBMITTAL	

KIER & WRIGHT
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2850 Collier Canyon Road
Livermore, California 94551
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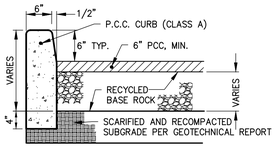
www.kierwright.com

REGISTERED PROFESSIONAL ENGINEER
JOHN A. MAHONEY
No. 62365
CIVIL
STATE OF CALIFORNIA

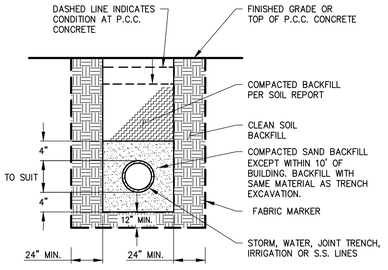
COVER SHEET
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT
OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C1.0
OF	10 SHEETS

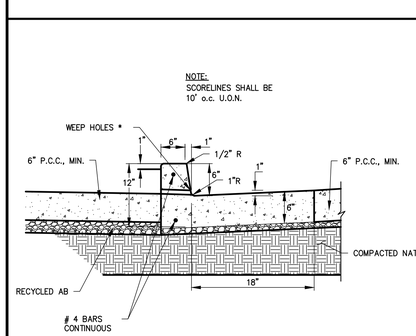


* WEEPHOLES TO BE PLACED ON ALL CURBS AND CURB & GUTTER WHERE LANDSCAPING SLOPES TOWARD CURB. WEEPHOLES SHALL BE PLACED AT 10' O.C. OR AT EACH SCORELINE OF THE CURB.

CONCRETE CURB
NOT TO SCALE — 1

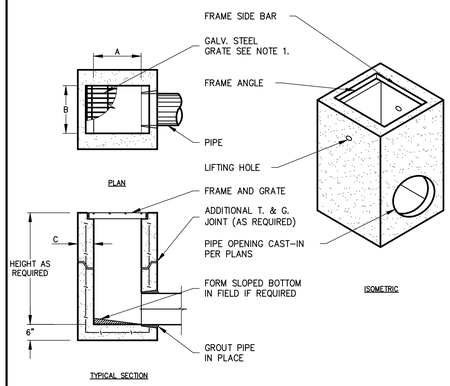


PIPE LINE BACKFILL AND BEDDING DETAIL
NOT TO SCALE — 2


NOTE:
SCORELINES SHALL BE 10' o.c. U.O.N.

* WEEPHOLES TO BE PLACED ON ALL CURBS AND CURB & GUTTER WHERE LANDSCAPING SLOPES TOWARD CURB. WEEPHOLES SHALL BE PLACED AT 10' O.C. OR AT EACH SCORELINE OF THE CURB.

CONCRETE CURB & GUTTER
NOT TO SCALE — 3

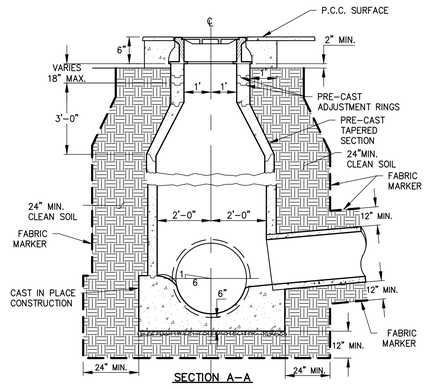


NOTES :
1. FRAMES AND GRATES MAY BE SPECIFIED FOR PEDESTRIAN OR H20 TRAFFIC LOADING. ALL GRATES ARE BICYCLE PROOF. OPTIONAL GRATE LOCKING DEVICE AVAILABLE ON REQUEST SEE DRAWING "LOCK" ON PAGE 1–7 OF THE CENTRAL PRECAST CATALOG. CLOSED–MESH GRATES OR CAST IRON FRAME AND GRATES ARE AVAILABLE ON REQUEST.
2. FOR SURFACE AND DISCHARGE OPTIONS AVAILABLE SEE DRAWING NO. "DI–50" PAGE 1–6 AND "DI–00" PAGE 1–5 OF THE CENTRAL PRECAST CATALOG.
3. FRAMES AND GRATES DETAILS SEE PAGES 1–8, 1–9, AND 1–10 OF THE CENTRAL PRECAST CATALOG.
4. WALL THICKNESSES ON ALL D.I.S. CAN BE CHANGED UPON REQUEST.
5. 18" WIDE D.I.'S REPLACE THE OLD 16" WIDE BOX BK & 1K.
6. ALL TRENCHES TO BE CAPPED PER DETAIL 2 ON THIS SHEET.

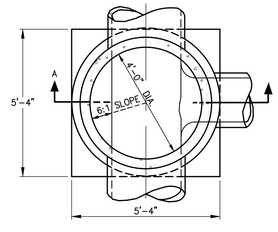
DROP INLET TABLE

MODEL No.	CPC MODEL NAME	A		B		C	
		IN	MM	IN	MM	IN	MM
CP1212	EK	12	300	12	300	4	100
CP1818	CK	18	450	18	450	5	125
CP1824	1K*	18	450	24	600	5	125
CP2424	2K	24	600	24	600	5	125
CP2430	3K	24	600	30	750	5	125
CP3030	5K	30	750	30	750	6	150
CP2436	1L	24	600	36	900	6	150
CP3636	1M	36	900	36	900	6	150
CP2448	3L	24	600	48	1200	6	150
CP3648	3M	36	900	48	1200	6	150
CP4848	1R	48	1200	48	1200	6	150

DROP INLET US CONCRETE PRECAST GROUP
NOT TO SCALE — 5



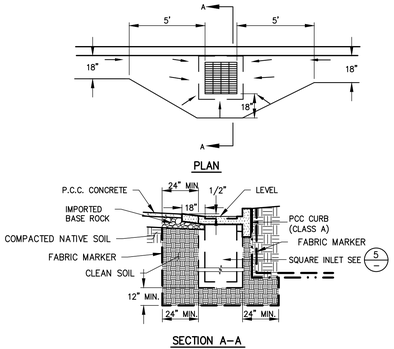
SECTION A–A



PLAN OF BASE

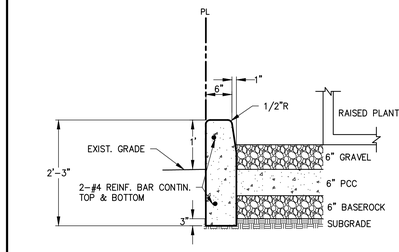
NOTES:
1. SANITARY SEWER MANHOLES SHALL BE CONSTRUCTED ACCORDING TO SPECIFICATIONS.
2. 3' I.D. MANHOLES WITH 24" HIGH CONES MAY BE USED IN CASE OF SHALLOW COVER IF PRIOR APPROVAL IS GRANTED BY THE CITY ENGINEER.
3. ALL UTILITY STRUCTURES TO BE CAPPED AS SHOWN HERE.
4. ALL PIPES WILL BE SURROUNDED WITH CLEAN IMPORT SOIL AND FABRIC MARKER PER DETAIL 2 ON THIS SHEET

STORM DRAIN MANHOLE
NOT TO SCALE — 6

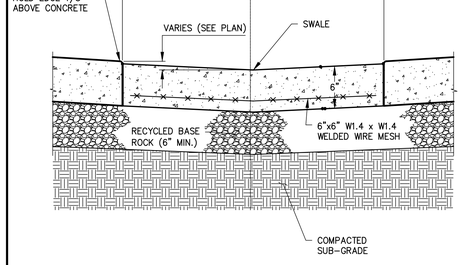


SECTION A–A

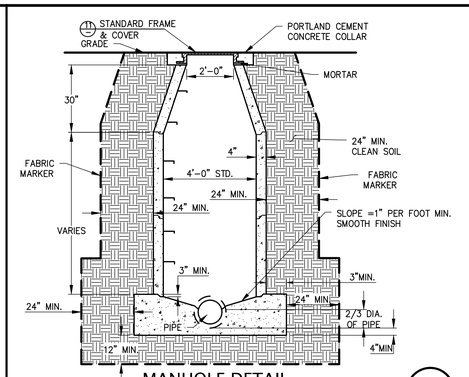
GUTTER FLARE DETAIL AT CURB
NOT TO SCALE — 4



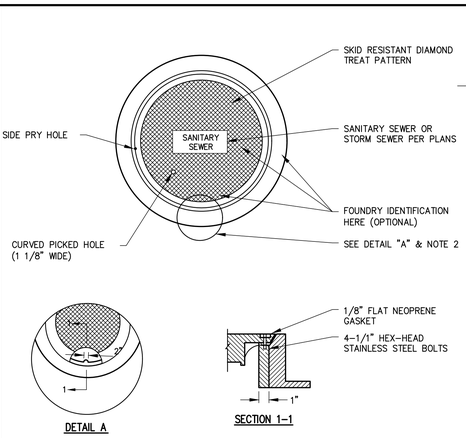
DEEPENED CURB WALL
NOT TO SCALE — 8

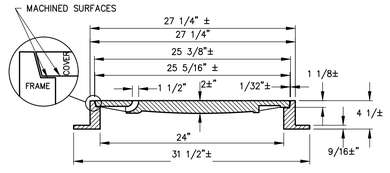


VALLEY GUTTER
NOT TO SCALE — 7



MANHOLE DETAIL
NOT TO SCALE — 10

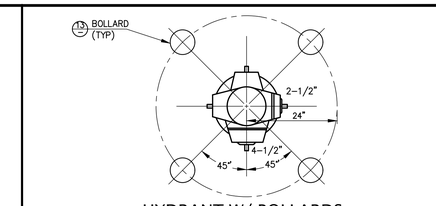




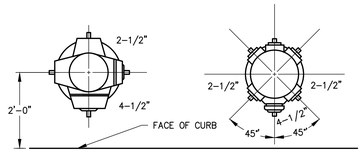
SECTION

NOTES:
1. MINIMUM WEIGHT OF SET 320 LBS.
2. ADDED BOSS TO FRAME (4 ea.@ 90°) FOR HOLDDOWN BOLTS, PRESSURE TYPE MANHOLES TO BE USED ONLY WHERE CALLED FOR ON THE PLANS.

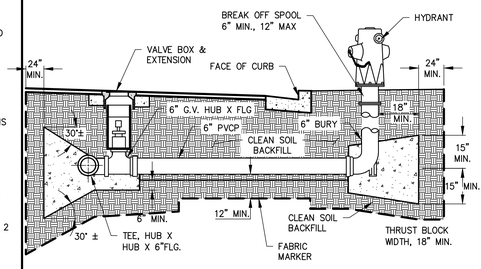
STANDARD MANHOLE FRAME AND COVER
NOT TO SCALE — 11



HYDRANT W/ BOLLARDS

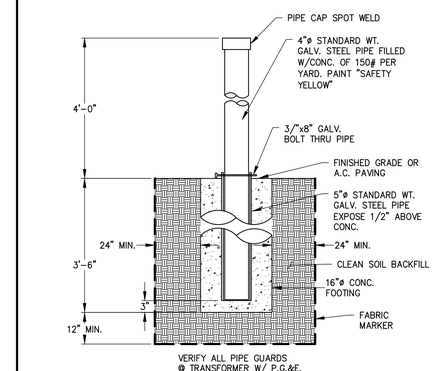


HYDRANT POSITION ON BURY

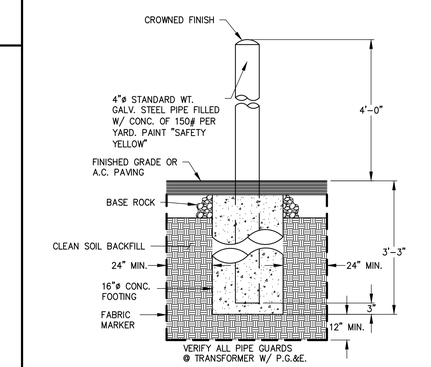


NOTE:
1. CAST CONCRETE AGAINST COMPACTED CLEAN SOIL

HYDRANT INSTALLATION
NOT TO SCALE — 12

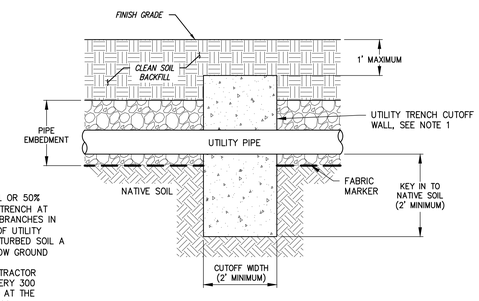


A–REMOVEABLE PIPE GUARD



B–CONC. FILLED PIPE GUARD

PIPE GUARDS
NOT TO SCALE — 13



NOTES:
1. CLSM (50% BENTONITE 3/8" PELLETS/50% NATIVE SOIL OR 50% CONCRETE/50% NATIVE SOIL) CUTOFF BARRIER INSIDE TRENCH AT 300 FOOT INTERVALS OR LESS, WITHIN 50 LF OF ALL BRANCHES IN THE DISTRIBUTION SYSTEM, AND AT ENTRANCE/EXITS OF UTILITY STRUCTURES. CUTOFF BARRIER SHALL KEY INTO UNDISTURBED SOIL A MINIMUM OF 2' AND EXTEND TO A MINIMUM OF 1' BELOW GROUND SURFACE.
2. IF A CLSM IS NOT USED TO BACKFILL A TRENCH, CONTRACTOR SHALL INSTALL CUTOFF WALLS WITHIN THE TRENCH EVERY 300 LINEAR FEET, WITHIN 50 FEET OF ANY BRANCHES AND AT THE ENTRANCES/EXITS TO UTILITY STRUCTURES OR BUILDINGS.
3. SEE VIMS DESIGN BY OTHERS FOR BUILDING PERIMETER UTILITY TRENCH CUTOFF WALL.

