

**Case File Number: PLN20038, PLN20038-ER01, PLN20108**

**April 14, 2021**

<b>Location:</b>	Lake Merritt BART TOD 51 9th Street (Block 1), 107 8th Street (Block 2)
<b>Assessor's Parcel Number(s):</b>	001 016900100; 001 017100200
<b>Proposal:</b>	Preliminary Development Plan (PDP) for a PUD with five new lots and one remainder parcel (Vesting Tentative Tract Map No. 8560 and 8577) on two separate blocks including 557 residential units (233 affordable units), approximately 500,000 square feet of administrative office commercial activity, approximately 16,500 sf of ground floor commercial retail, 2,000 square feet of custom manufacturing commercial kitchen activity, 6,200 square feet of Community Education Civic Activity daycare, and a total of 408 parking spaces. The project includes a public paseo between Buildings A and B in Block 1.
<b>Applicant:</b>	STRADA/EBALDC
<b>Contact Person/ Phone Number:</b>	William Goodman, 314-276-0707
<b>Owner:</b>	San Francisco Bay Area Rapid Transit District (BART)
<b>Case File Number:</b>	PLN20038, PLN20038-ER01, PLN20108, T2000021
<b>Planning Permits Required:</b>	PDP/Planned Unit Development, Variance for Off-Street Loading, Design Review, Vesting Tentative Tract Map, compliance with CEQA, Major Conditional Use Permit, Tree Removal Permit
<b>General Plan:</b>	Central Business District (CBD)
<b>Zoning:</b>	D-LM-2, D-LM-4, Height Area LM-275
<b>Environmental Determination:</b>	TBD, under review
<b>Historic Status:</b>	Non-Historic Property
<b>City Council District:</b>	CCD2
<b>Finality of Decision:</b>	Project will go to Planning Commission for review and approval
<b>For Further Information:</b>	Contact case planner <b>Dara O'Byrne</b> at <b>510-238-6983</b> or <a href="mailto:do Byrne@oaklandca.gov">do Byrne@oaklandca.gov</a>

## **1. SUMMARY**

The proposed project is a Preliminary Development Plan (PDP) for a Planned Unit Development at 51 9th Street (Block 1) and 107 8th Street (Block 2). The project proposes 557 residential units (including 233 affordable units), up to 500,000 square feet of office, up to 16,500 sf of ground floor commercial (retail and food service), approximately 2,000 square feet of custom manufacturing commercial kitchen, 6,200 square feet of day care, and a total of 408 parking spaces. The project includes a public paseo and BART plaza. The project includes a Vesting Tentative Tract Map, a Tree Permit, and project specific Design Guidelines.

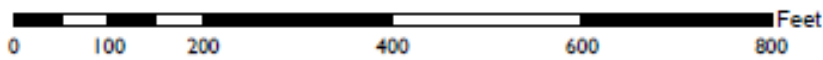
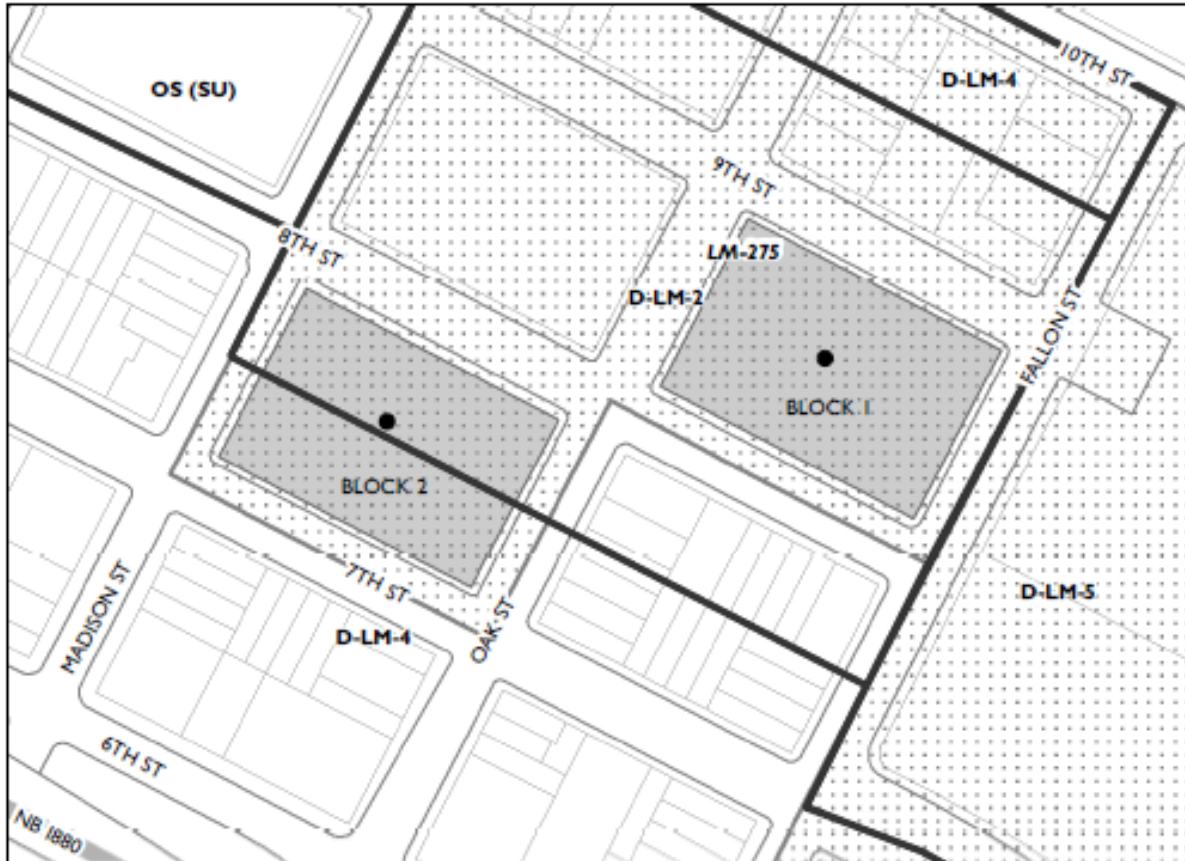
## **2. PROJECT SITE AND SURROUNDING AREA**

### **2.1 Existing Conditions and Surrounding Land Uses**

Block 1 is bounded by 9<sup>th</sup> St to the north, Fallon St to the east, 8<sup>th</sup> St to the south, and Oak St to west. The block currently contains BART parking, BART station head houses, and a small BART plaza. The block is surrounded by Laney College to the east, the BART plaza to the west, small scale commercial buildings to the south, and a pair of historic Colonial Revival rowhouses to the north.

Block 2 is bounded by 8<sup>th</sup> St to the north, Oak St to the east, 7<sup>th</sup> St to the south, and Madison St to the west. The block currently contains the Metro Center Building and surface parking. To the north of the block is the BART plaza, to the east and south is residential in character with houses with 'Potential Designated Historic Property' (PDHP) status. To the west of the block is a mix of multifamily and single-family residential activities.

# CITY OF OAKLAND PLANNING COMMISSION



Case File: PLN20038, PLN20038-ER01, PLN20108, T2000021  
Applicant: STRADA/EBALDC  
Address: Lake Merritt BART TOD - 51 9th Street (Block 1),  
107 8th Street (Block 2)  
Zone: D-LM-2, D-LM-4  
Height Area: LM-275



### 3. PROJECT BACKGROUND

The Lake Merritt Station Area Plan community engagement process began in 2008 and the Lake Merritt Station Area Plan was adopted by City Council in late 2014. The Station Area Plan lays out a community-based vision for the roughly one-half mile radius around the Lake Merritt BART Station in Downtown Oakland. The two development sites included in this Preliminary Development Plan are located in the center of the Planning Area and both blocks are identified as “Opportunity Sites” in the Station Area Plan. The two development blocks are assigned a transit oriented development height area of 275 feet, are designated as pedestrian transitional areas, and are primarily surrounded by commercial corridors.

The D-LM zoning was adopted concurrently with the Station Area Plan, implementing the land use vision of the plan.

In the Spring of 2018, BART released a request for qualifications for a Transit Oriented Development (TOD) at the two blocks owned by BART at the Lake Merritt BART station. In May 2018, BART invited four teams to submit a proposal and in September 2018, BART selected the STRADA/EBALC team to develop the site.

In March 2019, BART and the applicant team submitted a pre-application for initial review and coordination with the City. The applicant team submitted a formal application for the Preliminary Development Plan to the City of Oakland in February 2019 and CEQA review was initiated. The application was deemed complete in November 2020.

### 4. PROJECT DESCRIPTION

The Project is a Preliminary Development Plan for a multi-phase Planned Unit Development that will include two Blocks, each with two new buildings.

- Block 1 at 51 9<sup>th</sup> St proposes 3 new lots and 1 Remainder Parcel (Tract Map No. 8560) and includes two buildings:
  - Building A: 360 residential units (324 market rate, 36 moderate income), 4,500 square feet of ground floor commercial, and 105 parking spaces in a 275 ft tall tower
  - Building B: 97 affordable residential units, 2,029 sf of custom manufacturing commercial kitchen, and 963 square feet of limited-service restaurant/cafe in an 83 ft tall mid-rise building
  - Public paseo and BART plaza, including BART station entrances
- Block 2 at 107 8<sup>th</sup> St proposes two new lots (Tract Map No. 8577) and includes two separate buildings:
  - Building C: approximately 500,000 sf office, 11,000 sf of ground floor commercial, and 254 parking spaces in a 275 foot tall tower
  - Building D: 100 affordable residential units, 6,200 sf of daycare, and 49 parking spaces in an 83 ft tall building

## 5. GENERAL PLAN ANALYSIS

The General Plan land use designation for this site is Central Business District. The classification is intended to “encourage, support, and enhance the downtown area as a high density mixed use urban center of regional importance and a primary hub for business, communications, office, government, high technology, retail, entertainment, and transportation in Northern California.”

The desired character and use for the CBD classification includes a mix of large-scale offices, commercial, urban (high-rise) residential, institutional, open space, cultural, educational, arts, entertainment, service, community facilities, and visitor uses.

The following is an analysis of how the proposed project meets applicable General Plan objectives (staff analysis in indented, italicized text below each objective):

- Objective T2. Provide mixed use, transit-oriented development that encourages public transit use and increases pedestrian and bicycle trips at major transportation nodes.  
*The proposed project provides a mixed-use, transit-oriented development at the Lake Merritt BART station, including residential, office, and retail. The project provides streetscape improvements that improve the conditions for pedestrians and bicyclists and includes a paseo to improve connections to the Lake Merritt BART station.*
- Objective D1. Enhance the identify of Downtown Oakland and its distinctive districts
  - Policy D1.8 Planning for the Channel Park Arts, Educational, and Cultural Center. The area south of Lake Merritt that includes Laney College, the Henry J. Kaiser Auditorium, the Oakland Museum, and Alameda County offices should be enhanced as a walkable, bicycle-friendly educational, cultural and institutional center in downtown Oakland. Efforts to strengthen this area's identity and create transportation linkages with the Jack London Waterfront, City Center, and the Financial District, and BART should be promoted.  
*The proposed project creates a transit-oriented development at the Lake Merritt BART station that helps create a node of activity to connect Laney College and the Oakland Museum to the BART station, with improved bicycle and pedestrian connections and a mid-block activated paseo.*
- Objective D3. Create a pedestrian-friendly downtown  
*The project improves the sidewalks on the four frontages of each block, providing a minimum 5.5 foot clear width for pedestrians on all frontages. The project also includes intersection improvements that improve pedestrian safety. The streetscape improvements also include landscaping, street furniture, and other amenities. The pedestrian paseo provides mid-block pedestrian access to the BART station.*
- Objective D5. Enhance the safety and perception of safety downtown at all hours  
*The proposed project is being designed to activate the transit node at all hours, with ground floor active uses, activation of the paseo, and incorporation of Crime Prevention through Environmental Design (CPTED) techniques.*

- Objective D7. Facilitate and promote downtown Oakland’s position as the primary office center for the region.  
*The proposed project provides 500,000 square feet of office space located in Building C, with direct access to the BART station.*
- Objective D10. Maximize housing opportunities in the downtown to create a better sense of community  
*The proposed project provides 557 new residential units, including 233 affordable units of varying size.*
- Objective D11. Foster mixed use developments to help create a diverse, lively, and vibrant downtown  
*The proposed project provides a mixed-use transit-oriented development with ground floor retail. The pedestrian paseo envisions a food hub near the BART head houses, with a commercial kitchen, restaurants, and outdoor seating. The corner of 8<sup>th</sup> and Oak will also be a focal point for vibrant activity, with retail in Building C connecting to the activity of the Paseo.*

## 6. ZONING ANALYSIS OVERVIEW

The proposed project is located within the D-LM Lake Merritt Station Area District Zones. The intent of the Lake Merritt Station Area District (D-LM) Zones is to implement the Lake Merritt Station Area Plan. Development in this zoning district shall be consistent with the Lake Merritt Station Area Plan, of a high quality design, and include active ground floor uses where appropriate and feasible.

The objectives of the D-LM Lake Merritt Station Area District Zones are to:

1. Create a more active and vibrant Lake Merritt Station Area District to serve and attract residents, businesses, students, and visitors;  
*The proposed project creates a mixed-use, transit-oriented development on two central blocks of the Lake Merritt Station Area that will attract residents, businesses, office workers, students, and visitors. Block 1 will transform an existing parking lot into two mixed-use buildings with a paseo providing mid-block pedestrian connections to the BART station. Block 2 will provide an office tower and a mid-rise affordable residential building with ground floor active uses.*
2. Increase activity and vibrancy in the area by encouraging vital retail nodes that provide services, restaurants, and shopping opportunities;  
*The commercial spaces at Oak and the paseo will create a food hub, with restaurants and outdoor seating to create a food destination in the neighborhood. 8<sup>th</sup> and Oak will provide more ground floor commercial opportunities in Building C.*
3. Improve connections between the Lake Merritt BART Station and major destinations outside the Station Area District;  
*The pedestrian paseo provides an important connection between Laney College and the BART station.*
4. Improve safety and pedestrian-orientation;

*The proposed project provides pedestrian improvements including improved sidewalks with minimum 5.5 ft clear width, improved intersections with curb bulbs, curb ramps, and crosswalks, and activation to improve safety.*

5. Accommodate the future population, including families;  
*Building D provides 100 affordable residential units geared toward families. Building B provides 97 senior affordable residential units, accommodating the aging demographic.*
6. Increase the number of jobs and improve the local economy;  
*Building C provides 500,000 square feet of office space for future tenants. The office space will provide jobs and support the local economy.*
7. Identify additional recreation and open space opportunities and improve existing resources; and  
*The pedestrian paseo in Block 1 provides increased landscaping and greening of the block as well as opportunities for gathering and potential play space for children.*
8. Encourage and enhance a pedestrian-oriented streetscape.  
*Both blocks include improvements to the streetscape on all frontages, which provide pedestrian-oriented streetscapes with improved sidewalks, landscaping, and street furniture.*

D-LM-2 Lake Merritt Station Area District Pedestrian - 2 Commercial Zone. The intent of the D-LM-2 Zone is to create, maintain, and enhance areas of the Lake Merritt Station Area Plan District for ground-level, pedestrian-oriented, active storefront uses.

*The proposed project works to fulfill the Lake Merritt Station Area Plan vision for transit-oriented development at the Lake Merritt BART station. The project provides ground-level, pedestrian-oriented, active storefront uses along Oak St and portions of 9<sup>th</sup> St and 8<sup>th</sup> St. The project also provides pedestrian-oriented residential lobby entrances on Fallon St and a day care along Madison St.*

D-LM-4 Lake Merritt Station Area District Mixed - 4 Commercial Zone. The intent of the D-LM-4 Zone is to designate areas of the Lake Merritt Station Area Plan District appropriate for a wide range of Residential, Commercial, and compatible Light Industrial Activities.

*The proposed project within the D-LM-4 zone is consistent with the intent of the zoning. The portion of the project with D-LM-4 zoning includes the 7<sup>th</sup> St frontage and portions of Oak St and Madison St. Active retail use wrap the corner of Oak St onto 7<sup>th</sup> St and the day care activity wraps the corner of Madison and 7<sup>th</sup>. The rest of 7<sup>th</sup> is dedicated to back of house activities.*

**7. PROJECT SPECIFIC ZONING ANALYSIS AND DESIGN REVIEW**

**7.1 Zoning Analysis for the Preliminary Development Plan (PDP) for the Planned Unit Development**

	<b>Required D-LM-2, D-LM-4 LM-275</b> 001 016900100, 001 017100200 <b>(Block 1 and 2)</b>	<b>Proposed D-LM-2, LM-275</b> 001 016900100 <b>(Block 1)</b>	<b>Proposed D-LM-2/D-LM- 4, LM-275</b> 001 017100200 <b>(Block 2)</b>	Consistency with Base Zoning	PUD Bonus applied/ Variance/ CUP
<b>Use</b>					
Multifamily	P (with limits, not on ground floor)	Multifamily	Multifamily	Complies	
Administrative	P (with limits L4, L5)	None	Office	Complies	
General Retail	P	Retail	Retail	Complies	
Limited service restaurant	P	Limited service restaurant	NA	Complies	
Commercial Kitchen, Custom Manufacturing	C(L1)(L3)	Commercial kitchen with demonstration cooking area	NA	Not permitted in D-LM-2	PUD Bonus allows Custom Manufacturing 17.142.100.B
Daycare, Community Education Civic Activity (15 or more)	P (with limits L4, L5, see below)		Daycare facility (6,200 sf, up to 40 students)	Occupies more than 25% of ground floor facing Madison, so would need CUP	PUD Bonus allows Community Education Civic Activity 17.142.100A, so CUP not needed
Lot Dimensions	Min Width: 25 Min Frontage: 25 Min Lot Area: 4,000sf			Complies	
Min/Max Setbacks	Min front: 0 ft Max front and street side: 5 ft Max front and side for upper stories: 5 ft Side and rear: 0 ft		Building B has 22' setback from Fallon	Does not comply	PUD Bonus allows flexibility 17.142.100G
<b>Design Regulations</b>					
Ground floor commercial façade transparency	65% required for principal buildings with ground floor Nonresidential Facilities. On other facades, ½ the standard for the facade facing the principal street.	Not enough detail in PUD	Not enough detail in PUD	Not applicable: FDPs will provide more detail to determine compliance	
Min height of ground floor nonresidential facilities	15 ft	Building A: 15 ft Building B: 15 ft	Building C: 16 ft Building D: 16 ft	Complies	
Min width of storefronts	15 ft			Not applicable: FDPs will provide more detail to	



	<b>Required D-LM-2, D-LM-4 LM-275</b> 001 016900100, 001 017100200 <b>(Block 1 and 2)</b>	<b>Proposed D-LM-2, LM-275</b> 001 016900100 <b>(Block 1)</b>	<b>Proposed D-LM-2/D-LM- 4, LM-275</b> 001 017100200 <b>(Block 2)</b>	Consistency with Base Zoning	PUD Bonus applied/ Variance/ CUP
				determine compliance	
<b>Height, Bulk, and Intensity Area Specifications</b>					
Maximum Height	275' 275'	Building A: 275 ft Building B: 83 ft	Building C: 275 ft Building D: 83 ft	Complies	
Building Base	45' base, 85' with CUP 45' base, 85' with CUP	Building A: 48 ft Building B: 83 ft	Building C: 45 ft Building D: 83 ft	Building C complies Building A, B, D would need CUP to exceed 45' building base	PUD Bonus allows flexibility 17.142.100G, therefore CUP is not needed
Residential Density	110 sf of lot area/unit 110 sf of lot area/unit	Building A: 360 Building B: 97	Building C: 0 Building D: 100	Complies. 557 proposed	
Max Nonresidential FAR	12	40, 860 sf nonresidential	541,810 sf nonresidential	Complies	
Setback of Tower from Building Base	20 ft., along at least 50% of the perimeter length of the building base; 10 ft., along at least 50% of the perimeter length of base with CUP and additional findings 2.a, b, c	Building A: 10 ft setback, so CUP needed Building B: not a tower	Building C: 10 ft setback along Oak and 22 ft along Building D. Building D: not a tower	Building A and C provide 10 ft along at least 50% of perimeter, would need CUP	PUD Bonus allows flexibility 17.142.100G, therefore CUP is not needed
Max average per story lot coverage	75% of site area or 10,000 sf, whichever is greater	Building A: ~65% of site area (12,480/19,327)	Building C: 180x141=25,380 /35218 72%	Complies	
Max tower elevation length	150 ft. may be increased by up to 30% with CUP	Building A: 195'	Building C: 180'	Building A & Building C include 30% increase, CUP would be needed	PUD Bonus allows flexibility 17.142.100G, therefore CUP is not needed
Max diagonal Length	180 ft may be increased by up to 30% with CUP	204.5'	232'	Buildings include 30% increase, CUP would be needed.	PUD Bonus allows flexibility 17.142.100G, therefore CUP is not needed
Min distance btw towers	50 ft may be increased by up to 30% with CUP	NA	NA	NA	
<b>Open Space</b>					
Senior Housing Unit	38 sf/unit Block 1: B: 97 *38=3686	Building A requires: 27,000 Sf;	Building D requires: 6,000 sf Building D	Complies	
Affordable Housing Unit	60 sf/unit Block 2: D: 100*60=6,000	Building A provides: Paseo*.8= 22,361+	provides: Courtyard 5,600 sf + Community		

	<b>Required D-LM-2, D-LM-4 LM-275</b> 001 016900100, 001 017100200 <b>(Block 1 and 2)</b>	<b>Proposed D-LM-2, LM-275</b> 001 016900100 <b>(Block 1)</b>	<b>Proposed D-LM-2/D-LM- 4, LM-275</b> 001 017100200 <b>(Block 2)</b>	Consistency with Base Zoning	PUD Bonus applied/ Variance/ CUP
Other residential Unit	75 ft/unit Block 1:A: 360*75=27,000	2,500+2,570+12,900 = 40,331  Building B requires: 3,686 sf Building B provides: Paseo*.2 = 5,474 + 1690+250 sf = 7,414	Room 1,200 sf = 6,800 sf		
Landscaping Requirements	At least 50% of rooftop or courtyard usable open space area shall include landscaping enhancements. At least 30% of public ground floor plaza shall include landscaping enhancements.	NA: Detail to be provided at FDP.		NA. Compliance to be determined for each FDP	
17.101G.070 Special regulations for large-scale developments.	No development which involves more than 100,000 sf of new floor area shall be permitted except upon the granting of a Major Conditional Use Permit pursuant to Chapter 17.134.	Project over 200,000 sf of new floor area.		Major CUP needed for size	Major CUP needed for size
<b>17.116 Parking</b>					
Parking - Multifamily	Min: No spaces required Max: 1.25 space/dwelling unit	Building A:105 spaces Building B: 0	Building C: 0 Building D: 45	Complies	
Parking - Commercial	Min: No spaces required Max: Ground floor: 1 space/300 sf of floor area Above Ground floor: 1 space/500 sf of floor area	Building A: 0 Building B: 0	Building C: 254 Building D: 0	Complies	
Parking Civic	Min: No spaces required	0	Building D: 4	Complies	
Parking - Industrial	Min: No spaces required for under 25,000 sf	0	0	Complies	
17.116.105 A. Car share parking	Car share spaces required: A: 2 B: 1 C: 0	Building A: 0 Building B: 3	Building C:1 Building D: 0	Complies. Building B and D provide the car share for Building A and D. FDPs	

	<b>Required D-LM-2, D-LM-4 LM-275</b> 001 016900100, 001 017100200 <b>(Block 1 and 2)</b>	<b>Proposed D-LM-2, LM-275</b> 001 016900100 <b>(Block 1)</b>	<b>Proposed D-LM-2/D-LM- 4, LM-275</b> 001 017100200 <b>(Block 2)</b>	Consistency with Base Zoning	PUD Bonus applied/ Variance/ CUP
	D: 1			will provide detail for function of access to car share	
17.116.105 B. Transit Passes	Make permanently available a monthly transit benefit to each dwelling unit in an amount equal to either one-half the price of an Adult 31-Day AC Transit Pass or an AC Transit EasyPass.	Not enough detail		Condition of Approval will require proof of transit passes	
17.116.120 Loading	Building A: 1 berth Building B: 1 berth Building C: 3 berths Building D: 1 berth	A: 2 residential loading berths B: 0 residential loading berths	C: 3 office loading berths D: 1 residential loading berths	Buildings A, C, D comply. Building B does not comply	Building B requires Minor Variance for off- street residential loading
17.116.210 and 12.04.270 Driveway Openings	No more than 35 ft wide and driveways serving a single parcel of property separated by at least 25 feet			Complies	
<b>17.117 Bike Parking</b>					
Multifamily: Long Term	A: 1 sp/4 du = 90 B: 1 sp/10 du = 10 D: 1 sp/4 du = 25	A: 90 LT B: 10 LT	D: 28 LT	Complies	
Multifamily: Short Term	A: 1 sp/ 20 du = 18 B: 1 sp/ 20 du = 5 D: 1 sp/ 20 du = 5	A: 18 B: 5	C: NA D: 5	Complies	
Restaurant café: Long Term	A: 1 sp/ 12,000 sf = 0, Min 2 B: 1 sp/ 12,000 sf = 0, Min 2	A: 2 LT B: 2 LT	C: NA D: NA	Complies	
Restaurant café: Short Term	A: 1 sp/2,000 sf = 3 B: 1 sp/2,000 sf Min 2	A: 3 ST B: 2 ST	C: NA D: NA	Complies	
Retail: Long Term	C: 1 sp/12,000 sf Min 2	NA	C: 2 LT D: NA	Complies	
Retail: Short Term	C: 1 sp/5,000 sf Min 2 ST	NA	C: 2 ST	Complies	
Day Care: Long Term	D: 1 sp/ 10 employees. Min 2 sp 20 employees	NA	D: 2 LT	Complies	
Day Care Short Term	D: 1 sp/ 20 students Min 2 sp	NA	D: 2 ST	Complies	
Office: Long Term	C: 1 sp/10,000 sf = 50	NA	C: 52 LT	Complies	
Office: Short Term	C: 1 sp/20,000 sf = 25	NA	C: 26 ST	Complies	

	<b>Required D-LM-2, D-LM-4 LM-275</b> 001 016900100, 001 017100200 <b>(Block 1 and 2)</b>		<b>Proposed D-LM-2, LM-275</b> 001 016900100 <b>(Block 1)</b>	<b>Proposed D-LM-2/D-LM- 4, LM-275</b> 001 017100200 <b>(Block 2)</b>	Consistency with Base Zoning	PUD Bonus applied/ Variance/ CUP
Custom Manufacturing Long Term	B: Min 2 sp		B: 2 LT	NA	Complies	
Custom Manufacturing Short Term	B: 0 required		0	NA	Complies	
TOTAL Bike Parking	LT: 104 ST: 27	LT: 77 ST: 32	A: LT 92; ST 21 B: LT 12; ST 7	C: LT 54; ST 28 D: LT 27; ST 7	Complies	
17.117.130 Lockers/Showers	C: min 2 showers per gender +1 shower/ gender for each 150,000 sf. above 150,000 sf. 4 lockers per shower		NA	C: 5 showers per gender, 20 lockers per gender	Complies	
17.118 Recycling Space	2 cubic feet/ du, min 10 cubic feet. 2 cubic feet/ 1,000 sf commercial A: 720 + 9 = 729 cuft B: 194 +7 = 201 C: 1016 D: 200+12.4 = 212		A: 730 B: 204	C: 1071 D: 212	Complies	

The applicant is not pursuing the State Affordable Housing Density Bonus, but is providing a total of 233 affordable residential units, which is over 40% of the total residential units, including:

- Building A: 36 units up to 120% Area Mean Income (AMI);
- Building B: 30 units at 30% AMI, 32 units at 50% AMI, 35 units at 60% AMI; and
- Building D: 21 units at 30% AMI, 40 units at 50% AMI, 39 units at 60% AMI.

**7.2 Planned Unit Development Bonus – 17.142.100**

The following aspects of the project typically require variances or conditional use permits under the applicable base zoning regulations; however, these aspects of the project are permitted through the Planned Unit Development Bonus (17.142.100), as part of this PUD application:

- Commercial Kitchen (Custom Manufacturing) in Building B. The Commercial Kitchen (Custom Manufacturing) is a conditionally permitted activity in the D-LM-2 zone, but the PUD bonus (17.142.100.B4) allows Custom Manufacturing activity by right, so a CUP is not necessary.
- Daycare, Community Education Civic Activity in Building D. Community Education Civic Activity requires a Conditional Use Permit in the D-LM-2/D-LM-4 zones, but the PUD Bonus (17.142.100.A1) allows the Community Education Civic Activity by right, so a CUP is not necessary.
- Waiver or Reduction of Yard and Other Dimensional Requirements (17.142.100.G). Except as otherwise provided in Subsection 17.142.110.E, the minimum lot area, width, and frontage; height; and yard requirements otherwise applying may be waived or modified for the purpose of promoting an integrated site plan.

This PUD application is seeking a waiver or reduction of the following dimensional requirements through the PUD bonus:

- Maximum setback. The maximum front and street side setback for the first through third stories is a maximum of 5 feet for Building B, but the building is setback from Fallon Street 23.5 feet. The PUD Bonus (17.142.100.G) allows for waiver or reduction of this requirement if the waiver promotes an integrated site plan. In this case, the increased street side setback allows for a wide public entrance into the paseo and a plaza area in front of the lobby entrance to Building B.
- Maximum Height of Building Base. The maximum height of the building base in the LM-275 height area is 45 feet, or 85 feet upon the granting of a CUP and consistent with the additional findings below. Buildings A, B, C, and D all exceed the maximum building base height of 45 feet, but are below 85 feet. A CUP is not required, however, because the PUD Bonus (17.142.100.G) allows for a waiver of height requirements for the purpose of promoting an integrated site plan. The height in the building base of all four buildings meets the findings below:
  - a. The proposal is consistent with the intent and desired land use character identified in the Lake Merritt Station Area Plan and its associated policies;
  - b. The proposal will promote implementation of the Lake Merritt Station Area Plan; and
  - c. The proposal is consistent with the desired visual character described in the Lake Merritt Station Area Plan and Lake Merritt Station Area Design Guidelines, with consideration given to the existing character of the site and surrounding area.
- Setback of Tower from Building Base. The Building A and C towers are required to setback the tower from the building base a minimum of 20 ft., along at least 50% of the perimeter length of the building base or 10 ft., along at least 50% of the perimeter length of base upon granting of a CUP and additional findings in findings a, b, c below. Both Building A and C meet the standard of setting back the tower from the base a minimum of 10 feet along at least 50% of the perimeter of the base of the building, requiring a CUP, but the PUD Bonus allows a waiver of dimensional requirements, so a CUP is not required.
- Maximum Tower Elevation Length. Building A has a tower elevation length of 195 feet and Building C has a tower elevation length of 180 feet. The towers have a maximum tower elevation length requirement of 150 feet, but this may be increased by up to thirty percent (30%) upon determination that the proposal conforms to the general use permit criteria set forth in the conditional use permit procedure in Chapter 17.134 and to the following additional use permit criteria:
  - a. The proposal will result in a signature building within the neighborhood, City, or region based on qualities, including but not limited to, exterior visual quality, craftsmanship, detailing, and high quality and durable materials.

Both of the towers in Building A and Building C have the potential to be signature buildings, but many of the design details will be determined at the Final Development Plan (FDP) stage. The PUD Bonus allows waiver of this dimensional requirement, but because it is not directly related to an integrated site plan, the FDP shall ensure a signature building is provided.

- Maximum Diagonal Length. The Maximum Diagonal Length of each tower is 180 feet, but this may be increased by up to thirty percent (30%) upon determination that the



proposal conforms to the general use permit criteria set forth in the conditional use permit procedure in Chapter 17.134 and to the following additional use permit criteria:

- a. The proposal will result in a signature building within the neighborhood, City, or region based on qualities, including but not limited to, exterior visual quality, craftsmanship, detailing, and high quality and durable materials.

The diagonal length for the Building A Tower is 204.5 feet and the diagonal length for the Building B Tower is 232 feet, satisfying the increase by 30%, however the determination for a signature building cannot be made until the FDP stage. This dimensional standard can be waived through the PUD Bonus, but in order to ensure this waiver is consistent with contributing to an integrated site plan, the FDP shall ensure a signature building is provided.

## ***7.2 Design Review for the Preliminary Development Plan (PDP) for the Planned Unit Development***

The Preliminary Development Plan provides conceptual designs for the overall project with supporting Design Guidelines to guide the design of future phases of the project. The PDP and associated Design Guidelines need to follow the Lake Merritt Station Area Design Guidelines (LMSADG), as discussed below. All Final Development Plans will be expected to be consistent with the PDP, the LMSADG, and the Design Guidelines associated with the PDP.

### ***7.2.1 Consistency with the Lake Merritt Station Area Design Guidelines (LMSADG)***

In accordance with 17.101G.020, the project is required to conform with the design review criteria listed in Chapter 17.136 and with the “Design Guidelines for the Lake Merritt Station Area Plan” (LMSADG). The LMSADG are analyzed below, with staff feedback indented and in italics.

#### **Site Planning and Building Orientation**

**DG-2. Streetfront Location.** Spatially define the streetfront by locating storefronts near the property lines facing the street and adjacent to one another. Build the ground level of commercial buildings near sidewalks and close to side property lines. A consistent series of commercial buildings constructed at the sidewalk and adjacent to one another creates a street wall and a defined pedestrian space. Each zoning district has a maximum setback limit to create or maintain this storefront pattern.

*Buildings in Block 1 and Block 2 are located near the property lines facing the street.*

*Building B is setback more than 5 feet from Fallon St to accommodate improved access to the paseo and to create a small plaza in front of the primary entrance. Block 1 does not provide ground level commercial along the full frontage of 8<sup>th</sup> St and 9<sup>th</sup> St, but does provide active corners.*

**DG-5. Define Open Spaces.** Site buildings and locate plazas, courtyards, seating, and visually interesting architectural features to encourage interaction among occupants and passersby. Configure buildings to define open spaces and provide visibility and accessibility from a public street, as shown in Figure 2. Special building forms (i.e. towers) and site improvements should be incorporated to help organize and accent spaces by framing entrances, terminating views, and highlighting central focal points.

*Buildings A and B in Block 1 are sited well to define the public paseo and to welcome pedestrians from Fallon Street through the paseo to the buildings and the BART head houses.*

**DG-7 Corner Building Design.** Emphasize and highlight architectural features at block corners to visually define and animate the intersection and facilitate pedestrian flow.

*The PDP does not provide enough detail to determine if the corners are being adequately emphasized and highlighted, but staff would like to ensure both the project specific guidelines and Final Development Plans adequately satisfy this guideline.*

*In particular, the following corners should be emphasized:*

- *8<sup>th</sup> and Oak in Building C*
- *8<sup>th</sup> and Fallon in Building B*
- *9<sup>th</sup> and Fallon in Building A*

**DG-8 Primary Lot Frontage.** Locate the primary building façade and main entrance along the primary lot frontage. The primary frontage should further be maximized by active building walls and addressed by the most active, articulated and public façade of a building. Active uses, such as storefronts, dining areas, lobbies, and offices should front onto the primary lot frontage.

- Primary lot frontages include all street-facing frontages, and can also include frontages that address public spaces that will likely see the most pedestrian activity or serve as important gateways.
- Corner lots or sites that encompass a block may have more than one primary frontage. *Block 2 generally complies with this guideline, with the primary lot frontage on 8<sup>th</sup> St including pedestrian entrances to residential lobby, commercial lobby, and access to retail. The Oak St frontage also contains active uses, with retail storefronts, transparency, and pedestrian entrances.*

*Block 1 generally does not comply with this guideline, with back of house activities dominating the primary lot frontage. Building A primary lot frontage is 9<sup>th</sup> St. The applicant has improved the design from the initial submittal by consolidating the back of house activities and activating the corners. Ideally, the activated corners could be extended and the curb cuts could be reduced and consolidated to reduce the impact on this primary façade.*

*Building B's primary lot frontage is on 8<sup>th</sup> St. With a constrained lot, all of the back of house activities are located along this frontage. A secondary entrance to the residential lobby is provided on 8<sup>th</sup> St, but staff would ideally like to see additional pedestrian entrances along the frontage, potentially a pedestrian entrance to the commercial kitchen space near Oak St, although there is a grade change that makes this difficult. The FDP for Building B should show design details to activate the frontage, like adding windows, glazing, awnings, and other details to the 'back of house' activities, to help create an appearance of an activated facade.*

**DG-12 Screening and Location of Building Equipment and other Non-Active Spaces.**

Mechanical, electrical, and all other building equipment, as well as non-active spaces, such as parking area, locker areas or mechanical rooms, should be concealed from all public right-of-ways, pedestrian paths and adjacent buildings; and should not be located along or within 30 feet of the ground floor street frontage

*Block 2 has all of the back of house activities like mechanical rooms, loading, trash, etc located along and within 30 feet of 7<sup>th</sup> St, but staff feels this is the most logical location for these non-active spaces that need to have access to a street.*

*Block 1 has a more challenging site plan because there is not a logical location for non-active spaces. Building A has located a portion of the mechanical equipment below grade, but a portion of it is located along 9<sup>th</sup> St, which is the Principal Street and primary façade, so does not comply with this guideline. Building B has all of the non-active space fronting 8<sup>th</sup> St, which is the Principal Street and primary façade. There is not necessarily a more logical place for these non-active spaces, but staff would like to see this non-active space minimized as much as possible to reduce the negative impact to 8<sup>th</sup> St and 9<sup>th</sup> St.*

### **Building Massing and Scale**

#### **DG-18 Transitions in Building Height, DG-19 Step Back Above the Podium Height, and DG-20 Reduce Overall Massing.**

*Each block has one tower and one mid-rise building, creating a transition in building height within the block. In addition, tower step backs are used to transition from the building base to the tower. The overall massing in Block 1 is reduced by using the paseo to separate Building A and Building B, drawing the public through the site from Fallon to Oak St.*

### **Towers**

#### **DG 22-25: Slender Towers, Tower Spacing, Distinguish Tower Design, and Skyline.**

*Building A is naturally a slender tower along Fallon and Oak St because of the shape of the parcel, but does exceed the tower elevation dimensions along 9<sup>th</sup> St. Building A tower is located next to the mid-rise Building B, providing appropriate spacing. The specific design details of the tower will be evaluated at the FDP stage and will need to meet the condition of approval:*

*“In order to meet the requirements for waiving the tower dimensional standards for maximum diagonal length and maximum tower elevation length, ensure the proposals for both towers will result in a signature building within the neighborhood, City, or region based on qualities, including but not limited to, exterior visual quality, craftsmanship, detailing, and high quality and durable materials.”*

*Building C does provide the minimum step back from the base height, but is otherwise not a very narrow tower. It is not located adjacent to another tower. The specific tower design will be evaluated at the FDP stage and will need to meet the condition of approval:*

*“In order to meet the requirements for waiving the tower dimensional standards for maximum diagonal length and maximum tower elevation length, ensure the proposals for both towers will result in a signature building within the neighborhood, City, or region based on qualities, including but not limited to, exterior visual quality, craftsmanship, detailing, and high quality and durable materials.”*

### **Building Façade Articulation**

**DG-26 – Pedestrian Scale, DG-27 Active Upper-Stories, DG-28 Articulation, DG-29 District Ground Floor**

*The PDP is not required to provide this level of detail, but the FDP for each building will have to provide design details to ensure the design intent is met. Building A on 9<sup>th</sup> St and Building B on 8<sup>th</sup> St are particularly concerning and will need to provide fine grain scale, multiple entries, and articulation including bays, horizontal banding, sills, fenestration, alcoves, awnings/canopies, trellises, well defined entries, storefront design, and other pedestrian amenities.*

**DG-30 Ground Floor Entrances.** Carefully design entrances to be distinct and prominent features of a building, particularly lobby entrances.

- At least one prominent pedestrian entrance should be provided for each building and face the principle street. Main entrances should always face the principal street, not parking lots.
- A clear, hierarchical distinction should be made between primary entrances and secondary entrances. Main entrances should be larger than other doors on the façade and clearly

*This is a critical design issue that is not clearly being met in Building A or Building B. Building A does have pedestrian entrances for ground floor commercial spaces facing the Principal Street (9<sup>th</sup> St), but the primary pedestrian entrance for the residential lobby is located on Fallon. A corner entrance on 9<sup>th</sup> and Fallon might help to satisfy this design guideline. An entrance to the residential lobby for Building B was added on the Principal Street 8<sup>th</sup> St, but there are no other pedestrian entrances along the 8<sup>th</sup> St frontage. A pedestrian entrance for the commercial kitchen would help activate the frontage.*

**Parking**

**DG-91 Location.** Where possible, locate parking structures either partly or entirely below grade. Surface parking lots should be considered temporary uses. If parking is located above ground, locate commercial building space at the street, at least 15 feet in height and 20 feet deep.

*Building C provides the parking below grade. Building A provides parking on the 2 -4 floors of the project on Floors 2-4. Building D provides parking at the ground level, but it is wrapped with day care activities along Madison and residential lobby along 8<sup>th</sup> St.*

**DG-97 Encapsulation.** On sites that are half a block or greater (30,000 square feet or greater) in size, at least 50 percent of the above grade parking should be encapsulated, or wrapped so that the parking area is not apparent from the public right-of-way.

*Building A occupies approximately half a block, but the parcel size is less than 30,000 square feet, but the design concept could still apply to Building A. The parking is not encapsulated or wrapped on floors 2-4 and is apparent from the public right-of-way.*

**DG-98 Integral Design.** Design all visible structured parking as an integral part of the project it serves, consistent in style and materials with the balance of the project.

*The PUD provides preliminary designs for Building A with the parking structure fronting 9th St and visible from the right of way, but the preliminary designs do not dictate how the parking will be integrated into the overall design of the project or use contrasting, high quality materials to create an architectural feature. The FDP should provide the detail to show how this guideline will be met.*

## Utilities

The design meets all of the applicable Utilities guidelines, which include: Location and Utilities, and Undergrounding. These items correspond to Design Guidelines DG 108 – DG 109 (page 35).

*The impact of back of house uses should be minimized along 8<sup>th</sup> and 9<sup>th</sup>, including areas for utilities. Utilities that have to be accessed off 8<sup>th</sup> and 9<sup>th</sup> should be enclosed in the building, with high quality access doors, architectural details, landscaping, and other features to minimize the negative impact of the non-active space on the pedestrian realm.*

## Streetscape Design Guidelines

### General

*The proposed project includes streetscape improvement that improve walkability, pedestrian comfort, bicyclist comfort, and accommodate transit. Bike lanes are added on 9<sup>th</sup> St, Fallon St, 8<sup>th</sup> St, and Oak St. Each street frontage includes a minimum 5.5' pedestrian clear width sidewalks, landscape area, and street furniture area. All streetscape designs will be verified in detail at the FDP for public infrastructure phase.*

## 8. ZONING AND DESIGN RELATED ISSUES

### 8.1 Design

Staff has worked with the applicant to refine the site plan and preliminary design of the project. The PDP includes Design Guidelines, which will guide future phases of the project.

### 8.2 Issues

In general, staff finds the project to be well-designed and much improved since the original submittal. This is a signature project, satisfying the high-level vision from the Lake Merritt Station Area Plan of transit-oriented development at the BART station. Staff generally supports the project. That said, staff has a few remaining design concerns and asks the DRC to consider the following:

- **Building A:**

- **9<sup>th</sup> St Frontage.** 9<sup>th</sup> St is designated as the Principal Street, is planned for active ground floor uses in the LMSAP, and is designated as a Commercial Corridor in the Planning Code. The Planning Code requires 65% transparency between 2-9 ft along the frontage. The proposed project, however, focuses the back of house activities on this frontage, including garage access, residential off-street loading, and space for mechanical, electrical, and plumbing. Because these activities have to front on a public street, there isn't a preferred location for these non-active uses because this is a constrained site due to the BART tunnel, BART head houses, and narrow frontage on Fallon, but it does not strictly comply with the design guidelines or the intent of the Planning Code. Since the initial submittal, the applicant has decreased the back of house area on 9<sup>th</sup> St, adding a basement to accommodate utility space. The limited service restaurant wraps the corner onto 9<sup>th</sup> and pedestrian entrances to the restaurant space have been added on 9<sup>th</sup>, improving activation of that corner. In addition, the applicant increased the residential lobby and residential amenity space that wraps from Fallon St onto 9<sup>th</sup> St, increasing activation toward Fallon. To further improve the 9<sup>th</sup> St frontage, staff recommends removing the



second off-street residential loading berth, which exceeds the requirements of the Planning Code. This will further reduce the non-active façade on 9<sup>th</sup> and reduce the extent of curb cuts on 9<sup>th</sup> St, which is a concern for OakDOT because of the negative impact on the Class IV bike lane.

- *Does DRC think the off-street residential loading should be reduced on 9<sup>th</sup> St to reduce the non-active space on this frontage and to reduce the curb cut? Does DRC have other design feedback for this important frontage?*
- **Garage Screen** (floors 2-4). The proposed project includes three floors of parking garage that extend to the facade on Floors 2-4 on 9<sup>th</sup> St, Oak St, and along the Paseo. The applicant intends to use natural ventilation for the parking garage, so a minimum of 25% of the façade will be porous. The Design Guidelines recommend that 50% of the above grade parking be encapsulated or wrapped so the parking is not apparent from the public right of way. In addition, the guidelines state that visible structured parking should be consistent in style and materials with the rest of the project OR contrasting, high quality materials should be used to create an architectural feature. Because the proposal is preliminary in nature, the design details of the façade for these three stories has not been determined and will be provided in the FDP.
  - *Does the DRC have design guidance on the façade for the three stories of parking structure that are visible from the street that could be incorporated into the project specific design guidelines or inform the FDP for Building A?*
- **Corner of 9<sup>th</sup> and Fallon.** The LMSADG and the project specific guidelines call for an emphasized corner at 9<sup>th</sup> and Fallon, but the preliminary designs in the PUD do not indicate an emphasized corner.
  - *What type of design details would DRC like to see at this corner at the FDP phase to help emphasize this corner?*
- **Building B:**
  - **8<sup>th</sup> St Frontage.** 8<sup>th</sup> St is designated as the Principal Street, is proposed for active ground floor uses in the LMSAP, and is designated as a Commercial Corridor in the Planning Code. The Planning Code requires 65% transparency between 2-9 ft along the frontage. Building B is located on a constrained site, with 8<sup>th</sup> St the only primary frontage facing a street, because the building is blocked from Oak St by the BART head houses and only has a narrow frontage on Fallon St. Therefore, the back of house activities that are required to be located along a public street have to be located on the 8<sup>th</sup> St frontage. The applicant has worked to improve the activation of 8<sup>th</sup> St by wrapping the residential lobby from Fallon St onto 8<sup>th</sup> St and adding a pedestrian entrance from 8<sup>th</sup> St, however the design guidelines state that the primary entrance should be located on the Principal Street. They have also added windows on 8<sup>th</sup> St that provide visual access to the commercial kitchen and a community room, but do not provide physical pedestrian access.
    - *Based on the intent of the design guidelines, vision of the LMSAP, and the requirements of the Commercial Corridor designation, but considering the site constraints of Building B, does the DRC have design feedback on how to improve this façade?*

- **Towers – Building A and Building C**

- The tower regulations in the Planning Code and the LMSADG lay out key requirements for the design of towers. The Planning Code allows for the maximum tower elevation length and diagonal length to be increased by up to 30% through a CUP if the following criteria is met: Towers will result in a signature building within the neighborhood, City, or region based on qualities, including but not limited to, exterior visual quality, craftsmanship, detailing, and high quality and durable materials.
  - *Based on the criteria to be a signature building, does DRC have feedback on the preliminary designs of the tower for Building A or Building C or feedback on the project specific design guidelines for towers?*


### **8.3 On-going, Non-design Related Issues**

- Minor variance for Off-Street Residential Loading for Building B. Building B does not provide off-street parking and does not include any curb cuts or driveways off 8<sup>th</sup> St. The applicant has requested a variance from providing off-street residential loading and proposes providing a loading space on Fallon St.
- Major Conditional Use Permit for Large Project. 17.101G.070 requires development over 100,000 square feet of new floor area shall not be permitted except upon the granting of a conditional use permit pursuant to the conditional use permit procedure in Chapter 17.134.

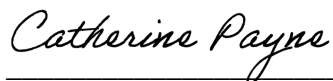
## **RECOMMENDATION**

Staff recommends the DRC review and comment on the proposed Lake Merritt BART TOD Preliminary Development Plan, Tentative Tract Map, and associated Design Guidelines (PLN20038), with attention to the issues raised by staff in this report.

Prepared by:

  
\_\_\_\_\_  
Dara O'Byrne, Planner IV

Reviewed by:

  
\_\_\_\_\_  
Catherine Payne, Acting Development Planning Manager  
Bureau of Planning

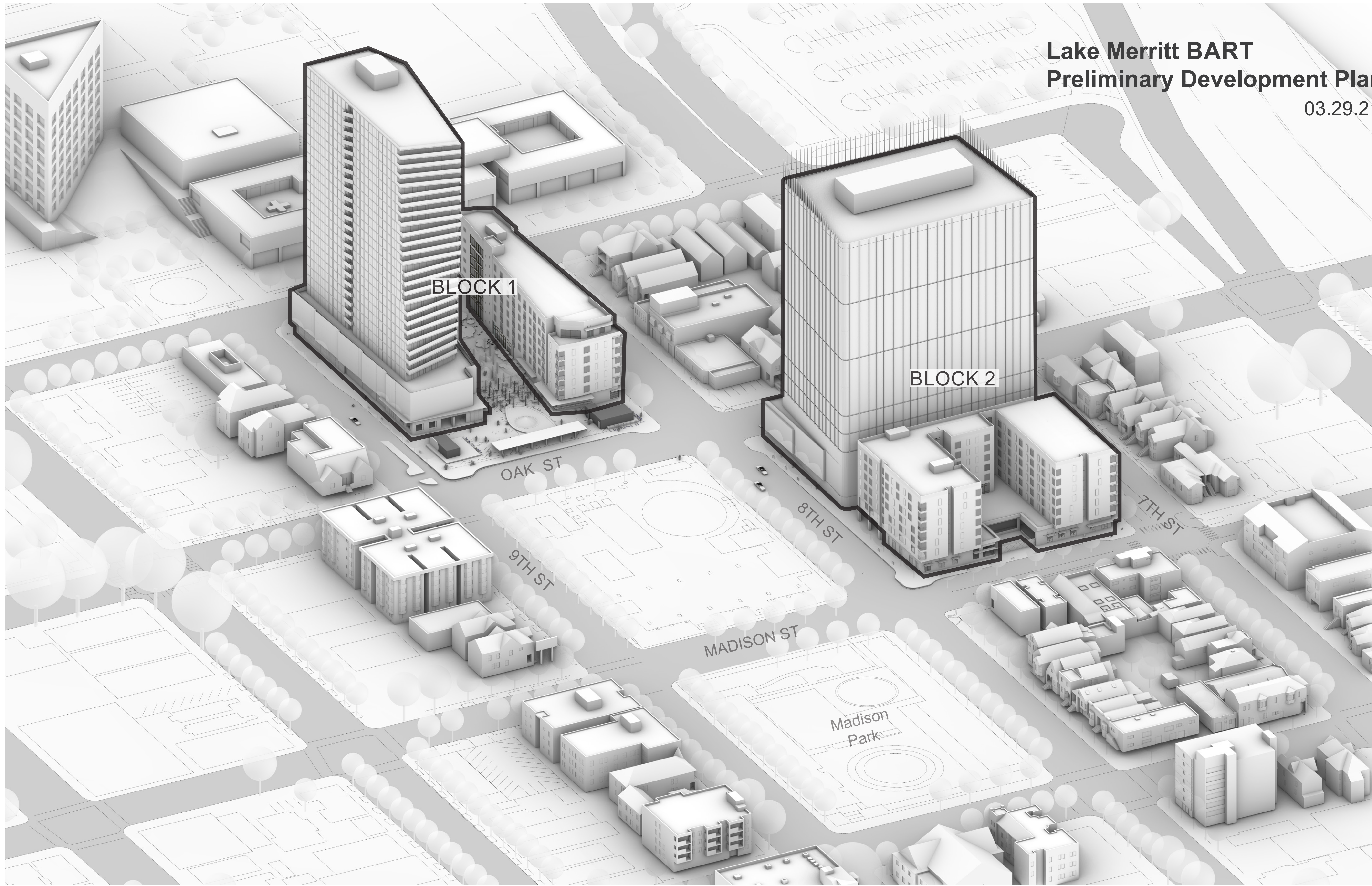
**Attachment A:** Proposed Lake Merritt BART TOD PUD/PDP and Design Guidelines

**Attachment B:** Vesting Tentative Tract Map dated March 29, 2021

## **ATTACHMENT A:**

Proposed Lake Merritt BART TOD PUD/PDP, dated March 29, 2021  
Lake Merritt BART TOD Design Guidelines





**Lake Merritt BART  
Preliminary Development Plan**

03.29.21

EAST BAY ASIAN LOCAL  
DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
ARCHITECTS  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com

SOLIMON CORDWELL BUENZ ARCHITECTS  
**SCB**  
335 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
415/776-2600  
www.scba.com

INWILLERUEHL  
ARCHITECTS  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/851-1686  
www.inwilleruehl.com

**BKF100**  
YEARS  
ENGINEERS, SURVEYORS, PLANNERS  
225 BUCKLE UP, SUITE 200  
REDWOOD CITY, CA 94063  
(650) 483-4300  
www.bkf.com

**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPK COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPK'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPK4 COMMENTS	03/19/2021

DATE:  
SCALE:  
**COVER SHEET**

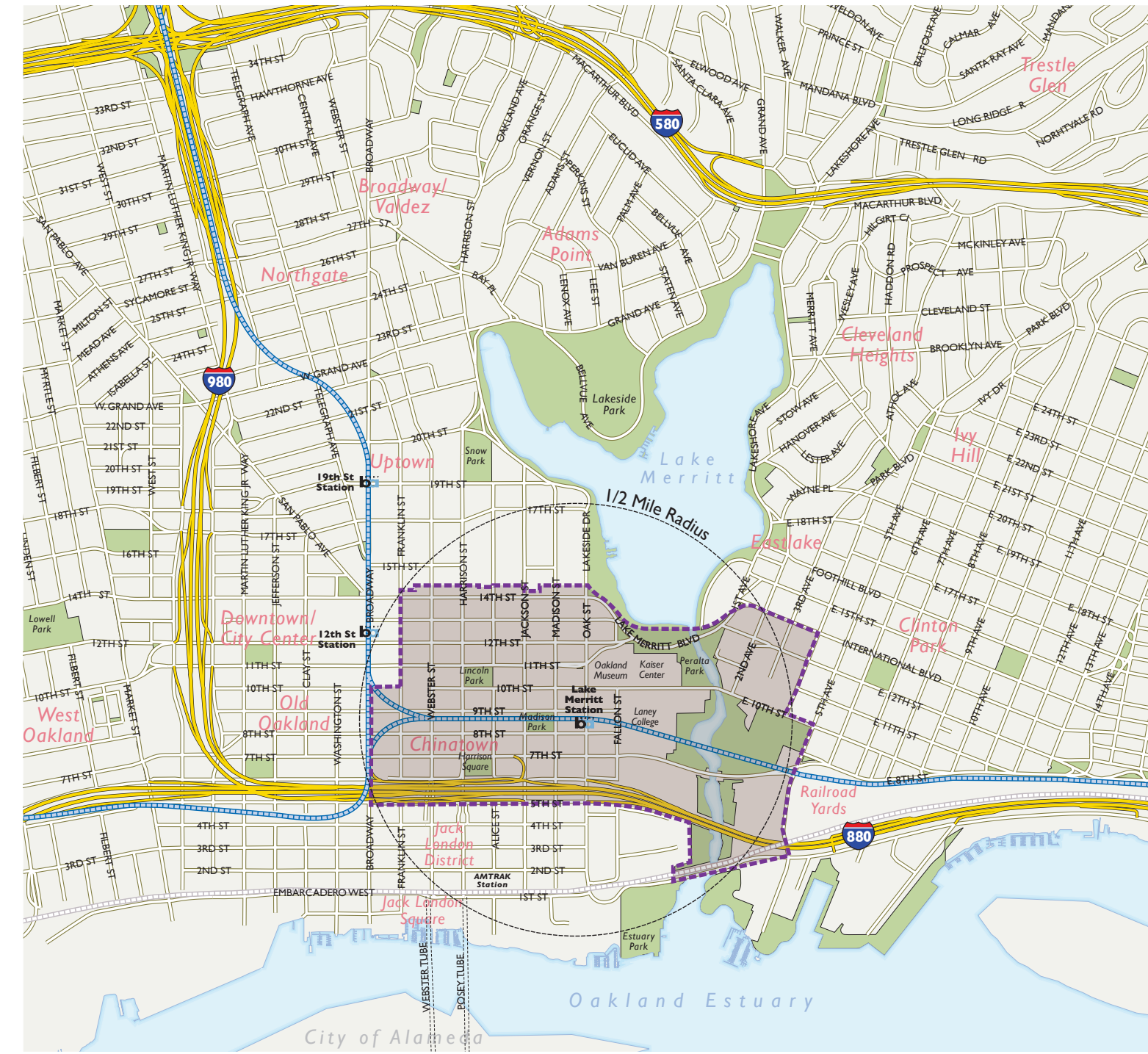
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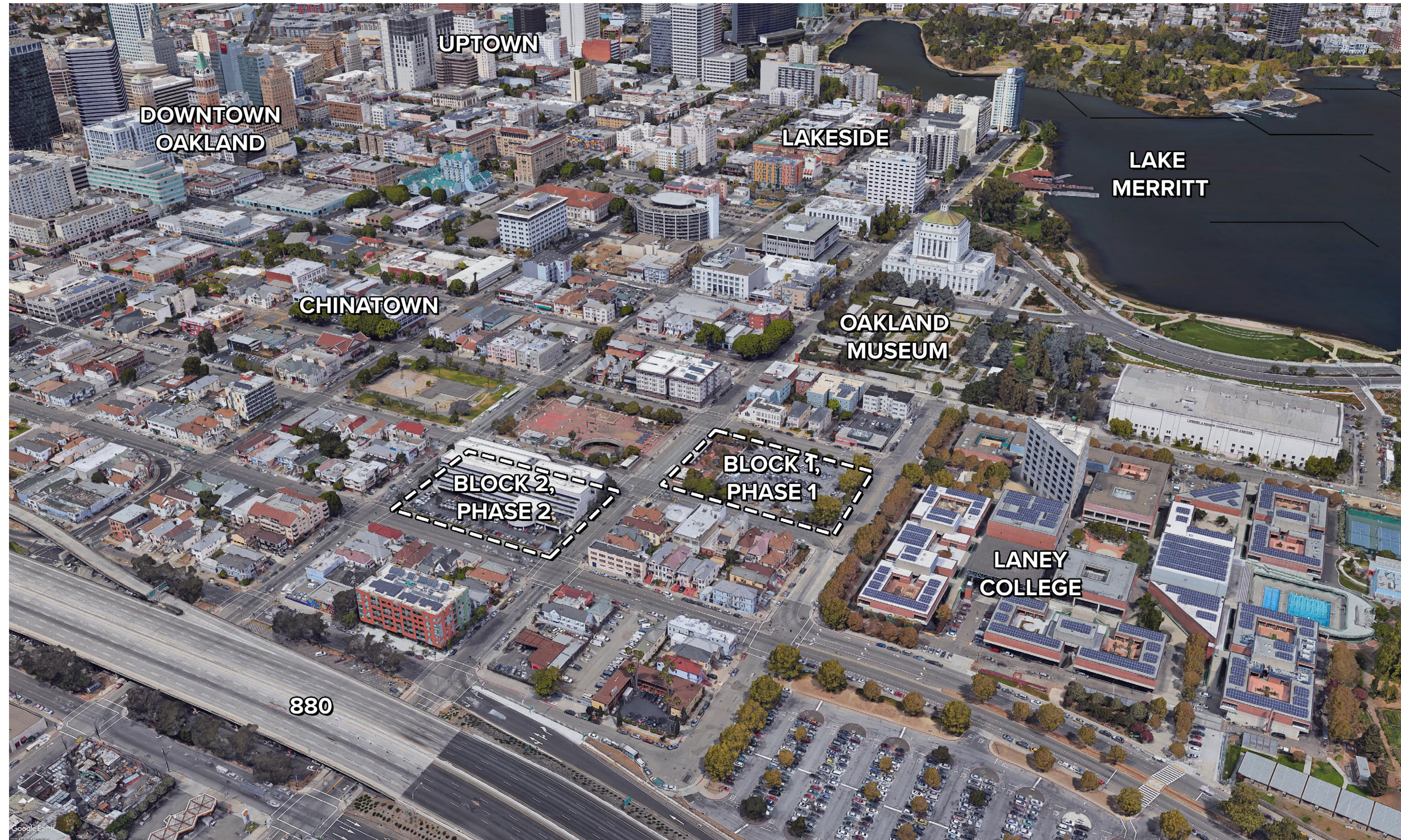
# Lake Merritt BART Oakland/ Chinatown Redevelopment PDP PACKAGE

## GENERAL NOTE:

BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the the project as presened is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations. FDP Design must demonstrate per BART's requirements that there are no structural impacts to the tunnel per BFS standards.



\*IMAGE FROM LAKE MERRITT SPECIFIC PLAN



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EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
101 MISSION ST #420  
SAN FRANCISCO, CA 94105

**PVATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pvatok.com

SOLIMON CORDWELL BUENZ ARCHITECTS  
355 CALIFORNIA ST  
SAN FRANCISCO, CA 94111  
415/716-2600  
www.scb.com

WINWILLERUEHL  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/851-1686  
www.winwilleruehl.com

**BKF100+**  
ENGINEERS, SURVEYORS, PLANNERS  
225 BROADWAY DR. SUITE 200  
REDWOOD CITY, CA 94063  
(650) 483-3300  
www.bkf.com

# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

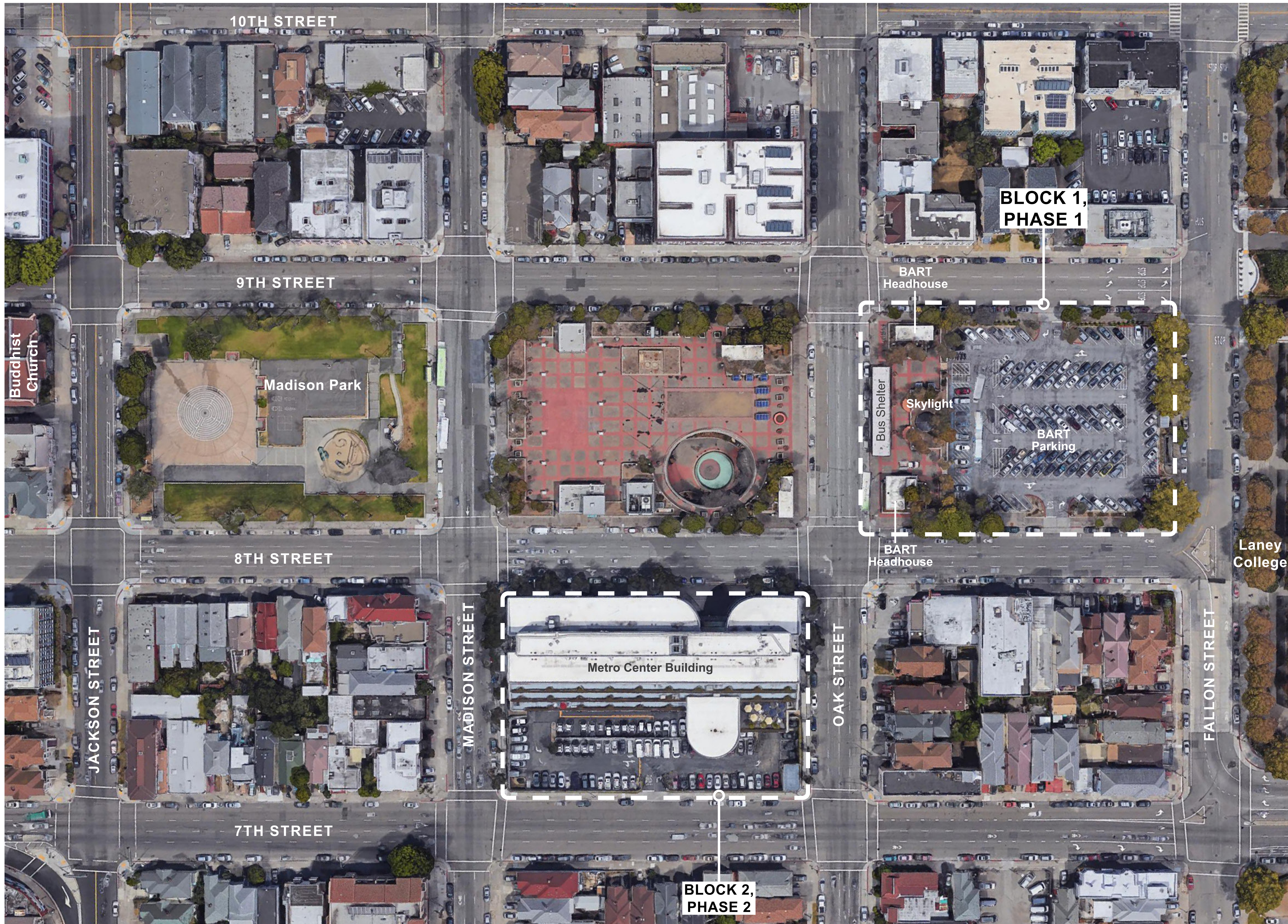
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6	REVISED RESPONSE TO POPK COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPK COMMENTS	03/19/2021

DATE:  
SCALE:  
**TITLE SHEET & DRAWING INDEX**

# A0.1





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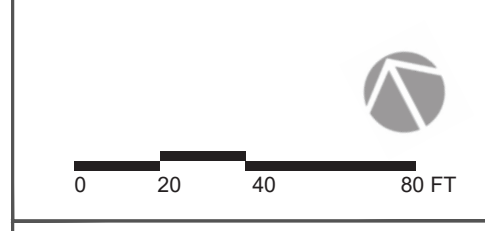
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/19/2021



DATE:  
SCALE:  
AERIAL VIEW OF SITE





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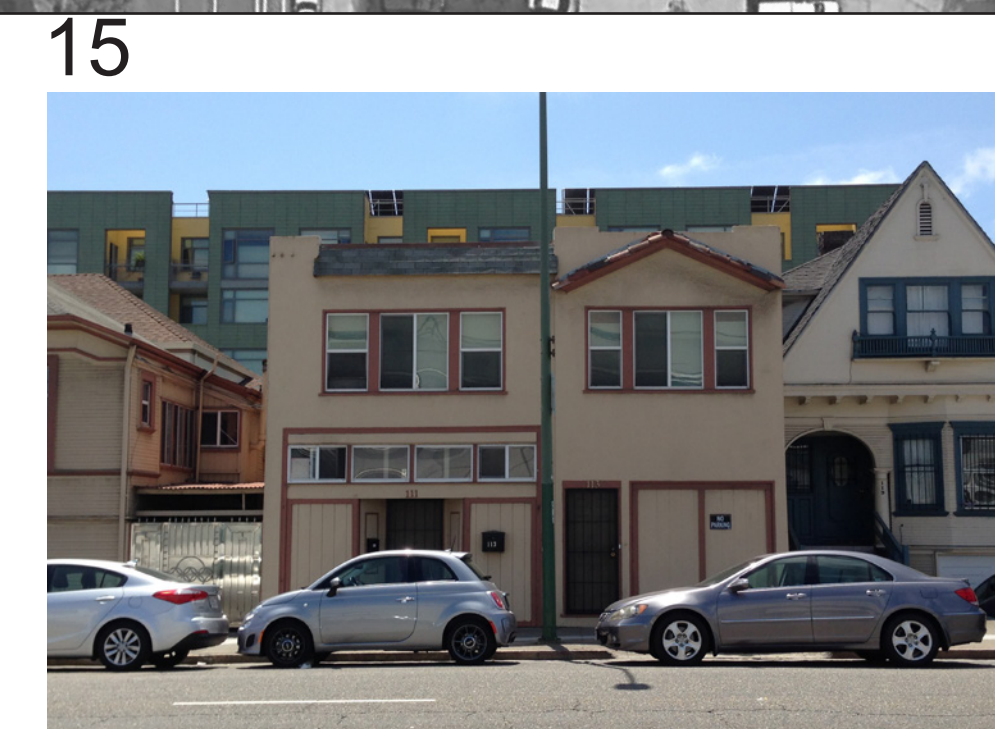
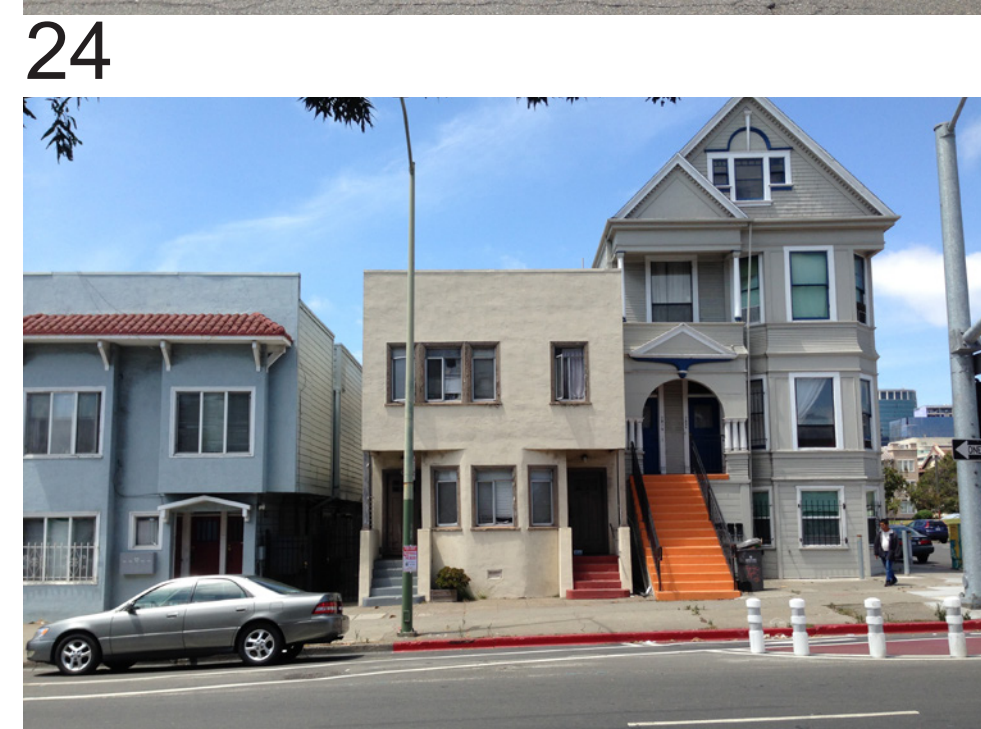
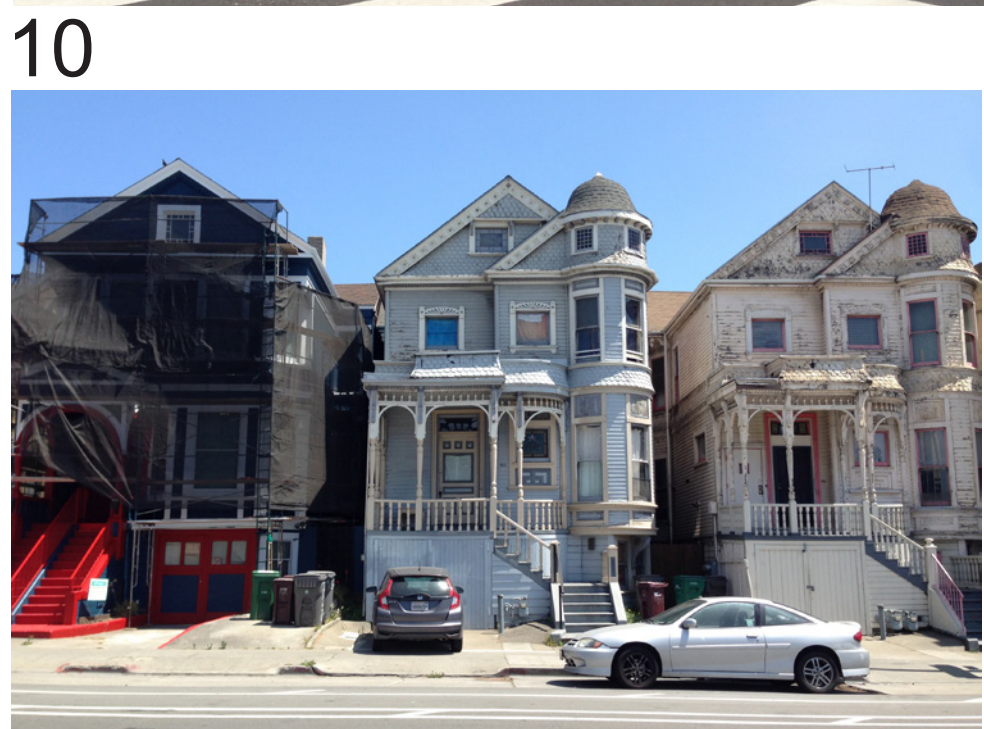
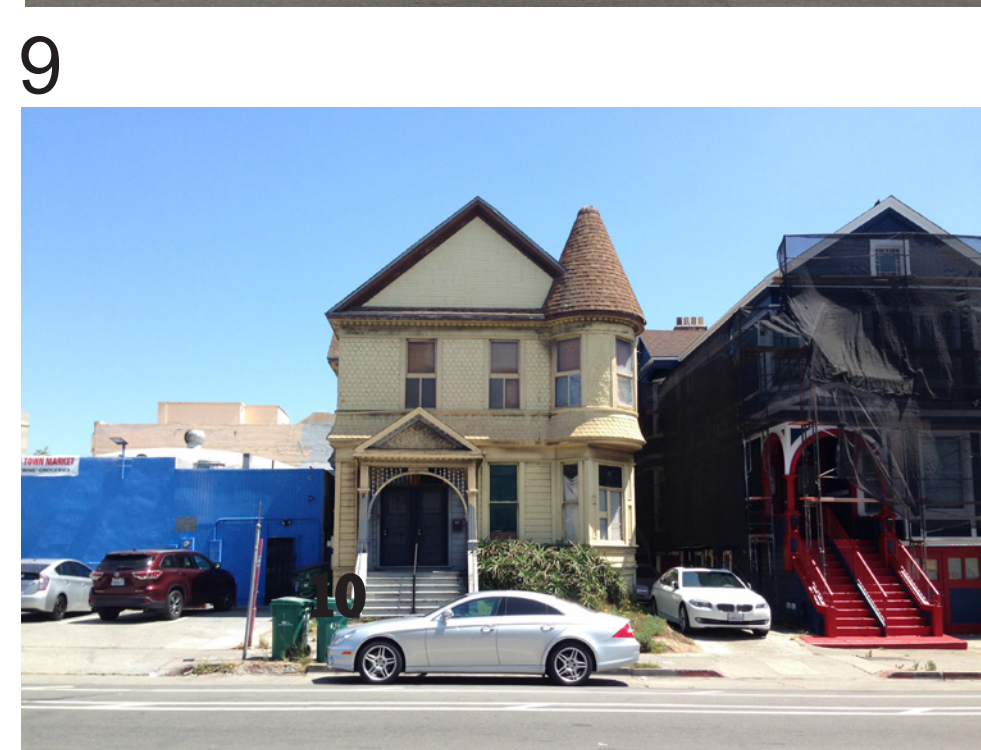
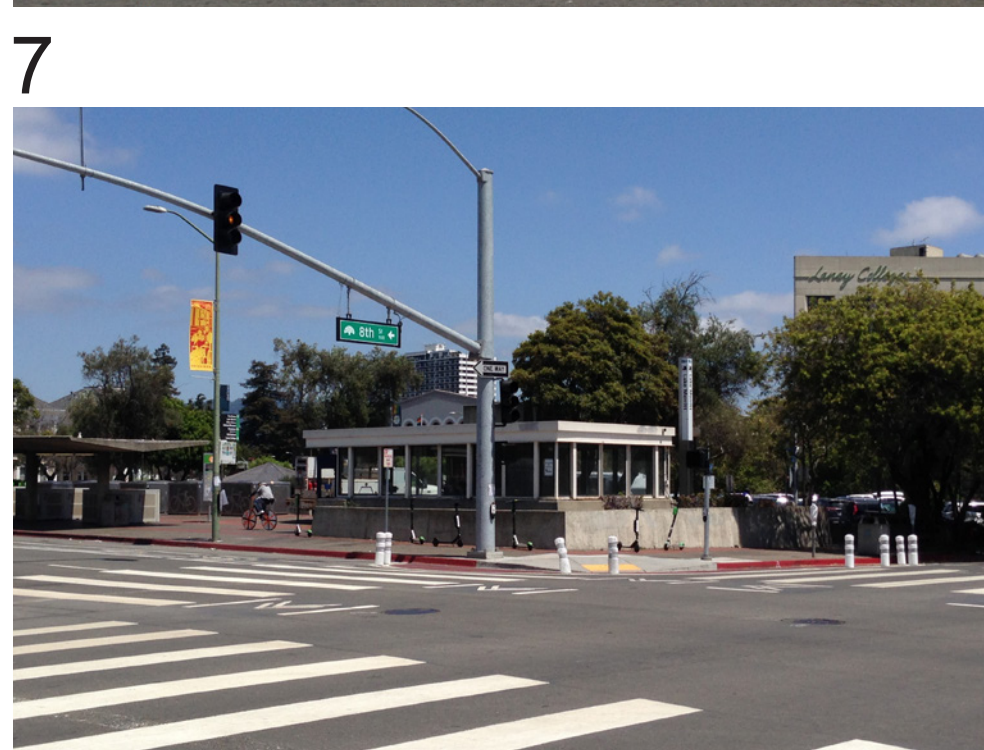
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DATE:  
SCALE:  
SITE PHOTOS

**A0.3**





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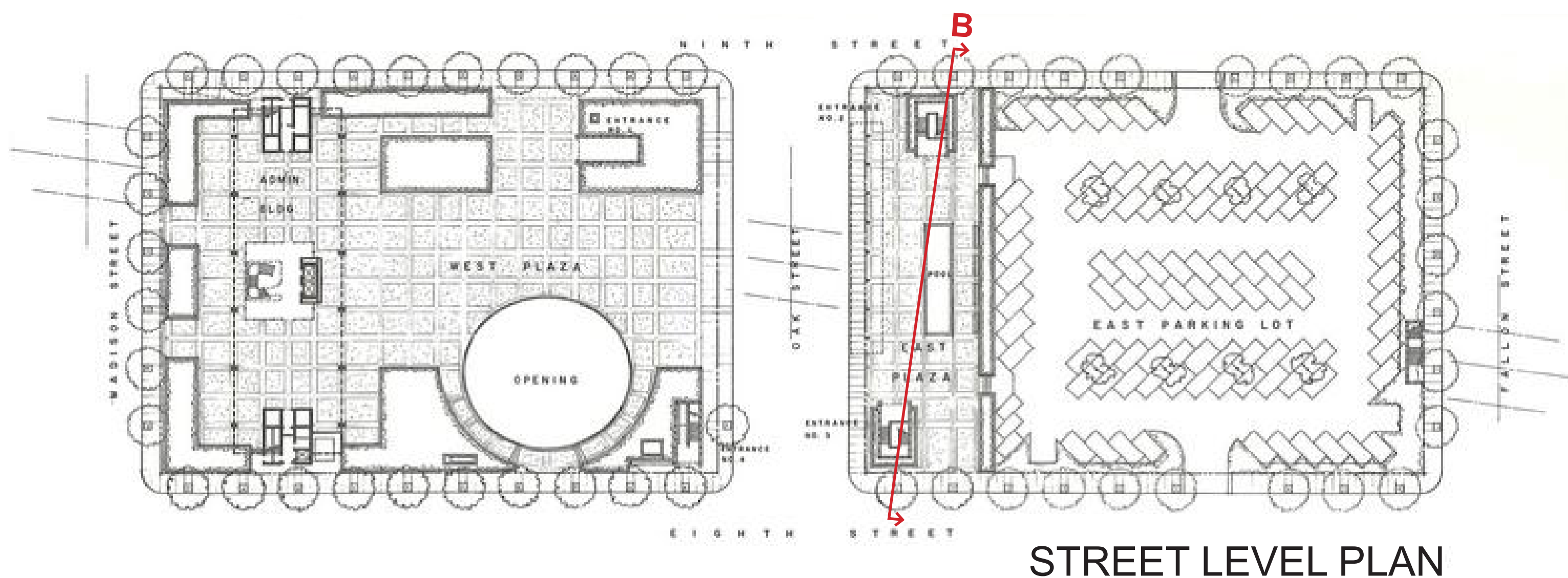
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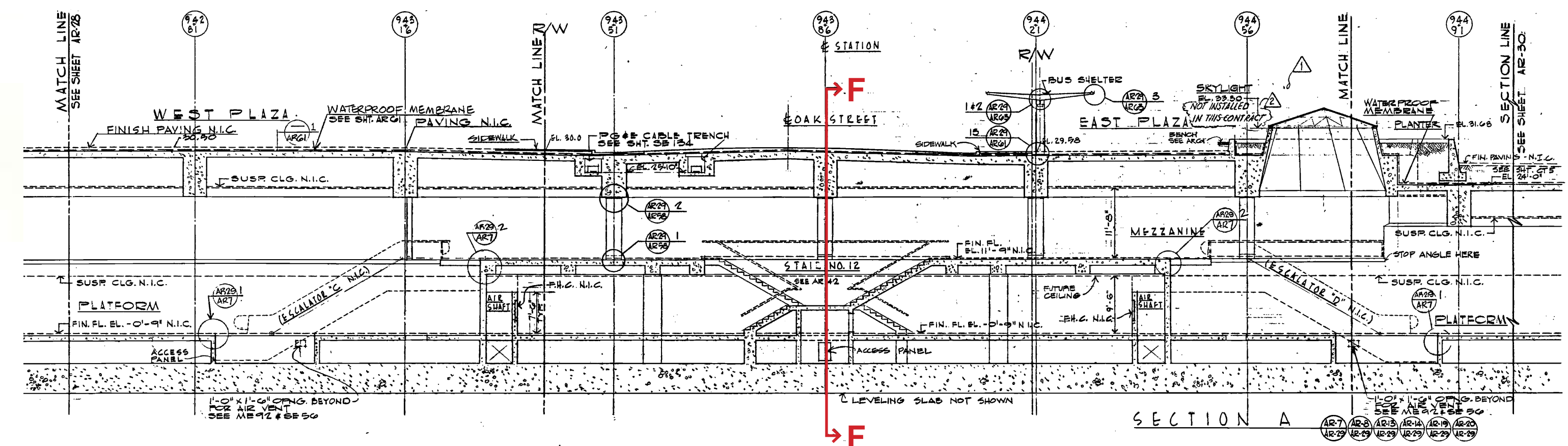
DATE:  
SCALE:  
**SITE PHOTOS**

**A0.4**

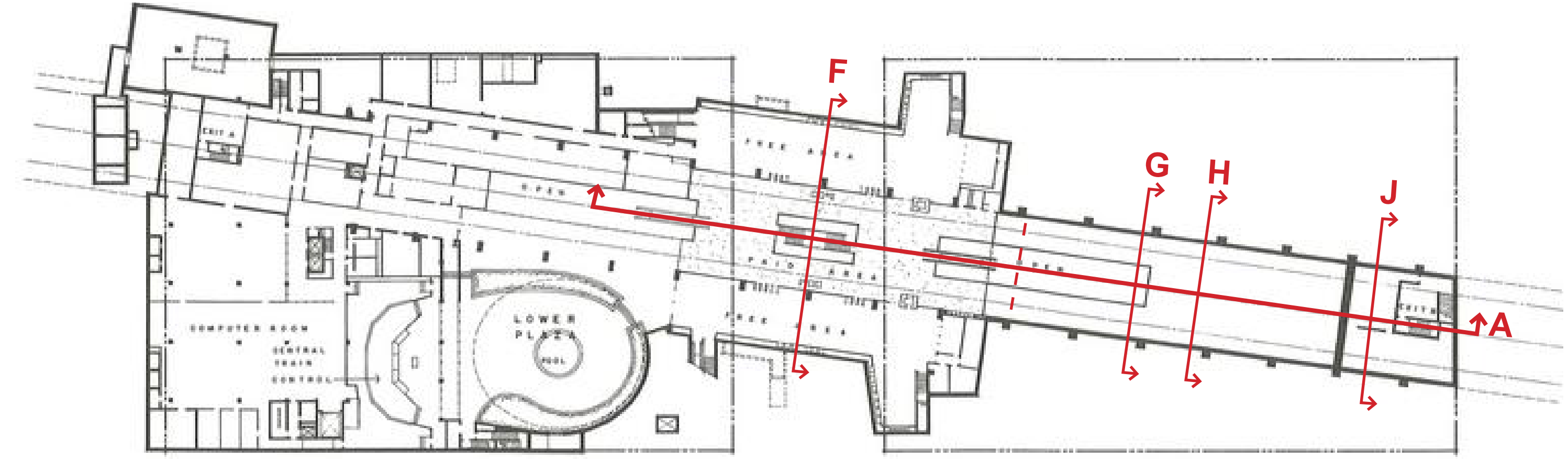




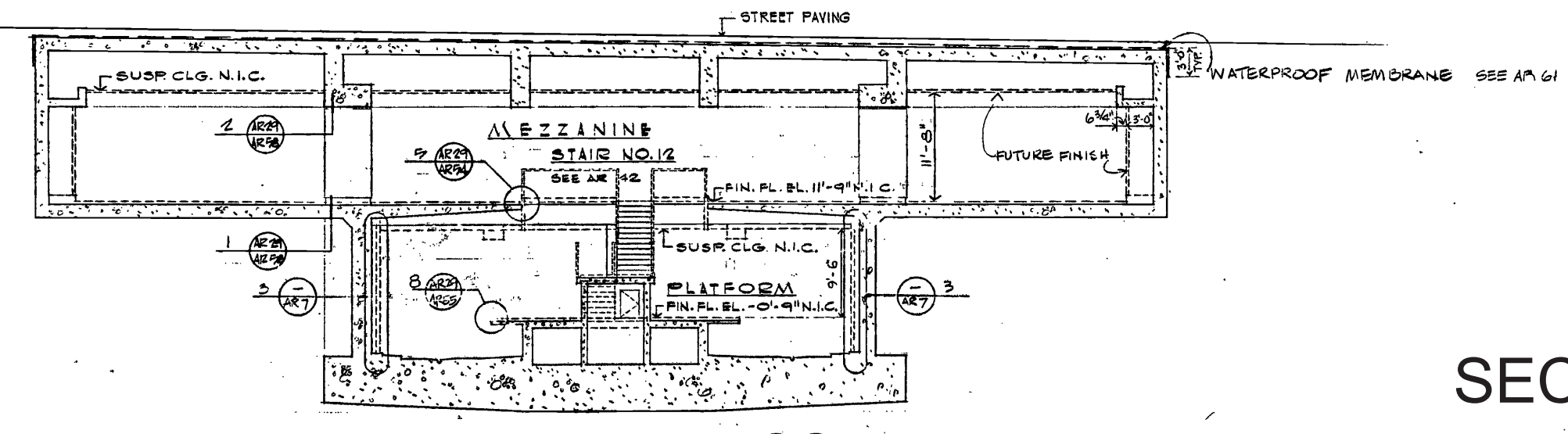
STREET LEVEL PLAN



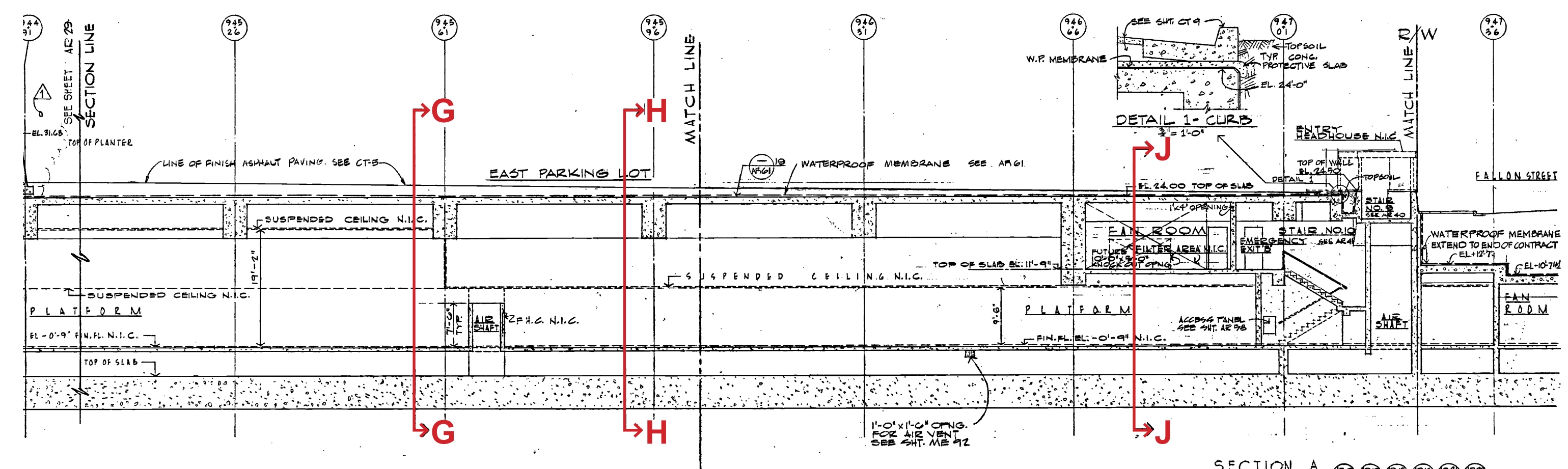
SECTION A



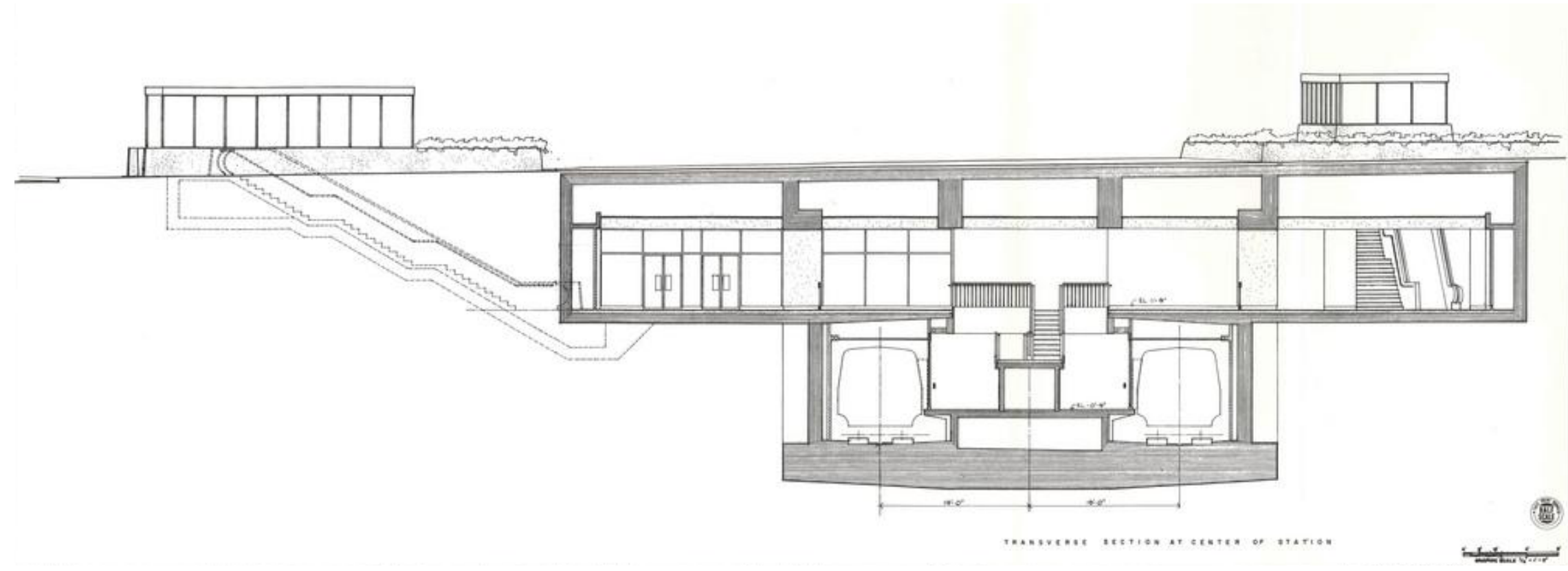
MEZZANINE LEVEL PLAN



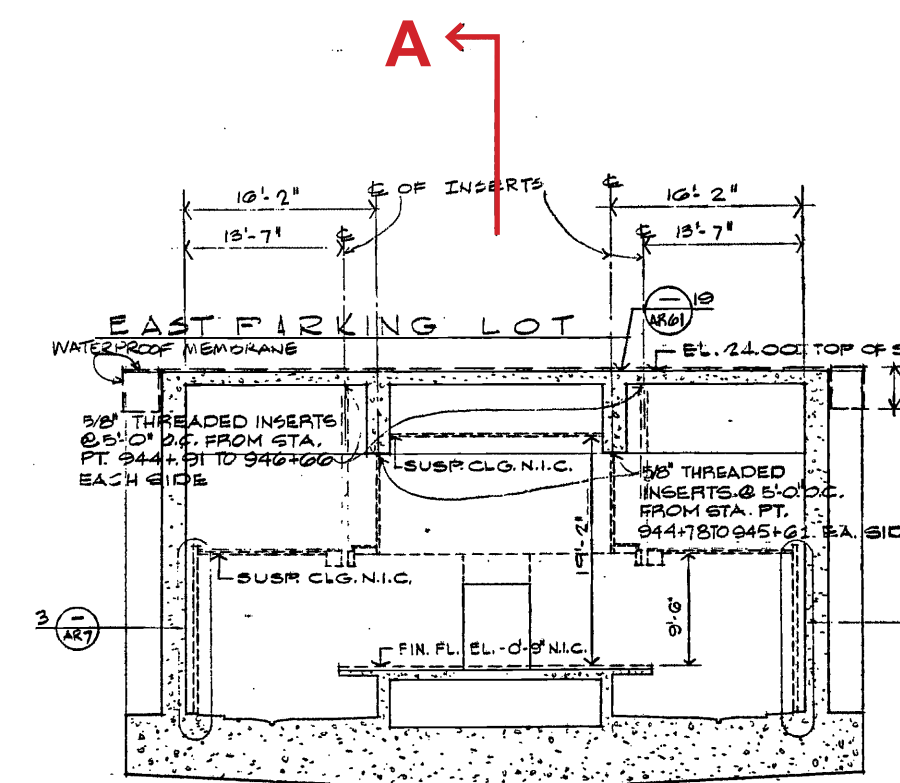
SECTION F



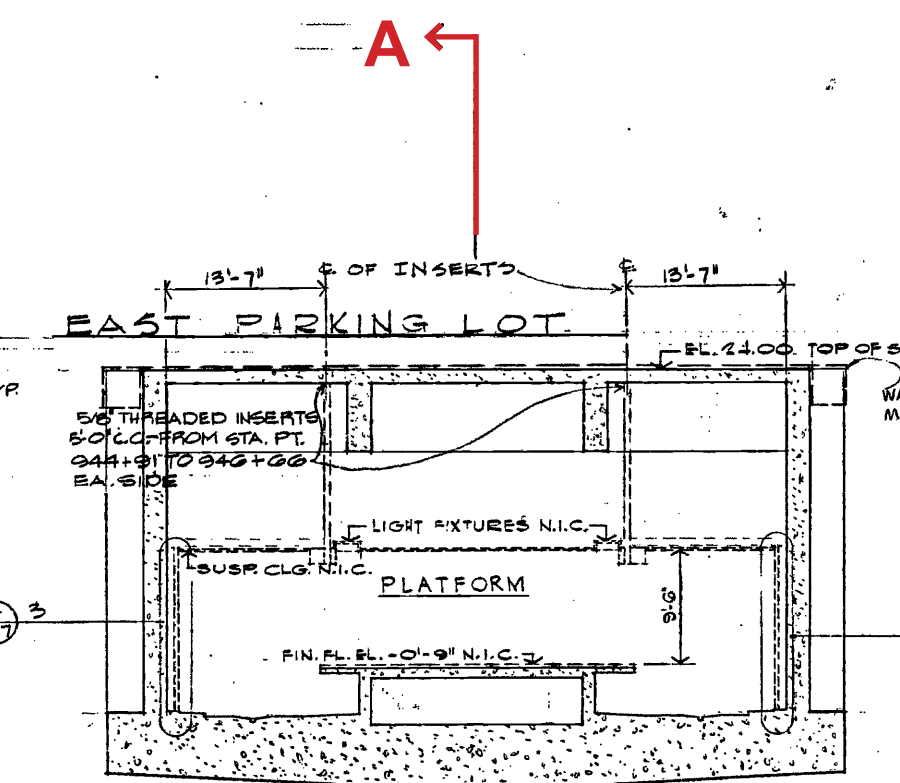
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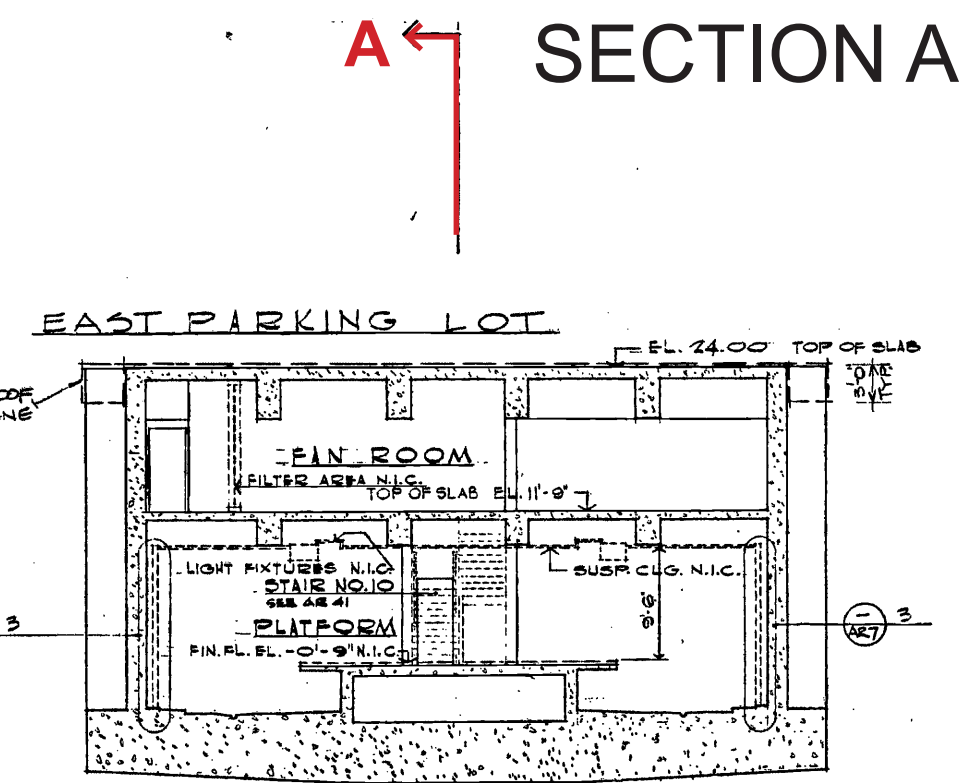
SECTION B



SECTION G



SECTION H



SECTION J

NOTE: As-Built drawings to be replaced with updated survey information when available.