UTILITY TRENCH CUTOFF WALL
NOT TO SCALE — 9

NO.	BY	REVISION

NO.	BY	REVISION
1		12.06.2019 – ENTITLEMENT SUBMITTAL
2		04.17.2020 – ENTITLEMENT SUBMITTAL

KIER & WRIGHT
CIVIL ENGINEERS & SURVEYORS, INC.
2850 Collier Canyon Road
Livermore, California 94551
www.kierwright.com
Phone (925) 245-8788
Fax (925) 245-8796

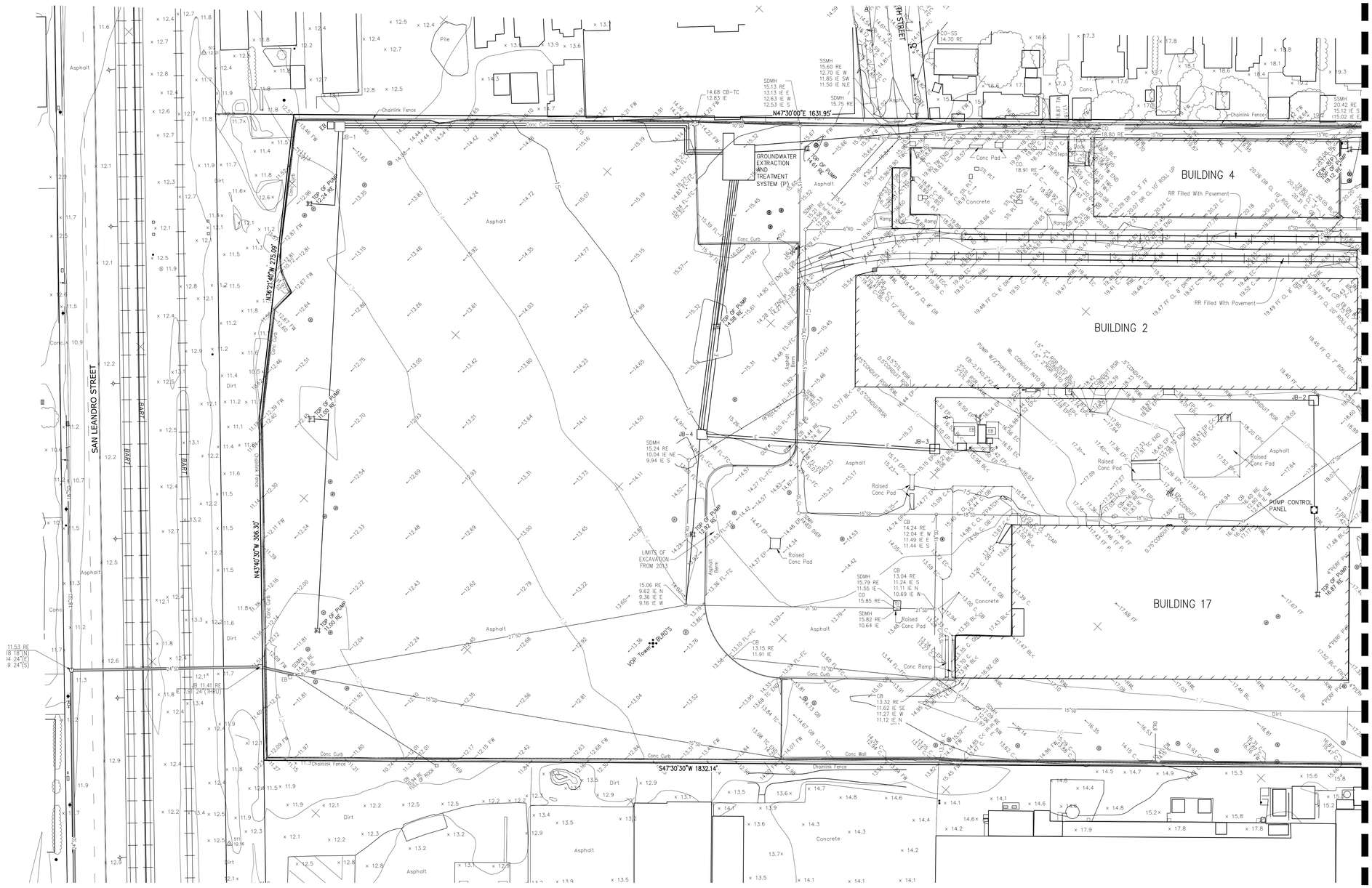
REGISTERED PROFESSIONAL ENGINEER — JOHN A. MARONEY — No. 62365 — CIVIL — STATE OF CALIFORNIA

CALIFORNIA

DETAILS OF 5441 INTERNATIONAL BOULEVARD FOR BRIDGE DEVELOPMENT

OAKLAND,

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C1.1
OF	10 SHEETS



0 20 40 80 120

Scale 1" = 40'

LEGEND

	BUILDING LINE
	CONCRETE CURB
	CONCRETE CURB & GUTTER
	CONTOUR LINE
	DRIVEWAY
	EDGE OF PAVEMENT
	FENCE LINE
	GAS METER
	GUARD RAIL
	LOT LINE
	MONUMENT/MONUMENT LINE
	PROPERTY LINE
	SANITARY SEWER-MANHOLE & CLEANOUT
	SIDEWALK
	SPOT ELEVATION
	STORM DRAIN-MANHOLE & CATCH BASIN
	BACKFLOW PREVENTION DEVICE
	ELECTROLIER
EW	GROUNDWATER EXTRACTION WELL
	FIRE DEPARTMENT CONNECTION
	FIRE HYDRANT
JB	JUNCTION BOX
	POST INDICATOR VALVE
	POWER POLE/JOINT POLE
	TRAFFIC SIGN
	TREE
	UTILITY BOX
<	ANGLE POINT
AD	AREA DRAIN
ASR	AUTOMATIC SPRINKLER RISER
BEG	BEGIN
BFP	BACK FLOW PREVENTION DEVICE
BL	BUILDING LINE
BS	BOTTOM OF STAIRS
BOTT	BOTTOM
BW	BACK OF WALK
△CNTRL	SURVEY CONTROL POINT
CB	CATCH BASIN
C	CONCRETE
CO	CLEAN OUT
DR	DOOR
DWY	DRIVEWAY
DYLT	DAY LIGHT LINE
EB	ELECTRIC BOX
EC	EDGE OF CONCRETE
EP	EDGE OF PAVEMENT
EW	EDGE OF WALK
FC	FACE OF CURB
FDC	FIRE DEPARTMENT CONNECTION
FD MON	FOUND MONUMENT
FF	FINISH FLOOR
FH	FIRE HYDRANT
FL	FLOW LINE
FNC	FENCE
FW	FACE OF WALL
GA	GUY ANCHOR
GB	GRADE BREAK
GM	GAS MARKER/METER
GRN	GROUND
GV	GAS VALVE
HCR	HANDICAP RAMP
HP	HIGH POINT
HDWL	HEADWALL
HSBIB	HOSEBIBB
IE	INVERT ELEVATION
LP	LOW POINT
LIP	LIP OF GUTTER
M-M	MONUMENT TO MONUMENT
MW	MONITORING WELL
P	PAVEMENT
PIV	POST INDICATOR VALVE
PP	POWER POLE
RE	RIM ELEVATION
SDMH	STORM DRAIN MANHOLE
SL	STREET LIGHT
SLB	STREET LIGHT BOX
SSMH	SANITARY SEWER MANHOLE
TB	TELEPHONE BOX
TC	TOP OF CURB
TR	TREE
TS	TOP OF STAIRS
TW	TOP OF WALL
UB	UNKNOWN UTILITY BOX
WB	WATER BOX
WM	WATER METER
WV	WATER VALVE

NOTES

1. THIS IS NOT A BOUNDARY SURVEY. BOUNDARY INFORMATION SHOWN HEREON WAS PREPARED FROM RECORD DATA INFORMATION. NO LIABILITY IS ASSUMED BY KIER & WRIGHT FOR THE EXISTENCE OF ANY EASEMENTS, ENCUMBRANCES, DISCREPANCIES IN BOUNDARY OR TITLE DEFECTS NOT SHOWN ON THIS DRAWING.
2. ALL DISTANCES SHOWN HEREON ARE IN FEET AND DECIMALS THEREOF.
3. BENCHMARK: USC&GS STANDARD DISC SET IN A SQUARE CONCRETE POST NEAR THE CENTERLINE OF 54TH AVENUE AND IN THE NORTHEAST SIDE OF THE RAILROAD RIGHT OF WAY IN EAST OAKLAND. NGS PID HT0003. ELEVATION = 11.50 (NAVD88)
4. BASIS OF BEARINGS: THE BEARING OF S 59°28'30" E TAKEN ON THE MONUMENT LINE OF EAST 14TH STREET AS SHOWN ON PARCEL MAP NO. 8104, FILED FOR RECORD ON FEBRUARY 5, 2003 IN BOOK 270 OF MAPS, PAGES 5 & 6, ALAMEDA COUNTY RECORDS WAS TAKEN AS THE BASIS FOR ALL BEARINGS SHOWN HEREON.
5. THE AERIAL MAPPING WAS PREPARED USING COMPUTER ASSISTED, PHOTOGRAMMETRIC METHODS BY HJW GEOSPATIAL, INC., IN OAKLAND CALIFORNIA. JOB NUMBER 10-070. IN AREAS OF DENSE VEGETATION, ACCURACY OF CONTOURS MAY DEVIATE FROM ACCEPTED ACCURACY STANDARDS. DATE OF PHOTOGRAPHY 6-21-10, ORIGINAL COMPILED MAP SCALE 1"=40', CONTOUR INTERVAL OF 1 FOOT. THE GRID IS BASED ON LOCAL ASSUMED COORDINATE SYSTEM. CONTROL SURVEY PERFORMED BY KIER & WRIGHT, PLEASANTON, CA.

PREPARED BY OR UNDER THE SUPERVISION OF JOSEPH D. THOMPSON, L.S. 8121
LICENSE EXPIRES: 12-31-20

DATE

LICENSED LAND SURVEYOR JOSEPH D. THOMPSON No. 8121 Exp. 12-31-20 STATE OF CALIFORNIA

NO.	BY	REVISION

NO.	BY	REVISION
1		12.06.2019 - ENTITLEMENT SUBMITTAL
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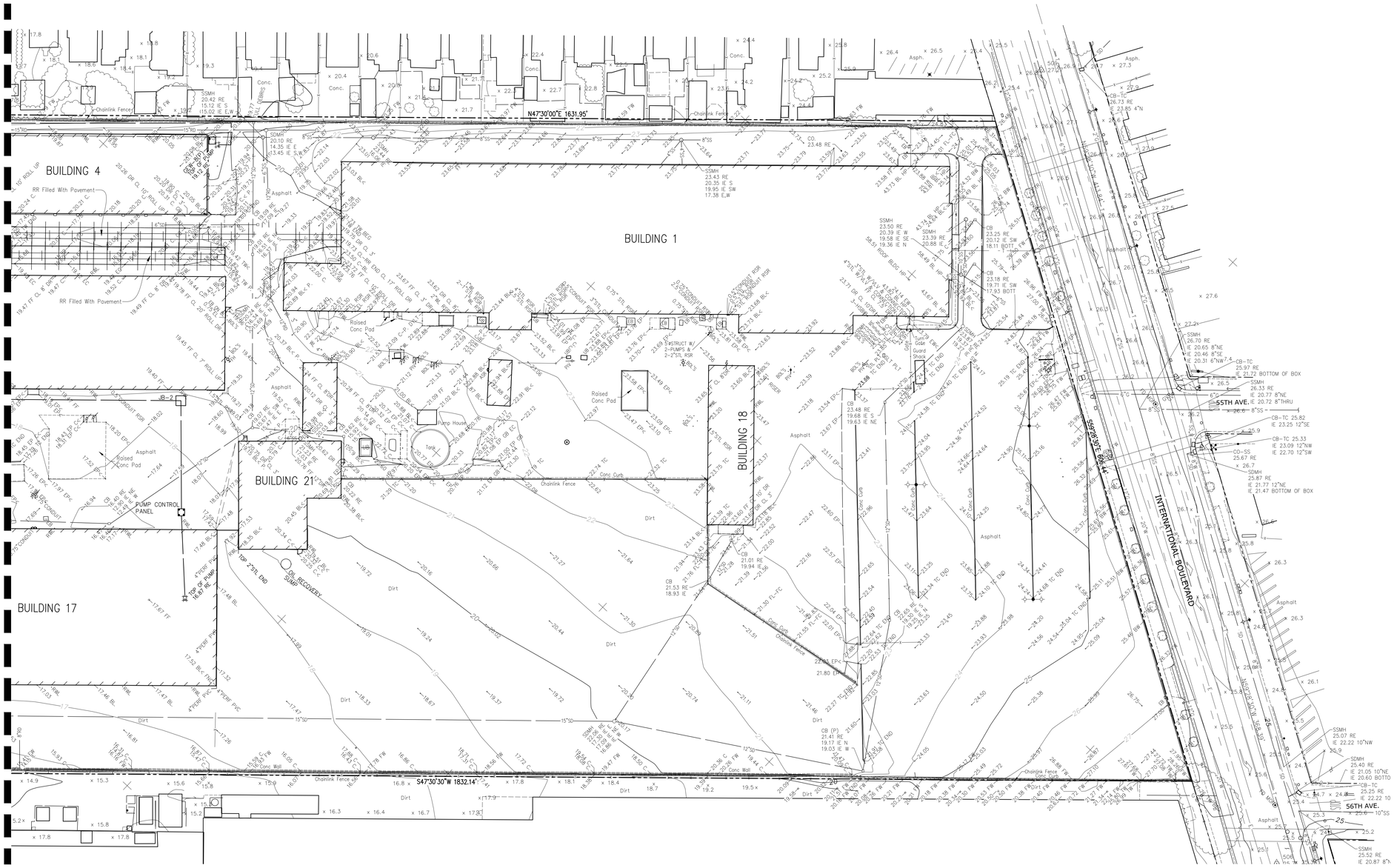
REGISTERED PROFESSIONAL ENGINEER JOHN A. MARONEY No. 62365 CIVIL STATE OF CALIFORNIA

TOPOGRAPHIC SURVEY OF 5441 INTERNATIONAL BOULEVARD FOR BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C2.0
OF	10 SHEETS



NORTH

0 20 40 80 120

Scale 1" = 40'

LEGEND

	BUILDING LINE
	CONCRETE CURB
	CONCRETE CURB & GUTTER
	CONTOUR LINE
	DRIVEWAY
	EDGE OF PAVEMENT
	FENCE LINE
	GAS METER
	GUARD RAIL
	LOT LINE
	MONUMENT/MONUMENT LINE
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	SANITARY SEWER–MANHOLE & CLEANOUT
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	SPOT ELEVATION
	STORM DRAIN–MANHOLE & CATCH BASIN
	BACKFLOW PREVENTION DEVICE
	ELECTROLIER
	GROUNDWATER EXTRACTION WELL
	FIRE DEPARTMENT CONNECTION
	FIRE HYDRANT
	JUNCTION BOX
	POST INDICATOR VALVE
	POWER POLE/JOINT POLE
	TRAFFIC SIGN
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	UTILITY BOX
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AD	AREA DRAIN
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3. BENCHMARK: USC&GS STANDARD DISC SET IN A SQUARE CONCRETE POST NEAR THE CENTERLINE OF 54TH AVENUE AND IN THE NORTHEAST SIDE OF THE RAILROAD RIGHT OF WAY IN EAST OAKLAND. NGS PID HT0003. ELEVATION = 11.50 (NAVD88)
4. BASIS OF BEARINGS: THE BEARING OF S 59°28'30" E TAKEN ON THE MONUMENT LINE OF EAST 14TH STREET AS SHOWN ON PARCEL MAP NO. 8104, FILED FOR RECORD ON FEBRUARY 5, 2003 IN BOOK 270 OF MAPS, PAGES 5 & 6, ALAMEDA COUNTY RECORDS WAS TAKEN AS THE BASIS FOR ALL BEARINGS SHOWN HEREON.
5. THE AERIAL MAPPING WAS PREPARED USING COMPUTER ASSISTED, PHOTOGRAMMETRIC METHODS BY HJW GEOSPATIAL, INC., IN OAKLAND CALIFORNIA. JOB NUMBER 10–070. IN AREAS OF DENSE VEGETATION, ACCURACY OF CONTOURS MAY DEVIATE FROM ACCEPTED ACCURACY STANDARDS. DATE OF PHOTOGRAPHY 6–21–10, ORIGINAL COMPILED MAP SCALE 1"=40', CONTOUR INTERVAL OF 1 FOOT. THE GRID IS BASED ON LOCAL ASSUMED COORDINATE SYSTEM. CONTROL SURVEY PERFORMED BY KIER & WRIGHT, PLEASANTON, CA.

PREPARED BY OR UNDER THE SUPERVISION OF JOSEPH D. THOMPSON L.S. 8121
LICENSE EXPIRES: 12–31–20

DATE

LICENSED LAND SURVEYOR JOSEPH D. THOMPSON No. 8121 Exp. 12–31–20 STATE OF CALIFORNIA

NO.	REVISION	BY

NO.	REVISION	BY
1	12.06.2019 – ENTITLEMENT SUBMITTAL	
2	04.17.2020 – ENTITLEMENT SUBMITTAL	

KIER & WRIGHT
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2850 Collier Canyon Road
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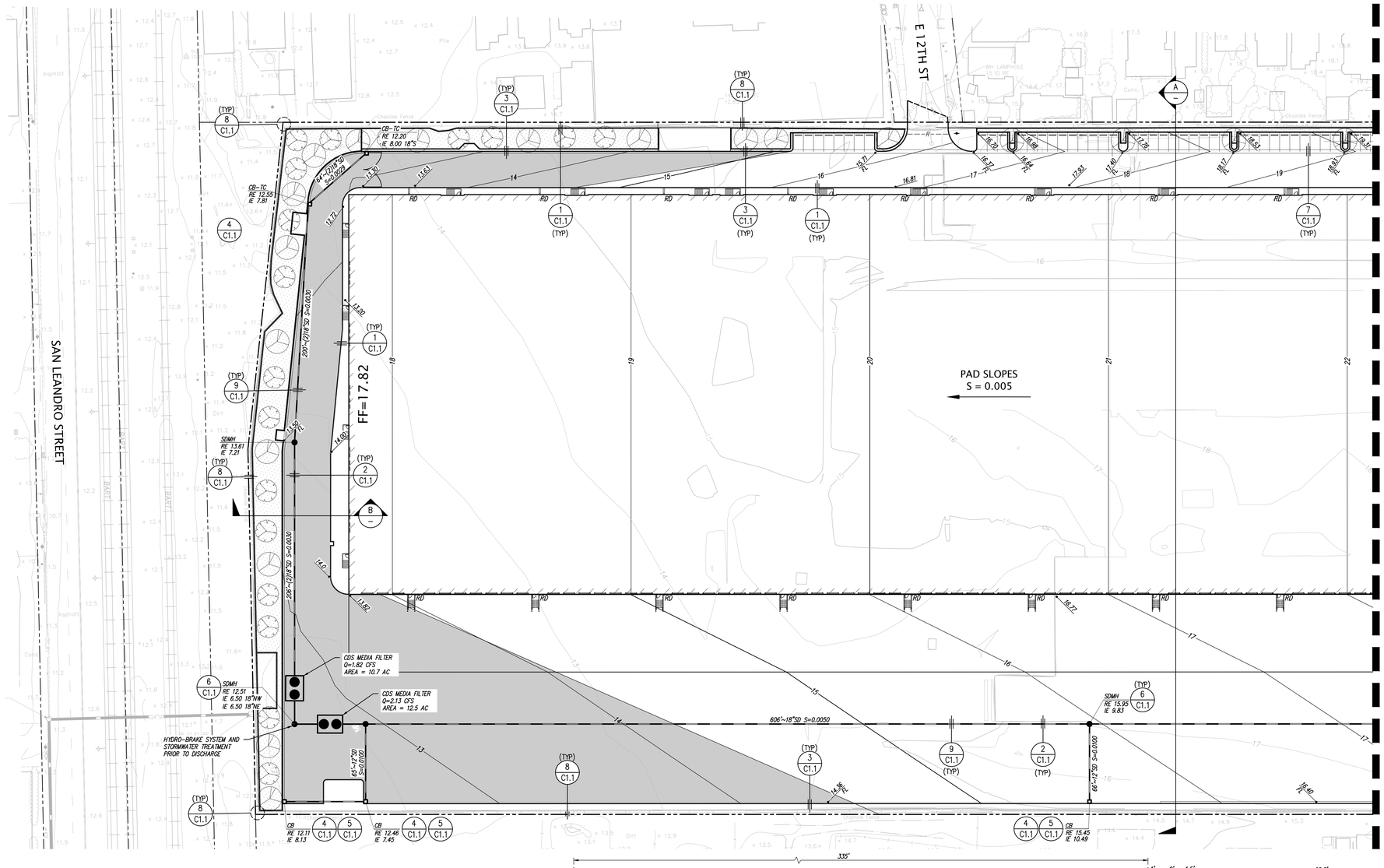
Phone (925) 245-8788
Fax (925) 245-8796
www.kierwright.com

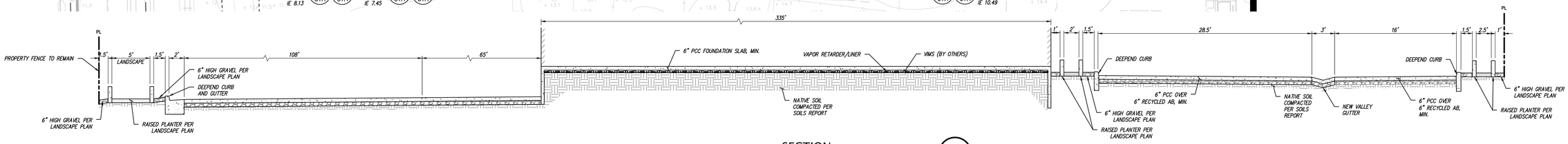
REGISTERED PROFESSIONAL ENGINEER JOHN K. MARONEY No. 62365 CIVIL STATE OF CALIFORNIA

TOPOGRAPHIC SURVEY OF 5441 INTERNATIONAL BOULEVARD FOR BRIDGE DEVELOPMENT

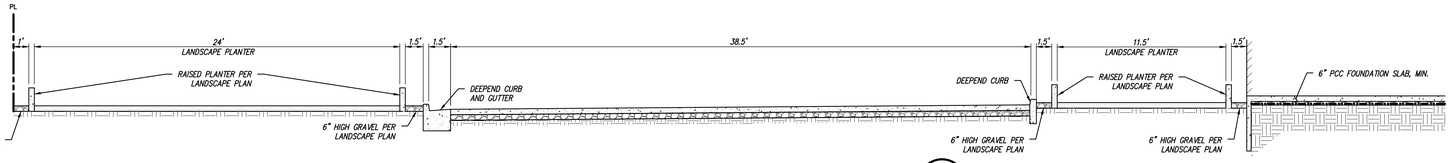
OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C2.1
OF	10 SHEETS





SECTION A
NOT TO SCALE



SECTION B
NOT TO SCALE

NORTH

0 20 40 80 120
Scale 1" = 40'

LEGEND

- STORM DRAIN CATCH BASIN
- STORM DRAIN JUNCTION BOX
- STORM DRAIN MANHOLE
- STORM DRAIN CLEANOUT
- FL FLOW LINE
- FF FINISH FLOOR
- P/V PAVEMENT
- RE RIM ELEVATION
- 23.8 SPOT ELEVATION
- X"SD STORM DRAIN LINE
- TC TOP OF CURB
- POTENTIAL SURFACE PONDING AREA (DEPTH VARIES FROM 5" TO 12")

NOTE:

1. EXISTING WALLS ARE TO REMAIN AND PROTECT IN PLAN UNTIL NEW PAVEMENT AND LANDSCAPE GRADING ARE COMPLETED AND INSTALLED.
2. STORMWATER MANAGEMENT AND SEDIMENT REMOVAL APPROACH TO BE DEVELOPED PENDING HYDRAULIC ANALYSIS TO MEET LOCAL AND STATE REQUIREMENTS. SURFACE PONDING WITH THE USE OF HYDRO-BRAKE SYSTEM SHOWN HERE IS ONE POTENTIAL ALTERNATIVE UNDER CONSIDERATION.