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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

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7	REVISED RESPONSE TO POP'S COMMENTS	03/19/2021

DATE:  
SCALE:  
**BART STATION PLAN AS-BUILTS**

**A0.5**

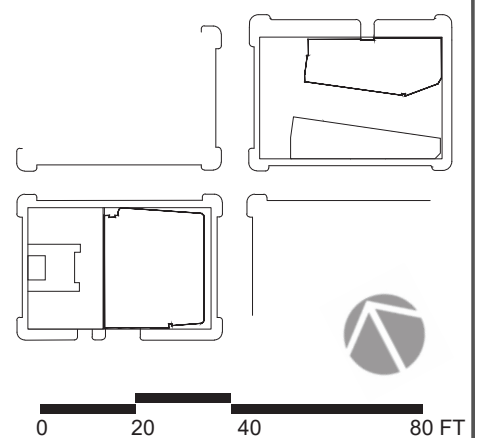


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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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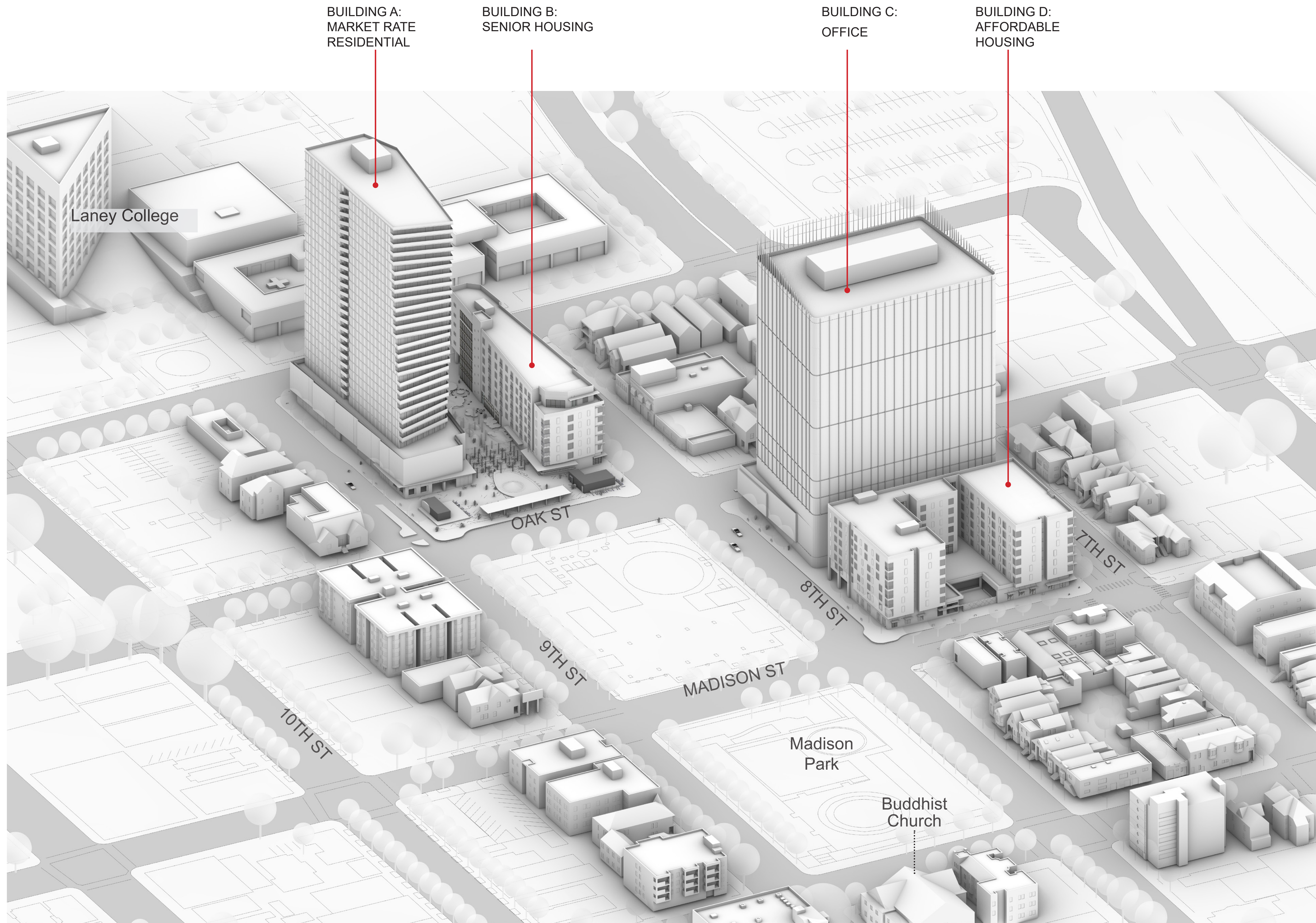
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DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")

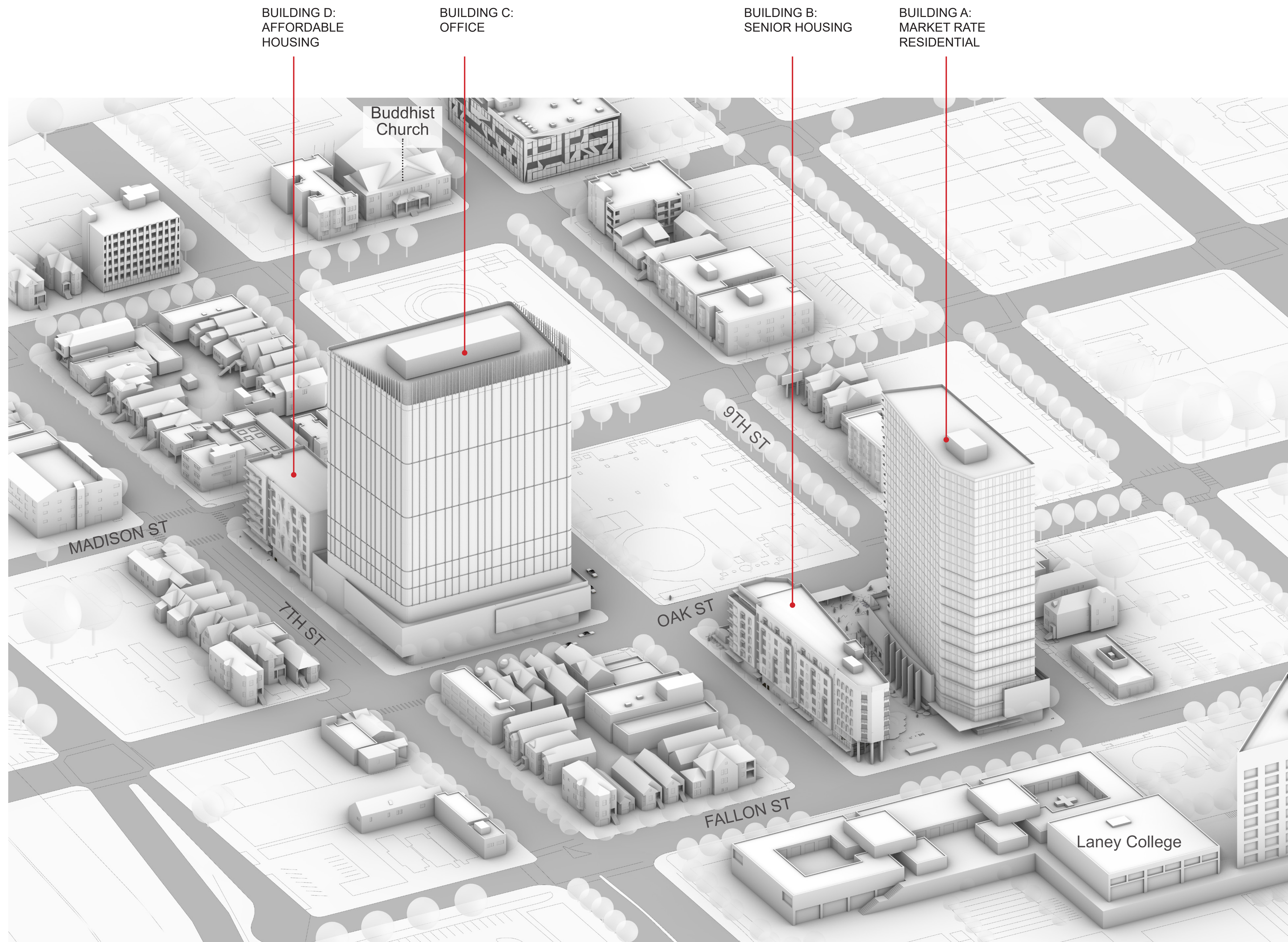
AXONOMETRIC VIEW OF SITE

**A1.0**



**AXONOMETRIC VIEW LOOKING SOUTH-EAST**





BUILDING D:  
AFFORDABLE  
HOUSING

BUILDING C:  
OFFICE

BUILDING B:  
SENIOR HOUSING

BUILDING A:  
MARKET RATE  
RESIDENTIAL

Buddhist  
Church

Laney College

MADISON ST

7TH ST

OAK ST

9TH ST

FALLON ST

AXONOMETRIC VIEW LOOKING NORTH-WEST

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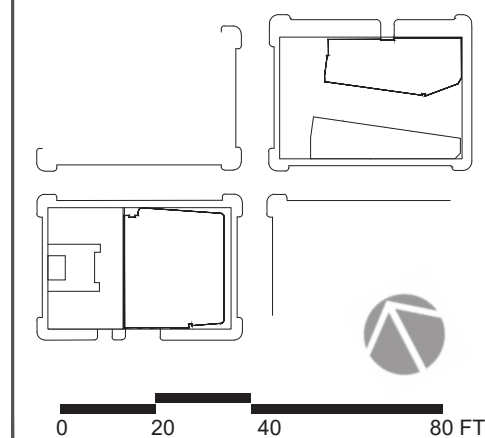
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**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

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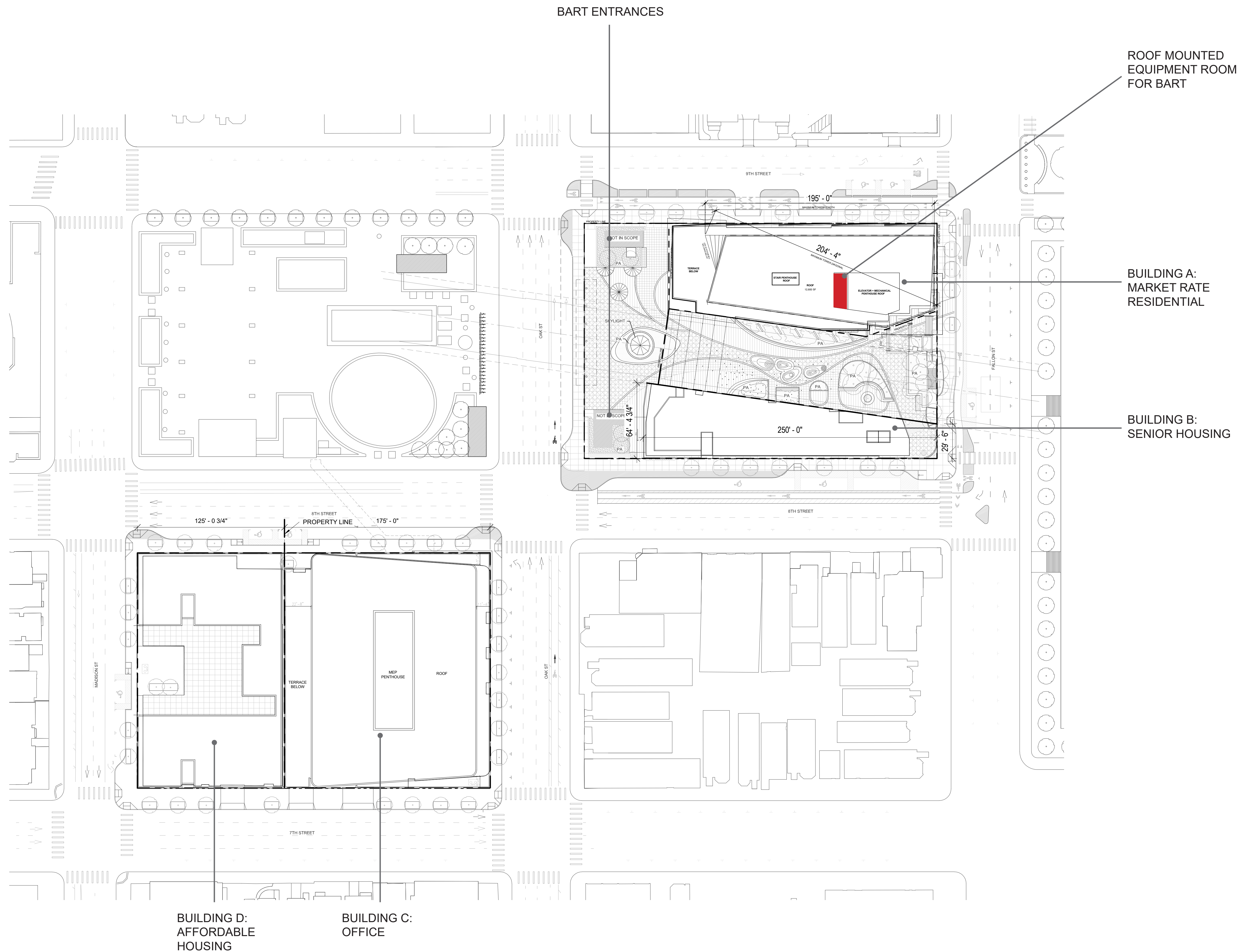
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DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")

AXONOMETRIC  
VIEW OF SITE





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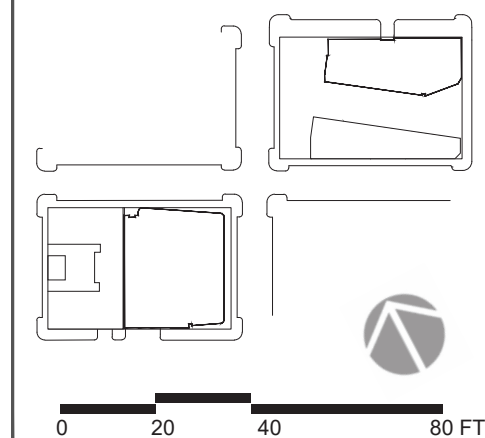
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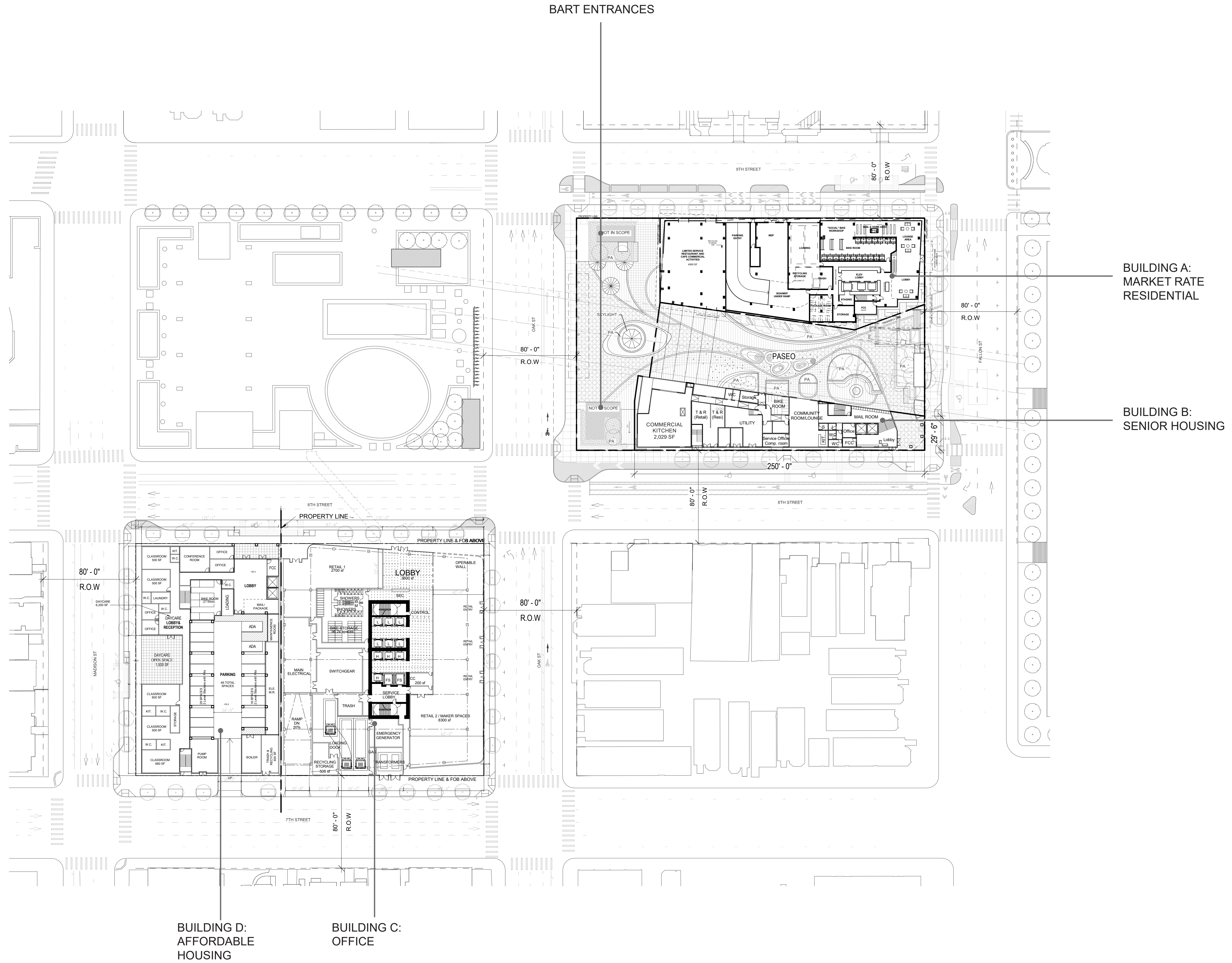
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DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
**OVERALL SITE PLAN BLOCK 1 & 2**





BART ENTRANCES

BUILDING A:  
MARKET RATE  
RESIDENTIAL

BUILDING B:  
SENIOR HOUSING

BUILDING D:  
AFFORDABLE  
HOUSING

BUILDING C:  
OFFICE

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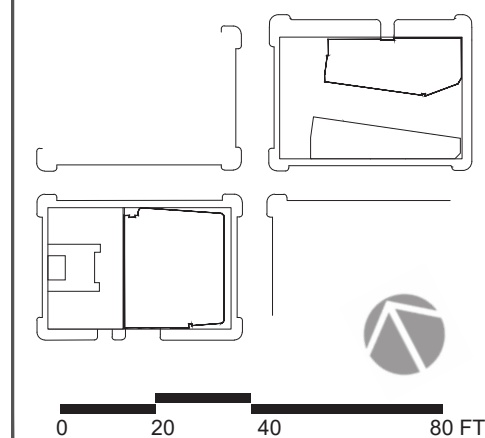
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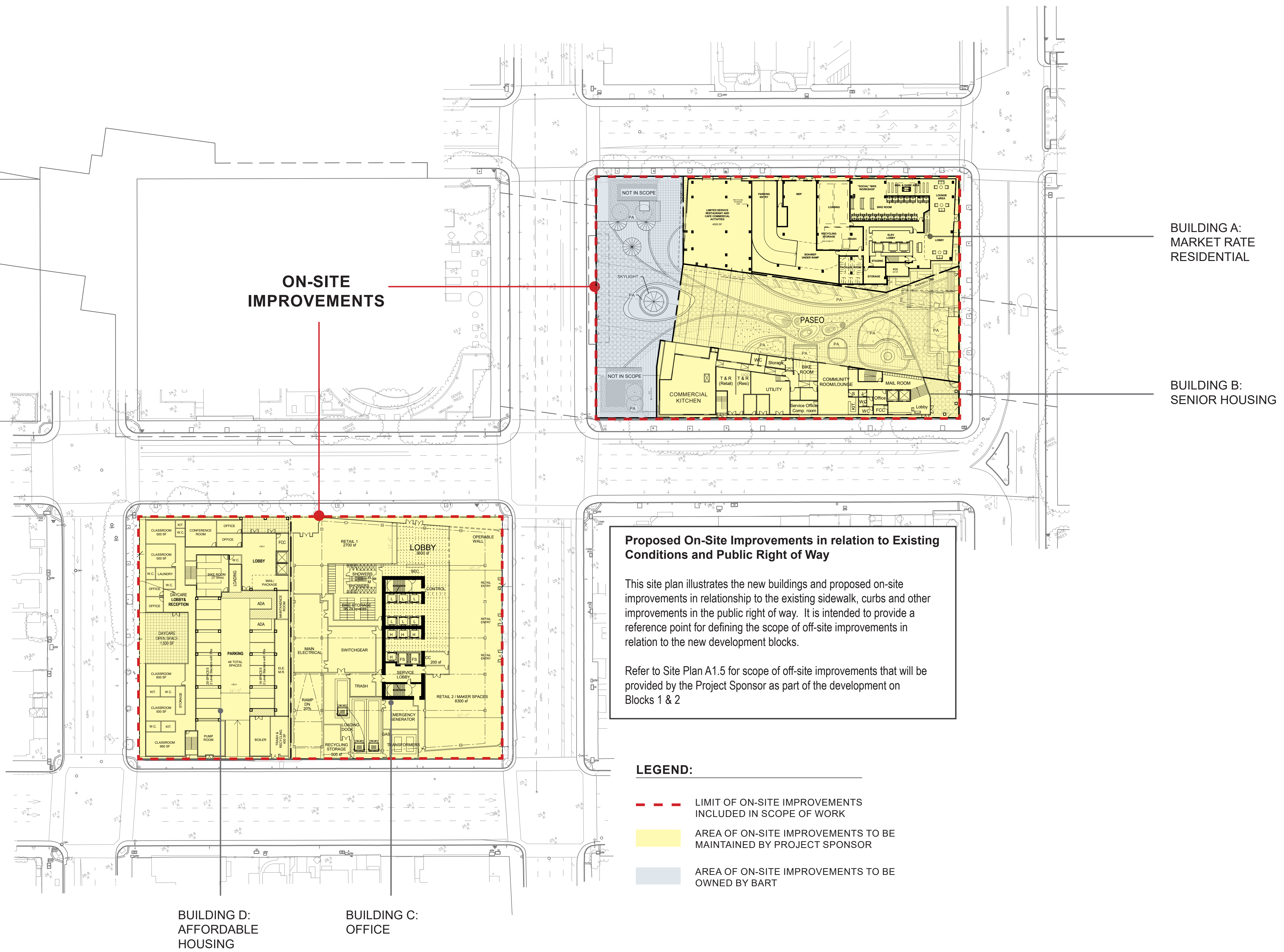
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DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
**OVERALL L1 FLOOR PLAN BLOCK 1 & 2**



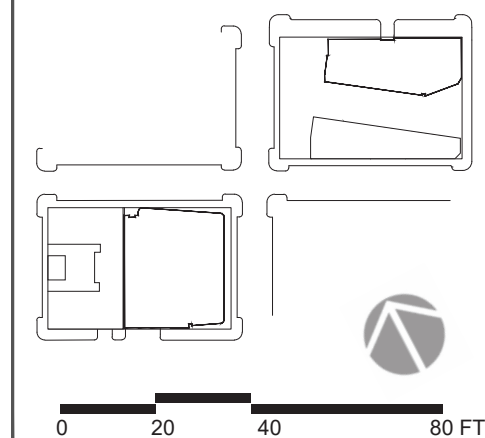


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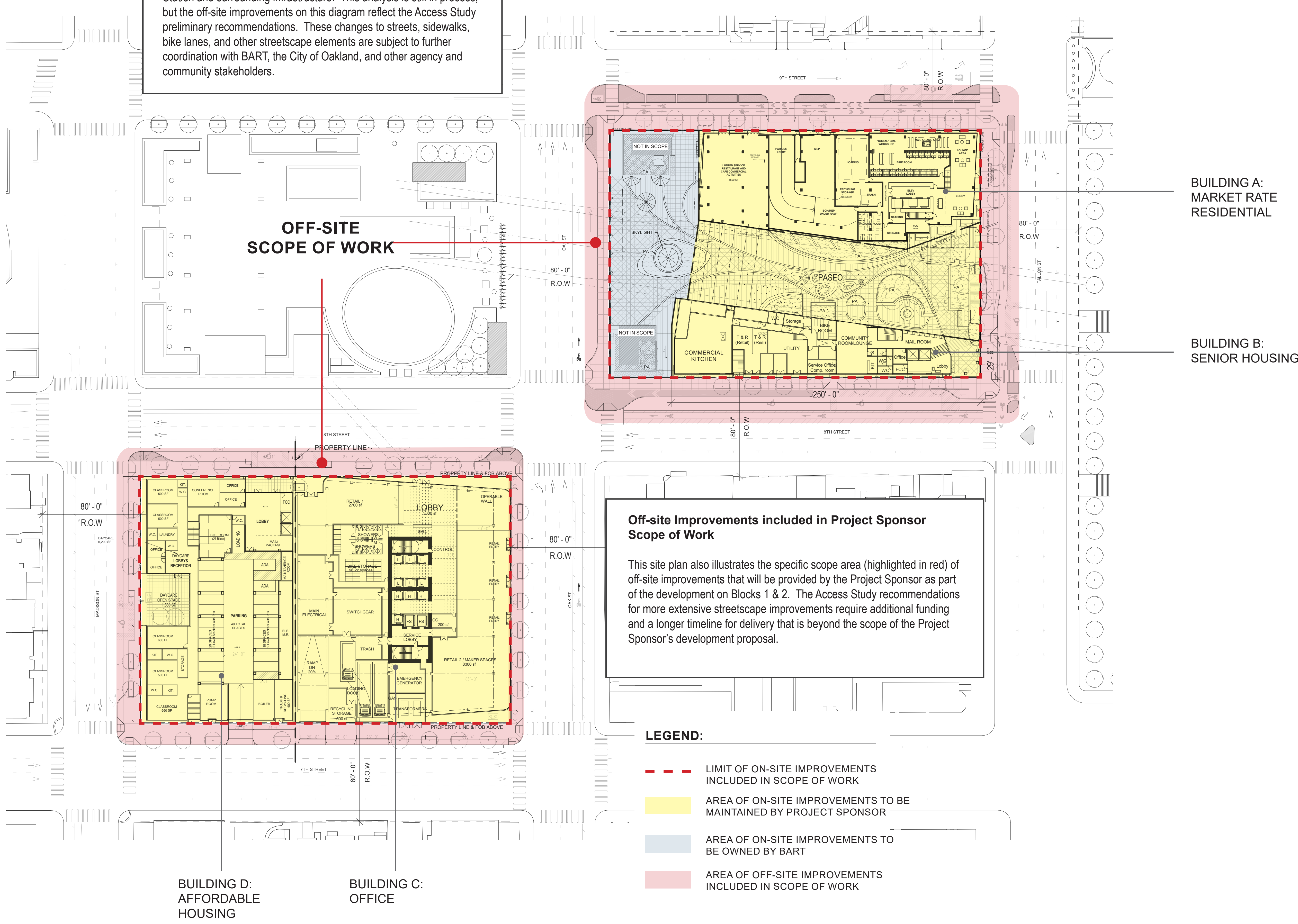
**OVERALL L1 FLOOR PLAN BLOCK 1 & 2 ON-SITE IMPROVEMENT**

**A1.4**



**Preliminary BART Access Study Proposed Off-Site Improvements**

BART is conducting an analysis of access to the Lake Merritt BART Station and surrounding infrastructure. This analysis is still in process, but the off-site improvements on this diagram reflect the Access Study preliminary recommendations. These changes to streets, sidewalks, bike lanes, and other streetscape elements are subject to further coordination with BART, the City of Oakland, and other agency and community stakeholders.



**Off-site Improvements included in Project Sponsor Scope of Work**

This site plan also illustrates the specific scope area (highlighted in red) of off-site improvements that will be provided by the Project Sponsor as part of the development on Blocks 1 & 2. The Access Study recommendations for more extensive streetscape improvements require additional funding and a longer timeline for delivery that is beyond the scope of the Project Sponsor's development proposal.

- LEGEND:**
- LIMIT OF ON-SITE IMPROVEMENTS INCLUDED IN SCOPE OF WORK
  - AREA OF ON-SITE IMPROVEMENTS TO BE MAINTAINED BY PROJECT SPONSOR
  - AREA OF ON-SITE IMPROVEMENTS TO BE OWNED BY BART
  - AREA OF OFF-SITE IMPROVEMENTS INCLUDED IN SCOPE OF WORK

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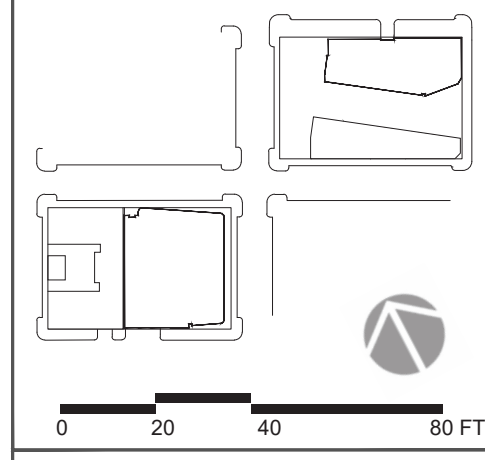
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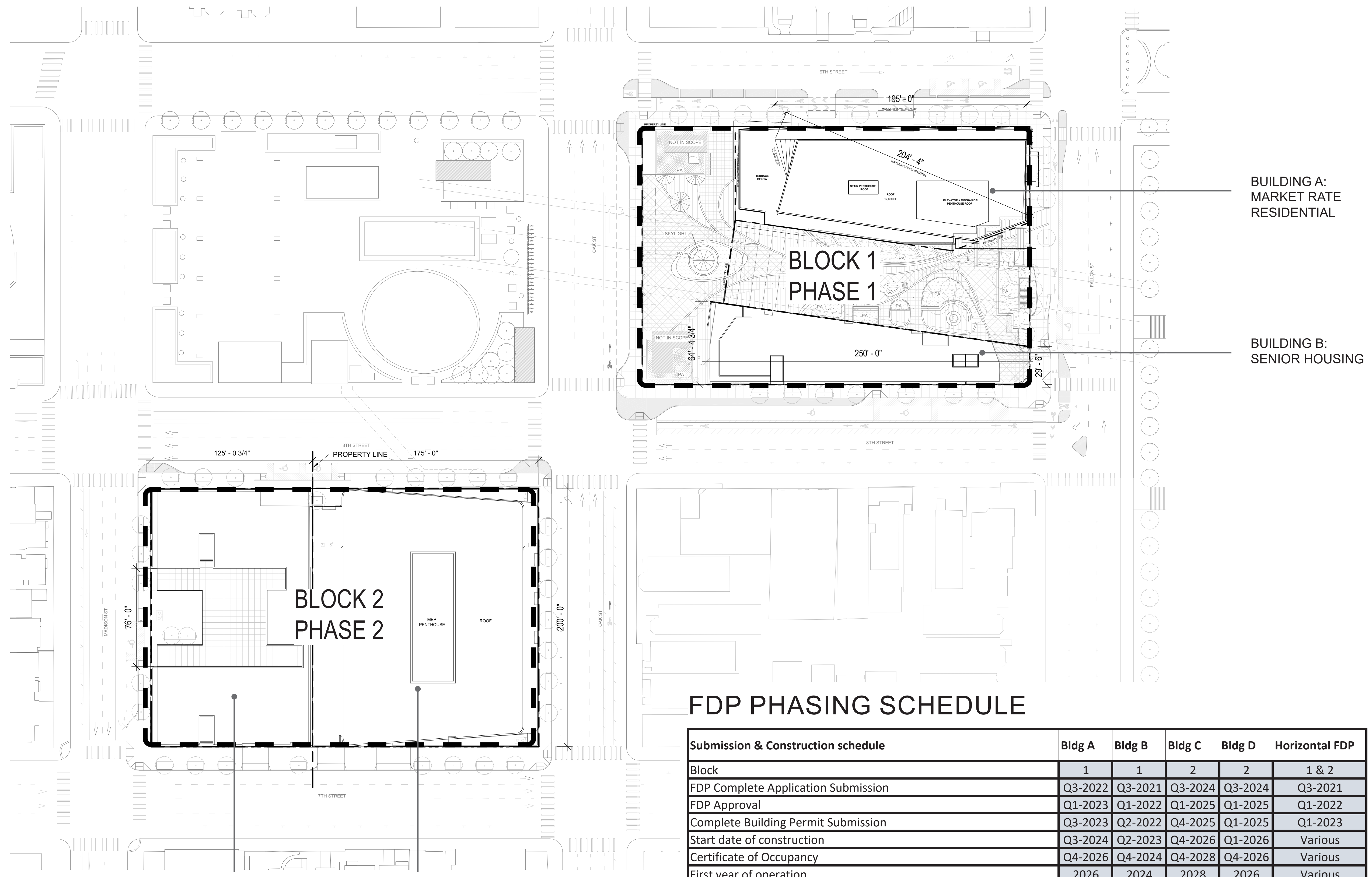
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DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
**OVERALL L1 FLOOR PLAN BLOCK 1 & 2 OFF-SITE SCOPE OF WORK**

**A1.5**





**FDP PHASING SCHEDULE**

Submission & Construction schedule	Bldg A	Bldg B	Bldg C	Bldg D	Horizontal FDP
Block	1	1	2	2	1 & 2
FDP Complete Application Submission	Q3-2022	Q3-2021	Q3-2024	Q3-2024	Q3-2021
FDP Approval	Q1-2023	Q1-2022	Q1-2025	Q1-2025	Q1-2022
Complete Building Permit Submission	Q3-2023	Q2-2022	Q4-2025	Q1-2025	Q1-2023
Start date of construction	Q3-2024	Q2-2023	Q4-2026	Q1-2026	Various
Certificate of Occupancy	Q4-2026	Q4-2024	Q4-2028	Q4-2026	Various
First year of operation	2026	2024	2028	2026	Various

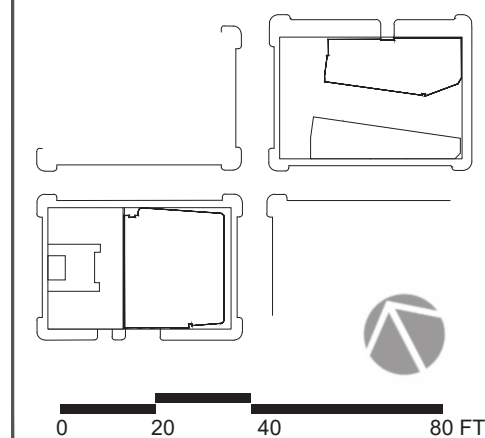
- Allow developers two years to submit their Final Development Plan (FDP) after PUD approval (Section 17.140.040)
- Block 2 construction cannot start until BART Police vacates the premise, and critical BART infrastructure is moved Dec-2025 at the earliest.
- Assumes PUD/PDP Planning Commission May-2021

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPR COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPR COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPR COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPR COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
**PHASING PLAN & FDP PHASING SCHEDULE**

**LAKE MERRITT BART DEVELOPMENT SUMMARY**

updated 03/30/21

Project Site and Shape														
	Block 1				Block 2				TOTAL					
Area	~ 60,000 SF (1.38 Acres)				~ 60,000 SF (1.38 Acres)				2.75 ACRES					
Maximum Length and Width	300 F X 200 F				300 F X 200 F									
Existing Uses to Be Removed														
	Block 1				Block 2				TOTAL					
Office Space	N/A				Office Space: 103,296									
Parking Space	Parking Spaces: 132				Parking Spaces: 82									
Proposed Land Use Program														
	Block 1			Block 2			TOTAL							
	Building A	Building B	Subtotal	Building C	Building D	Subtotal								
Residential (including amenity spaces, circulation and support )	326,055	69,276	395,331		101,703	101,703	497,034							
Residential Parking and associated service areas)	53,500	3,426	56,926		10,850	10,850	67,776							
Office (including circulation and support)	-	-	-	496,933		496,933	496,933							
Day Care	-	-	-		6,200	6,200	6,200							
Commercial Uses:														
Limited-Service Restaurant and Café	4,500	963	5,463				5,463							
Commercial Kitchen		2,029	2,029				2,029							
Retail	-	-	-	11,000		11,000	11,000							
Residential/Commercial Parking & Service	1,100	450	1,550	21,767		21,767	23,317							
<b>Total Building Area</b>	<b>385,155</b>	<b>76,144</b>	<b>461,299</b>	<b>529,700</b>	<b>118,753</b>	<b>648,453</b>	<b>1,109,752</b>							
Proposed Dwelling Units														
	Block 1						Block 2						TOTAL	
	Building A		Building B		Subtotal		Building C		Building D		Subtotal			
	#	%	#	%	#	%	#	%	#	%	#	%	#	%
Studio	108	30%	0	0%	108	24%			18	18%	18	18%	126	23%
1-Bedroom	180	50%	92	95%	272	60%			30	30%	30	30%	302	54%
2-Bedroom	72	20%	5	5%	77	17%			23	23%	23	23%	100	18%
3-Bedroom	0	0%	0	0%	0	0%			29	29%	29	29%	29	5%
<b>Total Dwelling Units</b>	<b>360</b>	<b>100%</b>	<b>97</b>	<b>100%</b>	<b>457</b>	<b>100%</b>			<b>100</b>	<b>100%</b>	<b>100</b>	<b>100%</b>	<b>557</b>	<b>100%</b>
Proposed Residential Density per Block														
	Block 1					Block 2								
*Dwelling per Acre	644					72								
*Note: For density calculation for Block 1, the BART plaza and the Paseo is excluded from the lot area.														
Proposed Parking														
	Block 1			Block 2			TOTAL							
	Building A	Building B	Subtotal	Building C	Building D	Subtotal								
Vehicle Parking Space (Total)	105		105	254	49	303	408							
**Car Share Spaces (Included in total)	2	1	3	(Not Required)	1	1	4							
Bicycle Parking			0			-	0							
Bicycle Parking Long-term	92	12	104	54	27	81	185							
Bicycle Parking Short-term	21	7	28	28	7	35	63							
Total Bicycle parking	113	19	132	82	34	116	248							
**Note: Car share spaces for building B is located in building A garage and for building D is located in building C garage.														
Open Space														
	Block 1			Block 2			TOTAL							
	Building A	Building B	Total Block 1	Building C	Building D	Subtotal								
Publicly Accessible Open Space:														
A. BART Plaza (publicly owned)			11,610											
B. Paseo @ Block 1 (separate parcel)	12,609	3,152	15,761											
C. Publicly Accessible Open Space on Building Parcel	305	***	305	-	-	-								
<b>Total Publicly Accessible Open Space</b>			<b>27,676</b>			<b>-</b>	<b>27,676</b>							
Group Useable Open Space	7,990	1,940	9,930	-	6,800	6,800	16,730							
Private Useable Open Space	12,900		12,900	-	-	-	12,900							
<b>Total Open Space</b>	<b>33,804</b>	<b>5,092</b>	<b>50,506</b>	<b>-</b>	<b>6,800</b>	<b>6,800</b>	<b>57,306</b>							
***Note: Covered open space is excluded from "Publicly Accessible Open Space" calculations.														
Building Characteristics														
	Block 1			Block 2			AVERAGE							
	Building A	Building B	Average	Building C	Building D	Average								
Stories	28	7		19	7									
Height	275'	83'		275'	83'									



**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPKZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021

DATE:  
SCALE:  
**DEVELOPMENT SUMMARY**

**A1.7**

**LAKE MERRITT BART DEVELOPMENT SUMMARY**



**PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING B (SENIOR BUILDING)**

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/29/2021

Property Development Standards					
Code Section for D-LM-2	17.101G.03	REQUIRED	PROPOSED	COMPLIANT?	NOTE
min lot width		25 ft	29.5 ft	YES	placement of imaginary lot line south of 9th, TBD
min lot frontage		25 ft	250.0 ft	YES	
min lot area		4,000 sf	11,695 sf	YES	
min front setback		0 ft	0 ft	YES	
max front and street side setback for the first story		5 ft	2' ft	YES	8th Street is considered as the Principal street.
max front and street side setback for 2/3 story or 35'		5 ft	0 ft	YES	
min interior side setback		0 ft	0 ft	YES	
min corner side setback		0 ft	22 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
min rear setback		0 ft	0 ft	YES	
average min setback from Lake Merritt Estuary Channel		60 ft		YES	
Ground floor commercial façade transparency		65 %	65 %	YES	minimum requirement to be detailed at FDP
min height of ground floor non-residential facilities		15 ft	15 ft	YES	
min width of storefront		15 ft	15 ft	YES	
Use					
Code Section for D-LM-2 & LM-275		REQUIRED	PROPOSED	COMPLIANT?	NOTE
Permanent Residential Activity	17.101G.03			YES	
Limited-Service Restaurant and Café (Commercial Activities)	17.101G.03			YES	
Commercial Kitchen (Custom Manufacturing)	17.142.100.B.4			YES	Project will employ the PUD Bonus for Additional Permitted Activities, Commercial Kitchen (Custom Manufacturing) (17.142.100.B)
Height, Density, Bulk & Tower Regulations					
Code Section for LM-275	17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE
building base max height		55 (85' W/ CUP) ft	85 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max height		275 ft	85 ft	YES	
max residential density		110 SF/unit	120 SF/unit	YES	11,695 SF Lot Area/97 Unit
max non residential intensity (FAR)		12	0.29	YES	3,442 SF Retail area / 11,695 SF Lot Area
Usable Open Space Standards					
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
total number of units	97				
open space requirements (SF)	38 SF/DU	97X38= 3686 SF	5,092 SF	YES	The total open space consists of 20% of paseo plus L7 shared balcony, and the community rooms
Required Dimensions of Usable Open Space					
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Private		10 ft on G flr	N/A		
Public Ground-Floor Plaza		10 ft	65 ft	YES	the whole width of the paseo
Rooftop		15 ft	15 ft	YES	
Courtyard		15 ft	N/A ft		
Off-site open space		10 ft	N/A ft		
Community room		250 SF	1,690 SF	YES	
Off Street Parking					
Code Section	17.116.060-080	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential (D-LM zones)	No minimum parking requirement	-	-	YES	
Food service	0 for less than 10,000 SF	-	-	YES	Food service area = 3,442 SF
Code Section	17.116.110	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Car Share Space	One space for 5-100 units	1	1	YES	1 Car share space for Bldg. B will be provided in Bldg. A garage
Off Street Loading					
Code Section	17.116.120-140	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential	Total Floor Area 50,000-149,999 SF	1 space	-	NO	*Total floor area = 76,209 SF Variance required for Bldg-B loading. Residential loading proposed on-street.
Retail/ Food Service	0 for less than 10,000 SF	0 space			Food service area = 3,442 SF
Bicycle Parking					
Code Section	17.117	REQUIRED	PROPOSED	COMPLIANT?	NOTE
long term: units with parking space	0.1	space per DU	10	YES	for 97 DU
short term: units with parking space	0.05	space per DU	5		
long term: restaurant/café	1	space per 12,000 SF (MIN. 2)	2		Food service area = 3,442 SF
short term: restaurant/café	1	space per 2000 SF (MIN. 2)	2		
Recycling Space					
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential		cubic ft per DU (Min. 10)	194	Min. 194	ft³ for 97 DU
Commercial		cubic ft per 1000 sf (Min. 10)	10	Min. 10	ft³ Food service area = 3,442 SF

**BUILDING B- SENIOR HOUSING**

**PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING A (RESIDENTIAL)**

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/30/2021

Property Development Standards					
Code Section for D-LM-2	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
min lot width		25 ft	120.0 ft	YES	placement of imaginary lot line south of 9th, TBD
min lot frontage		25 ft	74.25 ft	YES	measured on Fallon Street
min lot area		4,000 sf	19,332 sf	YES	imaginary assigned lot area to Bldgs A + B
min front setback		0 ft	up to 3 ft	YES	varies at Street Frontages, 3' maximum
max front and street side setback for the first story		5 ft	3 ft	YES	
max front and street side setback for 2/3 story or 35'		N/A	N/A ft	YES	
min interior side setback		0 ft	0 ft	YES	
min corner side setback		0 ft	0 ft	YES	
min rear setback		0 ft	0 ft	YES	
average min setback from Lake Merritt Estuary Channel		60 ft	N/A	YES	
Ground floor commercial façade transparency		65 %	65 %	YES	
min height of ground floor non-residential facilities		15 ft	16.5 ft	YES	measured from average grade plane
min width of storefront		15 ft	44.5 ft	YES	measured on 9th Street frontage
Height, Density, Bulk & Tower Regulations					
Code Section for LM-275	table 17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE
building base max height		45 (85 W/ CUP) ft	47 ft	YES	wind screens allowed above 45' per 17.108.030 (f)
max height		275 ft	275 ft	YES	
max residential density		110 SF/unit	53.7 SF/unit	YES	19,332SF lot area / 360 units
max non residential intensity (FAR)		12	0.23	YES	based on food retail area only
min setback of tower from base		50%	65%	YES	395.5ft of 609ft total perimeter set back min 10ft
max average per story lot coverage above base		75%	65%	YES	
max tower elevation length		195 ft	195 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max diagonal length		234 ft	204.33 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
Usable Open Space Standards					
Code Section	table 17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
total number of units	360				
open space requirements (SF)	75 SF/DU	360 x 75 = 27000 SF	33,804 SF	YES	The total open space consists of 80% of paseo plus L5 shared terrace, private balconies, and L5 community room
Required Dimensions of Usable Open Space					
Code Section	table 17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Private		10 ft on G floor	N/A	N/A	
Public Ground-Floor Plaza		10 ft	65 ft	YES	
Rooftop		15 ft	15 ft	YES	
Courtyard		15 ft	N/A ft	N/A	
Off-site open space		10 ft	N/A ft	N/A	
Community room		250 SF	2,000 SF	YES	
Off Street Parking					
Code Section	17.116.060-080	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential (D-LM zones)	No minimum parking requirement	-	105	YES	
Food service	0 for less than 10,000 SF	-	none	YES	
Off Street Loading					
Code Section	17.116.120-140	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential	Total Floor Area 50,000 SF or more	1 spaces	2 spaces	YES	
Retail/ Food Service	0 for less than 10,000 SF	0 spaces	0 spaces	YES	
Bicycle Parking					
Code Section	17.117	REQUIRED	PROPOSED	COMPLIANT?	NOTE
long term: residential unit	1	space per DU	4	90	YES 360 units / 4 = 90
short term: residential unit	1	space per DU	20	18	360 units / 20 = 18
long term: restaurant/café	1	space per 12,K sf (MIN. 2)	2	2	YES 4,500 sf, min 2
short term: restaurant/café	1	space per 2K sf (MIN. 2)	3	3	YES 4,500 sf / 2,000 = 2.5 rounds to 3
Recycling Space					
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential		cubic ft per DU (Min. 10)	720	Min. 720	ft³ stacked vertically in smaller footprint
Commercial		cubic ft per 1000 sf (Min. 10)	9	Min. 10	ft³ YES

**BUILDING A - MARKET RATE RESIDENTIAL**



**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#2 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#2 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#2 COMMENTS	03/19/2021

DATE:  
SCALE:

**ZONING SUMMARY FOR BLOCK 1**

**A1.8**



**PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING D (AFFORDABLE HOUSING)**

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/18/21

Property Development Standards					
Code Section for D-LM-2	Table 17.101G.03	REQUIRED	PROPOSED	COMPLIANT?	NOTE
min lot width		25 ft	125 ft	YES	
min lot frontage		25 ft	200 SF	YES	
min lot area		4,000 SF	25,000 ft	YES	
min front setback		0 ft	0 ft	YES	
max front and street side setback for the first story		5 ft	5 ft	YES	along Madison St.
max front and street side setback for 2/3 story or 35'		5 ft	0 ft	YES	
min interior side setback		0 ft	0 ft	YES	
min corner side setback		0 ft	0 ft	YES	
min rear setback		0 ft	0 ft	YES	
average min setback from Lake Merritt Estuary Channel		60 ft		YES	
Ground floor commercial façade transparency		65%	65%	YES	minimum requirement to be detailed at FDP
min height of ground floor non-residential facilities		15 ft	15 ft	YES	
min width of storefront		15 ft	20 ft	YES	

Use					
Code Section for LM-275	17.10.180	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Day Care (for more than 15 children)	Community Education Civic Activity	less than 25% of total linear frontage length	100%	YES	Daycare Capacity = Up to 40 children *Day care area = 6,200 SF indoor + 1,500 SF outdoor

Height, Density, Bulk & Tower Regulations					
Code Section for LM-275	Table 17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE
building base max height		55 (85' W/ CUP) ft	85 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max height		275 ft	85 ft	YES	
max residential density		110 SF/unit	250 SF/unit	YES	25,000 SF lot area/ 100 of units
max nonresidential intensity (FAR)		12	0.25	YES	*Daycare area = 6,200 SF

Usable Open Space Standards					
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
total number of units		100			
open space requirements (SF)	60 SF/DU	100X60= 6,000 sf	6,800 sf	YES	The total open space consists of entrance porch, courtyard/podium and the community rooms

Required Dimensions of Usable Open Space					
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Private		10 ft on G flr	N/A		
Public Ground-Floor Plaza		10 ft	N/A		
Rooftop		15 ft	N/A	YES	
Courtyard		15 ft	16 ft		ranges from 16' to 76'
Off-site open space		10 ft	N/A		
Community room		250 SF	1,200 SF	YES	

Off Street Parking					
Code Section	17.116.060-080	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential (D-LM zones)	No minimum requirement	-	45	YES	
Code Section	17.116.070	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Day Care	No minimum requirement	-	4	YES	
Code Section	17.116.110	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Car Share Space	One space for 5-100 units	1	1	YES	1 Car share space for Bldg. D will be provided in Bldg. C garage

Off Street Loading					
Code Section	17.116.120-140	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential	Total Floor Area 50,000-149,999 SF	1 space	1	YES	*Total floor area = 110,779 SF
Code Section	17.116.130	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Day Care	Less than 50,000 SF	0 space	-	YES	* Total Day Care area = 7,700 SF (6,200 SF indoor + 1,500 SF outdoor space)

Bicycle Parking					
Code Section	17.117	REQUIRED	PROPOSED	COMPLIANT?	NOTE
long term: units with parking space	0.25	space per DU	25	YES	for 100 DU
short term: units with parking space	0.05	space per DU	5		
long term: Day Care	1	space per 12,000 SF (MIN. = 2)	2	YES	* Total Day Care area = 7,700 SF (6,200 SF indoor + 1,500 SF outdoor space)
short term: Day Care	1	space per 2,000 SF (MIN. = 2)	2	YES	

Recycling Space					
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential	2	cubic ft per DU (Min. 10)	200	YES	for 100 DU
Commercial	2	cubic ft per 1000 sf (Min. 10)	12		for 6,200 sf Daycare

**BUILDING D - AFFORDABLE HOUSING**

**PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING C (OFFICE)**

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/18/21

Property Development Standards					
Code Section for D-LM-2	table 17.101G.03	REQUIRED	PROPOSED	COMPLIANT?	NOTE
min lot width		50 ft	175 ft	YES	
min lot frontage		50 ft	200 SF	YES	
min lot area		4,000 SF	35,000 ft	YES	
min front setback		0 ft	varies ft	YES	varies between 0-13 ft at 8th Street
max front and street side setback for the first story		5 ft	up to 10 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max front and street side setback for 2/3 story or 35'		5 ft	0 ft	YES	
min interior side setback		0 ft	0 ft	YES	
min corner side setback		0 ft	0 ft	YES	
min rear setback		0 ft	0 ft	YES	
average min setback from Lake Merritt Estuary Channel		60 ft	N/A	YES	
Ground floor commercial façade transparency		65%	65%	YES	minimum requirement to be detailed at FDP
min height of ground floor non-residential facilities		15 ft	16-21 ft	YES	site slope causes variable height
min width of storefront		15 ft	20 ft	YES	

Use					
Code Section for D-LM-2	table 17.101G.01	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Administrative Commercial	frontage linear frontage at ground floor	less than 25% of total linear frontage length	25%	YES	

Height, Density, Bulk & Tower Regulations					
Code Section for LM-275	table 17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE
building base max height		45 (85' W/ CUP) ft	45 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max height		275 ft	275 ft	YES	
max nonresidential intensity (FAR)		12	13.19	YES	FAR calculated at time of submittal per 2019 office building FAR method per P. Vollmann letter to SCB March 8 2019
setback from tower base		50% perimeter	53.3%	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max average per story lot coverage above base		75% site area	75.0%	YES	Note that Lot area includes 24" wide seismic separation bwn buildings on Block2
max tower elevation length		150 ft	200.00 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max diagonal length		180 ft	232.00 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G

Usable Open Space Standards					
Code Section	open space not required for Administrative Commercial Activities				

Required Dimensions of Usable Open Space					
Code Section	open space not required for Administrative Commercial Activities				

Off Street Parking					
Code Section	17.116.080	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Commercial Activity	none required	- spaces	254 spaces	YES	

Off Street Loading					
Code Section	17.116.140	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Retail	less than 25,000 sf	0 spaces	0 spaces	YES	11,000 sf Retail
Administrative Commercial	160,000 sf or more	3 spaces	3 spaces	YES	

Bicycle Parking					
Code Section	17.117.110	REQUIRED	PROPOSED	COMPLIANT?	NOTE
long term: Administrative Commercial	1	per 10K floor area	52	YES	
short term: Administrative Commercial	1	per 20K floor area	26		
long term: Retail	1	space per 12K sf (MIN. = 2)	2	YES	11,000 sf Retail
short term: Retail	1	space per 2K sf (MIN. = 2)	2	YES	

Recycling Space					
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Commercial	2	cuft per 1000 sf (Min. 10)	1,071	YES	vertical stacking of recycling material in limited footprint

Logos for STRADA (101 MISSION ST #420 SAN FRANCISCO, CA 94105), PYATOK (1611 TELEGRAPH AVE. SUITE 200 OAKLAND, CA 94612), SCB (SOLUTION CORDWELL BUENZ ARCHITECTS 335 CALIFORNIA ST SAN FRANCISCO, CA 94111), and BKF100+ (225 BROADWAY DR. SUITE 200 REDWOOD CITY, CA 94063).

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#2 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/19/2021

DATE:  
SCALE:  
**ZONING SUMMARY FOR BLOCK 2**

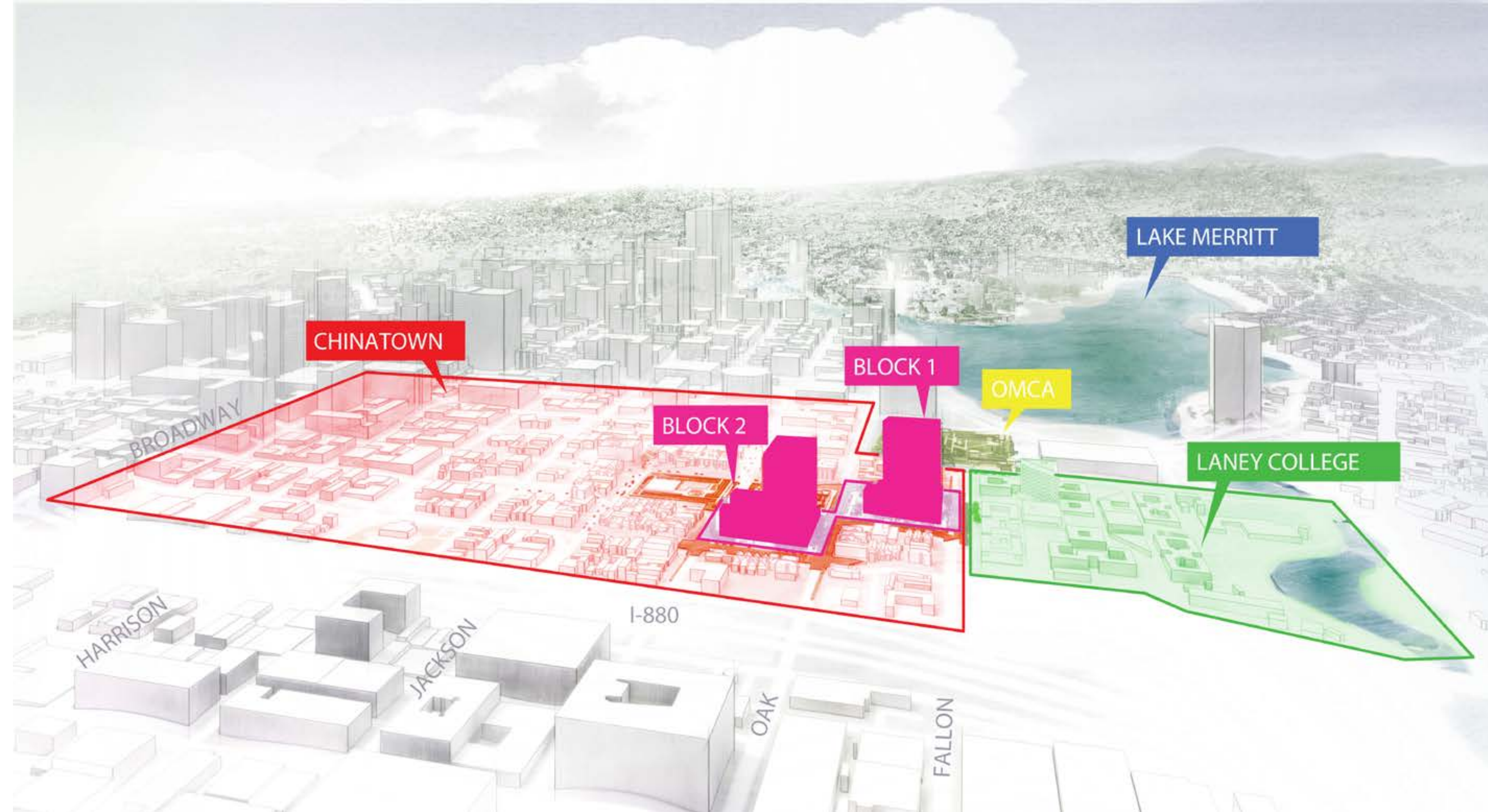
**A1.9**

**BUILDING C - OFFICE**





**View of Paseo Looking East**



### Connecting Transit to Place

Transit oriented development can enhance the arrival of riders and be a tool to connect people to places, cultures, and ecology. The proposed open space design for the Lake Merritt BART Redevelopment connects this important station with the existing communities and institutions surrounding the station.

Connections to the Oakland Museum of Art, Laney College, the Waterfront, and most importantly Chinatown are the basis of the design organization and will be the inspiration for culture, art, and planting selections. The development of an active pedestrian environment on all sides of the new buildings were designed into the basic fabric of the site. The ground floor and open space for the project is integrated into a complementary whole. Community serving uses and activation on the ground floor will provide eyes on the street as well as new community resources in the new development.

Multi-modal connections and the importance of space planning for the many new and evolving forms of transportation are designed into the base condition of the project with maximum flexibility for the anticipated changes of the future.

Careful consideration of existing BART infrastructure was integrated into the overall project. Advance planning for maintenance, access, and the significant responsibilities of BART Police and other staff were highlighted and designed into the new project to increase flexibility and improve security.

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www.pyatok.com

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## LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

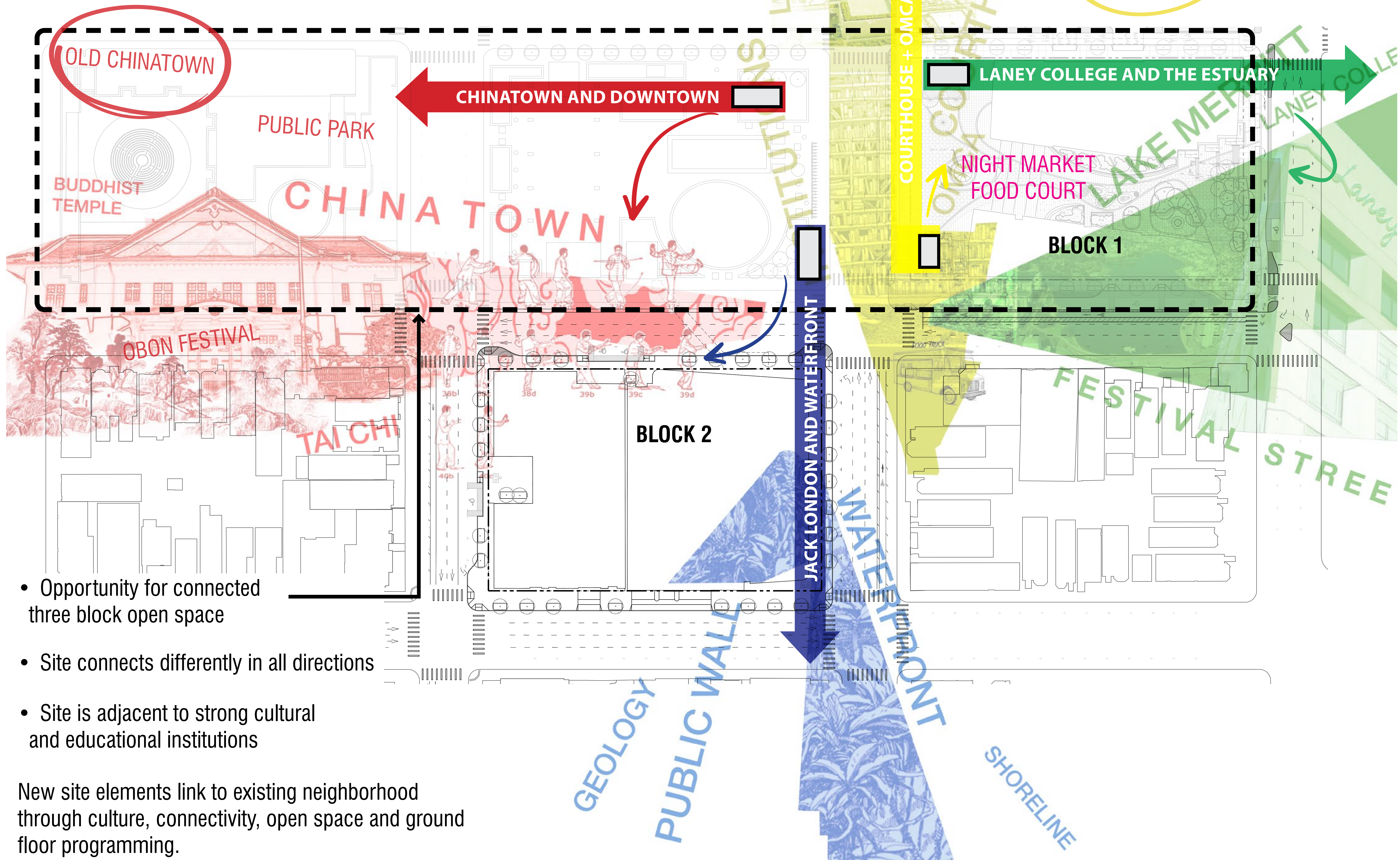
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPPIZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPPIZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPPIZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPIZ COMMENTS	03/17/2021

DATE: AUGUST 8, 2019  
SCALE:

OPEN SPACE CONCEPT

L0.1





- Opportunity for connected three block open space
- Site connects differently in all directions
- Site is adjacent to strong cultural and educational institutions

New site elements link to existing neighborhood through culture, connectivity, open space and ground floor programming.

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OAKLAND, CA 94612

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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

• PRELIMINARY - NOT FOR CONSTRUCTION •

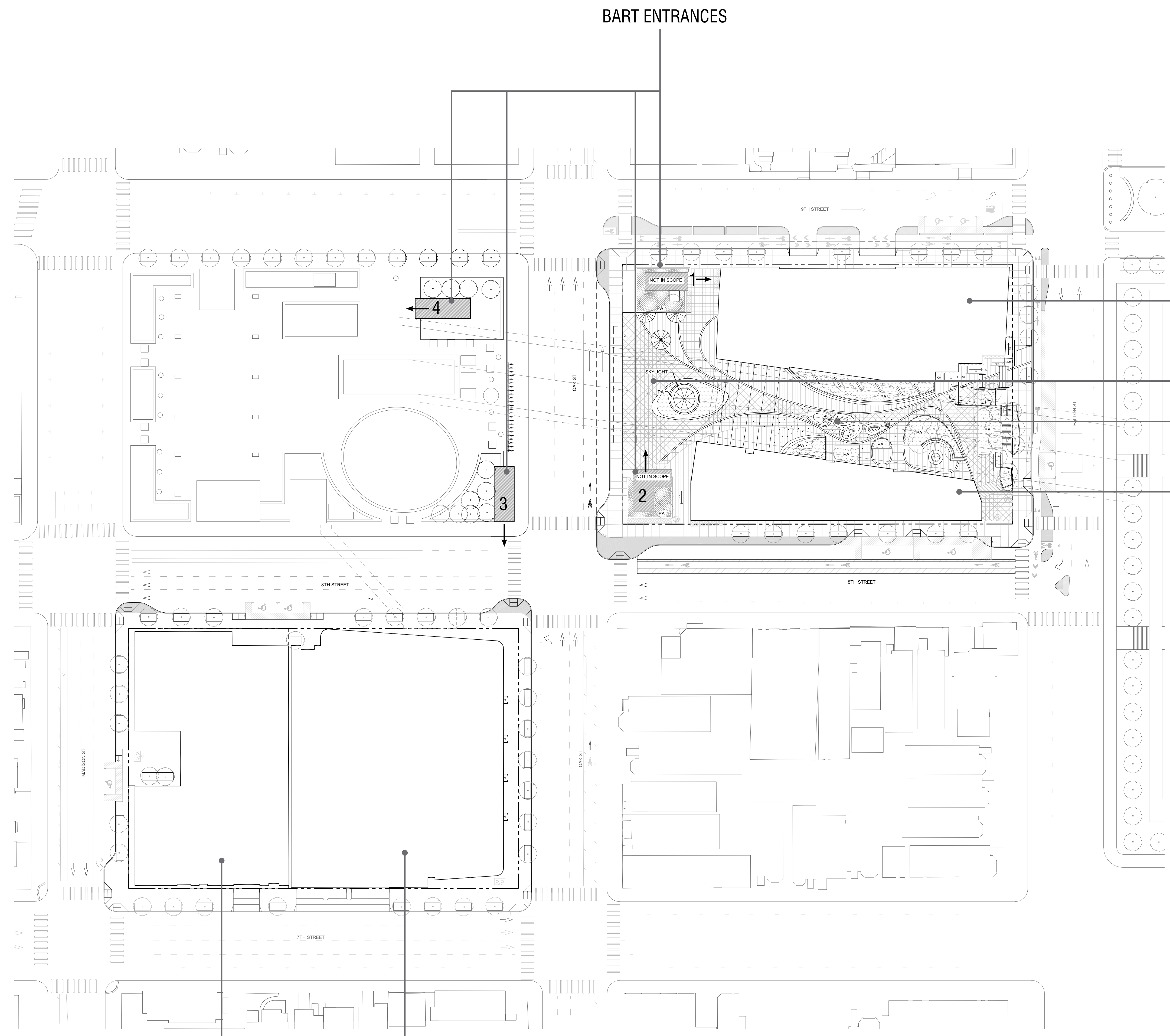
REVISION SCHEDULE	NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019	
2	RESPONSES TO COMMENTS FROM BART	1/20/2020	
3	PDP SUBMITTAL #1 TO CITY	2/12/2020	
4	REVISED RESPONSE TO POPPI COMMENTS	6/8/2020	
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6	REVISED RESPONSE TO POPPI COMMENTS	02/22/2021	
7	REVISED RESPONSE TO POPPI COMMENTS	03/17/2021	

0 20 40 80 FT  
NOT TO SCALE

DATE: AUGUST 8, 2019  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
URBAN DESIGN  
CONTEXT DIAGRAM

**L0.2**





BART ENTRANCES

BUILDING A:  
MARKET RATE  
RESIDENTIAL

WEST PLAZA

PASEO

BUILDING B:  
SENIOR HOUSING

BUILDING D:  
AFFORDABLE  
HOUSING

BUILDING C:  
OFFICE

**NOTE:**  
"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the the project as presened is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

Baseline design and construction in the West Plaza is fully funded by the developer.

Existing bus shelter, headhouses, and skylight to remain.

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1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
101 MISSION ST. #200  
SAN FRANCISCO, CA 94105

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1611 TELEGRAPH AVE. SUITE 200  
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www.pyatok.com

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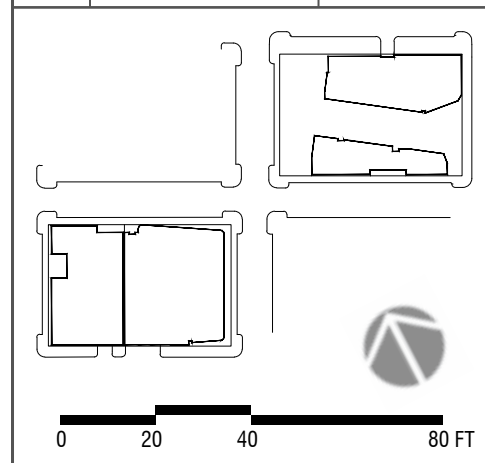
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**LAKE  
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REDEVELOPMENT**  
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**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

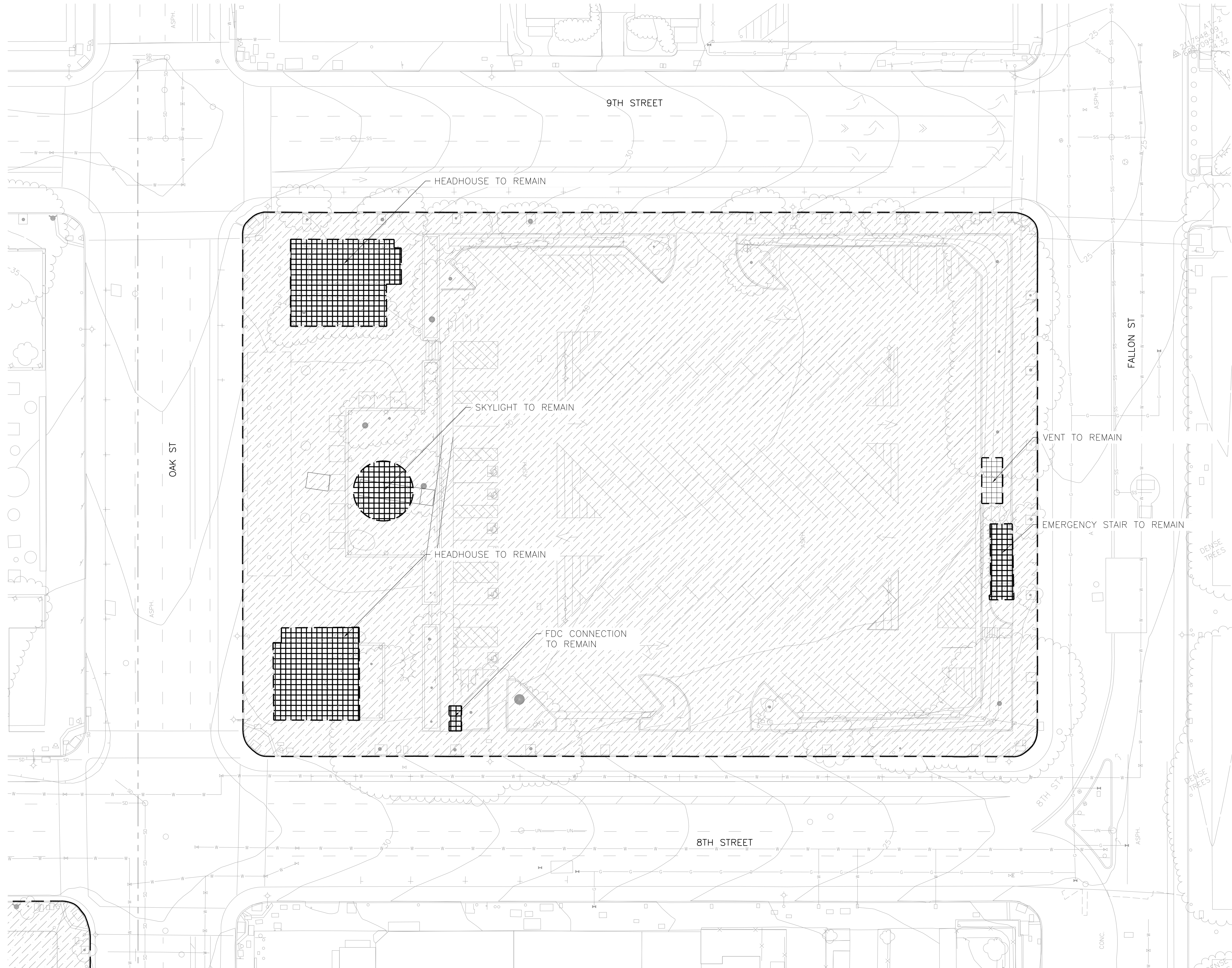
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
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5	REVISED RESPONSE TO POPPI COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPPI COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPI COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
**FOUR-BLOCK AREA  
PLAN**

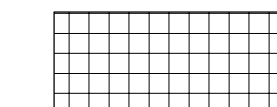
**L0.3**





LEGEND

 TO BE DEMOLISHED

 TO REMAIN

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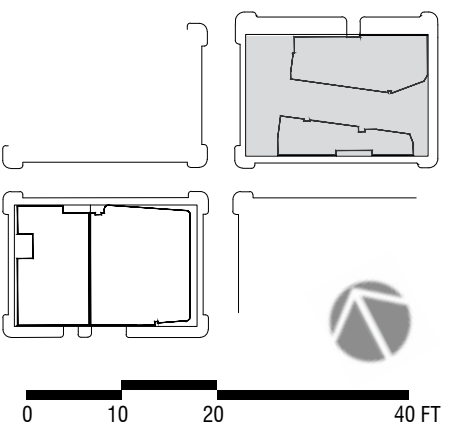
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**LAKE MERRITT BART REDEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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6	REVISED RESPONSE TO PDPA COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA COMMENTS	03/17/2021

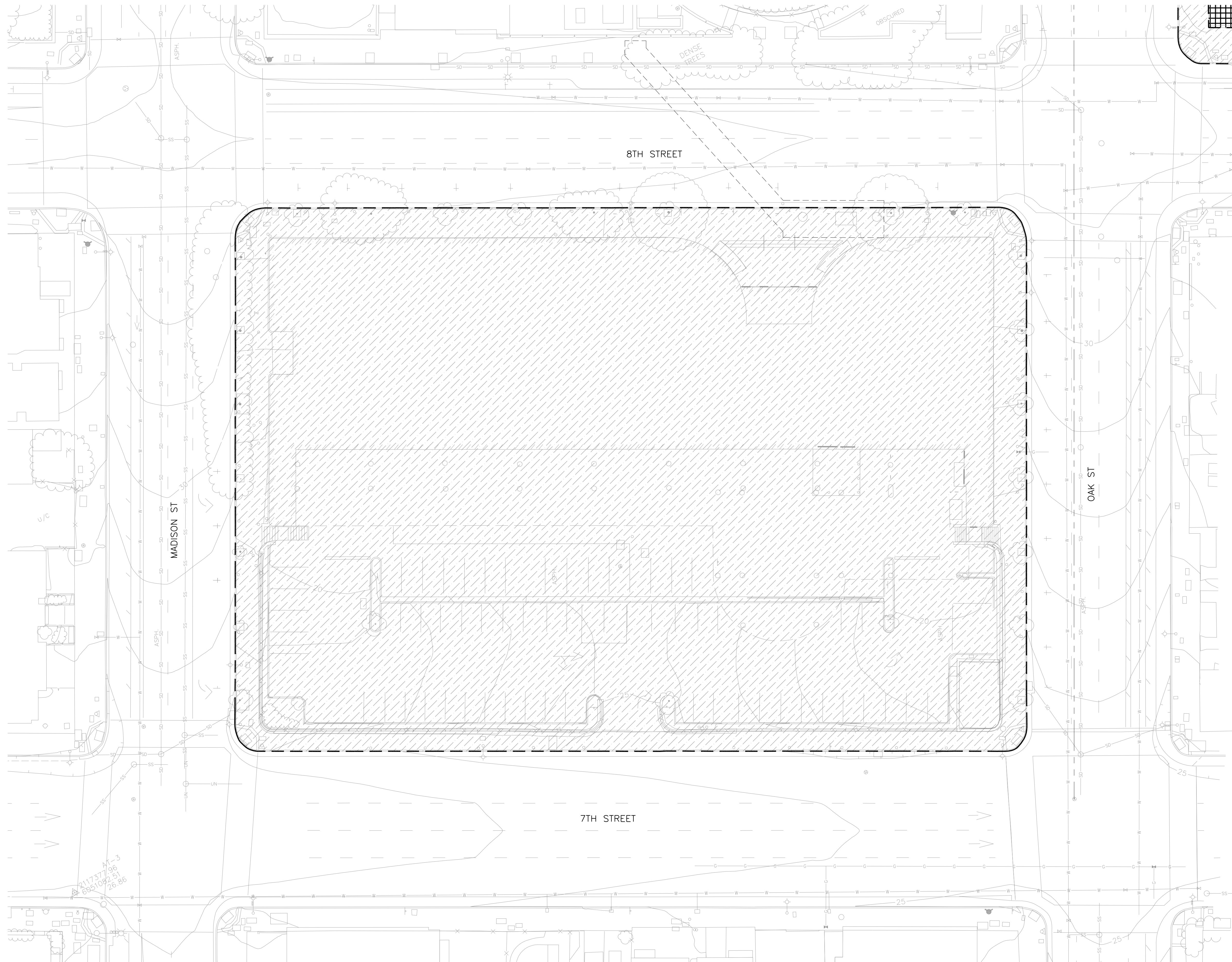


DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 DEMO DIAGRAM**

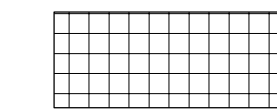
**L0.4**





LEGEND

 TO BE DEMOLISHED

 TO REMAIN

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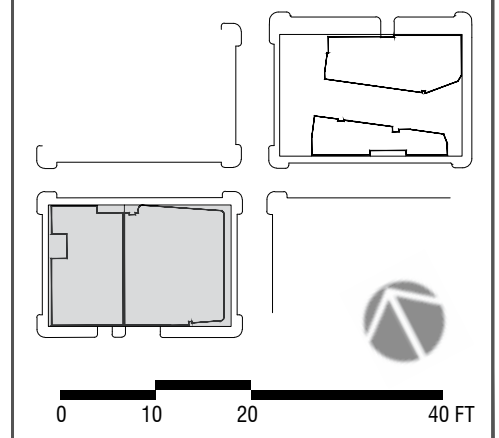
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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5	REVISED RESPONSE TO POPPIV COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPPIV COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPIV COMMENTS	03/17/2021

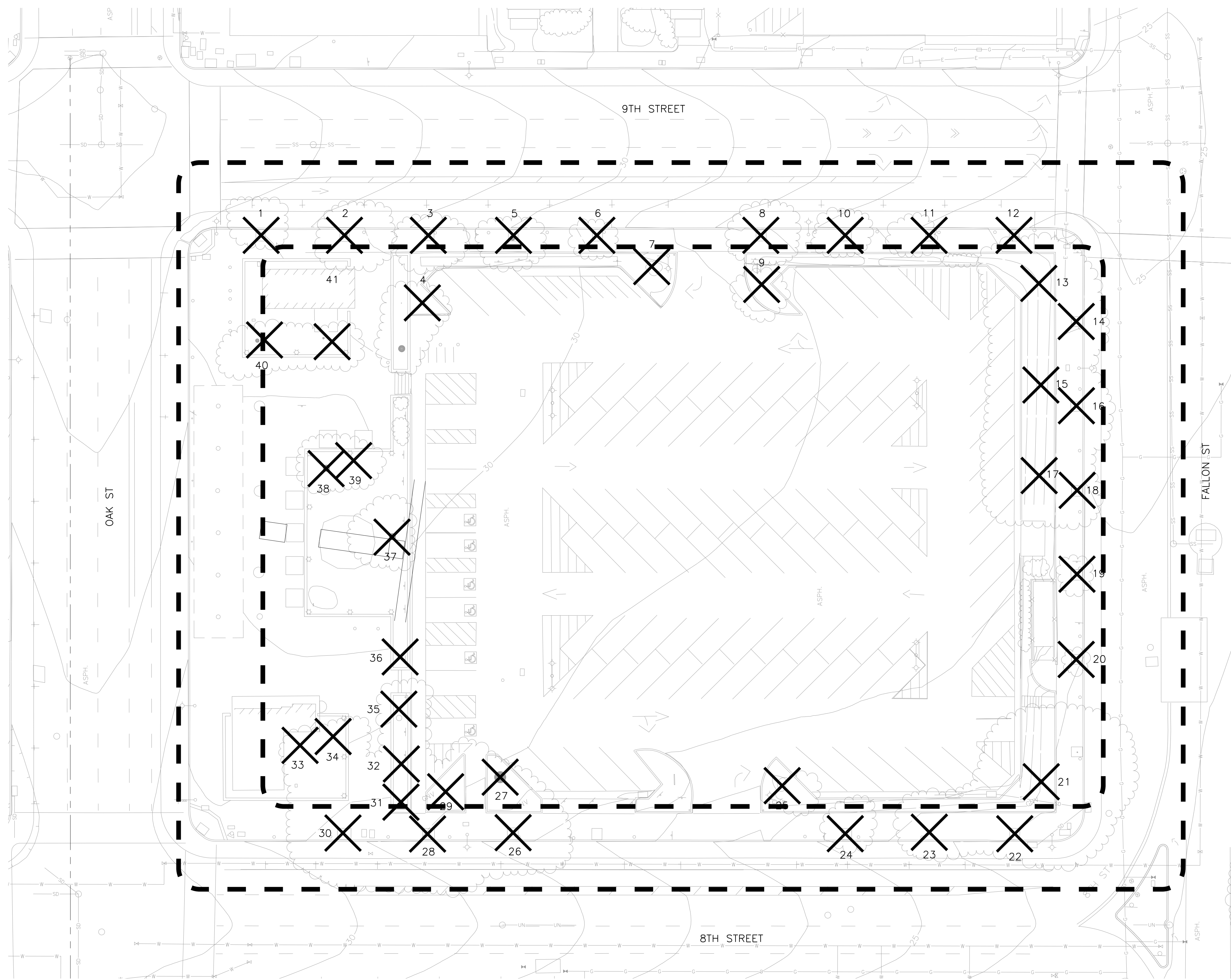


DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

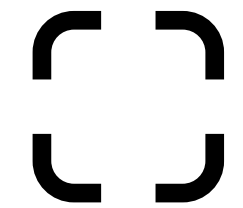
**BLOCK 2 DEMO DIAGRAM**

**L0.5**





Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
1	13"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
2	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
3	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
4	18"	Yes	<i>Olea europaea</i>	Olive Tree
5	24"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
6	6"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
7	10"	Yes	<i>Olea europaea</i>	Olive Tree
8	12"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
9	14"	Yes	<i>Olea europaea</i>	Olive Tree
10	8"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
11	7"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
12	14"	Yes	STUMP	STUMP
13	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
14	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
15	13"	Yes	<i>Platanus racemosa</i>	California Sycamore
16	15"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
17	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
18	5"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
19	14"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
20	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
21	24"	Yes	<i>Platanus racemosa</i>	California Sycamore
22	20"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
23	10"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
24	7"	Yes	<i>Rhus lancea</i>	African Sumac
25	14"	Yes	<i>Olea europaea</i>	Olive Tree
26	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
27	51"	Yes	<i>Olea europaea</i>	Olive Tree
28	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
29	14"	Yes	<i>Olea europaea</i>	Olive Tree
30	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
31	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
32	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
33	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
34	12"	Yes	<i>Olea europaea</i>	Olive Tree
35	15"	Yes	<i>Olea europaea</i>	Olive Tree
36	9"	Yes	<i>Pittosporum undulatum</i>	Victorian box
37	30"	Yes	<i>Olea europaea</i>	Olive Tree
38	30"	Yes	<i>Olea europaea</i>	Olive Tree
39	13"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
40	25"	Yes	<i>Olea europaea</i>	Olive Tree
41	20"	Yes	<i>Pittosporum undulatum</i>	Victorian box

LEGEND  
 30' OFFSET FROM CONSTRUCTION (LOW) FOR TREE ID AND PRESERVATION PURPOSES

**NOTE:**  
 The following are Protected Trees according to City Ordinance Chapter 12.36. **PROTECTED TREES:**  
 A. Any Coast Live Oak tree that is larger than 4 inches DBH. B. Any tree (except Eucalyptus) that is larger than 9 inches DBH. (Eucalyptus trees and up to 5 Monterey Pines per acre are not considered Protected Trees under this section. Monterey Pines must be inspected and verified by the Public Works Agency- Tree Division prior to their removal. C. Any tree of any size located in the public right-of-way (including street trees).

**NOTE ON REMOVAL:**  
 1. We are recommending removing 40 protected trees because of overall poor structure and poor health.  
 2. To meet the needs of a multi modal transit center, new sidewalks designs, bike paths, pick up and drop off, ADA multi-modal drop off will all require the relocation and demolition of sidewalks. This will disrupt the roots and will create larger soil volume areas.  
 3. The master plans aim is to make a cohesive neighborhood identity (coordinating with Madison Sq. Park, Bart plaza (TOF), and Block 1 + 2) and to have a cohesive plant palette.  
 4. See page L0.8 and L0.81 for new tree planting. Intent of design to provide street trees within the scope of the project.