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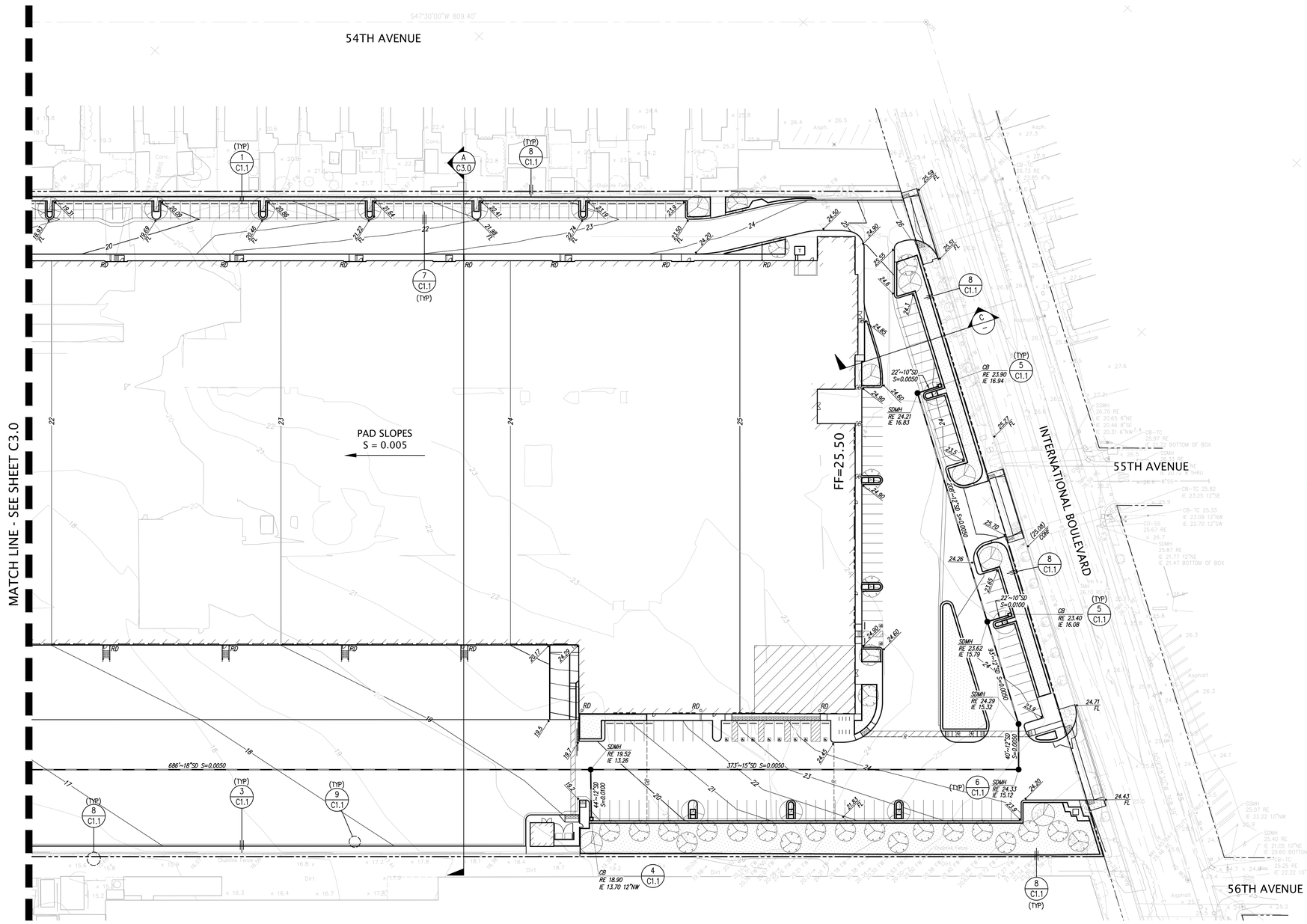
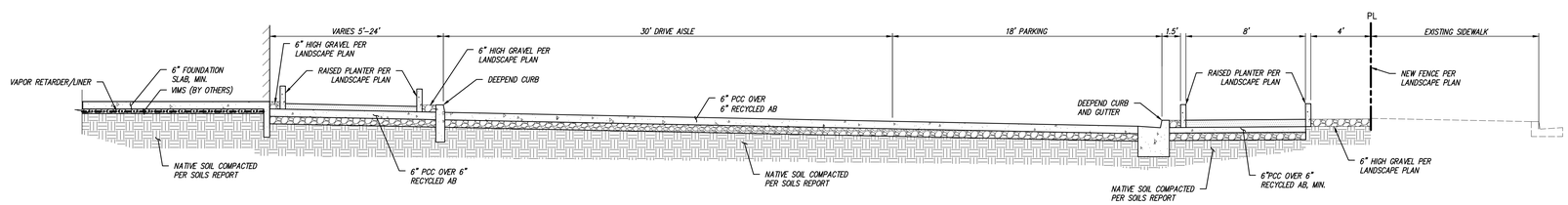
STATE OF CALIFORNIA

PRELIMINARY GRADING & DRAINAGE PLAN
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C3.0
OF	10 SHEETS





SECTION C

NORTH

0 20 40 80 120

Scale 1" = 40'

LEGEND

	STORM DRAIN CATCH BASIN
	STORM DRAIN JUNCTION BOX
	STORM DRAIN MANHOLE
	STORM DRAIN CLEANOUT
FL	FLOW LINE
FF	FINISH FLOOR
P/V	PAVEMENT
RE	RIM ELEVATION
23.8	SPOT ELEVATION
X"SD	STORM DRAIN LINE
TC	TOP OF CURB

NOTE:

1. EXISTING WALLS ARE TO REMAIN AND PROTECT IN PLAN UNTIL NEW PAVEMENT AND LANDSCAPE GRADING ARE COMPLETED AND INSTALLED.
2. STORMWATER MANAGEMENT AND SEDIMENT REMOVAL APPROACH TO BE DEVELOPED PENDING HYDRAULIC ANALYSIS TO MEET LOCAL AND STATE REQUIREMENTS. SURFACE PONDING WITH THE USE OF HYDRO-BRAKE SYSTEM SHOWN HERE IS ONE POTENTIAL ALTERNATIVE UNDER CONSIDERATION.

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PRELIMINARY GRADING & DRAINAGE PLAN
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA

DATE	APRIL, 2020
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SHEET	C3.1
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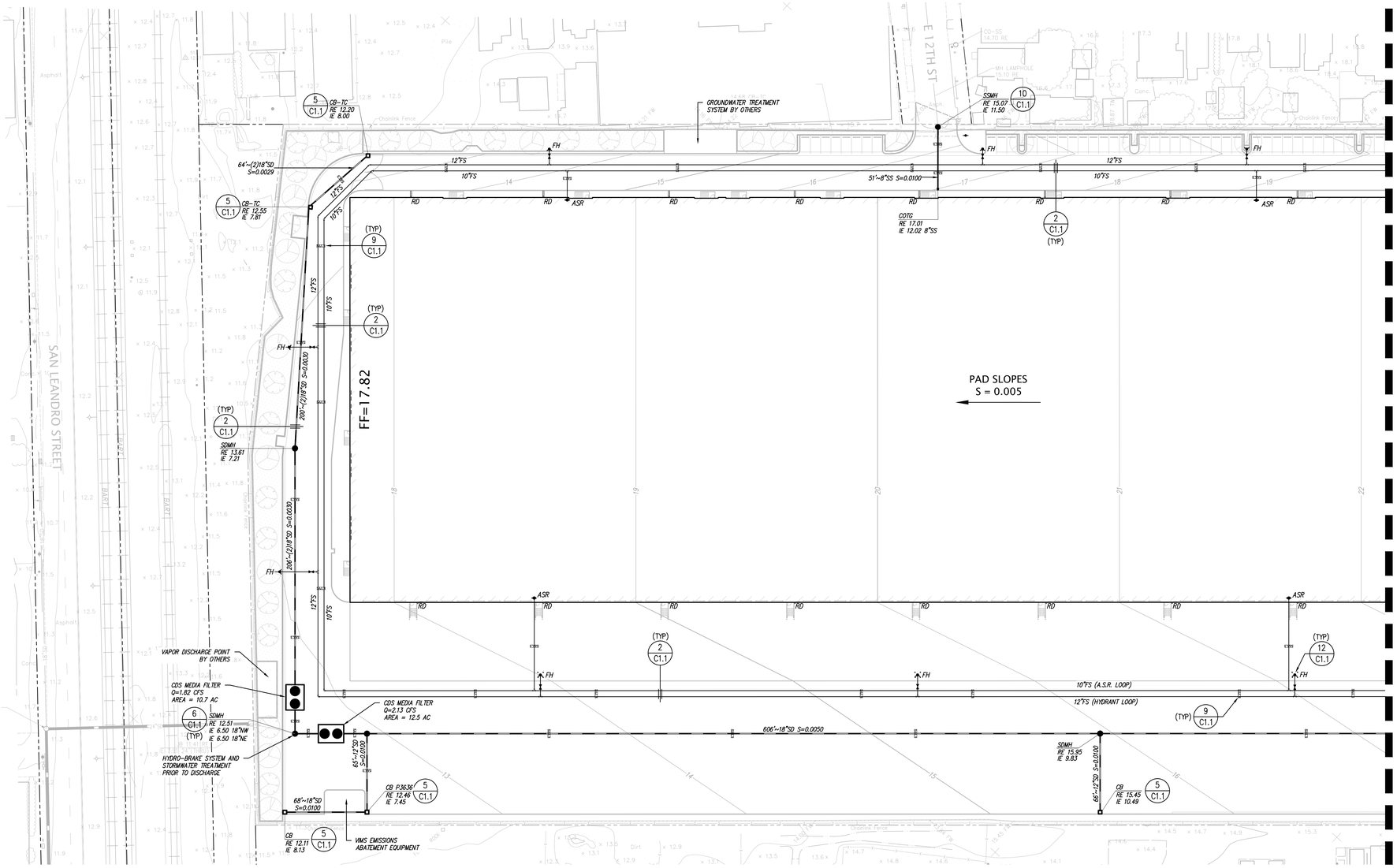

NORTH

0 20 40 80 120

Scale 1" = 40'

LEGEND

ASR	AUTOMATIC SPRINKLER RISER
RE	RIM ELEVATION
IE	INVERT ELEVATION
TC	TOP OF CURB
WS	WATER SERVICE
	EXISTING UTILITY TO BE ABANDONDED BY REMOVAL
FS	FIRE SERVICE
SS	SANITARY SEWER
COTG	CLEANOUT TO GRADE
	STORM DRAIN LINE
	STORM DRAIN CATCH BASIN
	STORM DRAIN JUNCTION BOX
	STORM DRAIN CLEANOUT
	BACK FLOW PREVENTION DEVICE
	FIRE DEPARTMENT CONNECTION
	FIRE HYDRANT & VALVE
	POST INDICATOR VALVE
SSMH	SANITARY SEWER MANHOLE
	SINGLE CHECK VALVE
SDMH	STORM DRAIN MANHOLE
	WATER METER
	TRENCH CUTOFF WALL

NOTE:

1. ONSITE IRRIGATION AND ELECTRICAL SHALL BE DESIGNED BY OTHERS

NO.	REVISION	BY

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PRELIMINARY UTILITY PLAN
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C4.0
OF	10 SHEETS

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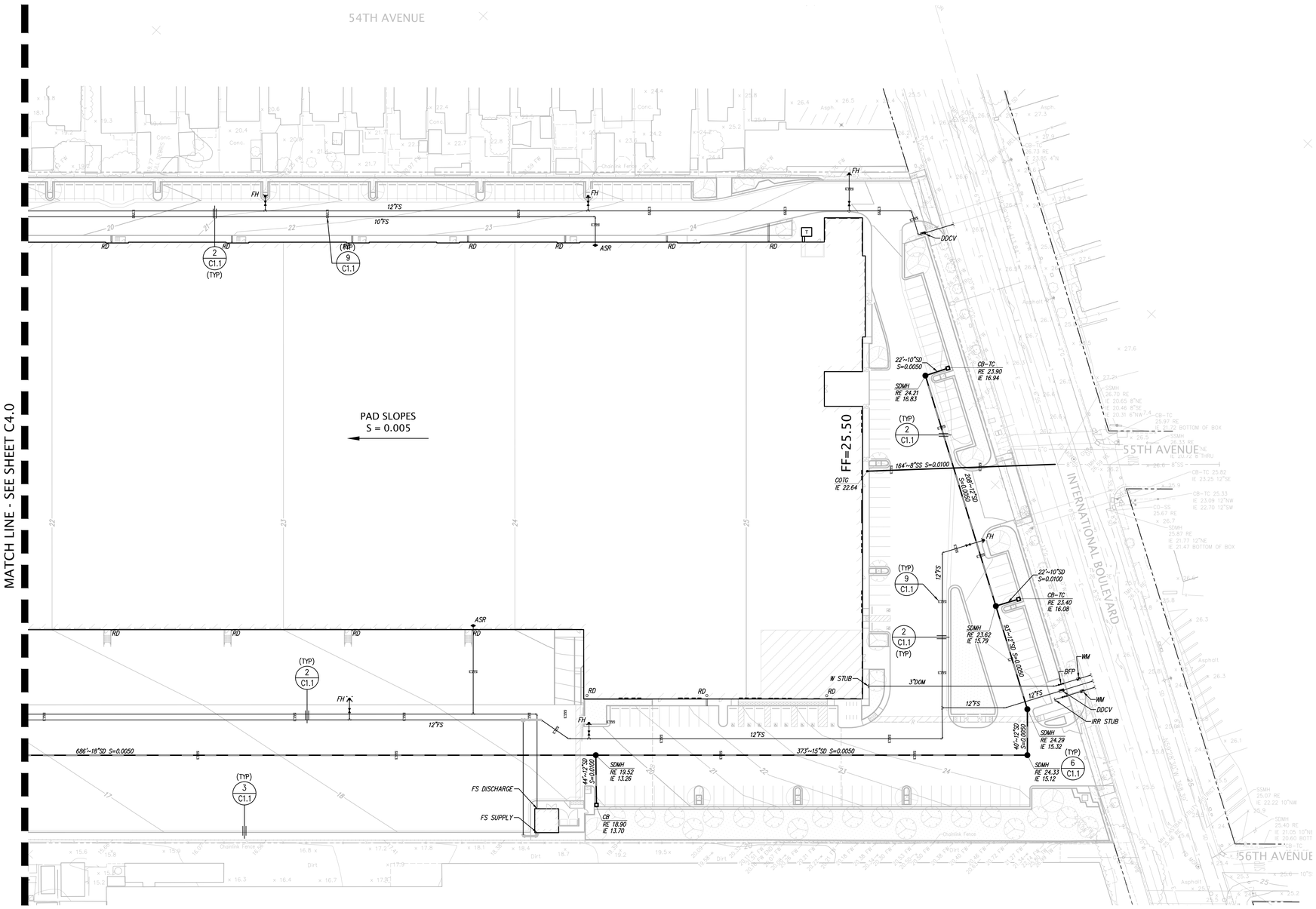

NORTH

0 20 40 80 120

Scale 1" = 40'

LEGEND

ASR	AUTOMATIC SPRINKLER RISER
RE	RIM ELEVATION
IE	INVERT ELEVATION
TC	TOP OF CURB
WS	WATER SERVICE
	EXISTING UTILITY TO BE ABANDONDED BY REMOVAL
FS	FIRE SERVICE
SS	SANITARY SEWER
COTG	CLEANOUT TO GRADE
	STORM DRAIN LINE
	STORM DRAIN CATCH BASIN
	STORM DRAIN JUNCTION BOX
	STORM DRAIN CLEANOUT
	BACK FLOW PREVENTION DEVICE
	FIRE DEPARTMENT CONNECTION
	FIRE HYDRANT & VALVE
	POST INDICATOR VALVE
SSMH	SANITARY SEWER MANHOLE
	SINGLE CHECK VALVE
SDMH	STORM DRAIN MANHOLE
	WATER METER
	TRENCH CUTOFF WALL

NOTE:

1. ONSITE IRRIGATION AND ELECTRICAL SHALL BE DESIGNED BY OTHERS

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PRELIMINARY UTILITY PLAN
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA

DATE	APRIL, 2020
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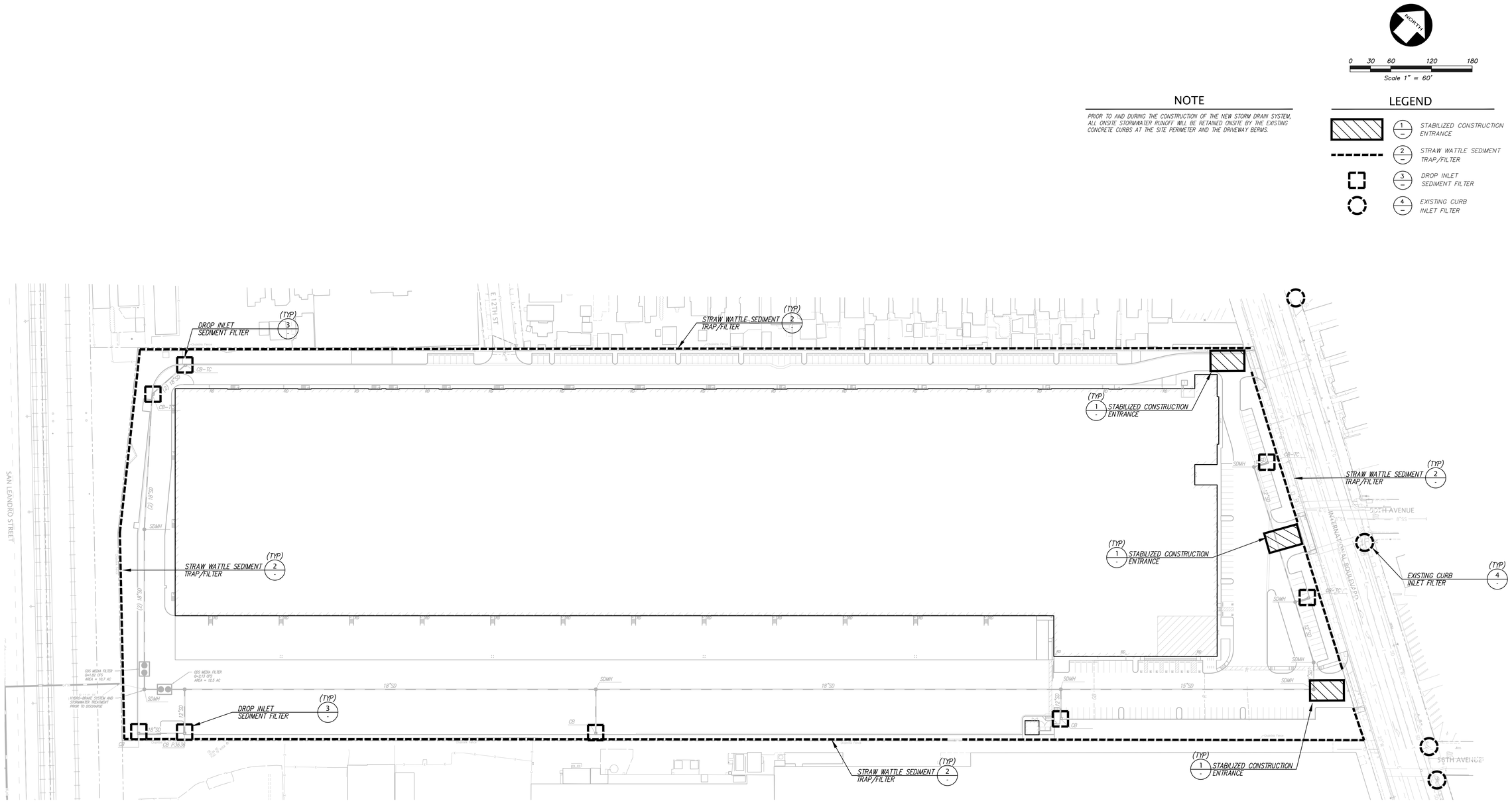
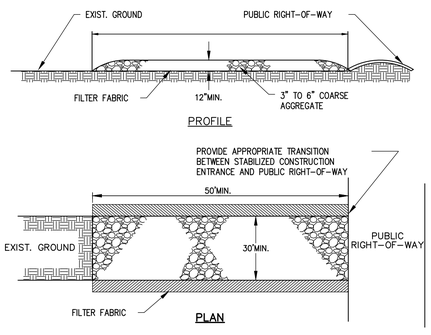
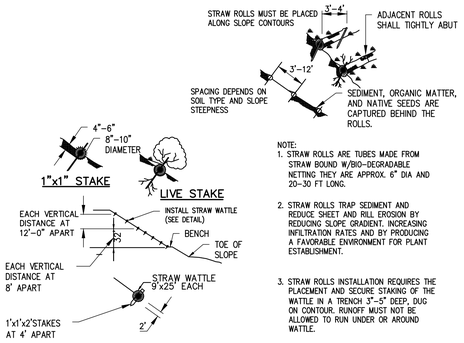




STABILIZED CONSTRUCTION ENTRANCE 1
NOT TO SCALE



STRAW WATTLE 2
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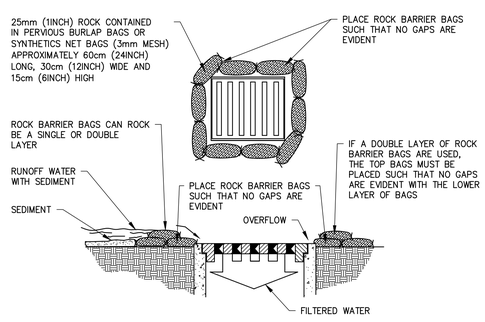
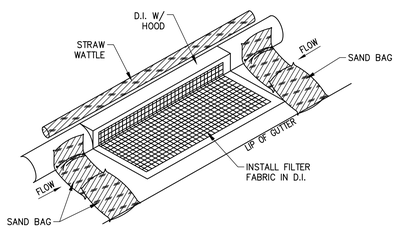


DROP INLET SEDIMENT FILTER UTILIZING ROCK BARRIER BAGS 3
NOT TO SCALE



EXISTING CURB INLET FILTER DETAIL 4
NOT TO SCALE

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REGISTERED PROFESSIONAL ENGINEER
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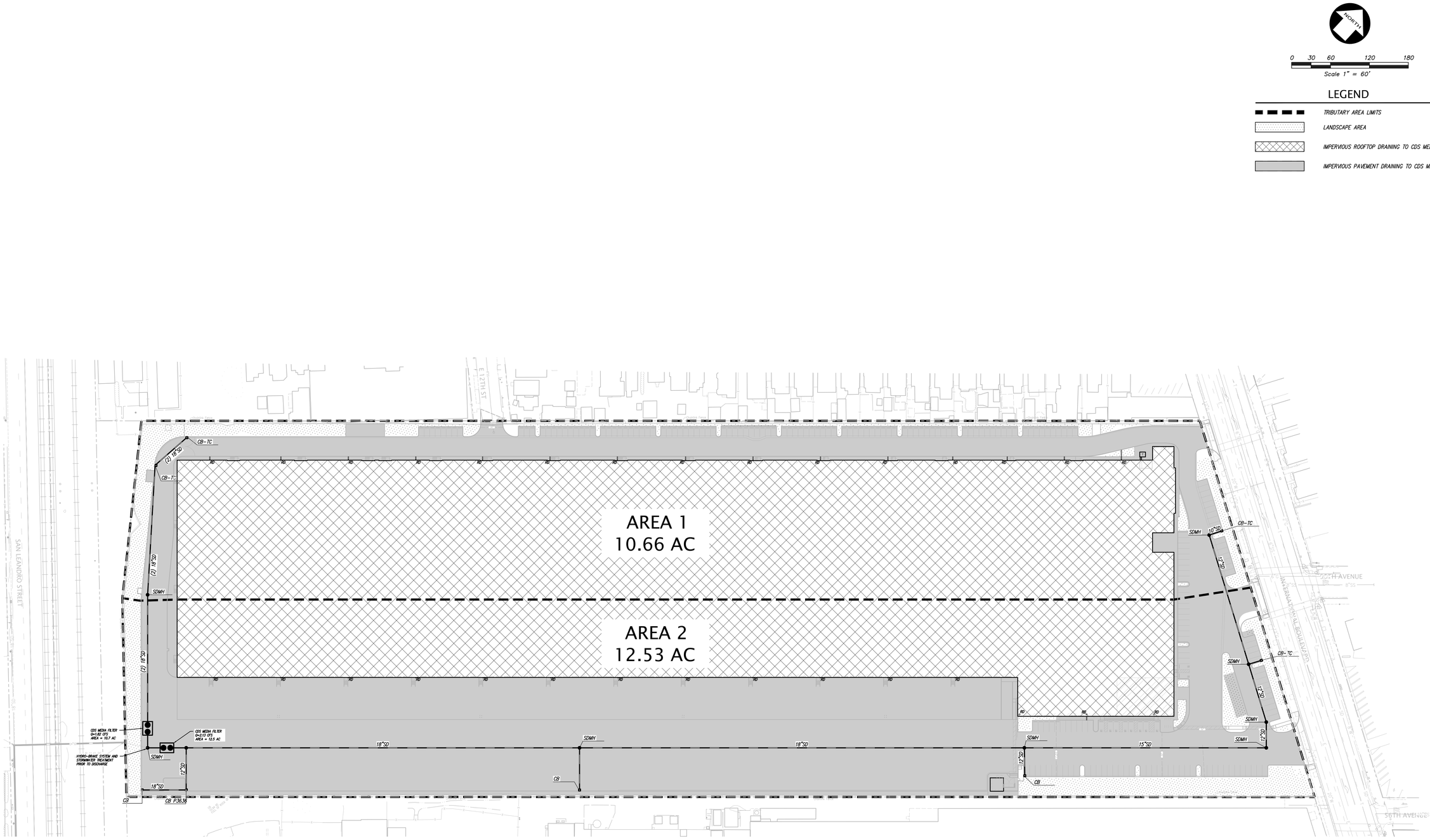
STATE OF CALIFORNIA

PRELIMINARY EROSION CONTROL PLAN
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT
OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C5.0
OF	10 SHEETS



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STATE OF CALIFORNIA

PRELIMINARY STORM WATER QUALITY CONTROL PLAN
OF
5441 INTERNATIONAL BOULEVARD
FOR
BRIDGE DEVELOPMENT

OAKLAND, CALIFORNIA

DATE	APRIL, 2020
SCALE	AS SHOWN
DESIGNER	KR
DRAFTER	CC
JOB NO.	A18535
SHEET	C6.0
OF	10 SHEETS

GENERAL NOTES

1. PLANS ARE BASED UPON CIVIL DRAWINGS PREPARED BY HERDMAN ARCHITECTURE & DESIGN AND KIER WRIGHT CIVIL ENGINEERING. CONTRACTOR SHALL REVIEW THESE DOCUMENTS, CONFIRM ALL DIMENSIONS AND ELEVATIONS AND NOTIFY OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES, EITHER ON THE PLANS OR OBSERVED IN THE FIELD PRIOR TO COMMENCING WORK.

2. ALL WORK SHALL BE PERFORMED BY QUALIFIED LICENSED CONTRACTORS OR SUBCONTRACTORS HAVING AT LEAST 5 YEARS EXPERIENCE WITH SIMILAR PROJECTS.

3. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS AND PAYING ALL FEES RELATED TO THE SCOPE OF WORK, UNLESS STATED OTHERWISE IN THE CONTRACT.

4. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK WITH THAT OF OTHER SUBCONTRACTORS ON THE SITE TO AVOID CONFLICTS WITH WORK SEQUENCE AND DELAYS IN THE PROGRESS OF WORK BY OTHERS.

5. CODES AND STANDARDS: ALL WORK SHALL BE IN ACCORDANCE WITH STANDARDS STATED IN THE PROJECT SPECIFICATIONS AND ALL APPLICABLE LOCAL, COUNTY, STATE AND FEDERAL CODES AND ORDINANCES, INCLUDING BUT NOT LIMITED TO:
   AMERICAN DISABILITIES ACT
   UNIFORM BUILDING CODE
   2016 CALIFORNIA BUILDING CODE
   CALIFORNIA STATE TITLE 24
   CALIFORNIA STATE BOARD OF CONSUMER AFFAIRS FOR AREAS OF WORK GOVERNED BY APPLICABLE LICENCE REQUIREMENTS
   CITY OF OAKLAND
   ALAMEDA COUNTY

6. CONSTRUCTION LAYOUT: CONTRACTOR SHALL STAKE IN THE FIELD ALL ITEMS SHOWN ON THESE PLANS TO BE CONSTRUCTED FOR APPROVAL BY LANDSCAPE ARCHITECT PRIOR TO COMMENCEMENT OF WORK. IF CONFLICTS BETWEEN PROPOSED CONSTRUCTION AND EXISTING SURFACE AND SUBSURFACE CONDITIONS ARE APPARENT, NOTIFY OWNER'S REPRESENTATIVE IMMEDIATELY AND DO NOT PROCEED WITH WORK UNTIL AUTHORIZED TO PROCEED. ALL CHANGES TO PROPOSED CONSTRUCTION WORK MUST BE AUTHORIZED BY LANDSCAPE ARCHITECT IN WRITING. UNAUTHORIZED WORK SHALL BE SUBJECT TO REMOVAL AND PROPERLY CONSTRUCTED IN ACCORDANCE WITH CONDITIONS IDENTIFIED ON THE DRAWINGS AND PROJECT SPECIFICATIONS OR AS AMENDED IN WRITING BY THE LANDSCAPE ARCHITECT AT NO ADDITIONAL COST TO THE OWNER.