1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612



101 MISSION ST. #200  
SAN FRANCISCO, CA 94105



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OAKLAND, CA 94612  
www.pyatok.com



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OAKLAND, CA 94607  
(510) 891-1686  
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REDDING, CA 96001  
(909) 483-6300  
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## LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP#4 COMMENTS	03/17/2021

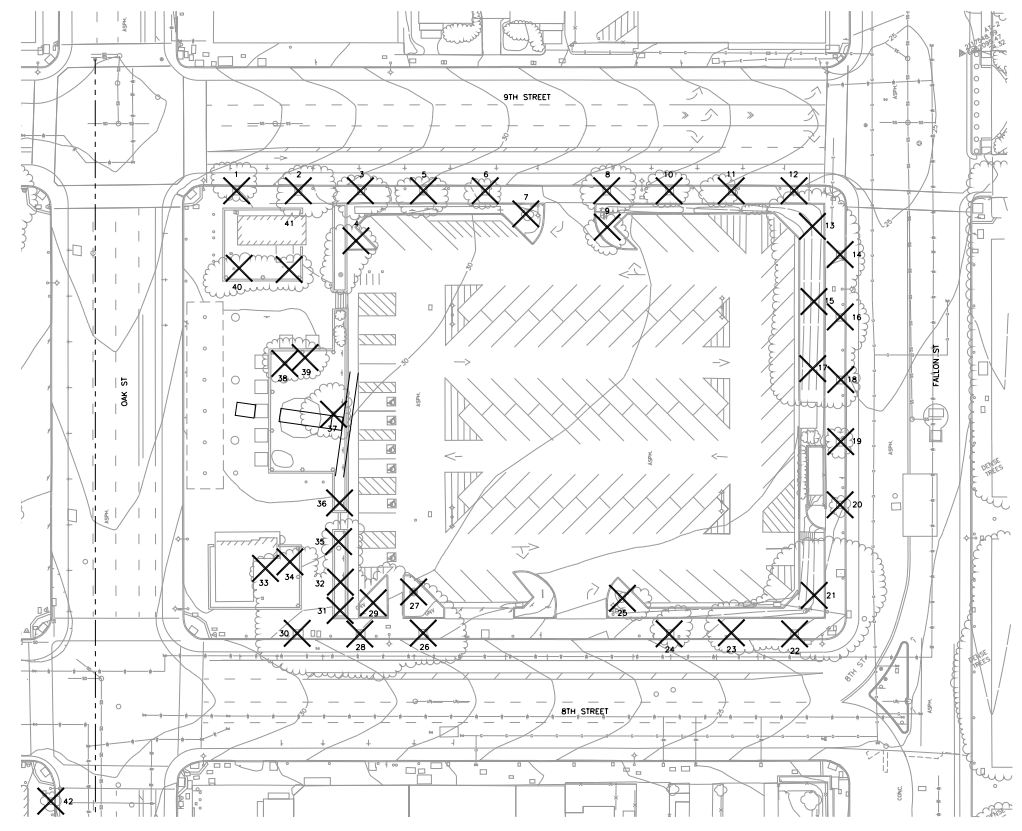


DATE: AUGUST 8, 2019  
 SCALE: 1" = 20' (VIEWED AT 24" X 36")

## BLOCK 1 TREE SURVEY

# L0.6





Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
1	13"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
2	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
3	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
4	18"	Yes	<i>Olea europa</i>	Olive Tree
5	24"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
6	6"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
7	10"	Yes	<i>Olea europa</i>	Olive Tree
8	12"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
9	14"	Yes	<i>Olea europa</i>	Olive Tree
10	8"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
11	7"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
12	14"	Yes	STUMP	STUMP
13	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
14	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
15	13"	Yes	<i>Platanus racemosa</i>	California Sycamore
16	15"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
17	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
18	5"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
19	14"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
20	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
21	24"	Yes	<i>Platanus racemosa</i>	California Sycamore
22	20"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
23	10"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
24	7"	Yes	<i>Rhus lancea</i>	African Sumac
25	14"	Yes	<i>Olea europa</i>	Olive Tree
26	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
27	51"	Yes	<i>Olea europa</i>	Olive Tree
28	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
29	14"	Yes	<i>Olea europa</i>	Olive Tree
30	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
31	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
32	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
33	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
34	12"	Yes	<i>Olea europa</i>	Olive Tree
35	15"	Yes	<i>Olea europa</i>	Olive Tree
36	9"	Yes	<i>Pittosporum undulatum</i>	Victorian box
37	30"	Yes	<i>Olea europa</i>	Olive Tree
38	30"	Yes	<i>Olea europa</i>	Olive Tree
39	13"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
40	25"	Yes	<i>Olea europa</i>	Olive Tree
41	20"	Yes	<i>Pittosporum undulatum</i>	Victorian box

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
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OAKLAND, CA 94612

**STRADA**  
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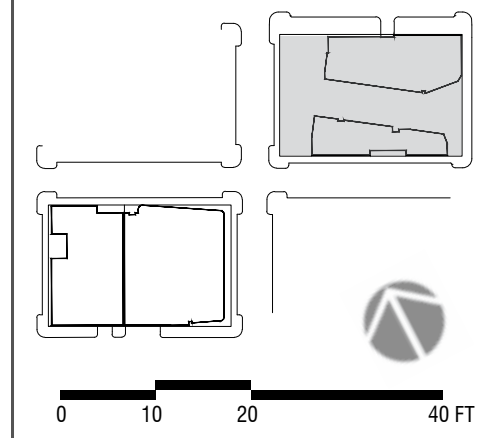
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPH COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPH COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPH COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPH COMMENTS	03/17/2021

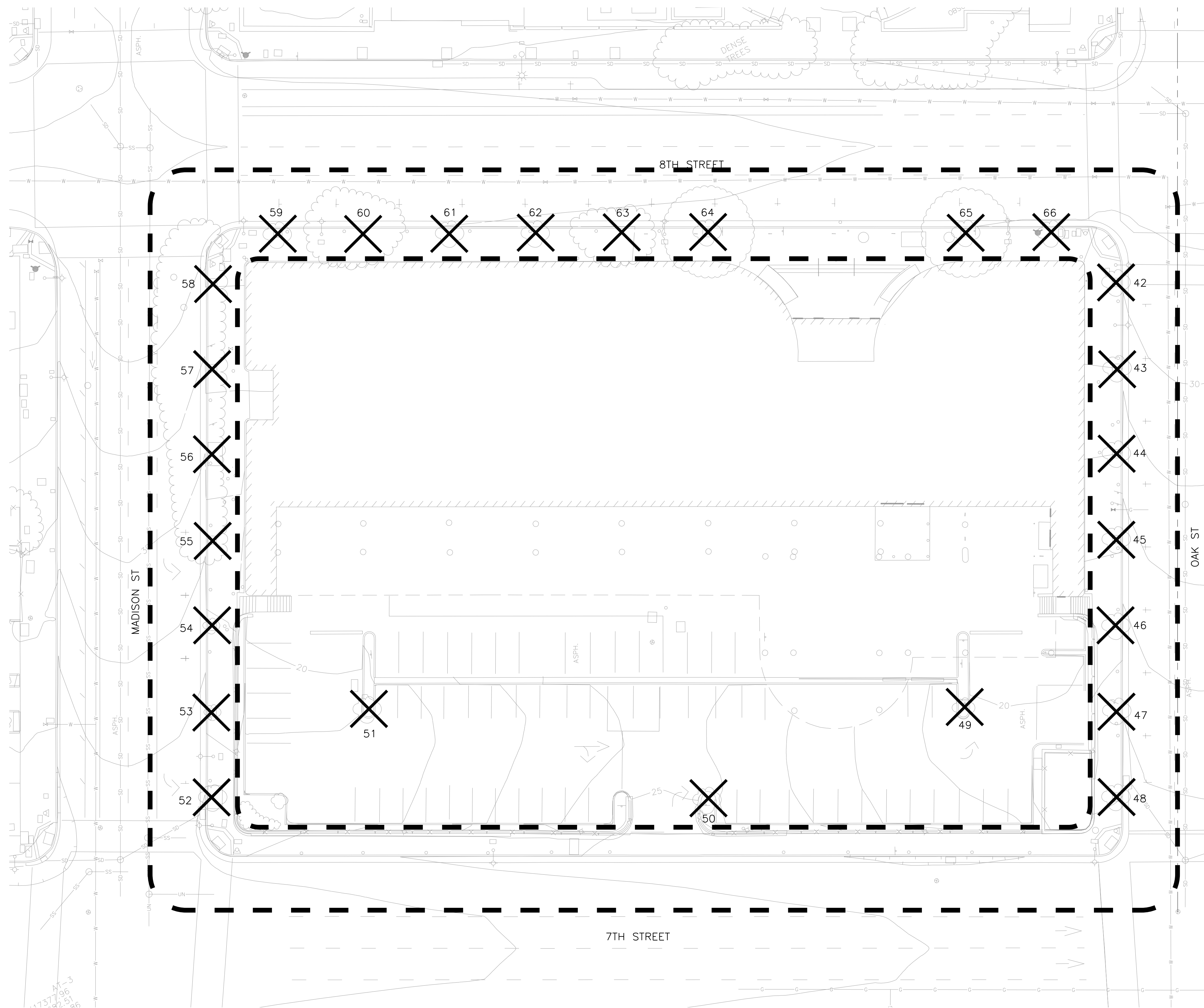


DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

### BLOCK 1 TREE SURVEY

# L0.61





Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
42	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
43	13"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
44	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
45	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
46	3"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
47	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
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52	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
53	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
54	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
55	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
56	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
57	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
58	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
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LEGEND

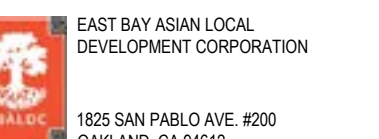
 30' OFFSET FROM CONSTRUCTION (LOW) FOR TREE ID AND PRESERVATION PURPOSES

NOTE:

The following are Protected Trees according to City Ordinance Chapter 12.36. PROTECTED TREES:  
 A. Any Coast Live Oak tree that is larger than 4 inches DBH. B. Any tree (except Eucalyptus) that is larger than 9 inches DBH. (Eucalyptus trees and up to 5 Monterey Pines per acre are not considered Protected Trees under this section. Monterey Pines must be inspected and verified by the Public Works Agency- Tree Division prior to their removal. C. Any tree of any size located in the public right-of-way (including street trees).

NOTE ON REMOVAL:

1. We are recommending removing 40 protected trees because of overall poor structure and poor health.
2. To meet the needs of a multi modal transit center, new sidewalks designs, bike paths, pick up and drop off, ADA multi-modal drop off will all require the relocation and demolition of sidewalks. This will disrupt the roots and will create larger soil volume areas.
3. The master plans aim is to make a cohesive neighborhood identity (coordinating with Madison Sq. Park, Bart plaza (TOF), and Block 1 + 2) and to have a cohesive plant palette.
4. See page L0.8 and L0.81 for new tree planting. Intent of design to provide street trees within the scope of the project.



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## LAKE MERRITT BART REDEVELOPMENT

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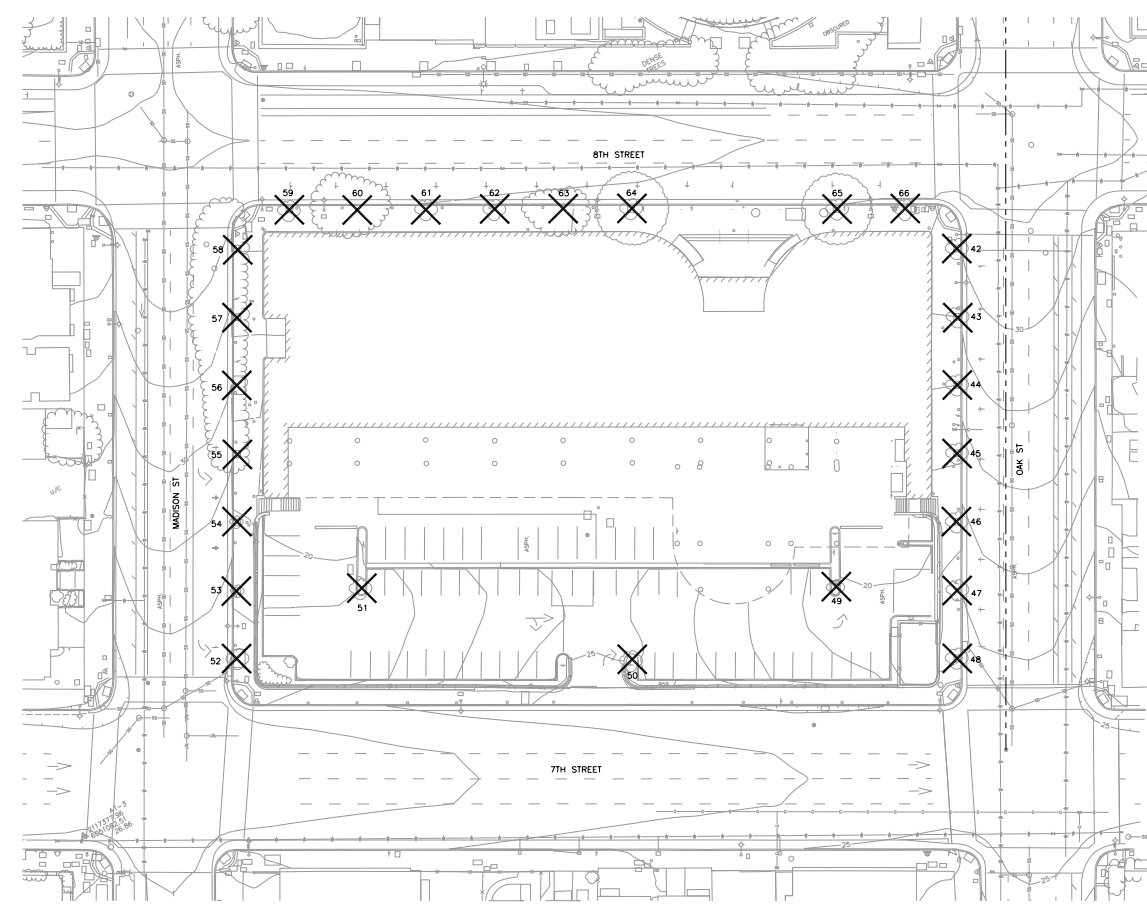


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## BLOCK 2 TREE SURVEY

# L0.7





41



42



43



44



45



46



47



48



49



50



51



52



53



54



55



56



57



58



59



60



61



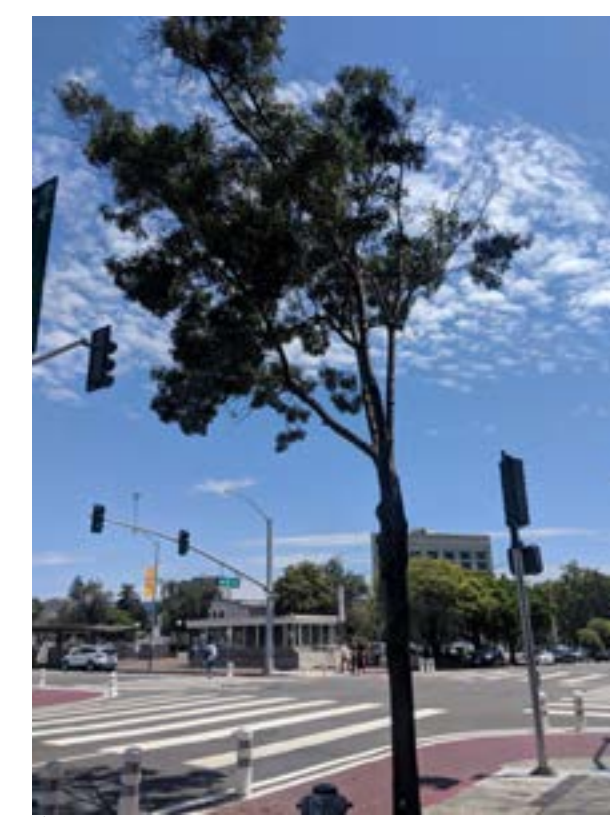
62



63



64



65

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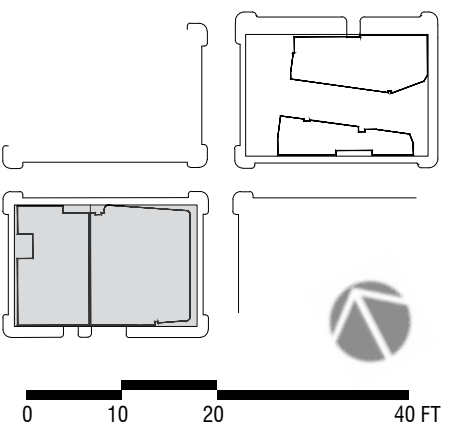
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

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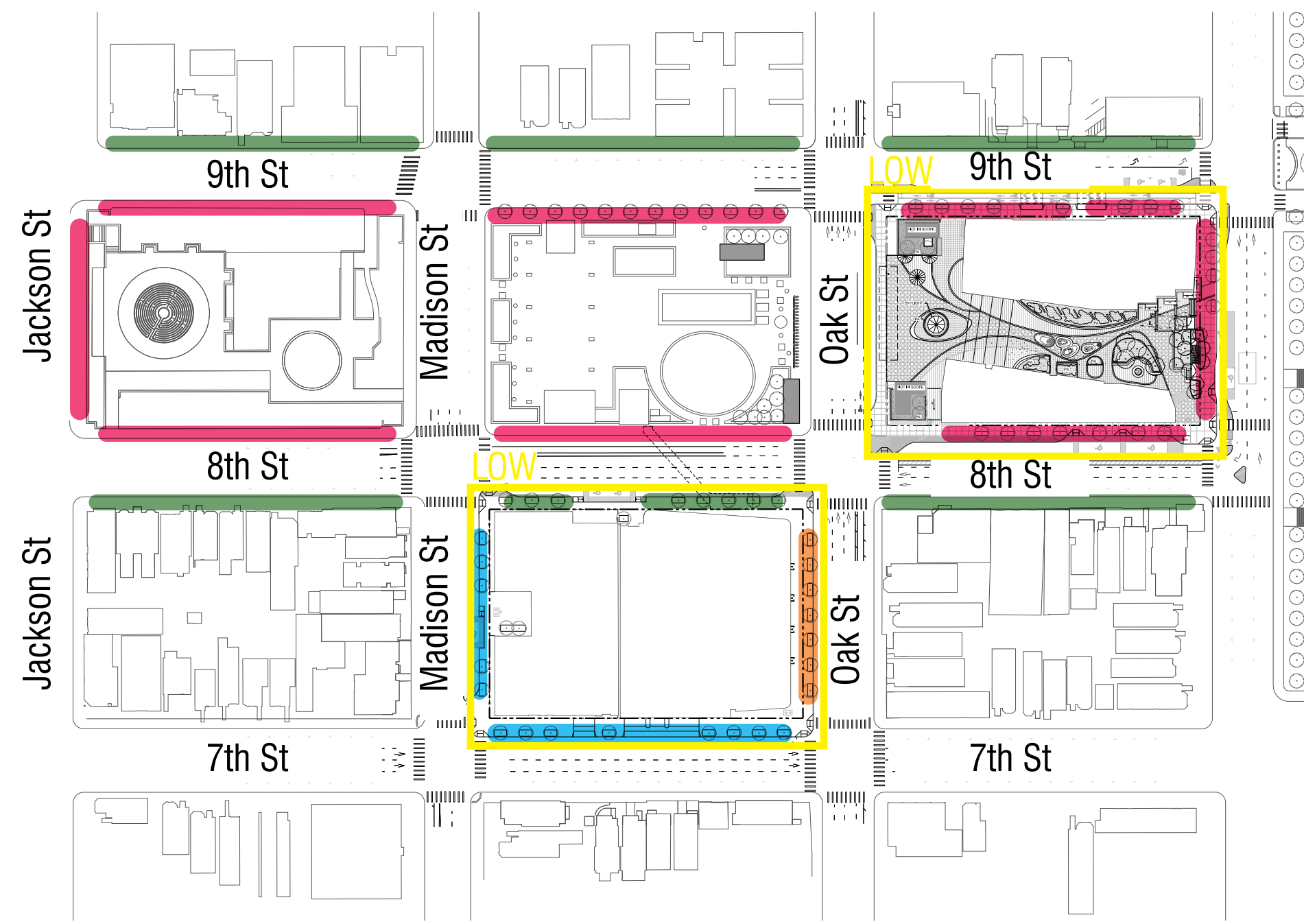


DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

## BLOCK 2 TREE SURVEY

**L0.71**





Key Plan

**NOTE:**

1. The project shall demonstrate compliance with the City of Oakland's Landscape Ordinance prior to building permit final. Project will be responsible to carry out any changes that may result from compliance with the Landscape Ordinance, which may include, but not limited to, revised plans, additional technical information, and/or additional planning applications.

2. Species selections may change based on availability, disease, or overall coordination of planting palette.

3. The proposed planting type, character, and water use is in accordance to the City of Oakland's master street tree list. The species selection will develop as we work in conjunction with TOF and Madison Square Park.

4. This is intended to work as a 3 block open space, and the continuity of plant species will be consistent between project blocks.

**LEGEND AND WUCOLS RATING**

- Pollinator
- Water Use Low
- Water Use Regular
- Water Use High
- Oakland Master Street Tree List

**EXISTING TREE SPECIES**

**Oak Street**



Block 2: Eucalyptus microtheca

**PROPOSED TREE SPECIES BY STREET**

**Oak Street - 7 Trees**



Red Horsechestnut, *Aesculus carnea 'Briotii'*  
Upright-oval rounded form, shade tree and ornamental tree. Showy flowers.



Non native, moderate water use, pollinator

**EXISTING STREET TREES WITHIN THE 3 BLOCK OPEN SPACE FRAME**

**9th Street**



Block 1: California Sycamore



Block 1: Olive tree



Block 1: African sumac



Block 1: California Sycamore

**8th Street**



Block 1: California Sycamore



Block 1: African Sumac



Block 1: Olive Tree



Block 2: Eucalyptus microtheca

**Fallon Street**



Block 1: California Sycamore



Block 1: African sumac



Block 1: African Fir Pine

**PROPOSED TREE SPECIES**

**3 Block Open Space Frame - 75 Trees**



Block 1 & 2: Cork Oak, *Quercus suber*  
Medium-sized, evergreen tree, with open spreading form. Interesting bark texture.



Non native, Low water use



Enlargement of Cork Oak

The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads.

Plant areas shown over the tunnel structure must comply with BES standards.

**Madison Street**



Block 2: Eucalyptus microtheca

**7th Street**



Block 2: Eucalyptus microtheca

**9th + 8th**



Block 2: Eucalyptus microtheca

**Madison Street - 5 Trees**



Brisbane Box, *lophostemon confertus*  
Large, upright, evergreen tree. Effective screen or wind break.

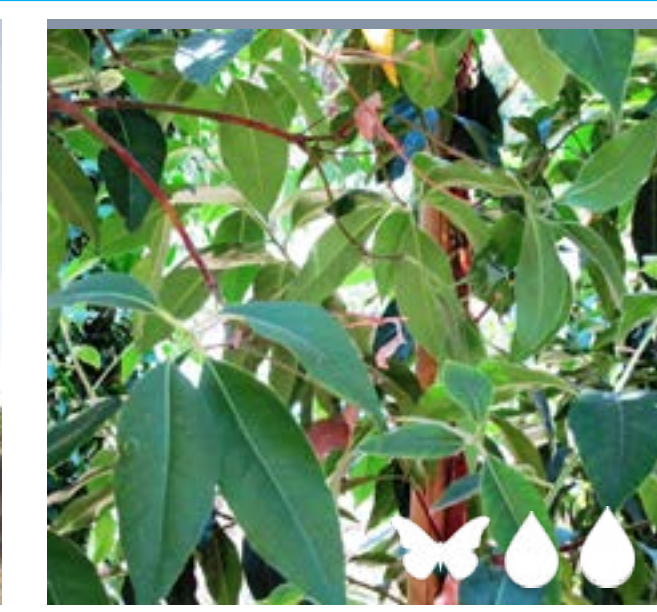


Water regular, non-native, pollinator

**7th Street - 8 Trees**



Brisbane Box, *lophostemon confertus*  
Large, upright, evergreen tree. Effective screen or wind break.



Water regular, non-native, pollinator

**9th & 8th Opposite Open Space Frame**



Paperbark Tree, *Melaleuca quinquenervia*  
Multi trunk. Erect, spreading form with low canopy. Evergreen. Showy flowers in Summer or Fall. Paper like bark.



Drought tolerant, non-native, pollinator

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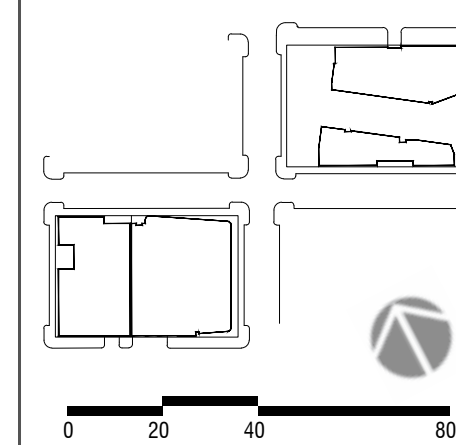
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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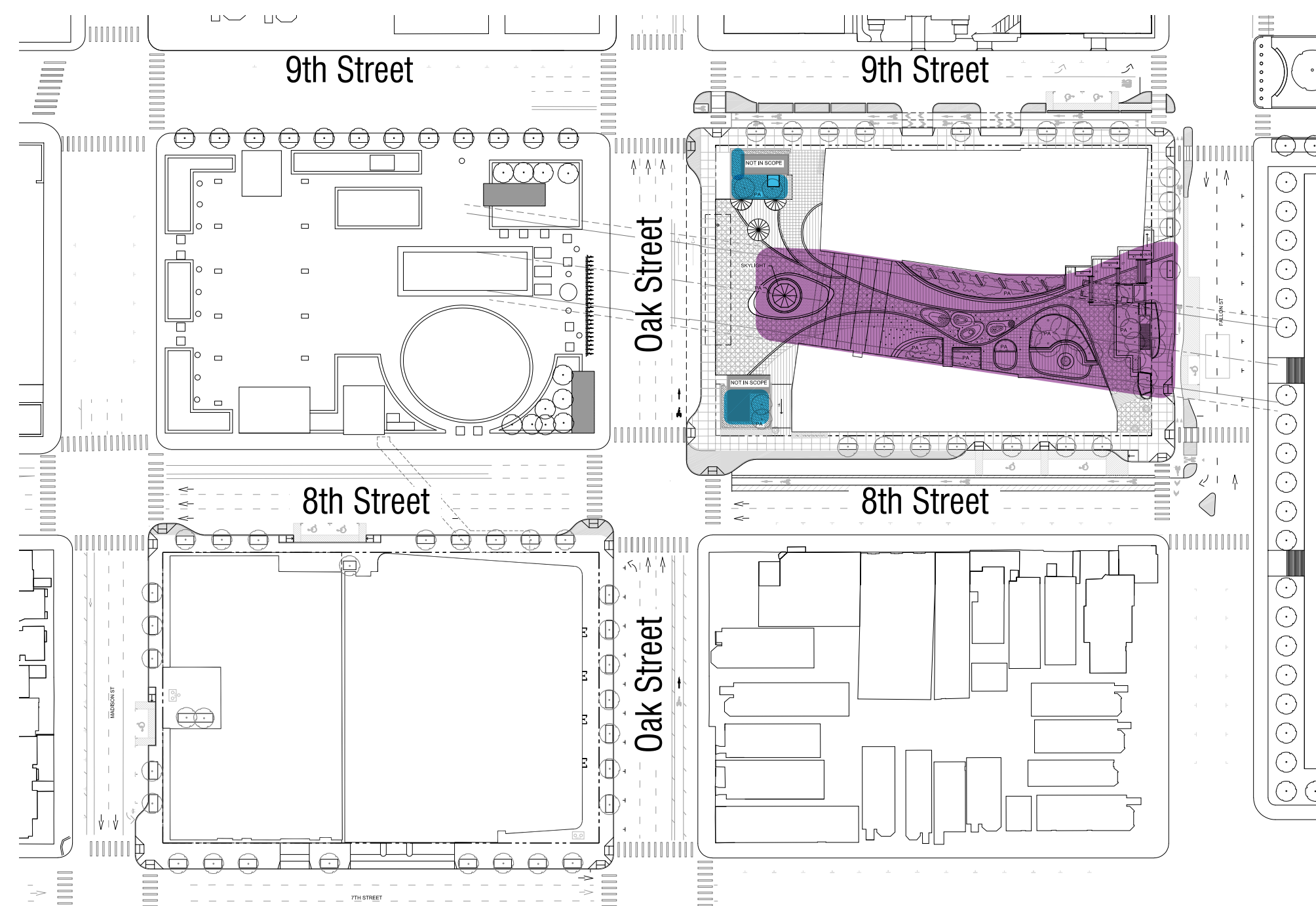
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**L0.8**





Key Plan

Proposed Concrete Planters



Headhouse planters and Paseo planters will be constructed with concrete at seat height.

- NOTE:**
1. The project shall demonstrate compliance with the City of Oakland's Landscape Ordinance prior to building permit final. Project will be responsible to carry out any changes that may result from compliance with the Landscape Ordinance, which may include, but not limited to, revised plans, additional technical information, and/or additional planning applications.
  2. Species selections may change based on availability, disease, or overall coordination of planting palette.
  3. We're proposing planting type, character, and water use in accordance to the City of Oakland's approved street tree list. The species selection will develop as we work in conjunction with TOF and Madison Square Park.
  4. Species selected are sourced from Oakland's approved street tree species list.

LEGEND AND WUCOLS RATING

- Pollinator
- Water Use Low
- Water Use Regular
- Water Use High
- Oakland Master Street Tree List

Existing Skylight Planter Planting



Skylight Planter: Defensive planting for skylight  
 Ornamental grass mix: Carex  
 Skylight Planter: Peruvian Pepper tree

No ground cover or understory planting noted at the skylight planter.

Proposed Skylight and Paseo Planting



Paseo Planter: *Sesleria nitida* mix with *euphorbia*  
 Regular water use, non natives  
 Vine on parking garage: *Passiflora caerulea*, Low water use, non native  
 Enlargement: *Accacia cognata* 'Cousin It' and Aeoniums mix.  
 Low water use, non native  
 Paseo Planter: Western Sword Fern, Regular water, native  
 Paseo Planter: Sweet Box, *Sarcococca confusa*  
 Regular water, non native, pollinator  
 Tree Choice: Fern Pine, *Afrocarpus Gracilior*  
 Non native, regular to low water  
 Tree Choice: Flowering Dogwood, *Cornus florida*  
 Non native, regular water

Existing Headhouse Planter Planting



N. Headhouse: Olive tree  
 N. Headhouse: Victorian Box  
 S. Headhouse: Olive tree  
 S. Headhouse: Victorian Box

No ground cover planting noted  
 Agapanthus as understory planting found in the northern headhouse planter.

Proposed Headhouse Planting



Monkey Grass, Mondo grass  
 Non native, regular to low water use  
 Green Leaf Japanese Maple, *Acer Platanum*, with Mondo grass  
 Non native, regular water  
 Tree Choice: Fern Pine, *Afrocarpus Gracilior*  
 Non native, regular to low water  
 Tree Choice: Flowering Cherry Tree, *Prunus 'Kwanzan'*  
 Non native, regular water use

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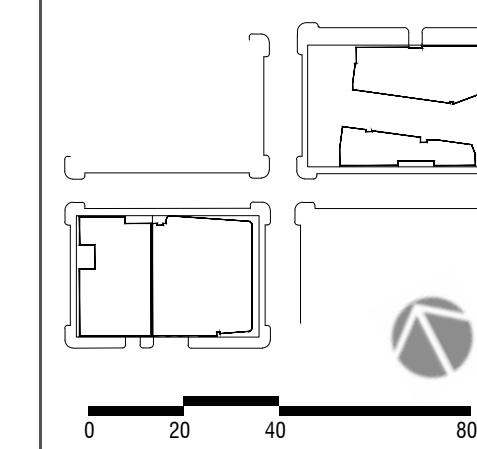
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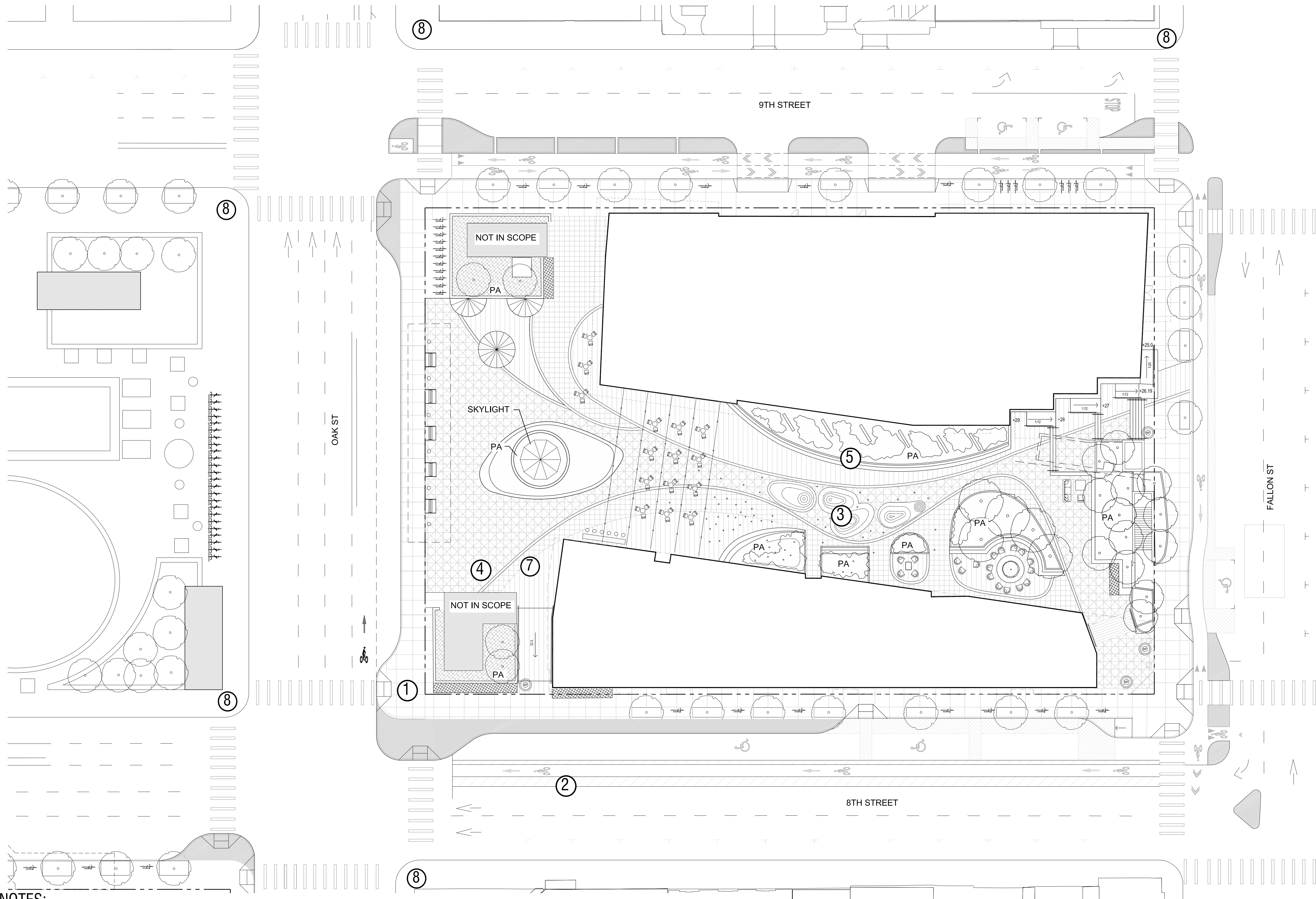
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DATE: AUGUST 8, 2019  
 SCALE: 1" = 40' (VIEWED AT 24" X 36")





**NOTES:**

- ① See Civil Drawings for limit of project sidewalk improvements.
- ② Street access plan striping shown for planning purposes only.
- ③ Paseo design under study. Furniture is movable. Typical conditions shown.
- ④ All open space elements to be maintained by BART will be designed to BFS standards.
- ⑤ The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads. Plant areas shown over the tunnel structure must comply with BFS standards.
- ⑥ BART security cameras to be included as agreed to with BART.
- ⑦ Property lines between the headhouse and building frontage of Building A and Building B will be called out in the paving with physical markings. See Civil Drawings for Property Lines
- ⑧ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping and crosswalks as necessary.

**NOTE:**

"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval" and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

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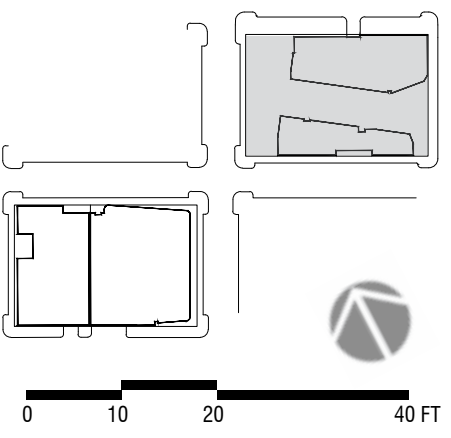
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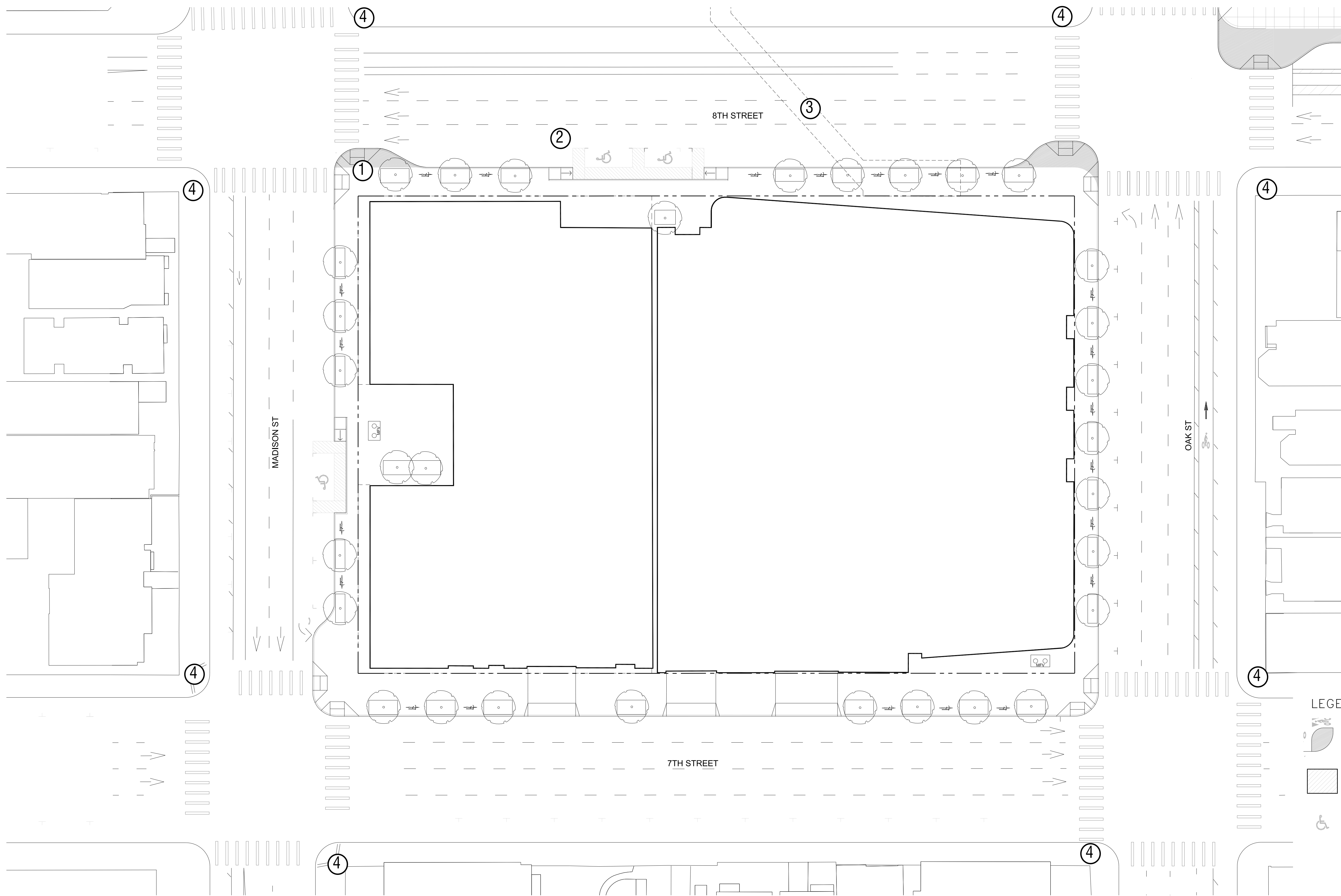
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**BLOCK 1 OVERALL PLAN**





**LEGEND**

	BULBOUT
	PAINT STRIPING
	ADA PARKING OR LOADING

- NOTES:**
- ① See Civil Drawings for limit of project sidewalk improvements.
  - ② Street access plan striping shown for planning purposes only.
  - ③ Sub surface tunnel (approximate location based On As-Built Drawings from Bart Project #17hc-110).
  - ④ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping as necessary.

**NOTE:**  
 "BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

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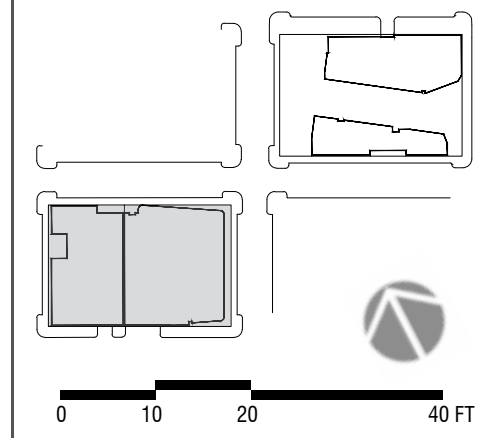
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

**REVISION SCHEDULE**

NO.	ISSUE	DATE
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3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP#4 COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
 SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 OVERALL PLAN**

**L2.2**



## TRANSIT



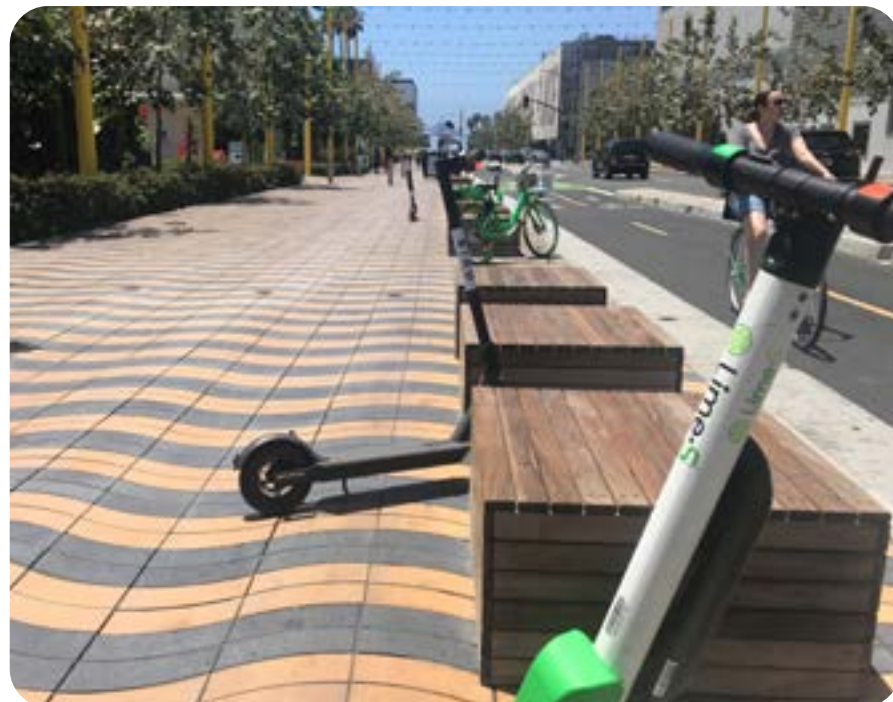
BIKE SHARE



BART STATION



BIKE PARKING



MODE TRANSFER AND WAITING AREAS

## DINING



OUTDOOR DINING



FOOD COURT

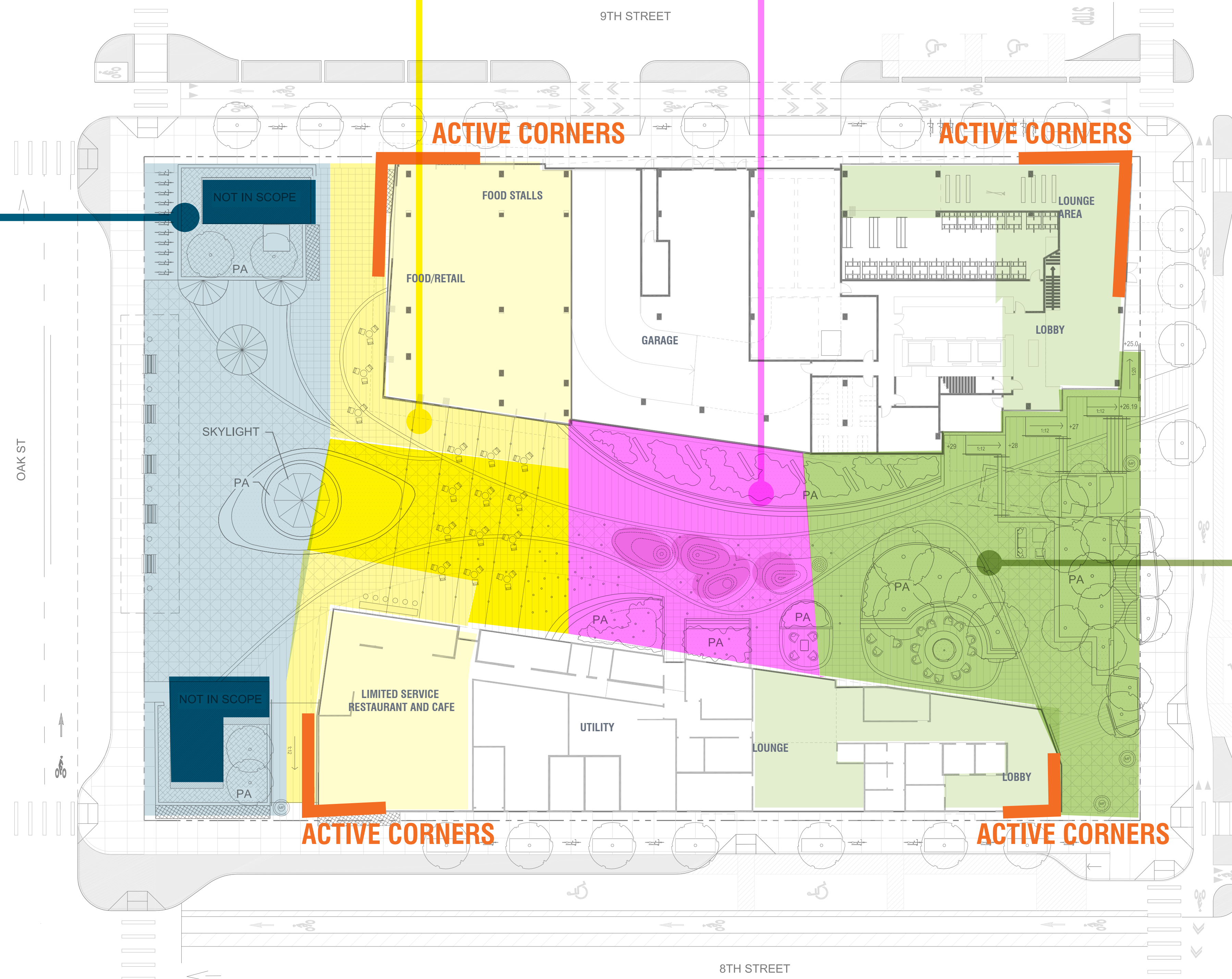
## ART + PLAY



PUBLIC ART/ PUBLIC CULTURE



INTERACTIVE SCULPTURE



## RECREATION



COMMUNAL SEATING



GARDEN FURNISHINGS



MAKER SPACE

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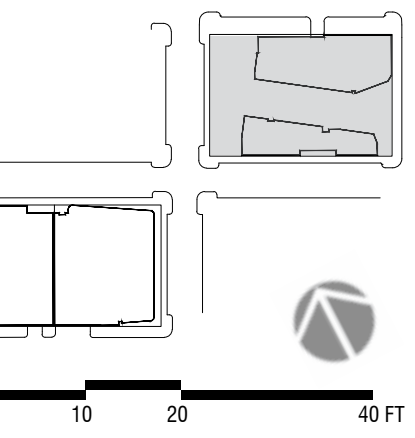
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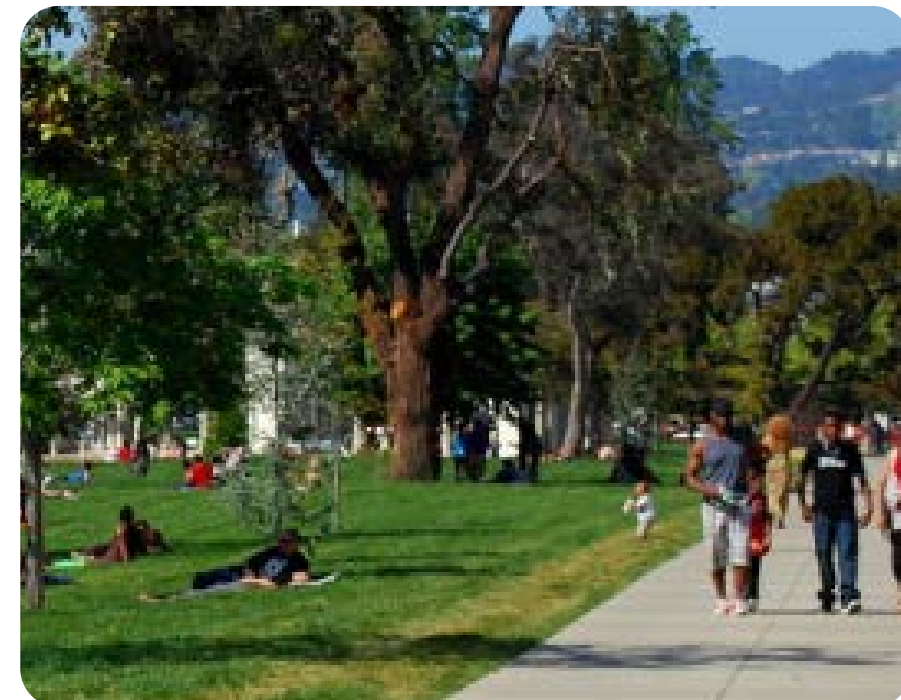
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### BLOCK 1 PROGRAM

# L2.3



## DAYCARE



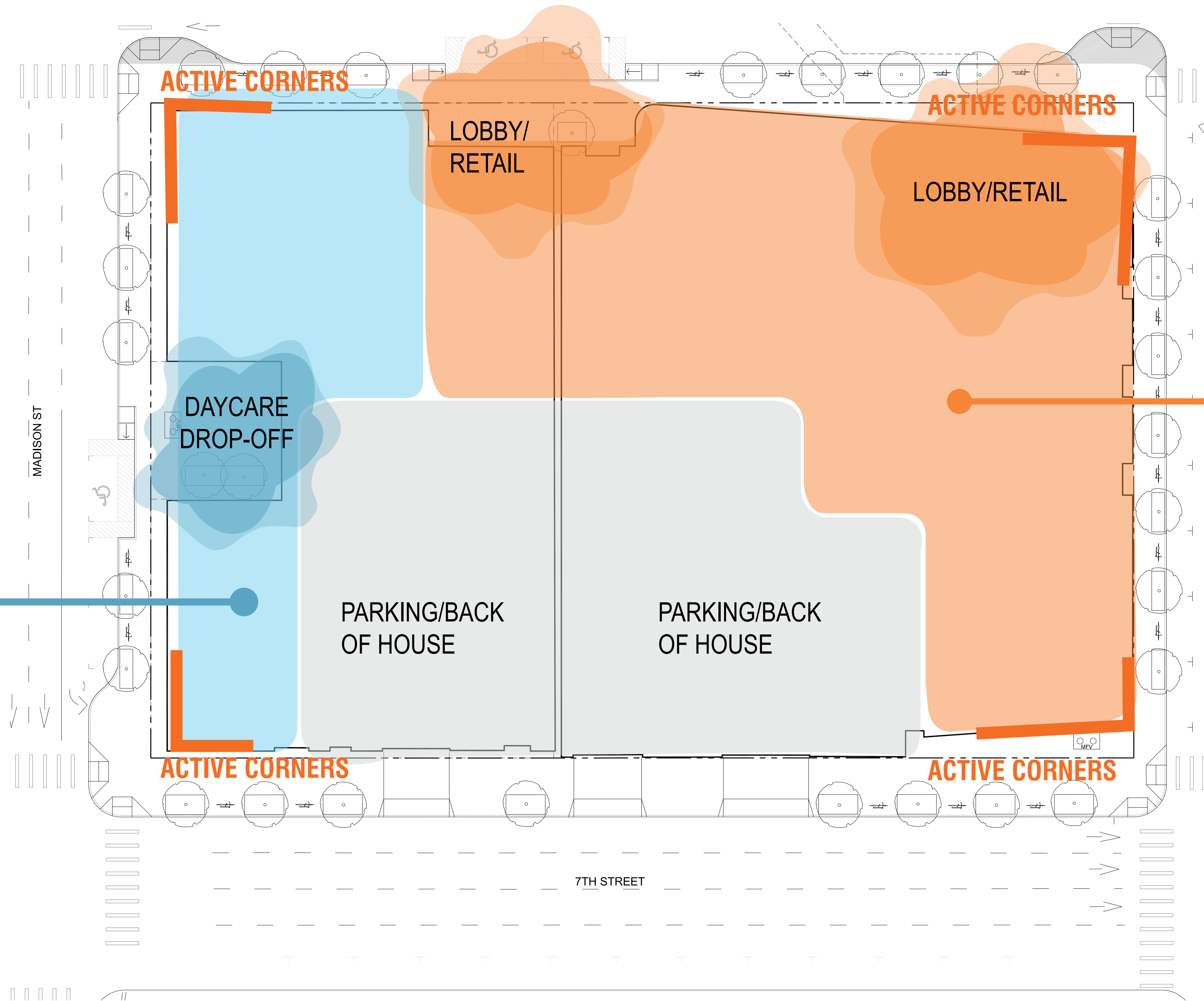
ADJACENCY TO PUBLIC PARKS



DAYCARE DROP-OFF



DAYCARE



## LOBBY/RETAIL



LOBBY



SEMI PUBLIC LOBBY



INFORMAL WORKSPACES

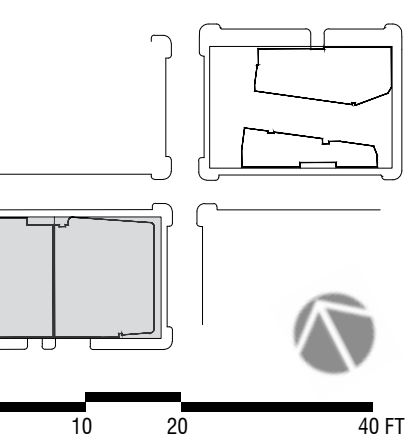
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## BLOCK 2 PROGRAM

# L2.4





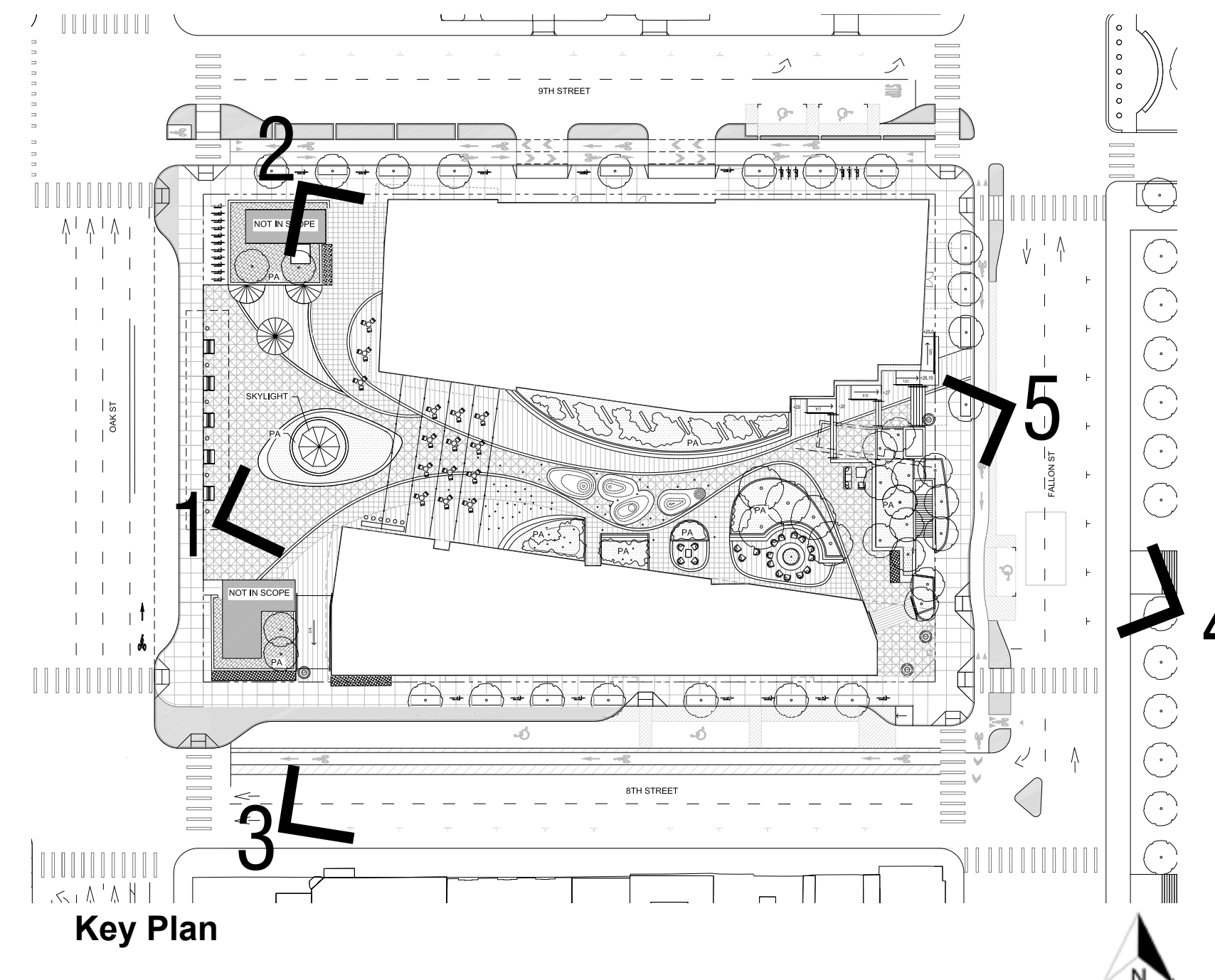
1 Paseo from Oak St.



2 9th St. looking in towards Building A and Paseo



4 Paseo from Fallon St.



Key Plan



3 8th St. looking towards Building B



5 Fallon St. looking at Building A lobby and Paseo

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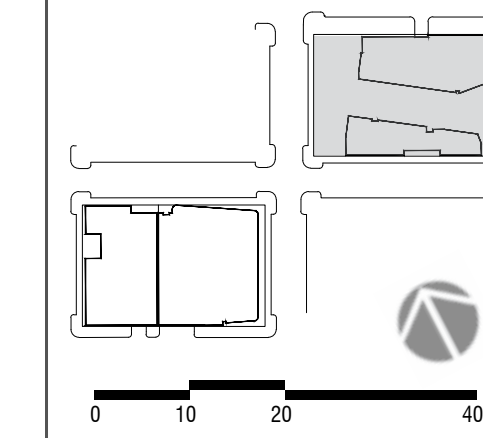
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**BLOCK 1 VIEWS**

**L2.5**



Lake Merritt BART TOD Site #1 - Proposed Street Design Options by Access Plan Team

	8th Street		Fallon Street			9th Street			Oak Street		
	Option 1	Option 2	Option 1	Option 2	Option 3	Option 1	Option 2	Option 3	Option 1	Option 2	Option 3
<b>Travel Lanes</b>	2 lanes WB		1 lane NB, 1 lane SB			1 lane WB, 1 lane EB	3 lanes EB	2 lanes EB	2 lanes NB		3 lanes NB
<b>Transit?</b>	Yes - outside lane should be 11'. Awaiting Confirmation from AC Transit if Bus Only Lane is Needed		Yes - lanes should be 11'			No - 10' Lanes OK, consider 11' outside lane if significant shuttle/paratransit/truck activity anticipated			Yes - outside lane should be 11'; install bus bulb on east side. Move layover elsewhere on the route due to spatial constraints.		Yes - outside lane should be 11' - could stripe outside lane as bus only lane
<b>Traffic Operations</b>	One-Way WB		Existing two-way operations; consider extending two-way operations to 7th Street			Two-Way Conversion (assuming TOD/outside funding is identified to build at least to Harrison Street)			One-way operations		One-Way NB
<b>Curb Use/Parking Lane</b>	Create wide shuttle zone/ accessible passenger loading zone the whole length of block. Design as a pull out with a boarding island. Retain existing Shuttle Zone for most of north side; add accessible passenger loading needed for senior housing entrance; assume no metered parking on north side.		Passenger loading needed along Laney frontage, west side pick up and drop off near paseo entrance			Passenger loading zone likely most of block (for BART and tower) plus commercial loading (? for tower). 5' buffer preferred for passenger/goods loading.			Passenger loading zone on west side - add passenger side buffer if space allows; bus stop on east side. Layover becomes a pullout or moves to another location if not enough space with double bus stop. No metered parking.		
<b>Bikeway</b>	One-way WB In-Roadway Protected Bike Lane	Buffered bike lane WB (existing condition)	In-Roadway, Directional Protected Bike Lanes	In-Roadway, Two-Way Protected Bike Lane	Shared Street (?) with Two-Way Protected Bike Lane	Raised (or in roadway) Two-Way Protected Bike Lane	Raised One-Way Protected Bike Lane	In-Roadway One-Way Protected Bike Lane	Raised Two-Way Protected Bike Lane	In-Roadway Two-Way Protected Bike Lane	In-Roadway Two-Way Protected Bike Lane
<b>Bike Implications/Notes</b>	Upgrade existing bike lane as safety/comfort enhancement though lower bike priority	Not upgraded due to bike priority on 9th Street	Cross-section should continue between 7th and 10th			Addresses BART and TOD tower access needs; Consider directional PBLs west of Oak Street		Does not resolve need for two-way bike traffic to BART and TOD site	Would need channelization of pedestrian coming from passenger loading; buffer needs to be at least 4' for loading		Would need channelization of pedestrian coming from passenger loading plus mid-block ramps
<b>Sidewalks (including furnishings zone)</b>	Widen north sidewalk widened by 1.5'		West Sidewalk Widened by 6'	West Sidewalk Widened by X'	West Sidewalk Widened by X'	South sidewalk widened by 6.5'	West Sidewalk Widened by X'	West Sidewalk Widened by X'	No sidewalk widening (beyond bus bulb)		No sidewalk widening
<b>Intersection Notes</b>	Protected intersection at 8th/Oak and 8th/Fallon plus pedestrian safety upgrades		Upgrade intersections at 9th/Fallon and 8th/Fallon with Protected Intersections and Pedestrian Safety Enhancements			Special signalization (bike contra-flow signal), pedestrian safety upgrades needed at most signals; 9th/Fallon and 9th/Oak protected intersections plus pedestrian safety enhancements. Need to assess need for left-turn pockets.		9th/Fallon and 9th/Oak protected intersections plus pedestrian safety enhancements	Special signalization (bike contra-flow signal); pedestrian safety upgrades needed at most signals; 9th/Oak and 8th/Oak protected intersections plus pedestrian safety enhancements		
<b>Pedestrian Implications/Notes</b>	Consideration of matching DOSP sidewalk widening (at least on north side of street) and cross-section		Consideration of mid-block pedestrian crossings (or channelization) given location of double staircase + ramp		May address need for mid-block	Consideration of Pedestrian access across the PBL from the heavily used passenger loading zone					
<b>Access Team Recommends Moving the Cross-Section Forward?</b>	Yes - recommend assuming north curbline/sidewalk consistent with DOSP	No - adding the protection to the bike lane will enhance safety and comfort	Yes - Recommend directional protected bike lanes between 7th and 9th Street with two-way conversion between 7th and 8th. Consider EITHER (1) directional or (2) two-way protected bike lanes between 9th and 10th, depending on how realistic East Bay Greenway project on 10th Street is.	No - not a strong reason to do a two-way protected bike lane south of 9th Street.	No - may detract from TOD paseo. Would require traffic diversion, Laney College stakeholder engagement, and slow design speed.	Yes - between Oak and Fallon Streets. Raising it will allow for a comfortable streetscape in front of the tower. Note that DOT would rather see that investment made to extend the protected bike lane farther west. PBLs can continue as directional or two-way west of Oak Street.	No - does not resolve need for two-way bike traffic along TOD site	No - does not resolve need for two-way bike traffic along TOD site	No - would not be consistent with DOSP though might function well against passenger loading zone today.	Yes - could be converted consistent with DOSP cross-section future. Consistent curbline needed with bus bulb.	No - inconsistent with recently installed pedestrian safety project + ongoing need to address High Injury Intersections
<b>Consistency with Other Plans</b>	-Consistent with Let's Bike Oakland -Allows for long-term two-way conversion and consistency with Downtown Oakland Specific Plan assuming streetscape is entirely rebuilt OR streetscape can be designed to make sense with a sidewalk extension of 8.5'	-Inconsistent with Let's Bike Oakland, which calls for protected bike lane -Allows for long-term two-way conversion and consistency with Downtown Oakland Specific Plan assuming streetscape is entirely rebuilt OR streetscape can be designed to make sense with a sidewalk extension of 8.5'	-Consistent with Let's Bike Oakland -Consistent with Downtown Oakland Specific Plan -Inconsistent with East Bay Greenway	-Consistent with Let's Bike Oakland -Consistent with Downtown Oakland Specific Plan -Consistent with East Bay Greenway	-Inconsistent with Let's Bike Oakland -Consistent with Downtown Oakland Specific Plan -Consistent with East Bay Greenway	-Allows for near-term OR long-term two-way conversion and consistency with Downtown Oakland Specific Plan (DOSP) -DOSP shows directional protected bike lanes, but suggest need for two-way between Oak and Fallon Streets -Consistent with Let's Bike Oakland	-Consistent with Alameda CTC Access Project, but widens buffer -Narrows the double-wide passenger loading zone on west, but provides a buffer between the passenger loading zone and the travel lane -Two travel lanes consistent with OakDOT pedestrian safety project -Allows for long-term two-way conversion and consistency with Downtown Oakland Specific Plan -Consistent with Let's Bike Oakland				

For the block one street designs a series of options were developed by the design team for evaluation by the Access Plan Team (Fehr and Peers and Eisen|Letunic). Comments for each option are shown above and a preferred direction was selected for the project plans. Any time signals are added/alterd we will recommend the traffic engineer to provide an accessible pedestrian signal (aps) upgrade as referenced in Oakland Walks! Pedestrian Plan (2017, specifically recommended at 9th and Fallon), the draft Downtown Specific Plan, in PROWAG and MUTCD). Because the final street design and full spectrum of transportation issues will continue to evolve options were selected for maximum future flexibility as well as their success meeting the current stated programs of various transportation entities. A future proofing test for the Downtown Oakland Specific Plan is provided for each option to demonstrate flexibility. As additional criteria become apparent, additional testing of the flexibility of the scheme can be added.

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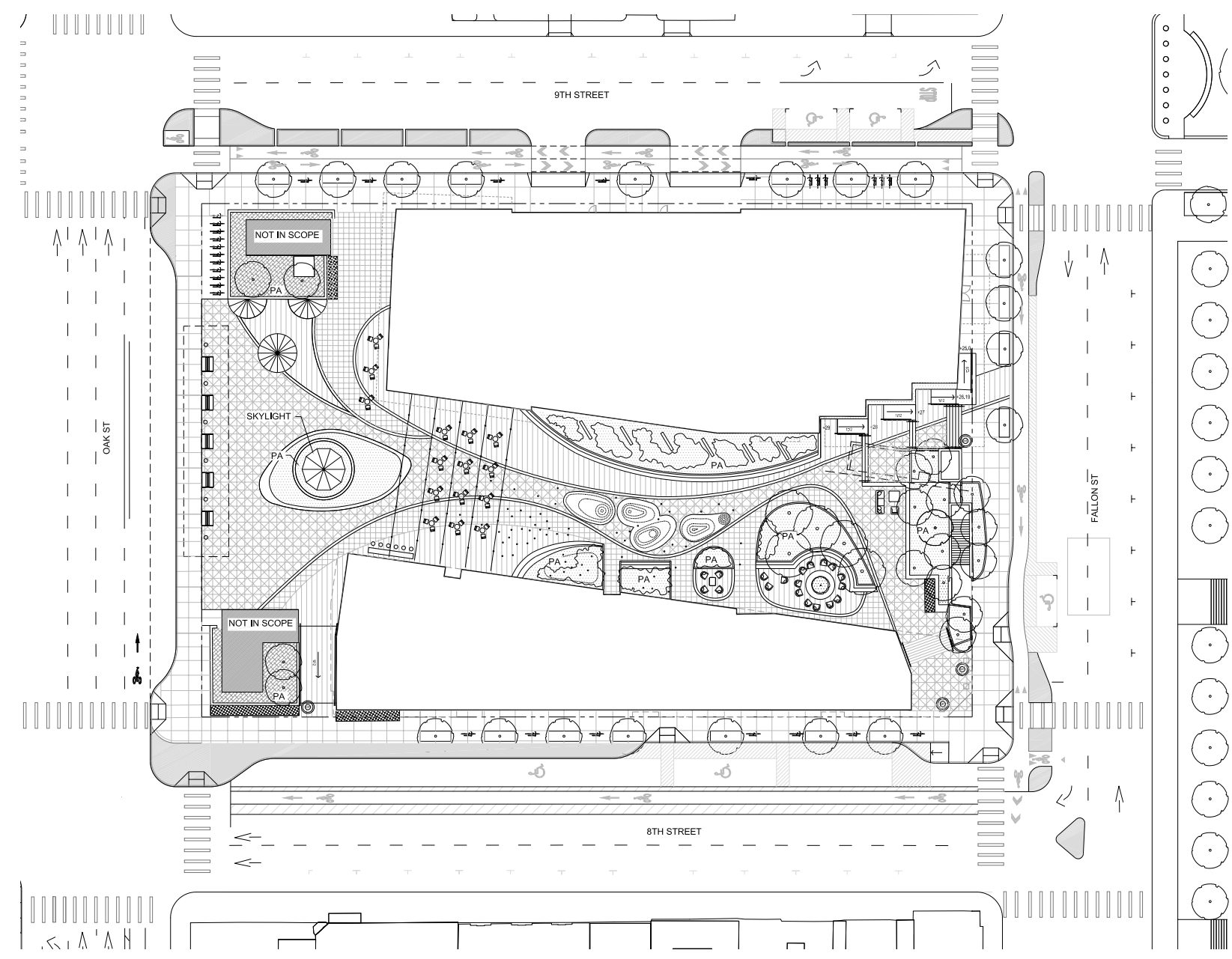
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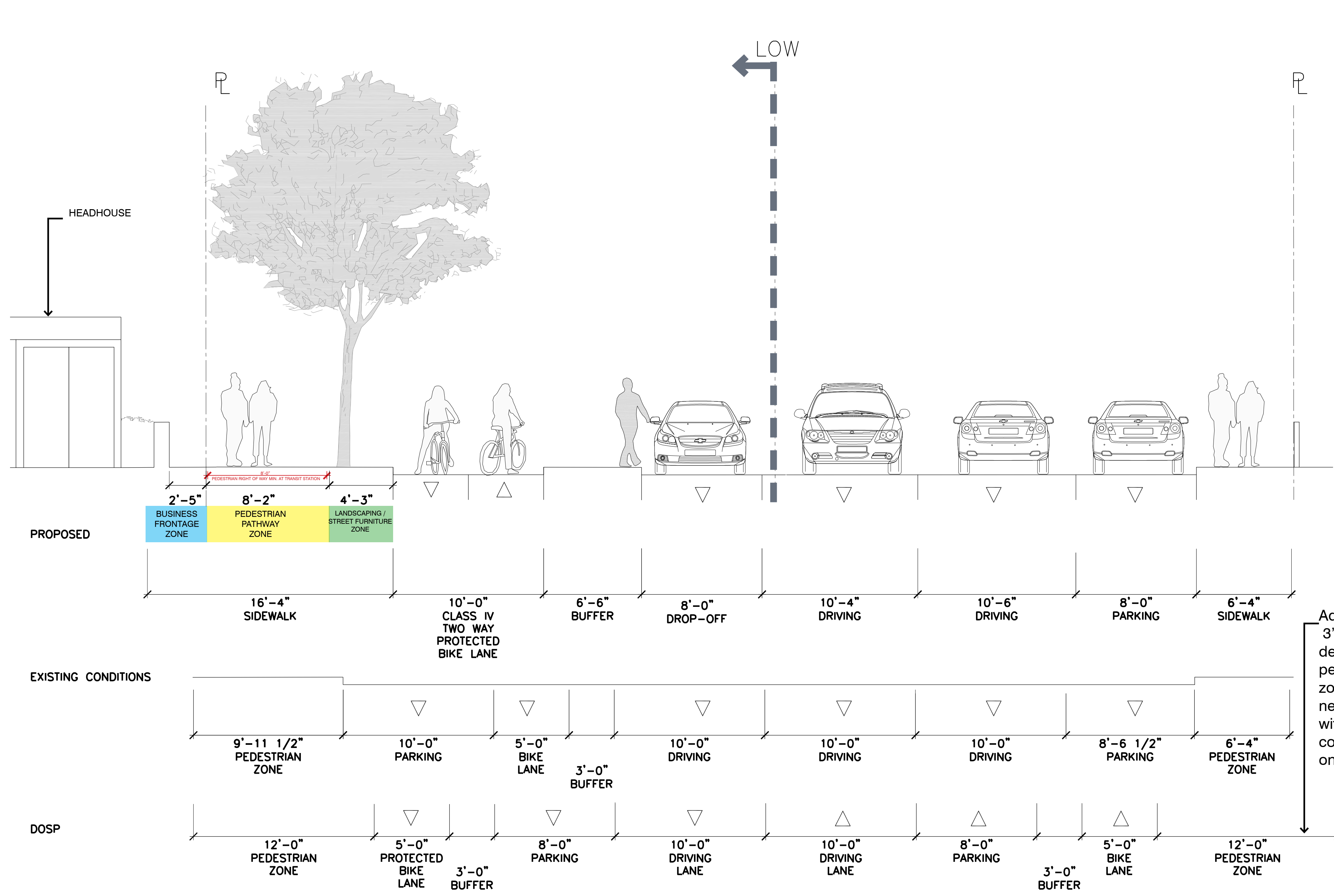
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**BLOCK 1 ACCESS FRAMEWORK**





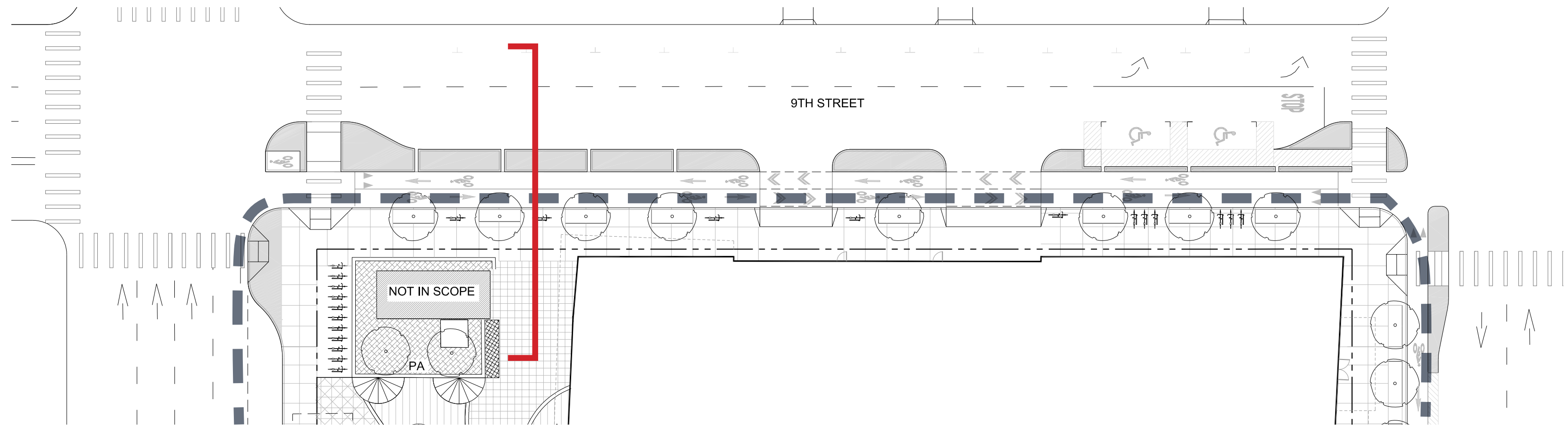
Key Plan



Additional 3'-0" of desired pedestrian zone to be negotiated with future construction on 9th.

Typical Cross Section

Note:  
This Street Design Framework was developed to allow the project to progress with building locations and program functions that connect the on site improvements to offsite improvements. Current planning documents are not consistent in their recommendations, so the strategy of the framework was to insure flexibility. The proposed option 1 is a potential working solution, but the final street design and dimensions will be the result of a process involving multiple stakeholders and transportation experts. The design team reserves the right to modify and refine these sections in light of new information and the evolving BART Transit Operations Facility (TOF) in the block bordered by 8th St., Madison St., 9th St., and Oak Street. These plans represent a best effort at capturing all information available today in a physical design solution which meets the maximum number of stated criteria for this important public realm. The final design will be ADA compliant.



Plan

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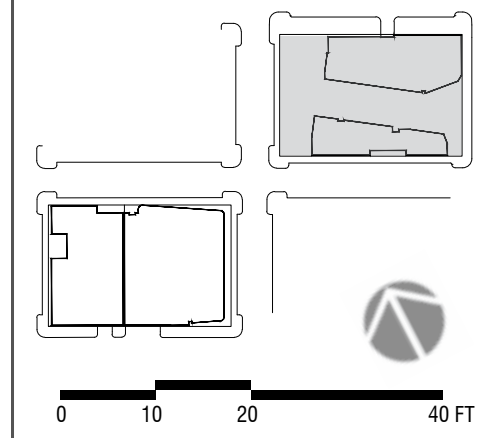
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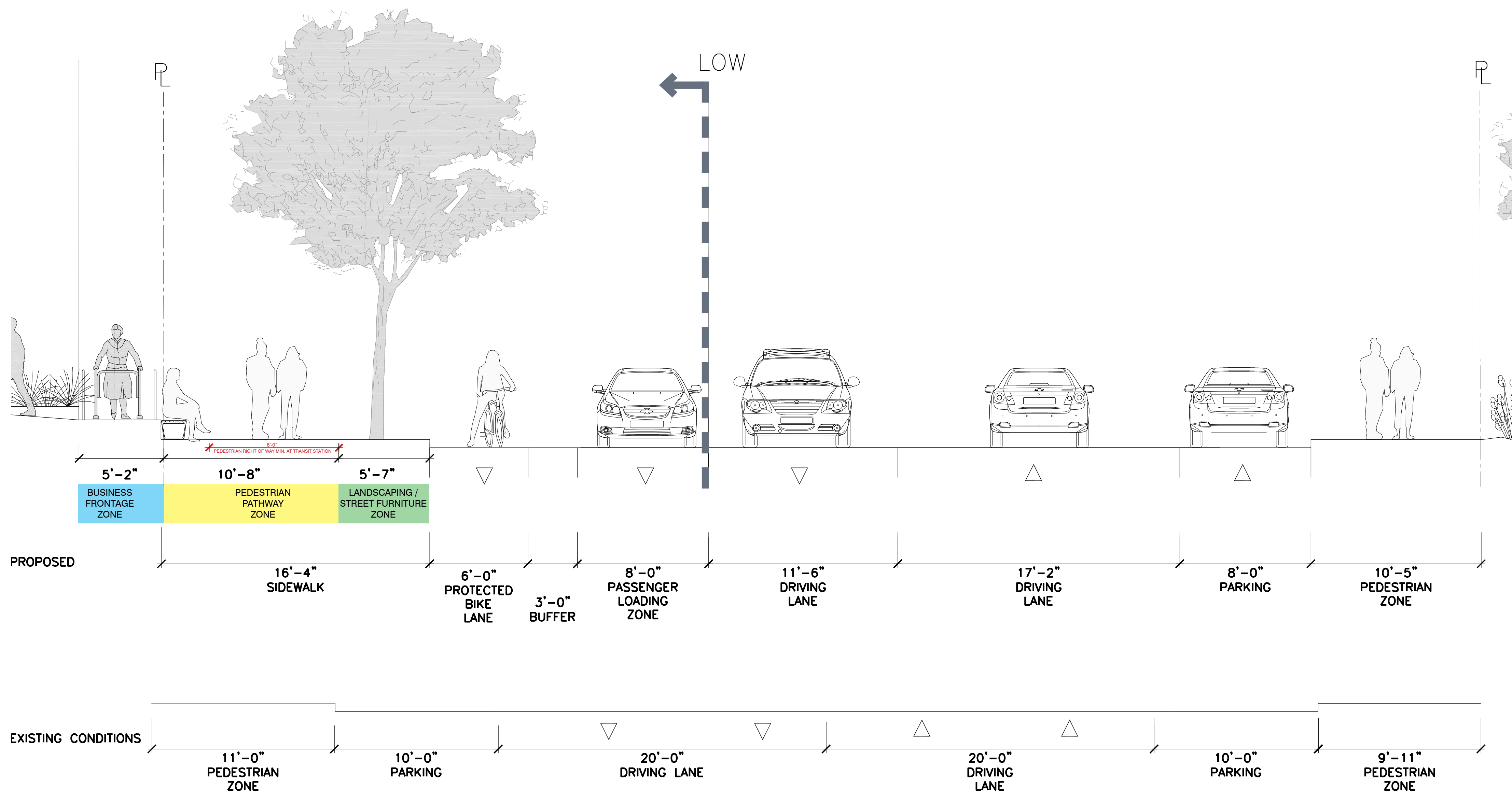
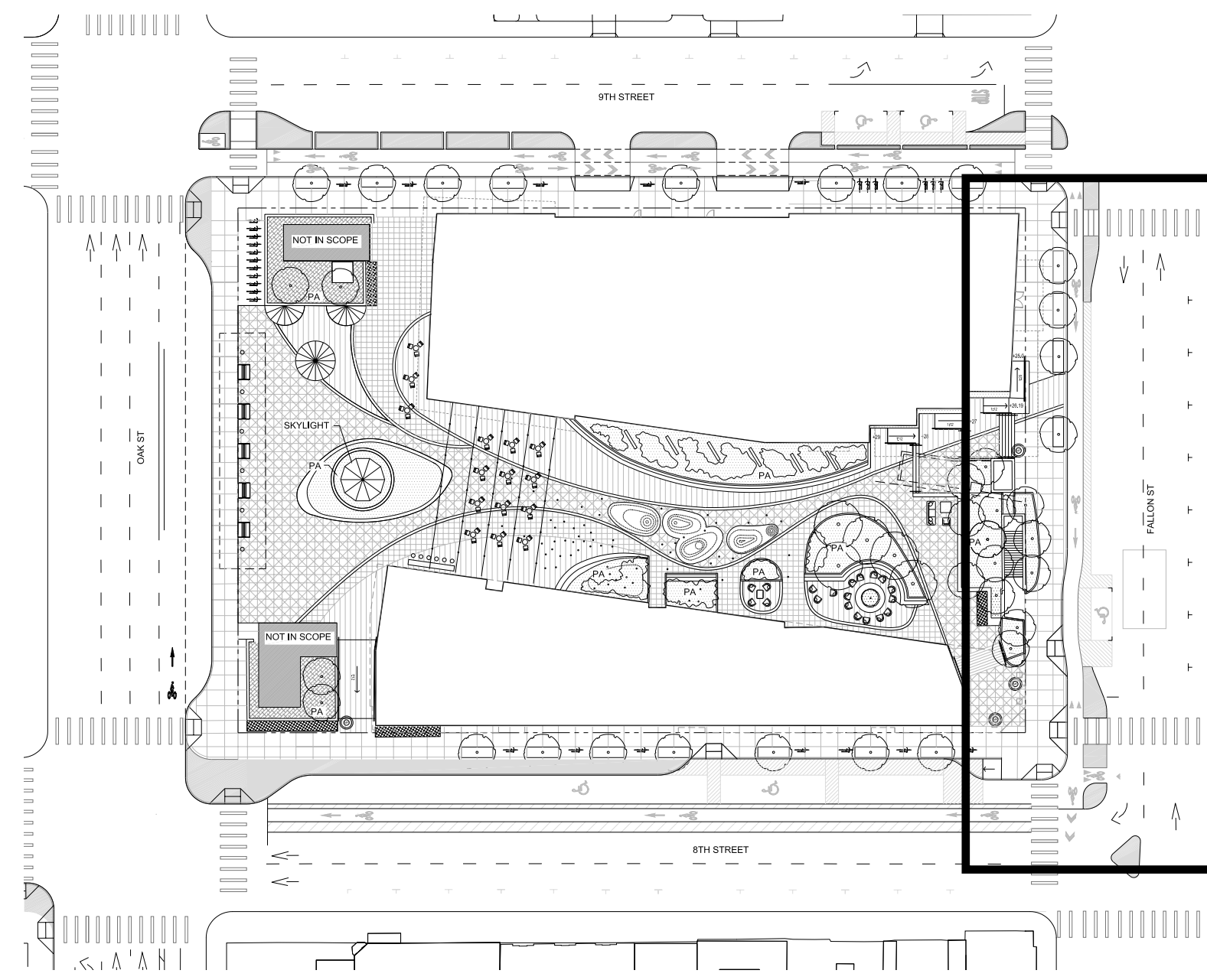
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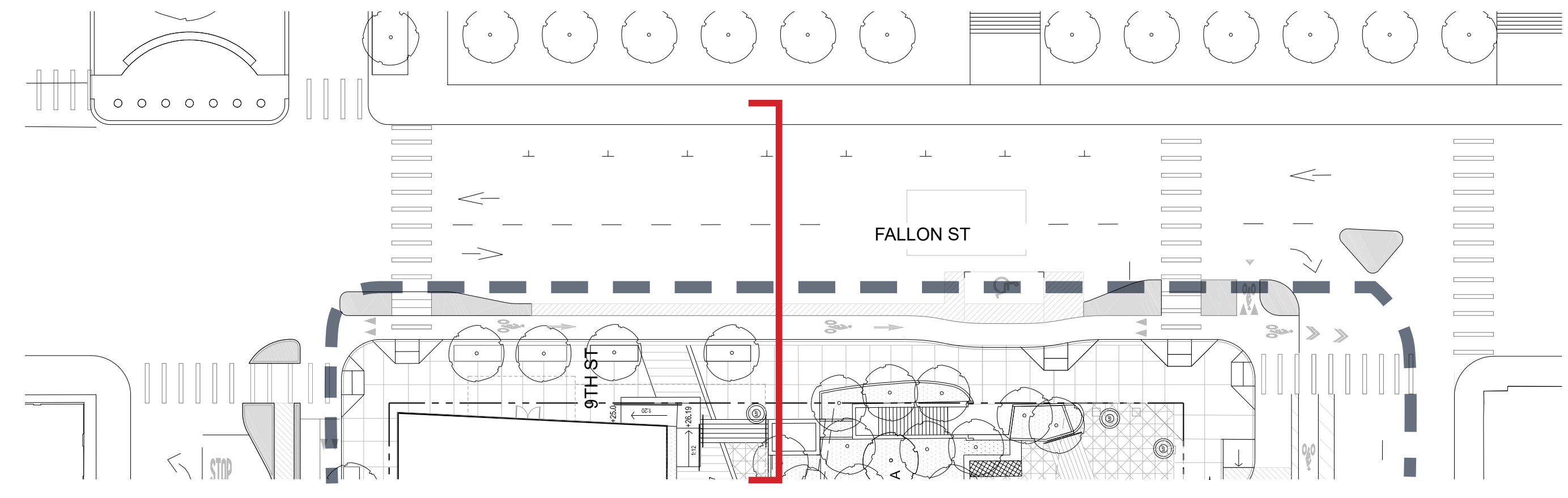
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**BLOCK 1 9TH STREET SECTIONS**





Typical Cross Section



Plan

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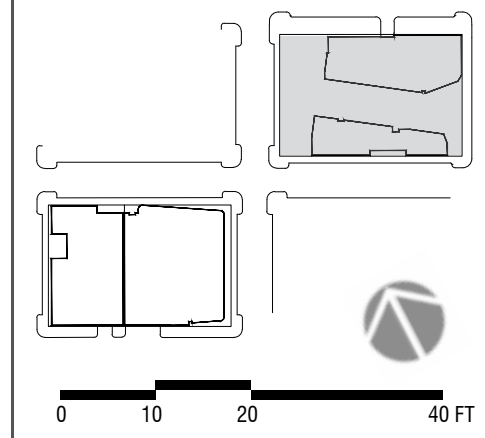
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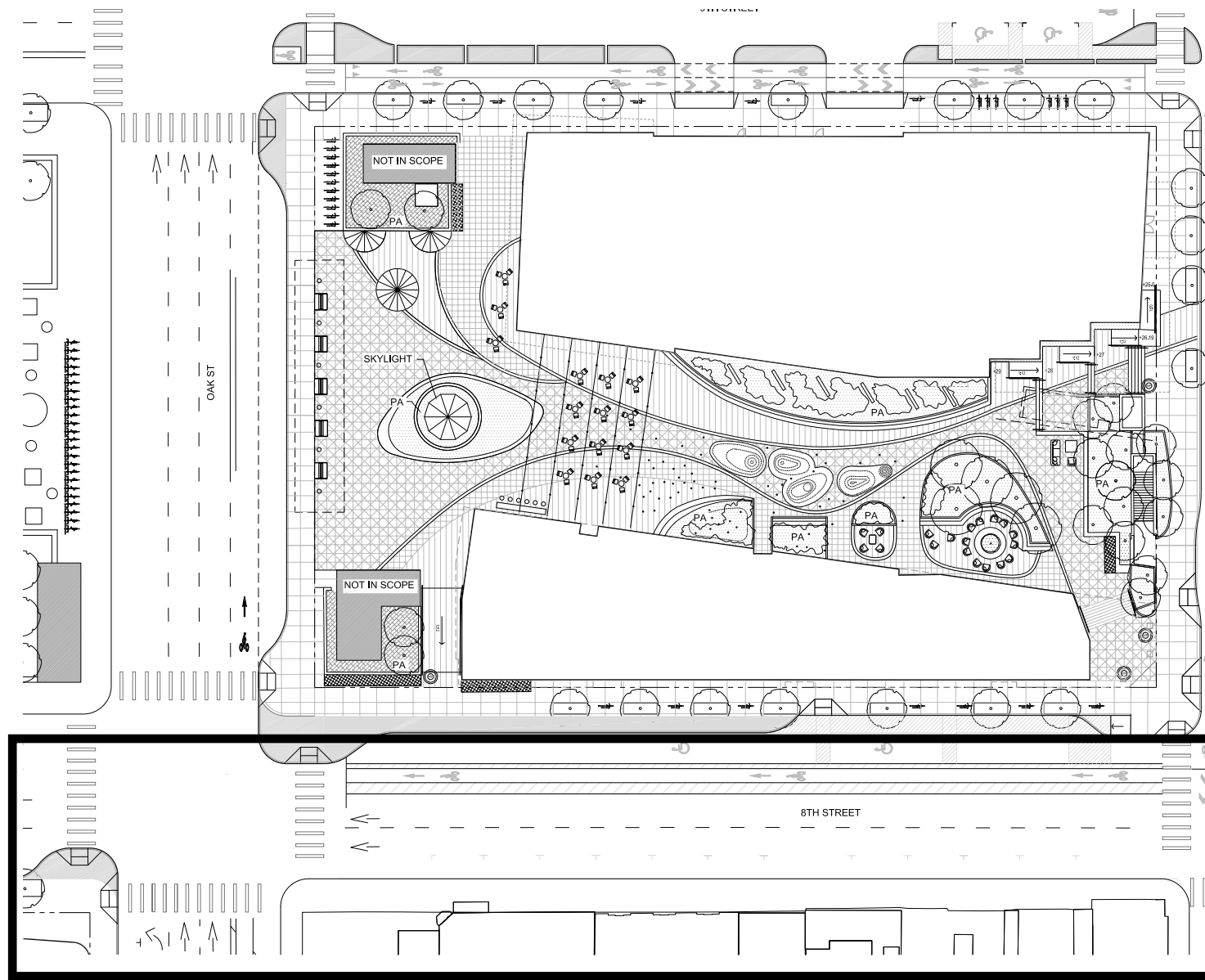
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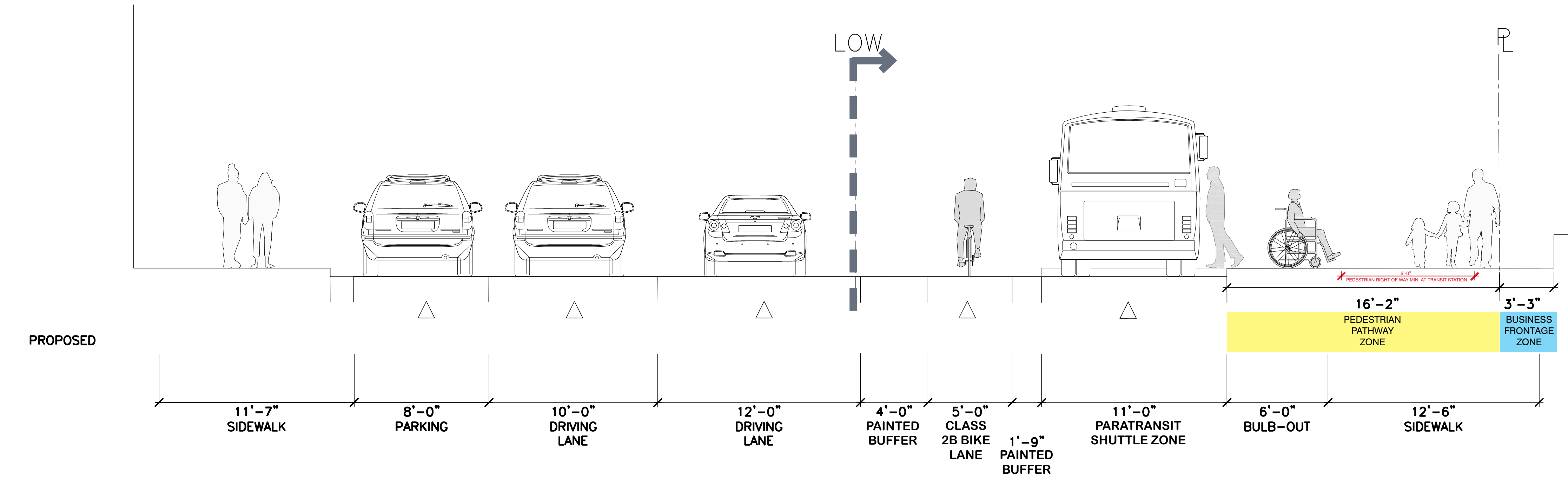
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**BLOCK 1 FALLON STREET SECTIONS**

**L3.3**

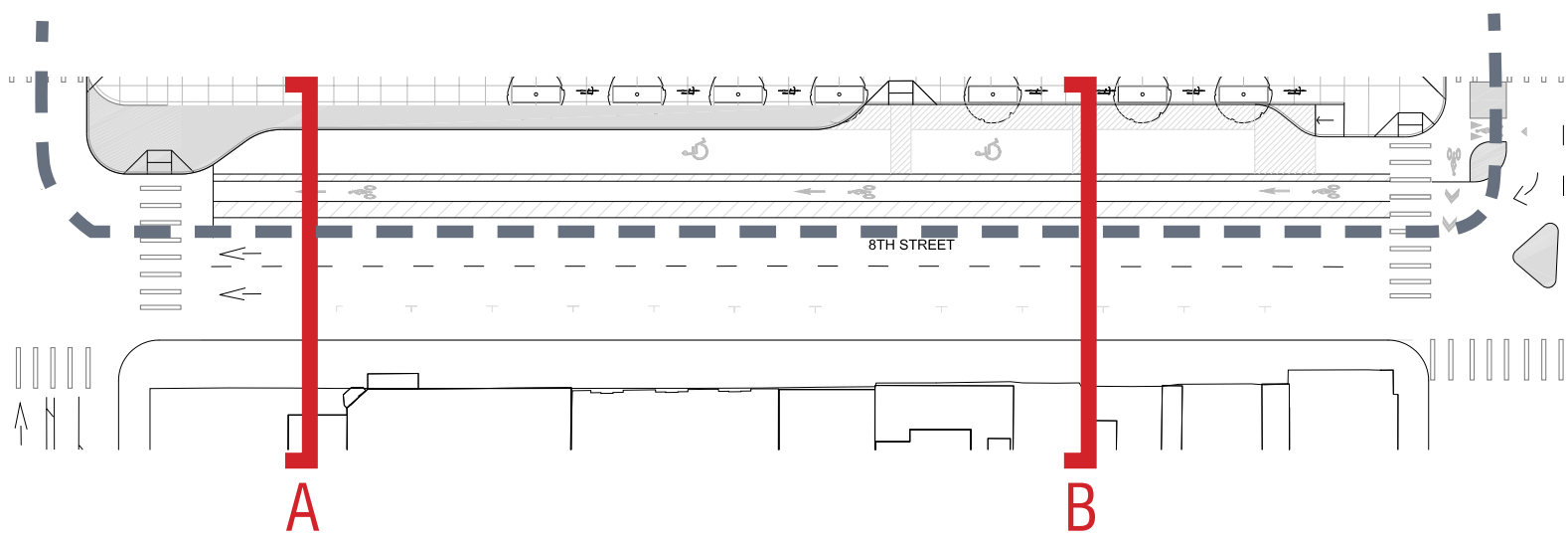




Key Plan



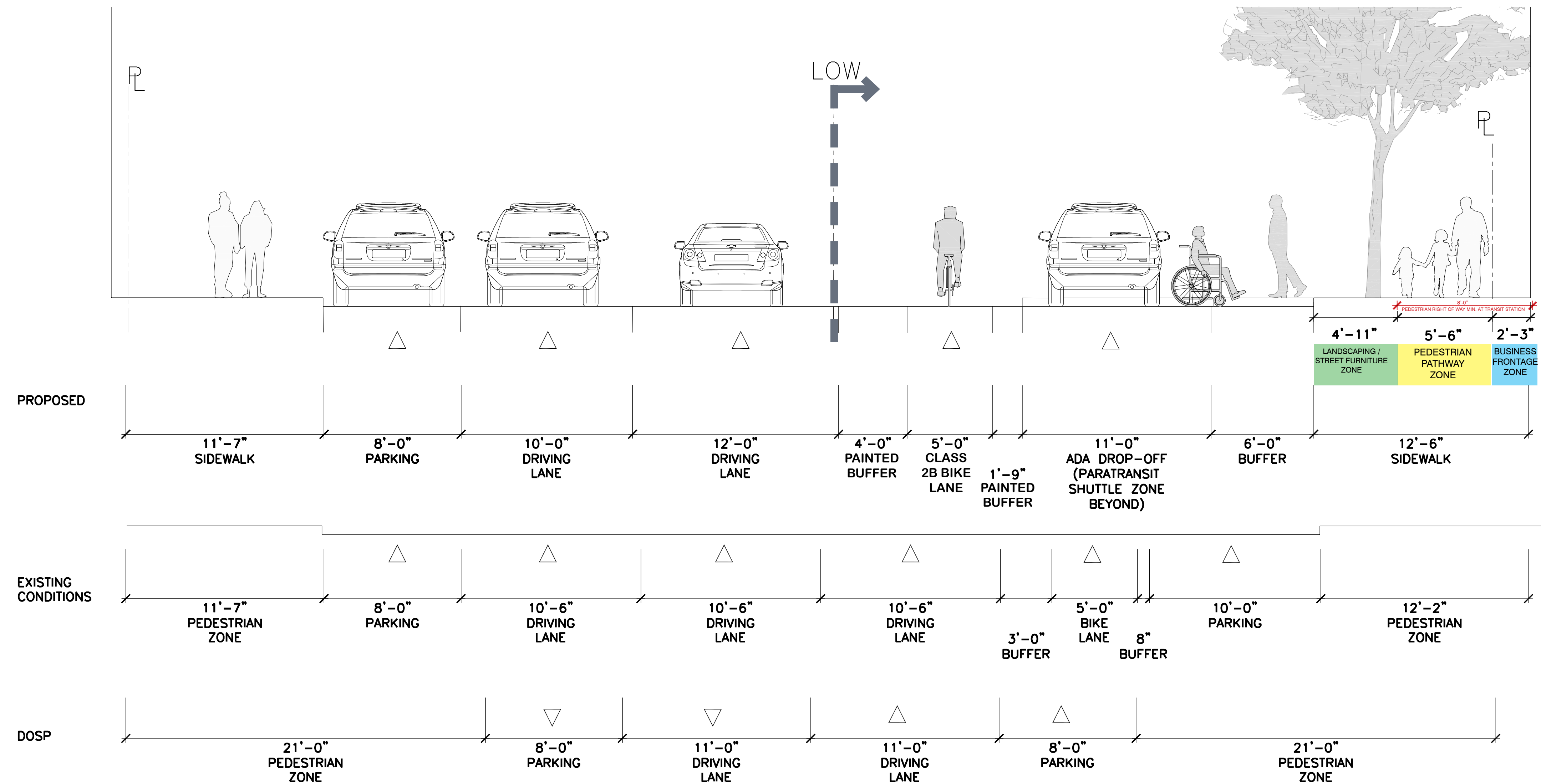
Typical Cross Section: A  
Shuttle Zone



Plan



Note:  
This Street Design Framework was developed to allow the project to progress with building locations and program functions that connect the on site improvements to off site improvements. Current planning documents are not consistent in their recommendations, so the strategy of the framework was to insure flexibility. The proposed option 1 is a potential working solution, but the final street design and dimensions will be the result of a process involving multiple stakeholders and transportation experts. The design team reserves the right to modify and refine these sections in light of new information and the evolving BART Transit Operations Facility (TOF) in the block bordered by 8th St., Madison St., 9th St., and Oak Street. These plans represent a best effort at capturing all information available today in a physical design solution which meets the maximum number of stated criteria for this important public realm. The final design will be ADA compliant.



Typical Cross Section: B  
Not official BART Loading

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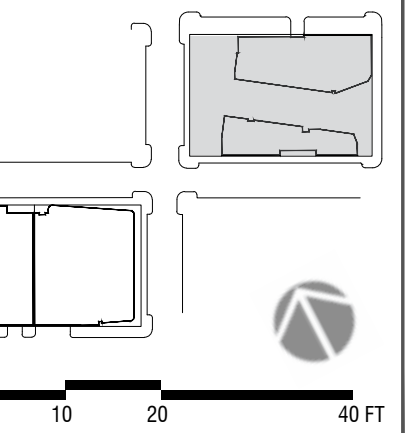
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
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6	REVISED RESPONSE TO POPPIZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPIZ COMMENTS	03/17/2021

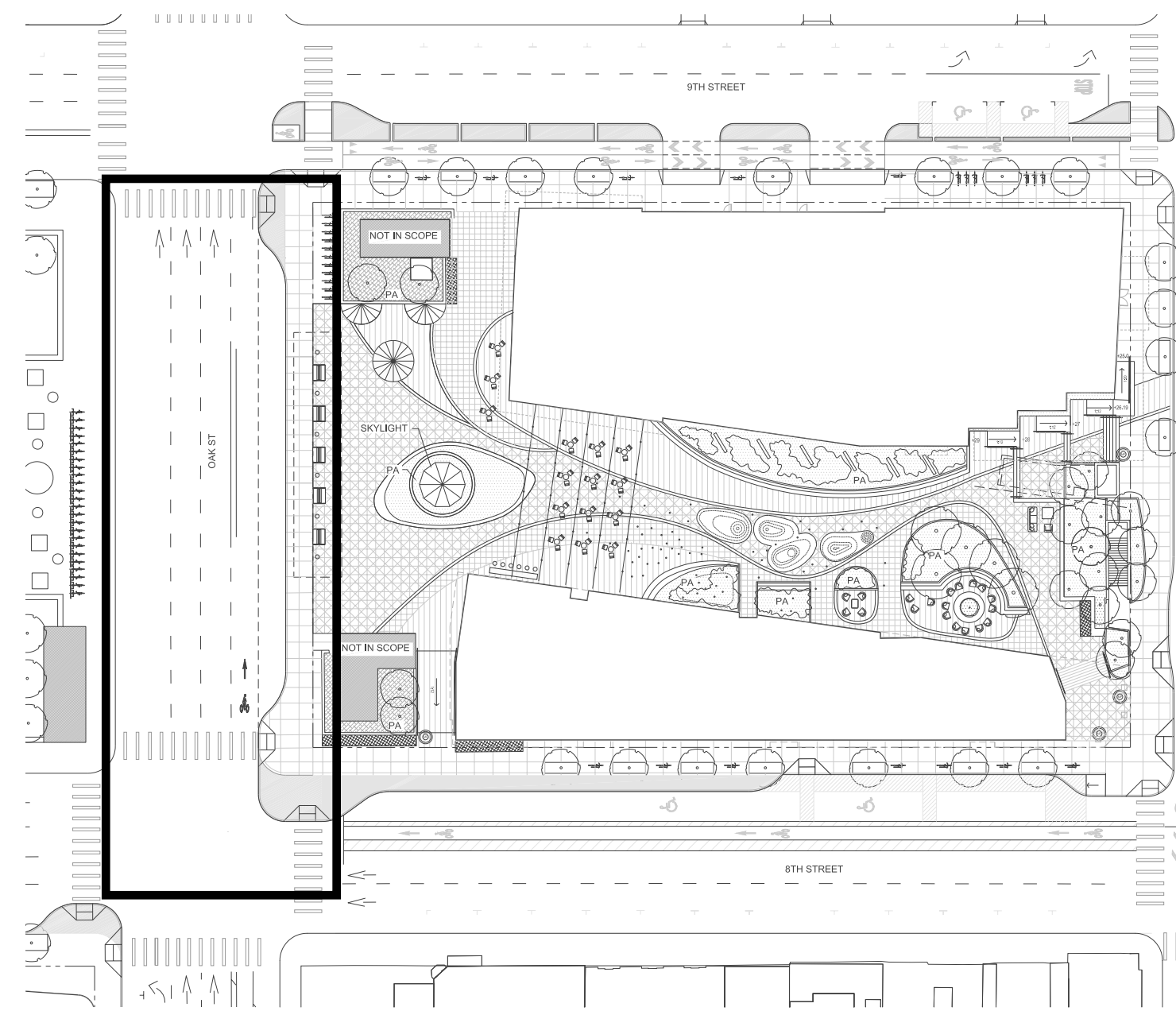


DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

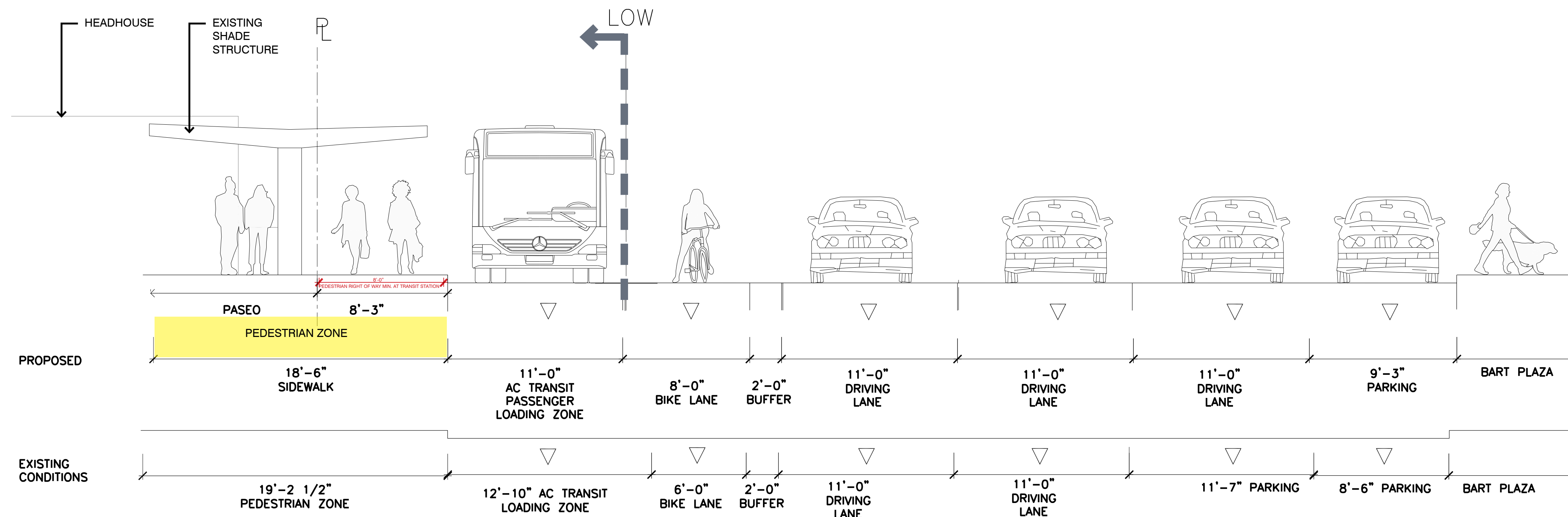
BLOCK 1  
8TH STREET SECTIONS

# L3.4



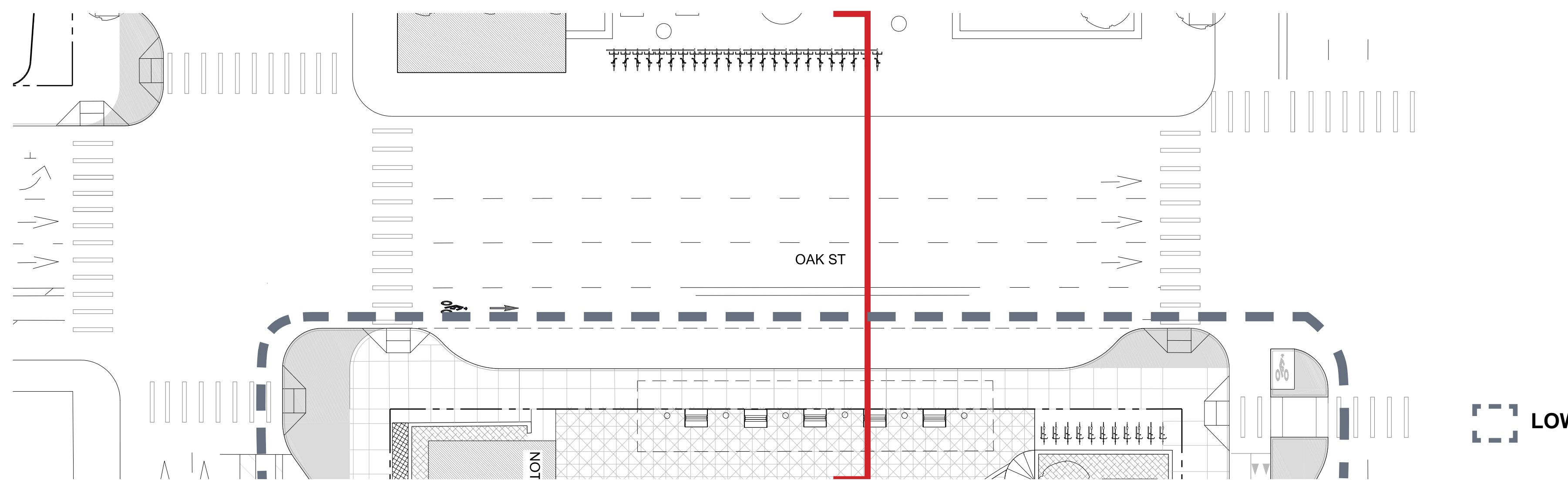


Key Plan



Typical Cross Section

Note:  
Alameda CTC is proposing to construct a two-way bike lane on the west side of Oak, between 3rd and 9th Streets as part of the Oakland-Alameda Access Project. This facility has been planned in conjunction with City of Oakland Planning and OakDot.



Plan

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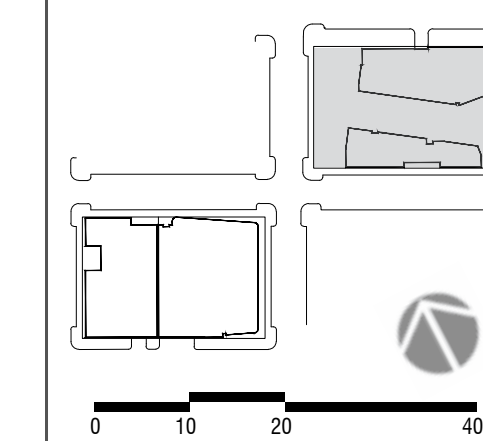
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

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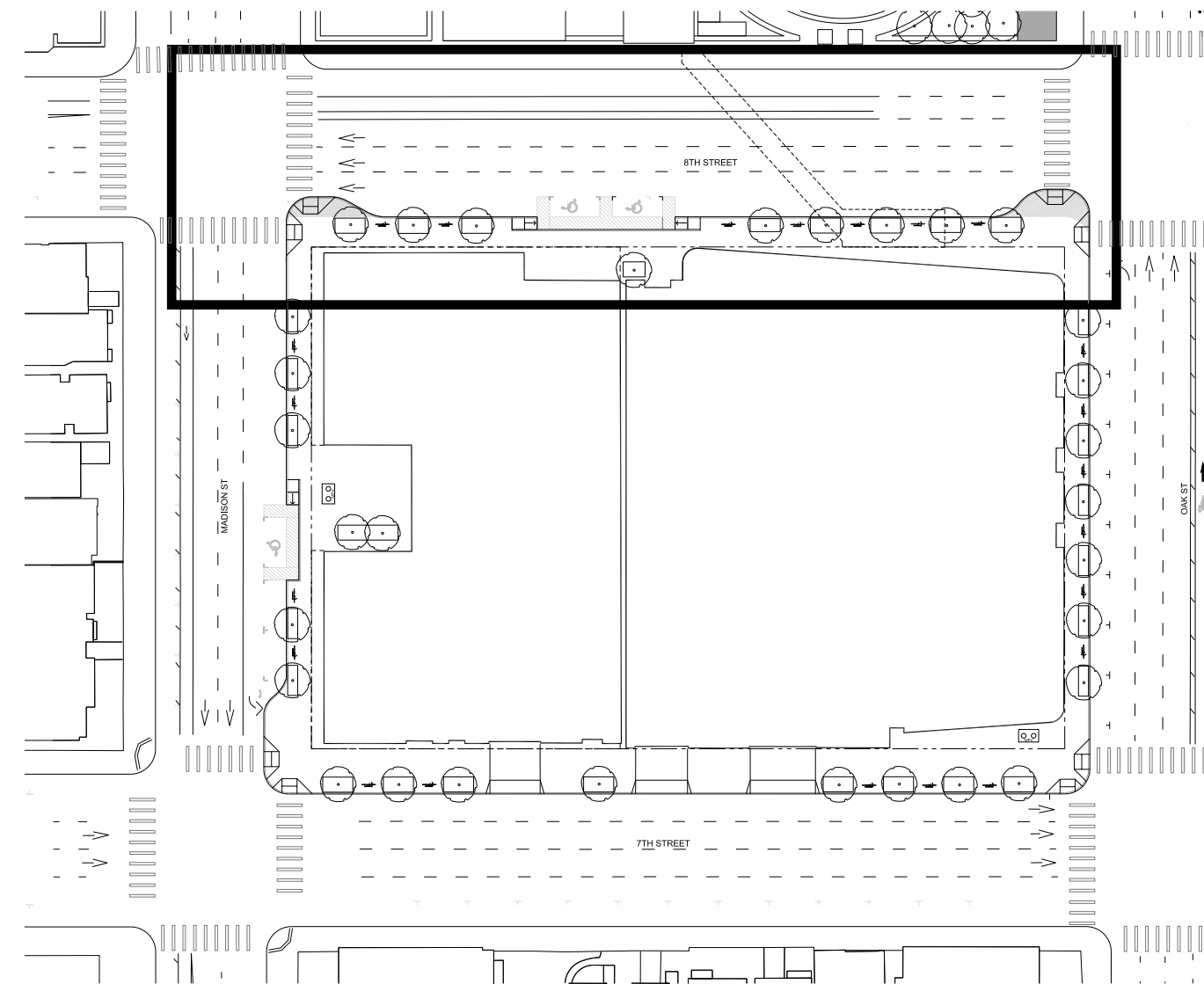


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SCALE: 1" = 20' (VIEWED AT 24" X 36")

### BLOCK 1 OAK STREET SECTIONS

# L3.5

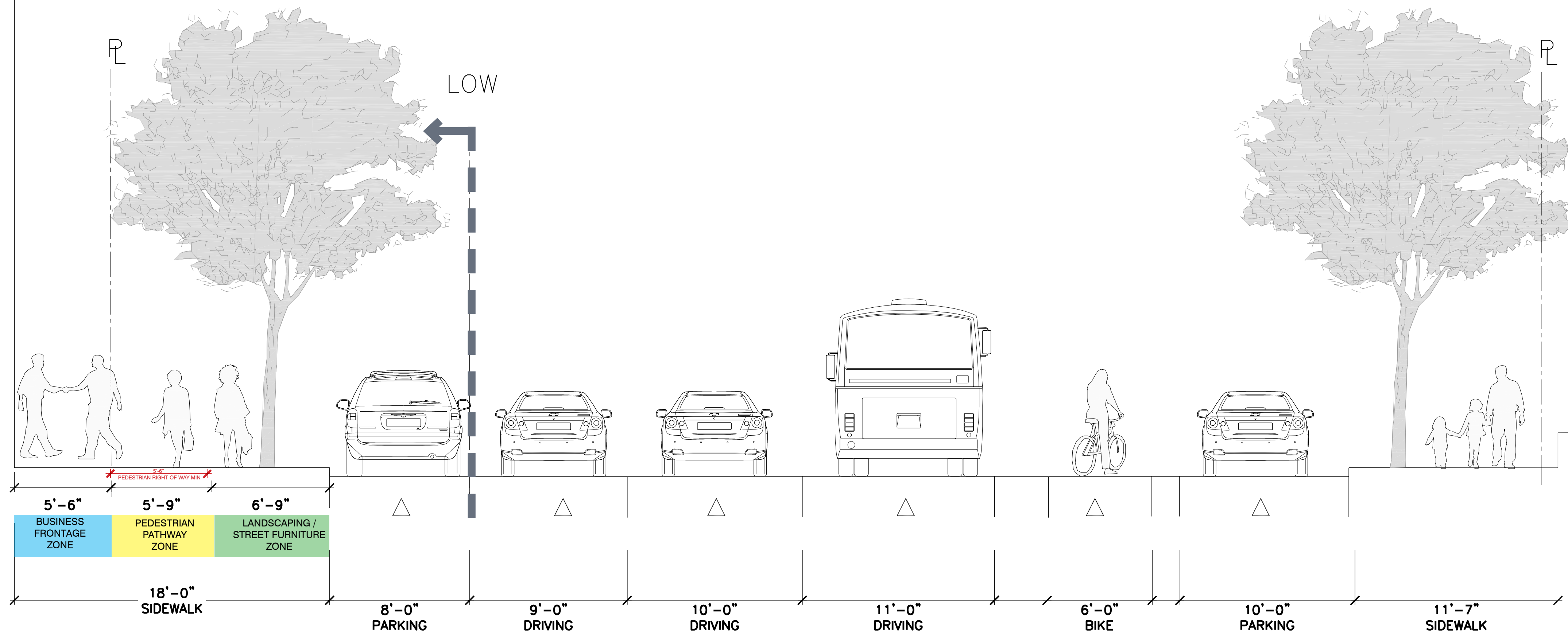




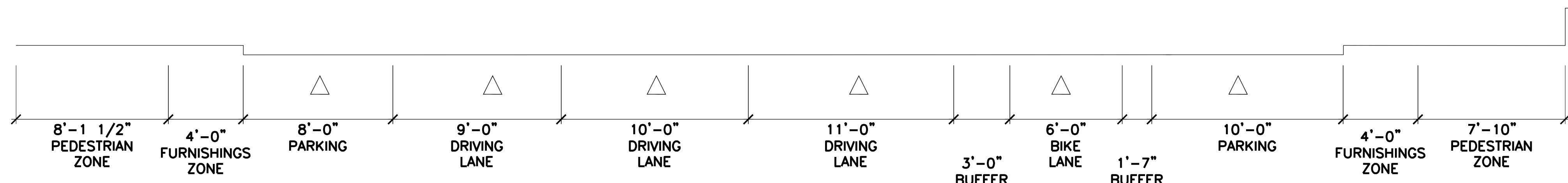
Key Plan



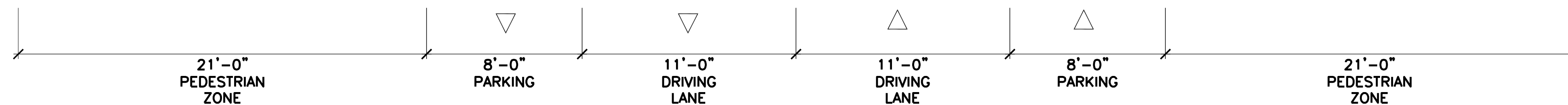
PROPOSED



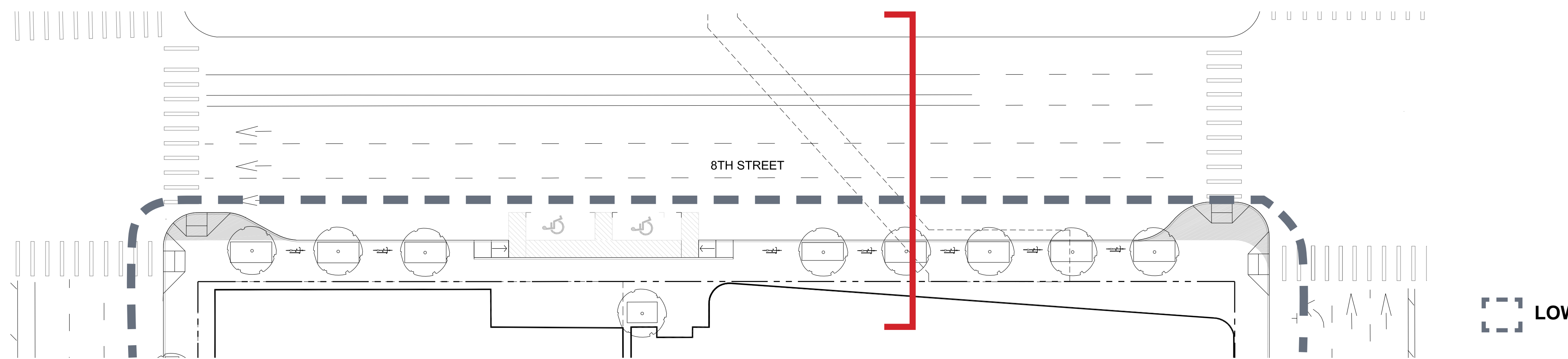
EXISTING CONDITIONS



DOSP



Typical Cross Section



Plan

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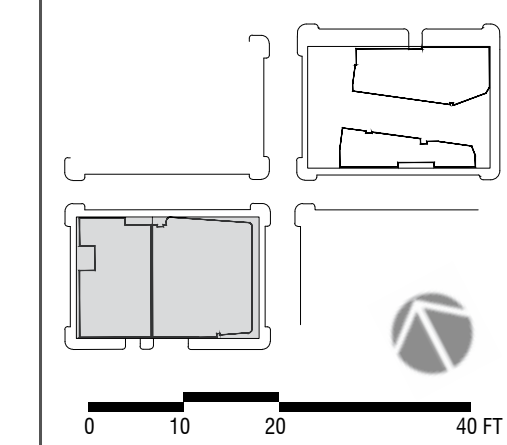
# LAKE MERRITT BART

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## PRELIMINARY DEVELOPMENT PLAN PACKAGE

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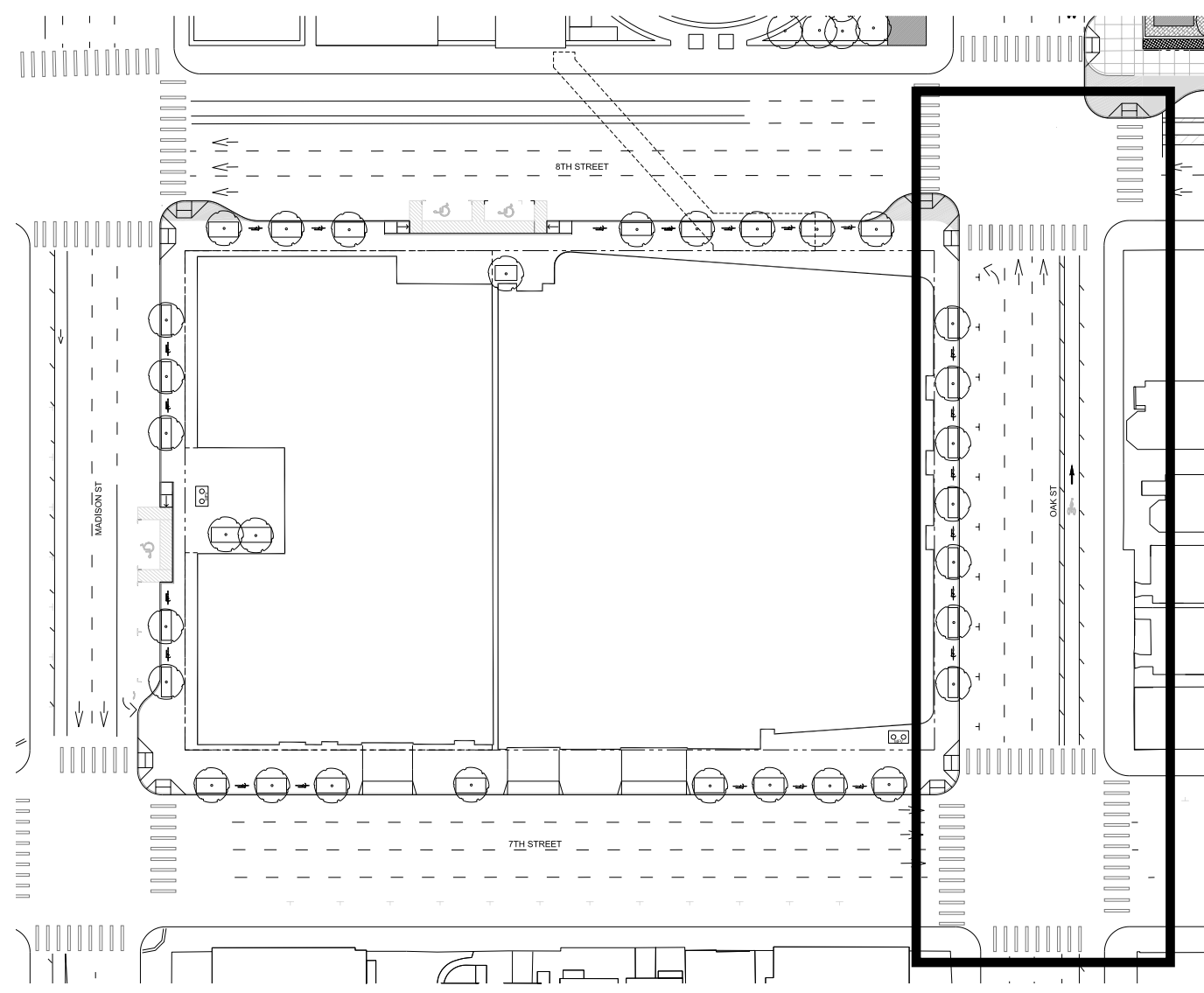
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7	REVISED RESPONSE TO POPPI4 COMMENTS	03/17/2021



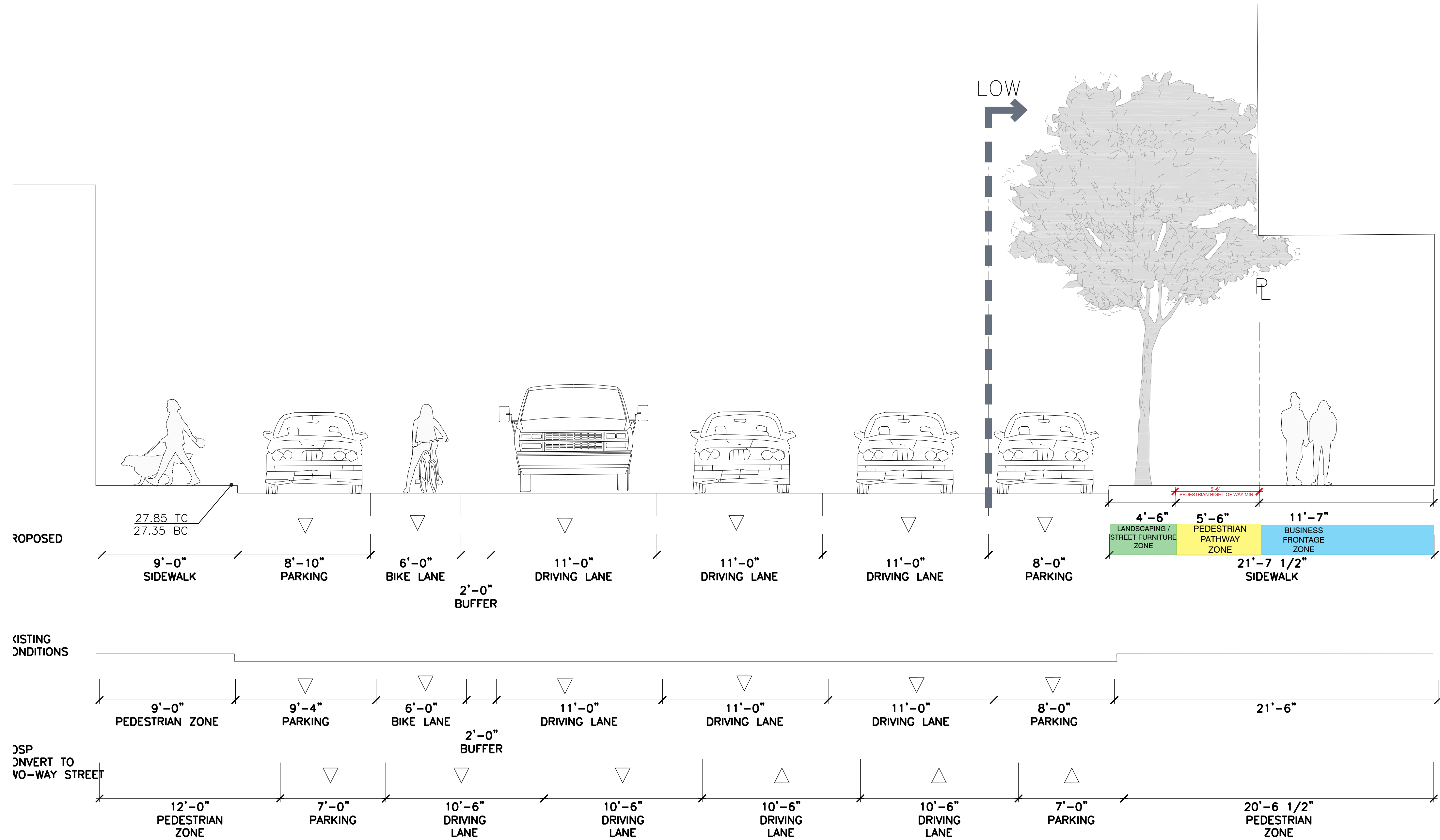
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**BLOCK 2  
8TH STREET  
SECTIONS**

# L3.6

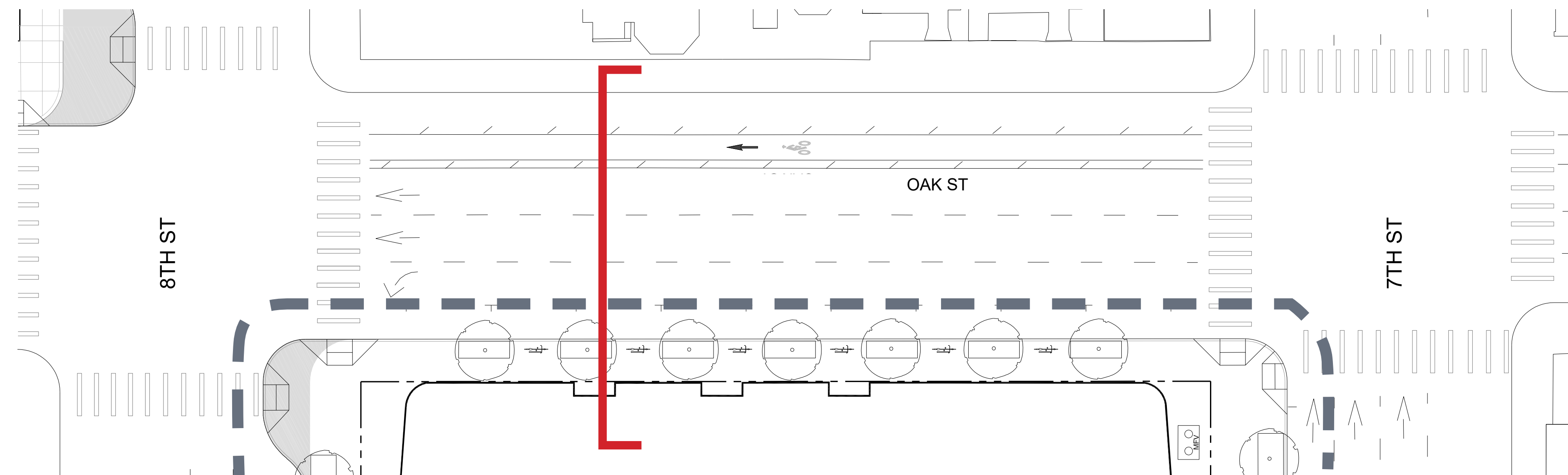




Key Plan



Typical Cross Section



Plan

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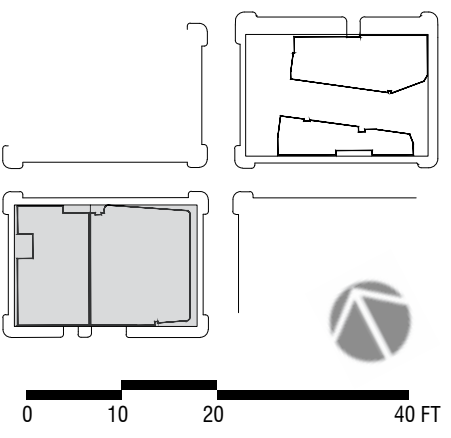
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7	REVISED RESPONSE TO POPF4 COMMENTS	03/17/2021

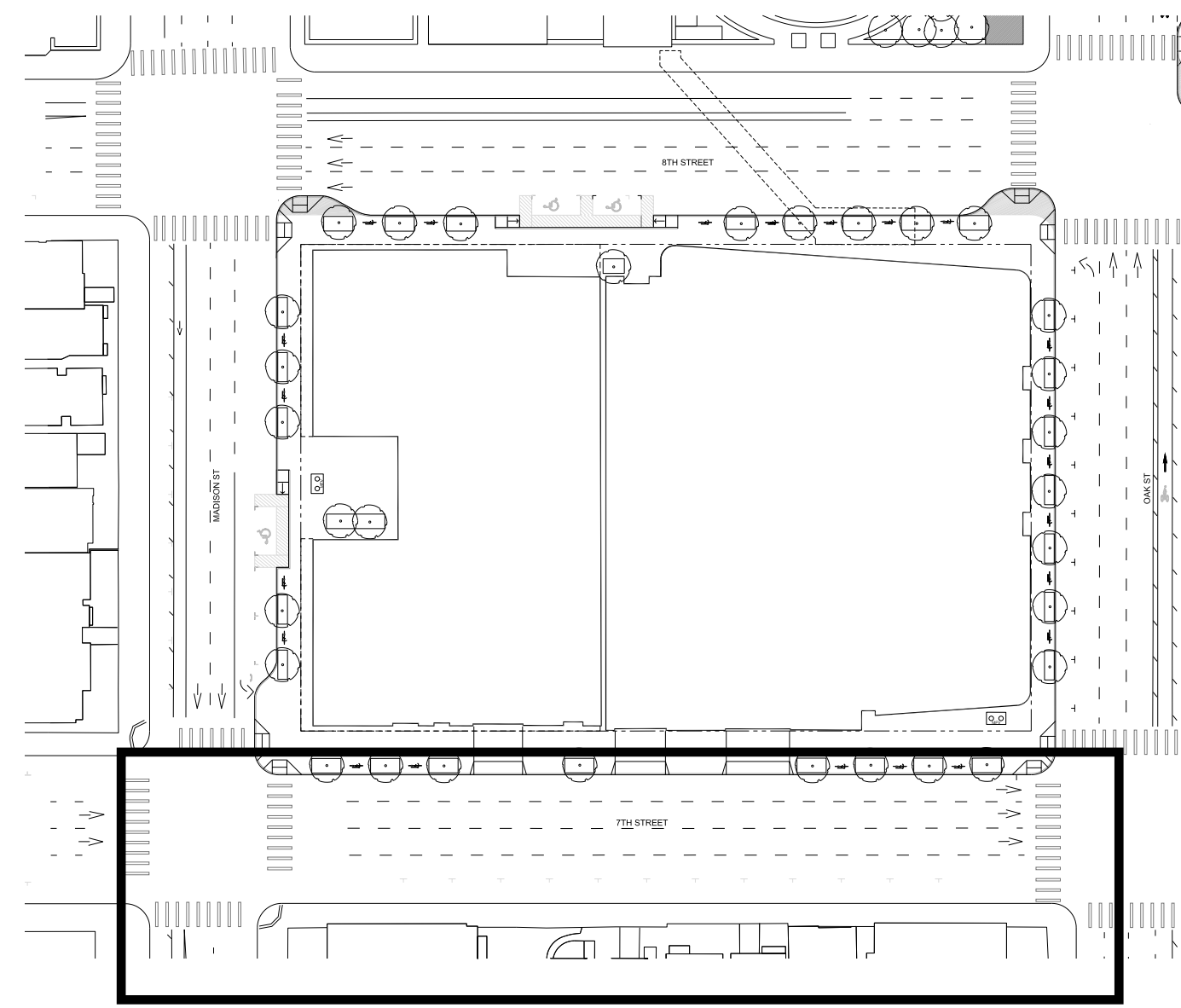


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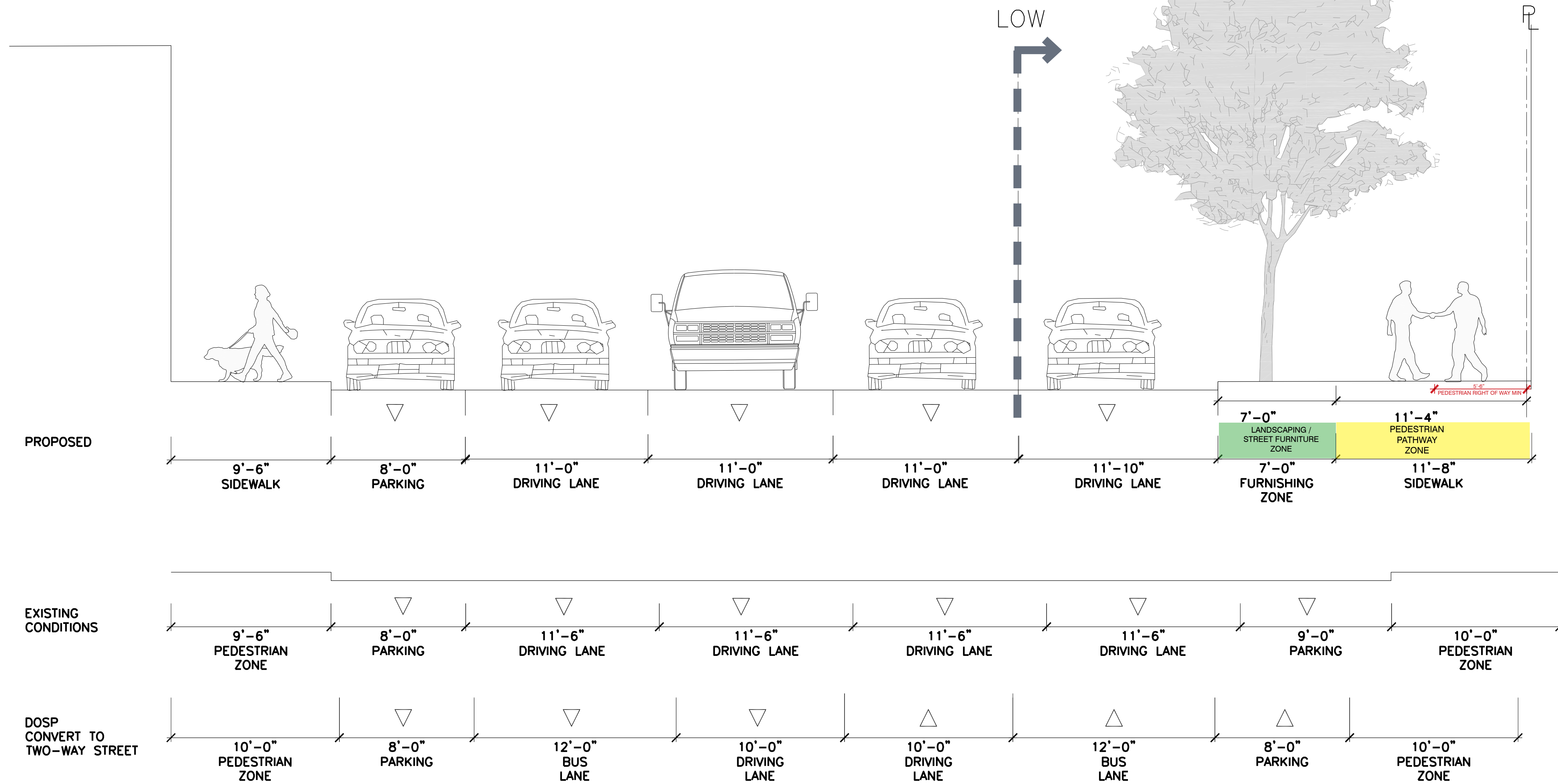
**BLOCK 2 OAK STREET SECTIONS**

**L3.7**

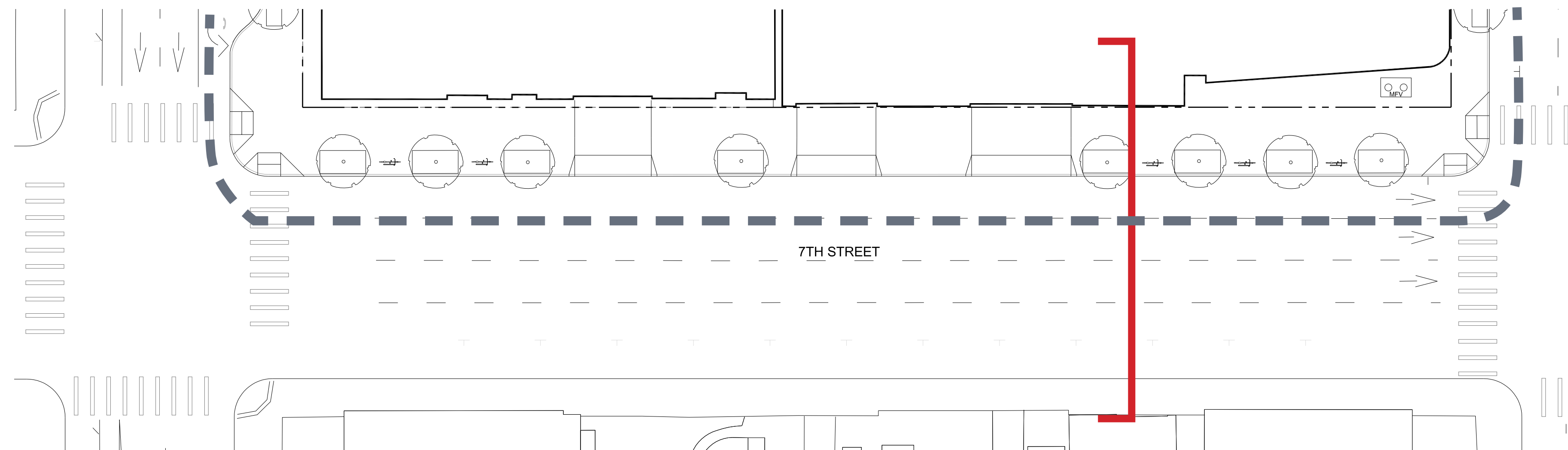




Key Plan



Typical Cross Section



Plan



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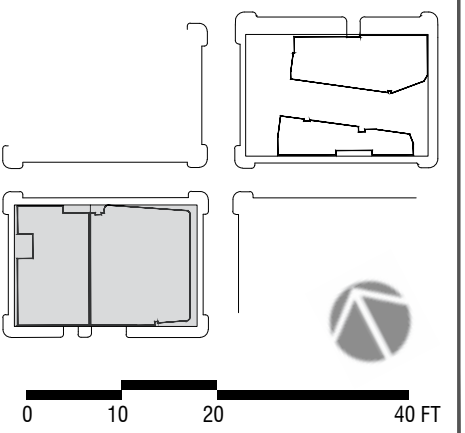
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5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/17/2021

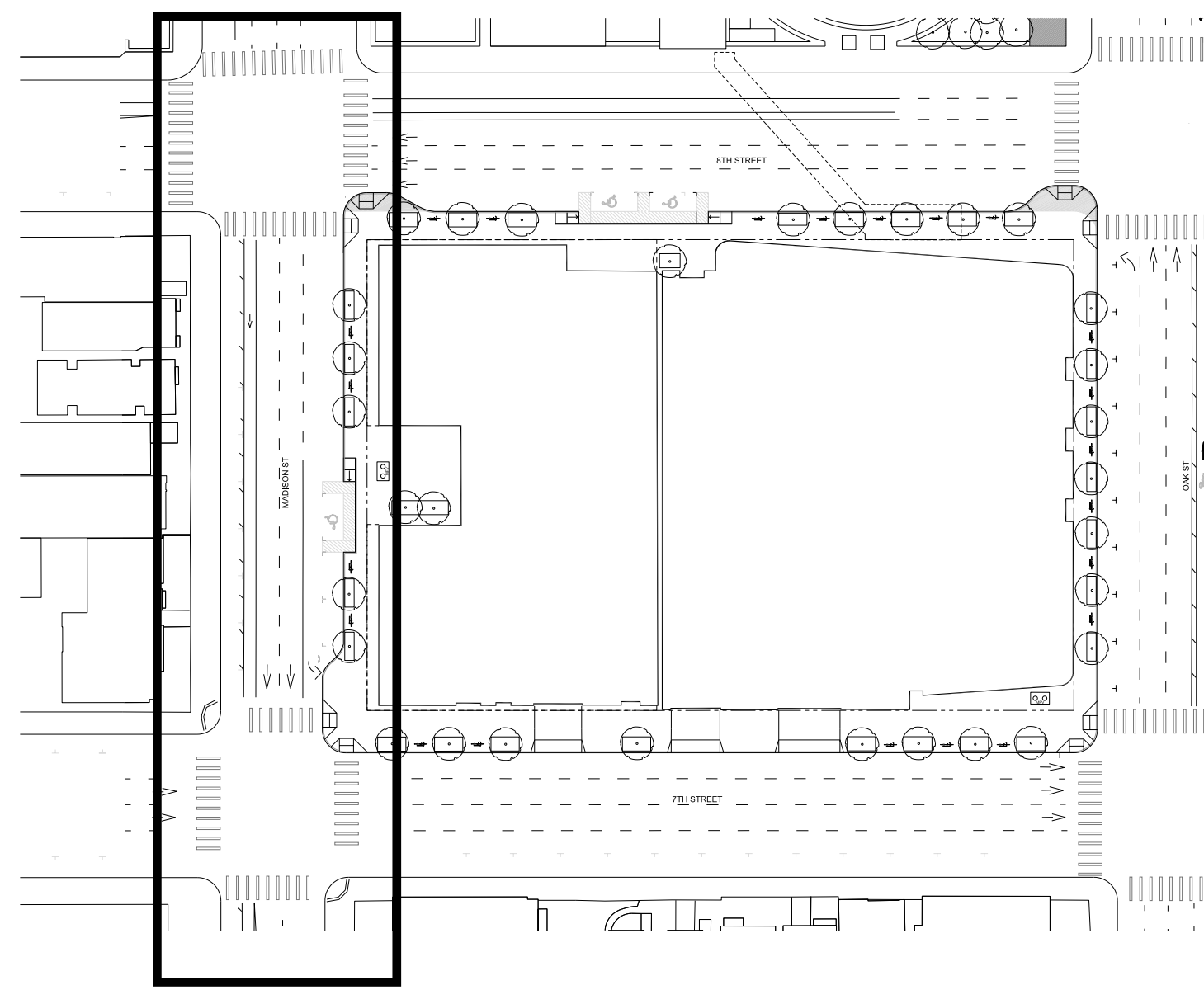


DATE: AUGUST 8, 2019  
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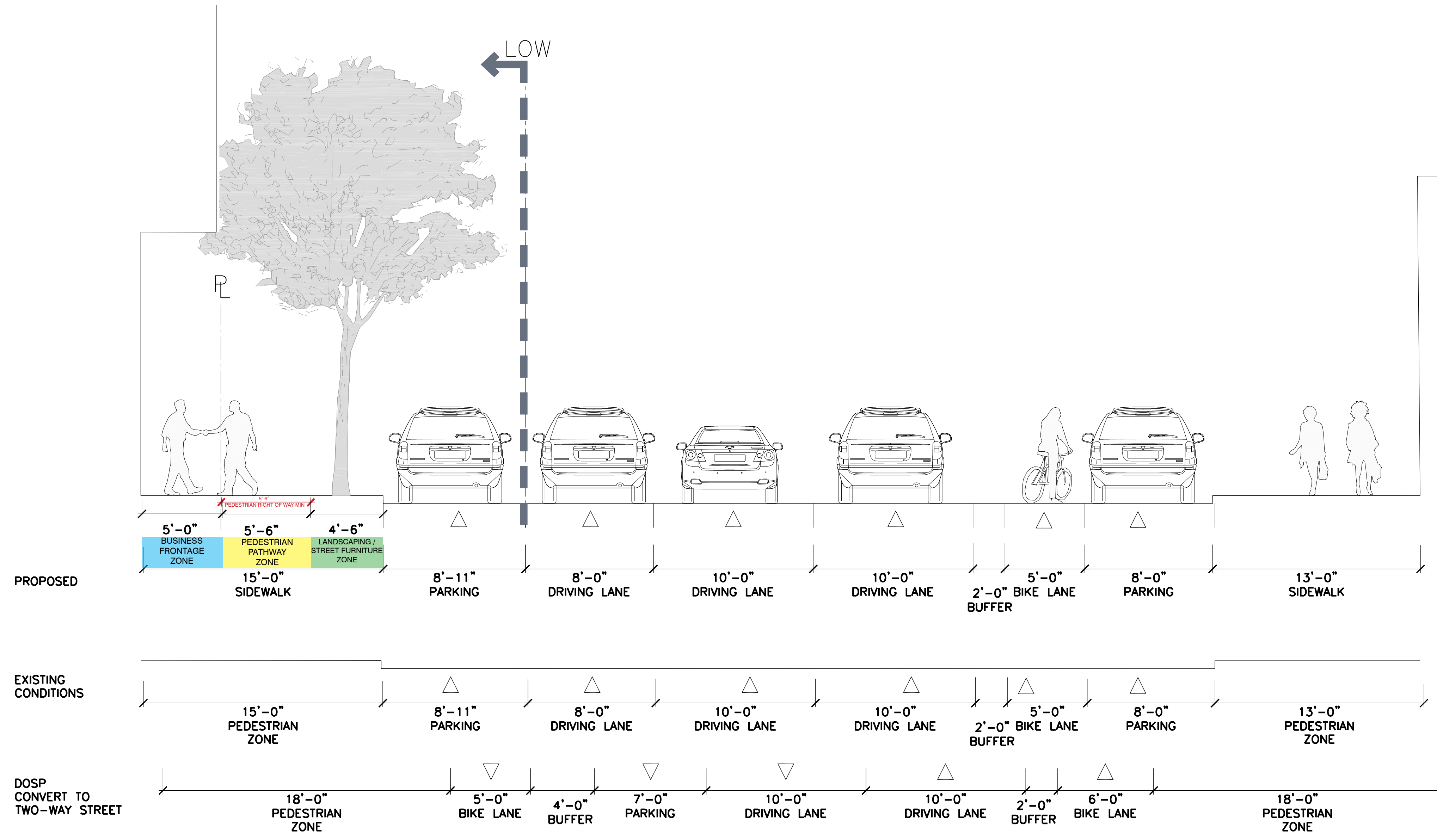
**BLOCK 2 7TH STREET SECTIONS**

**L3.8**

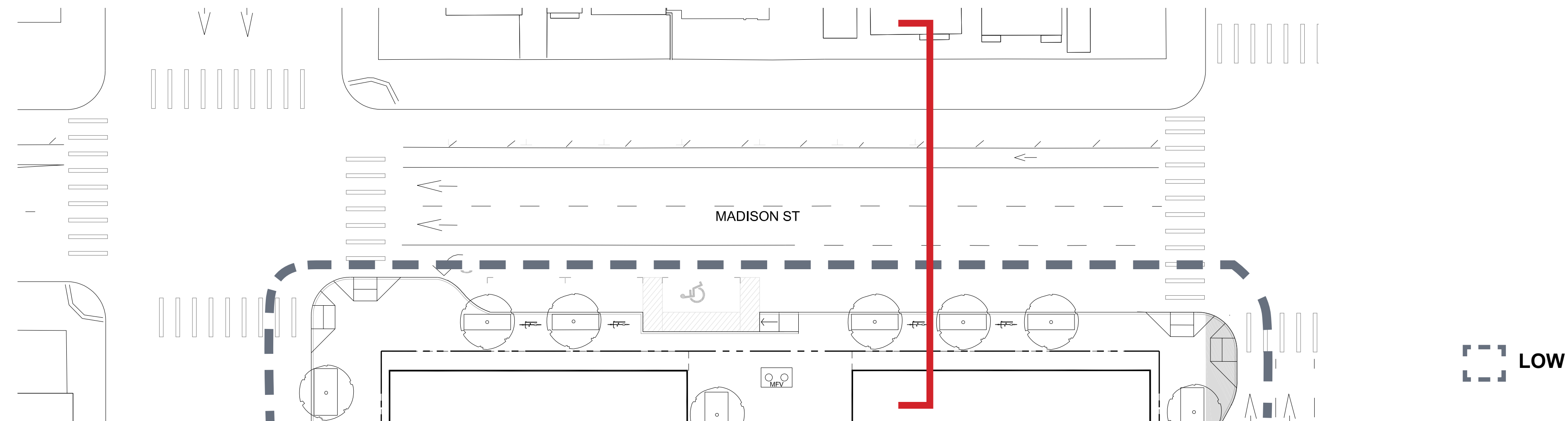




Key Plan



Typical Cross Section



Plan

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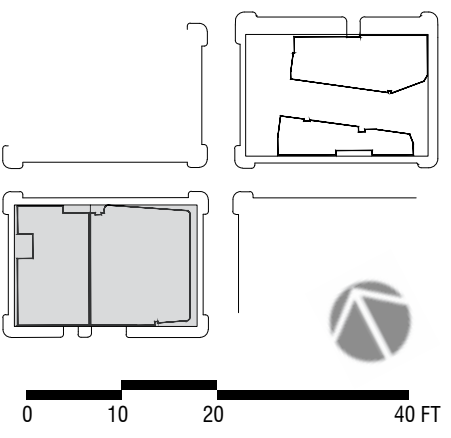
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DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 MADISON STREET SECTIONS**

**L3.9**



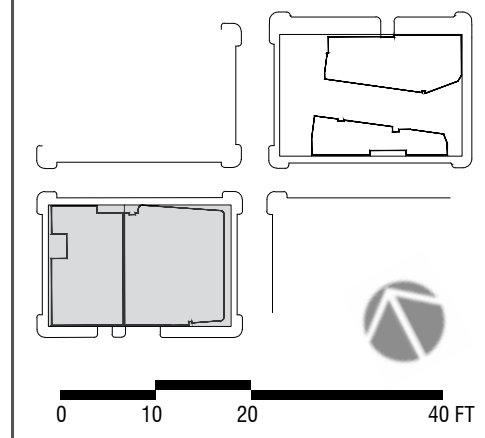
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

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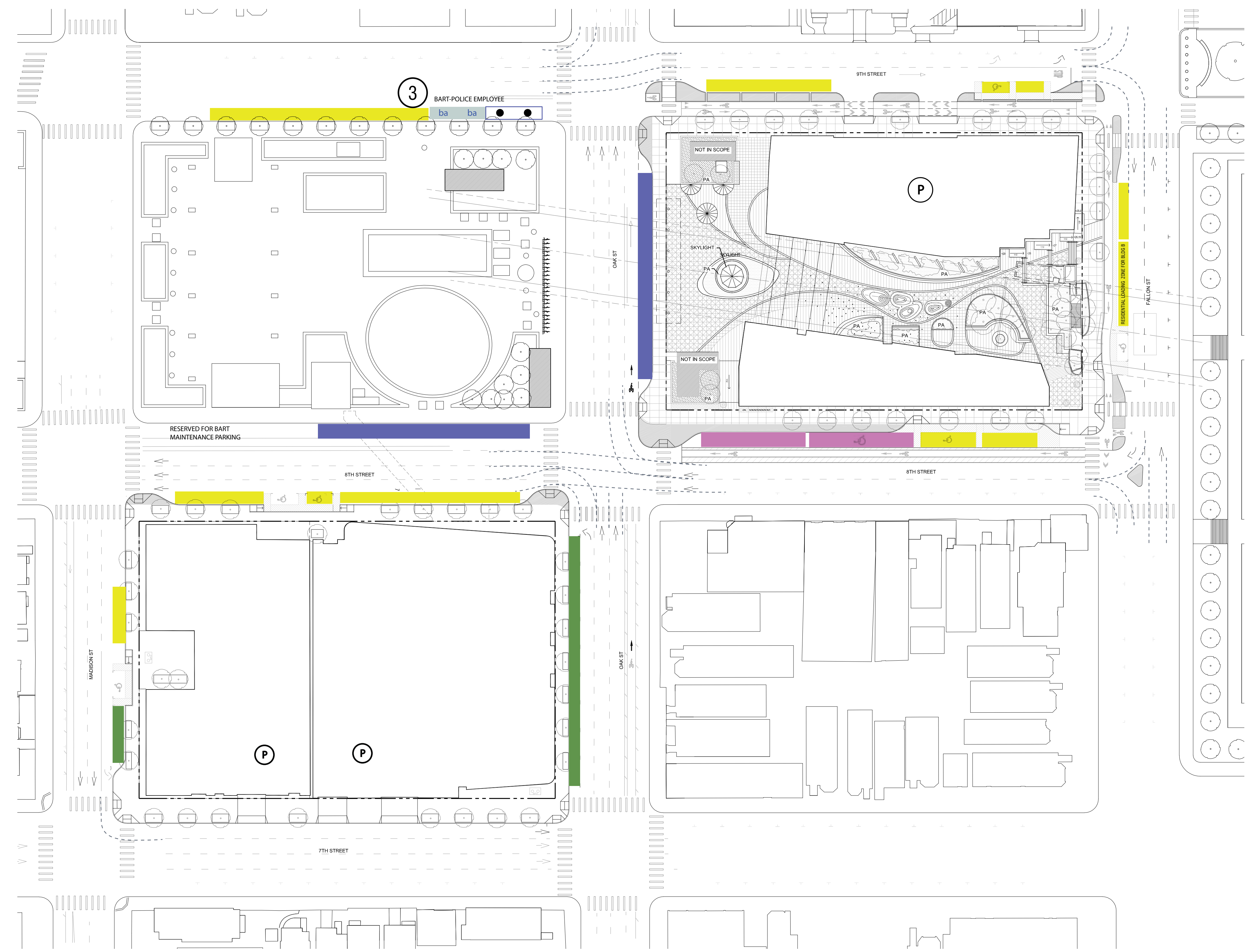
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7	REVISED RESPONSE TO POP14 COMMENTS	03/17/2021



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### 4 BLOCK PUBLIC LOADING AND PARKING

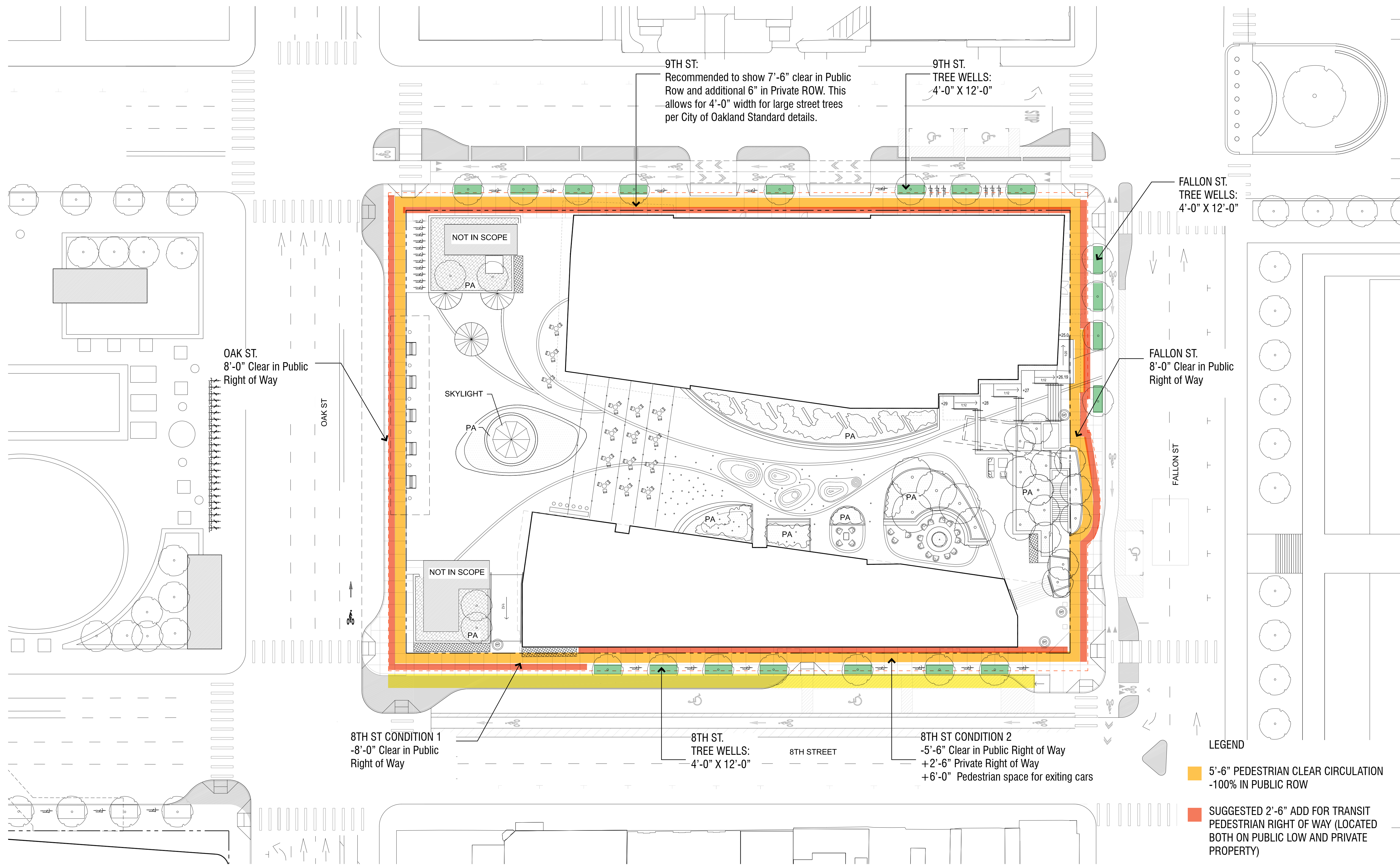
# L4.1



- LEGEND**
- PASSENGER LOADING ZONE
  - ADA PARKING
  - ADA LOADING ZONE
  - BUS/SHUTTLE ZONE
  - ADA BUS/SHUTTLE ZONE
  - AC TRANSIT
  - AUTO PARKING
  - RESERVED FOR BART STATION AGENT PARKING
  - RESERVED FOR POLICE PARKING
  - P PARKING GARAGE
  - LANE TRANSITIONS ON MODIFIED INTERSECTIONS

- NOTES:**
1. Locations shown are diagrammatic based on information at this time.
  2. Transportation planning decisions and phasing will affect ultimate build-out.
  3. BART station agent parking is subject to BART and City coordination.
  4. The final design will be ADA compliant.





9TH ST.  
Recommended to show 7'-6" clear in Public Row and additional 6" in Private ROW. This allows for 4'-0" width for large street trees per City of Oakland Standard details.

9TH ST.  
TREE WELLS:  
4'-0" X 12'-0"

FALLON ST.  
TREE WELLS:  
4'-0" X 12'-0"

FALLON ST.  
8'-0" Clear in Public Right of Way

OAK ST.  
8'-0" Clear in Public Right of Way

8TH ST CONDITION 1  
-8'-0" Clear in Public Right of Way

8TH ST.  
TREE WELLS:  
4'-0" X 12'-0"

8TH ST CONDITION 2  
-5'-6" Clear in Public Right of Way  
+ 2'-6" Private Right of Way  
+ 6'-0" Pedestrian space for exiting cars

LEGEND

- 5'-6" PEDESTRIAN CLEAR CIRCULATION -100% IN PUBLIC ROW
- SUGGESTED 2'-6" ADD FOR TRANSIT PEDESTRIAN RIGHT OF WAY (LOCATED BOTH ON PUBLIC LOW AND PRIVATE PROPERTY)
- ADDITIONAL CLEAR PED MOVEMENT AREAS
- 8'-0" OFFSET FROM PROPERTY LINE
- PROPERTY LINE
- 12' x 4' TREE WELLS MEETS MINIMUM STANDARD FOR A LARGE TREE.

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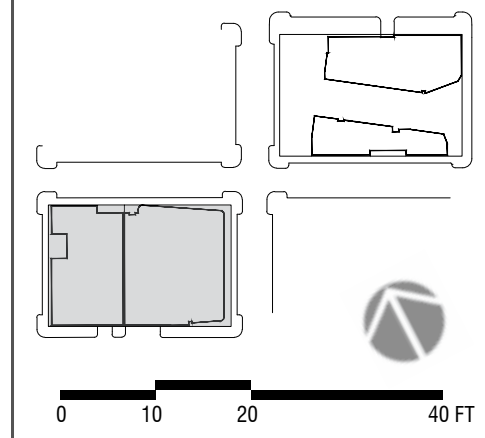
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7	REVISED RESPONSE TO POP14 COMMENTS	03/17/2021

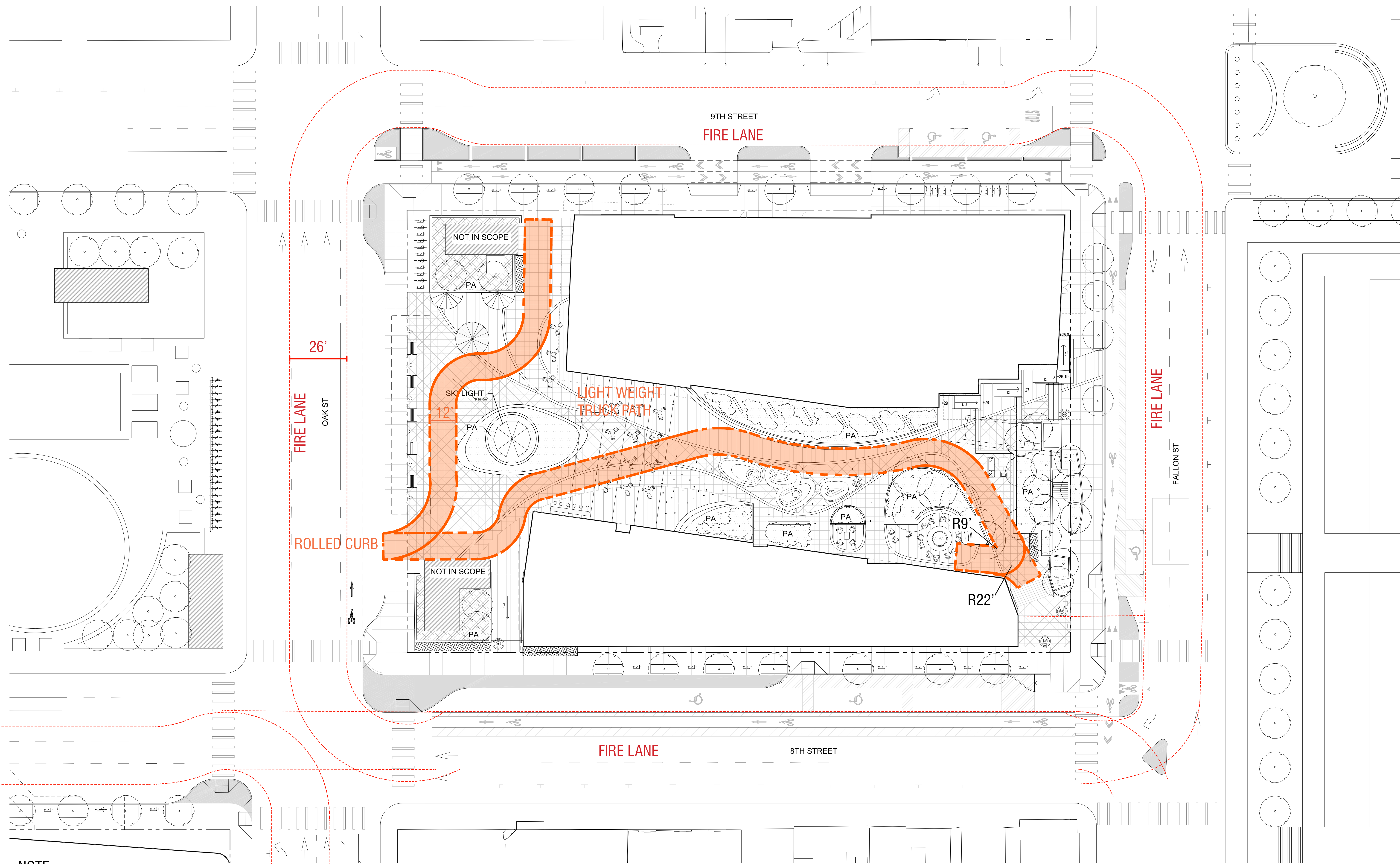


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**BLOCK 1 PEDESTRIAN RIGHT OF WAY**

**L4.2**





**NOTE:**

1. Security cameras will be installed in the West Plaza and Paseo.
2. Final design of paseo under study.
3. Truck path inner radius minimum is 18'-0", location diagrammatic
4. Separation between maintenance path and play structure to be considered.
5. BART station agent parking will be located to the satisfaction of the city and BART
6. Location of Public Art, Moveable Furniture, and Paseo hardscape improvements to be approved by BART at the FDP to meet their requirements for a light-weight service maintenance vehicle

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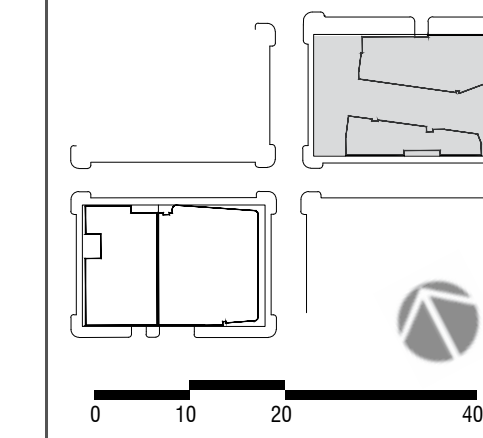
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Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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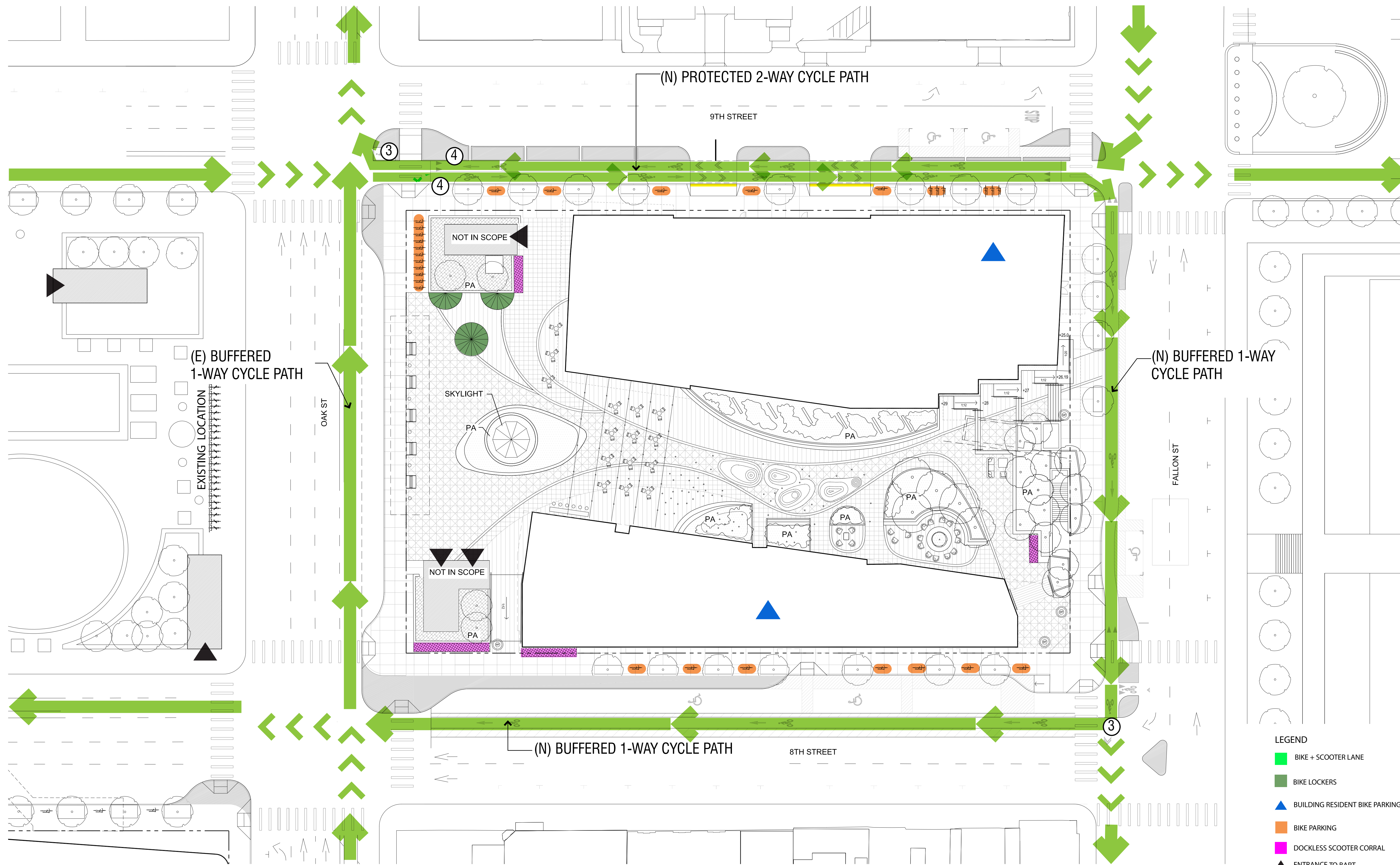
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NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
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5	REVISED RESPONSE TO POPF COMMENTS	10/02/2020
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7	REVISED RESPONSE TO POPF COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 BART MAINTENANCE AND SECURITY**





- NOTES:**
- ① Locations shown are diagrammatic based on information at this time.
  - ② Transportation planning decisions and phasing will affect ultimate build-out.
  - ③ Directional bike transitions are coordinating with city wide planning and subject to change. See Access plans for planning purposes.
  - ④ 1'-0" detectable edge at two-way protected bike lane.

- LEGEND**
- █ BIKE + SCOOTER LANE
  - █ BIKE LOCKERS
  - ▲ BUILDING RESIDENT BIKE PARKING
  - █ BIKE PARKING
  - █ DOCKLESS SCOOTER CORRAL
  - ▲ ENTRANCE TO BART
  - █ 1'-0" DETECTABLE EDGES

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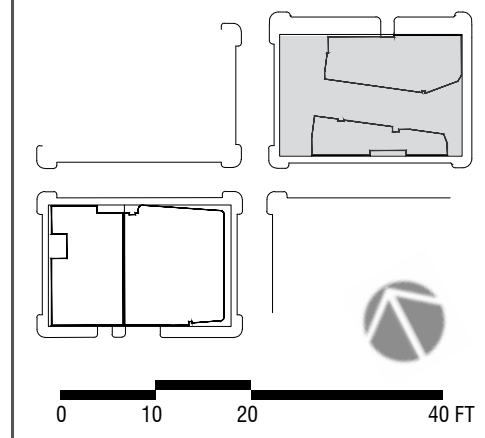
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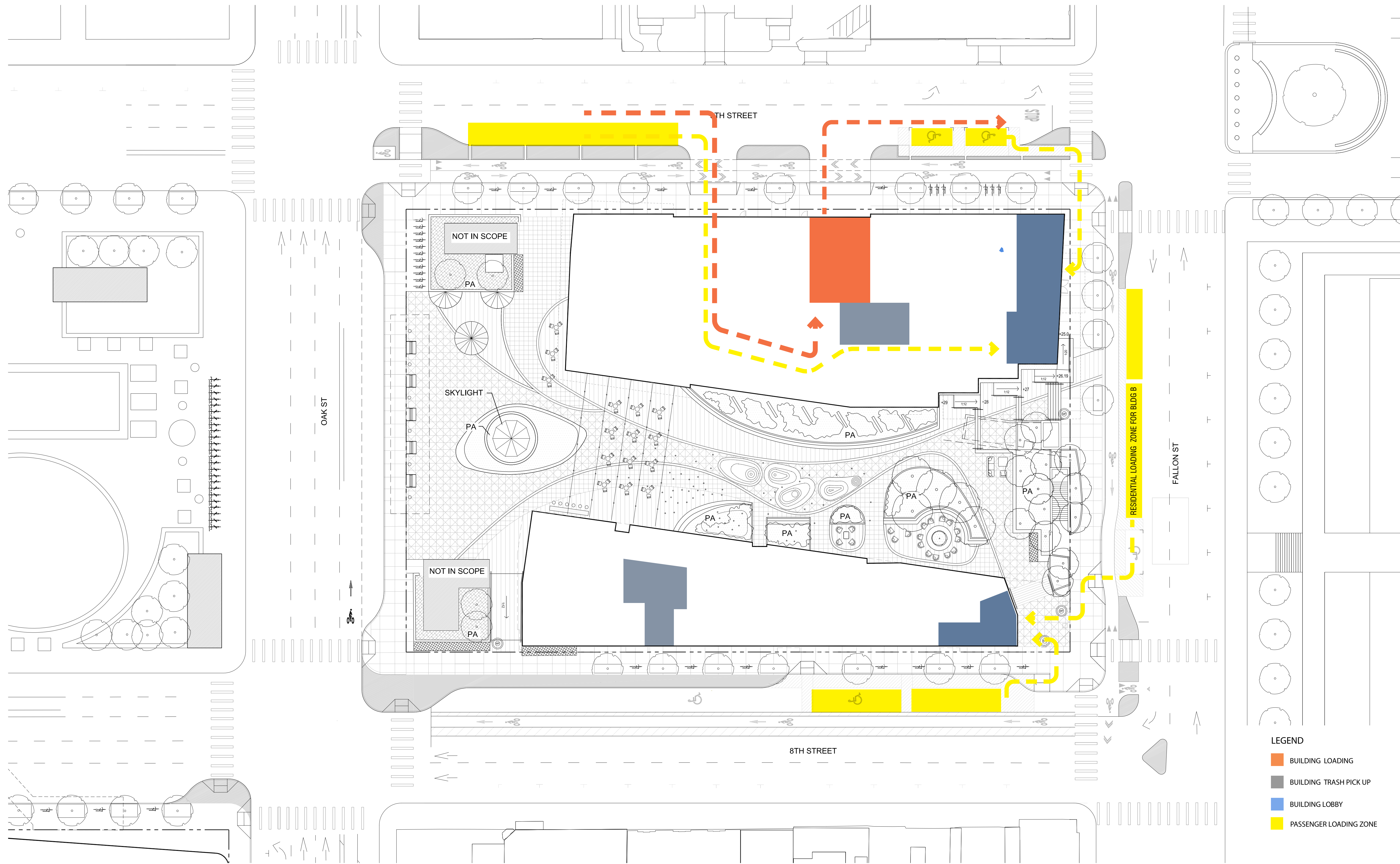
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**BLOCK 1 BICYCLE AND SCOOTER ACCESS**





**NOTES:**

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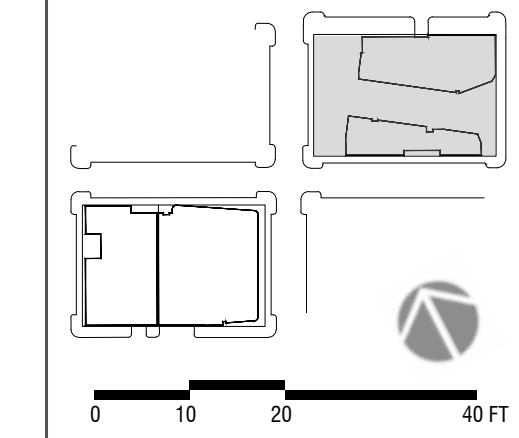
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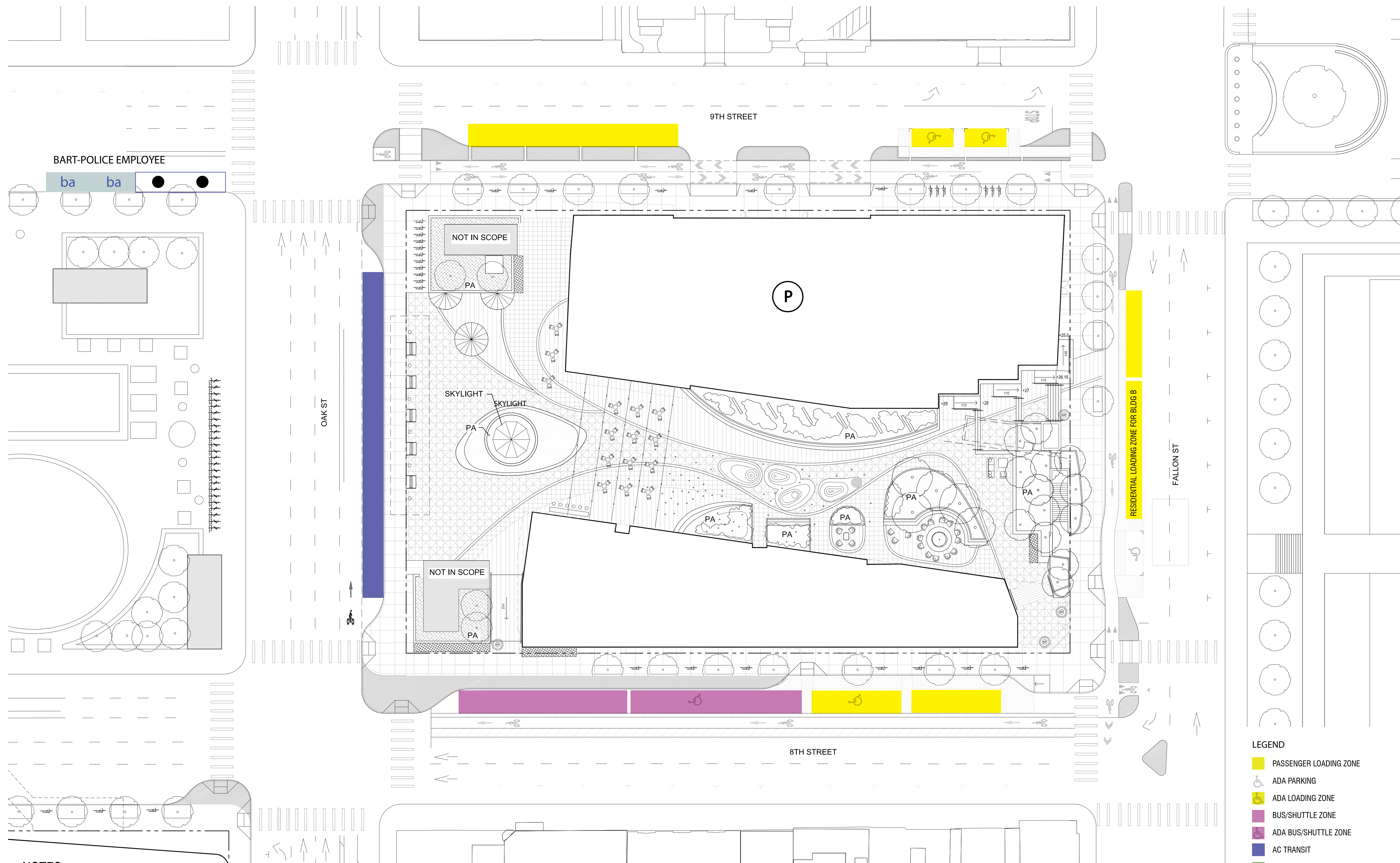
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**BLOCK 1 BUILDING ACCESS**





- NOTES:**
1. Locations shown are diagrammatic based on information at this time.
  2. Transportation planning decisions and phasing will affect ultimate build-out.
  3. The final design will be ADA compliant.
  4. BART station agent parking will be located to the satisfaction of the city and BART

- LEGEND**
- PASSENGER LOADING ZONE
  - ♿ ADA PARKING
  - ♿ ADA LOADING ZONE
  - BUS/SHUTTLE ZONE
  - ♿ ADA BUS/SHUTTLE ZONE
  - AC TRANSIT
  - AUTO PARKING
  - ba RESERVED FOR BART STATION AGENT PARKING
  - RESERVED FOR POLICE PARKING
  - P PARKING GARAGE

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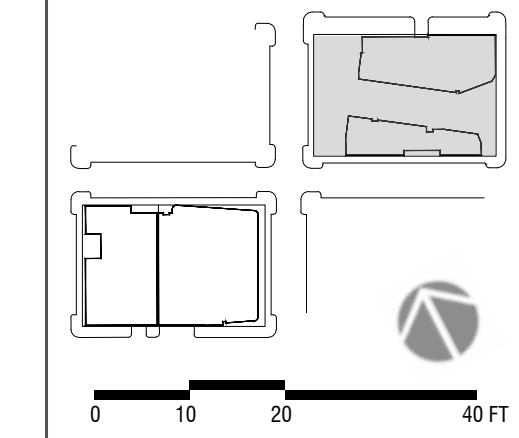
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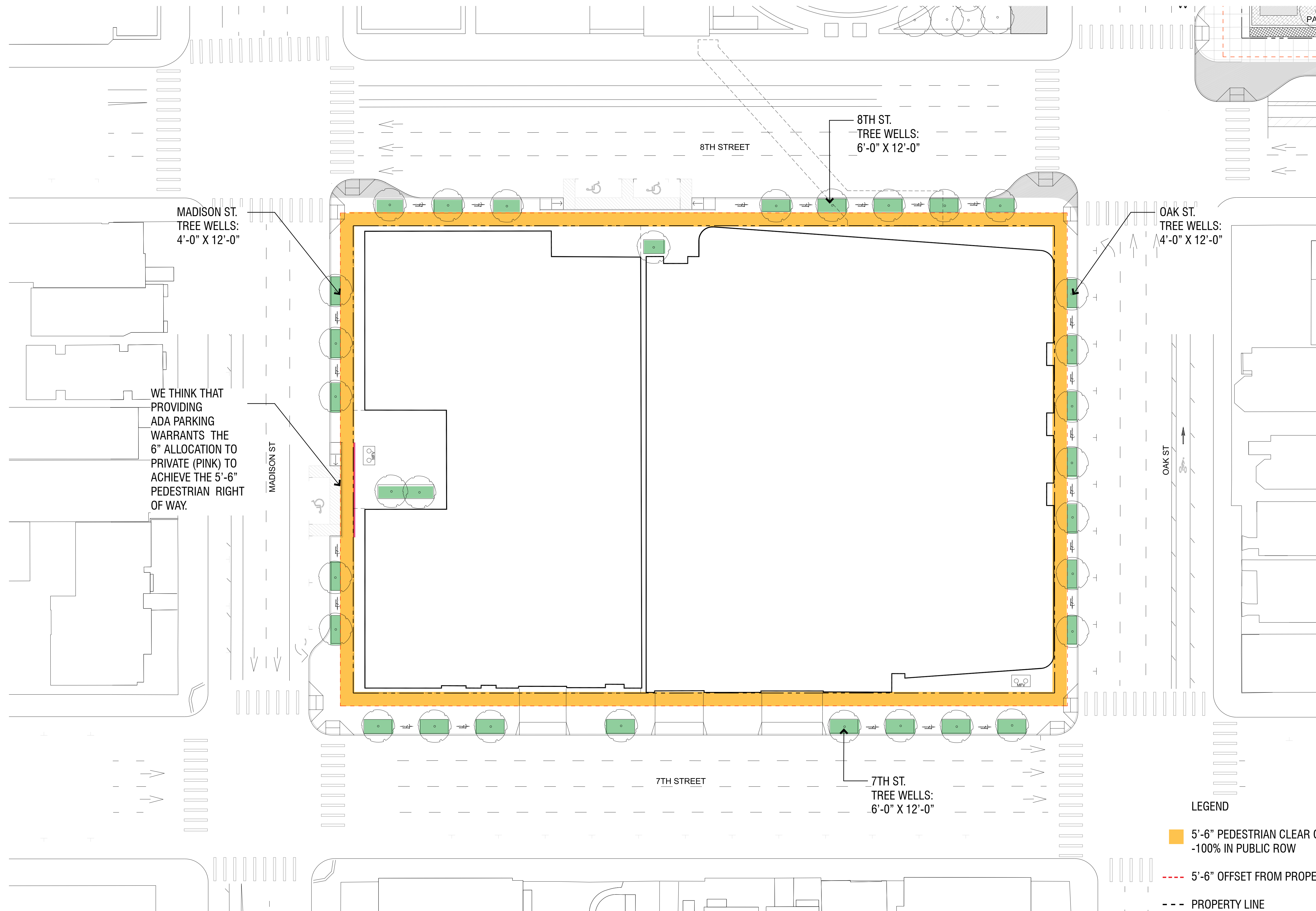
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**BLOCK 1 PUBLIC LOADING AND PARKING**





MADISON ST.  
TREE WELLS:  
4'-0" X 12'-0"

8TH ST.  
TREE WELLS:  
6'-0" X 12'-0"

OAK ST.  
TREE WELLS:  
4'-0" X 12'-0"

WE THINK THAT  
PROVIDING  
ADA PARKING  
WARRANTS THE  
6" ALLOCATION TO  
PRIVATE (PINK)  
TO ACHIEVE THE 5'-6"  
PEDESTRIAN RIGHT  
OF WAY.