7. GRADING AND DRAINAGE: CONTRACTOR SHALL REVIEW AND BE FAMILIAR WITH ALL RELATED PLANS, DRAWINGS AND SPECIFICATIONS PRIOR TO COMMENCING WORK, INCLUDING BUT NOT LIMITED TO: ARCHITECTURAL, CIVIL, STRUCTURAL, PLUMBING, MECHANICAL, AND ELECTRICAL DRAWINGS. NOTIFY OWNER'S REPRESENTATIVE OF ANY CONFLICTS OR OBSTRUCTIONS RELATED TO THE WORK IMMEDIATELY FOR DIRECTIONS OR MODIFICATIONS TO THE PROPOSED PLANS. DO NOT PROCEED WITH WORK WHICH MODIFIES OR CHANGES THE PLANS WITHOUT AUTHORIZATION BY THE OWNER'S REPRESENTATIVE.

8. INSPECTIONS: PROVIDE MINIMUM 48 HOURS PRIOR NOTICE TO LANDSCAPE ARCHITECT TO SCHEDULE INSPECTIONS. INSPECTIONS SHALL INCLUDE BUT NOT BE LIMITED TO THE FOLLOWING ITEMS. CONTRACTOR TO MAKE EVERY EFFORT TO COMBINE ITEMS FOR INSPECTION WHENEVER POSSIBLE.
   a. PRE-CONSTRUCTION SITE WALK THROUGH
   b. CONSTRUCTION LAYOUT
   c. GRADING AND FORM WORK
   d. IRRIGATION MAINLINES AND VALVING, PRIOR TO BACK FILLING TRENCHES (INCLUDES FULLY PRESSURIZED MAIN LINES)
   e. IRRIGATION HEAD LAYOUT PRIOR TO INSTALLATION AND PLANTING LAYOUT PRIOR TO INSTALLATION

8. SUBMITTALS: CONTRACTOR SHALL ALLOW 21 DAYS REVIEW BY OWNER'S REPRESENTATIVE FOR APPROVAL OF SUBMITTAL ITEMS. CONTRACTOR TO IDENTIFY LEAD TIMES FOR PRODUCTION, MANUFACTURE AND DELIVERY OF ALL SUBMITTAL ITEMS IDENTIFIED IN THE SPECIFICATIONS AND PROVIDE ADEQUATE TIME FOR REVIEW BY OWNER'S REPRESENTATIVE. DELAYS TO THE WORK SCHEDULE RESULTING FROM INADEQUATE TIME TO REVIEW SUBMITTAL ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

9. THESE PLANS COMPLY WITH THE CRITERIA OF THE CALIFORNIA MODEL WATER EFFICIENCY ORDINANCE AND APPLY THOSE CRITERIA FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLAN.

LAYOUT NOTES

1. DIMENSIONS ARE TO FACE OF CURB, FACE OF WALL, FACE OF COLUMN, AND EDGE OF PAVING UNLESS OTHERWISE NOTED.

2. VERIFY EXISTING AND NEW UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY ANY CONFLICTS OR DISCREPANCIES TO THE OWNER'S REPRESENTATIVE IMMEDIATELY.

3. AFTER NOTIFYING UNDERGROUND SERVICE ALERT AND HAVING MARKED & LOCATED UTILITIES, CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE WHICH UTILITIES, IF ANY, HAVE BEEN ABANDONED BY EACH SPECIFIC UTILITY COMPANY.

4. NOTIFY THE OWNER'S REPRESENTATIVE OF ANY CONFLICTS OR OBSTRUCTIONS RELATED TO THE WORK IMMEDIATELY FOR DIRECTIONS OR MODIFICATIONS TO THE PROPOSED PLANS. DO NOT PROCEED WITH WORK WHICH MODIFIES OR CHANGES THE DESIGN WITHOUT AUTHORIZATION BY OWNER'S REPRESENTATIVE.

5. STAKE PROPOSED IMPROVEMENTS FOR APPROVAL BY THE OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION.

6. COORDINATE ALL CONSTRUCTION ELEMENTS PRIOR TO INSTALLATION. VERIFY CRITICAL DIMENSIONS, REFERENCE, AND CONSTRUCTION CONDITIONS PRIOR TO INITIATING WORK.

7. THE CONTRACTOR SHALL REPAIR ANY DAMAGE CAUSED BY HIS OPERATIONS TO THE SATISFACTION OF THE OWNER'S REPRESENTATIVE WITHOUT ANY ADDITIONAL CHARGE.

8. CONTRACTOR SHALL NOT PLACE ANY PAVING WITHOUT APPROVAL OF FORMWORK BY THE OWNER'S REPRESENTATIVE. NOTIFY OWNER'S REPRESENTATIVE IF THERE IS ANY DISCREPANCY BETWEEN EXISTING CONDITIONS & PROPOSED GRADING OR LAYOUT. ANY UN-APPROVED CONCRETE POURED NOT CONSISTENT WITH PLANS OR EXISTING CONDITIONS WILL BE REMOVED & REPLACED AT CONTRACTOR'S EXPENSE.

PLANTING AND SOIL PREPARATION NOTES

1. ALL WORK SHALL BE PERFORMED BY PERSONS FAMILIAR WITH PLANTING WORK AND UNDER THE SUPERVISION OF A QUALIFIED PLANTING FOREMAN.

2. SOIL MANAGEMENT REPORT: CONTRACTOR SHALL FURNISH SOIL ANALYSIS BY A QUALIFIED SOIL-TESTING LABORATORY FOR ALL IMPORT TOPSOIL AND ANY ON-SITE TOPSOIL TO REMAIN. SOIL ANALYSIS TO INCLUDE SOIL TEXTURE, INFILTRATION RATE, PH, TOTAL SOLUBLE SALTS, SODIUM, ESSENTIAL NUTRIENTS, AND PERCENT ORGANIC MATTER. SOIL ANALYSIS LAB TO MAKE RECOMMENDATIONS FOR AMENDING THE TOPSOIL WITH COMPOST TO BRING ORGANIC MATTER TO A MINIMUM OF 5% DRY WEIGHT WITH A MINIMUM OF 4 CUBIC YARDS COMPOST PER 1000 SQUARE FEET (1.3" COMPOST PER 1000SF) AND INCORPORATING NON-SYNTHETIC FERTILIZERS TO RECOMMENDED LEVELS FOR PLANTING AREA. TESTING OF IMPORT TOPSOIL SHALL OCCUR PRIOR TO PLACEMENT.

3. CONTRACTOR SHALL AMEND IMPORT TOPSOIL AND ANY ON-SITE TOPSOIL PER THE RECOMMENDATIONS OF THE SOIL ANALYSIS.

4. INSTALLATION OF IMPORT TOPSOIL: AMEND LOOSENED SOIL AS RECOMMENDED IN THE SOILS REPORT. PLACE 9-INCHES OF IMPORTED PLANTING TOPSOIL IN 3-INCH LIFTS. ROTOTILL THE REMAINING LIFTS INTO THE PREVIOUS LIFTS A MINIMUM OF 2-INCHES. SOIL SHALL BE IN A MOIST, NOT WET, CONDITION AT TIME OF MIXING. DO NOT AMEND OR INSTALL IMPORT TOPSOIL WHEN SOIL OR SUBGRADE IS MUDDY, FROZEN, OR EXCESSIVELY WET.

5. ALL BOXED MATERIALS SHALL BE HAND-PICKED AND TAGGED BY THE LANDSCAPE ARCHITECT AT THEIR PLACE OF ORIGIN. BOXED MATERIALS PLANTED WITHOUT SELECTION BY LANDSCAPE ARCHITECT MAY BE SUBJECT TO REMOVAL AND REPLACEMENT AT NO ADDITIONAL COST TO THE OWNER.

6. PLANT MATERIAL LOCATIONS SHOWN ARE DIAGRAMMATIC AND MAY BE SUBJECT TO CHANGE IN THE FIELD BY THE OWNER'S REPRESENTATIVE. PLANT LOCATIONS ARE TO BE ADJUSTED IN THE FIELD AS NECESSARY TO SCREEN UTILITIES BUT NOT SIGNS NOR TO IMPEDE ACCESS.

7. THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO MAKE SUBSTITUTIONS, ADDITIONS, AND DELETIONS IN THE PLANTING SCHEME AS THEY FEEL NECESSARY WHILE WORK IS IN PROGRESS UPON APPROVAL OF THE OWNER. SUCH CHANGES ARE TO BE ACCOMPANIED BY EQUITABLE ADJUSTMENTS IN THE CONTRACT PRICE IF NECESSARY.

8. ALL TREES ARE TO BE STAKED AS SHOWN ON THE TREE STAKING DIAGRAMS. BRANCHING HEIGHT OF TREES SHALL BE A 6'-0" MINIMUM ABOVE FINISH PAVING. ALL TREES IN A FORMAL GROUP PLANTING SHALL BE MATCHING IN SIZE AND SHAPE. LANDSCAPE ARCHITECT SHALL BE CONSULTED REGARDING ORIENTATION OF TREES PRIOR TO PLANTING AND/OR BACKFILLING. TREES INSTALLED WITHOUT THIS APPROVAL WILL BE SUBJECT TO REMOVAL AND REPLANTING TO THE SATISFACTION OF THE LANDSCAPE ARCHITECT AT NO ADDITIONAL COST TO THE OWNER.

9. TREES MUST HAVE AN UNCUT LEADER WITH A UNIFORM TAPER FROM BASE TO TIP. TREES MUST MEET AT LEAST NORMAL CALIPER AND HEIGHT FOR CONTAINER SIZE. TREES WHICH ARE OVERGROWN, UNDERSIZED, ROOT BOUND, OR WITH CO-DOMINANT LEADERS ARE NOT ACCEPTABLE AND SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT NO COST TO THE OWNER.

10. PLANT COUNT IS FOR THE CONVENIENCE OF THE CONTRACTOR. IN CASE OF DISCREPANCIES, THE PLAN SHALL GOVERN.

11. ALL PLANTING AREAS SHALL BE TOP-DRESSED WITH A 3" LAYER OF MULCH. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO ORDERING.

UTILITY NOTES

1. REFER TO CIVIL DRAWINGS FOR GENERAL UTILITIES REQUIREMENTS, INCLUDING IRRIGATION LINES AND TRENCH. SEE SHEET C1.1

2. ALL UTILITY TRENCHES AND BELOW-GROUND FEATURES SHALL BE INSTALLED WITH IMPORT BACKFILL "CLEAN CORRIDORS" IN ACCORDANCE WITH THE RDIP ADDENDUM SECTION 2.25 AND 3.11 AND IMPORT FILL REQUIREMENTS. MARKER FABRIC SHALL BE PLACED AT THE INTERFACE BETWEEN IMPORT FILL AND NATIVE SOIL. SEE CIVIL DRAWING SHEET C2.0.

ABBREVIATIONS

AD	AREA DRAIN
ADJ	ADJACENT
AGG	AGGREGATE
AL	ALIGN
BR	BOTTOM OF RAMP
BS	BOTTOM OF STAIR
BW	BOTTOM OF WALL
CB	CATCH BASIN
CIP	CAST IN PLACE
CJ	CONTROL JOINT
CL	CENTERLINE
CLR	CLEARANCE
CO	CLEAN OUT
CONC	CONCRETE
CONF	CONFIGURATION
CONT	CONTINUOUS
DBH	DIAMETER AT BREAST HT
DG	DECOMPOSED GRANITE
DWG	DRAWING(S)
(E)	EXISTING
EJ	EXPANSION JOINT
EQ	EQUAL
EW	EACH WAY
FF	FINISH FLOOR
FG	FINISHED GRADE
FOB	FACE OF BUILDING
FOW	FACE OF WALL
FP	FINISH PAVING
GAL	GALLON
GFRC	GLASS FIBER REINFORCED CONCRETE
GB	GRADE BREAK
HP	HIGH POINT
HPS	HIGH POINT SWALE
HT	HEIGHT
IE	INVERT ELEVATION
INT	INTERSECTION
INV	INVERT
LT	LIGHT
MAX	MAXIMUM
MIN	MINIMUM
MFR	MANUFACTURER
(N)	NEW
NTS	NOT TO SCALE
OC	ON CENTER
PA	PLANTING AREA
PN	PROJECT NORTH
POB	POINT OF BEGINNING
PTDS	PRESSURE TREATED DOUG FIR
R	RISER
RE	RIM ELEVATION
ROW	RIGHT OF WAY
SAD	SEE ARCHITECTURAL DRAWINGS
SCD	SEE CIVIL ENGINEERING DRAWINGS
SDS	STORM DRAIN SYSTEM
SED	SEE ELECTRICAL ENGINEERING DRAWINGS
SD	STORM DRAIN
SIM	SIMILAR
SJ	SCORE JOINT
SMD	SEE MECHANICAL ENGINEERING DRAWINGS
SPECS	SPECIFICATIONS
SS	STRUCTURAL SLAB
SSD	SEE STRUCTURAL ENGINEERING DRAWINGS
SST	STAINLESS STEEL
STL	STEEL
TC	TOP OF CURB
TF	TOP OF FENCE
THLD	THRESHOLD
TP	TOP OF PLANTER
TR	TOP OF RAMP
TS	TOP OF STAIR
TW	TOP OF WALL
TYP	TYPICAL
UON	UNLESS OTHERWISE NOTED
VIF	VERIFY IN FIELD
WPM	WATERPROOF MEMBRANE
WP	WATERPROOFING

SHEET LIST

SHEET NUMBER	SHEET TITLE
L0.01	COVER SHEET
L1.01	CONCEPTUAL LANDSCAPE PLAN
L1.02	CONCEPTUAL LANDSCAPE PLAN
L2.01	CONCEPTUAL IRRIGATION PLAN
L2.02	CONCEPTUAL IRRIGATION PLAN
L3.01	PLANT PALETTE

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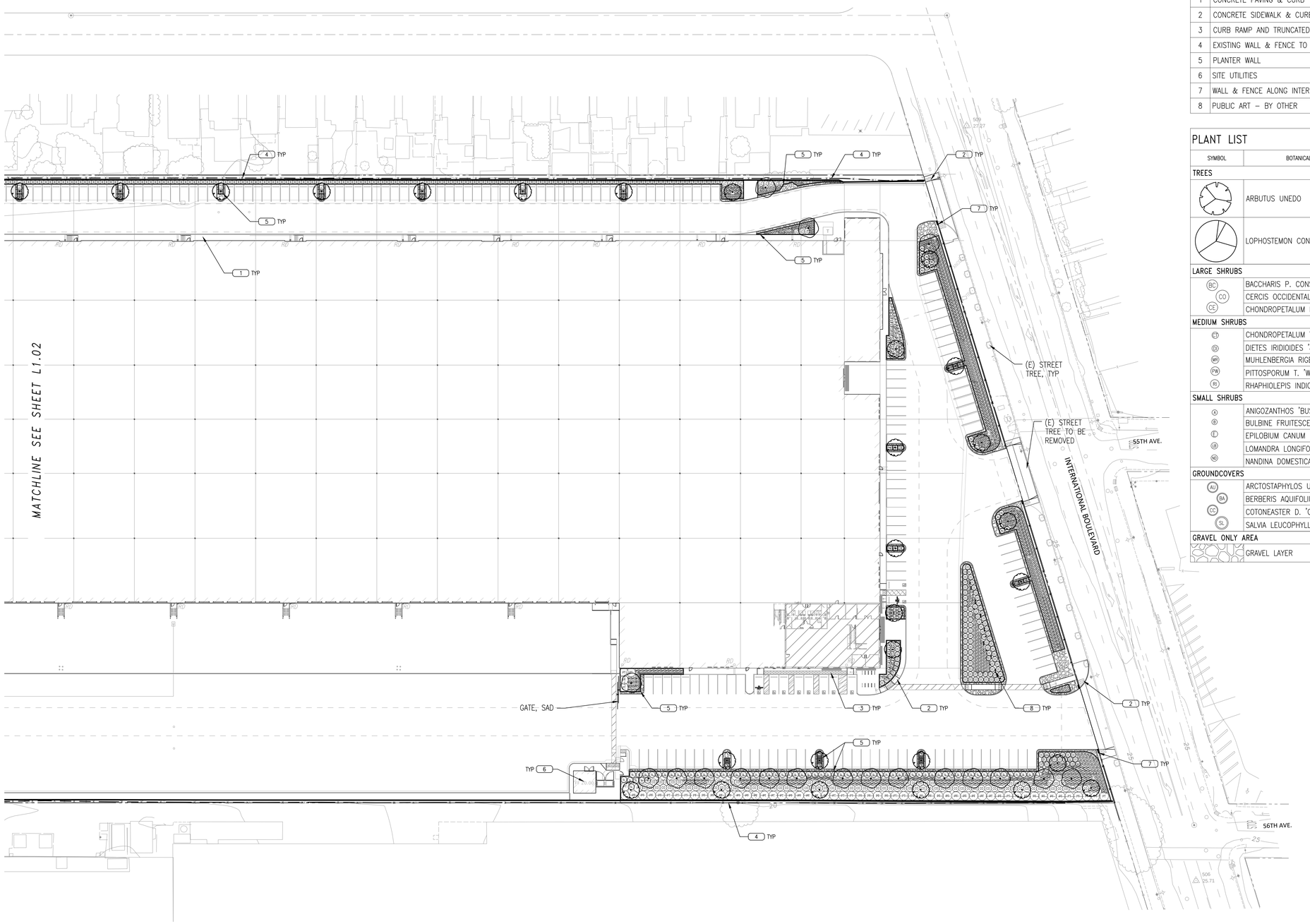
NORTH

CONCEPTUAL
COVER SHEET

L0.01



LEGEND

KEY	DESCRIPTION	DETAIL
1	CONCRETE PAVING & CURB	SCD
2	CONCRETE SIDEWALK & CURB CUT	SCD
3	CURB RAMP AND TRUNCATED DOMES	SCD
4	EXISTING WALL & FENCE TO REMAIN	–
5	PLANTER WALL	–
6	SITE UTILITIES	SAD, SCD
7	WALL & FENCE ALONG INTERNATIONAL BLVD.	SAD
8	PUBLIC ART – BY OTHER	–

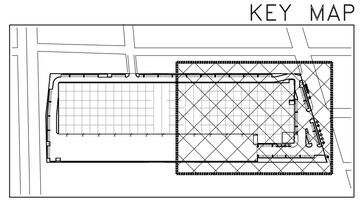
PLANT LIST

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	WTR USE	HEIGHT X SPREAD
TREES						
	ARBUTUS UNEDO	STRAWBERRY TREE	24" BOX	PER PLAN	L	18' X 18'
	LOPHOSTEMON CONFERTUS	BRISBANE BOX	24" BOX	PER PLAN	M	25' X 20'
LARGE SHRUBS						
BC	BACCHARIS P. CONSANGUINEA	COYOTE BRUSH	5 GAL	5'–0" OC	L	
CO	CERCIS OCCIDENTALIS	WESTERN REDBUD	5 GAL	6'–0" OC	VL	
CE	CHONDROPETALUM ELEPHANTINUM	LARGE CAPE RUSH	5 GAL	6'–0" OC	L	
MEDIUM SHRUBS						
CT	CHONDROPETALUM TECTORUM	CAPE RUSH	5 GAL	3'–0" OC	L	
DI	DIETES IRIDIOIDES 'JOHN'S RUNNER'	FORTNIGHT LILY	5 GAL	3'–0" OC	L	
MR	MUHLENBERGIA RIGENS	DEER GRASS	5 GAL	3'–0" OC	L	
PW	PITTOSPORUM T. 'WHEELER'S DWARF'	DWARF MOCK ORANGE	5 GAL	4'–0" OC	L	
RI	RHAPHIOLEPIS INDICA 'CLARA'	INDIA HAWTHORNE	5 GAL	4'–0" OC	L	
SMALL SHRUBS						
A	ANIGOZANTHOS 'BUSH RANGER'	KANGAROO PAW	5 GAL	2'–6" OC	L	
B	BULBINE FRUITESCENS	STALKED BULBINE	1 GAL	2'–6" OC	L	
E	EPILOBIUM CANUM	CALIFORNIA FUCHSIA	1 GAL	3'–0" OC	L	
LB	LOMANDRA LONGIFOLIA 'BREEZE'	DWARF MAT RUSH	1 GAL	3'–0" OC	L	
NC	NANDINA DOMESTICA 'LEMON LIME'	HEAVENLY BAMBOO	5 GAL	3'–0" OC	L	
GROUNDCOVERS						
AU	ARCTOSTAPHYLOS UVA–URSI	MANZANITA	1 GAL	5'–0" OC	L	
BA	BERBERIS AQUIFOLIUM 'REPENS'	CREEPING BARBERRY	1 GAL	5'–0" OC	L	
CC	COTONEASTER D. 'CORAL BEAUTY'	COTONEASTER	1 GAL	5'–0" OC	L	
SL	SALVIA LEUCOPHYLLA 'POINT SAL'	PURPLE SAGE	1 GAL	6'–0" OC	L	
GRAVEL ONLY AREA						
	GRAVEL LAYER	–	–	–	–	

0 20' 40' 80' 120'

SCALE: 1" = 40'–0"

KEY MAP



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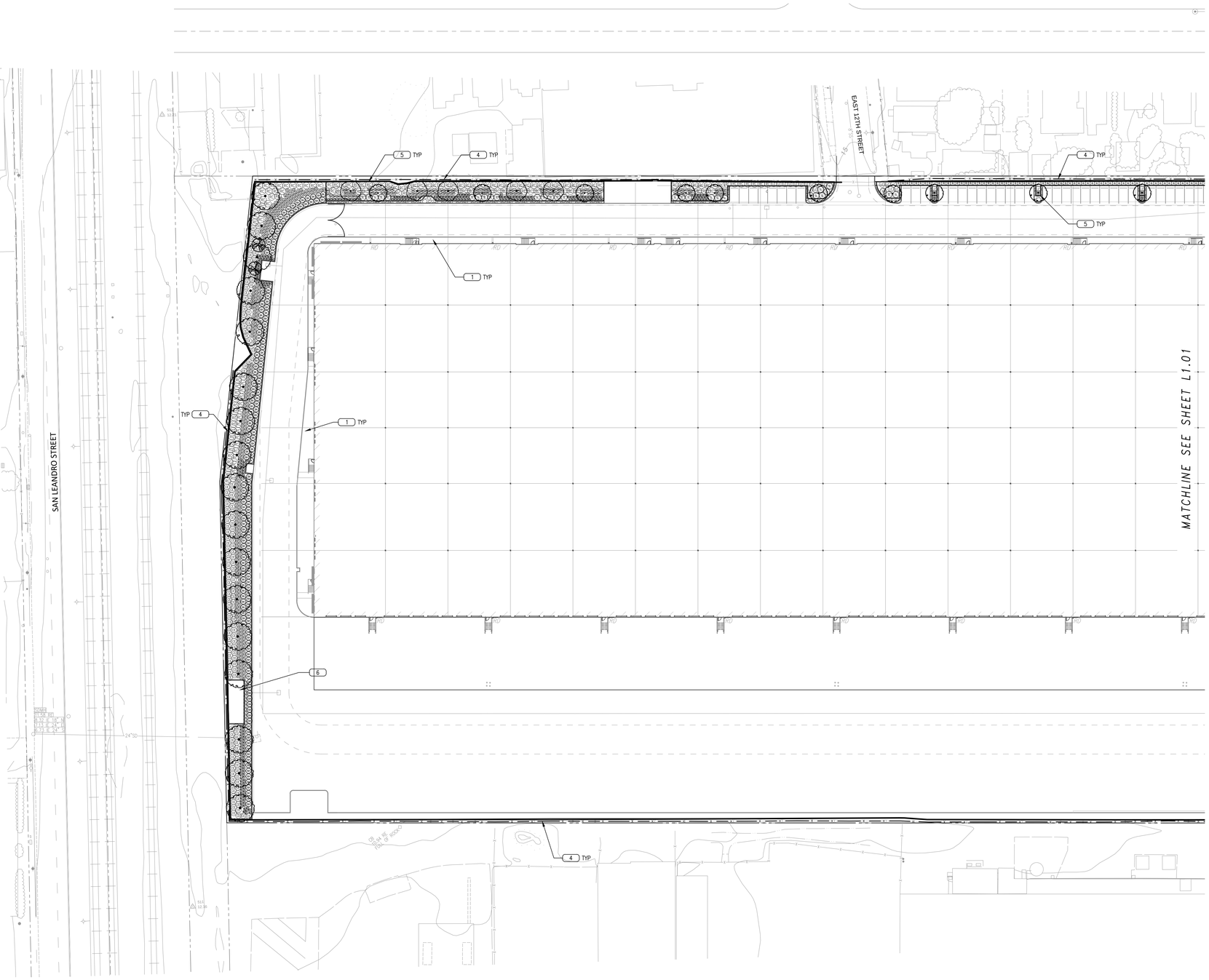
CONCEPTUAL
LANDSCAPE PLAN