7TH STREET

7TH ST.  
TREE WELLS:  
6'-0" X 12'-0"

LEGEND

- 5'-6" PEDESTRIAN CLEAR CIRCULATION  
-100% IN PUBLIC ROW
- 5'-6" OFFSET FROM PROPERTY LINE
- PROPERTY LINE
- 12' x 4' OAK AND MADISON  
12' x 6' 7th and 8th  
TREE WELLS MEETS MINIMUM  
STANDARD FOR A LARGE TREE.

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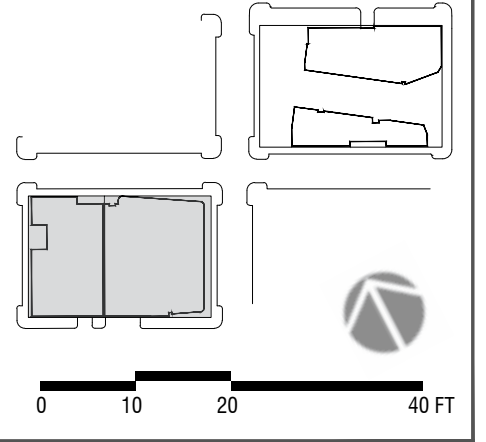
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**BLOCK 2 PEDESTRIAN RIGHT OF WAY**





(E) BUFFERED 1-WAY CYCLE PATH

(E) BUFFERED 1-WAY CYCLE PATH  
8TH STREET

(E) BUFFERED 1-WAY CYCLE PATH

MADISON ST

OAK ST

7TH STREET

- LEGEND**
- BIKE + SCOOTER LANE
  - BIKE PARKING
  - BIKE PATH OF TRAVEL
  - ▲ BUILDING BIKE PARKING

- NOTES:**
- ① Locations shown are diagrammatic based on information at this time.
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  - ③ Directional bike transitions are coordinating with city wide planning and subject to change. See Access plans for planning purposes.

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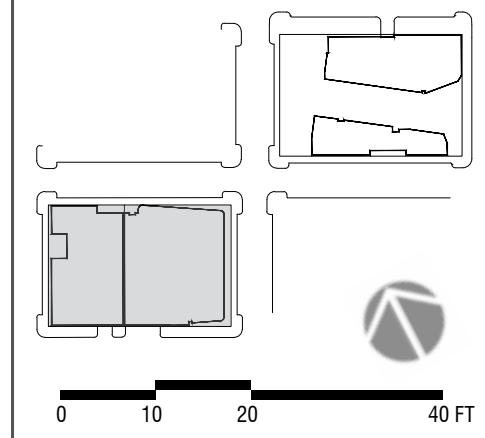
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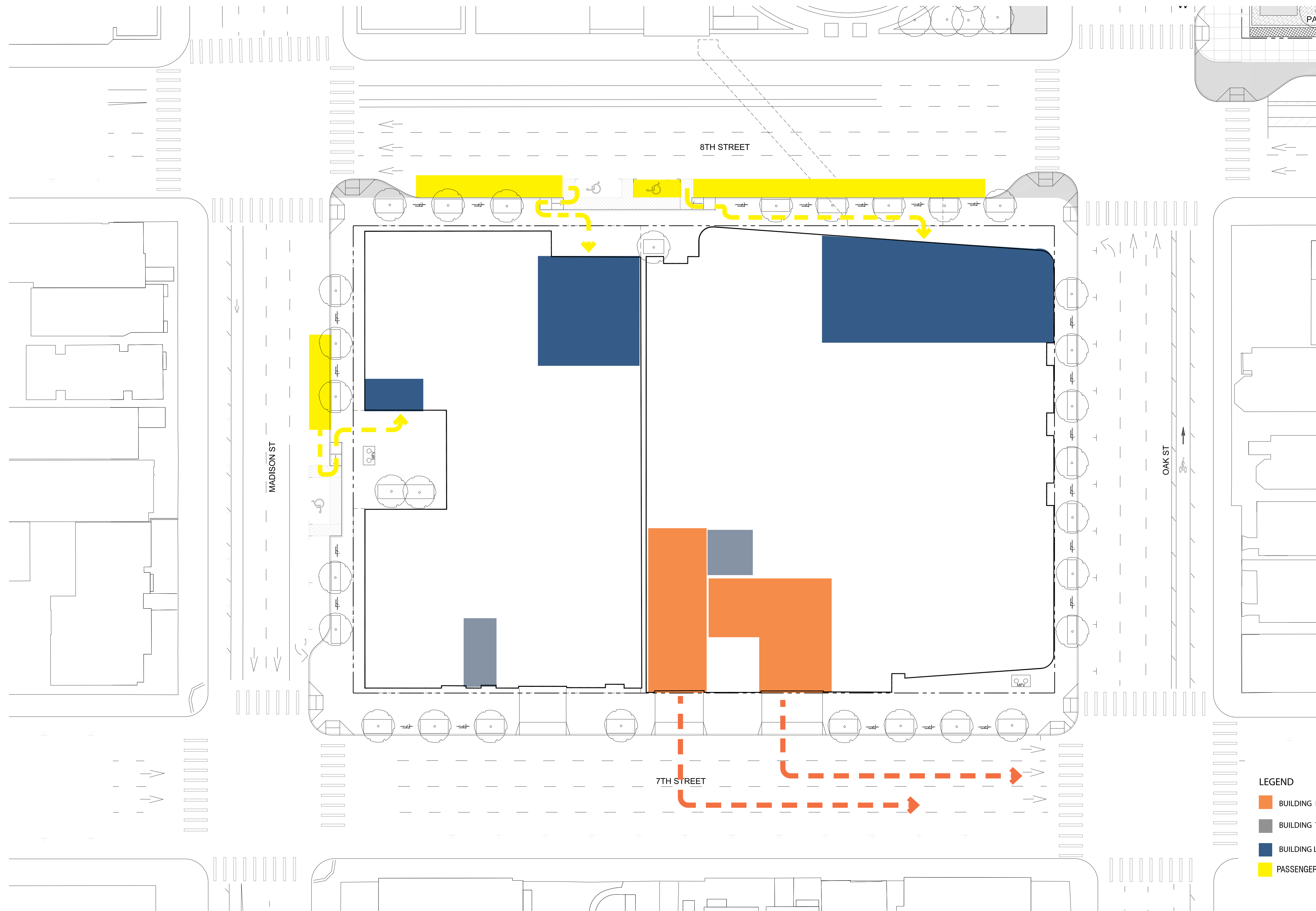
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DATE: AUGUST 8, 2019  
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**BLOCK 2 BICYCLE AND SCOOTER ACCESS**





- LEGEND**
- BUILDING LOADING
  - BUILDING TRASH PICK UP
  - BUILDING LOBBY
  - PASSENGER LOADING ZONE

- NOTES:**
1. Locations shown are diagrammatic based on information at this time.
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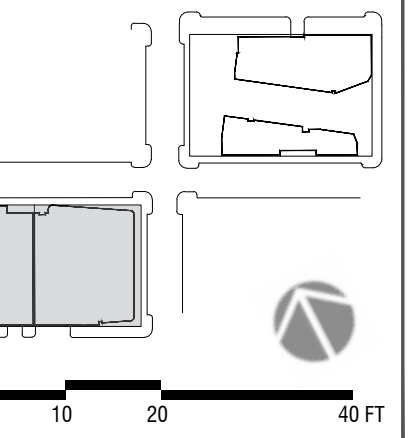
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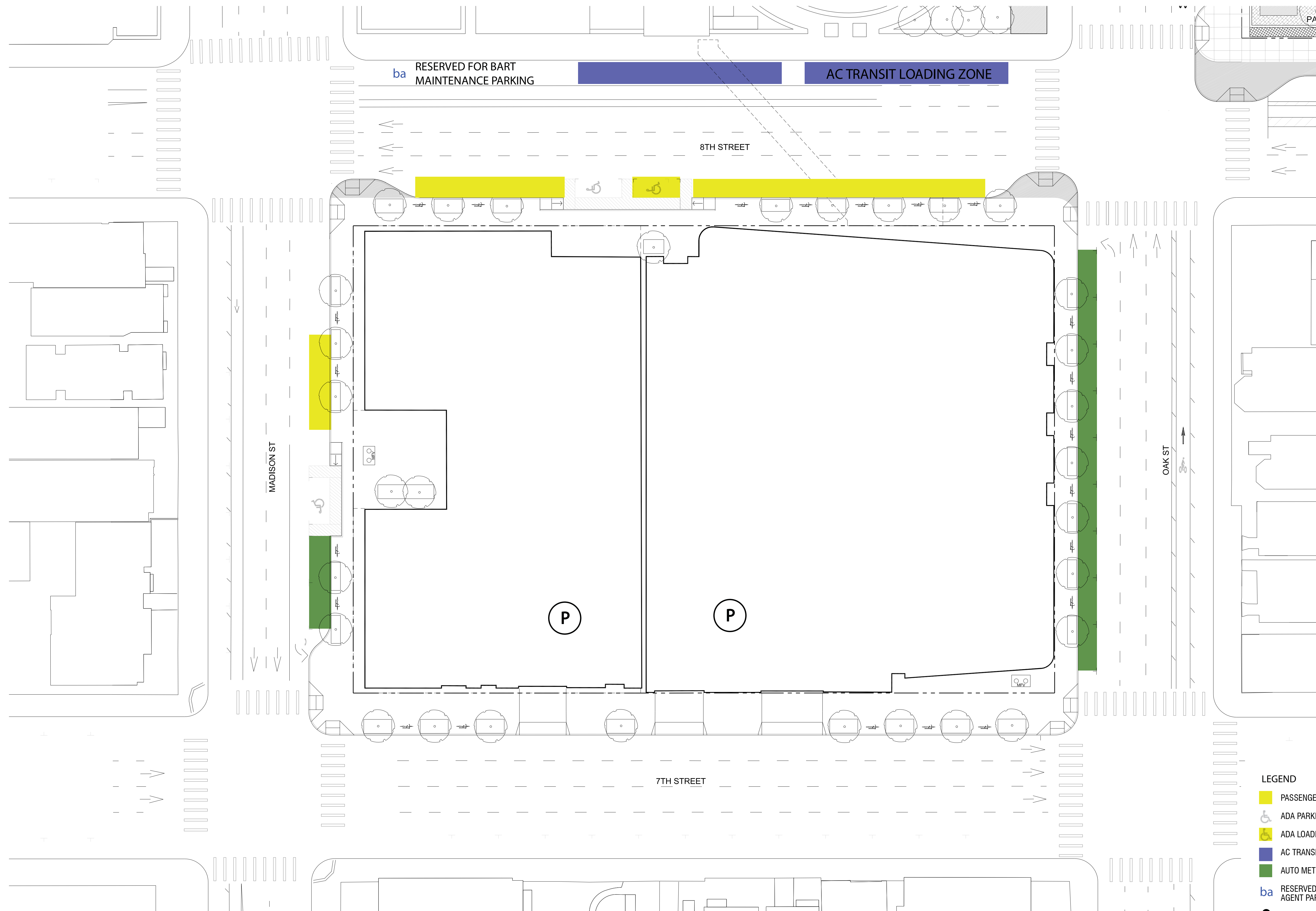
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**BLOCK 2 BUILDING ACCESS**





**NOTES:**

1. Locations shown are diagrammatic based on information at this time.
2. Transportation planning decisions and phasing will affect ultimate build-out.

**LEGEND**

- PASSENGER LOADING ZONE
- ADA PARKING
- ADA LOADING ZONE
- AC TRANSIT
- AUTO METERED PARKING
- RESERVED FOR BART STATION AGENT PARKING
- RESERVED FOR POLICE PARKING
- P PARKING GARAGE

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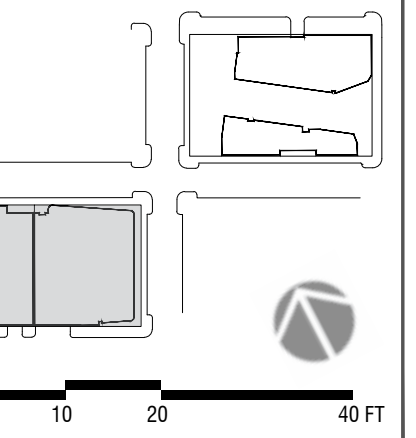
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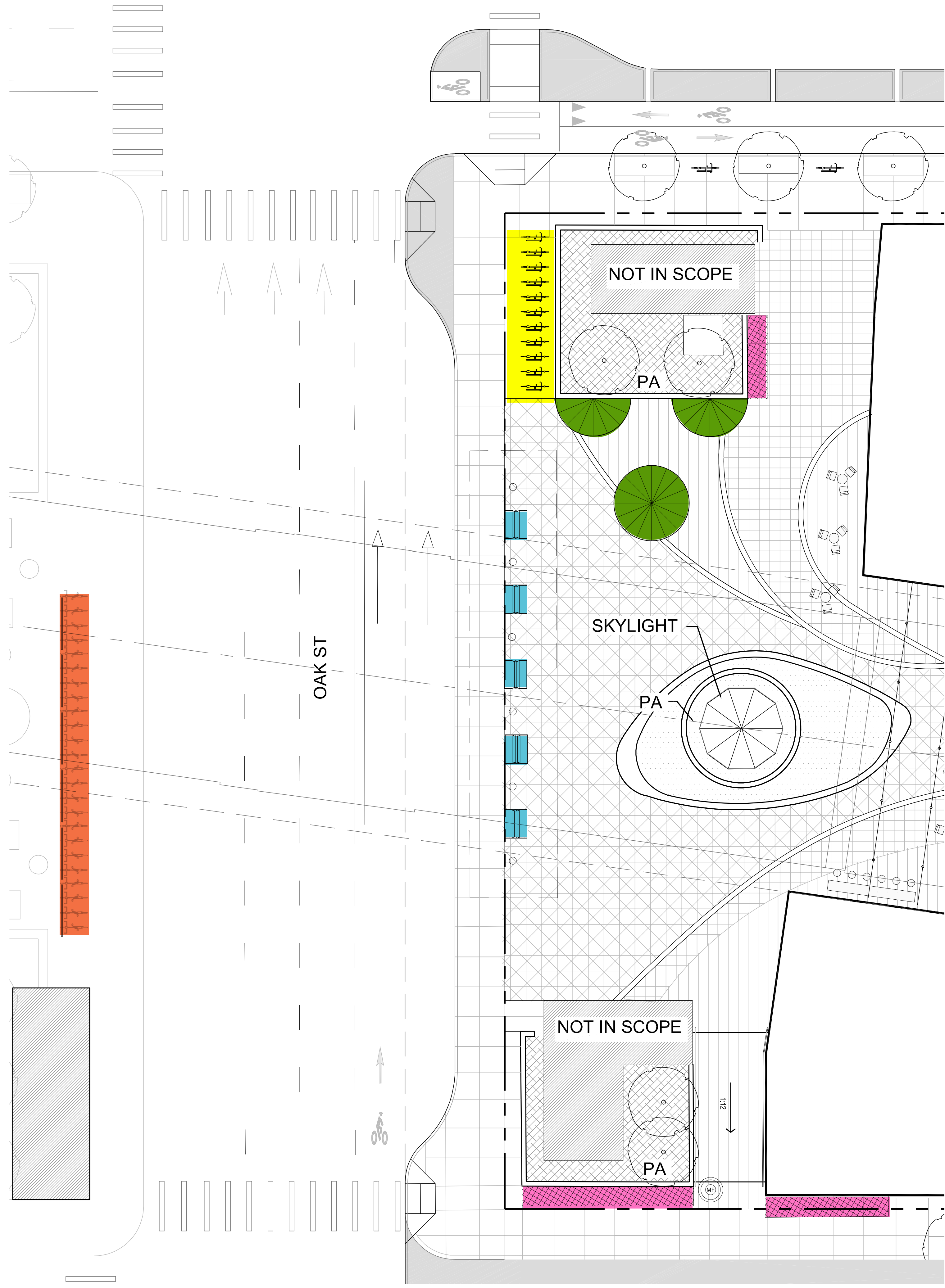
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**BLOCK 2 PUBLIC LOADING AND PARKING**

**L4.10**





BART TRANSIT FACILITIES  
BLOCK 1

	KEY	EXISTING	PROPOSED
BIKE RACKS		2 RACKS ACCOMMODATES 25 BIKES	1 RACK 12- INDIVIDUAL U-RACKS ACCOMMODATES 46 BIKES
BIKE LOCKERS		ACCOMMODATES 24 BIKES	ACCOMMODATES 32 BIKES
DOCKLESS SCOOTER CORRAL		0	4 CORRALS TOTTALING 372 SQFT
EXISTING SHARED BIKE DOCK (BAY WHEELS)		1 DOCK TOF BLOCK 70'-0" x 5'-0"	1 DOCK TOF BLOCK 70'-0" x 5'-0"
SEATING FOR AC TRANSIT * FINAL MANAGEMENT & OPERATION STRATEGY TO BE DEVELOPED		0	5 DUAL FACING BENCHES WILL COMPLY WITH BFS 

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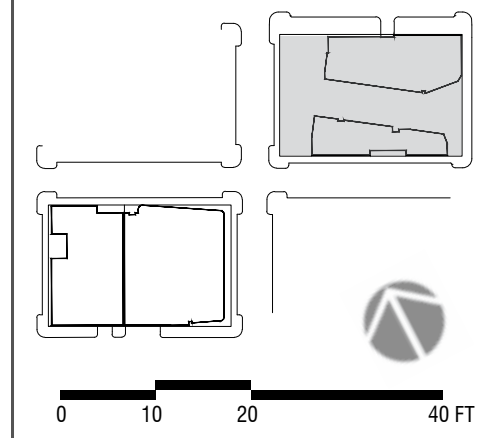
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**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

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REVISION SCHEDULE

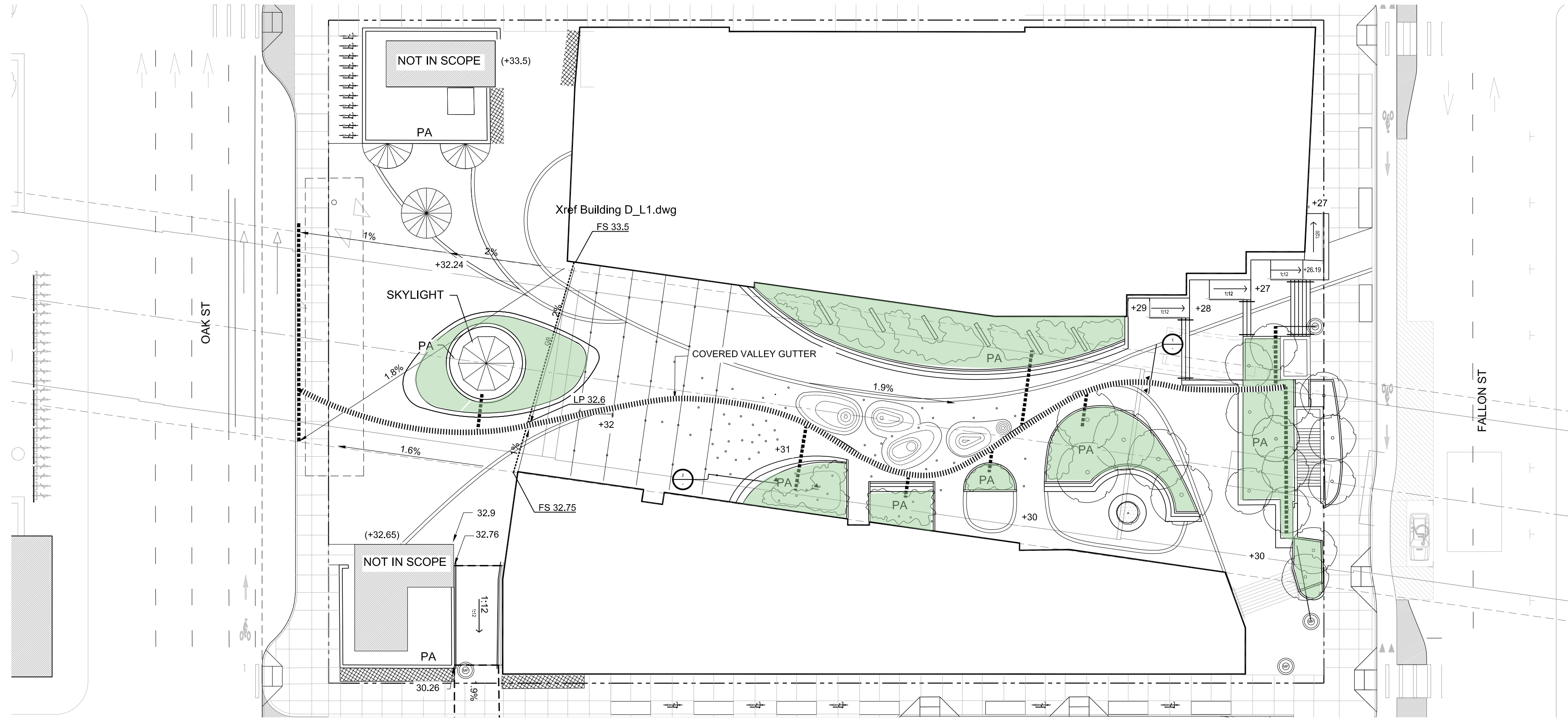
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7	REVISED RESPONSE TO PDP#4 COMMENTS	03/17/2021



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**BLOCK 1  
DETAILED DESIGN  
BART ARRIVAL  
EXPERIENCE**





3 DRAINAGE PLAN OF THE PASEO  
NTS

- NOTES:
1. All planting and irrigation above tunnel will be designed to meet BFS standards.
  2. Waterproofing design to be determined. Final design will comply with BFS standards.
  3. Existing drainage (see Civil C2.1 and C2.2) to be replaced with new drainage trench drain system.

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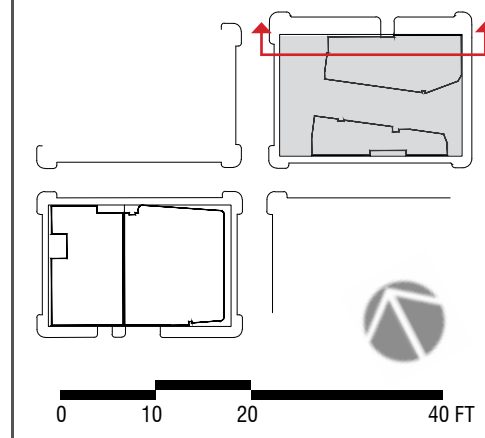
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

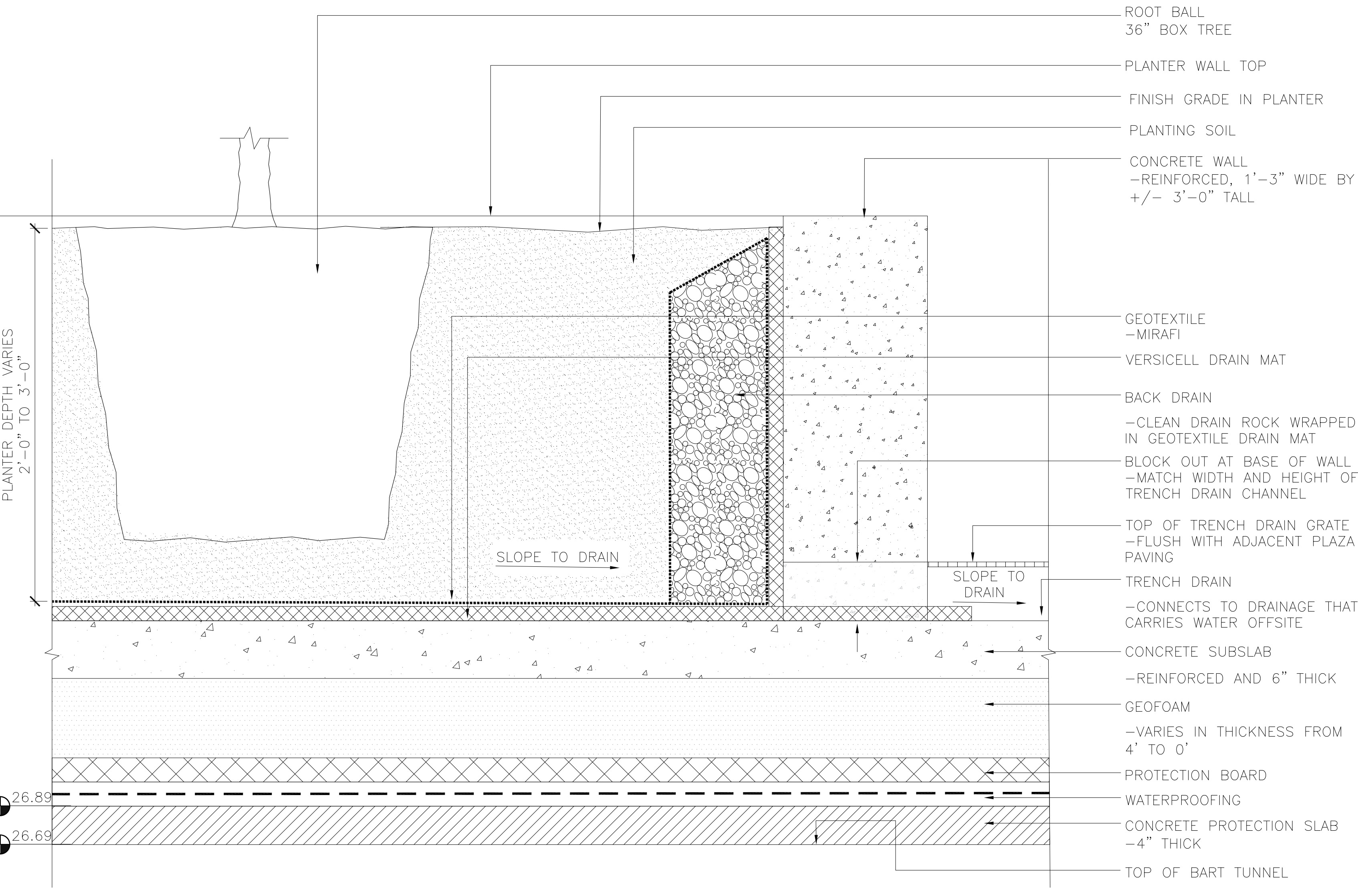
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

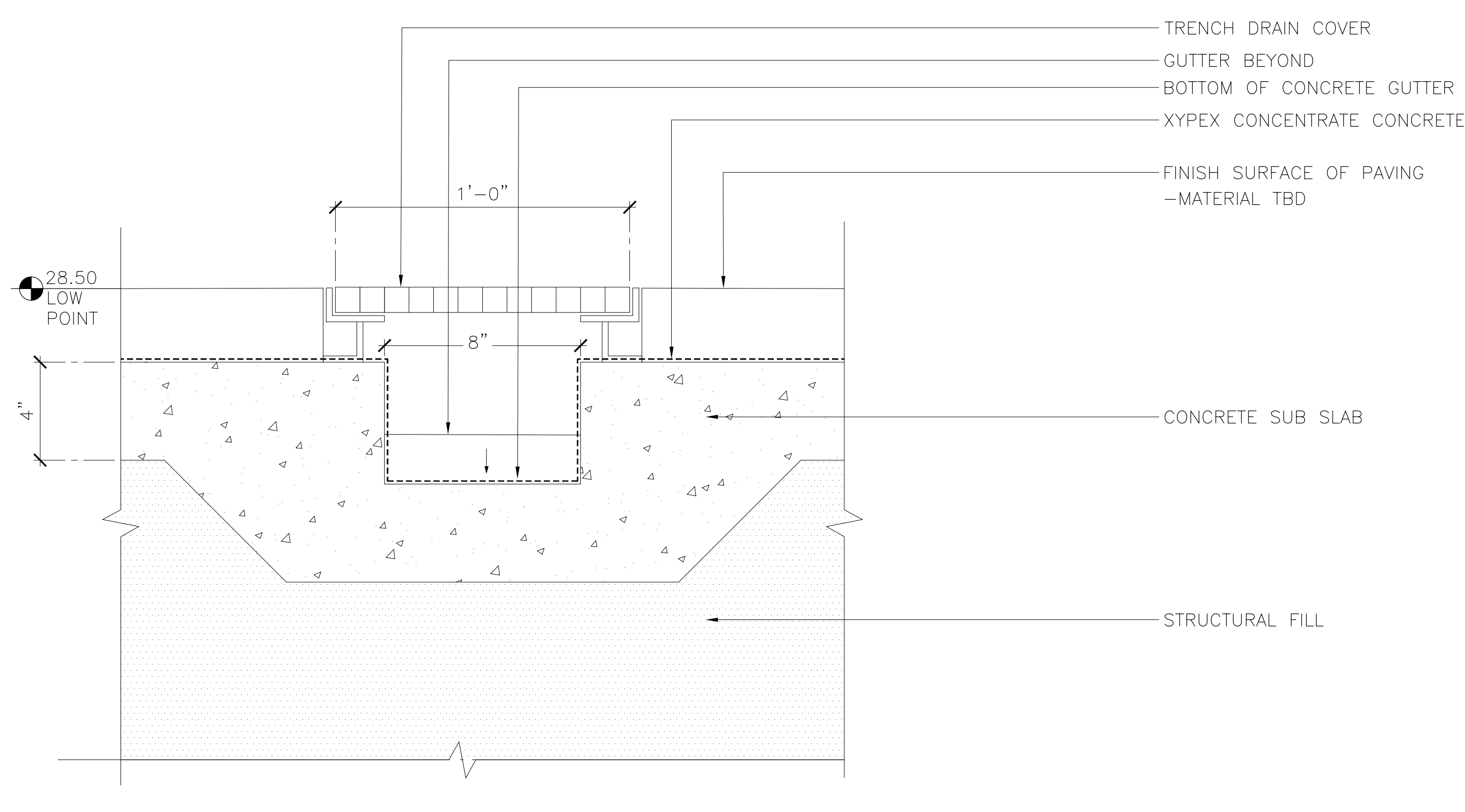
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPH COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPH COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPH COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPH COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 DETAILED DESIGN PASEO DRAINAGE**

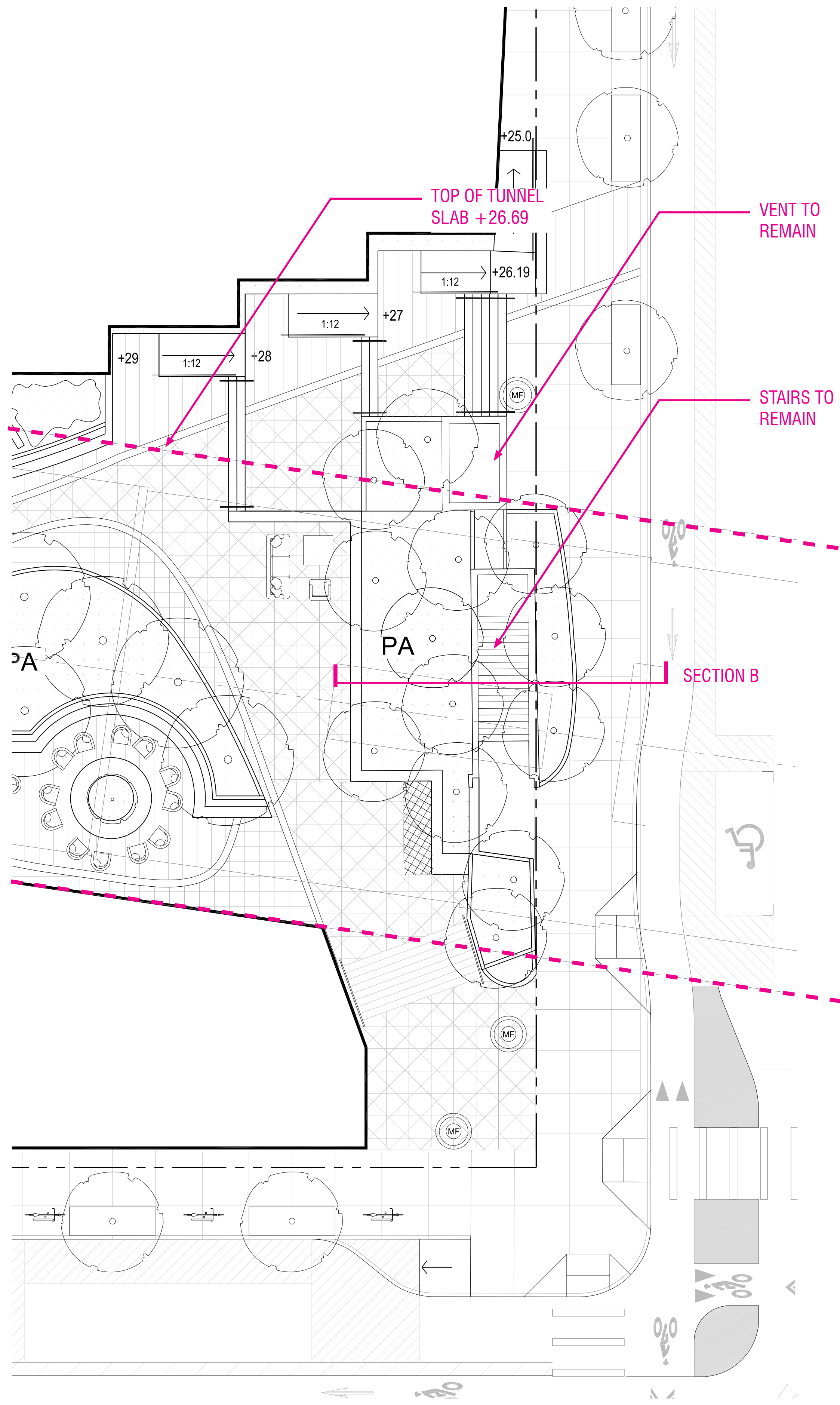


2 PLANTER WITH CONNECTION TO TRENCH DRAIN  
SCALE: 1-1/2"=1'-0" 0 6" 1' 2' TYPICAL SECTION



1 TRENCH DRAIN  
SCALE: 3"=1'-0" 0 6" 1' TYPICAL SECTION





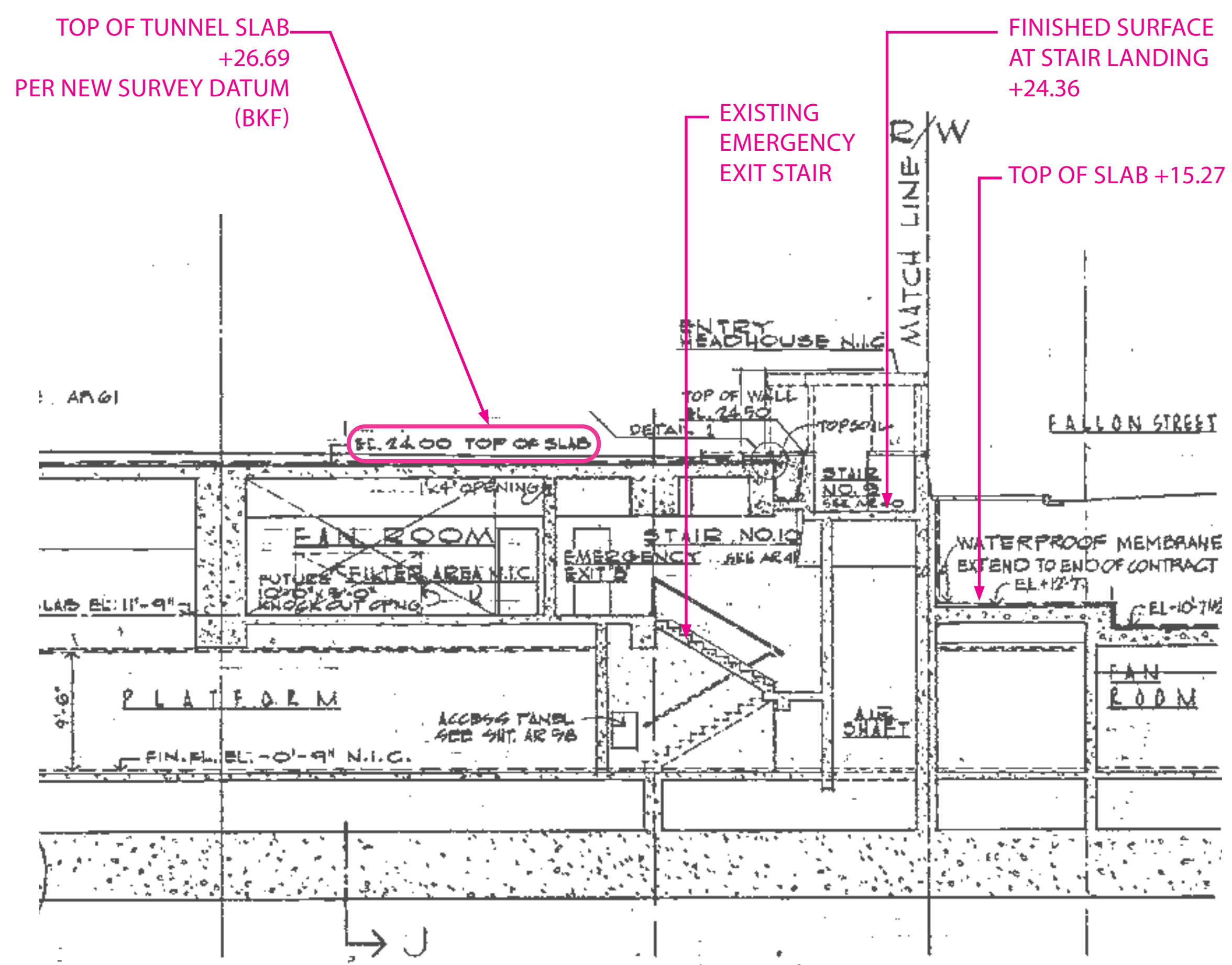
RENDERING OF THE PASEO ENTRANCE ON FALLON



VENT: TO REMAIN  
FINAL DESIGN TREATMENT TO BE DEVELOPED WITH  
BART. OPPORTUNITY FOR ART.



STAIR: TO REMAIN  
FINAL DESIGN TREATMENT TO BE DEVELOPED WITH  
BART. OPPORTUNITY FOR ART.



SECTION B  
STAIR: TO REMAIN AS-BUILT OVERLAY AT EMERGENCY EXIT STAIR

- 

EAST BAY ASIAN LOCAL  
DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612
- 

STRADA  
101 MISSION ST. #200  
SAN FRANCISCO, CA 94105
- 

PYATOK  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com
- 

SCB  
335 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
415/776-2400  
www.scb.com
- 

ENWILLERUEHL  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/891-1696  
www.enwilleruehl.com
- 

BKF  
225 SHREVE DR. SUITE 200  
REDDING, CA 96001  
970/823-3300  
www.bkf.com

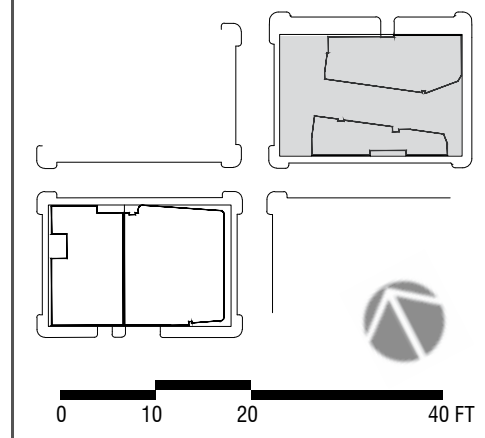
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

### BLOCK 1 DETAILED DESIGN EAST END

# L5.3



# LAKE MERRITT BART DEVELOPMENT

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

CITY OF OAKLAND

ALAMEDA COUNTY

STATE OF CALIFORNIA

### PROJECT DESCRIPTION

**BLOCK 1**  
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 1, BOUND BY 8TH STREET, FALLON STREET, 9TH STREET, AND OAK STREET. A NEW MARKET RATE RESIDENTIAL BUILDING (BUILDING A), A NEW SENIOR HOUSING BUILDING (BUILDING B), AND A NEW PEDESTRIAN PASEO WILL BE CONSTRUCTED ON THE PROPERTY SITE AND WILL MAINTAIN AND IMPROVE ACCESS TO THE EXISTING LAKE MERRITT BART STATION.

**BLOCK 2**  
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 2, BOUND BY 7TH STREET, OAK STREET, 8TH STREET, AND MADISON STREET. A NEW OFFICE BUILDING (BUILDING C) AND A NEW AFFORDABLE HOUSING BUILDING (BUILDING D) WILL BE CONSTRUCTED ON THE PROPERTY SITE.

### GENERAL NOTES

- SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY UNDER THE SUPERVISION OF DAVIS THRESH, PLS #6868, PERFORMED ON MAY 13TH, MAY 15TH, AND MAY 22ND, 2019. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
- FEMA DESIGNATED FLOOD ZONE:** PURSUANT TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY, NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, COMMUNITY NO. 06001C0067H, EFFECTIVE DATE DECEMBER 21, 2018, THE SUBJECT PROPERTY LIES WITHIN FLOOD ZONE "X" - AREAS DETERMINED OF MINIMAL FLOOD HAZARD.
- UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES, AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
- BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HEREON IS BASED UPON RESOLUTIONS OF RECORD STREET AND LOT DIMENSIONS AND COLLECTED STREET MONUMENT LOCATIONS WITHIN THE SURROUNDING STREETS. MONUMENT COLLECTION WAS CONDUCTED ON APRIL 19, 2019. NO CURRENT MAP OR RECORD OF SURVEY CURRENTLY EXISTS FOR THE MAPPED BLOCKS; DEEDS MAKE REFERENCE TO KELLERSBERGER'S MAP OF OAKLAND FILED IN BOOK 7 OF MISCELLANEOUS MAPS AT PAGE 3, ALAMEDA COUNTY RECORDS.
- BENCHMARK:** FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD88)
- HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.

### PROJECT DATA

**OWNERS:**  
STRADA INVESTMENT GROUP  
101 MISSION STREET, SUITE 420  
SAN FRANCISCO, CA 94105  
PHONE: (415) 263-9151  
CONTACT: WILLIAM GOODMAN  
EAST BAY ASIAN LOCAL DEVELOPMENT CORP  
1825 SAN PABLO AVENUE, SUITE 200  
OAKLAND, CA 94612  
PHONE: (510) 287-5353  
CONTACT: ANDREW MATSAS

**ARCHITECTS:**  
PYATOK ARCHITECTS  
1611 TELEGRAPH AVENUE, SUITE 200  
OAKLAND, CA 94612  
PHONE: (510) 465-7010  
CONTACT: PETER WALLER  
SOLOMON CORDWELL BUENZ ARCHITECTS  
255 CALIFORNIA STREET, 3RD FLOOR  
SAN FRANCISCO, CA 94111  
PHONE: (415) 216-2450  
CONTACT: CYRIL CHONG

**CIVIL ENGINEER:**  
BKF ENGINEERS  
255 SHORELINE DRIVE, SUITE 200  
REDWOOD CITY, CA 94065  
PHONE: (650) 482-6377  
CONTACT: SIMON NORTH

**LANDSCAPE ARCHITECT:**  
EINWILLERKUEHL LANDSCAPE ARCHITECTURE  
318 HARRISON STREET, SUITE 301  
OAKLAND, CA 94607  
PHONE: (510) 891-1696  
CONTACT: SARAH KUEHL

**ASSESSOR PARCEL NO.:**  
001-0169-001 (BLOCK 1)  
001-0171-002 (BLOCK 2)

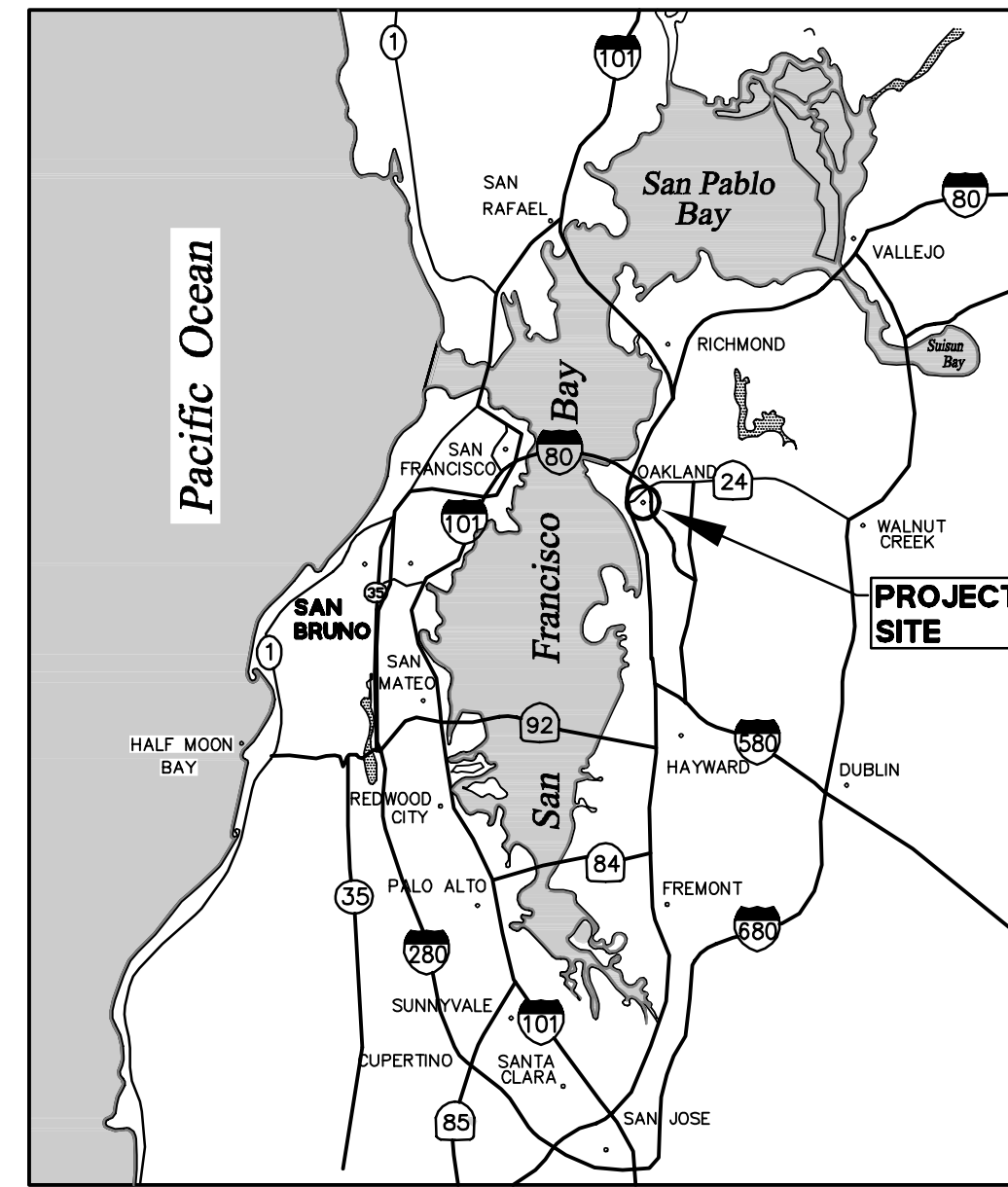
**EXISTING LAND USE:** COMMERCIAL

**PROPOSED LAND USE:** MIXED USE

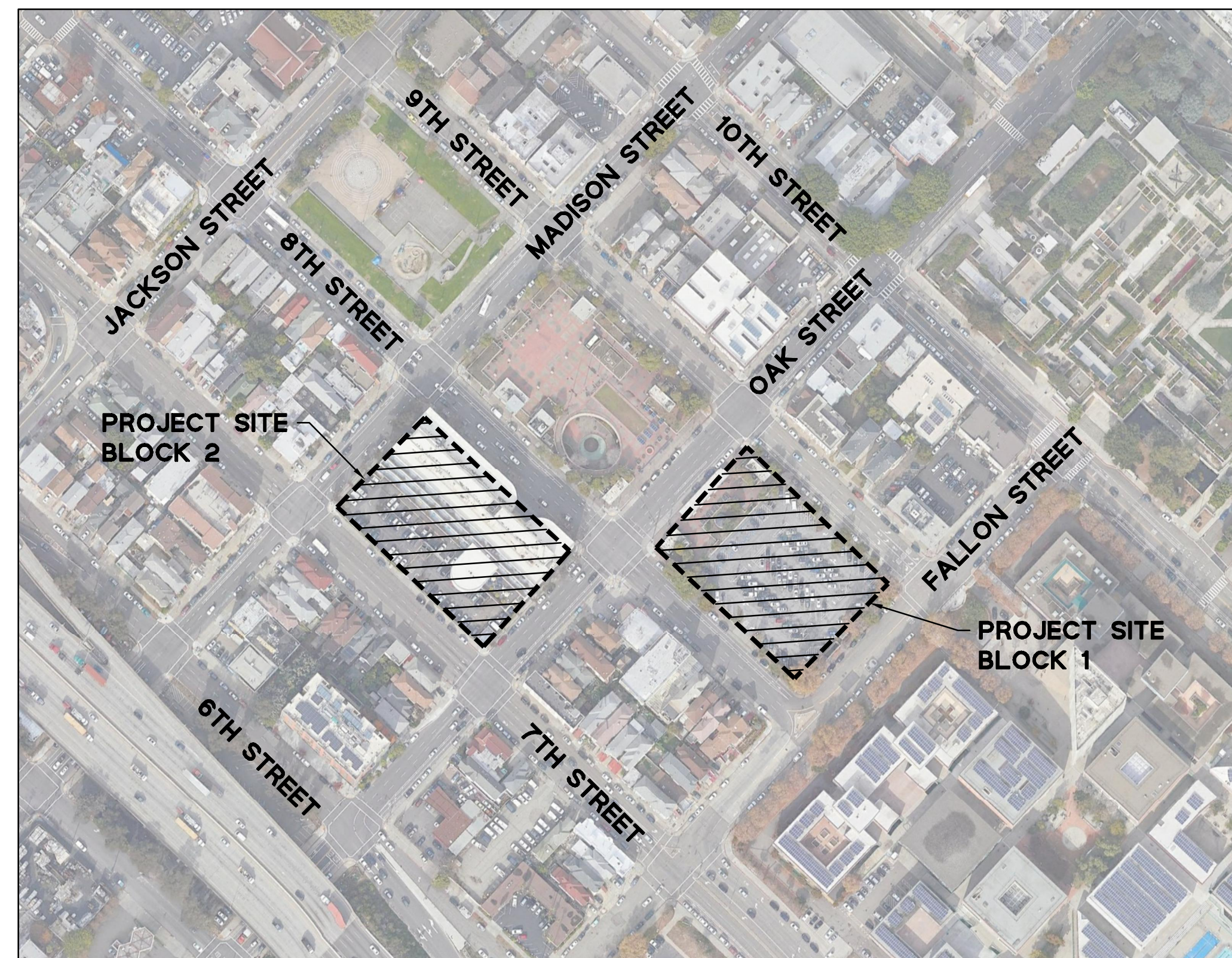
**LAND AREA:**  
1.383 ACRES (BLOCK 1)  
1.377 ACRES (BLOCK 2)

### UTILITY INFORMATION:

WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD)  
FIRE PROTECTION: CITY OF OAKLAND / EBMUD  
SEWAGE DISPOSAL: CITY OF OAKLAND  
STORM DRAIN: CITY OF OAKLAND  
GAS: PACIFIC GAS & ELECTRIC (PG&E)  
ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E)  
TELEPHONE: AT&T  
CABLE TELEVISION: COMCAST



LOCATION MAP  
NTS



VICINITY MAP  
NTS

### ABBREVIATIONS

AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BIKE WAY
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EV	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
HCR	=	HANDICAP RAMP
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
P	=	POST
PKM	=	PARKING METER
SDCO	=	STORM DRAIN CLEANOUT
SDDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UB	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	RIGHT OF WAY
---	---	CURB AND GUTTER
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWOUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	CATCH BASIN
---	---	SSMH
---	---	SDMH
---	---	SDDI
---	---	WATER VALVE
---	---	GAS VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DCDA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE

### SHEET INDEX

SHEET NO	DESCRIPTION
C1.0	TITLE SHEET
C2.0	OVERALL SHEET INDEX
C2.1	EXISTING CONDITIONS (BLOCK 1)
C2.2	EXISTING CONDITIONS (BLOCK 2)
C3.1	PRELIMINARY DEMOLITION PLAN (BLOCK 1)
C3.2	PRELIMINARY DEMOLITION PLAN (BLOCK 2)
C4.1	PRELIMINARY SITE PLAN (BLOCK 1)
C4.2	PRELIMINARY SITE PLAN (BLOCK 2)
C5.1	PRELIMINARY GRADING PLAN (BLOCK 1)
C5.2	PRELIMINARY GRADING PLAN (BLOCK 2)
C5.3	PRELIMINARY SECTIONS
C6.1	PRELIMINARY UTILITY PLAN (BLOCK 1)
C6.2	PRELIMINARY UTILITY PLAN (BLOCK 2)
C7.1	PRELIMINARY STORMWATER PLAN (BLOCK 1)
C7.2	PRELIMINARY STORMWATER PLAN (BLOCK 2)
C8.1	PRELIMINARY DETAILS

### ENGINEER'S STATEMENT

THESE CONSTRUCTION DOCUMENTS HAVE BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

*Simon North*  
SIMON R. NORTH, P.E.  
VICE PRESIDENT  
BKF ENGINEERS



03/17/2021  
DATE

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

STRADA INVESTMENT GROUP  
101 MISSION STREET, SUITE 420  
SAN FRANCISCO, CA 94105

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OAKLAND, CA 94607  
(510) 891-1696  
www.einwillerkuehl.com

BKF ENGINEERS  
255 SHORELINE DR., SUITE 200  
REDWOOD CITY, CA 94065  
(650) 482-6377  
www.bkf.com

## LAKE MERRITT BART DEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

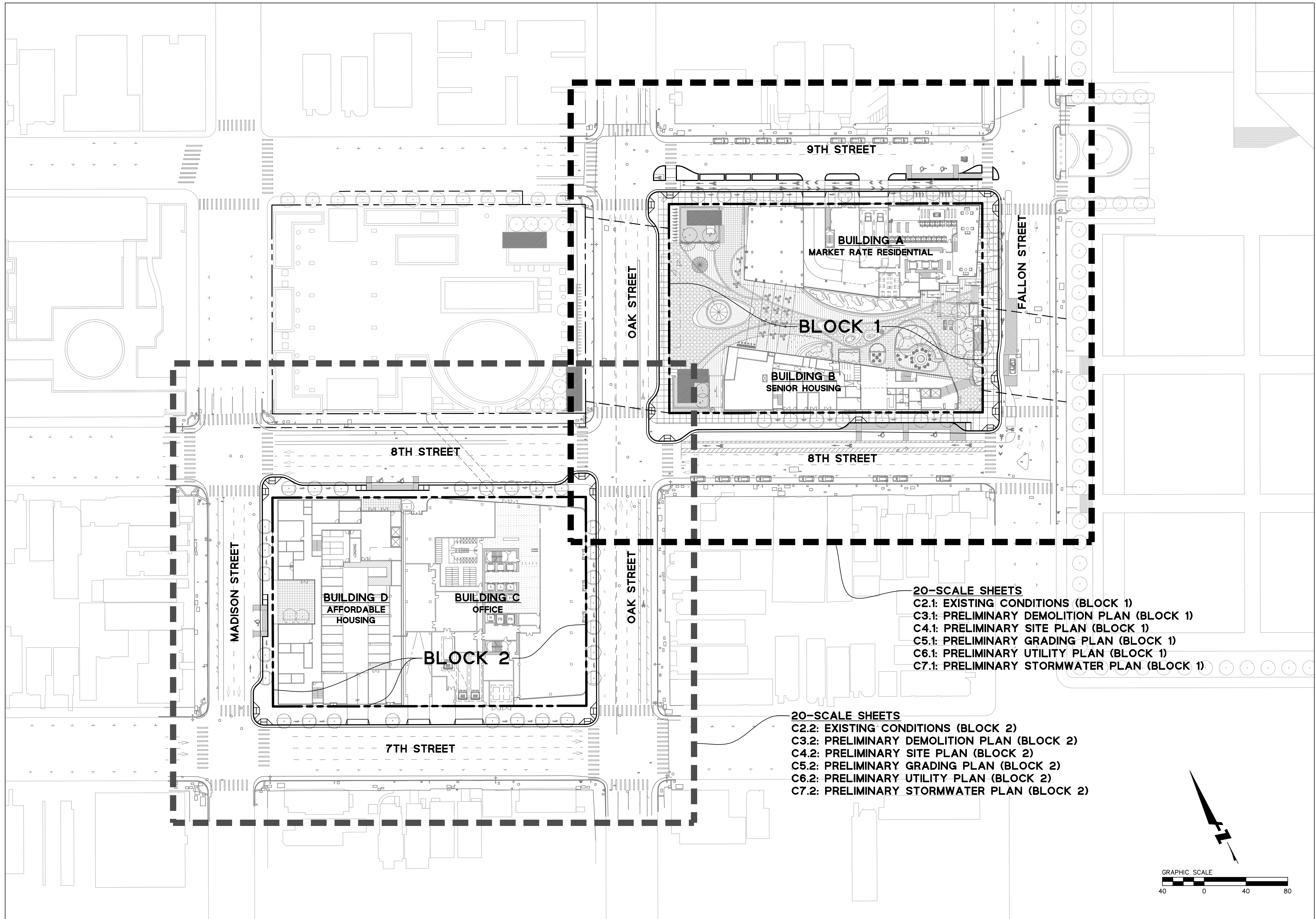
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: AS NOTED

### TITLE SHEET

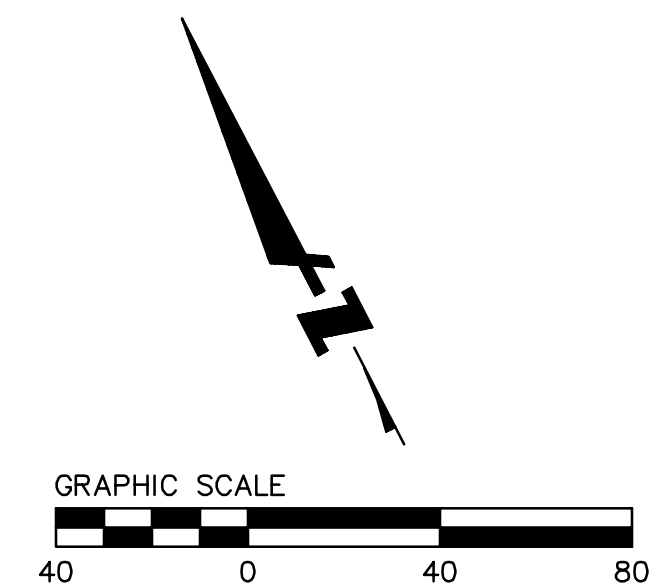
# C1.0





- 20-SCALE SHEETS**  
 C2.1: EXISTING CONDITIONS (BLOCK 1)  
 C3.1: PRELIMINARY DEMOLITION PLAN (BLOCK 1)  
 C4.1: PRELIMINARY SITE PLAN (BLOCK 1)  
 C5.1: PRELIMINARY GRADING PLAN (BLOCK 1)  
 C6.1: PRELIMINARY UTILITY PLAN (BLOCK 1)  
 C7.1: PRELIMINARY STORMWATER PLAN (BLOCK 1)

- 20-SCALE SHEETS**  
 C2.2: EXISTING CONDITIONS (BLOCK 2)  
 C3.2: PRELIMINARY DEMOLITION PLAN (BLOCK 2)  
 C4.2: PRELIMINARY SITE PLAN (BLOCK 2)  
 C5.2: PRELIMINARY GRADING PLAN (BLOCK 2)  
 C6.2: PRELIMINARY UTILITY PLAN (BLOCK 2)  
 C7.2: PRELIMINARY STORMWATER PLAN (BLOCK 2)



**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
 1825 SAN PABLO AVE. #200  
 OAKLAND, CA 94612

**STRADA**  
 CONSULTANTS GROUP  
 101 MISSION ST. #425  
 SAN FRANCISCO, CA 94105

**PYATOK**  
 1611 TELEGRAPH AVE. SUITE 200  
 OAKLAND, CA 94612  
 www.pyatok.com

**SCB**  
 SOLOMON CORDWELL BUENZ ARCHITECTS  
 255 CALIFORNIA ST.  
 SAN FRANCISCO, CA 94111  
 (415) 216-2450  
 www.scb.com

**ENVIRONMENTAL**  
 318 HARRISON ST. STE 301  
 OAKLAND, CA 94607  
 (510) 891-1696  
 www.environmental.com

**BKF100**  
 YEARS  
 ENGINEERS SURVEYORS PLANNERS  
 255 SHORELINE DR. SUITE 200  
 REDWOOD CITY, CA 94065  
 (650) 482-0300  
 www.bkf.com

**LAKE MERRITT BART DEVELOPMENT**  
 Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

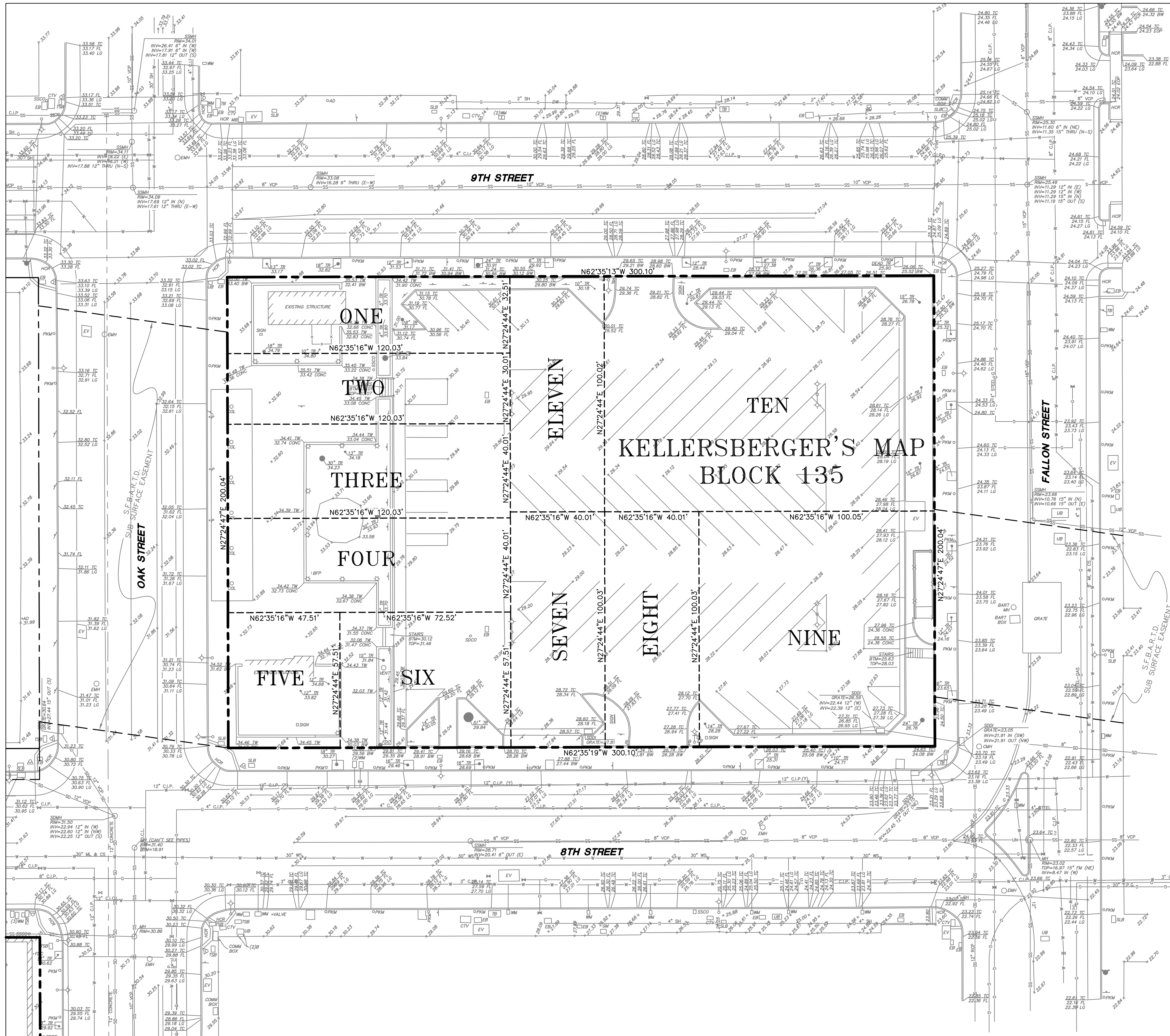
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
 DATE: 03/19/2021  
 SCALE: 1" = 40'

**OVERALL SHEET INDEX**

**C2.0**





**ANNOTATION & LEGEND**

- AD = AREA DRAIN
- B = BOLLARD
- BFP = BACKFLOW PREVENTOR
- BR = BIKE RACK
- BW = BACK OF WALK
- CLDR = CENTERLINE OF DOOR
- CMH = COMMUNICATION MANHOLE
- COL = COLUMN
- COMM = COMMUNICATION
- CONC = CONCRETE
- CTV = CABLE TELEVISION
- DW = DRIVEWAY
- EB = ELECTRICAL BOX
- EMH = ELECTRICAL MANHOLE
- EP = EDGE OF PAVEMENT
- EV = ELECTRICAL VAULT
- FDC = FIRE DEPARTMENT CONNECTION
- FL = FLOWLINE
- HCR = HANDICAP RAMP
- LIP = LIP OF GUTTER
- MB = MAIL BOX
- MH = MANHOLE
- P = POST
- PKM = PARKING METER
- SDCO = STORM DRAIN CLEANOUT
- SDDI = STORM DRAIN DROP INLET
- SDMH = STORM DRAIN MANHOLE
- SLB = STREET LIGHTING BOX
- SSCO = SANITARY SEWER CLEANOUT
- SSMH = SANITARY SEWER MANHOLE
- TB = TELEPHONE BOX
- TC = TOP OF CURB
- TR = TREE
- TSB = TRAFFIC SIGNAL BOX
- TW = TOP OF WALL
- UB = UTILITY BOX
- UV = UTILITY VAULT
- WM = WATER METER
- WP = WATER PIPE
- ☆ = AREA/YARD LIGHT
- ⊙ = FIRE HYDRANT
- ⊙ = SIGN
- ⊙ = STREET LIGHT
- ⊙ = GAS VALVE
- ⊙ = WATER VALVE
- ⊙ = SIGNAL LIGHT
- = ELECTRICAL LINE
- = GAS LINE
- = STORM DRAIN LINE
- = SANITARY SEWER LINE
- = UNKNOWN UTILITY LINE
- = DOMESTIC WATER LINE

**NOTES**

FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019

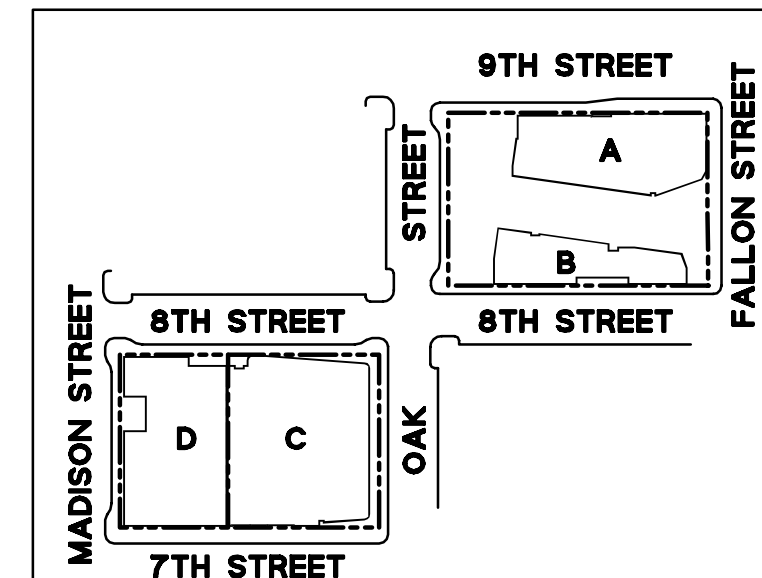
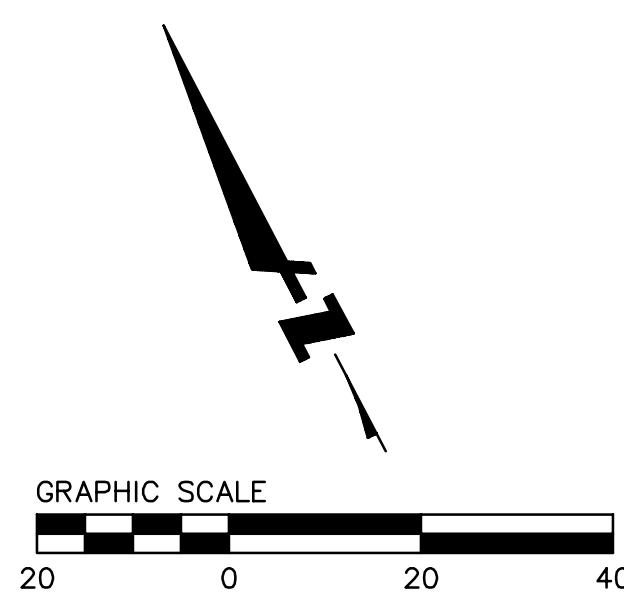
ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

**HORIZONTAL CONTROL**

HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS83), EPOCH 2017.00

**BENCHMARK**

FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET.  
ELEVATION = 23.062 (NAVD88)



**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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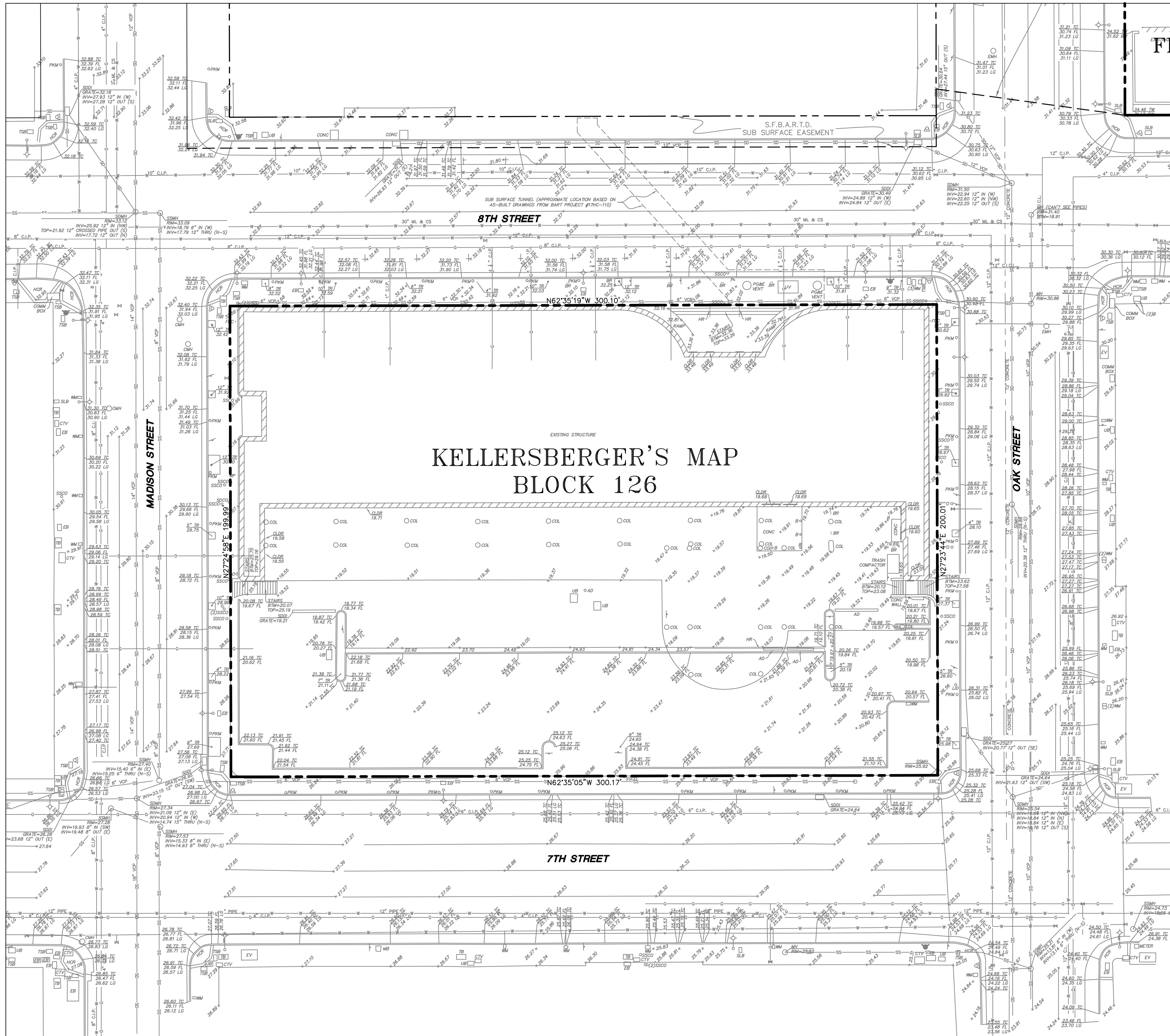
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JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'

**EXISTING CONDITIONS (BLOCK 1)**

**C2.1**





# KELLERSBERGER'S MAP BLOCK 126

### ANNOTATION & LEGEND

- AD = AREA DRAIN
- B = BOLLARD
- BFP = BACKFLOW PREVENTOR
- BR = BIKE RACK
- BW = BACK OF WALK
- CLDR = CENTERLINE OF DOOR
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- MH = MANHOLE
- P = POST
- PKM = PARKING METER
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- SSDI = STORM DRAIN DROP INLET
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- SSCO = SANITARY SEWER CLEANOUT
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- TB = TELEPHONE BOX
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- UB = UTILITY BOX
- UV = UTILITY VAULT
- WM = WATER METER
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- ☆ = AREA/YARD LIGHT
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- ⊙ = SIGN
- ⊙ = STREET LIGHT
- ⊕ = GAS VALVE
- ⊕ = WATER VALVE
- ⊙ = SIGNAL LIGHT
- E—E— = ELECTRICAL LINE
- G—G— = GAS LINE
- SD—SD— = STORM DRAIN LINE
- SS—SS— = SANITARY SEWER LINE
- UN—UN— = UNKNOWN UTILITY LINE
- W—W— = DOMESTIC WATER LINE

### NOTES

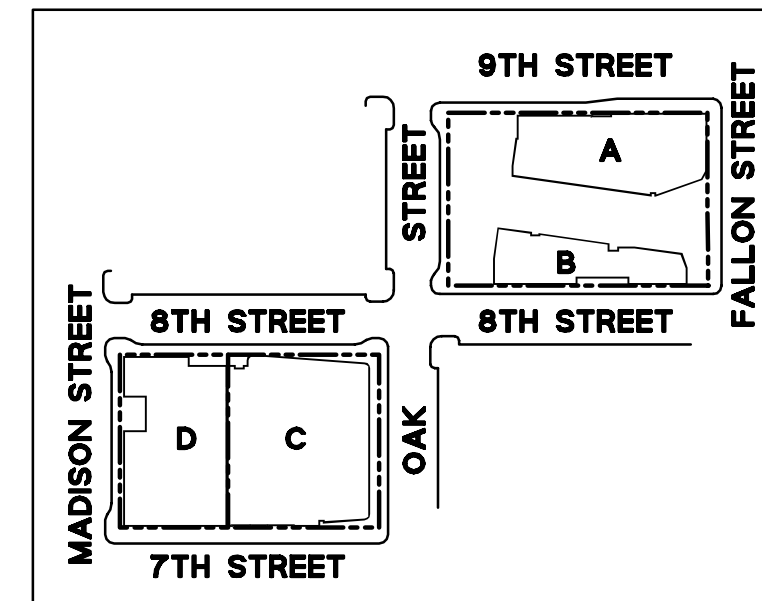
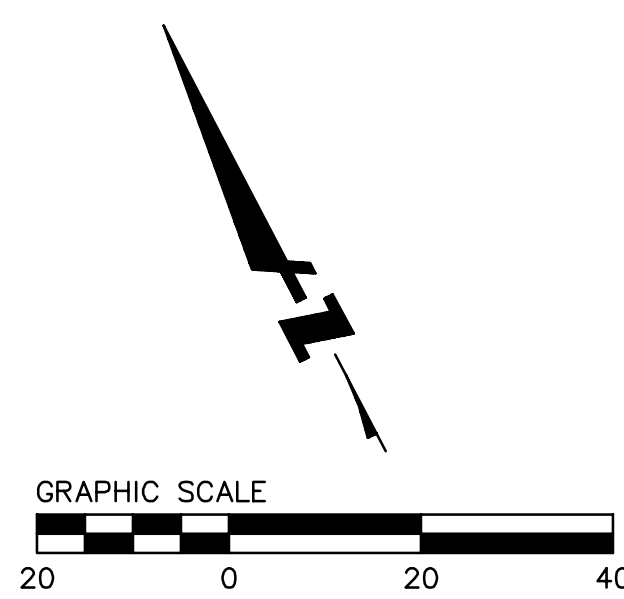
FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019  
 ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

### HORIZONTAL CONTROL

HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS83), EPOCH 2017.00

### BENCHMARK

FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET.  
 ELEVATION = 23.062 (NAVD88)



## LAKE MERRITT BART DEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

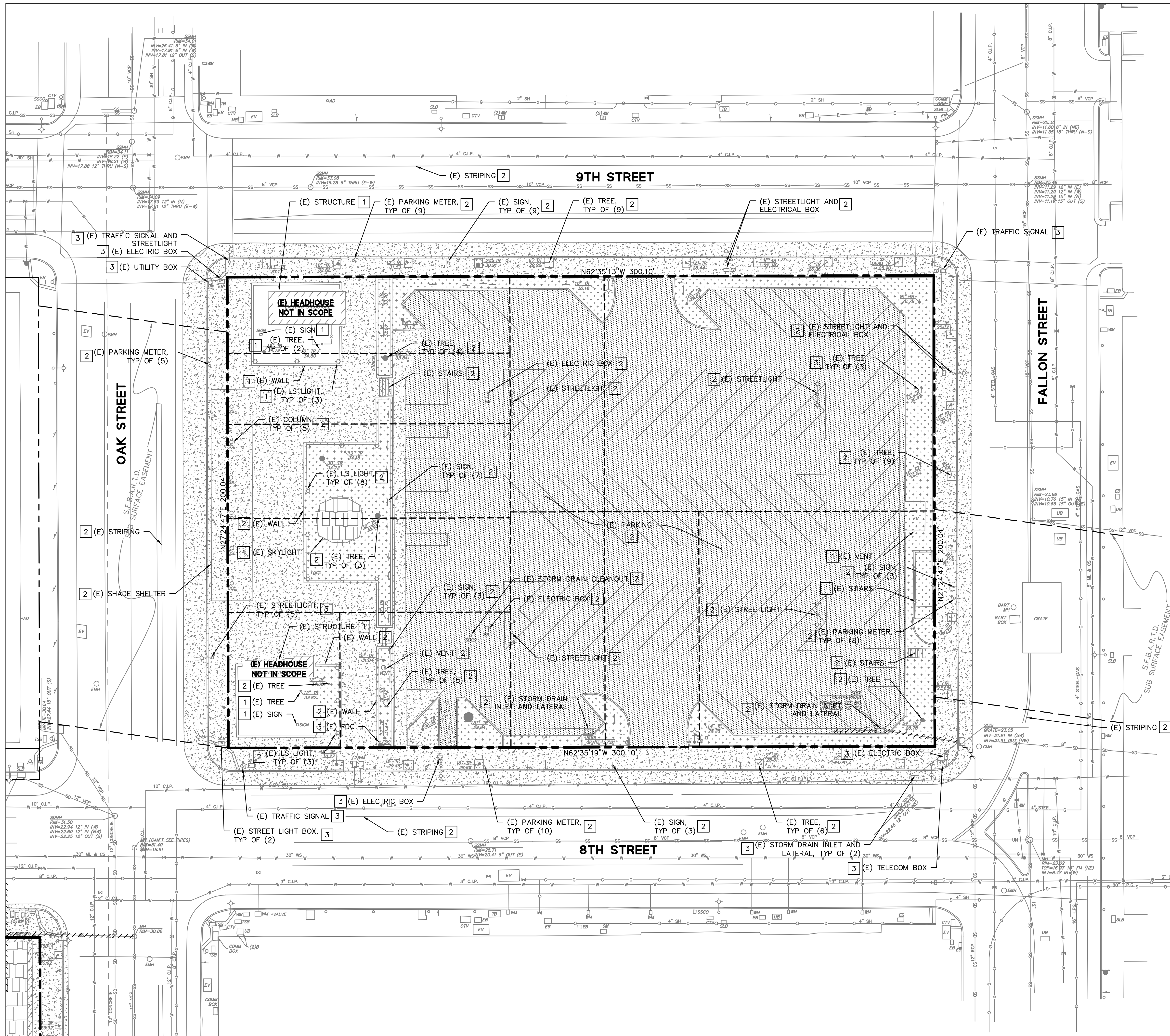
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
 DATE: 03/19/2021  
 SCALE: 1" = 20'

EXISTING CONDITIONS (BLOCK 2)

# C2.2





**DEMOLITION LEGEND:**

- SAWCUT DEMO & REMOVE EXISTING ASPHALT PARKING LOT & FULL DEPTH AC
- SAWCUT DEMO & REMOVE EXISTING CONCRETE HARDSCAPE, INCLUDING SIDEWALK, CURB, & GUTTER
- EXISTING BUILDING TO BE DEMOLISHED
- EXISTING LANDSCAPE TO BE REMOVED
- SAWCUT LINE
- EXISTING UTILITY LINE TO BE ABANDONED/REMOVED AS NEEDED

**DEMOLITION KEYNOTES:**

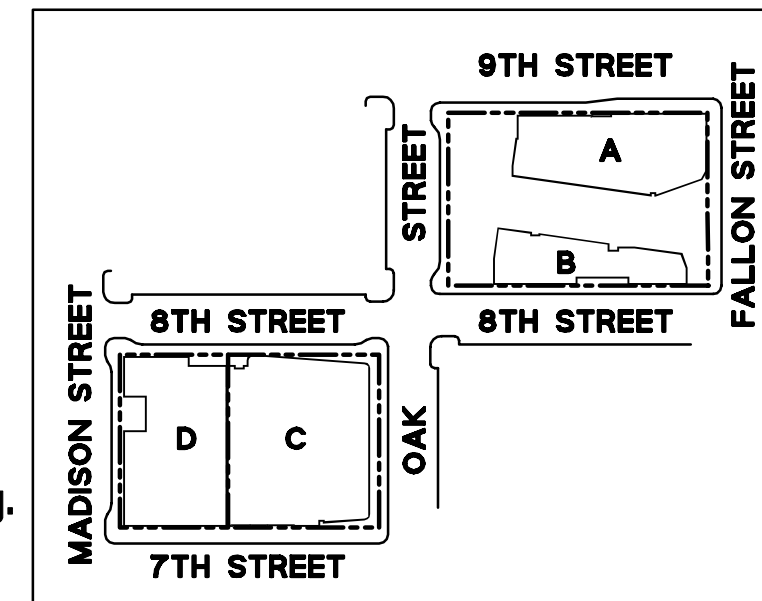
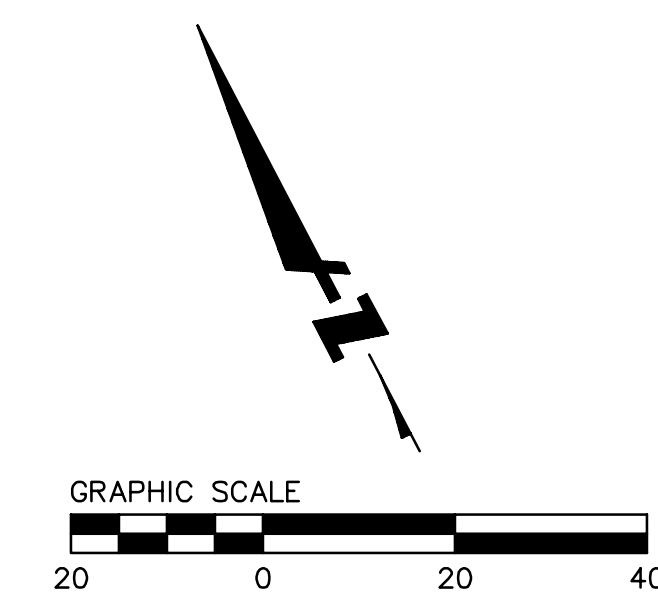
- 1 TO REMAIN, PROTECT IN PLACE
- 2 TO BE REMOVED
- 3 TO BE RELOCATED

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

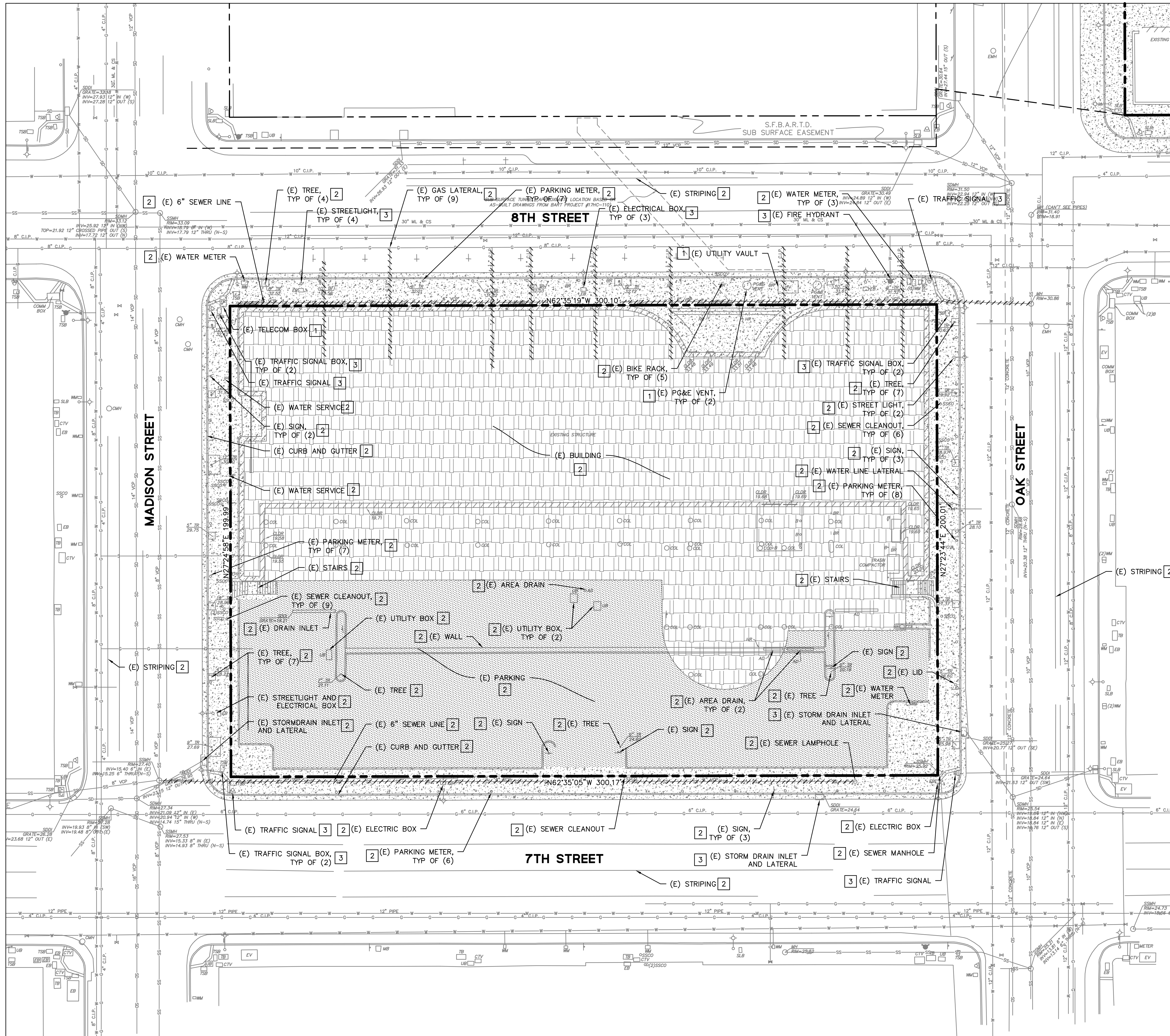
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7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021



JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'  
**PRELIMINARY DEMOLITION PLAN (BLOCK 1)**

**C3.1**





**DEMOLITION LEGEND:**

- SAWCUT DEMO & REMOVE EXISTING ASPHALT PARKING LOT & FULL DEPTH AC
- SAWCUT DEMO & REMOVE EXISTING CONCRETE HARDSCAPE, INCLUDING SIDEWALK, CURB, & GUTTER
- EXISTING BUILDING TO BE DEMOLISHED
- EXISTING LANDSCAPE TO BE REMOVED
- SAWCUT LINE
- EXISTING UTILITY LINE TO BE ABANDONED/REMOVED AS NEEDED

**DEMOLITION KEYNOTES:**

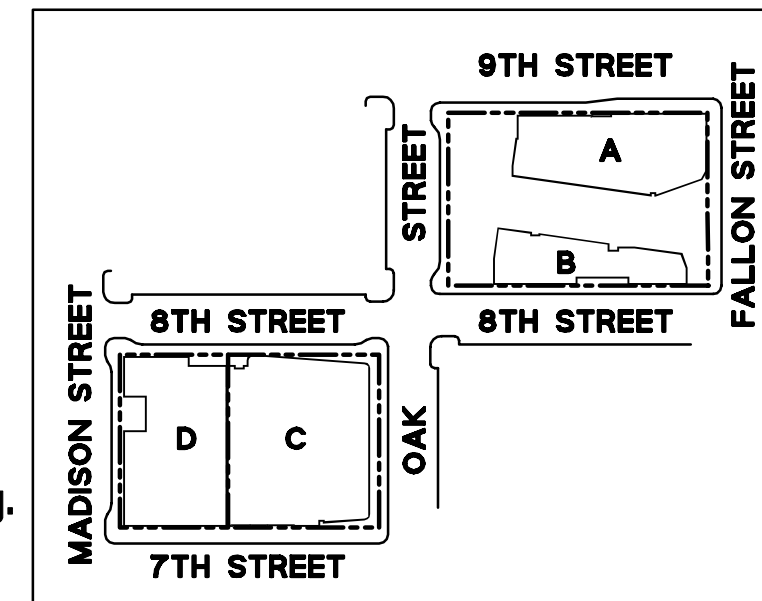
- 1** TO REMAIN, PROTECT IN PLACE
- 2** TO BE REMOVED
- 3** TO BE RELOCATED

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

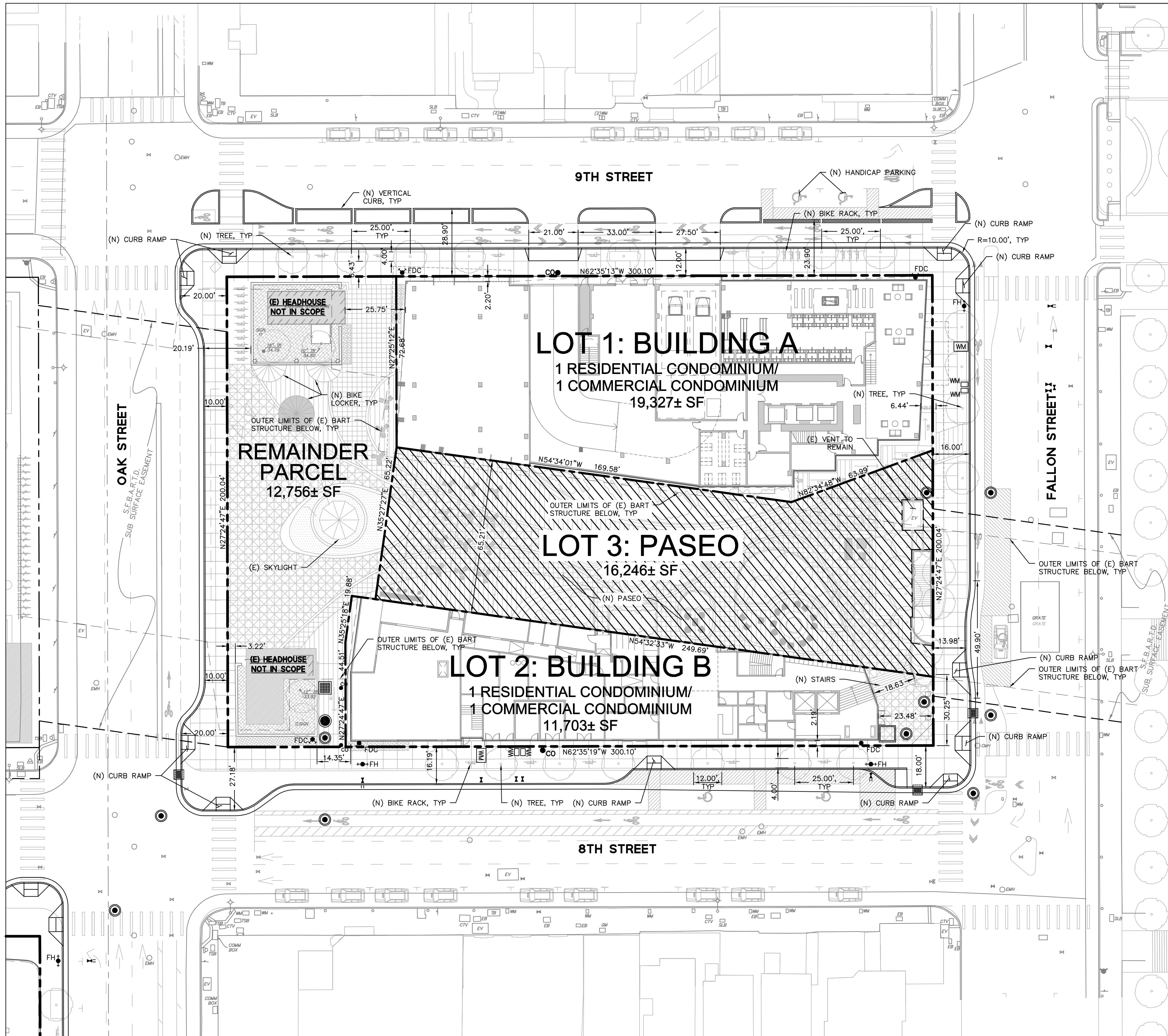
NO.	ISSUE	DATE
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JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'  
**PRELIMINARY DEMOLITION PLAN (BLOCK 2)**

**C3.2**





**SITE PLAN NOTES:**

1. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
2. SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
3. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
4. ALL PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS.
5. COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
6. SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
7. FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
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OAKLAND, CA 94612

**STRADA**  
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www.pyatok.com

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(650) 462-0300  
www.bkf.com

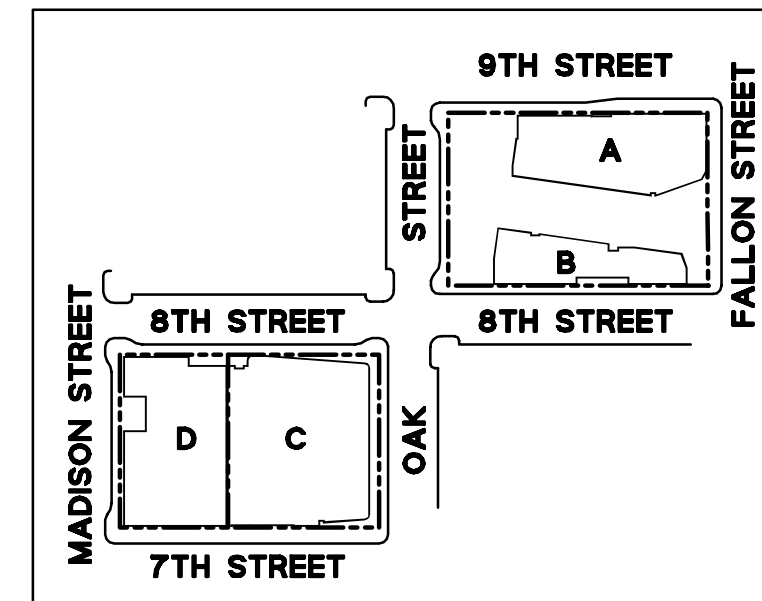
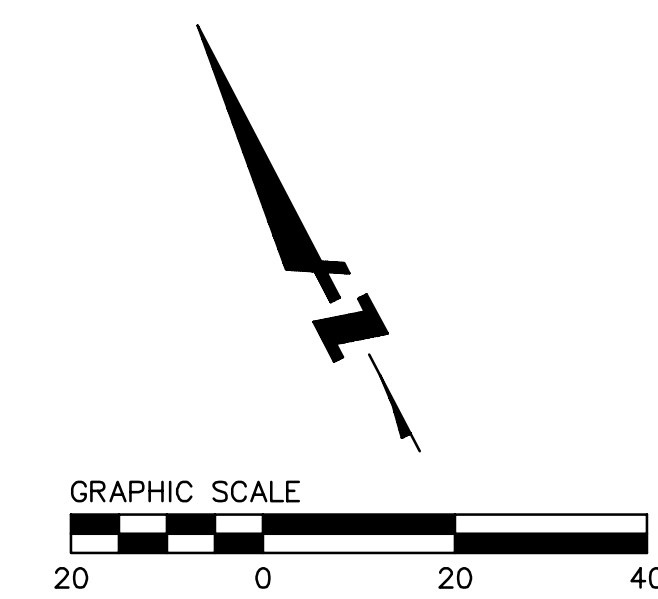
**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

**REVISION SCHEDULE**

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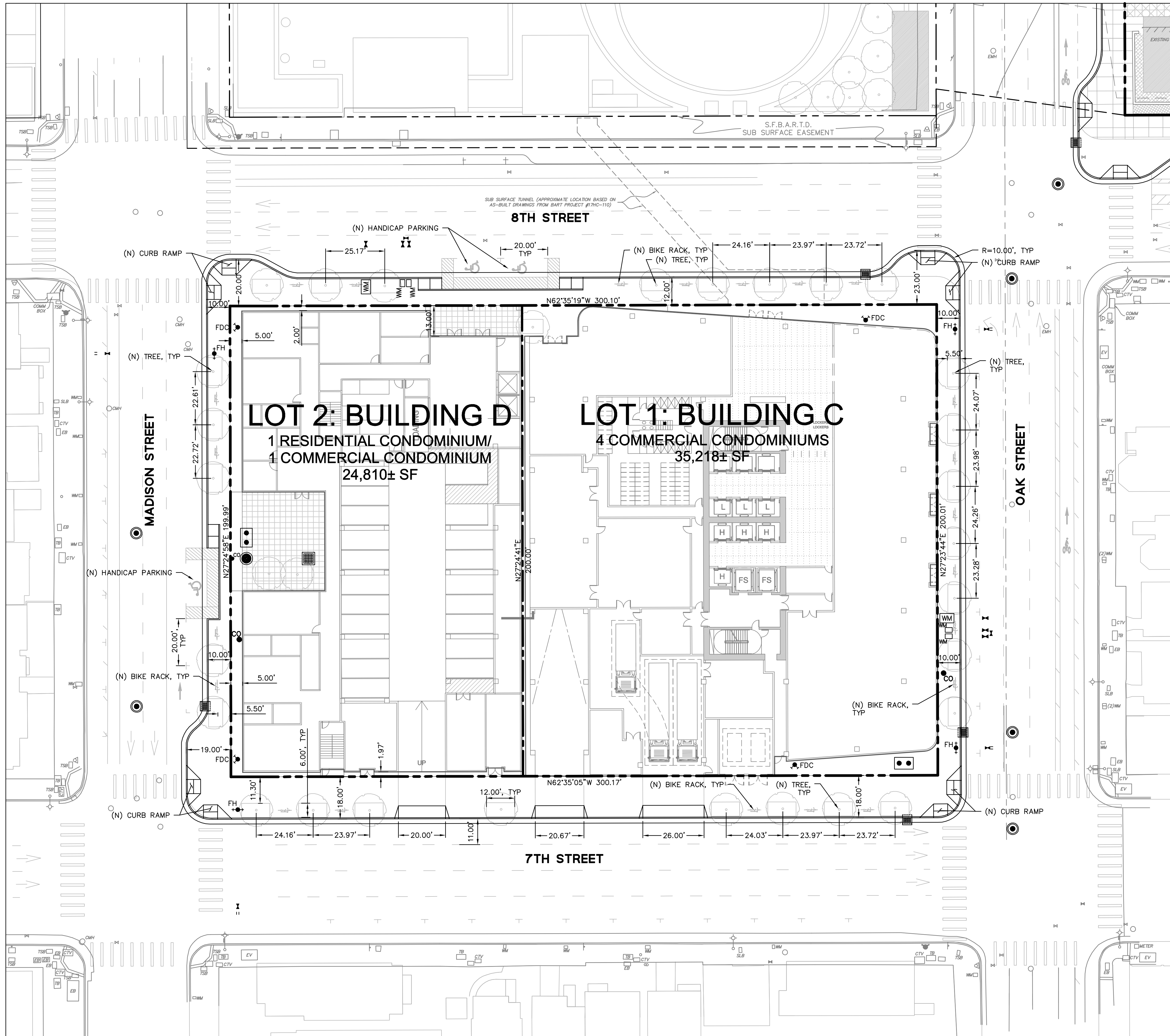


JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'

**PRELIMINARY SITE PLAN (BLOCK 1)**

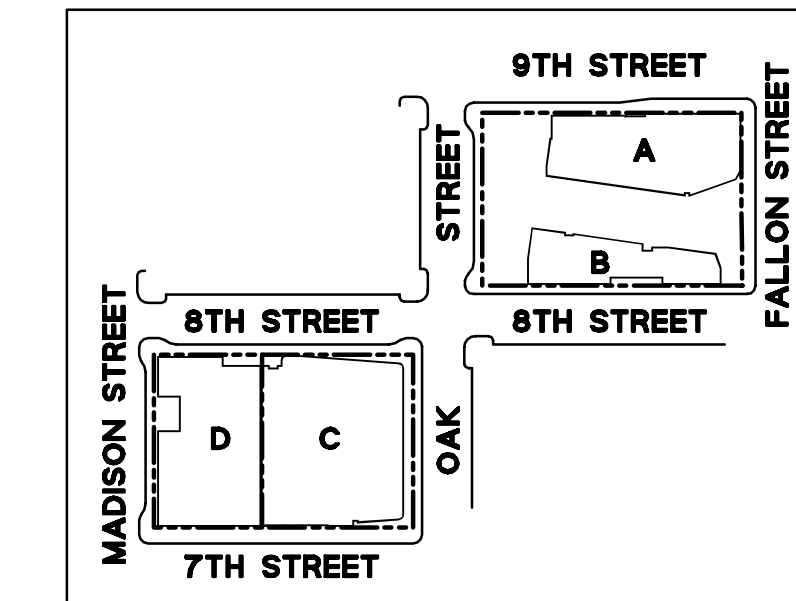
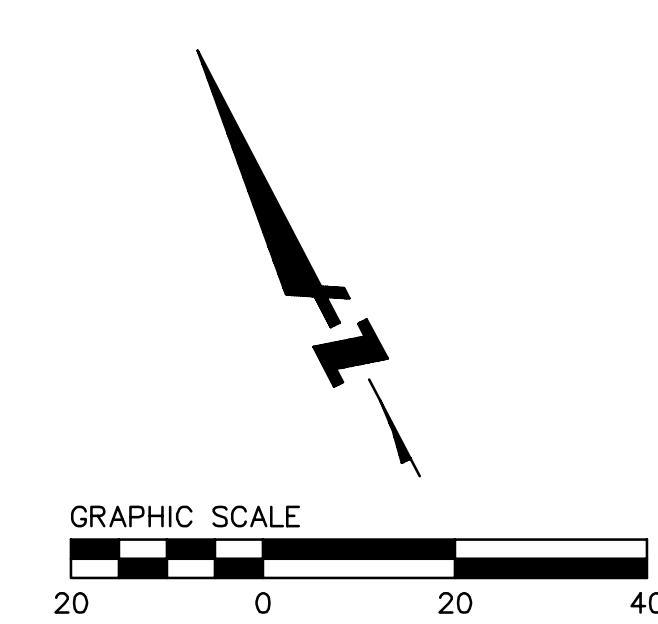
**C4.1**





- SITE PLAN NOTES:**
1. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
  2. SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
  3. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
  4. ALL PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS.
  5. COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
  6. SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
  7. FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

- LEGEND:**
- BOUNDARY LINE
  - LOT LINE
  - - - EASEMENT LINE
  - ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE



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**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

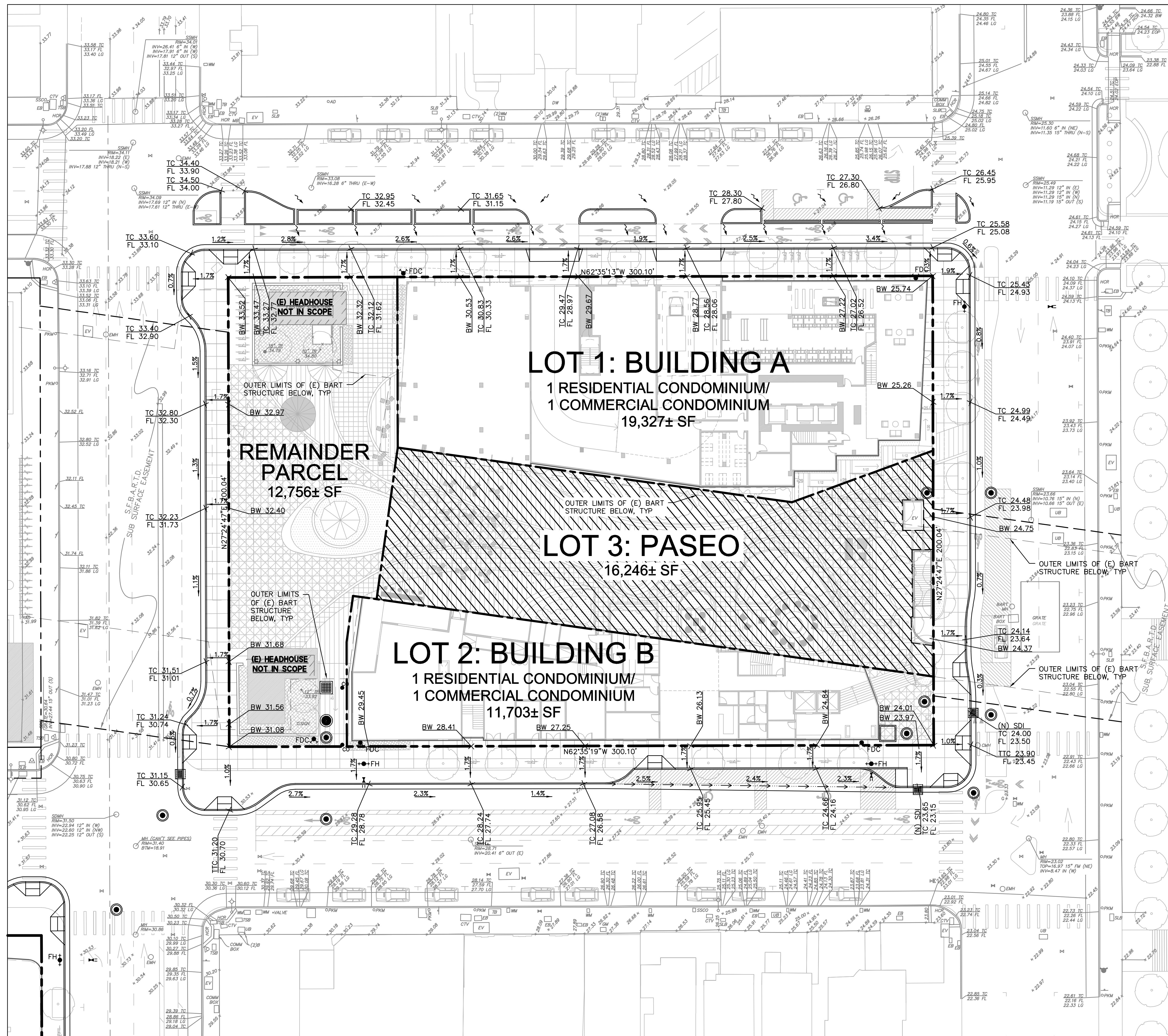
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JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'

**PRELIMINARY SITE PLAN (BLOCK 2)**

**C4.2**



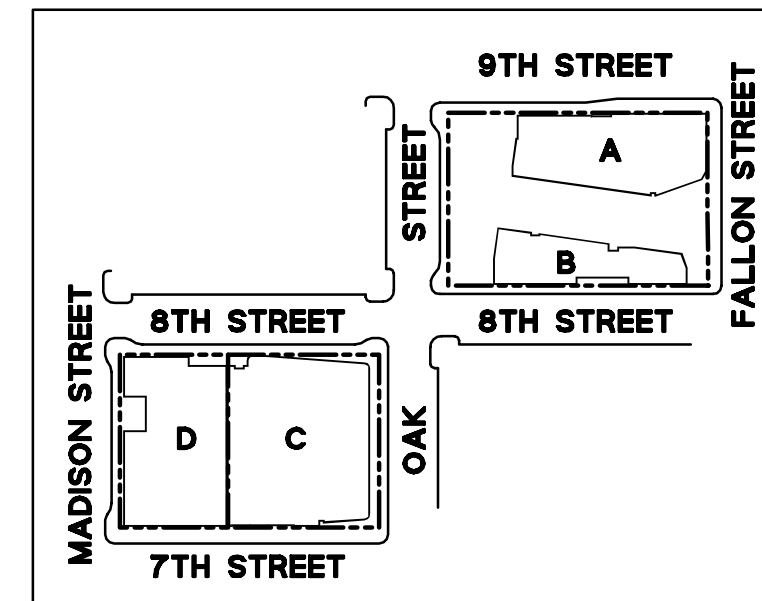
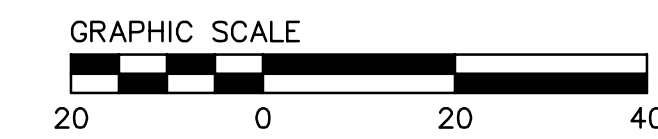


**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05', HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- - - EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE



**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

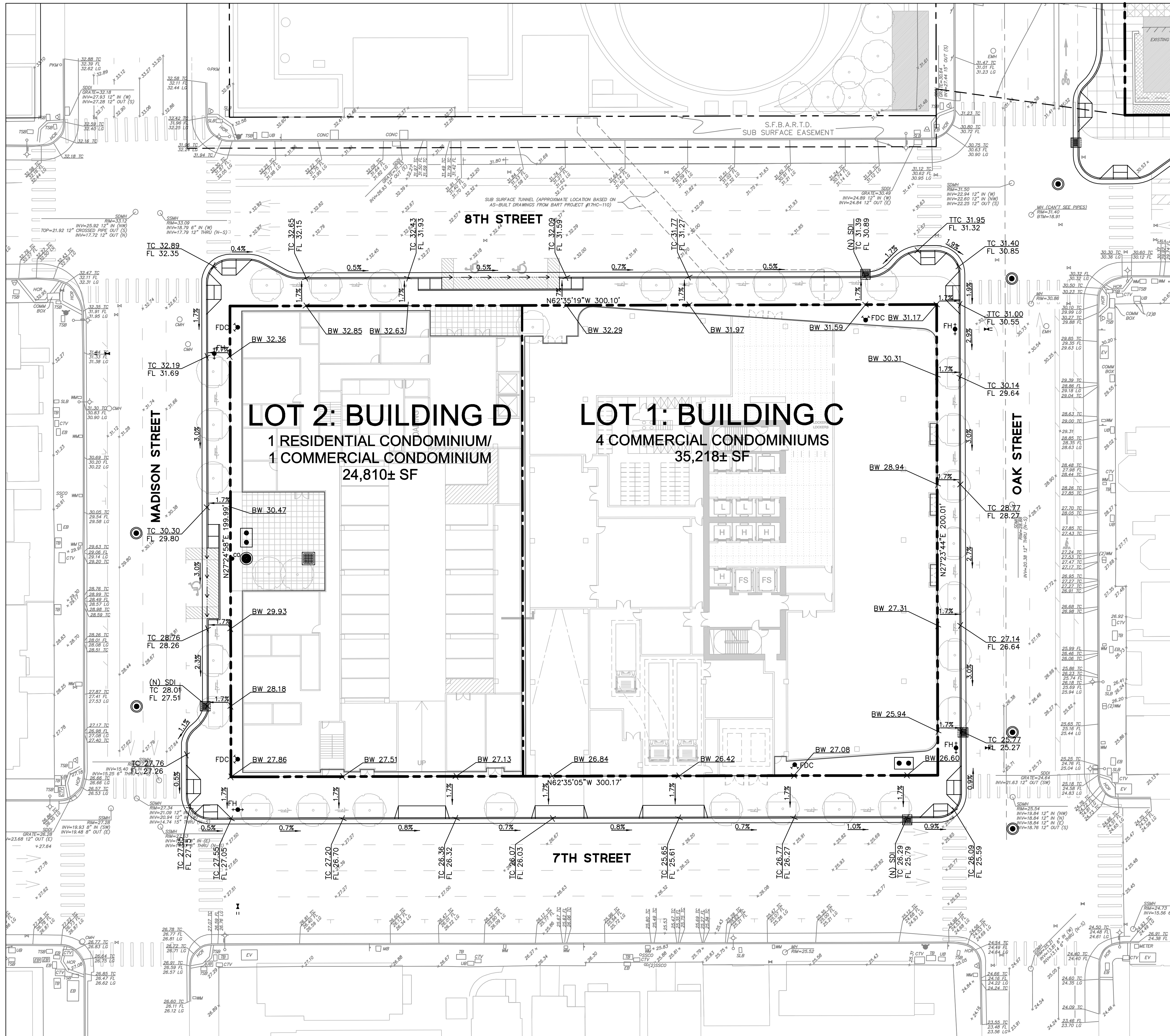
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JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'

**PRELIMINARY GRADING PLAN (BLOCK 1)**

**C5.1**





**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
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5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05', HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
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**LEGEND:**

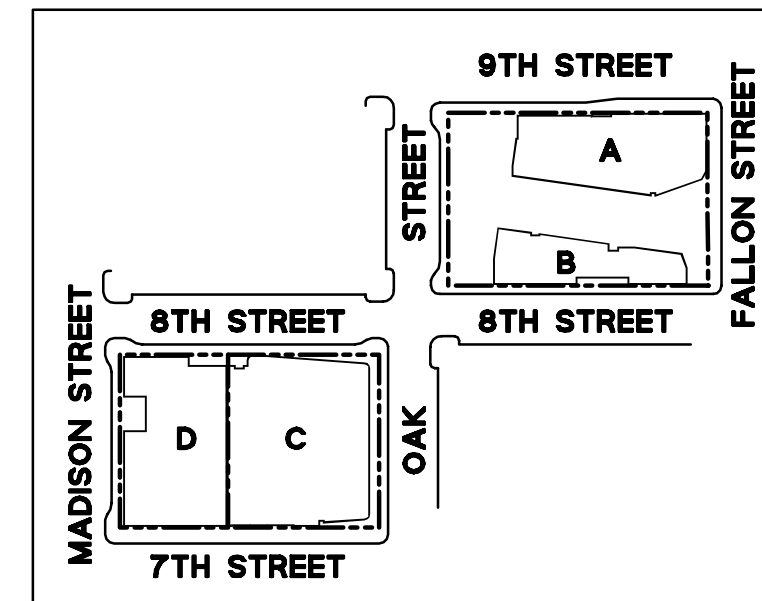
- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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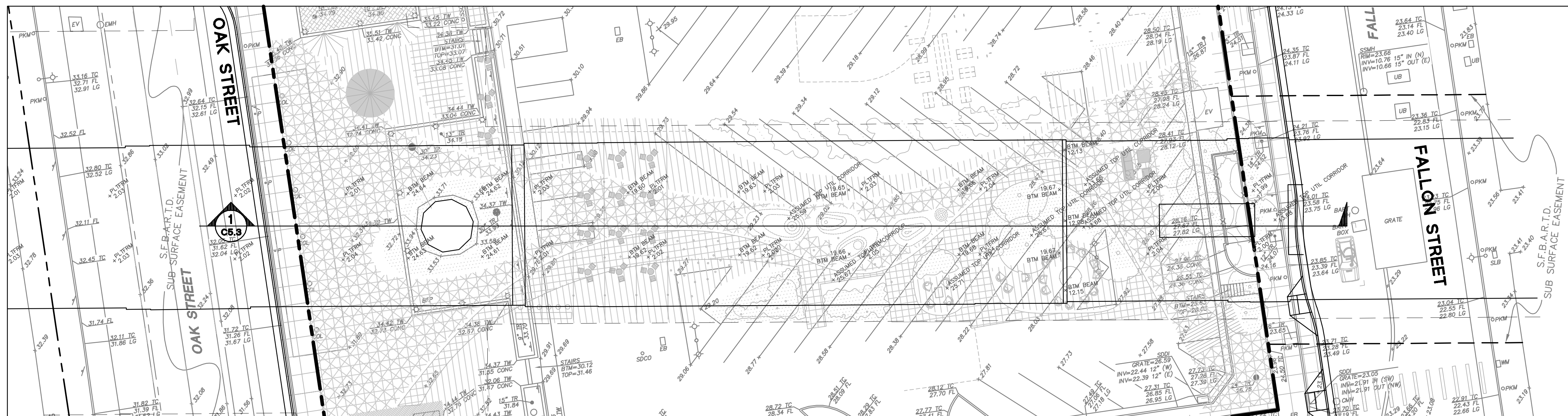


JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'

**PRELIMINARY GRADING PLAN (BLOCK 2)**

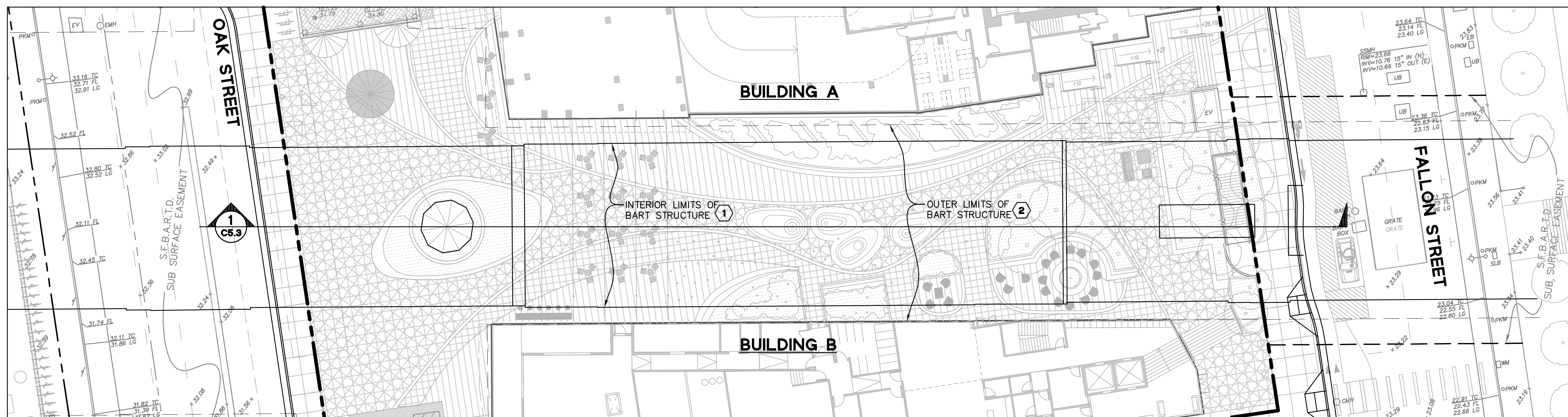
**C5.2**





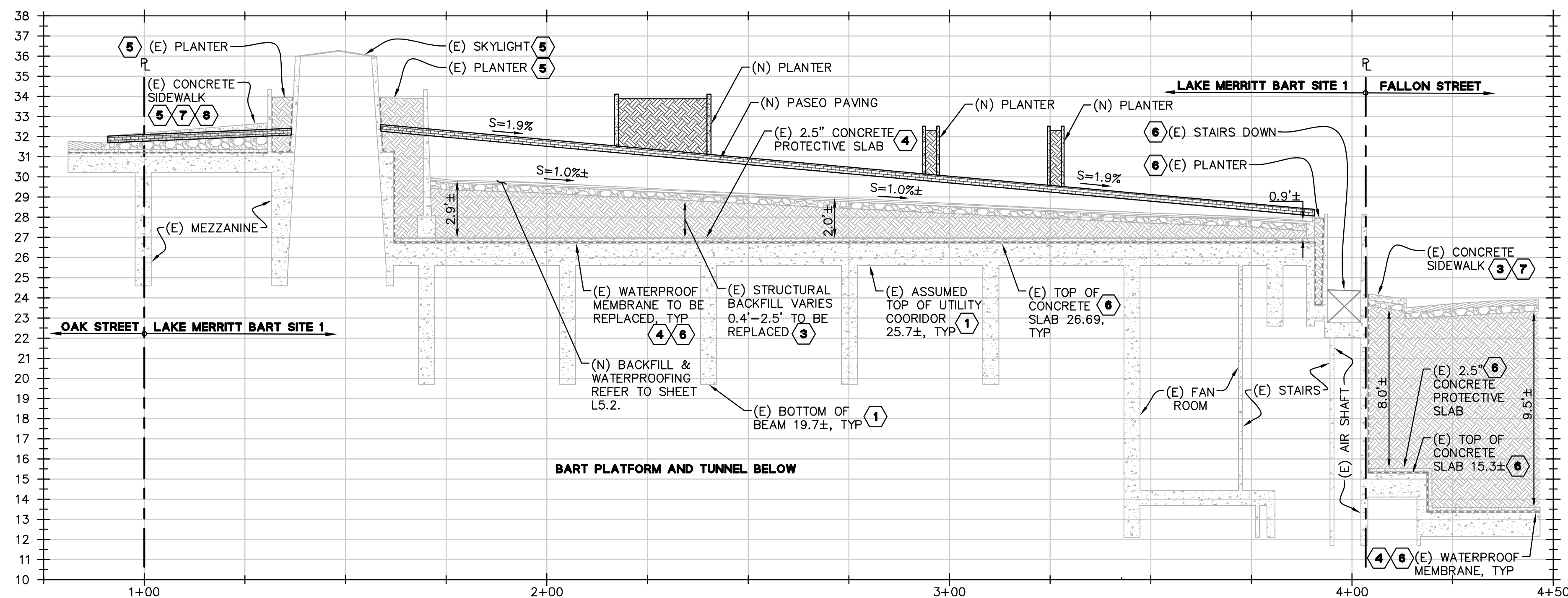
**EXISTING PLAN VIEW**

SCALE: 1" = 20'



**PROPOSED PLAN VIEW**

SCALE: 1" = 20'



**LAKE MERRITT BART TUNNEL PROFILE**

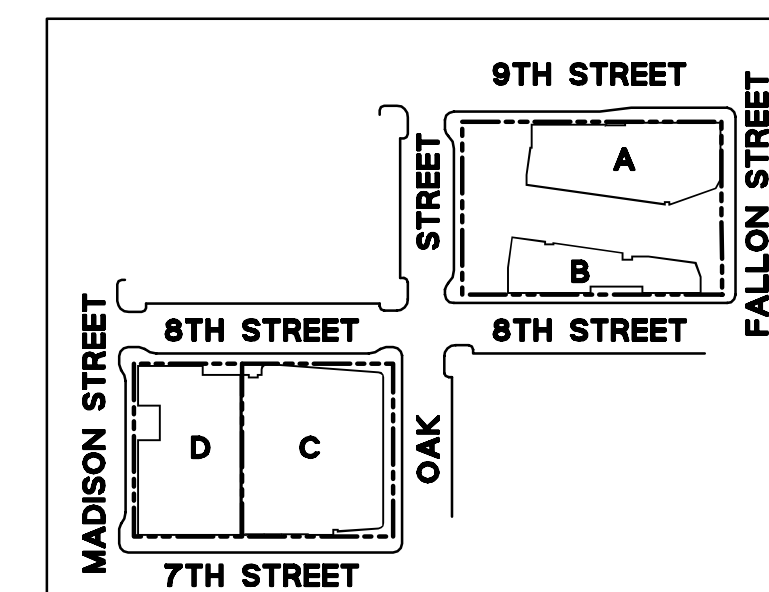
HORZ SCALE: 1" = 20' - VERT SCALE: 1" = 4'

**NOTES:**

- REFER TO THE KEYNOTES BELOW THAT REFERENCE VARIOUS BART AS-BUILT RECORD DRAWINGS USED TO CREATE THESE PROFILES AND SECTIONS. NOTE THAT ANY ELEVATIONS TAKEN FROM THESE AS-BUILT PLANS HAVE BEEN INCREASED BY 2.69' TO CORRELATE WITH THE NAVD88 SURVEY COORDINATE SYSTEM.

**KEYNOTES:**

- INTERIOR TUNNEL LIMITS OBTAINED FROM SCAN INFORMATION COLLECTED ON 9/15/19, PER BKF "UNDERGROUND BART STATION EXHIBIT", DATED 9/23/19
- EXTERIOR TUNNEL LIMITS WERE DETERMINED BY OFFSETTING THE CALCULATED, BEST-FIT CENTERLINE OF THE PLATFORM BY 31.33 FT, PER BKF "UNDERGROUND BART STATION EXHIBIT" DATED 9/23/19
- REFER TO SHEET CT5-2 FROM BART CONTRACT #K0071-K007
- REFER TO SHEET AR61-1 FROM BART CONTRACT #K0071-K007
- REFER TO SHEET AR29-1 FROM BART CONTRACT #K0071-K007
- REFER TO SHEET AR30-1 FROM BART CONTRACT #K0071-K007
- REFER TO SHEET CT3-2 FROM BART CONTRACT #K0071-K007
- REFER TO SHEET CT9-1 FROM BART CONTRACT #K0071-K007



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Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

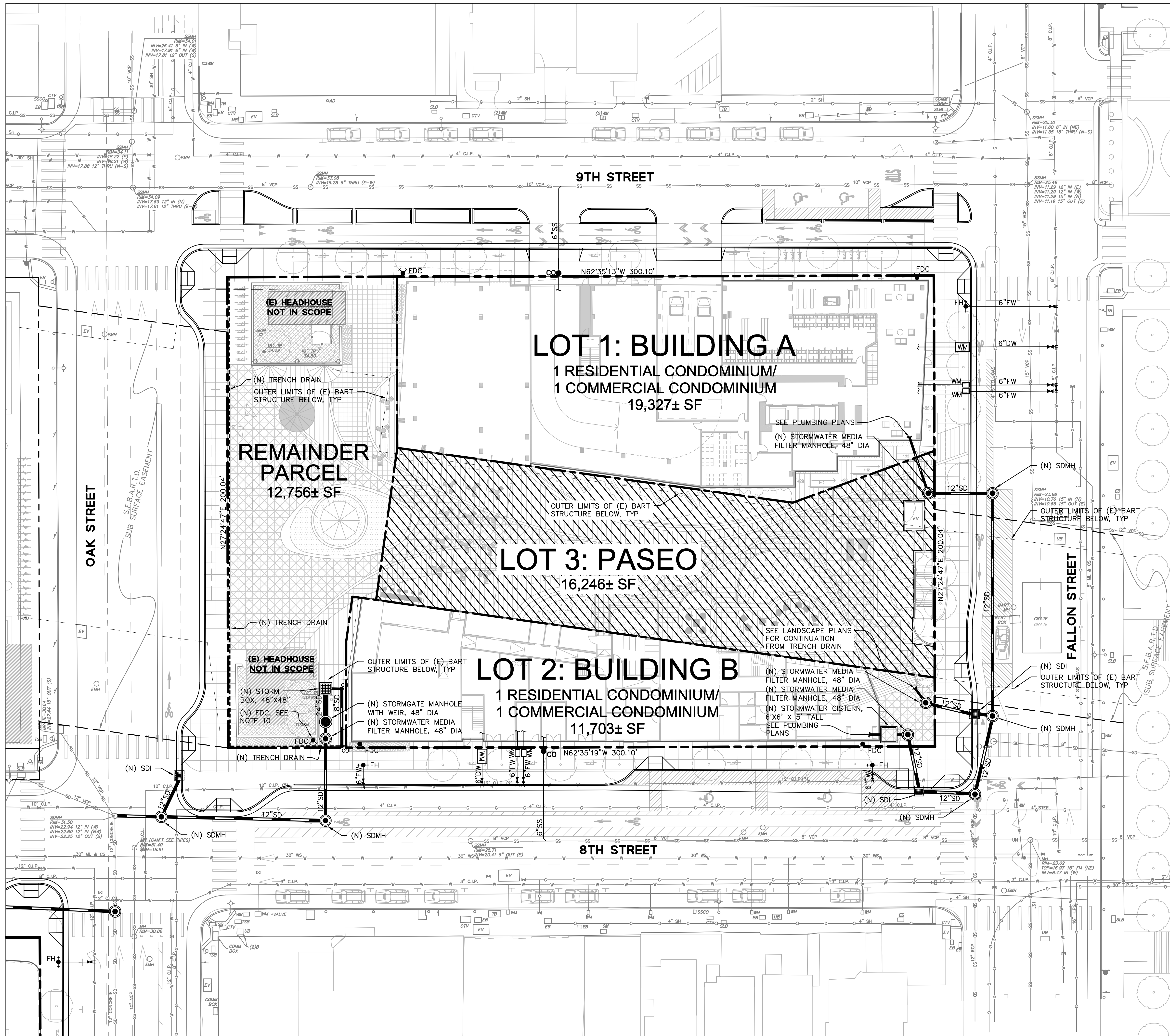
REVISION SCHEDULE		
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JOB NUMBER: 20190110  
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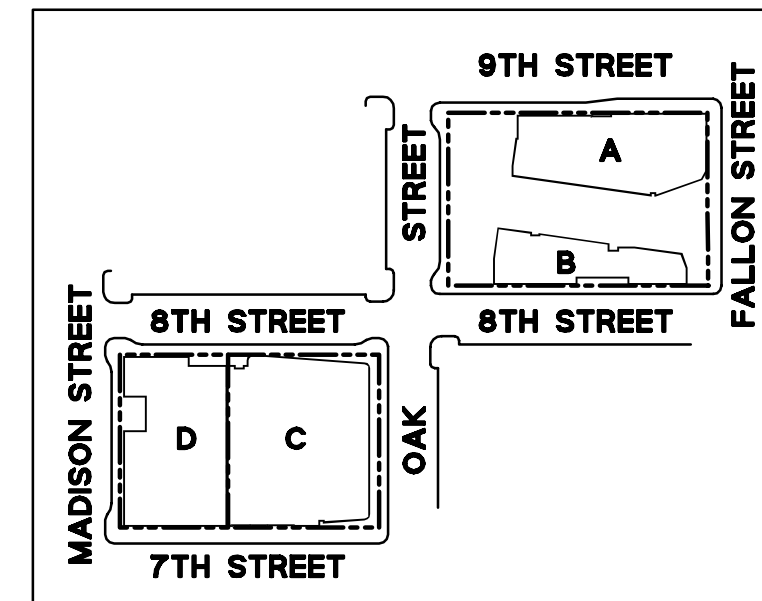
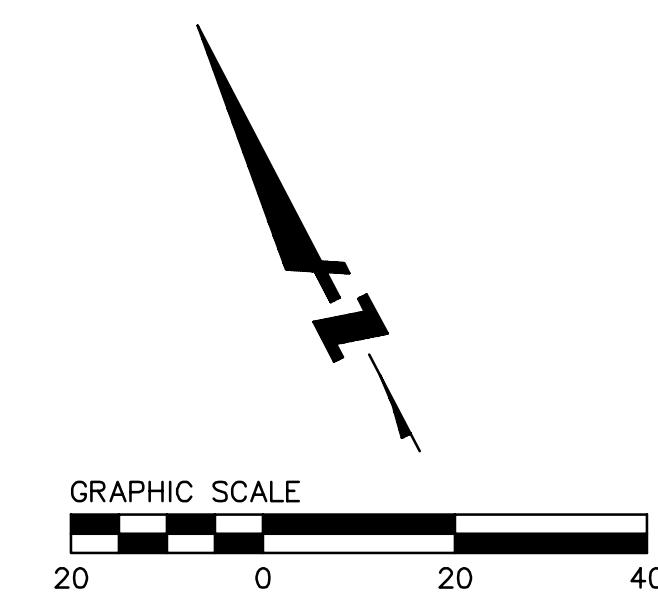
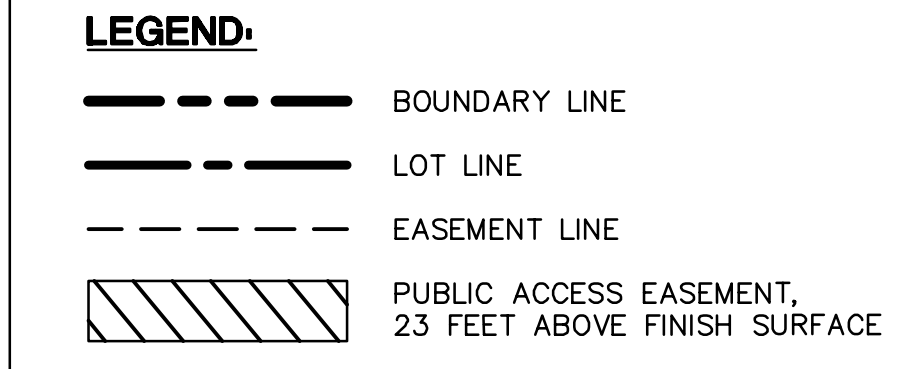
**PRELIMINARY SECTIONS**

**C5.3**





- UTILITY NOTES:**
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  - DOMESTIC WATER, FIRE WATER, AND SANITARY SEWER LATERAL SIZES TO BE CONFIRMED DURING THE DETAILED DESIGN PHASE.
  - CONTRACTOR SHALL COORDINATE GRAVITY UTILITY WORK WITH ALL JOINT TRENCH/RULE 20 UNDERGROUNDING WORK. IF JOINT TRENCH SCOPE OF WORK IS TO BE PERFORMED FIRST, CONTRACTOR SHALL STAKE LOCATIONS AND ELEVATIONS OF ALL PROPOSED GRAVITY UTILITY CROSSINGS. JOINT TRENCH TO BE INSTALLED WITH MINIMUM 12" VERTICAL CLEARANCE TO PROPOSED GRAVITY UTILITY AT ALL CROSSINGS.
  - ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
  - ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
  - ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
  - FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POTHOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR CROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
  - DRAINS SHOWN ON CIVIL PLANS ARE NOT INTENDED TO BE THE FINAL NUMBER AND LOCATION OF ALL DRAINS, PLACEMENT AND NUMBER OF LANDSCAPING DRAINS ARE HIGHLY DEPENDENT ON GROUND COVER TYPE AND PLANT MATERIAL. CONTRACTOR SHALL ADD ADDITIONAL AREA DRAINS AS NEEDED AND AS DIRECTED BY THE LANDSCAPE ARCHITECT OR CIVIL ENGINEER.
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  - THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.



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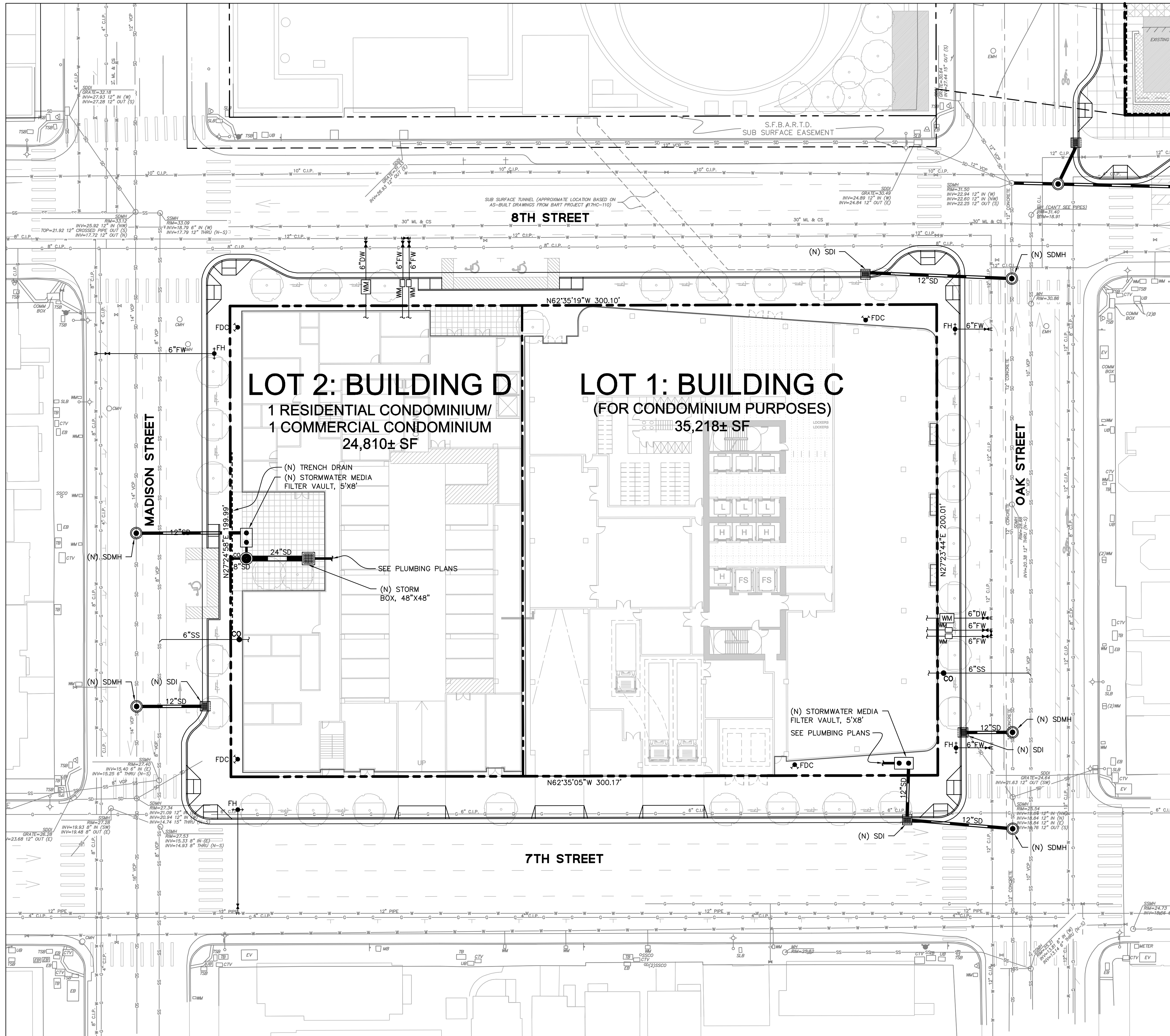
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**PRELIMINARY UTILITY PLAN (BLOCK 1)**

**C6.1**



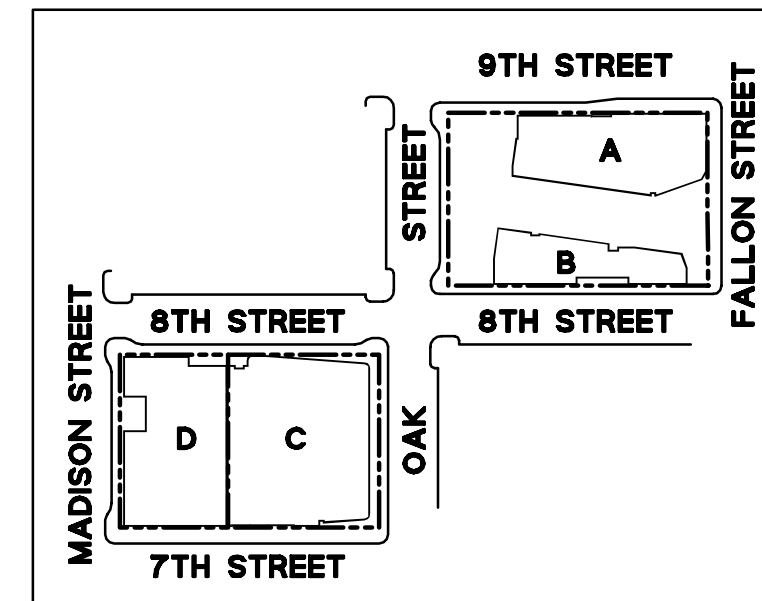
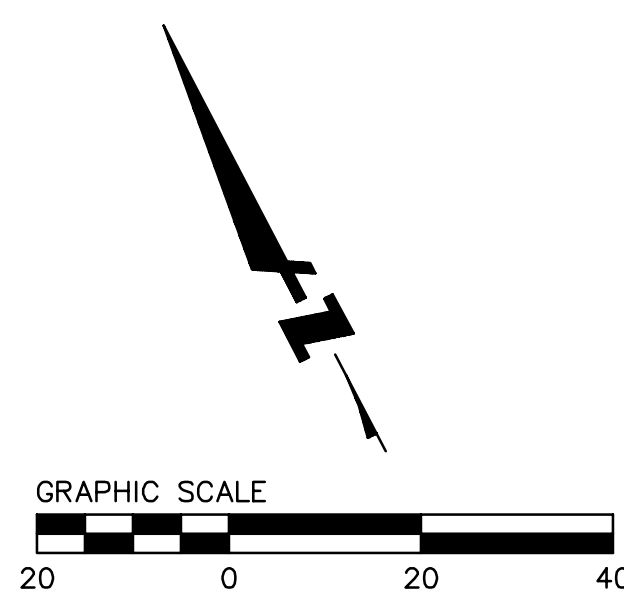


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**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE



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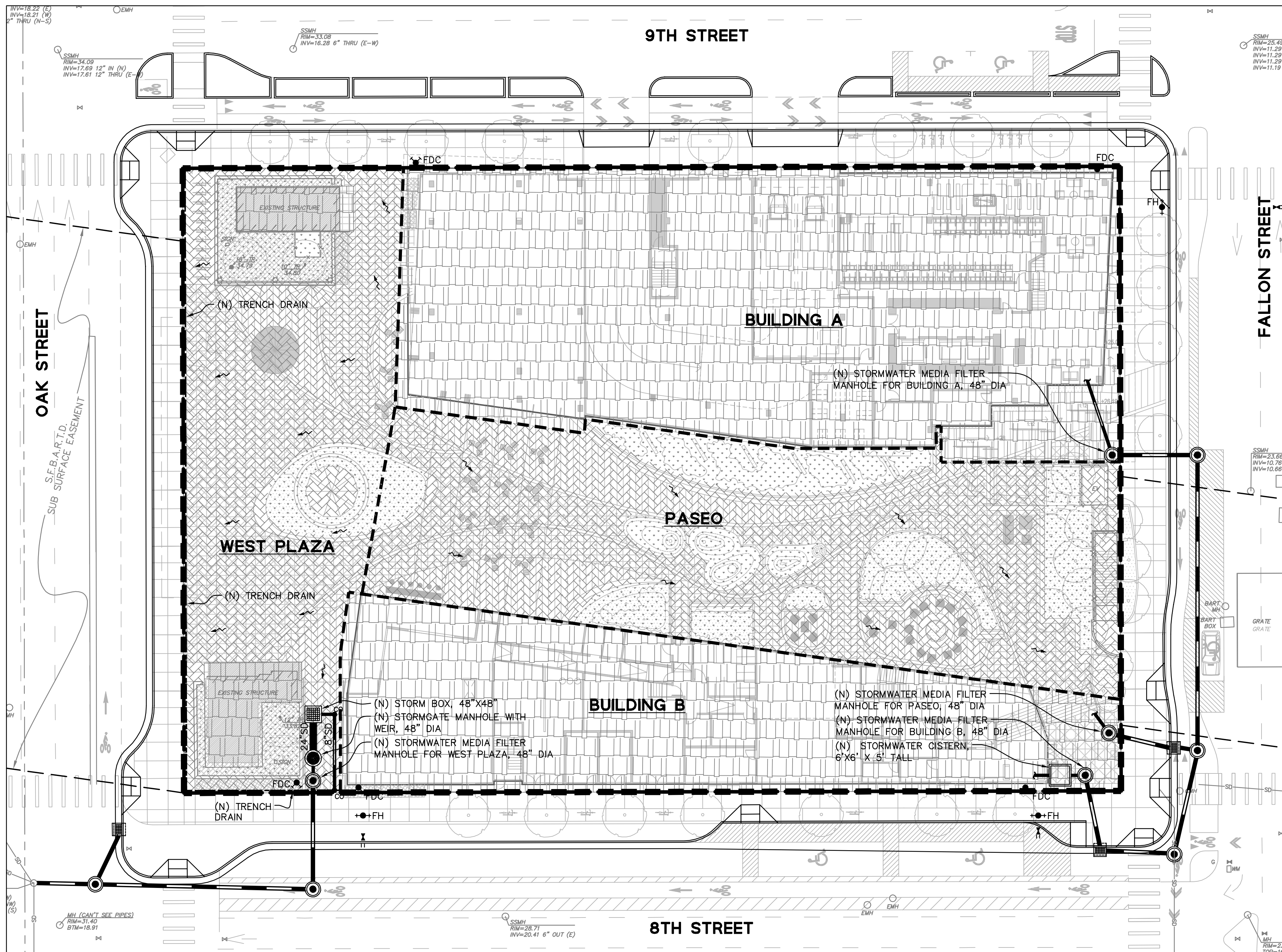
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**PRELIMINARY UTILITY PLAN (BLOCK 2)**

**C6.2**





**STORMWATER COMPLIANCE DATA (BLOCK 1)**

PER THE MUNICIPAL REGIONAL STORMWATER PERMIT ORDER NO. R2-0074, CERTAIN DEVELOPMENT PROJECTS THAT QUALIFY AS "SPECIAL PROJECTS" ARE ELIGIBLE FOR LOW IMPACT DESIGN TREATMENT REDUCTION CREDITS. THE LID TREATMENT REDUCTION CREDIT IS THE MAXIMUM PERCENTAGE OF THE AMOUNT OF RUNOFF THAT MAY BE TREATED WITH EITHER TREE-BOX-TYPE HIGH FLOWRATE BIOFILTERS OR VAULT-BASED HIGH FLOWRATE MEDIA FILTERS. THIS PROJECT IS CLASSIFIED AS A CATEGORY C SPECIAL PROJECT (TRANSIT ORIENTED) AND QUALIFIES FOR A TOTAL LID TREATMENT REDUCTION CREDIT OF 100% AS DESCRIBED BELOW.

CRITERIA FOR CATEGORY C (TRANSIT ORIENTED DEVELOPMENT) SPECIAL PROJECTS TO BE CONSIDERED A CATEGORY C SPECIAL PROJECT, A PROVISION C.3 REGULATED PROJECT MUST MEET ALL OF THE FOLLOWING CRITERIA:

1. BE CHARACTERIZED AS A NON AUTO-RELATED LAND USE PROJECT. THAT IS, CATEGORY C SPECIFICALLY EXCLUDES ANY REGULATED PROJECT THAT IS A STAND-ALONE SURFACE PARKING LOT; CAR DEALERSHIP; AUTO AND TRUCK RENTAL FACILITY WITH ONSITE SURFACE STORAGE; FAST-FOOD RESTAURANT, BANK OR PHARMACY WITH DRIVE-THROUGH LANES; GAS STATION, CAR WASH, AUTO REPAIR AND SERVICE FACILITY; OR OTHER AUTO RELATED PROJECT UNRELATED TO THE CONCEPT OF TRANSIT-ORIENTED DEVELOPMENT.
2. IF A COMMERCIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1.
3. IF A RESIDENTIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST A DENSITY OF 25 DU/AC.
4. IF A MIXED-USE DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1 OR A DENSITY OF 25 DU/AC.

**100% LID TREATMENT REDUCTION CREDIT**

1. 50% REDUCTION CREDIT - PROJECT IS LOCATED WITHIN A 1/4 MILE RADIUS OF A TRANSIT HUB
2. 30% REDUCTION CREDIT - MIXED USE PROJECT WITH DENSITY GREATER THAN 100 DU/ACRE
3. 20% REDUCTION CREDIT - 0% OF TOTAL POST-PROJECT IMPERVIOUS SURFACE IS DEDICATED TO AT-GRADE, SURFACE PARKING

**STORMWATER LID TREATMENT SIZING**

TOTAL LID TREATMENT REDUCTION CREDIT = 100%

TOTAL BLOCK 1 IMPERVIOUS AREA OF 56,508 SF ALLOWED TO BE TREATED W/ NON-LID TREATMENT MEASURES (MEDIA FILTER UNITS)

**BUILDING A**

$Q_{c.3} = C_{iA} = (0.9) * (0.2 \text{ IN/HR}) * (20,334 \text{ SF})$   
 $Q_{c.3} = 0.0840 \text{ CFS}$

A 48" MEDIA FILTER MANHOLE WITH (2) 12"x12" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.11 CFS.

**BUILDING B**

$Q_{c.3} = C_{iA} = (0.9) * (0.2 \text{ IN/HR}) * (11,486 \text{ SF})$   
 $Q_{c.3} = 0.0475 \text{ CFS}$

A 48" MEDIA FILTER MANHOLE WITH (1) 12"x12" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.05 CFS.

**PASEO**

$Q_{c.3} = C_{iA} = (0.9) * (0.2 \text{ IN/HR}) * (13,117 \text{ SF})$   
 $Q_{c.3} = 0.0542 \text{ CFS}$

A 48" MEDIA FILTER MANHOLE WITH (1) 12"x18" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.07 CFS.

**WEST PLAZA**

$Q_{c.3} = C_{iA} = (0.9) * (0.2 \text{ IN/HR}) * (11,534 \text{ SF})$   
 $Q_{c.3} = 0.0477 \text{ CFS}$

A 48" MEDIA FILTER MANHOLE WITH (2) 12" CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.06 CFS.

**STORMWATER AREA SUMMARY**

THE CITY OF OAKLAND STORM DRAINAGE DESIGN GUIDELINES ESTABLISHES A 25% GOAL FOR PEAK FLOW REDUCTION COMPARED TO EXISTING CONDITIONS, TO THE EXTENT POSSIBLE. DUE TO THE FACT THAT ALMOST 90% OF THE SITE IS COVERED BY PERMANENT STRUCTURE, INCLUDING THE EXISTING BART TUNNEL, THERE IS LIMITED SPACE FOR DETENTION MEASURES ONSITE. AS A RESULT, A 25% PEAK FLOW REDUCTION WAS ACCOMPLISHED IN ALL AREAS EXCEPT FOR THE PASEO.

**BUILDING A**

EXISTING IMPERVIOUS SURFACE	17,982 SF
EXISTING PERVIOUS SURFACE	2,352 SF
<b>TOTAL</b>	<b>20,334 SF</b>

**BUILDING B**

PROPOSED IMPERVIOUS SURFACE	20,334 SF
PROPOSED PERVIOUS SURFACE	0 SF
<b>TOTAL</b>	<b>20,334 SF</b>

**BUILDING B**

EXISTING IMPERVIOUS SURFACE	9,854 SF
EXISTING PERVIOUS SURFACE	1,632 SF
<b>TOTAL</b>	<b>11,486 SF</b>

**PASEO**

PROPOSED IMPERVIOUS SURFACE	11,486 SF
PROPOSED PERVIOUS SURFACE	0 SF
<b>TOTAL</b>	<b>11,486 SF</b>

**PASEO**

EXISTING IMPERVIOUS SURFACE	14,331 SF
EXISTING PERVIOUS SURFACE	998 SF
<b>TOTAL</b>	<b>15,329 SF</b>

**WEST PLAZA**

PROPOSED IMPERVIOUS SURFACE	13,117 SF
PROPOSED PERVIOUS SURFACE	2,212 SF
<b>TOTAL</b>	<b>15,329 SF</b>

**WEST PLAZA**

EXISTING IMPERVIOUS SURFACE	9,392 SF
EXISTING PERVIOUS SURFACE	3,476 SF
<b>TOTAL</b>	<b>12,868 SF</b>

**WEST PLAZA**

PROPOSED IMPERVIOUS SURFACE	11,534 SF
PROPOSED PERVIOUS SURFACE	1,334 SF
<b>TOTAL</b>	<b>12,868 SF</b>

**STORMWATER PEAK FLOW REDUCTION**

PEAK FLOW CALCULATED PER CITY OF OAKLAND STORM DRAINAGE DESIGN STANDARDS.

DESIGN STORM = 10-YEAR MEAN ANNUAL PRECIPITATION (MAP) = 21 INCHES TIME OF CONCENTRATION = 5 MINUTES

**DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:**

**BUILDING A**

$Q_{EX-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(17,982 \text{ SF})$   
 $Q_{EX-10YR} = 1.293 \text{ CFS}$

$Q_{PR-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(20,334 \text{ SF})$   
 $Q_{PR-10YR} = 1.462 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((1.462/1.293)*100) = -13.1\%$

**BUILDING B**

$Q_{EX-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(9,854 \text{ SF})$   
 $Q_{EX-10YR} = 0.709 \text{ CFS}$

$Q_{PR-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(11,486 \text{ SF})$   
 $Q_{PR-10YR} = 0.826 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((0.826/0.709)*100) = -16.5\%$

**PASEO**

$Q_{EX-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(14,331 \text{ SF})$   
 $Q_{EX-10YR} = 1.030 \text{ CFS}$

$Q_{PR-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(13,117 \text{ SF})$   
 $Q_{PR-10YR} = 0.943 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((0.943/1.030)*100) = \pm 8.4\%$

THE PASEO IMPROVEMENTS PROVIDE AN 8.4% PEAK FLOW REDUCTION.

**WEST PLAZA**

$Q_{EX-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(9,392 \text{ SF})$   
 $Q_{EX-10YR} = 0.675 \text{ CFS}$

$Q_{PR-10YR} = C_{iA} = (0.90)(3.48 \text{ IN/HR})(11,534 \text{ SF})$   
 $Q_{PR-10YR} = 0.829 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((0.829/0.675)*100) = -5.3\%$

**DUE TO IMPLEMENTATION OF DETENTION TANK/CISTERN:**

**BUILDING A**

DETENTION VOLUME REQUIRED FOR 25%+13.1% REDUCTION  
 $V_{DET \text{ REQD}} (CF) = (0.25+0.131) * (Q_{EX-10YR}) * T_c$   
 $V_{DET \text{ REQD}} (CF) = (0.381) * (1.293 \text{ CFS}) * (5 \text{ MIN}) * (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQD}} (CF) = 221.7 \text{ CF OR } 1,661 \text{ GALLONS}$

DETENTION PIPE PROVIDED BY CISTERN IN BUILDING  
 $V_{DET \text{ PROVIDED}} = 7'X7' X 5' \text{ TALL CISTERN} = 245 \text{ CF}$

CISTERN TO BE LOCATED INSIDE THE BUILDING.

**BUILDING B**

DETENTION VOLUME REQUIRED FOR 25%+16.5% REDUCTION  
 $V_{DET \text{ REQD}} (CF) = (0.25+0.165) * (Q_{EX-10YR}) * T_c$   
 $V_{DET \text{ REQD}} (CF) = (0.415) * (0.709 \text{ CFS}) * (5 \text{ MIN}) * (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQD}} (CF) = 132.4 \text{ CF OR } 992 \text{ GALLONS}$

DETENTION PIPE PROVIDED BY CISTERN OUTSIDE THE BUILDING  
 $V_{DET \text{ PROVIDED}} = 6'X6' X 5' \text{ TALL CISTERN} = 180 \text{ CF}$

CISTERN TO BE LOCATED OUTSIDE THE BUILDING.

**DUE TO IMPLEMENTATION OF DETENTION PIPE:**

**WEST PLAZA**

DETENTION VOLUME REQUIRED FOR 27.6%+5.3% REDUCTION  
 $V_{DET \text{ REQD}} (CF) = (0.276+0.053) * (Q_{EX-10YR}) * T_c$   
 $V_{DET \text{ REQD}} (CF) = (0.329) * (0.675 \text{ CFS}) * (5 \text{ MIN}) * (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQD}} (CF) = 100.0 \text{ CF OR } 751 \text{ GALLONS}$

DETENTION PROVIDED BY 24" PIPE  
 $V_{DET \text{ PROVIDED}} = \pi * R^2 * \text{LENGTH} = \pi * (1.00FT)^2 * 8.7FT = 27 \text{ CF}$

DETENTION PROVIDED BY 8" PIPE  
 $V_{DET \text{ PROVIDED}} = \pi * R^2 * \text{LENGTH} = \pi * (0.33FT)^2 * 17FT = 9 \text{ CF}$

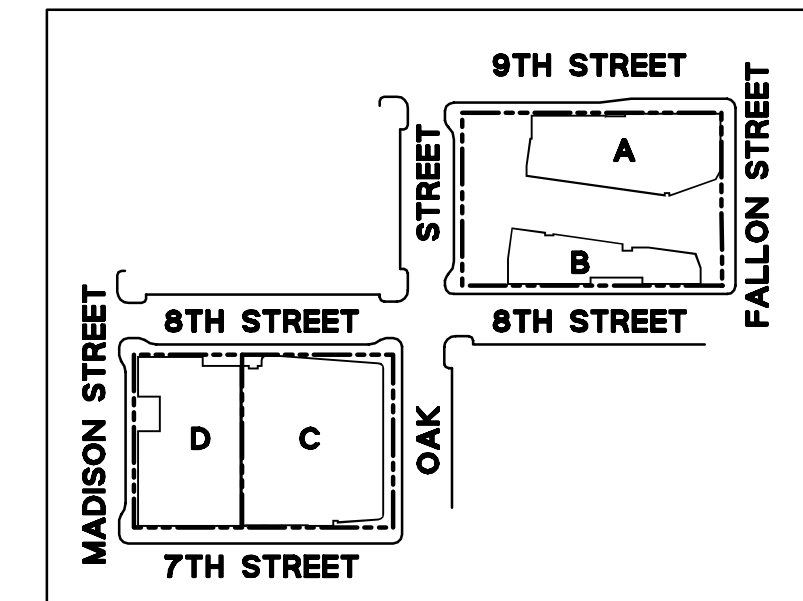
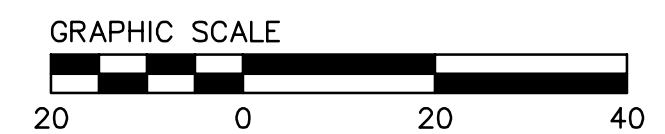
DETENTION PROVIDED BY 48" STORM BOX BASIN  
 $V_{DET \text{ PROVIDED}} = \text{LENGTH} * \text{WIDTH} * \text{HEIGHT} = 4.0FT * 4.0FT * 4.0FT = 64 \text{ CF}$

$V_{DET \text{ PROVIDED TOTAL}} = 100 \text{ CF}$

DETENTION PIPE TO BE LOCATED ADJACENT TO BUILDING B, IN THE WEST PLAZA.

**LEGEND**

- ● STORMWATER FILTER UNIT
- FLOW DIRECTION
- ▒ IMPERVIOUS ROOF AREAS
- ▒ IMPERVIOUS PLAZA/PODIUM AREAS
- ▒ PERVIOUS LANDSCAPE AREAS
- - - DRAINAGE AREA BOUNDARY



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**PRELIMINARY STORMWATER PLAN (BLOCK 1)**

**C7.1**



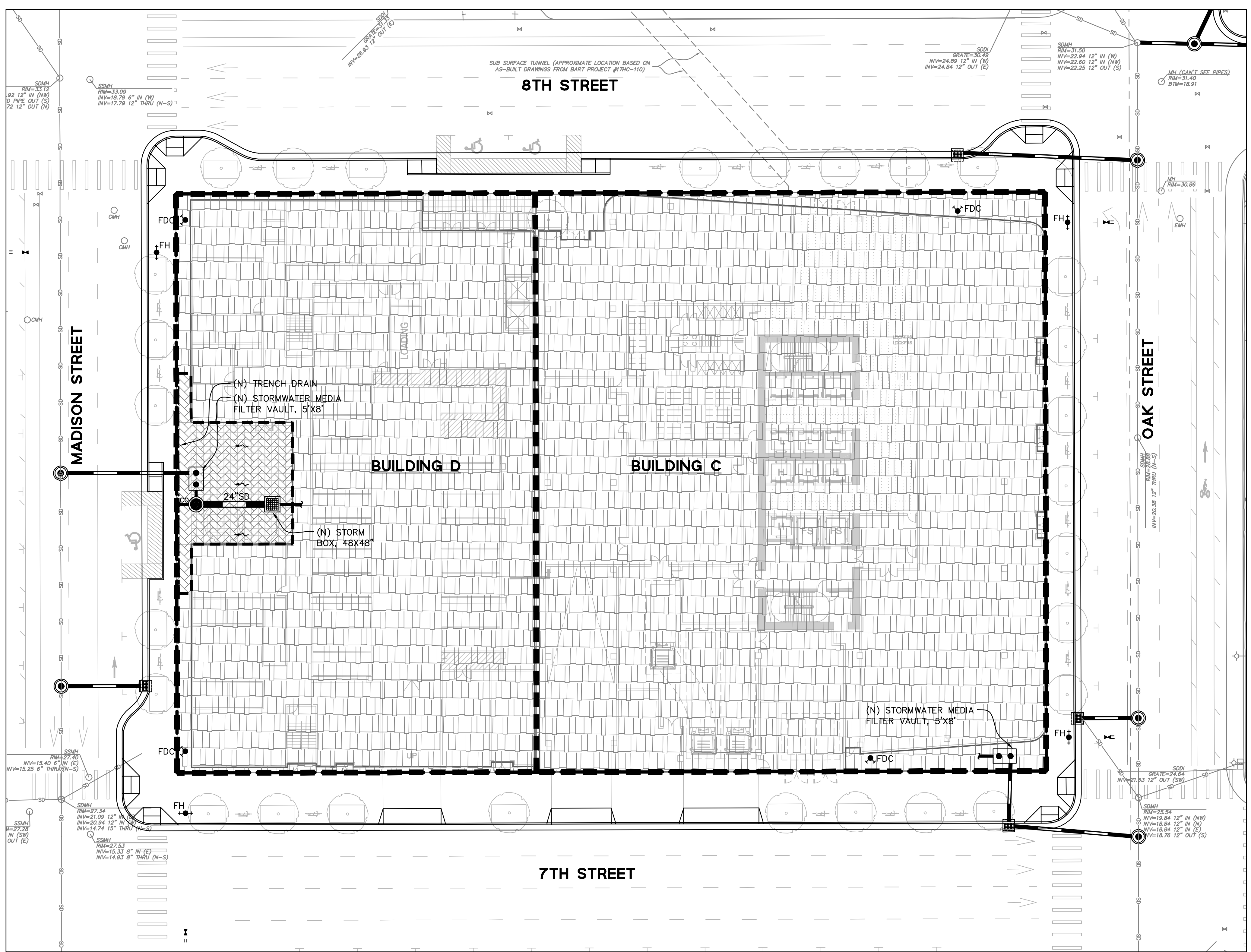
# LAKE MERRITT BART DEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021



### STORMWATER COMPLIANCE DATA (BLOCK 2)

PER THE MUNICIPAL REGIONAL STORMWATER PERMIT ORDER NO. R2-0074, CERTAIN DEVELOPMENT PROJECTS THAT QUALIFY AS "SPECIAL PROJECTS" ARE ELIGIBLE FOR LOW IMPACT DESIGN TREATMENT REDUCTION CREDITS. THE LID TREATMENT REDUCTION CREDIT IS THE MAXIMUM PERCENTAGE OF THE AMOUNT OF RUNOFF THAT MAY BE TREATED WITH EITHER TREE-BOX-TYPE HIGH FLOWRATE BIOFILTERS OR VAULT-BASED HIGH FLOWRATE MEDIA FILTERS. THIS PROJECT IS CLASSIFIED AS A CATEGORY C SPECIAL PROJECT (TRANSIT ORIENTED) AND QUALIFIES FOR A TOTAL LID TREATMENT REDUCTION CREDIT OF 100% AS DESCRIBED BELOW.

CRITERIA FOR CATEGORY C (TRANSIT ORIENTED DEVELOPMENT) SPECIAL PROJECTS TO BE CONSIDERED A CATEGORY C SPECIAL PROJECT, A PROVISION C.3 REGULATED PROJECT MUST MEET ALL OF THE FOLLOWING CRITERIA:

- BE CHARACTERIZED AS A NON AUTO-RELATED LAND USE PROJECT. THAT IS, CATEGORY C SPECIFICALLY EXCLUDES ANY REGULATED PROJECT THAT IS A STAND-ALONE SURFACE PARKING LOT; CAR DEALERSHIP; AUTO AND TRUCK RENTAL FACILITY WITH ONSITE SURFACE STORAGE; FAST-FOOD RESTAURANT, BANK OR PHARMACY WITH DRIVE-THROUGH LANES; GAS STATION, CAR WASH, AUTO REPAIR AND SERVICE FACILITY; OR OTHER AUTO RELATED PROJECT UNRELATED TO THE CONCEPT OF TRANSIT-ORIENTED DEVELOPMENT.
- IF A COMMERCIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1.
- IF A RESIDENTIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST A DENSITY OF 25 DU/AC.
- IF A MIXED-USE DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1 OR A DENSITY OF 25 DU/AC.

#### 100% LID TREATMENT REDUCTION CREDIT

- 50% REDUCTION CREDIT - PROJECT IS LOCATED WITHIN A 1/4 MILE RADIUS OF A TRANSIT HUB
- 30% REDUCTION CREDIT - MIXED USE PROJECT WITH DENSITY GREATER THAN 100 DU/ACRE
- 20% REDUCTION CREDIT - 0% OF TOTAL POST-PROJECT IMPERVIOUS SURFACE IS DEDICATED TO AT-GRADE, SURFACE PARKING

#### LEGEND

- ○ STORMWATER FILTER UNIT
- FLOW DIRECTION
- ▨ IMPERVIOUS ROOF AREAS
- ▩ IMPERVIOUS PLAZA/PODIUM AREAS
- ▤ PERVIOUS LANDSCAPE AREAS
- - - DRAINAGE AREA BOUNDARY

#### STORMWATER LID TREATMENT SIZING

TOTAL LID TREATMENT REDUCTION CREDIT = 100%  
TOTAL BLOCK 2 IMPERVIOUS AREA OF 60,032 SF ALLOWED TO BE TREATED W/ NON-LID TREATMENT MEASURES (MEDIA FILTER UNITS)

**BUILDING C**  
 $Q_{c,3} = CIA = (0.9) \cdot (0.2 \text{ IN/HR}) \cdot (35,216 \text{ SF})$   
 $Q_{c,3} = 0.1455 \text{ CFS}$

A 5'x8' MEDIA FILTER VAULT WITH (3) 12"X12" STACKED CARTRIDGES TO BE PROVIDED WITHIN BUILDING. TREATMENT FLOW CAPACITY = 0.16 CFS.

**BUILDING D**  
 $Q_{c,3} = CIA = (0.9) \cdot (0.2 \text{ IN/HR}) \cdot (24,816 \text{ SF})$   
 $Q_{c,3} = 0.1025 \text{ CFS}$

A 5'x8' MEDIA FILTER VAULT WITH (3) 18" CARTRIDGES TO BE PROVIDED WITHIN BUILDING. TREATMENT FLOW CAPACITY = 0.12 CFS.

#### STORMWATER AREA SUMMARY

THE CITY OF OAKLAND STORM DRAINAGE DESIGN GUIDELINES ESTABLISHES A 25% GOAL FOR PEAK FLOW REDUCTION COMPARED TO EXISTING CONDITIONS, TO THE EXTENT POSSIBLE. DUE TO THE FACT THAT ALMOST 90% OF THE SITE IS COVERED BY PERMANENT STRUCTURE, THERE IS LIMITED SPACE FOR DETENTION MEASURES ONSITE. AS A RESULT, A 25% PEAK FLOW REDUCTION WAS ACCOMPLISHED AT BUILDING C, AND 16% FOR BUILDING D.

EXISTING IMPERVIOUS SURFACE	EXISTING PERVIOUS SURFACE
35,216 SF	0 SF
35,216 SF	35,216 SF

**BUILDING C**

PROPOSED IMPERVIOUS SURFACE	PROPOSED PERVIOUS SURFACE
35,216 SF	0 SF
35,216 SF	35,216 SF

**BUILDING D**

EXISTING IMPERVIOUS SURFACE	EXISTING PERVIOUS SURFACE
24,816 SF	0 SF
24,816 SF	24,816 SF

**BUILDING D**

PROPOSED IMPERVIOUS SURFACE	PROPOSED PERVIOUS SURFACE
24,816 SF	0 SF
24,816 SF	24,816 SF

#### STORMWATER PEAK FLOW REDUCTION

PEAK FLOW CALCULATED PER CITY OF OAKLAND STORM DRAINAGE DESIGN STANDARDS.

DESIGN STORM = 10-YEAR MEAN ANNUAL PRECIPITATION (MAP) = 21 INCHES  
TIME OF CONCENTRATION = 5 MINUTES

**DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:**

**BUILDING C**  
 $Q_{EX-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(35,216 \text{ SF})$   
 $Q_{EX-10YR} = 2,532 \text{ CFS}$

$Q_{PR-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(35,216 \text{ SF})$   
 $Q_{PR-10YR} = 2,532 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((2,532/2,532) \cdot 100) = 0.0\%$

#### DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:

**BUILDING D**  
 $Q_{EX-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(24,816 \text{ SF})$   
 $Q_{EX-10YR} = 1,784 \text{ CFS}$

$Q_{PR-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(24,816 \text{ SF})$   
 $Q_{PR-10YR} = 1,784 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((1,784/1,784) \cdot 100) = 0.0\%$

#### DUE TO IMPLEMENTATION OF DETENTION TANK/CISTERN:

**BUILDING C**  
DETENTION VOLUME REQUIRED FOR 25% REDUCTION  
 $V_{DET \text{ REQ'D}} (CF) = (0.25) \cdot (Q_{EX-10YR}) \cdot T_c$   
 $V_{DET \text{ REQ'D}} (CF) = (0.25) \cdot (2,532 \text{ CFS}) \cdot (5 \text{ MIN}) \cdot (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQ'D}} (CF) = 284.9 \text{ CF OR } 2,134 \text{ GALLONS}$

DETENTION PIPE PROVIDED BY CISTERN IN BUILDING  
 $V_{DET \text{ PROVIDED}} = 7' \times 7' \times 6' \text{ TALL CISTERN} = 294 \text{ CF}$

CISTERN TO BE LOCATED INSIDE THE BUILDING.

#### DUE TO IMPLEMENTATION OF DETENTION PIPE:

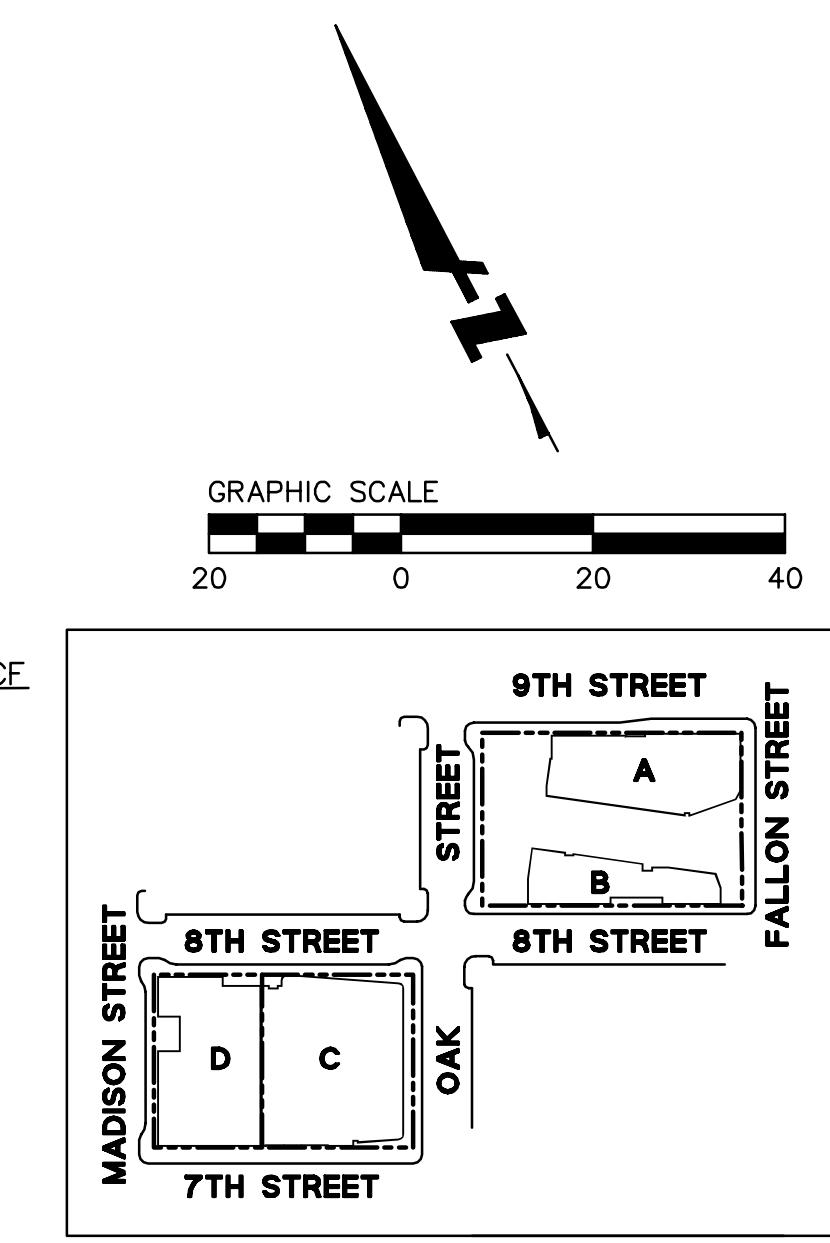
**BUILDING D**  
DETENTION VOLUME REQUIRED FOR 16% REDUCTION  
 $V_{DET \text{ REQ'D}} (CF) = (0.16) \cdot (Q_{EX-10YR}) \cdot T_c$   
 $V_{DET \text{ REQ'D}} (CF) = (0.16) \cdot (1,784 \text{ CFS}) \cdot (5 \text{ MIN}) \cdot (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQ'D}} (CF) = 128.0 \text{ CF OR } 962 \text{ GALLONS}$

DETENTION PROVIDED BY 24" PIPE  
 $V_{DET \text{ PROVIDED}} = \pi \cdot R^2 \cdot \text{LENGTH} = \pi \cdot (1.00\text{FT})^2 \cdot 21.0\text{FT} = 66 \text{ CF}$

DETENTION PROVIDED BY 48" STORM BOX BASIN  
 $V_{DET \text{ PROVIDED}} = \text{LENGTH} \cdot \text{WIDTH} \cdot \text{HEIGHT} = 4.0\text{FT} \cdot 4.0\text{FT} \cdot 4.0\text{FT} = 64 \text{ CF}$

$V_{DET \text{ PROVIDED TOTAL}} = 130 \text{ CF}$

DETENTION PIPE TO BE LOCATED ADJACENT TO THE BUILDING.



JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: 1" = 20'

# C7.2



**PF-V-4-0001**

2x #36.00" BOLTED & GASKETED ACCESS COVERS, RISERS & SLAB T&G IMPRESSIONS AS REQUIRED. FIELD POURED CONCRETE COLLAR REQUIRED, BY OTHERS. SEE NOTE 2.

VENTED OUTLET HOOD.

TOP SLAB RISER T&G IMPRESSIONS AS REQUIRED.

BASE.

#18" MAXIMUM SEE NOTE 3.

PERK FILTER™ CARTRIDGES.

CONCRETE FALSE FLOOR.

OUTLET CHAMBER.

CONCRETE DIVIDER WALL.

INLET WERY/BYPASS ASSEMBLY.

INLET CHAMBER.

Notes:

- Precast concrete structure shall be manufactured in accordance with ASTM Designation C857 and C858.
- Filter system shall be supplied with traffic rated (H20) bolted & gasketed Ø36" circular access covers with risers as required. Shallow applications may require configurations with (H20) bolted & gasketed square/rectangular access haunches. Field poured concrete collars required by others.
- Inlet & outlet pipe(s) (Ø 18" maximum) may enter device on all three sides of the inlet & outlet chambers respectively.
- Inlet chamber shall be supplied with a drain-down device designed to remove standing water between storm events.
- For depths less than specified minimums contact Oldcastle® Stormwater Solutions for engineering assistance.

**Perk Filter™**  
4" Wide Concrete Vault  
Three to Seven Cartridges / Stacks

**Oldcastle®**  
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PF-V-4-0001 | E | JPR 10/3/14 | JPR 7/8/08 | SHEET 1 OF 2

**PF-V-4-0001**

DETAIL A  
INLET WERY/BYPASS ASSEMBLY  
SCALE: NONE

PERK FILTER™ CARTRIDGE/STACKS.

CARTRIDGE BYPASS PORT.

VENTED OUTLET HOOD.

MINIMUM DEPTH SEE NOTE 3.

CONCRETE FALSE FLOOR.

OUTLET CHAMBER.

CONCRETE DIVIDER WALL.

INLET WERY/BYPASS ASSEMBLY.

INLET CHAMBER.

Notes:

- Precast concrete structure shall be manufactured in accordance with ASTM Designation C857 and C858.
- Filter system shall be supplied with traffic rated (H20) bolted & gasketed Ø36" circular access covers with risers as required. Shallow applications may require configurations with (H20) bolted & gasketed square/rectangular access haunches. Field poured concrete collars required by others.
- Inlet & outlet pipe(s) (Ø 18" maximum) may enter device on all three sides of the inlet & outlet chambers respectively.
- Inlet chamber shall be supplied with a drain-down device designed to remove standing water between storm events.
- For depths less than specified minimums contact Oldcastle® Stormwater Solutions for engineering assistance.

**Perk Filter™**  
4" Wide Concrete Vault  
Three to Seven Cartridges / Stacks

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**TYPE "A"**

**TYPE "B"**

**TYPE "C"**

**TYPE "D"**

**PROFILE**

CONCRETE CURB & GUTTER

Notes:

- OMIT THIS PORTION OF CURB AT DRIVEWAYS.
- STANDARD SLOPE EXCEPT AS SHOWN ON PLANS.
- PLACE #4 OF PROCESSED MISCELLANEOUS BASE UNDER CURB, GUTTER AND SIDEWALK UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

**CITY OF OAKLAND** DESIGN AND CONSTRUCTION SERVICES DEPARTMENT

**CONCRETE CURB AND GUTTER**

ENGINEERING DESIGN MANAGER  
DATE: JANUARY 2002  
REV. DATE: S-1

**PERSPECTIVE**

**RESIDENTIAL**

**OTHER THAN RESIDENTIAL**

**TYPICAL SECTIONS**

**GENERAL NOTES**

- IN ALL COMMERCIAL, INDUSTRIAL, OR SPECIAL DISTRICTS, THE DRIVEWAY OPENINGS SHALL BE NOT LESS THAN TWELVE (12) FEET IN WIDTH NOR MORE THAN THIRTY-FIVE (35) FEET IN WIDTH.
- IN RESIDENTIAL DISTRICTS, THE DRIVEWAY OPENINGS SHALL BE NOT LESS THAN TEN (10) FEET IN WIDTH NOR MORE THAN NINETEEN (19) FEET IN WIDTH PROVIDED, HOWEVER, A DRIVEWAY OPENING SERVING TWO OR MORE PARCELS MAY BE TWENTY-NINE (29) FEET IN WIDTH. FOR THE SPECIAL CASE OF FULLY LIT RESIDENTIAL BUILDINGS, THE DRIVEWAY OPENING MAY BE TWENTY-FIVE (25) FEET IN WIDTH.
- DRIVEWAYS SERVING A SINGLE PARCEL OF PROPERTY OR SERVING ANY OF SEVERAL ADJACENT PARCELS UNDER SINGLE OWNERSHIP SHALL BE SEPARATED BY AT LEAST TWENTY-FIVE (25) FEET OF FULL VERTICAL CURB.
- DRIVEWAYS SERVING SEPARATE BUT ADJOINING PARCELS OF PROPERTY UNDER DIFFERENT OWNERSHIP SHALL BE SEPARATED BY AT LEAST TEN (10) FEET OF FULL VERTICAL CURB.
- DRIVEWAYS ON EITHER STREET SHALL BE AT LEAST TWENTY-FIVE (25) FEET FROM THE PROJECTED CURB LINE OF THE INTERSECTING STREET.
- NO PART OF THE DRIVEWAY SHALL EXTEND INTO THE CURB RETURN A DISTANCE GREATER THAN FIVE (5) FEET.
- DRIVEWAYS SHALL BE MINIMUM 18" MINIMUM COMPRESSIVE STRENGTH OF 2000 PSI IN ACCORDANCE WITH SUBSECTION 201.1 UNLESS OTHERWISE NOTED.
- REFER TO CHAPTER 12.64 OF THE OAKLAND MUNICIPAL CODE DRIVEWAYS AND CURBS.
- CONCRETE SHALL BE MINIMUM 18" MINIMUM COMPRESSIVE STRENGTH OF 2000 PSI IN ACCORDANCE WITH SUBSECTION 201.1 UNLESS OTHERWISE NOTED.
- BASE MATERIAL SHALL CONFORM TO SUBSECTION 201.1 UNLESS OTHERWISE OTHERWISE NOTED.
- EXPANSION JOINTS 1/4" THICK SHALL EXTEND THROUGH THE ENTIRE THICKNESS OF THE SIDEWALK AT EACH END OF DRIVEWAY CURB RETURNS, AS DIRECTED BY THE ENGINEER TO CONFORM TO EX. CONDITIONS.

**CITY OF OAKLAND** ENGINEERING DESIGN SERVICES DIVISION

**CONCRETE DRIVEWAY**

ENGINEERING DESIGN MANAGER  
DATE: FEBRUARY 2002  
REV. DATE: S-2

**1 STORMWATER MEDIA FILTER VAULT (1 OF 2)**  
NTS

**2 STORMWATER MEDIA FILTER VAULT (2 OF 2)**  
NTS

**3 CONCRETE CURB & GUTTER**  
NTS

**4 CONCRETE DRIVEWAY**  
NTS

**PF-MH-48**

SECTIONAL PLAN

SHAFT AND BASE DETAILS

FRAME AND COVER DETAILS

Notes:

- Frames and covers to conform to section 201.2.
- All contact and bearing surfaces of both ramp and cover shall be machined to fit accurately.
- Type H frame and cover are used on manholes in public streets, parking lots, and other areas where heavy traffic is expected.
- Manhole covers shall be used on manholes where located on public streets (TYPE H-MANHOLE).
- CONCRETE OR METAL COVERS SHALL BE 4" CONFORMANCE WITH ASTM A-303.
- TYPE H-MANHOLE SHALL BE THE OPTION TO USE TO PROVIDE FLAT 2" TO 3" PRECAST MANHOLES (2" PRECAST MANHOLES MANUFACTURED BY M.A. HODGKINS INC. OR APPROVED EQUAL).

**Perk Filter™**  
Ø48.00" Manhole  
One to Two Cartridges / Stacks

**Oldcastle®**  
Stormwater Solutions  
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PF-MH-48 | NR | SM 1/26/15 | SM 1/26/15 | SHEET 1 OF 2

**PF-MH-48**

BOLTED & GASKETED ACCESS COVER/HATCH, STANDARD, ALTERNATE COVERS & GRATED INLET OPTIONS AVAILABLE.