L1.01



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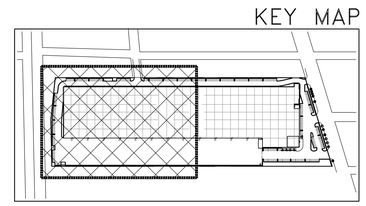
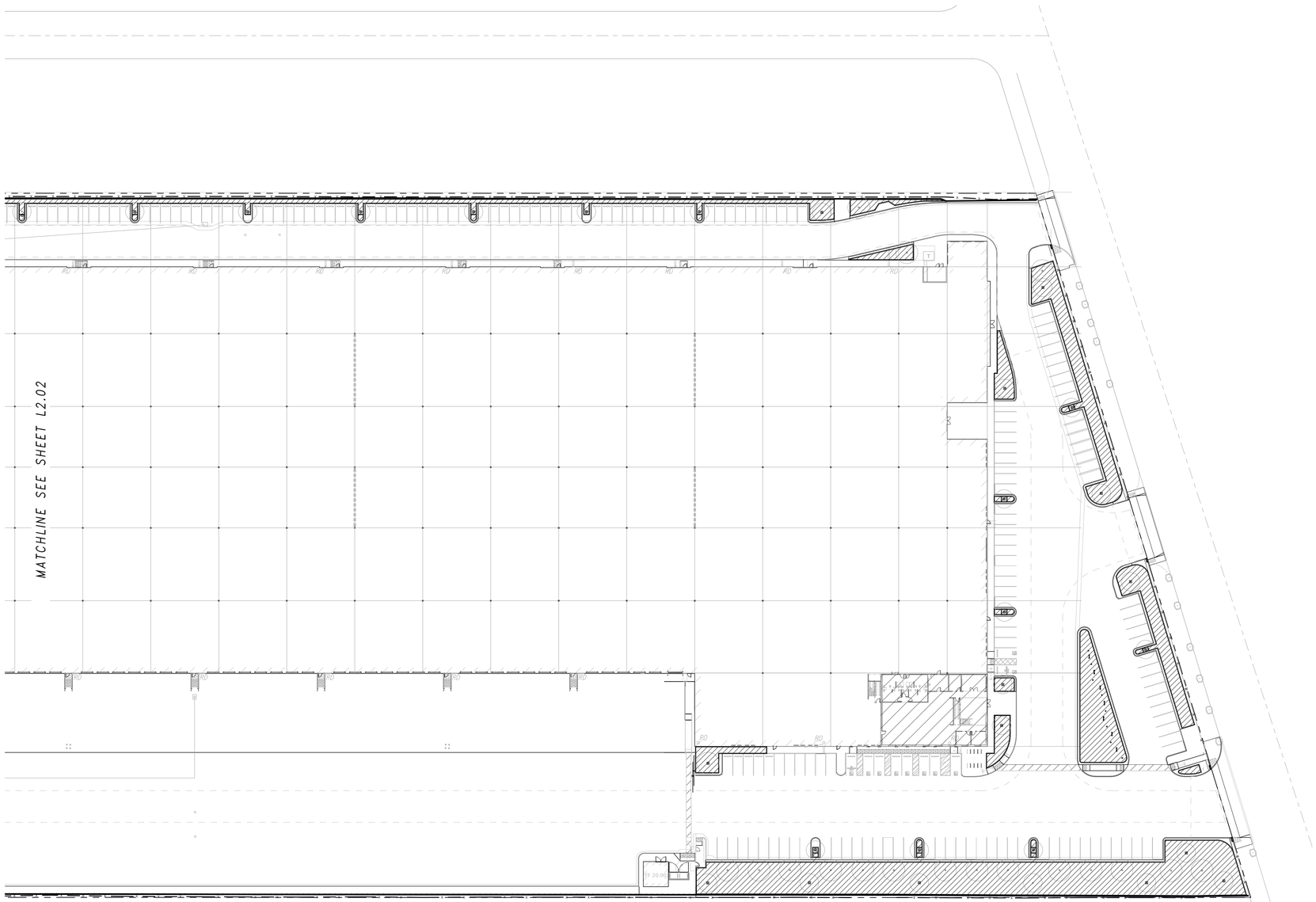
KEY MAP



NORTH

CONCEPTUAL LANDSCAPE PLAN

L1.02



HYDROZONE LEGEND

SYMBOL	ZONE	HYDROZONE	PLANT TYPE	IRRIGATION TYPE	AREA (SF)	TOTAL (SF)	% LANDSCAPE
	1	LOW WATER USE	SHRUB/G.COVER	DRIP	44,375	44,543.0	100%
	2	LOW WATER USE	TREES	BUBBLER	168		
	3	MODERATE WATER USE	TREES	BUBBLER	144	144.0	<1%
					TOTAL	44,687.0	100%

IRRIGATION DESIGN INTENT

1. THIS PLAN SHALL COMPLY WITH THE REQUIREMENTS OF THE STATE OF CALIFORNIA'S MODEL WATER EFFICIENT LANDSCAPE ORDINANCE (MWELO), CITY OF OAKLAND, AND EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD).
2. THE IRRIGATION SYSTEM SHALL BE DESIGNED TO PROVIDE THE MINIMUM AMOUNT OF WATER NECESSARY TO SUSTAIN GOOD PLANT HEALTH.
3. THE IRRIGATION SYSTEM IS TO BE A FULLY AUTOMATIC, WEATHER-BASED SYSTEM USING RAIN SENSOR, LOW FLOW DRIP, BUBBLER DISTRIBUTION, AND ROTOR WITH MATCHED PRECIPITATION RATE NOZZLES DESIGNED FOR HEAD-TO-HEAD COVERAGE.
4. ALL SELECTED COMPONENTS SHALL BE PERMANENT, COMMERCIAL GRADE, SELECTED FOR DURABILITY, VANDAL RESISTANCE AND MINIMUM MAINTENANCE REQUIREMENT, INSTALLED BELOW-GRADE, AND DESIGNED FOR 100% COVERAGE.
5. THE SYSTEM SHALL INCLUDE A MASTER CONTROL VALVE AND FLOW SENSING CAPABILITY WHICH WILL SHUT DOWN ALL OR PART OF THE SYSTEM IF LEAKS ARE DETECTED.
6. THE IRRIGATION SYSTEM SHALL BE DESIGNED TO DELIVER WATER TO HYDROZONES BASED ON MOISTURE REQUIREMENTS OF THE PLANT GROUPING.

IRRIGATION CALCULATION:

HYDROZONE INFORMATION TABLE

REFERENCE ANNUAL ET_0 FOR:	OAKLAND (WUCOLS)	41.8

ET ADJUSTMENT FACTOR	0.45	ET ADJ FACTOR PER MWELO & CALGREEN: 0.80= EXISTING NON-REHABILITATED LANDSCAPE, 0.65= SCHOOL 0.55= RESIDENTIAL, 0.45= NON-RESIDENTIAL	SLA ADDITIONAL WATER ALLOWANCE (1.0-ETAF)	0.55

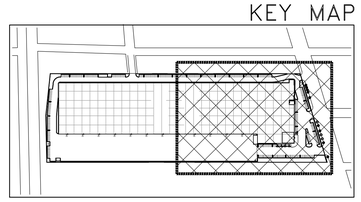
HYDROZONE	WUCOLS IV PLANT FACTOR (PF)	IRR METHOD DRIP:0.81 ROTOR:0.75 BUBB:0.81 SPRAY:0.75	IRRIGATION EFFICIENCY (IE)	$ETAF_Z$ (PF/IE)	LANDSCAPE AREA (SQ FT)	$ETAF_Z$ X AREA	ESTIMATED TOTAL WATER USE (ETWU)
1	.3	D	0.81	0.37	44375	16435.19	425934.26
2	.3	B	0.81	0.37	168	62.22	1612.55
3	.6	B	0.81	0.74	144	106.67	2764.37
–							
				TOTAL	44687.00	16604.07	430,311.18
SPECIAL LANDSCAPE AREAS							
– –				1	0	0.00	0.00
– –				0	0	0.00	0.00
				TOTAL	0	0.00	0.00
TOTAL LANDSCAPE AREA (LA + SLA)					44,687.00		

TOTAL ETWU	TOTAL ETWU ALL AREAS (SLA AND REGULAR LA)	TOTAL ETWU	430,311.18
MAWA	(ANNUAL ETO)(0.62 CONVERSION FACTOR) [(ET ADJUSTMENT FACTOR)(TOTAL LANDSCAPE AREA) + (1-ETAF)*SLA))]	MAWA	521,148.73

AVERAGE ETAF	SUM($ETAF_Z$ X AREA) / TOTAL AREA (AVERAGE ETAF AS DESIGNED, EXCLUSIVE OF SLA_S)	0.37
SITEWIDE ETAF	TOTAL ETAF X AREA / TOTAL LANDSCAPE AREA (INCLUDES SLA_S)	0.37

0 20' 40' 80' 120'
SCALE: 1" = 40'-0"

KEY MAP



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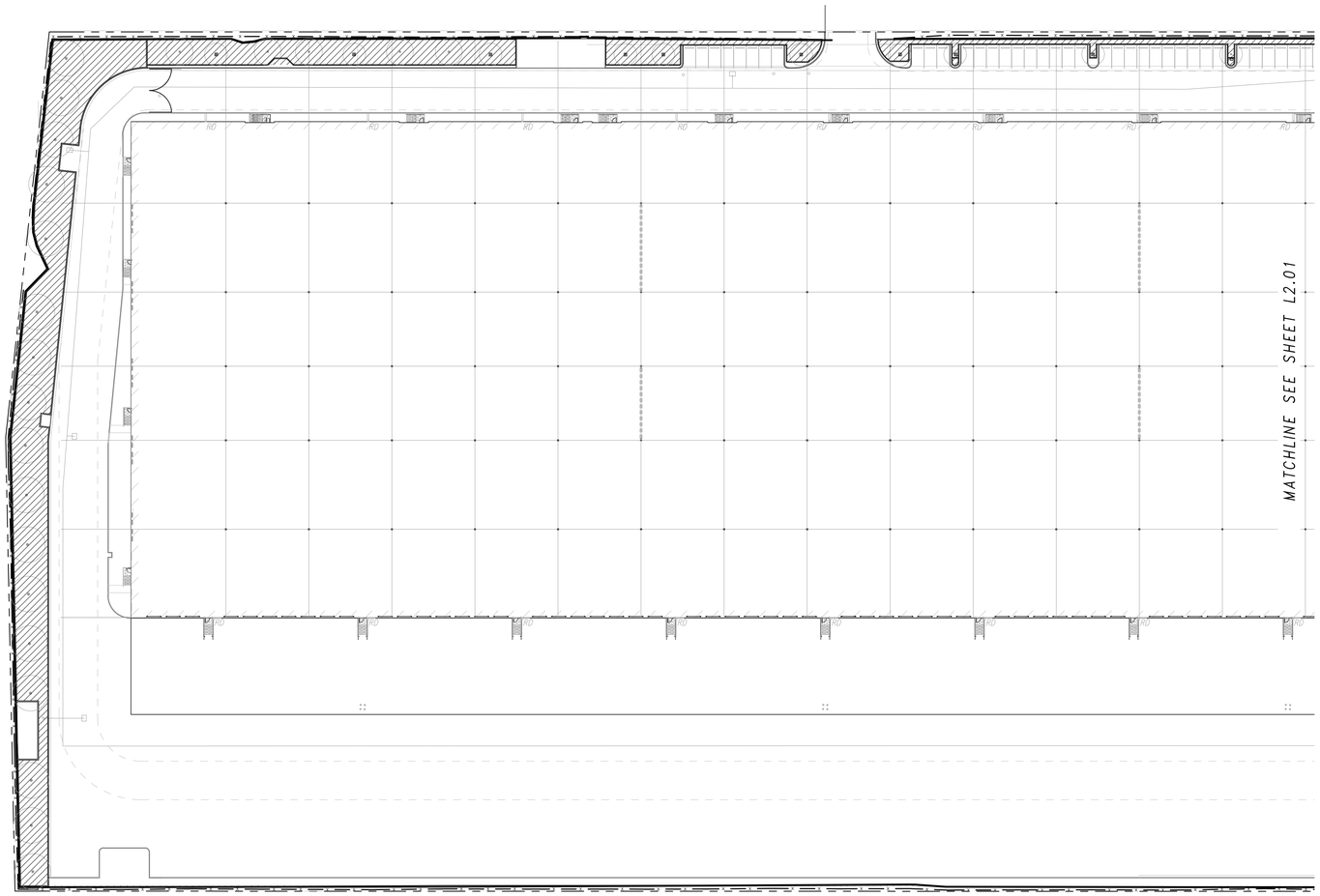
NORTH

CONCEPTUAL IRRIGATION PLAN

L2.01

SEE SHEET L2.01 FOR NOTES AND LEGEND



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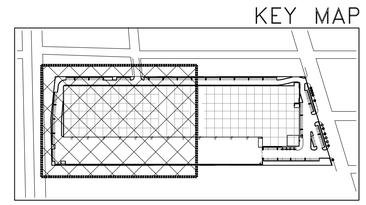

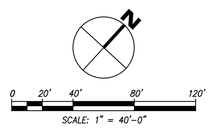

NORTH

CONCEPTUAL
IRRIGATION PLAN

L2.02

TREES


ARBUTUS UNEDO


LOPHOSTEMON CONFERTUS

NOTES:

1. IRRIGATION SYSTEM TO BE FULLY AUTOMATIC WEATHER-BASED SYSTEM UTILIZING A WEATHER-BASED EVAPOTRANSPIRATION/SMART CONTROLLER WITH A RAIN SENSING SYSTEM.
2. IRRIGATION SHALL BE IN ACCORDANCE WITH REGULATIONS OF THE CITY OF OAKLAND, EBMUD – EAST BAY MUNICIPAL UTILITY DISTRICT, & STATE WATER ORDINANCE.
3. PLANT MATERIAL SHALL BE NATIVE OR DROUGHT TOLERANT SPECIES.

SHRUBS AND GROUNDCOVERS


ARCTOSTAPHYLOS UVA-URSI


ANIGOZANTHOS 'PINK JOEY'


BACCHARIS P. CONSANGUINEA


BULBINE FRUTESCENS


CERCIS OCCIDENTALIS


CHONDROPETALUM TECTORUM


COTONEASTER 'CORAL BEAUTY'


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MUHLENBERGIA RIGENS


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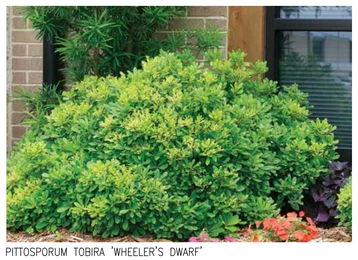
'LEMON LIME'


PITTOSPORUM TOBIRA 'WHEELER'S DWARF'


RHAPHIOLEPIS INDICA 'CLARA'


RHAMNUS CALIFORNICA 'EVE CASE'

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