TOP SLAB.

RISER SECTION AS REQUIRED.

INLET.

INLET/BYPASS MODULE.

PERK FILTER™ CARTRIDGE.

BASE SECTION.

CONCRETE FALSE FLOOR.

OUTLET.

Notes:

- Precast concrete structure shall be manufactured in accordance with ASTM Designation C478.
- Filter system shall be supplied with traffic rated (H20) bolted & gasketed Ø36" circular access covers with risers as required. Field poured concrete collar required by others.
- Inlet & outlet pipe(s) are to be Ø18.00" maximum. Inlet pipes must enter the structure in the inlet bay.
- Inlet chamber shall be supplied with drain-down device designed to remove standing water between storm events.
- Minimum separation between Invert In & Invert out is outlet pipe diameter plus 4.00".
- For depths less than specified minimums contact Oldcastle® Stormwater Solutions for engineering assistance.

**Perk Filter™**  
Ø48.00" Manhole  
One to Two Cartridges / Stacks

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**PF-MH-48**

BOLTED & GASKETED ACCESS COVER/HATCH, STANDARD, ALTERNATE COVERS & GRATED INLET OPTIONS AVAILABLE.

TOP SLAB & COVER NOT SHOWN IN THIS VIEW FOR CLARITY.

FIELD POURED CONCRETE COLLAR REQUIRED, BY OTHERS. SEE NOTE 2.

INLET, #18.00" MAX. PIPE SIZE SEE NOTE 3.

OUTLET, #18.00" MAX. PIPE SIZE SEE NOTE 3.

MINIMUM DEPTH

SECTION A-A

**Minimum Depth**

PIPE SIZE	Ø6.00"	Ø6.00"	Ø10.00"	Ø12.00"	Ø15.00"	Ø18.00"
CARTRIDGE TYPE	MINIMUM DEPTH FROM INLET TO OUTLET	MINIMUM DEPTH FROM INLET TO OUTLET	MINIMUM DEPTH FROM INLET TO OUTLET	MINIMUM DEPTH FROM INLET TO OUTLET	MINIMUM DEPTH FROM INLET TO OUTLET	MINIMUM DEPTH FROM INLET TO OUTLET
12"	3.67' (44.00")	3.92' (47.00")	4.17' (50.00")	4.42' (53.00")	4.67' (56.00")	4.92' (59.00")
18"	4.42' (53.00")	4.67' (56.00")	4.92' (59.00")	5.17' (62.00")	5.42' (65.00")	5.67' (68.00")
12" x 12"	3.17' (38.00")	3.42' (41.00")	3.67' (44.00")	3.92' (47.00")	4.17' (50.00")	4.42' (53.00")
12" x 18"	3.67' (44.00")	3.92' (47.00")	4.17' (50.00")	4.42' (53.00")	4.67' (56.00")	4.92' (59.00")

**Ø48.00" PERK FILTER MANHOLE**

TREATMENT FLOW RATES, TOTAL FLOW CAPACITIES & MAXIMUM HEAD LOSS

CARTRIDGE STACK QUANTITY	CARTRIDGE STACK CONFIGURATION							
	12"		12" x 12"		12" x 18"			
TREATMENT FLOW RATE (GPM / CFS)	TOTAL FLOW CAPACITY (GPM / CFS)	TREATMENT FLOW RATE (GPM / CFS)	TOTAL FLOW CAPACITY (GPM / CFS)	TREATMENT FLOW RATE (GPM / CFS)	TOTAL FLOW CAPACITY (GPM / CFS)	TREATMENT FLOW RATE (GPM / CFS)	TOTAL FLOW CAPACITY (GPM / CFS)	
1	12 / 0.03	2.47	18 / 0.04	3.05	24 / 0.05	3.45	30 / 0.07	3.62
2	24 / 0.06	2.47	36 / 0.08	3.05	48 / 0.11	3.45	60 / 0.13	3.62
MAXIMUM HEAD LOSS	1.7 FEET	2.3 FEET	2.3 FEET	2.9 FEET	2.9 FEET	3.5 FEET	3.5 FEET	

**Perk Filter™**  
Ø48.00" Manhole  
One to Two Cartridges / Stacks

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PF-MH-48 | NR | SM 1/26/15 | SM 1/26/15 | SHEET 2 OF 2

**5 MANHOLE (TYPE 1)**  
NTS

**6 STORMWATER MEDIA FILTER MANHOLE (1 OF 2)**  
NTS

**7 STORMWATER MEDIA FILTER MANHOLE (2 OF 2)**  
NTS

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

**REVISION SCHEDULE**

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
DATE: 03/19/2021  
SCALE: AS NOTED

**PRELIMINARY DETAILS**

**C8.1**



EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com

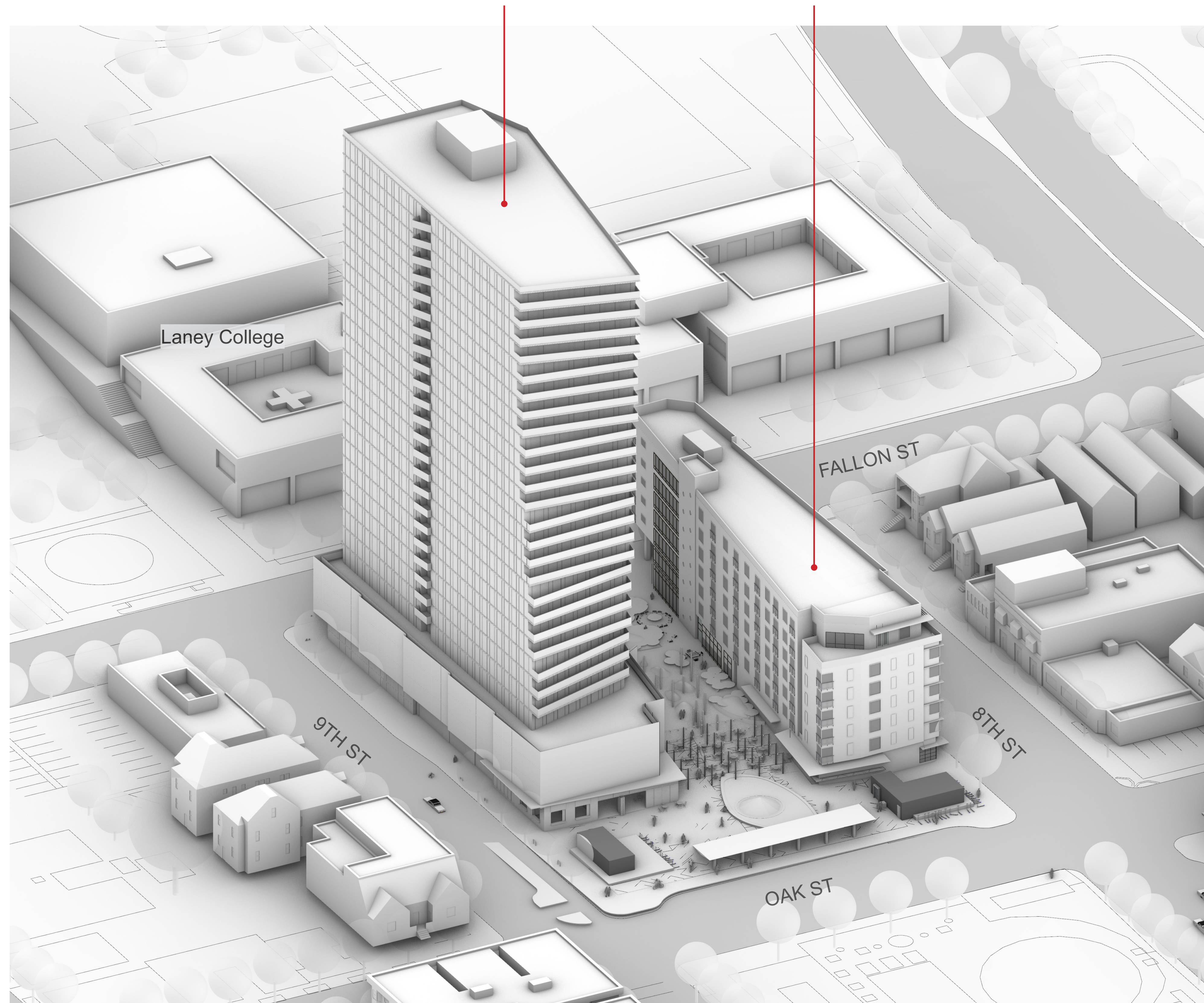
**SCB** SOLIMON CORDWELL BUENZ ARCHITECTS  
335 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
415/761-2600  
www.scb.com

**INWILLERUEHL**  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/891-1686  
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**BKF100+**  
ENGINEERS, SURVEYORS, PLANNERS  
225 BUCKLEUP DR. SUITE 200  
REDWOOD CITY, CA 94063  
(650) 483-6300  
www.bkf.com

**BUILDING A:**  
MARKET RATE  
RESIDENTIAL

**BUILDING B:**  
SENIOR HOUSING



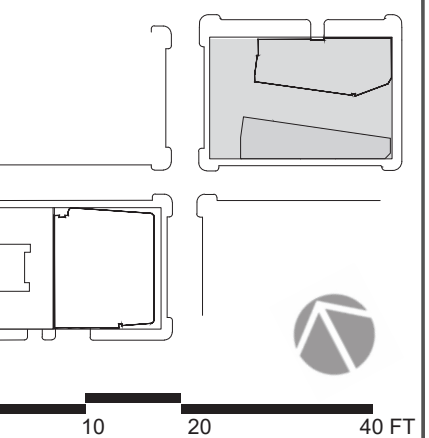
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MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

**REVISION SCHEDULE**

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPA COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPA COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPA COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1  
MASSING VIEWS**

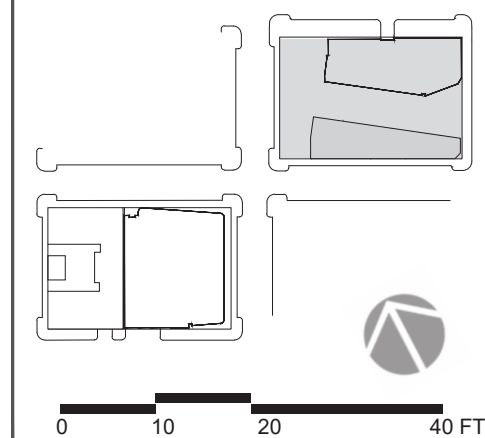


**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

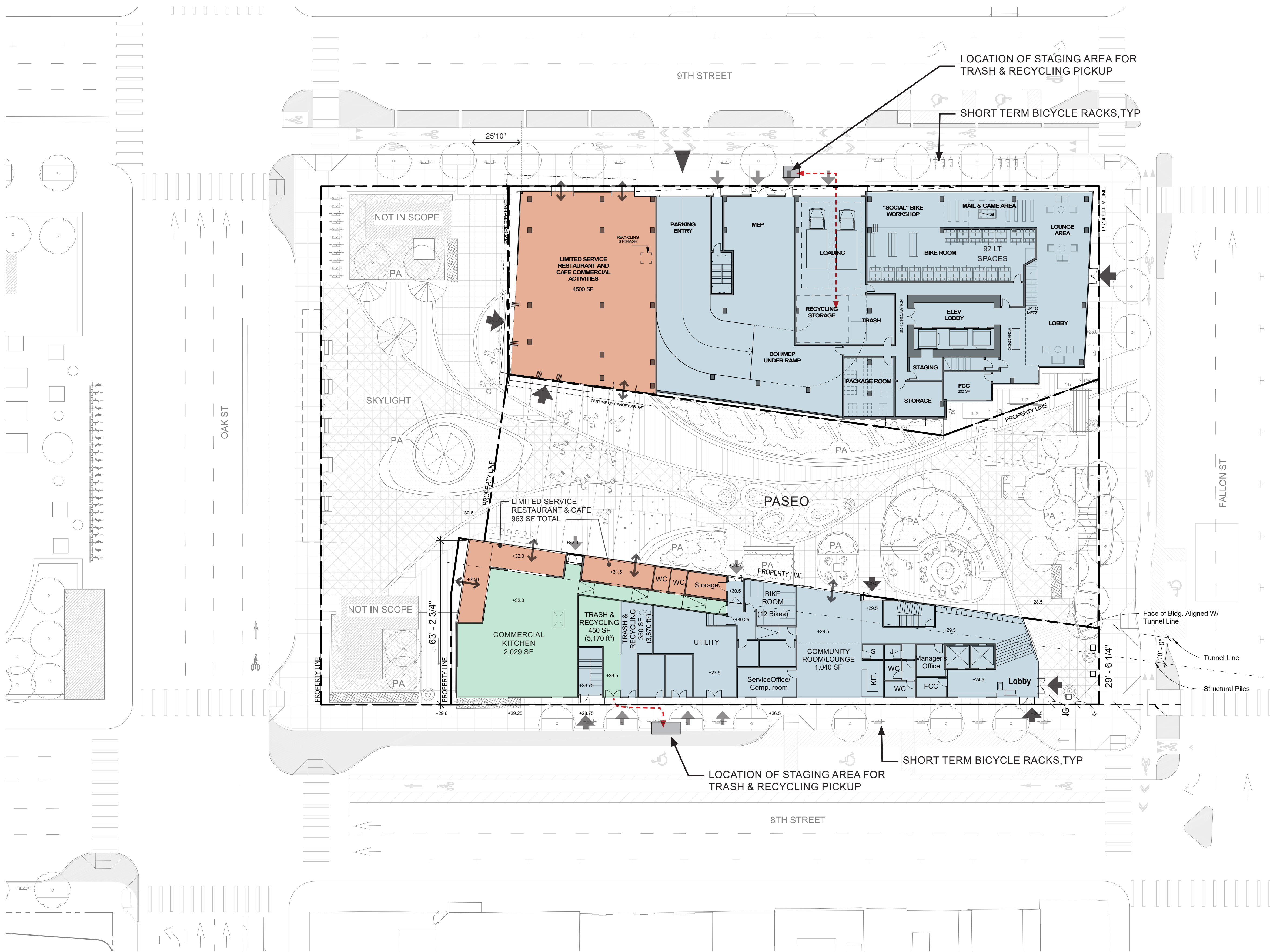
- PRELIMINARY - NOT FOR CONSTRUCTION -

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4	REVISED RESPONSE TO POP#2 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1**  
Ground FLOOR PLAN



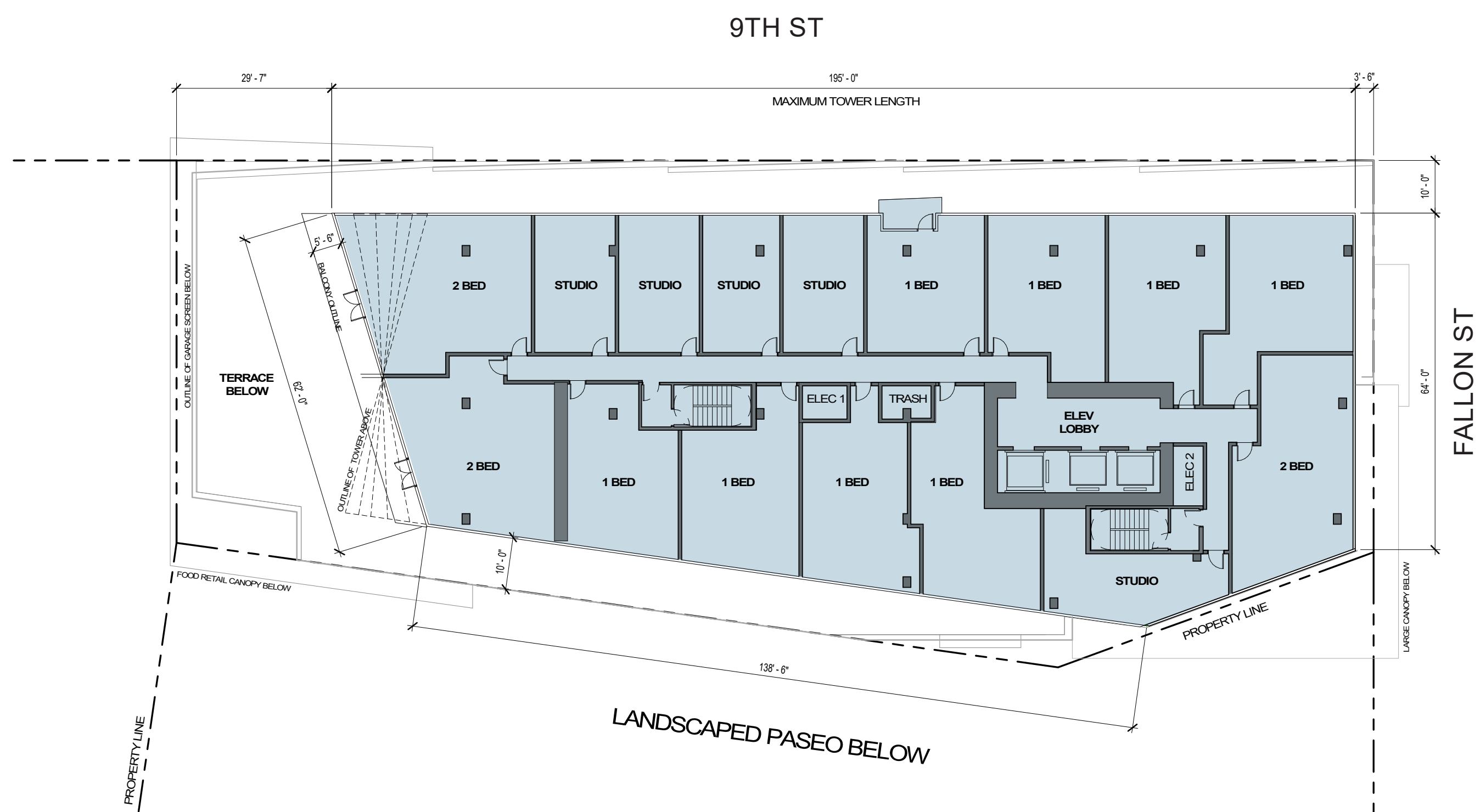
- KEY LEGEND**
- ← PRIMARY PEDESTRIAN ACCESS
  - ← UTILITY/SERVICES OR EMERGENCY ACCESS
  - ▲ PARKING ENTRANCE
  - ↔ OPENINGS
  - PATH OF TRAVEL

- LAND USE LEGEND**
- PERMANENT RESIDENTIAL ACTIVITIES 17.101G.01
  - LIMITED-SERVICE RESTAURANT AND CAFE (COMMERCIAL ACTIVITIES) 17.101G.01
  - COMMERCIAL KITCHEN (CUSTOM MANUFACTURING) 17.142.100.B

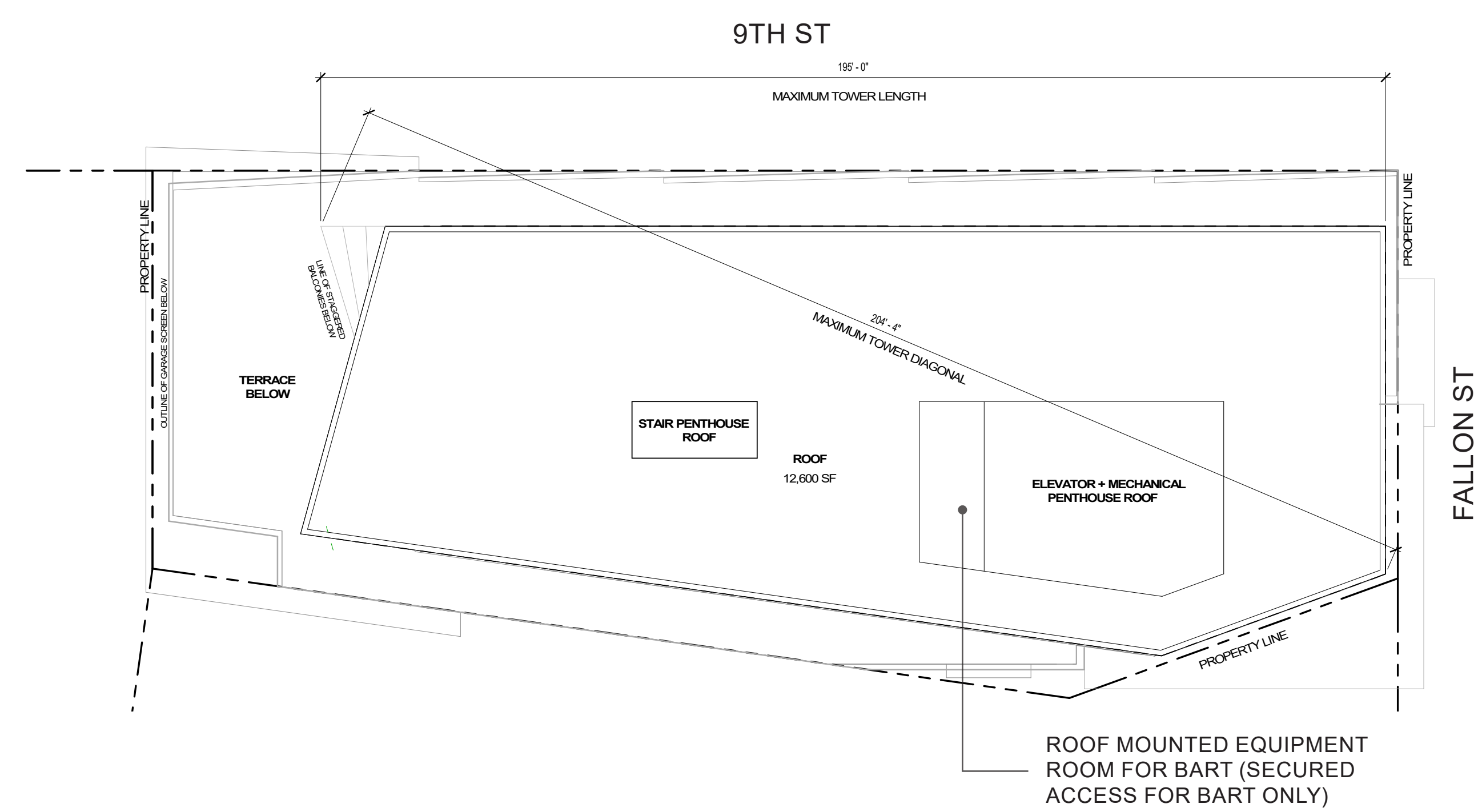




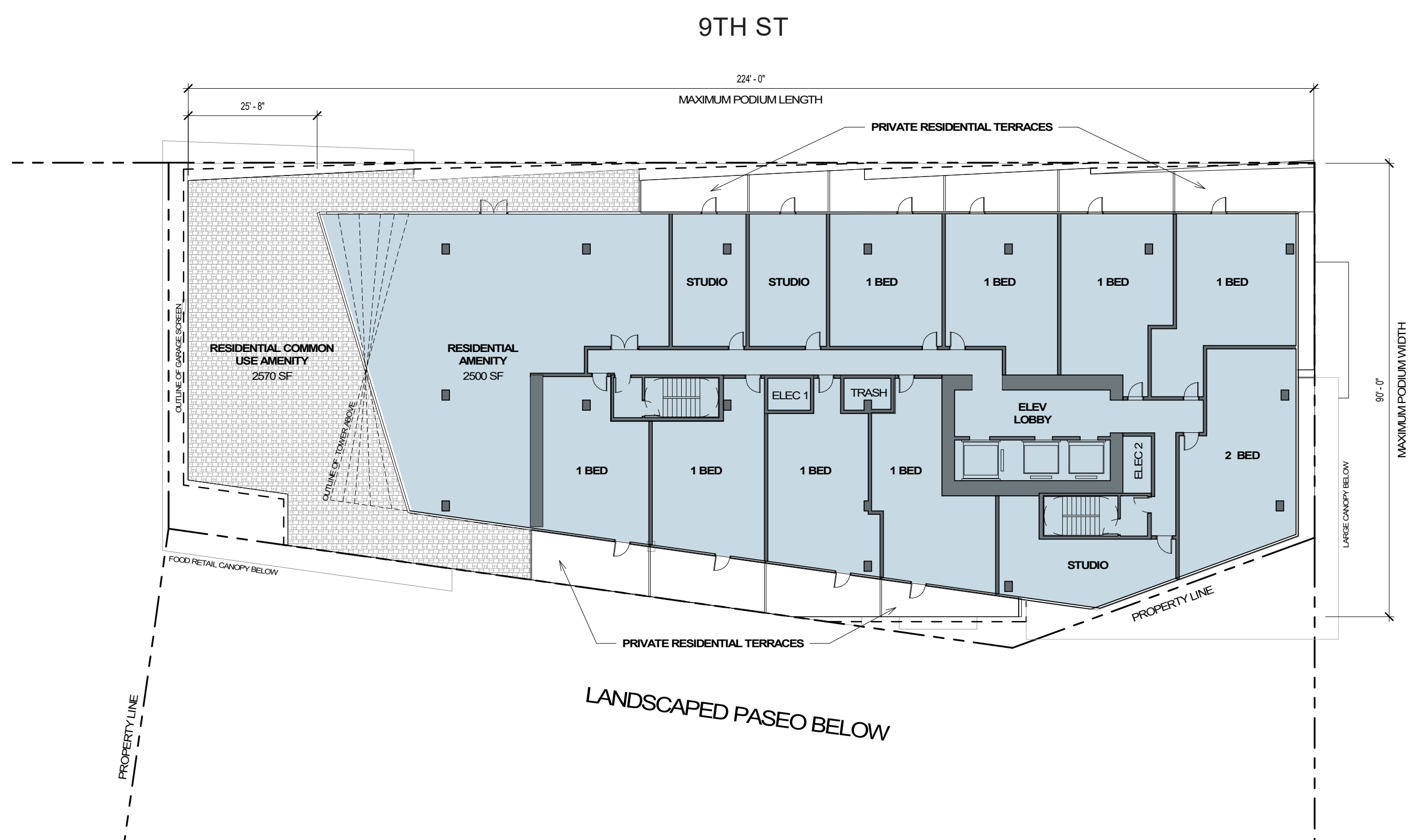




**TYPICAL RESIDENTIAL FLOOR PLAN (6-28)**

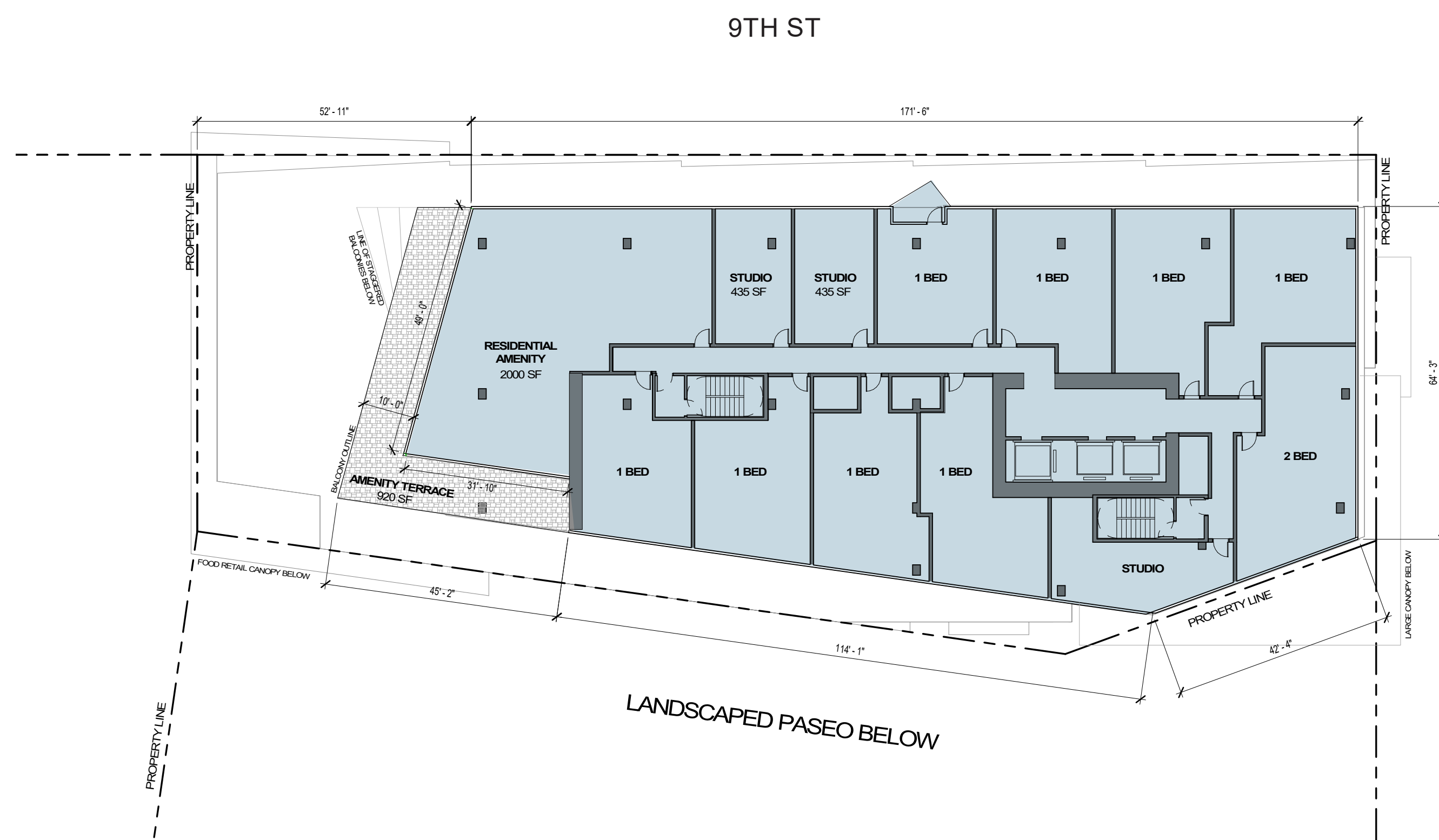


**ROOF PLAN**



**LEVEL 5 PLAN**

NOTE: LOT SIZE IS 19,332 SF. AVERAGE RESIDENTIAL TOWER FLOOR AREA IS 12,665 SF OR 65% LOT COVERAGE (REF TABLE 17.101G.04)

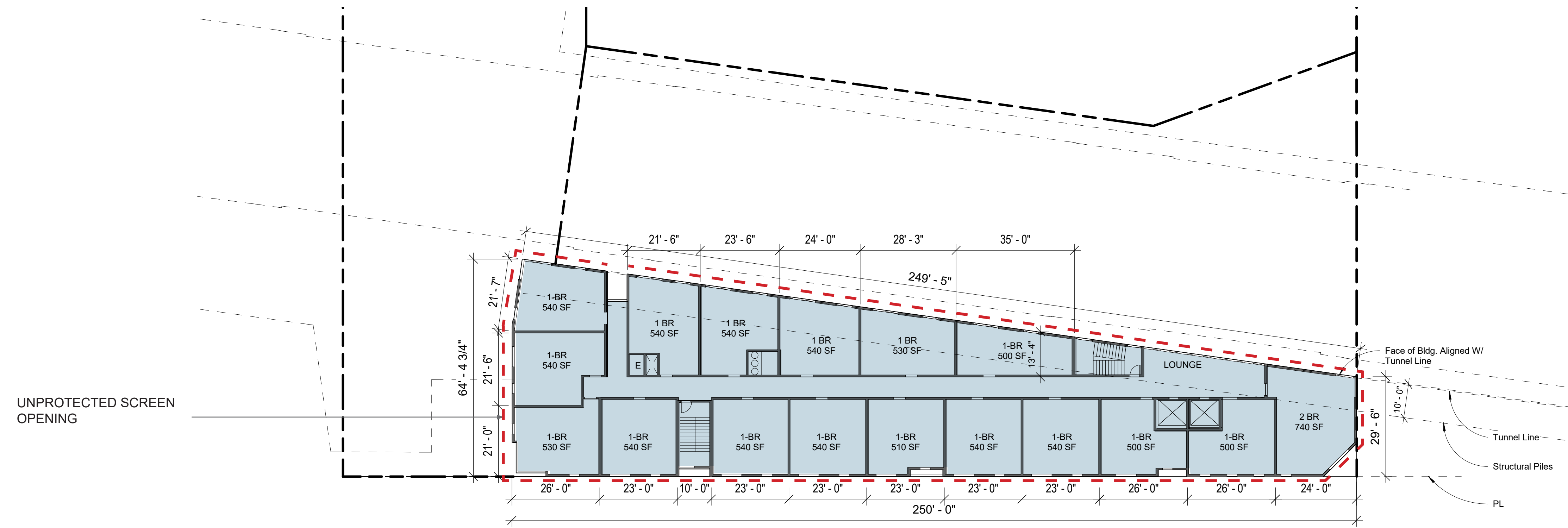


**LEVEL 28 PLAN**

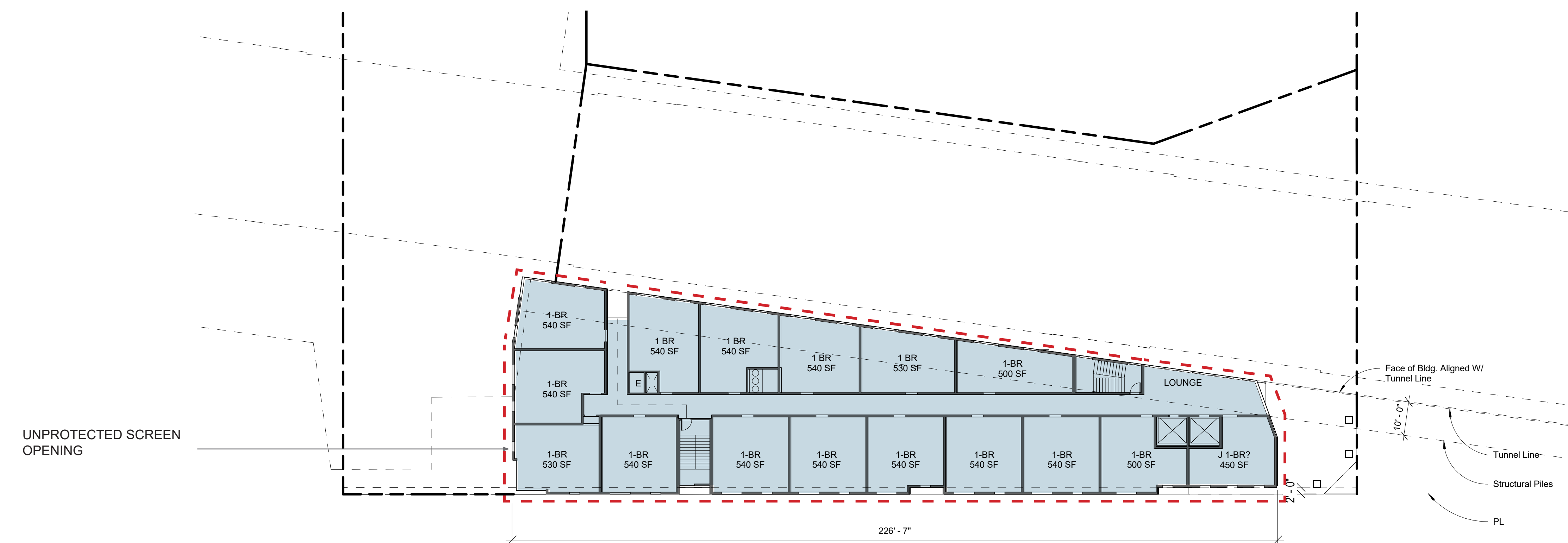
**LAND USE LEGEND (PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES





**TYPICAL RESIDENTIAL FLOOR PLAN (L3-L6)**



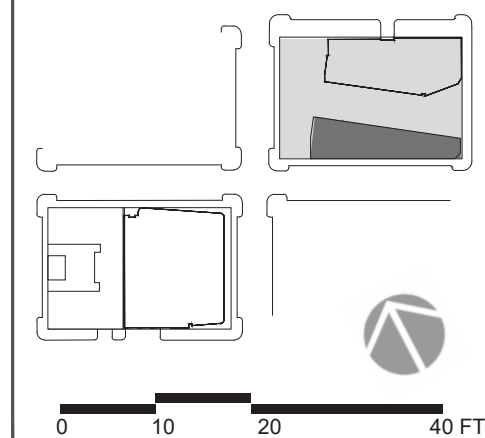
**RESIDENTIAL FLOOR PLAN (L2)**

**LAND USE LEGEND  
(PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPRZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPRZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPRZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPRZ COMMENTS	03/19/2021

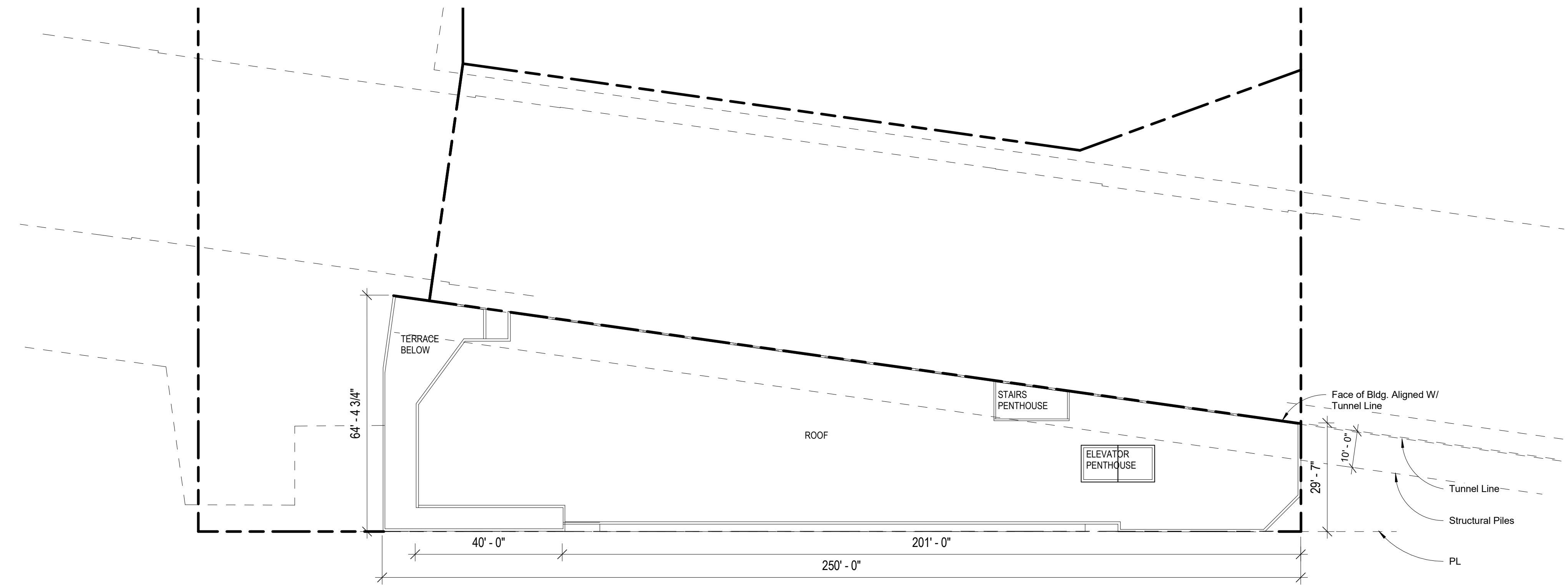


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDG B UPPER FLOOR PLANS**

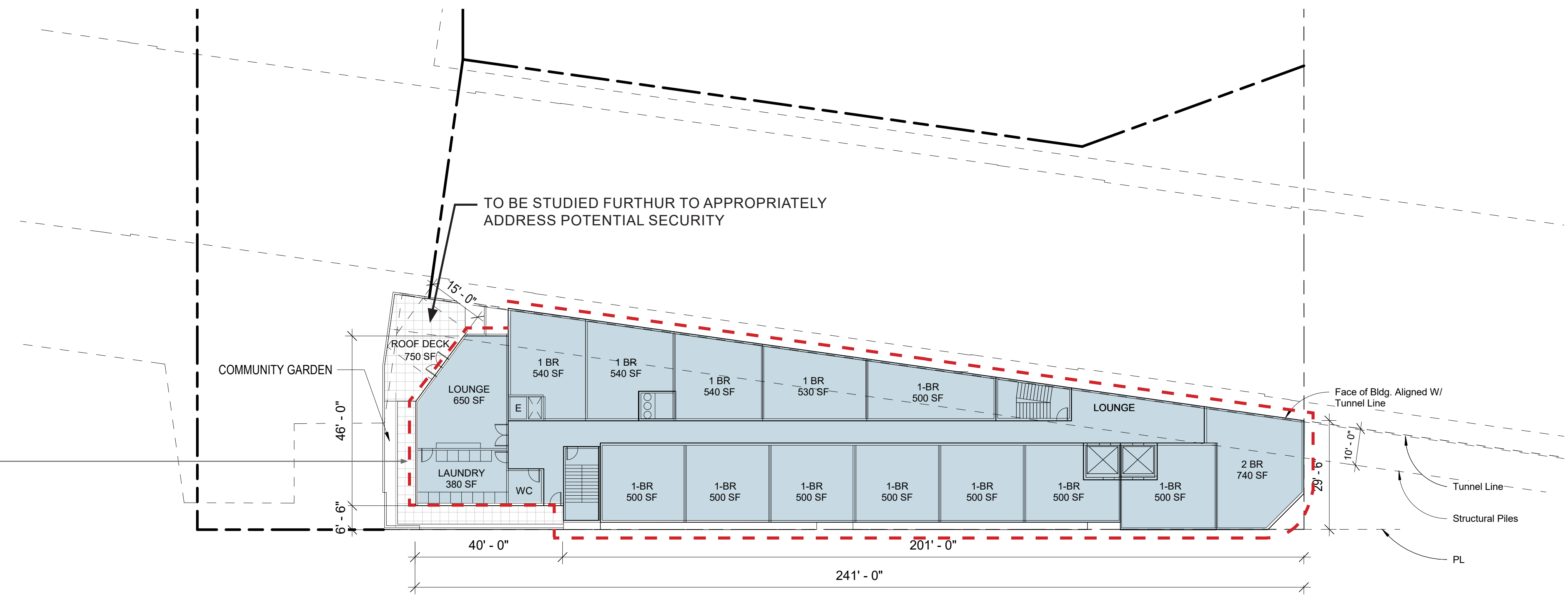
**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**





**ROOF PLAN**



**RESIDENTIAL FLOOR PLAN (L7)**

**LAND USE LEGEND (PER TABLE 17.101G.01)**

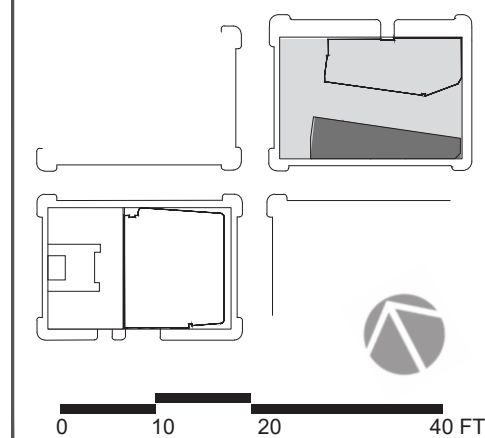
- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPFZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPFZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPFZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPFZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDG B UPPER FLOOR PLANS**

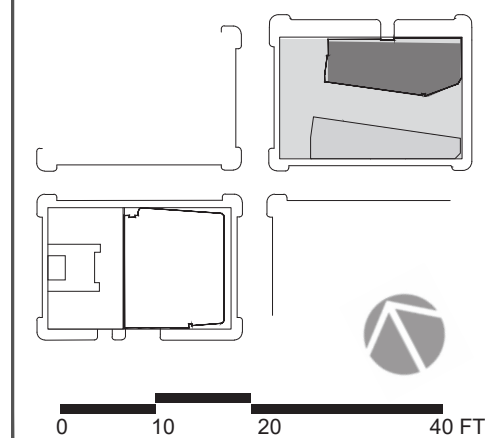


**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPA COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPA COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPA COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA COMMENTS	03/19/2021

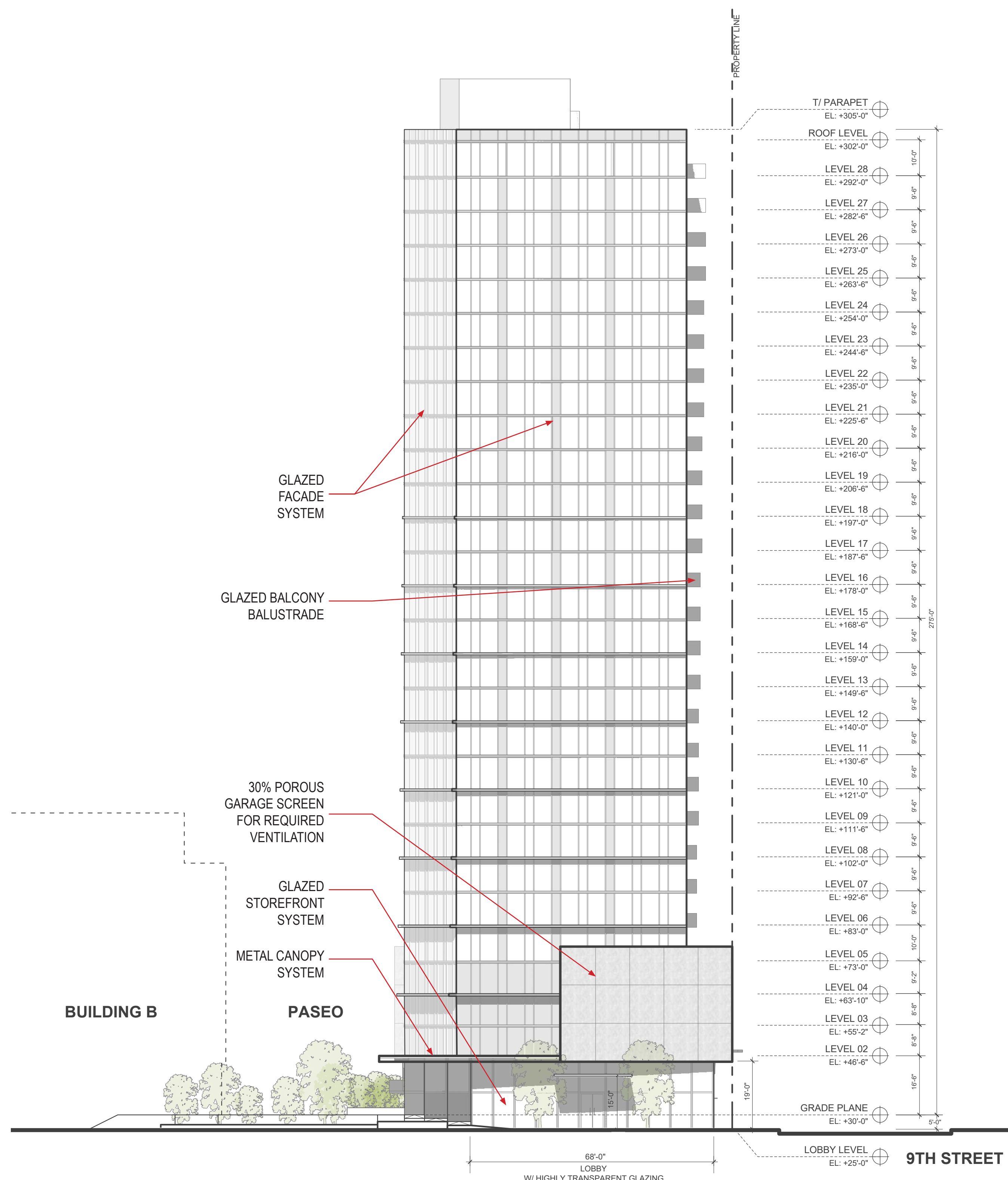


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDGA EAST & SOUTH ELEVATIONS**

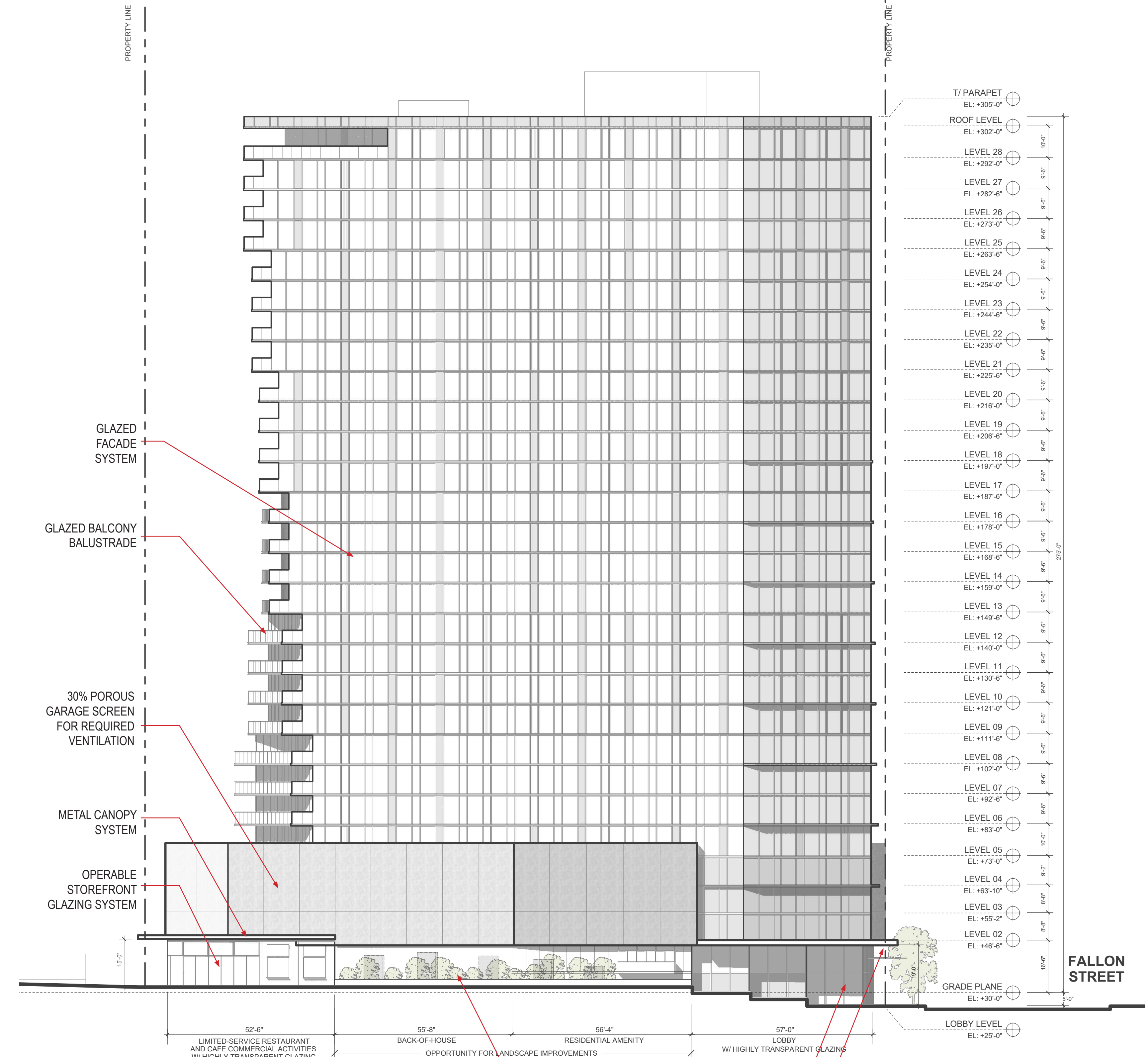
**NOTE:**

- WINDOWS AND BALCONY/DECKS FACING BART HEADHOUSE AREA NEED TO MITIGATE TO ENSURE THAT NO ITEMS CAN BE THROWN FROM THE ROOF OR WINDOW ONTO OR OVER THE HEADHOUSE AND SKYLIGHT AREA. DETAILS WILL BE PROVIDED LATER TO BE REVIEW AND APPROVED BY BART/CITY OF OAKLAND.
- COLORS AND MATERIALS TO BE FINALIZED AT FDP PHASE.

SIZE, SPECIFICATION, ACCESS, AND LOCATION OF ROOF MOUNTED EQUIPMENT TO BE APPROVED BY BART AT A LATER PHASE OF DESIGN



**EAST ELEVATION**



**SOUTH ELEVATION**

SOLID WALL PAINT FINISH OPPORTUNITY FOR LANDSCAPE IMPROVEMENTS

GLAZED STOREFRONT SYSTEM

METAL CANOPY SYSTEM

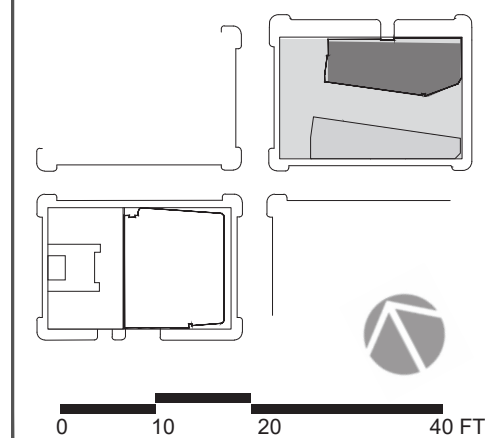


**LAKE MERRITT BART**  
REDEVELOPMENT  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
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1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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3	FDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPS COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPS COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPS COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPS COMMENTS	03/19/2021

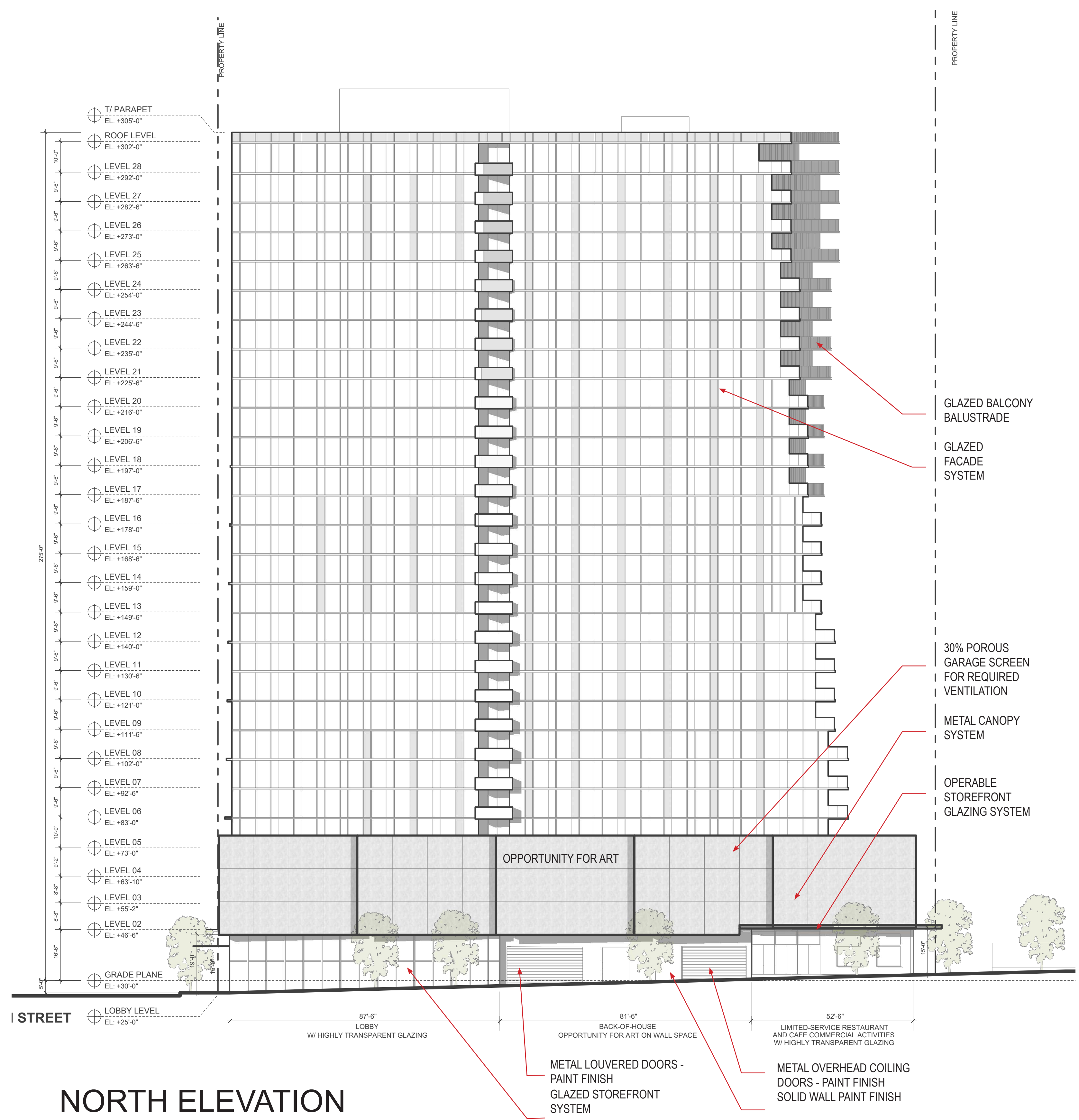
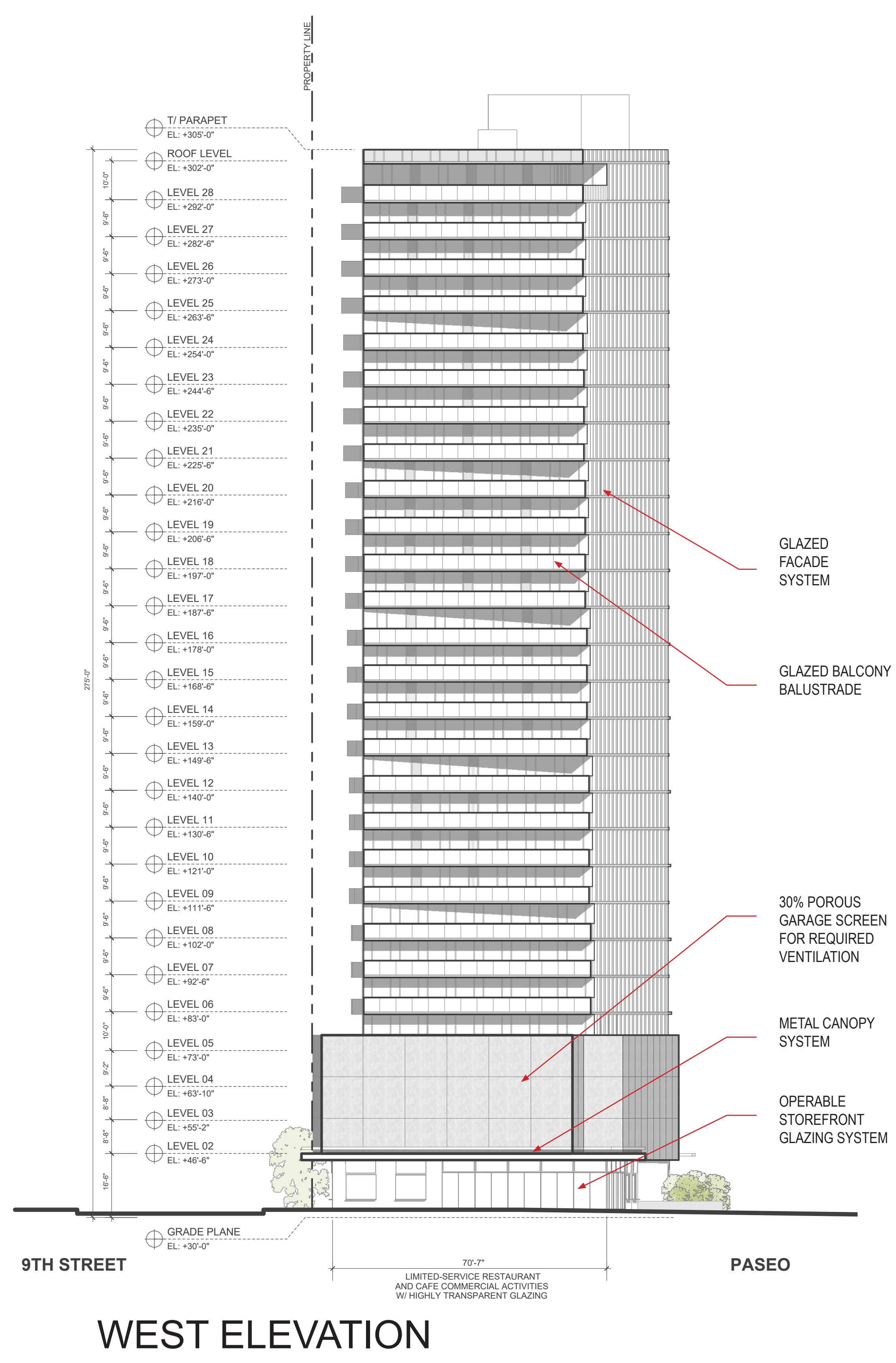


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDGA WEST & NORTH ELEVATIONS**

**NOTE:**

- WINDOWS AND BALCONY/DECKS FACING BART HEADHOUSE AREA NEED TO MITIGATE TO ENSURE THAT NO ITEMS CAN BE THROWN FROM THE ROOF OR WINDOW ONTO OR OVER THE HEADHOUSE AND SKYLIGHT AREA. DETAILS WILL BE PROVIDED LATER TO BE REVIEW AND APPROVED BY BART/CITY OF OAKLAND.
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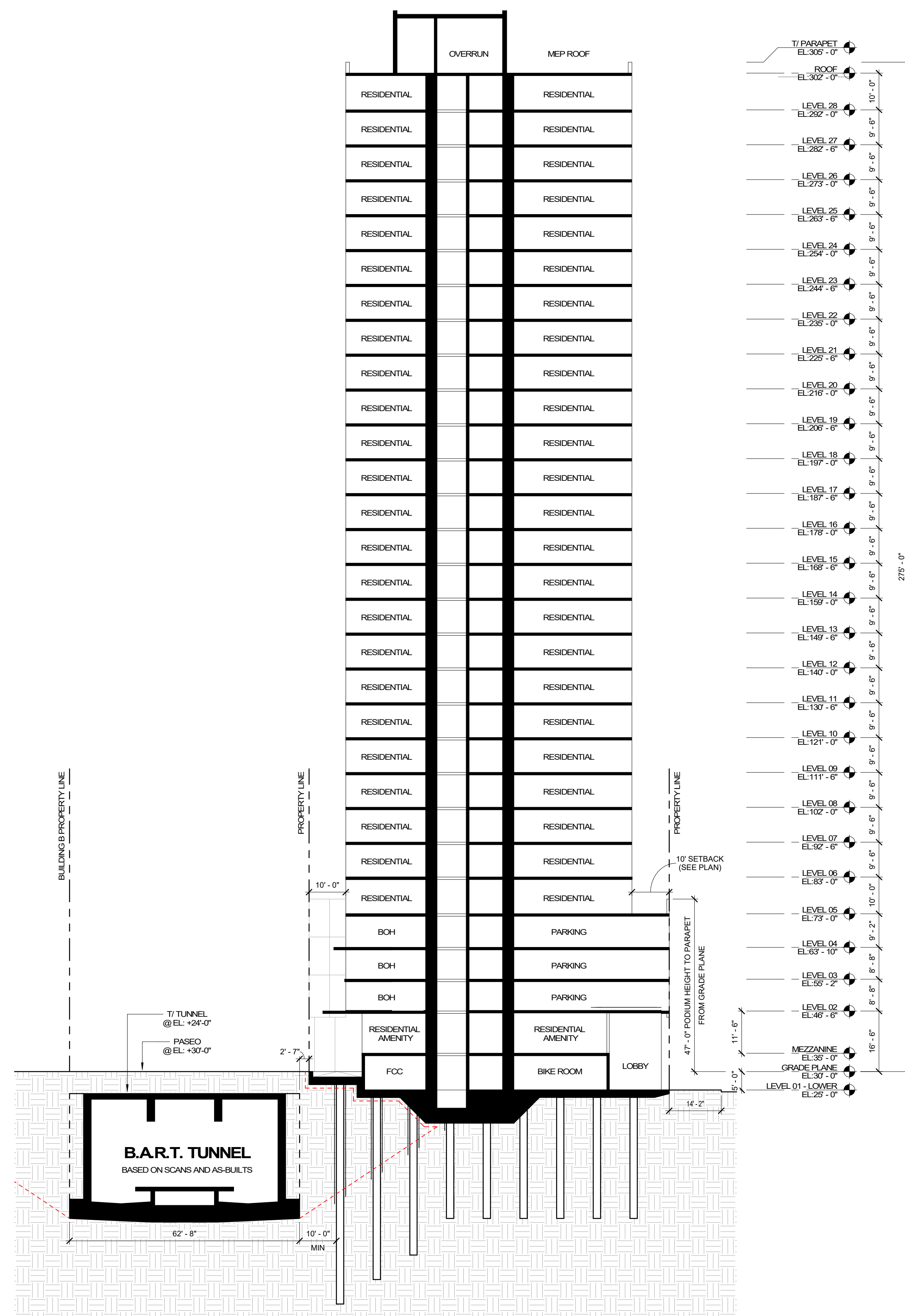
SIZE, SPECIFICATION, ACCESS, AND LOCATION OF ROOF MOUNTED EQUIPMENT TO BE APPROVED BY BART AT A LATER PHASE OF DESIGN



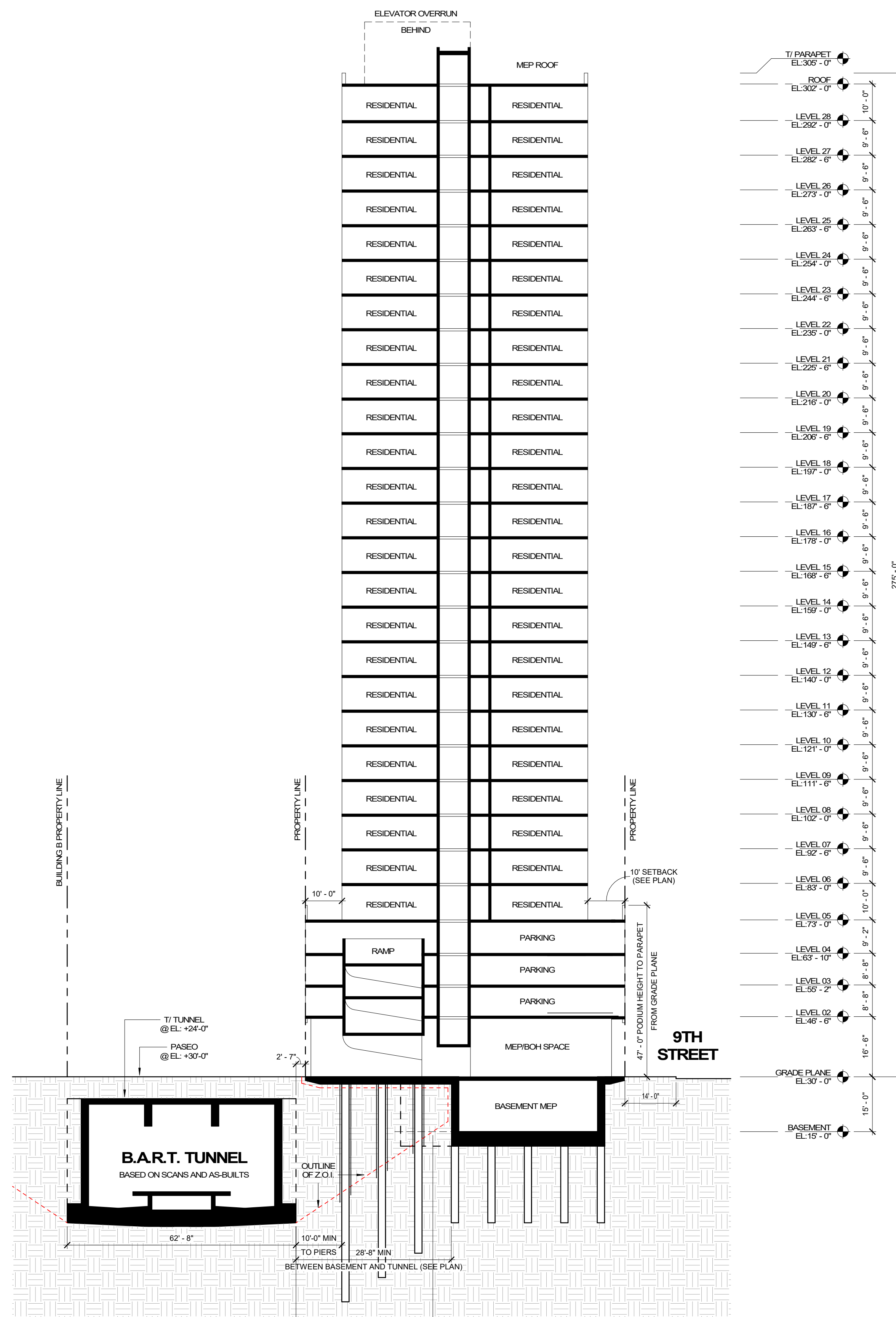


**NOTE:**

- FDP DESIGN MUST DEMONSTRATE PER BART'S REQUIREMENTS THAT THERE ARE NO STRUCTURAL IMPACTS TO THE TUNNEL PER BFS STANDARDS.



**SECTION A: NORTH-SOUTH THROUGH LOBBY**



**SECTION B: NORTH-SOUTH THROUGH BASEMENT**

**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com

**SCB**  
SOLIMON CORDWELL BUENZ ARCHITECTS  
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SAN FRANCISCO, CA 94111  
415/716-2600  
www.scba.com

**INWILLERAEHL**  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
www.inwilleraehl.com

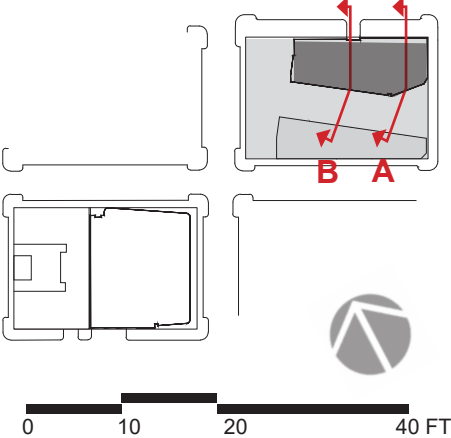
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

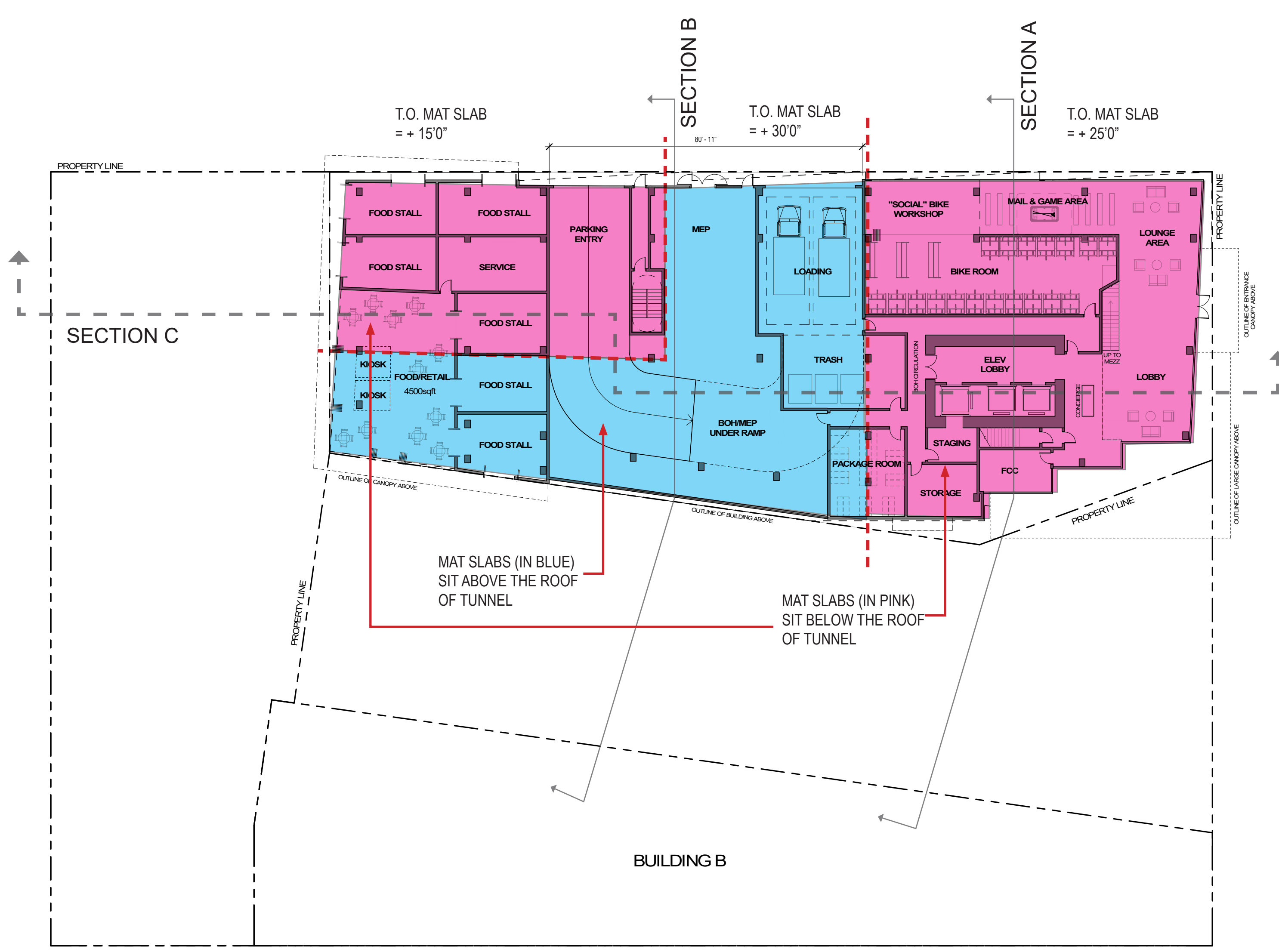
PRELIMINARY - NOT FOR CONSTRUCTION

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPK COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPK COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPK COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPK COMMENTS	03/19/2021

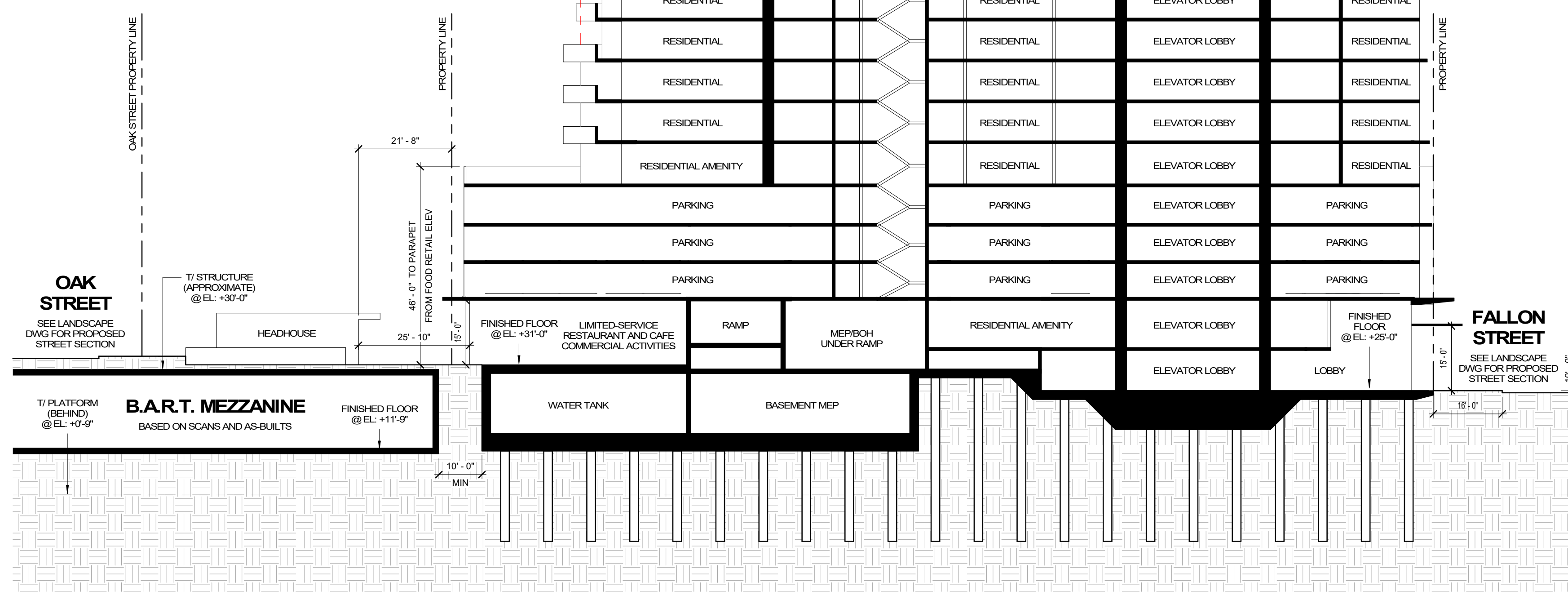
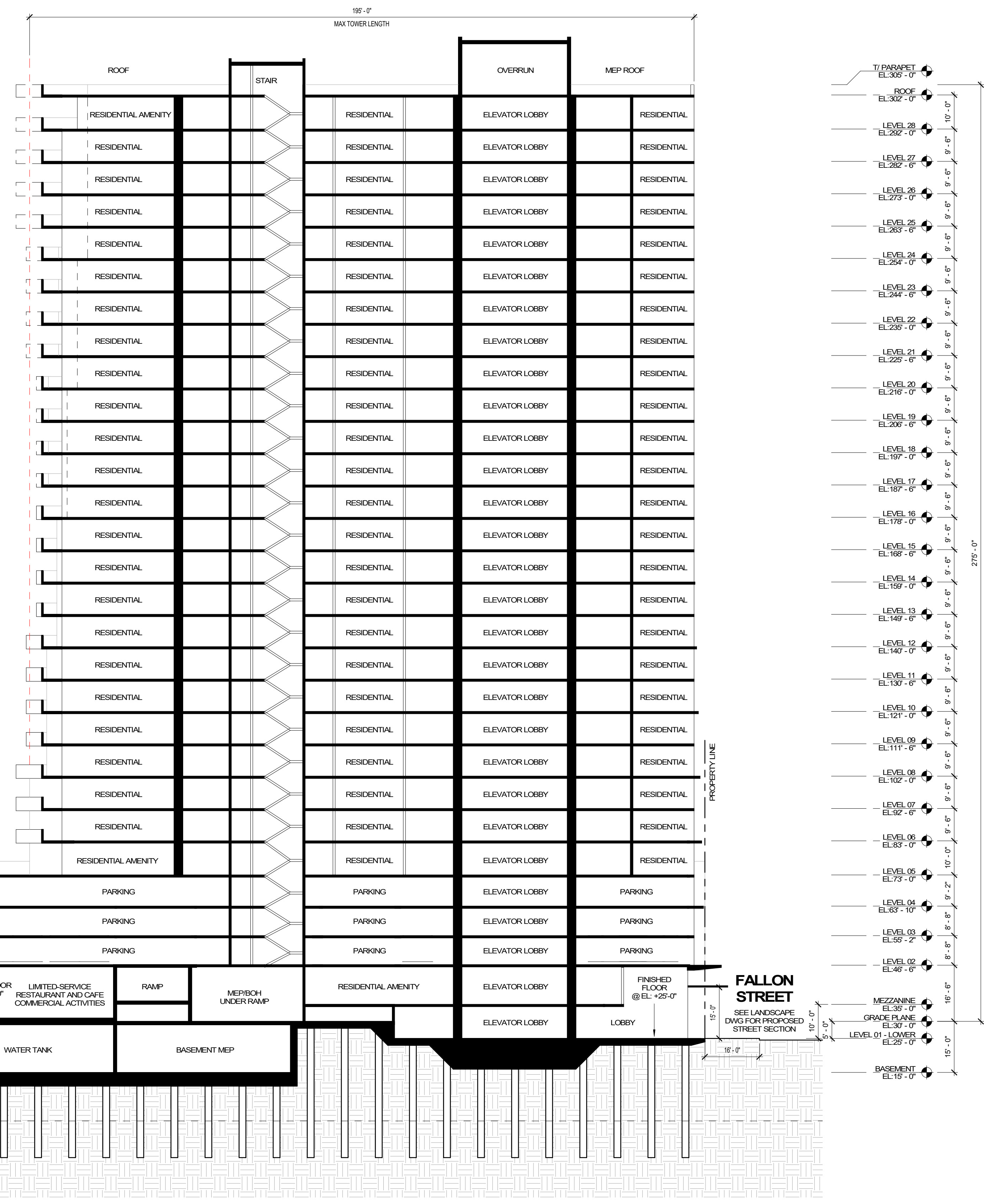


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 BUILDING A NORTH - SOUTH SECTIONS**





**NOTE:**  
 • FDP DESIGN MUST DEMONSTRATE PER BART'S REQUIREMENTS THAT THERE ARE NO STRUCTURAL IMPACTS TO THE TUNNEL PER BFS STANDARDS.



**SECTION C: EAST-WEST**

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
 1825 SAN PABLO AVE. #200  
 OAKLAND, CA 94612

**STRADA**  
 CONSULTING ARCHITECTS  
 101 MISSION ST. #420  
 SAN FRANCISCO, CA 94105

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 ENGINEERS, SURVEYORS, PLANNERS  
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 REDWOOD CITY, CA 94063  
 (650) 483-4300  
 www.bkf.com

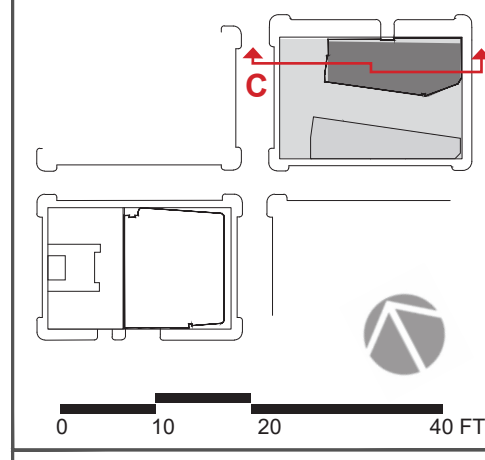
**LAKE MERRITT BART REDEVELOPMENT**  
 Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

• PRELIMINARY - NOT FOR CONSTRUCTION •

**REVISION SCHEDULE**

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	FDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPRZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPRZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPRZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPRZ COMMENTS	03/19/2021



DATE:  
 SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 BUILDING A EAST-WEST SECTION**

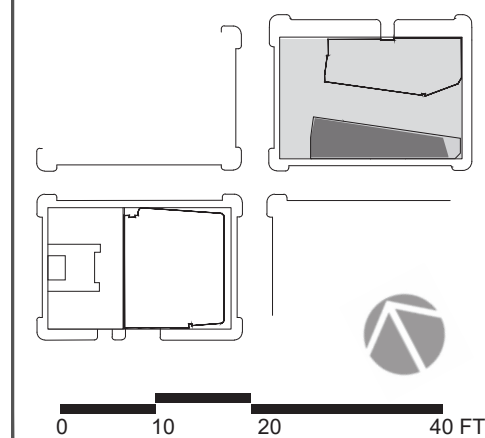


**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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3	PDP SUBMITTAL #1 TO CITY	2/12/2020
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5	REVISED RESPONSE TO POPP COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPP COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPP COMMENTS	03/19/2021

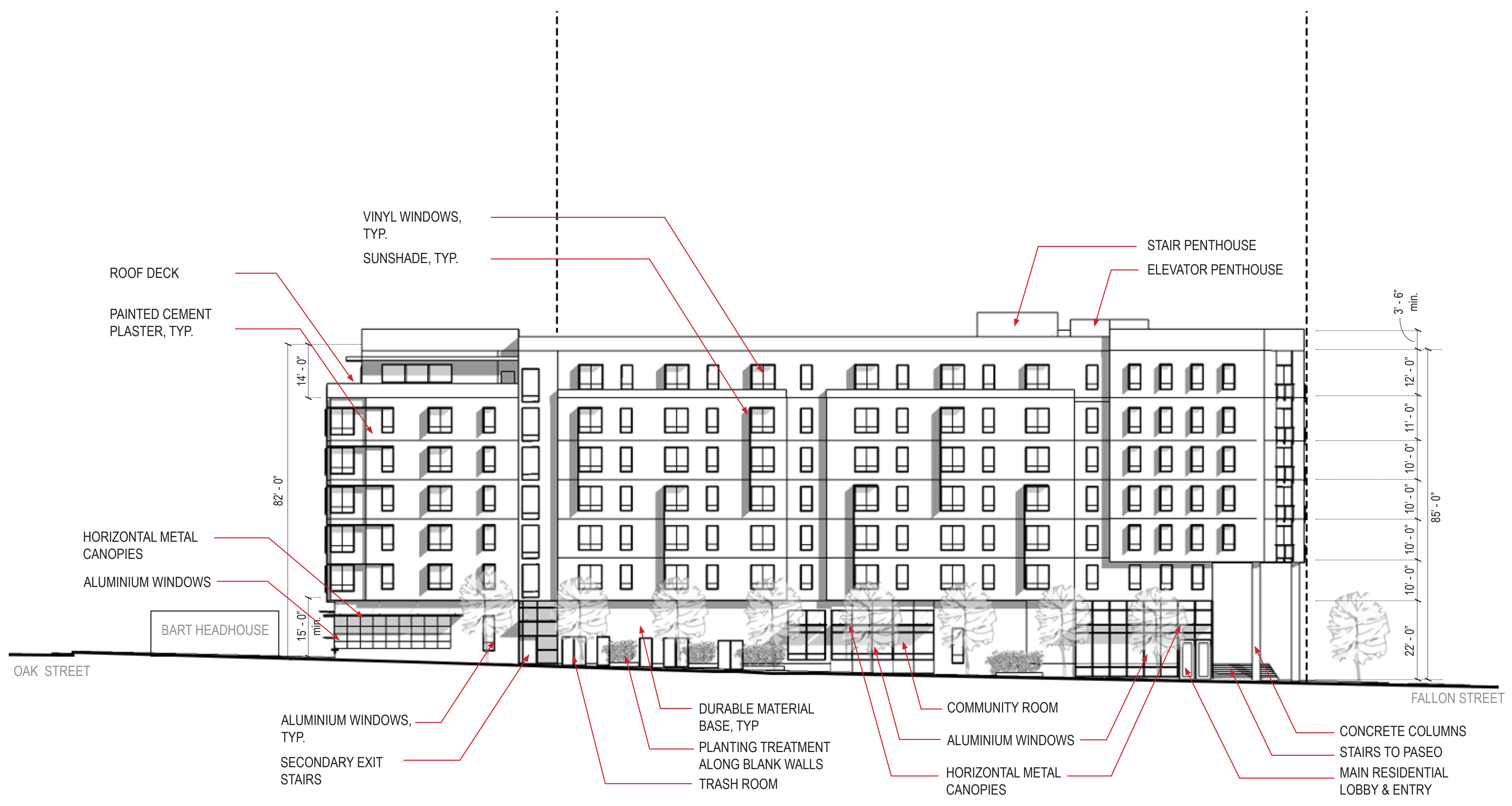


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

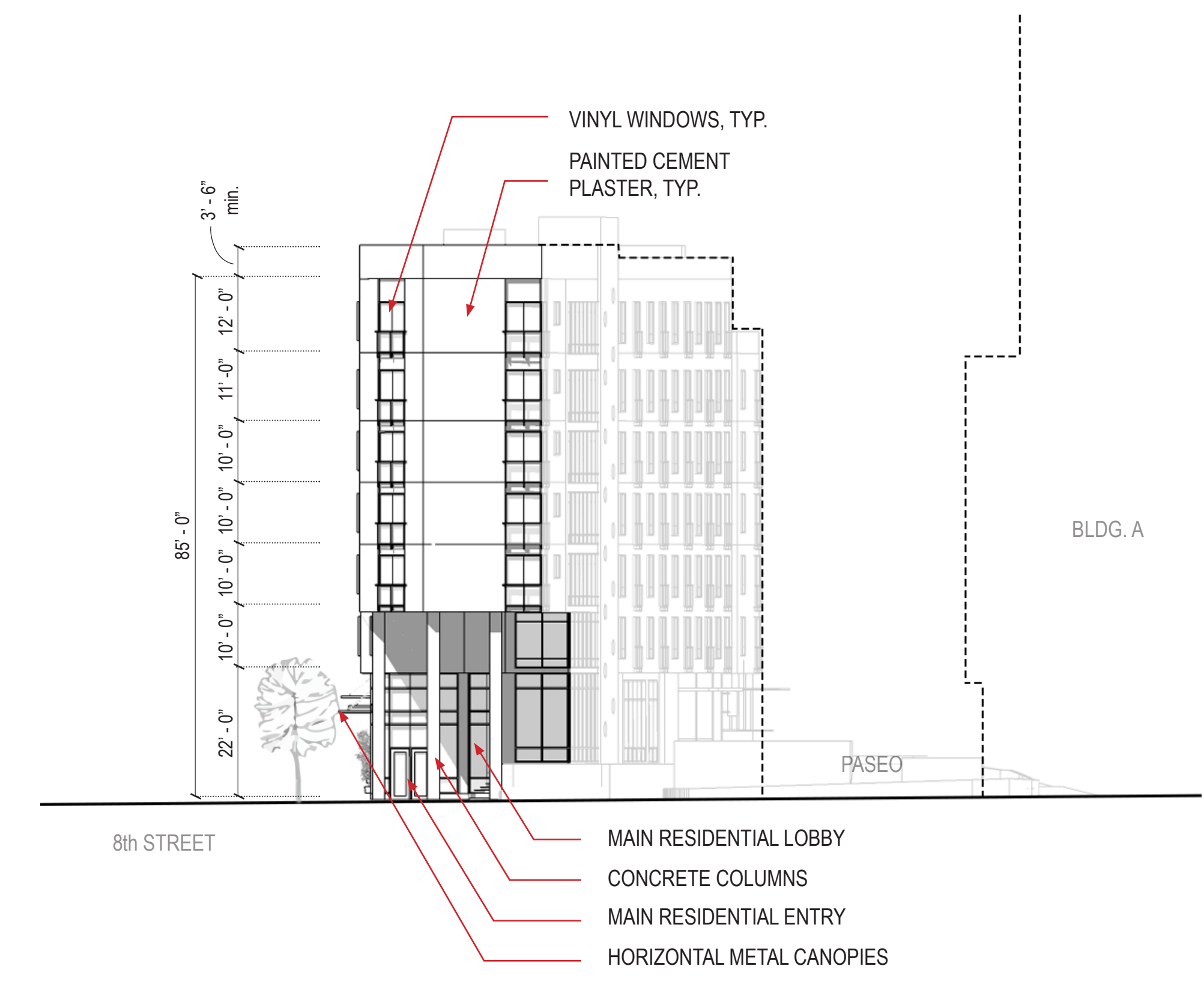
**BLOCK 1 BUILDING B ELEVATIONS**

**NOTE:**

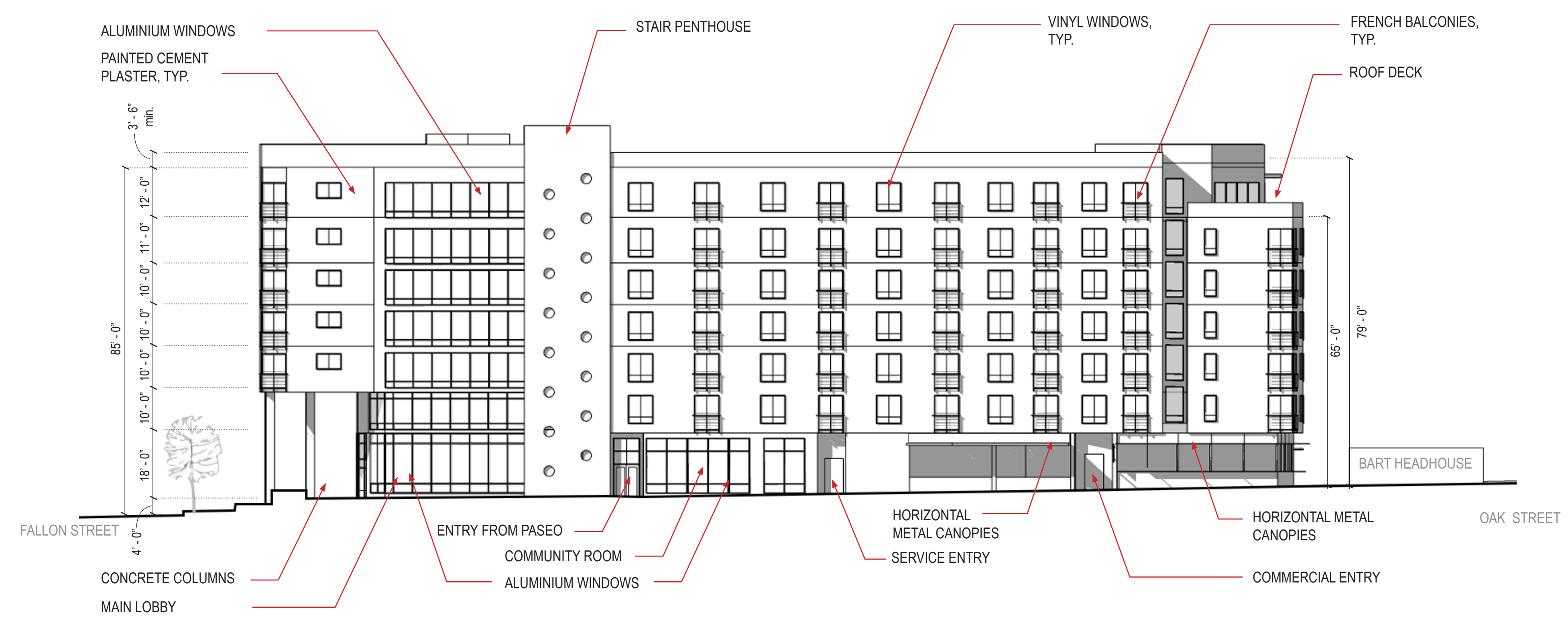
- WINDOWS AND BALCONY/DECKS FACING BART HEADHOUSE AREA NEED TO MITIGATE TO ENSURE THAT NO ITEMS CAN BE THROWN FROM THE ROOF OR WINDOW ONTO OR OVER THE HEADHOUSE AND SKYLIGHT AREA. DETAILS WILL BE PROVIDED LATER TO BE REVIEW AND APPROVED BY BART/CITY OF OAKLAND.
- COLORS AND MATERIALS TO BE FINALIZED AT FDP PHASE.



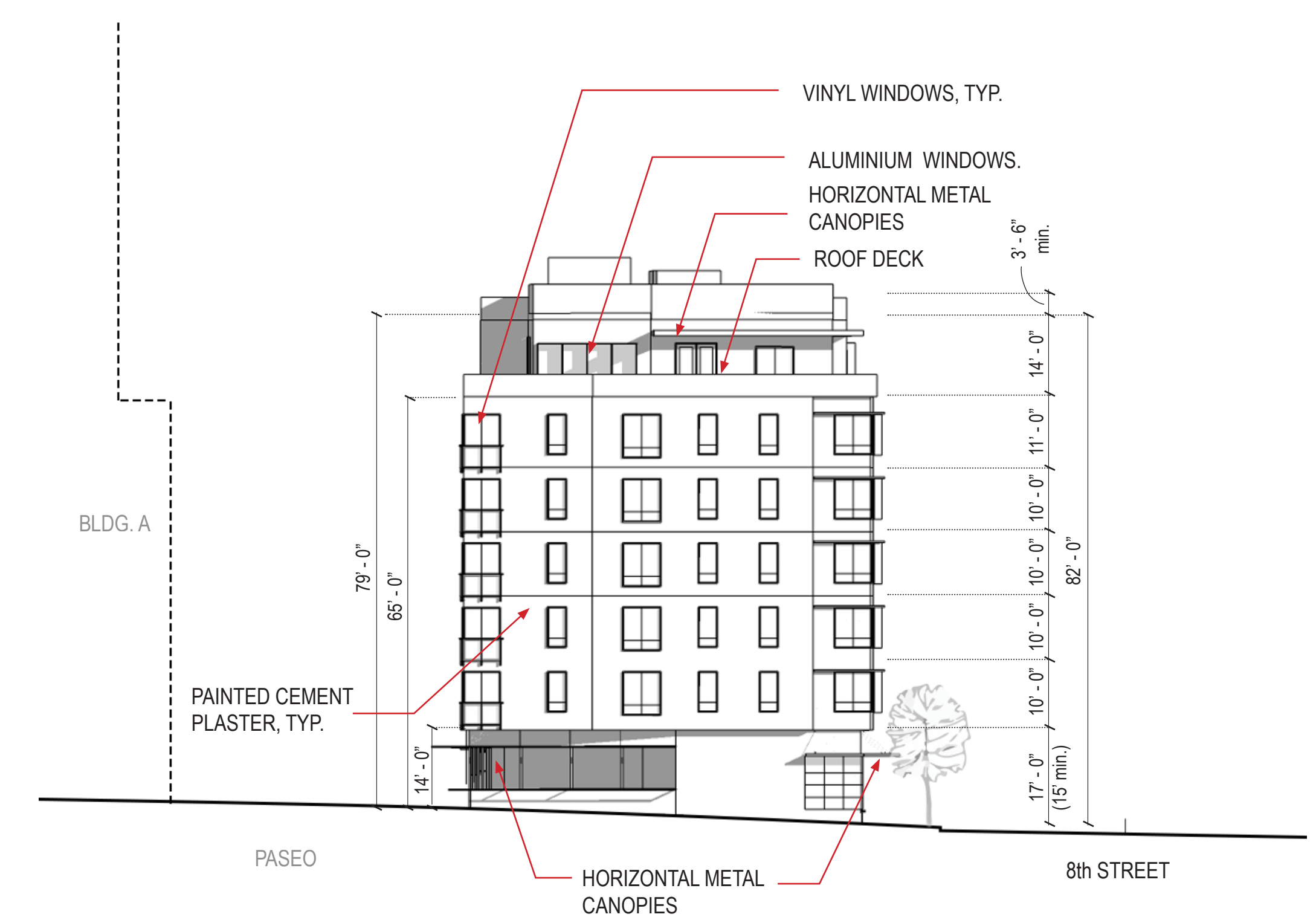
**2. BUILDING B SOUTH ELEVATION**



**1. BUILDING B EAST ELEVATION**

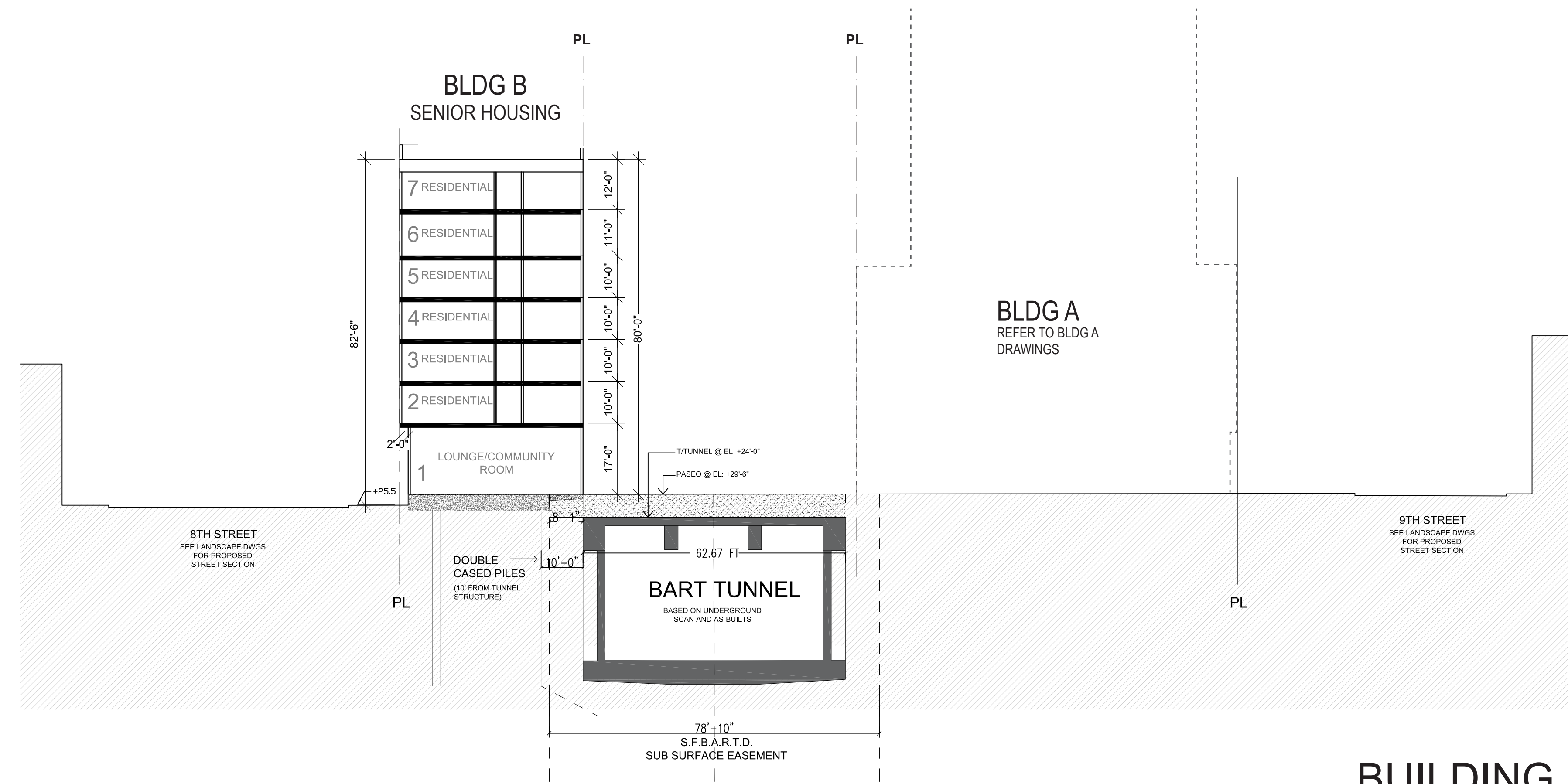


**4. BUILDING B NORTH ELEVATION**

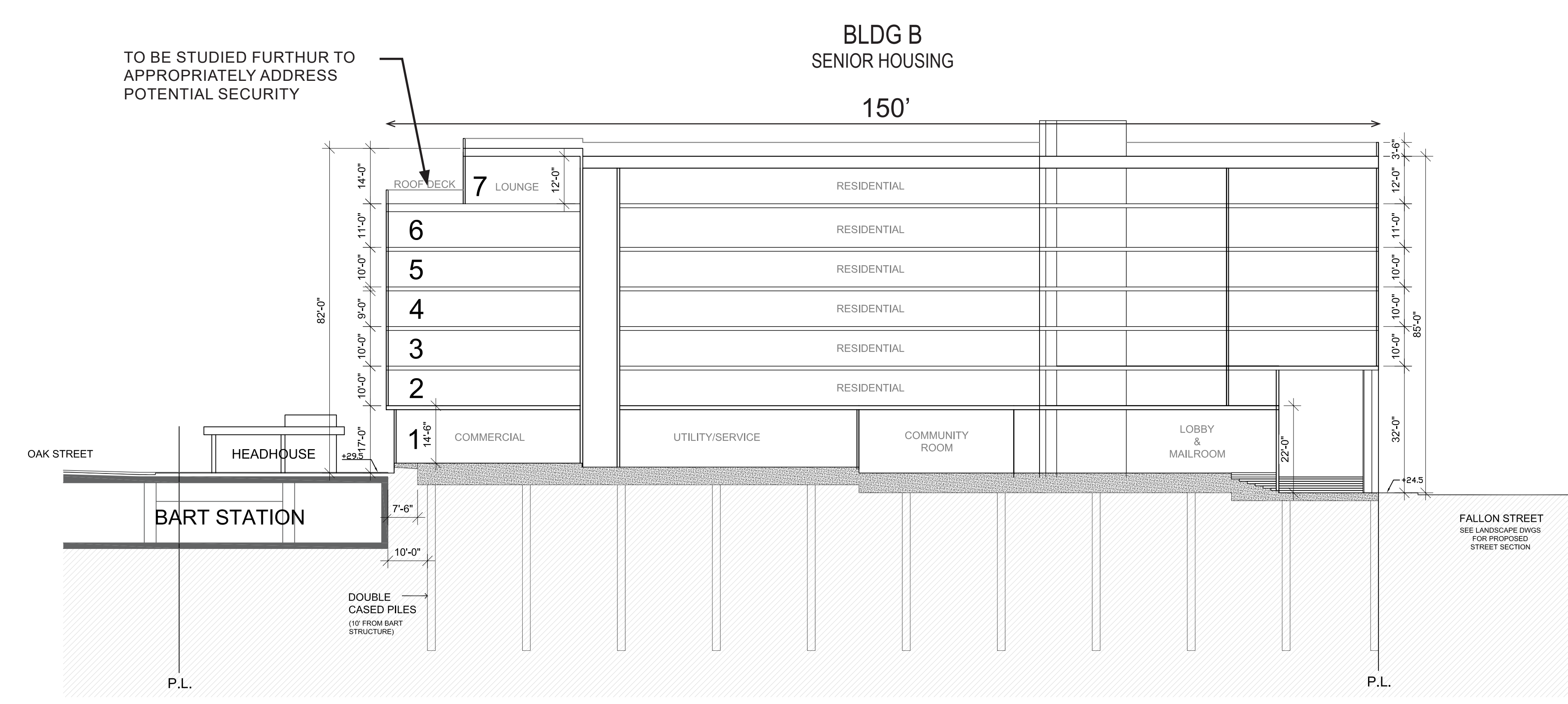


**3. BUILDING B WEST ELEVATION**





**BUILDING B - SECTION A**



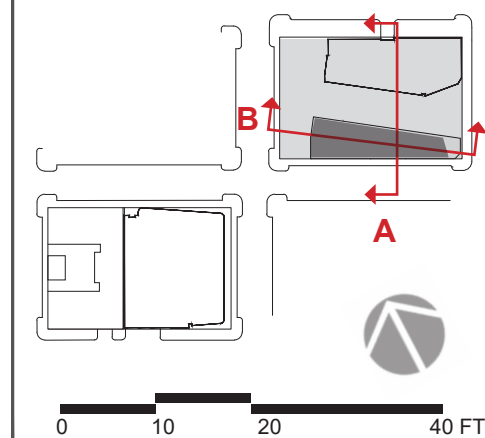
**BUILDING B - SECTION B**

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDP#2 COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDP#3 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP#5 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP#4 COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 BUILDING B SECTIONS**

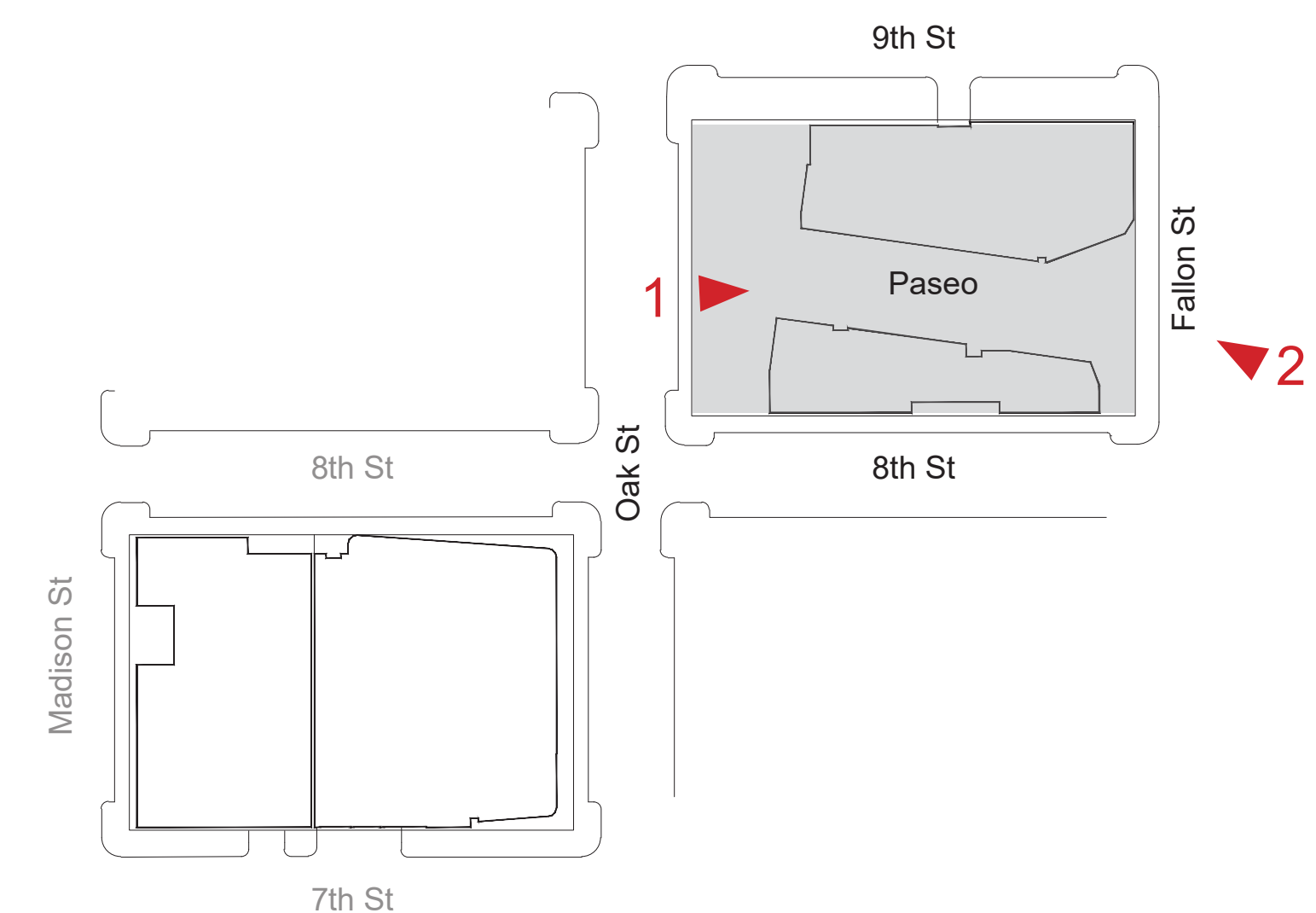




FALLON STREET  
VIEW TOWARDS THE PASEO



PASEO  
VIEW TOWARDS EAST



EAST BAY ASIAN LOCAL  
DEVELOPMENT CORPORATION  
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OAKLAND, CA 94612

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ARCHITECTS  
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ARCHITECTS  
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OAKLAND, CA 94607  
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www.inwilleruehl.com

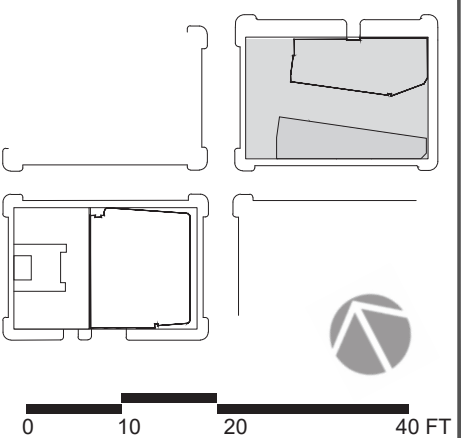
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**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

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1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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5	REVISED RESPONSE TO PDPA'S COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPA'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA'S COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 /  
CONCEPT 3D  
VIEWS**



**NOTE:**

- SEE C2.1, C2.2, C6.1, C6.2 FOR FIRE HYDRANT LOCATIONS

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
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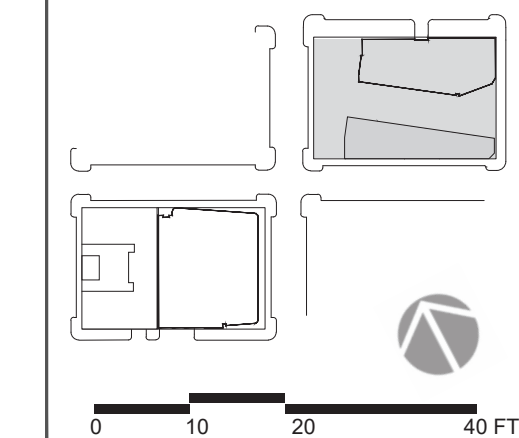
**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE

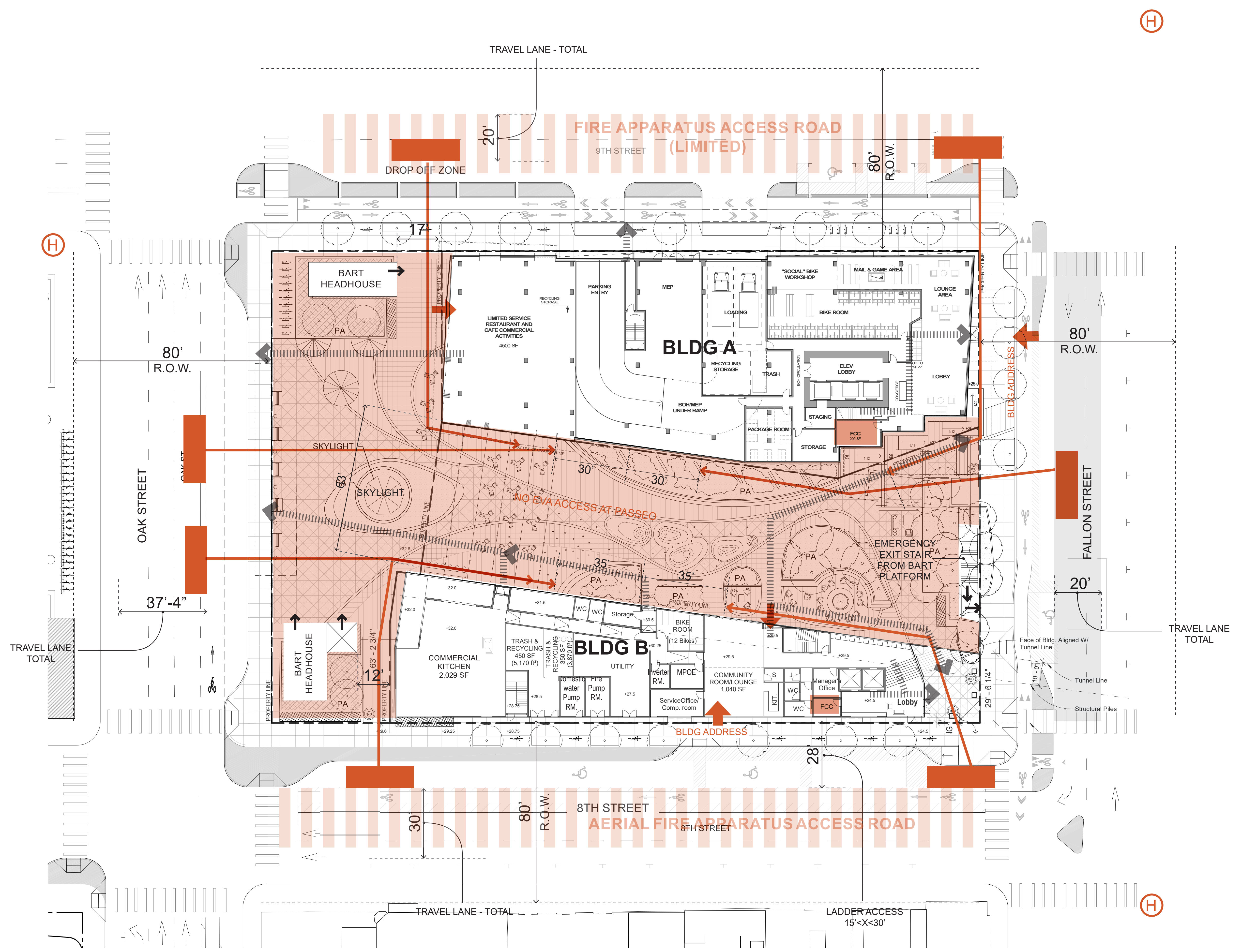
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3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDP'S COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDP'S COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP'S COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 FIRE ACCESS DIAGRAM**

**A2.23**



**BUILDING A:**  
27 STORIES/ 275' HEIGHT  
RESIDENTIAL/MIXED USE  
TYPE I  
FULLY SPRINKLERED

**BUILDING B:**  
7 STORIES/ 85' HEIGHT  
RESIDENTIAL/MIXED USE  
TYPE III OVER TYPE I  
FULLY SPRINKLERED

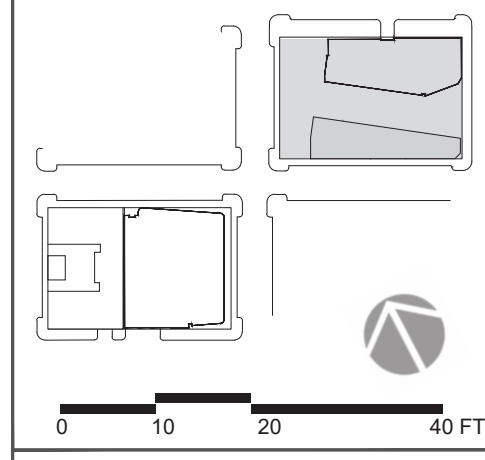
**KEY LEGEND**

- EXISTING HYDRANT LOCATION
- NO EVA ACCESS
- 150' HOSE DISTANCE
- FIRE TRUCK
- AERIAL FIRE APPARATUS ACCESS ROAD (26 FEET WIDE)
- NON-AERIAL FIRE APPARATUS ACCESS ROAD (20 FEET WIDE)
- PRIMARY BLDG ENTRANCE
- EXIT

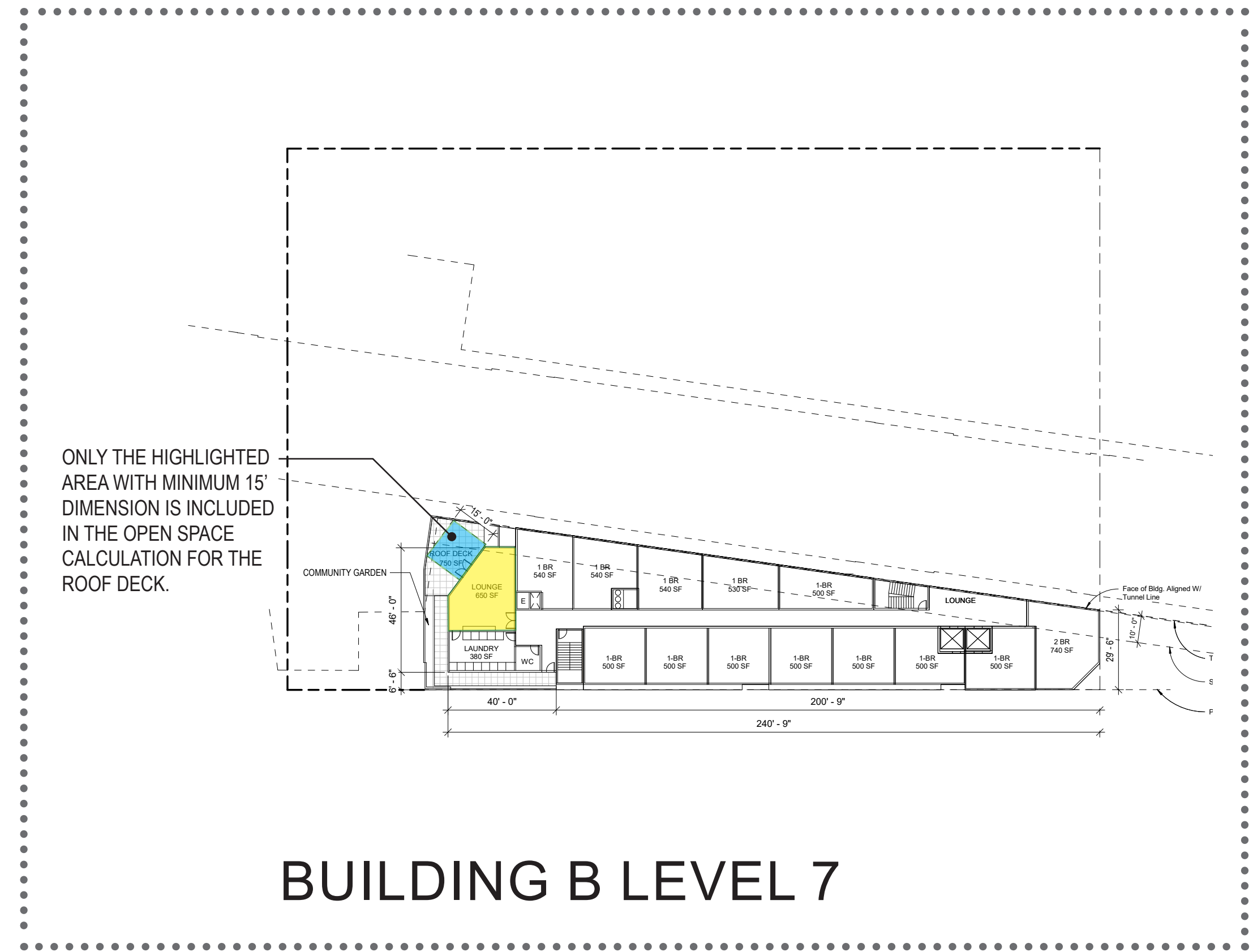


REVISION SCHEDULE

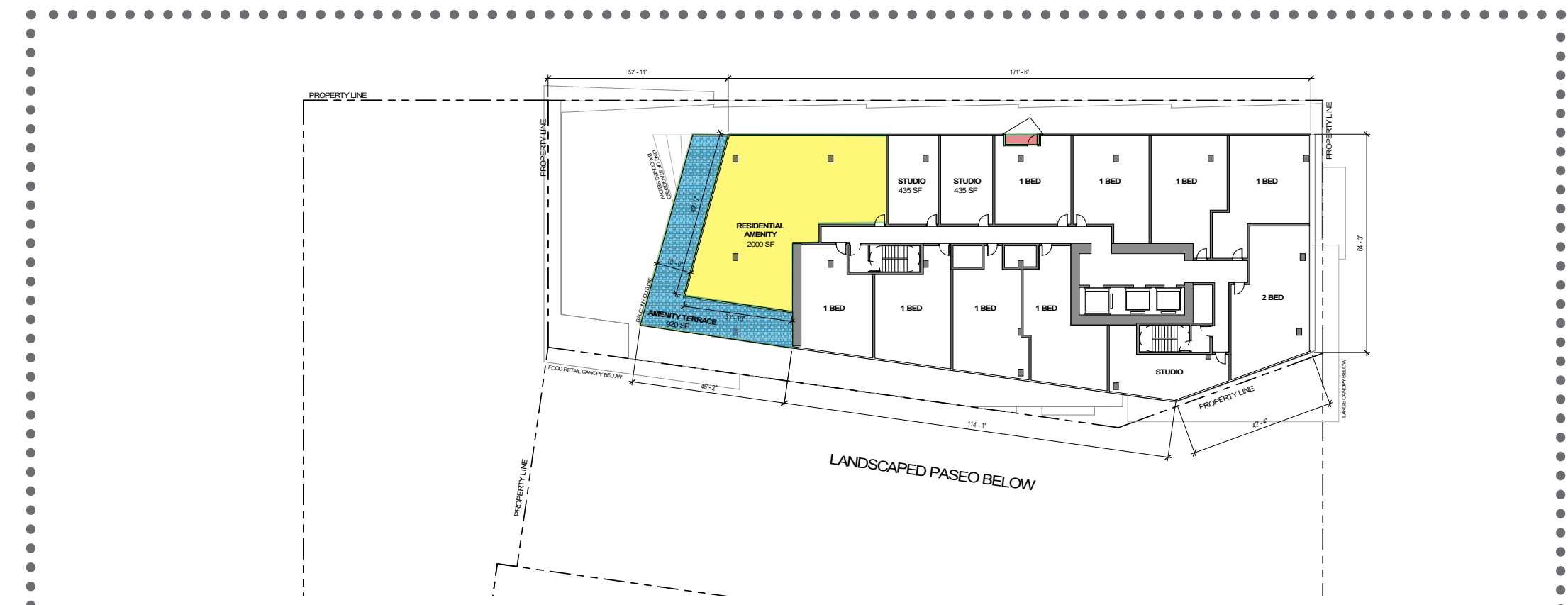
NO.	ISSUE	DATE
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2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
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6	REVISED RESPONSE TO POPZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPZ COMMENTS	03/19/2021



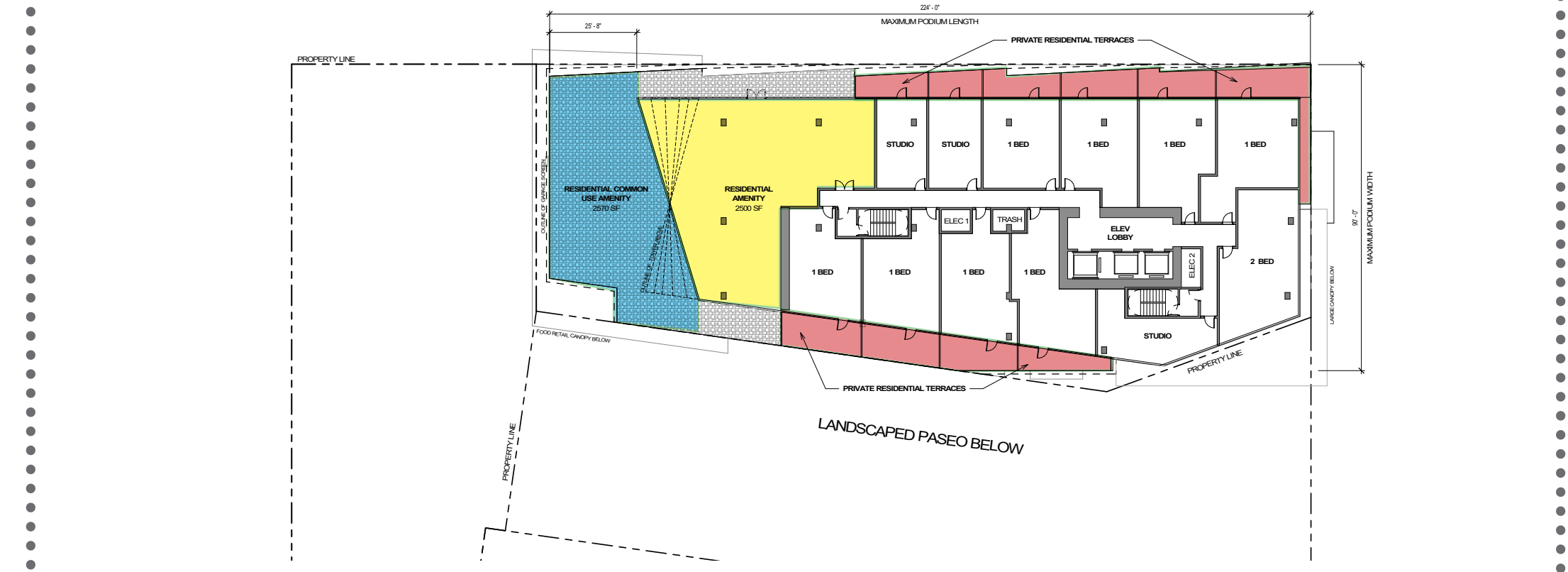
DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 OPEN SPACE EXHIBIT**



**BUILDING B LEVEL 7**

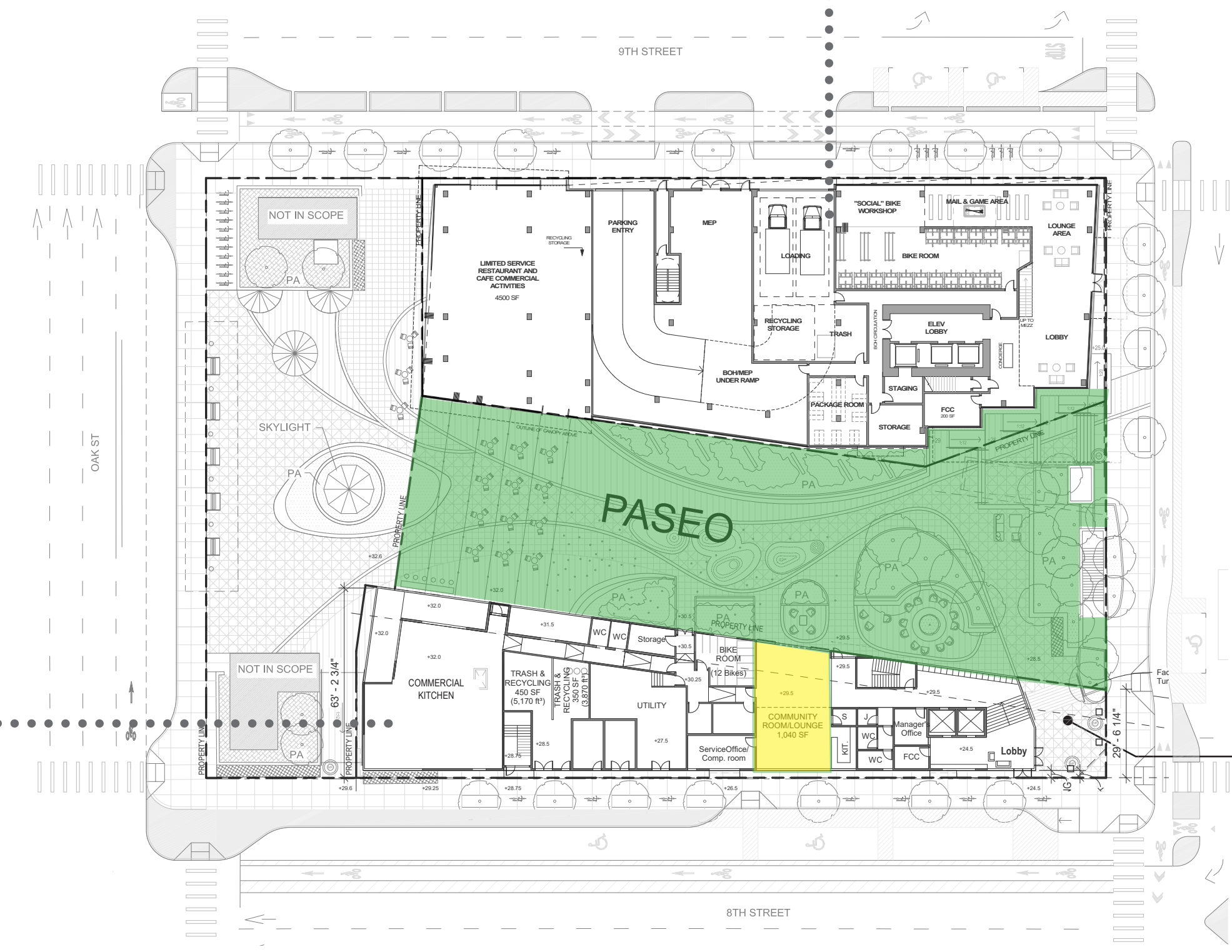


**BUILDING A LEVEL 28**



**BUILDING A LEVEL 05**

- PUBLICLY ACCESSIBLE OPEN SPACE
- GROUP USEABLE INDOOR OPEN SPACE (FOR BLDG OCCUPANTS)
- GROUP USEABLE OPEN SPACE (FOR BLDG OCCUPANTS)
- PRIVATE USEABLE OPEN SPACE (FOR UNIT OCCUPANTS)



**BLOCK 1 - GROUND LEVEL**

COVERED PUBLICLY ACCESSIBLE (NOT INCLUDED IN OPEN SPACE CALCULATION)

	BUILDING A	BUILDING B	TOTAL
PUBLICLY ACCESSIBLE OPEN SPACE (PASEO)	12,914	3,152	15,761
GROUP USEABLE INDOOR OPEN SPACE (FOR BUILDING OCCUPANTS)	4,500	1,690	6,190
GROUP USEABLE OPEN SPACE (FOR BLDG OCCUPANTS)	3,490	250	3,740
PRIVATE USEABLE OPEN SPACE (FOR UNIT OCCUPANTS)	12,900	-	12,900
<b>TOTAL</b>	<b>33,804</b>	<b>5,092</b>	<b>38,591</b>

**NOTE:**

\*Publicly Accessible Open Space = Paseo (minus BART exit stair & Exhaust Shaft)

\*\* 20% of the Paseo is used for Bldg. B open space calc.

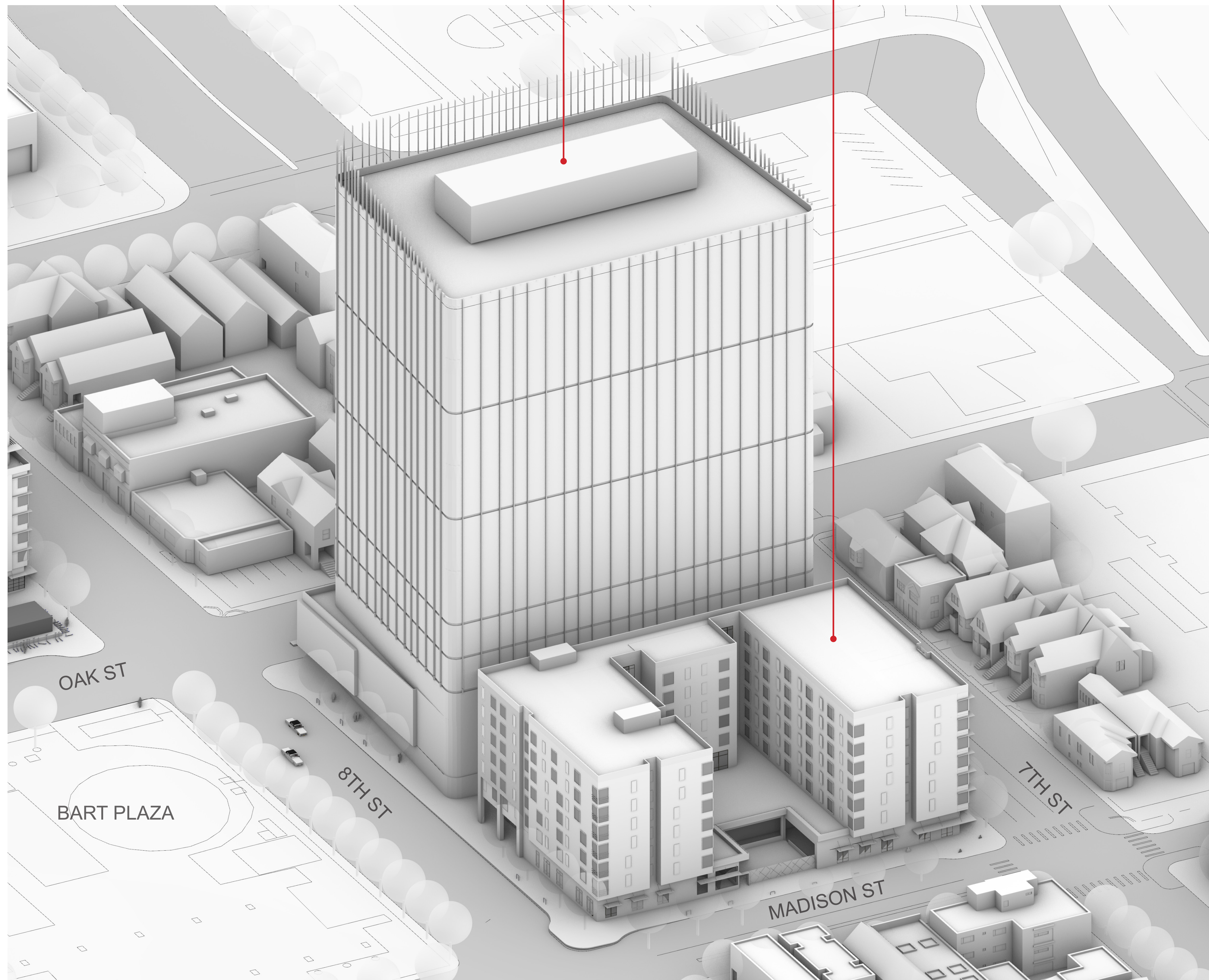
\*\*\* 80% of the Paseo is used for Bldg. A open space calc.

\*\*\*\* Publicly Accessible Open Space includes 305 SF actually on Bldg A parcel.



BUILDING C:  
OFFICE

BUILDING D:  
AFFORDABLE  
HOUSING



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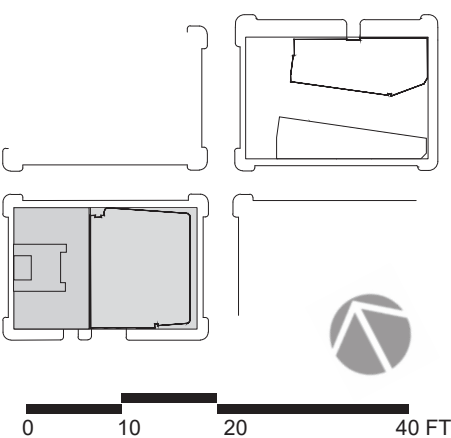
**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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7	REVISED RESPONSE TO POPRZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 2  
MASSING VIEWS**

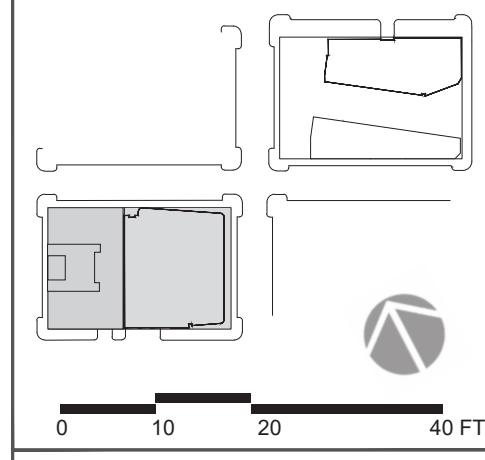


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Oakland, CA 94607

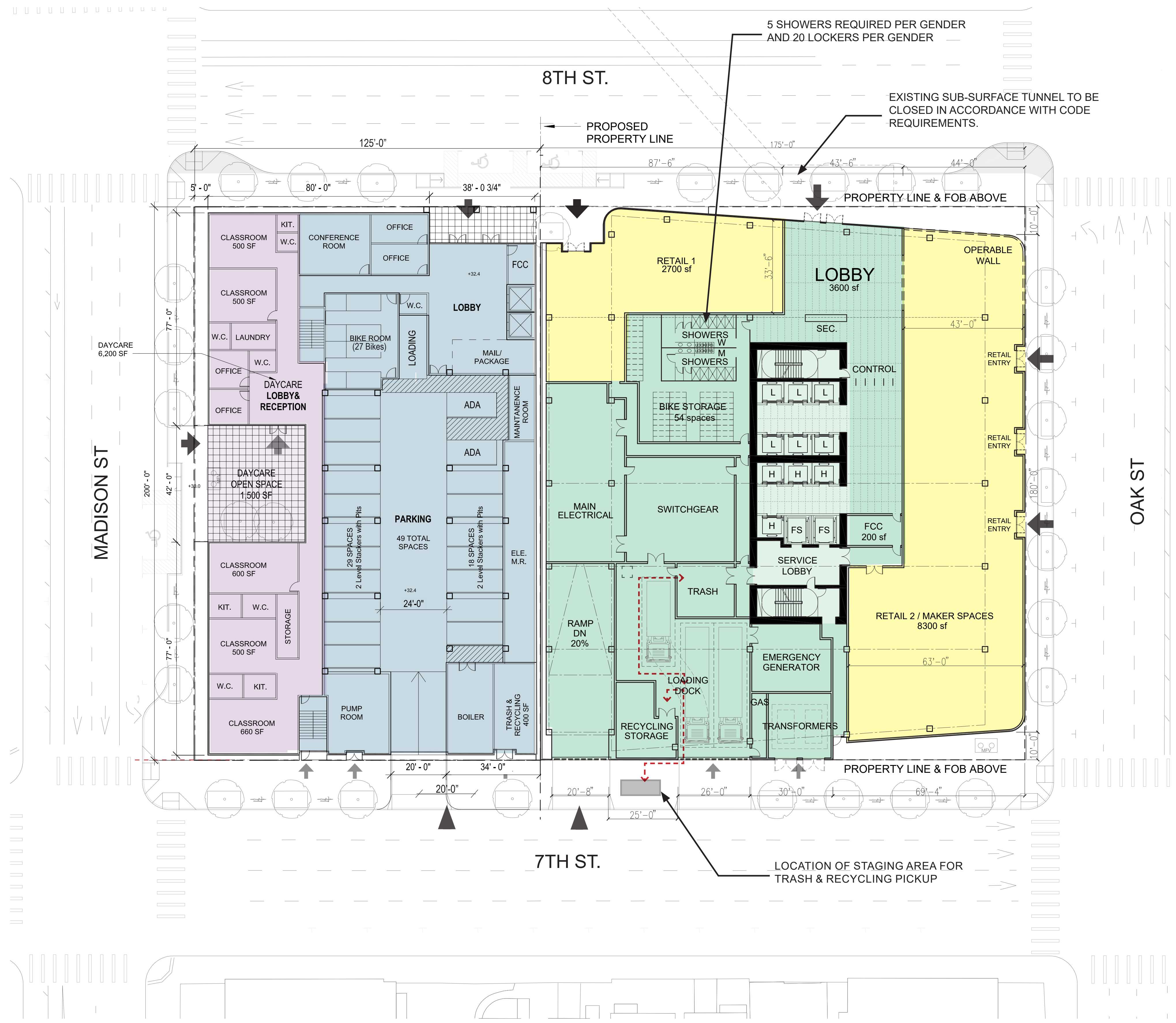
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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 L1 FLOOR PLAN**



5 SHOWERS REQUIRED PER GENDER AND 20 LOCKERS PER GENDER

EXISTING SUB-SURFACE TUNNEL TO BE CLOSED IN ACCORDANCE WITH CODE REQUIREMENTS.

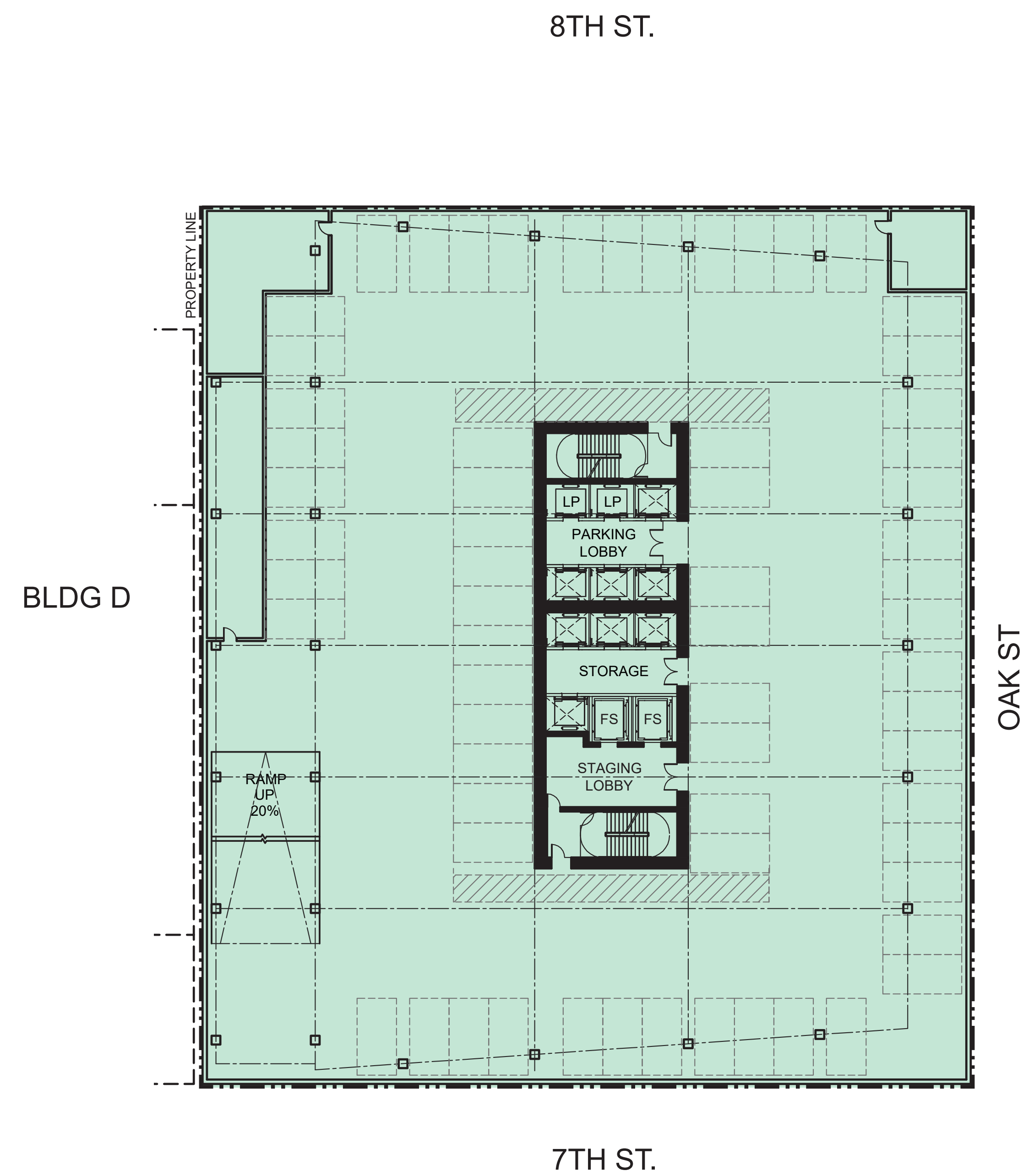
BIKES REQUIRED:  
OFFICE 52 LT  
OFFICE 26 ST  
RETAIL 2 LT  
RETAIL 2ST

- KEY LEGEND**
- ➡ PRIMARY PEDESTRIAN ACCESS
  - ➡ UTILITY/SERVICES OR EMERGENCY ACCESS
  - ➡ PARKING ENTRANCE

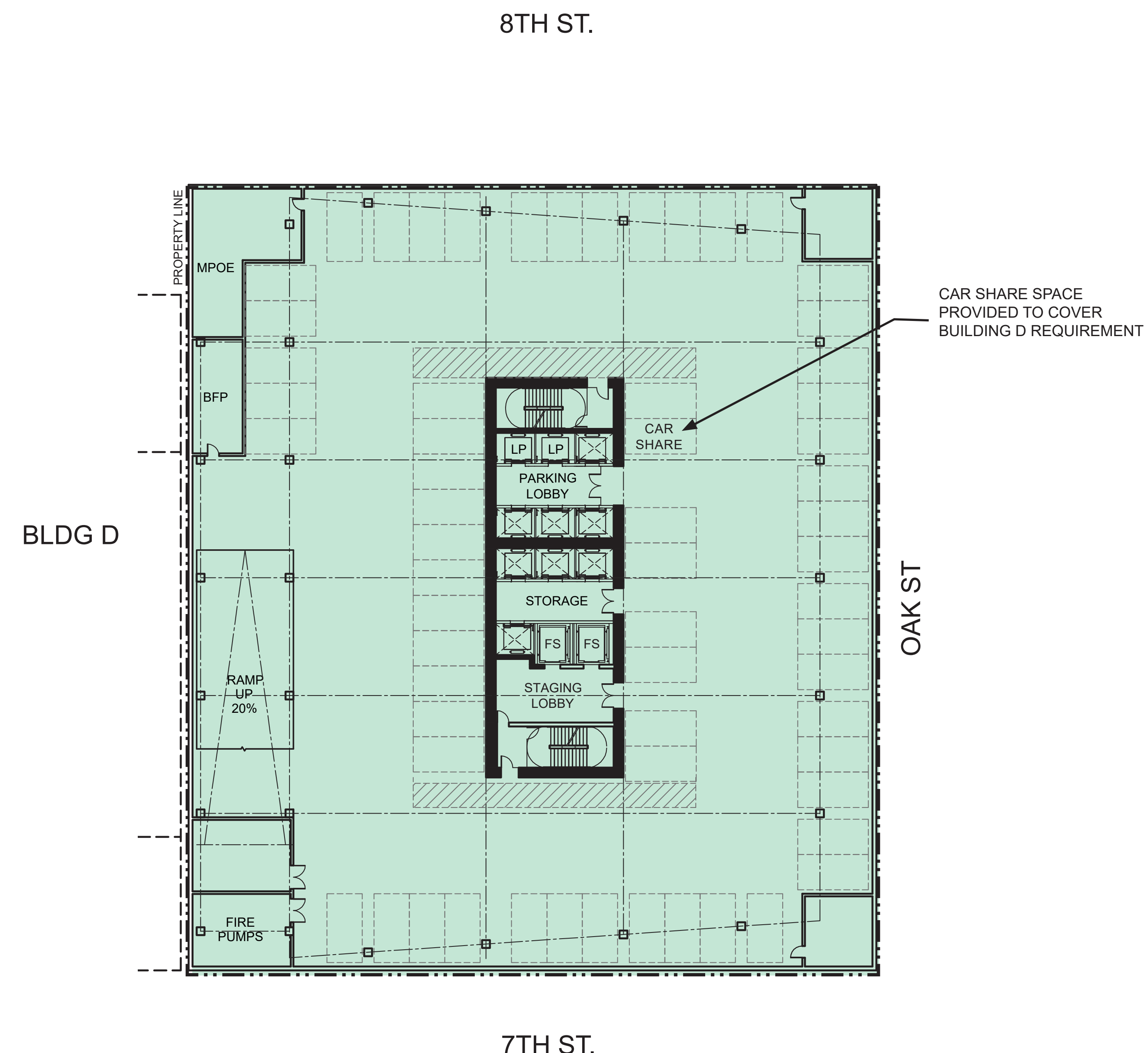
- LAND USE LEGEND**
- PERMANENT RESIDENTIAL ACTIVITIES 17.101G.01
  - BUSINESS ACTIVITIES 17.101G.01
  - GENERAL RETAIL & SALES ACTIVITIES 17.101G.01
  - DAYCARE (COMMUNITY EDUCATION CIVIC ACTIVITY) 17.142.100.A

LOCATION OF STAGING AREA FOR TRASH & RECYCLING PICKUP





B2 PARKING PLAN



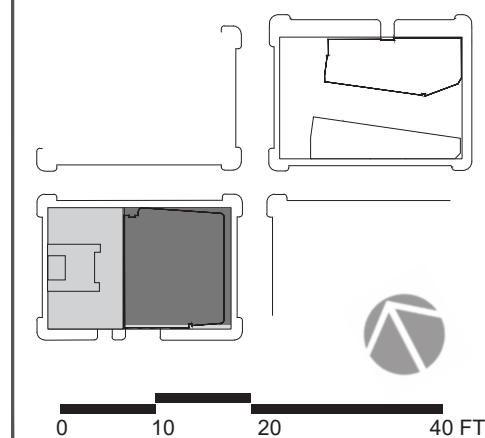
B1 PARKING PLAN

**LAND USE LEGEND  
(PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- ADMINISTRATIVE/  
COMMERCIAL ACTIVITIES
- GENERAL RETAIL & SALES ACTIVITIES

PRELIMINARY - NOT FOR CONSTRUCTION -

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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
BLOCK 2 / BLDG C  
TYPICAL FLOOR  
PLANS

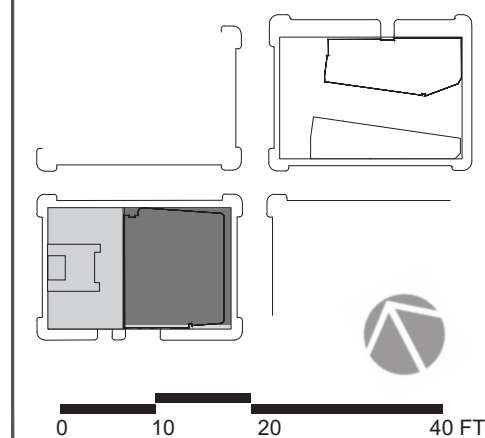


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Oakland, CA 94607

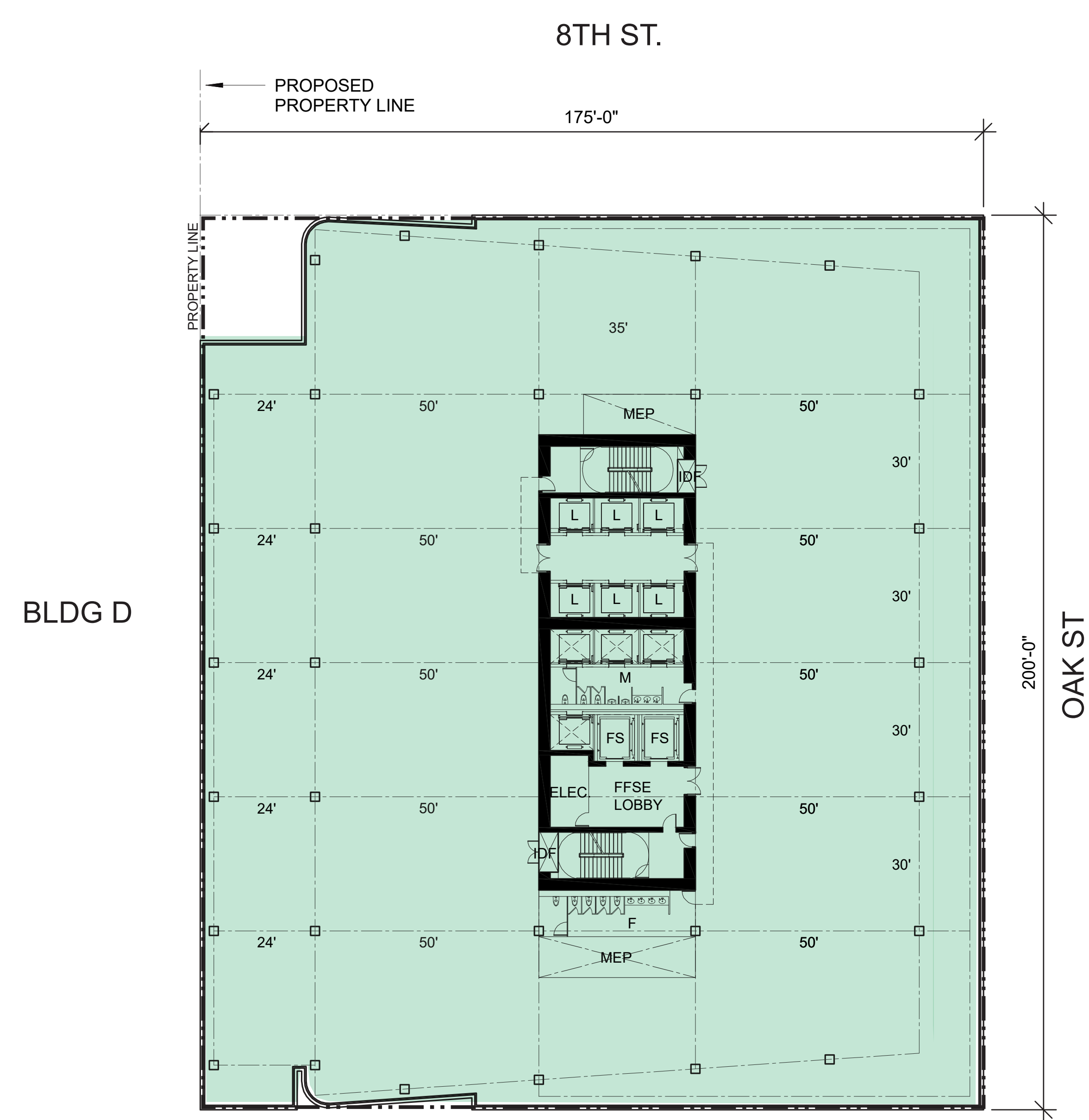
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

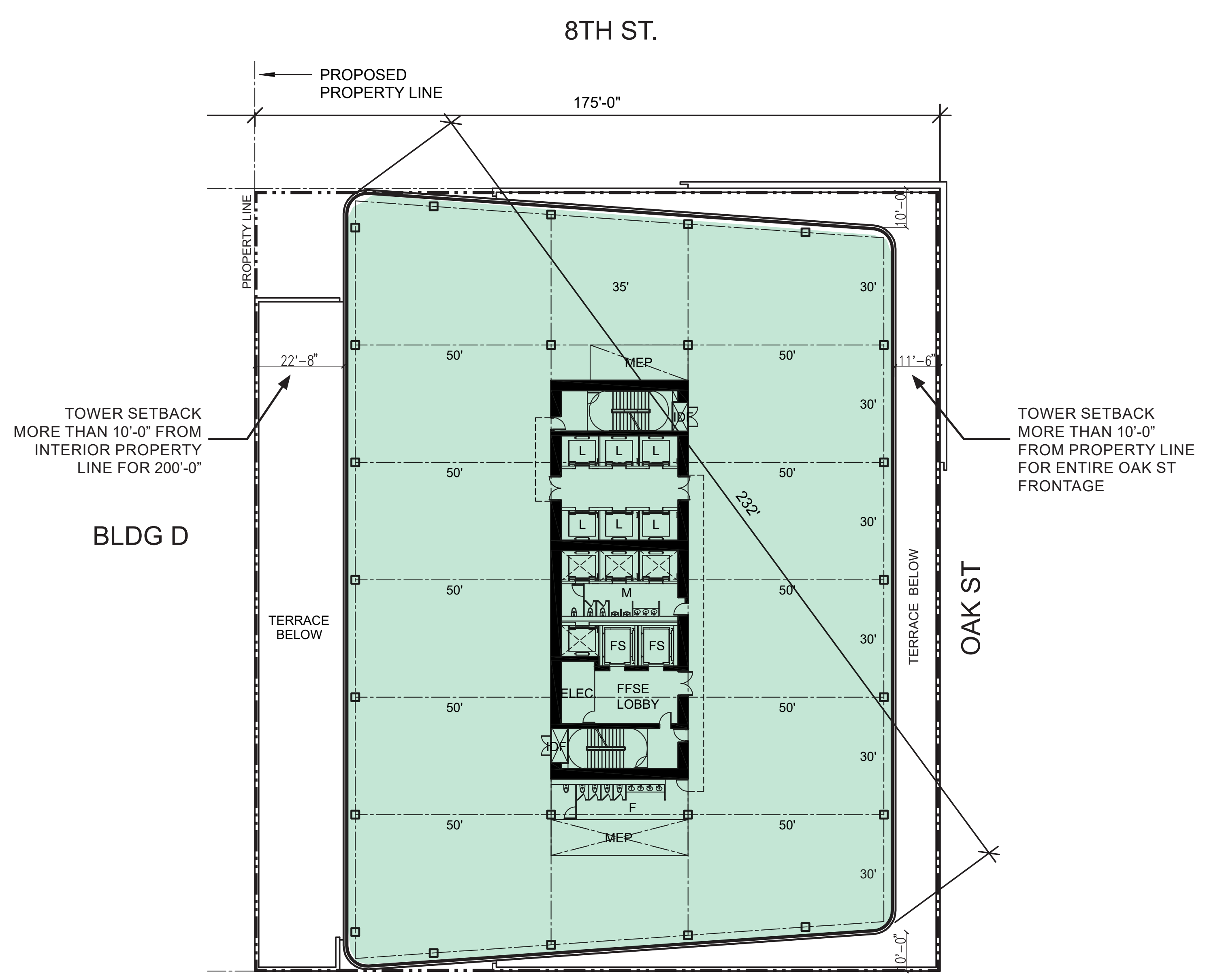
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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C TYPICAL FLOOR PLANS**



8TH ST.  
PROPOSED PROPERTY LINE  
175'-0"  
PROPERTY LINE  
200'-0"  
OAK ST  
7TH ST.  
**FLOORS 2-3 TYPICAL PLAN**



8TH ST.  
PROPOSED PROPERTY LINE  
175'-0"  
PROPERTY LINE  
22'-8"  
TERRACE BELOW  
11'-6"  
OAK ST  
7TH ST.  
**FLOORS 4-12 TYPICAL PLAN**

TOWER SETBACK MORE THAN 10'-0" FROM INTERIOR PROPERTY LINE FOR 200'-0"

TOWER SETBACK MORE THAN 10'-0" FROM PROPERTY LINE FOR ENTIRE OAK ST FRONTAGE

TOWER SETBACKS AT INTERIOR LOT LINE (WEST) AND OAK STREET FRONTAGE (EAST) BY MINIMUM 10FT FOR MIN 50% OF PERIMETER LENGTH OF BUILDING BASED UPON GRANTING OF CUP.

WEST SETBACK = 26.66% OF PERIMETER LENGTH OF BASE  
EAST SETBACK = 26.66% OF PERIMETER LENGTH OF BASE  
TOTAL SETBACK = 53.33% OF PERIMETER LENGTH OF BASE

**LAND USE LEGEND (PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- ADMINISTRATIVE/COMMERCIAL ACTIVITIES
- GENERAL RETAIL & SALES ACTIVITIES

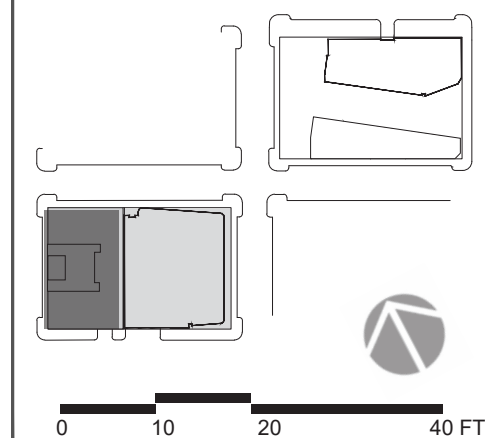


**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

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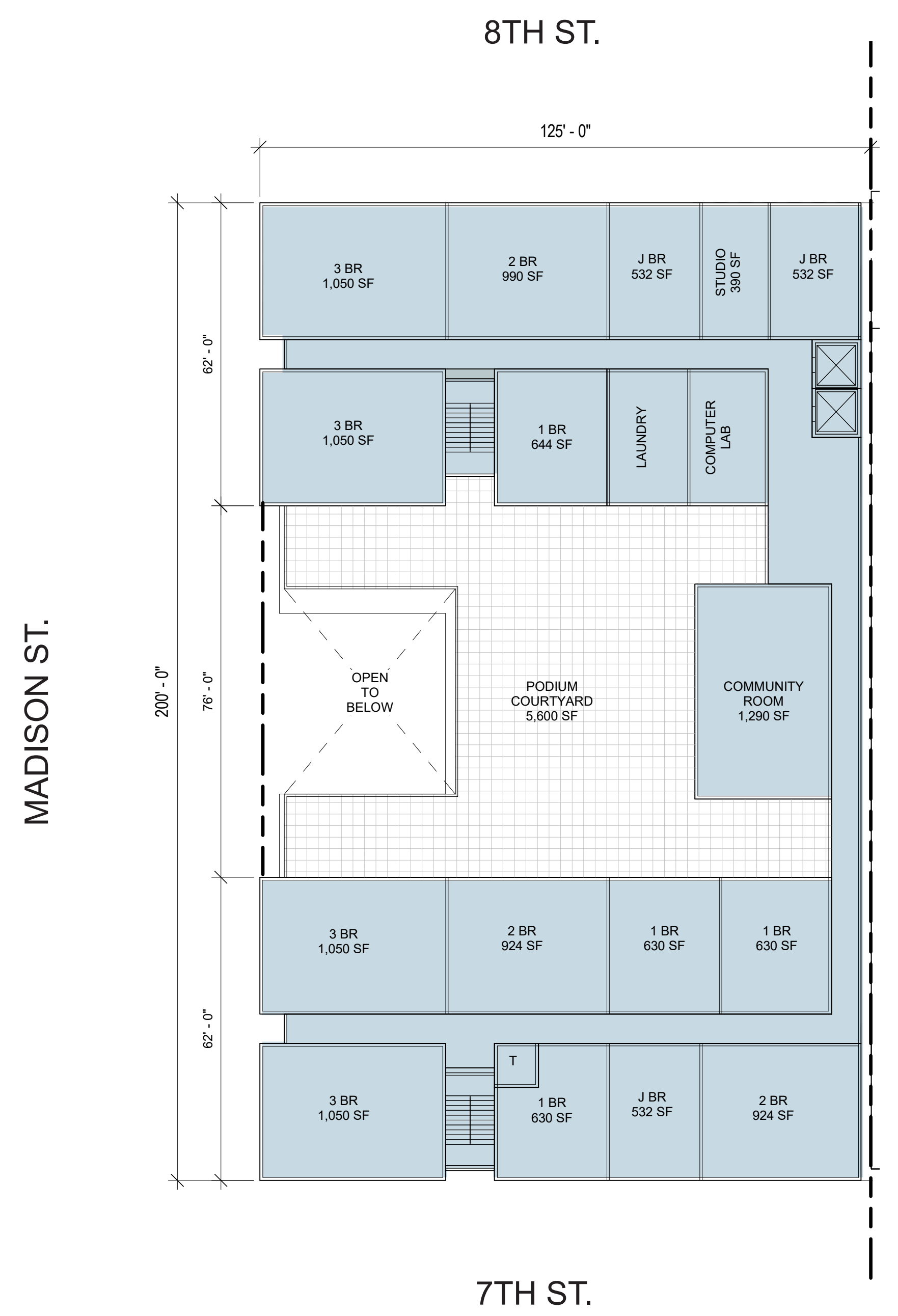


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

BLOCK 2 / BLDG D  
UPPER FLOOR  
PLANS



TYPICAL RESIDENTIAL FLOOR PLAN (L3-7)



RESIDENTIAL FLOOR PLAN AT PODIUM LEVEL (L2)

LAND USE LEGEND (PER TABLE 17.101G.01)

PERMANENT RESIDENTIAL ACTIVITIES

0 10 20 40 FT



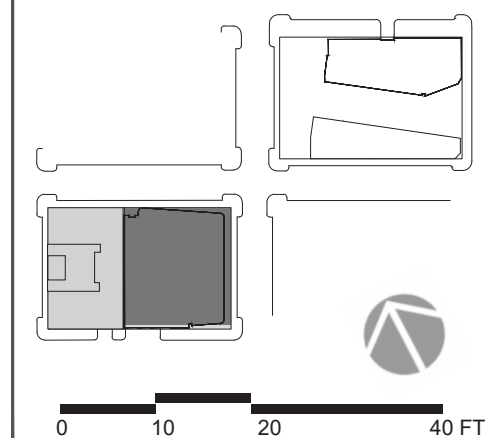
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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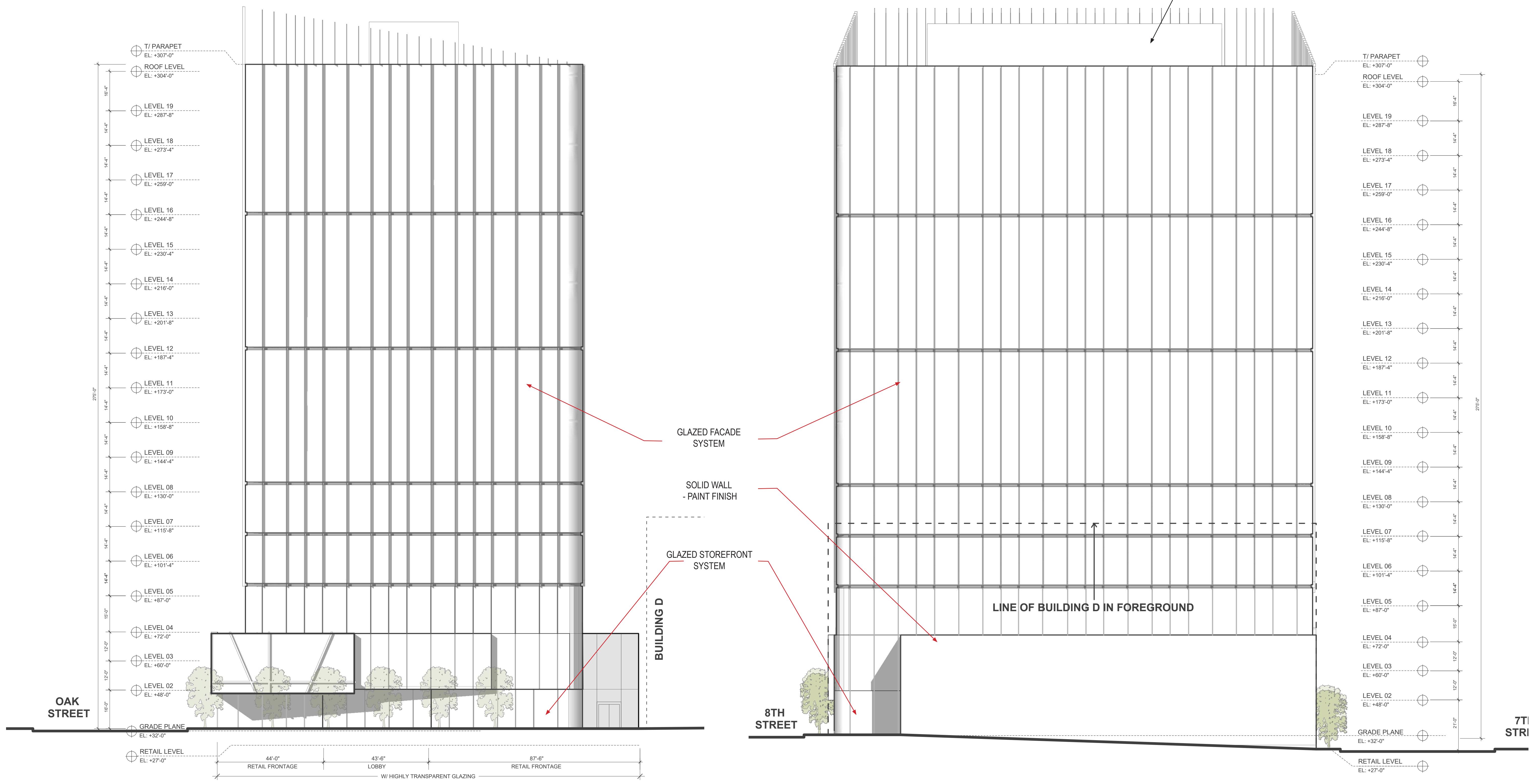
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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C ELEVATIONS**

SIZE, SPECIFICATION, ACCESS, AND LOCATION OF ROOF MOUNTED EQUIPMENT TO BE APPROVED BY BART AT A LATER PHASE OF DESIGN



**NORTH ELEVATION**

**WEST ELEVATION**

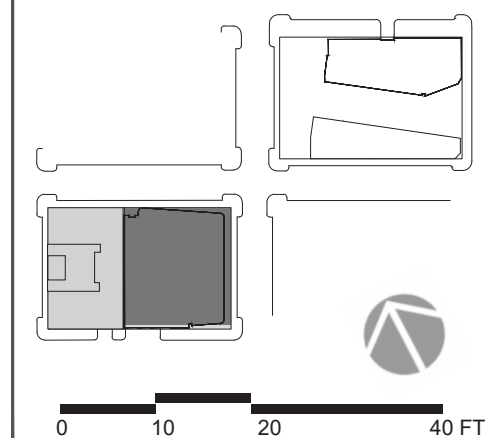


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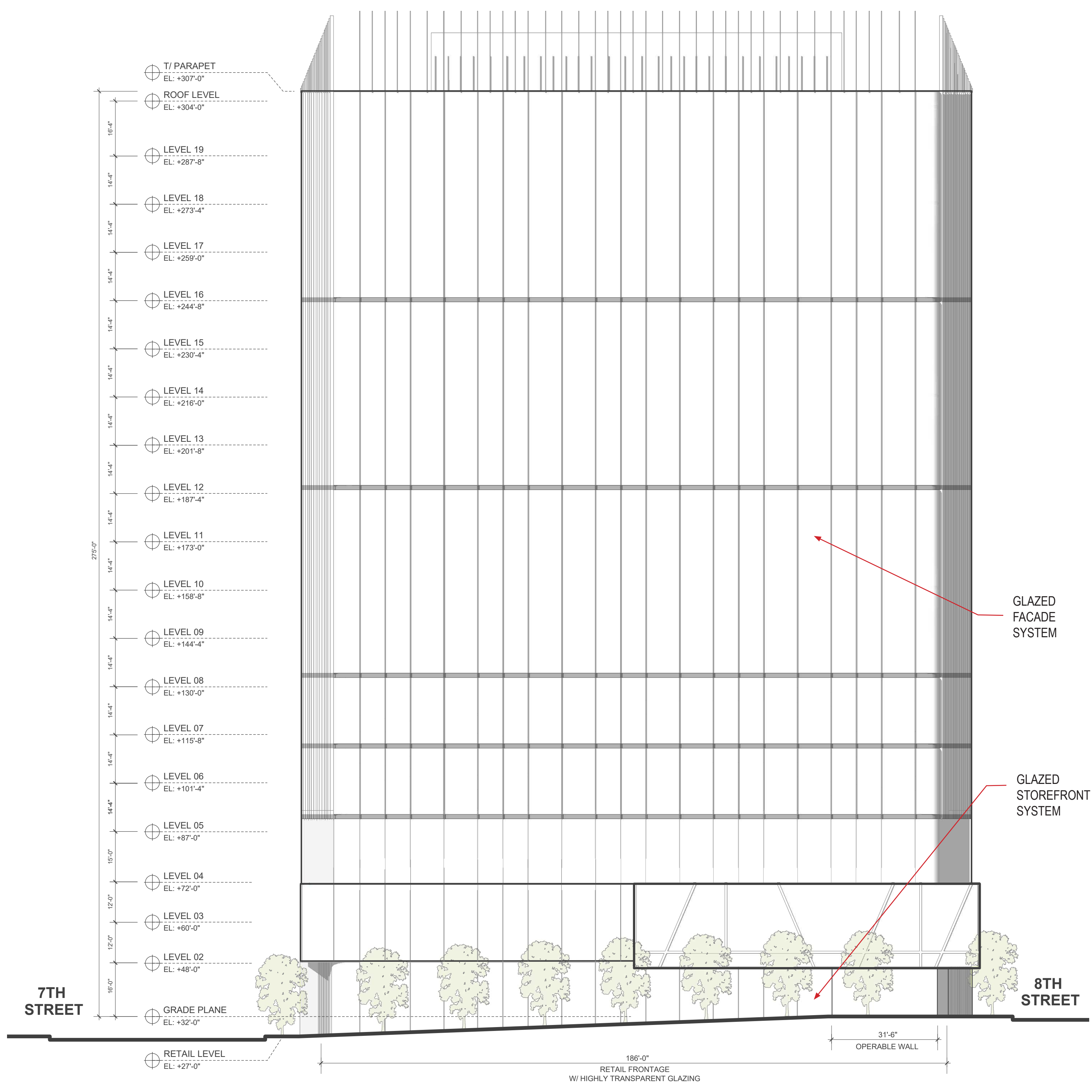
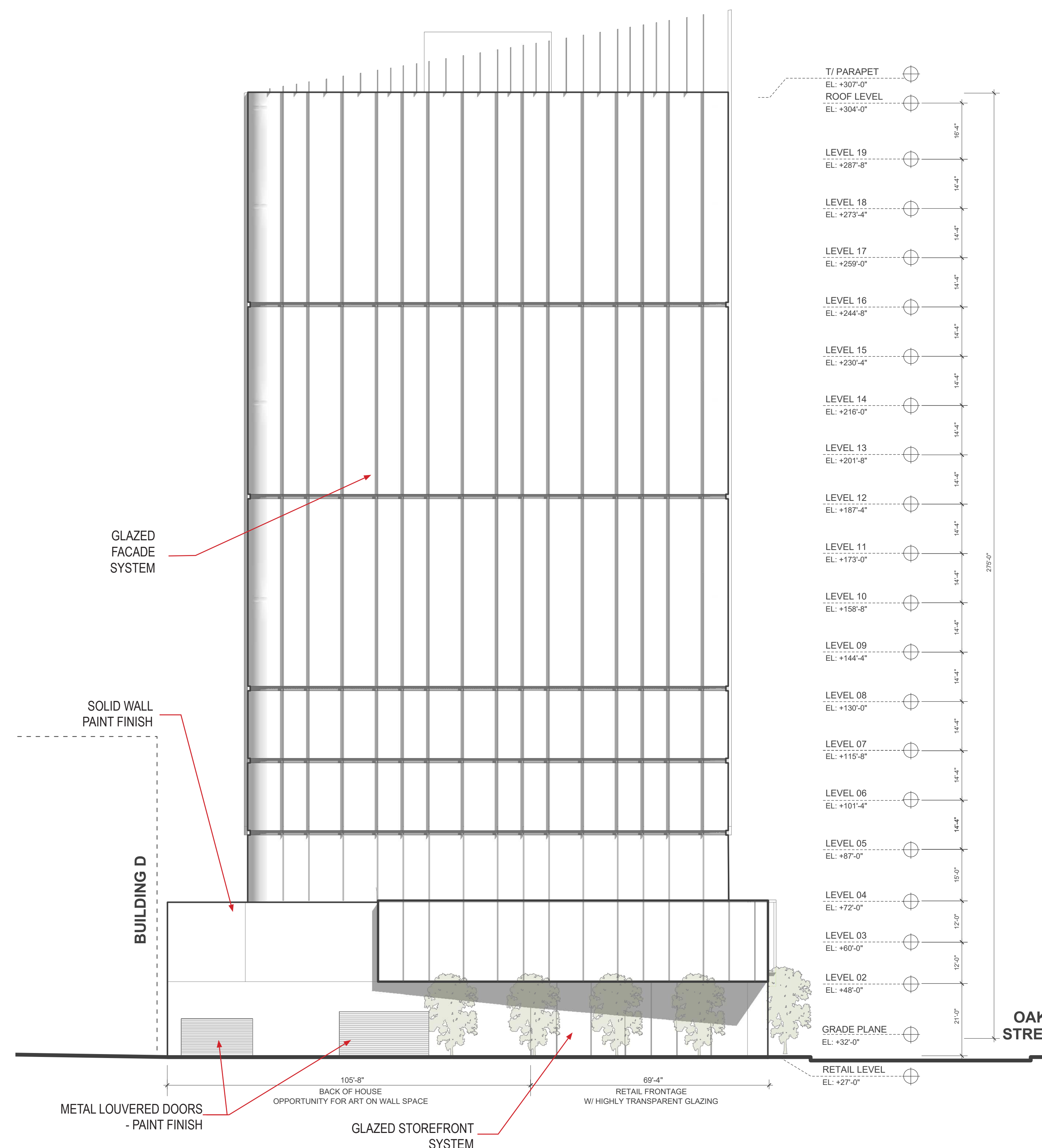
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SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C ELEVATIONS**







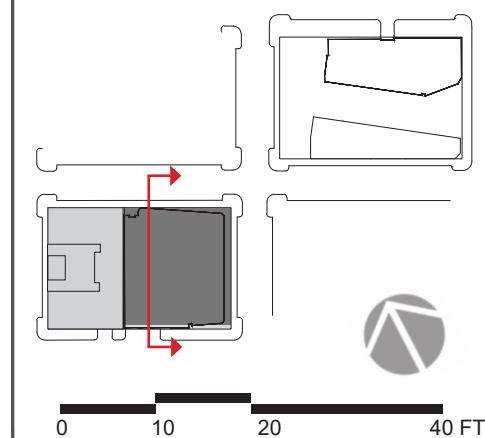


**LAKE MERRITT BART REDEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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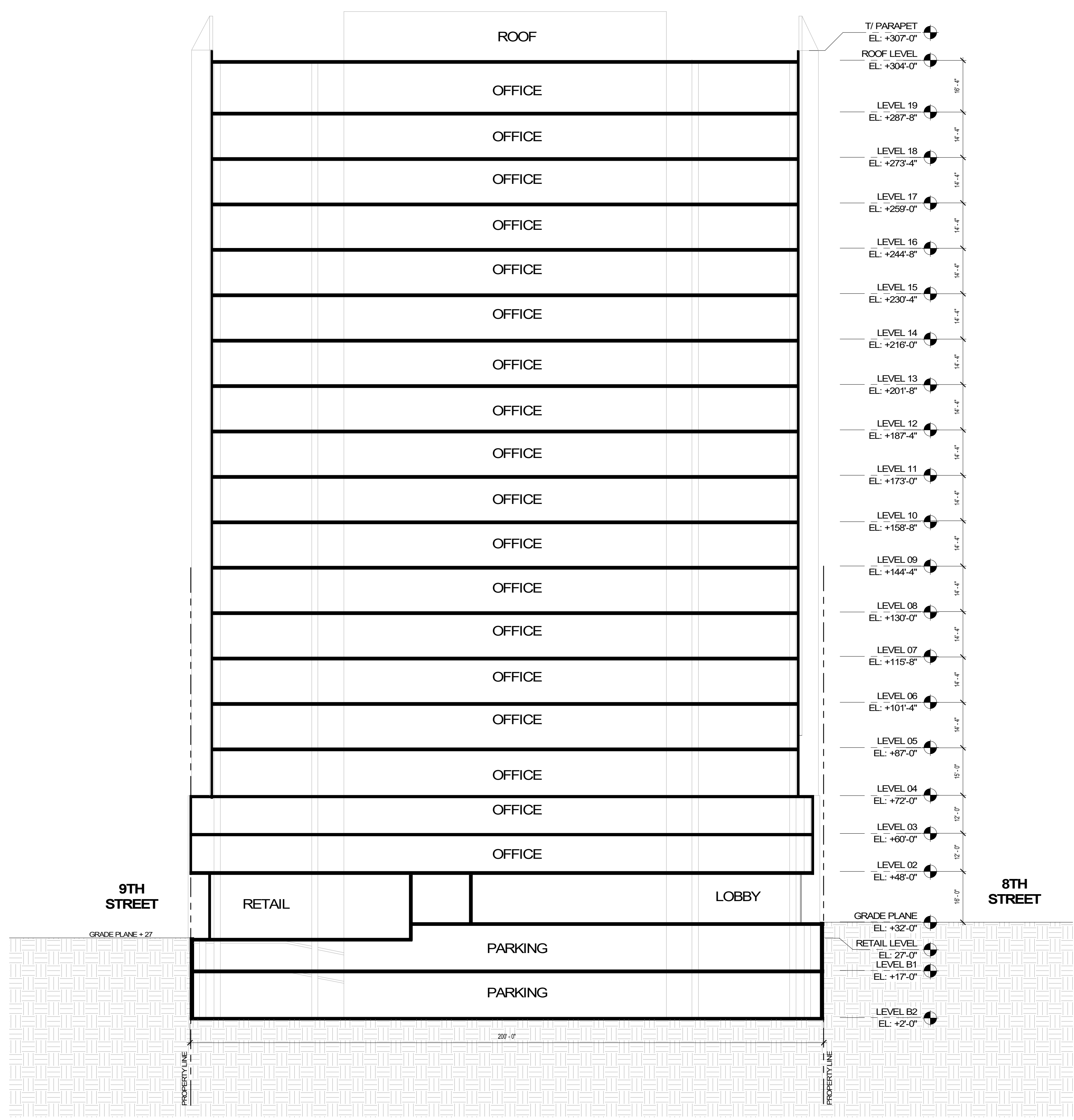
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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 2 / BLDG C NORTH-SOUTH SECTION**

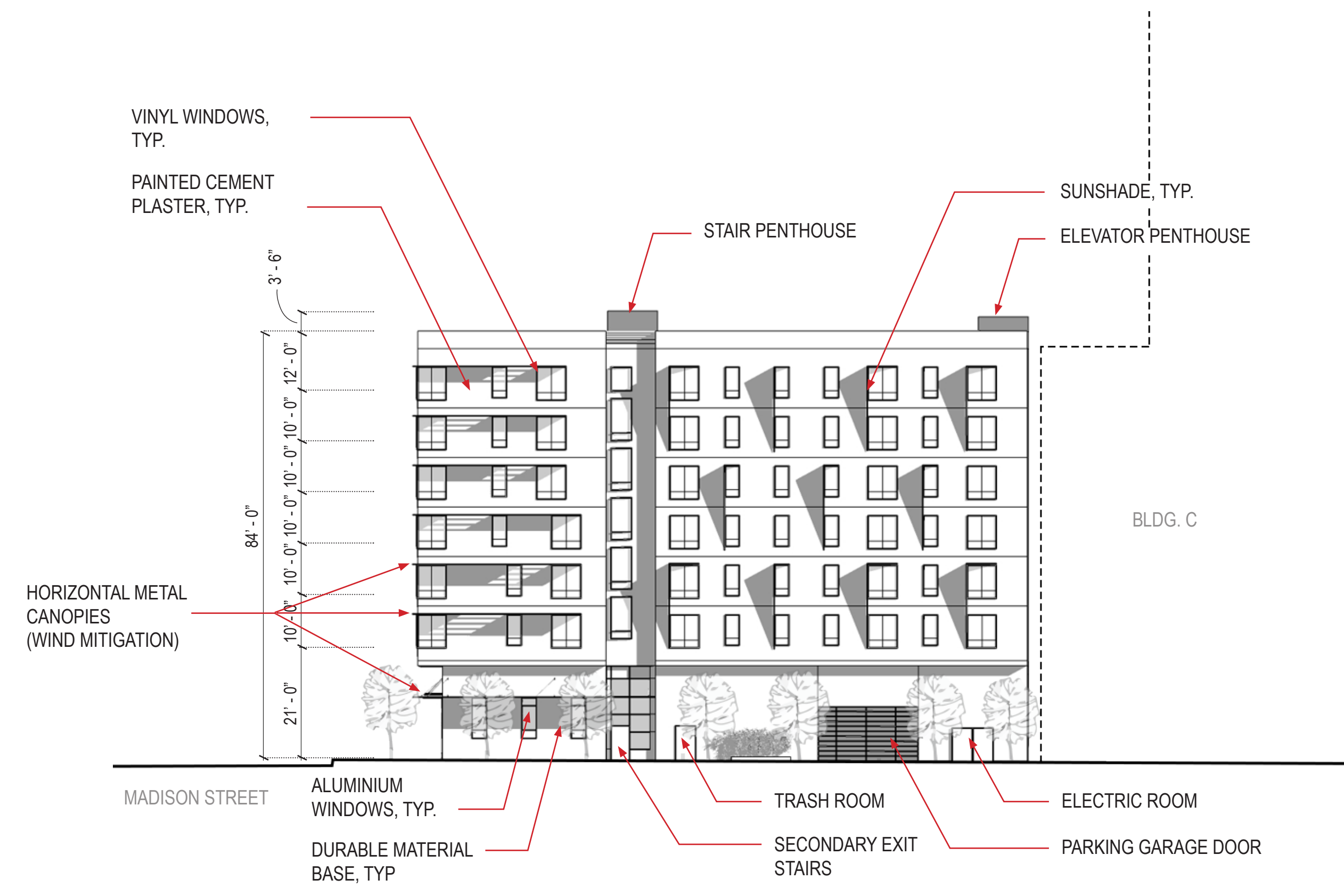
**A2.38**



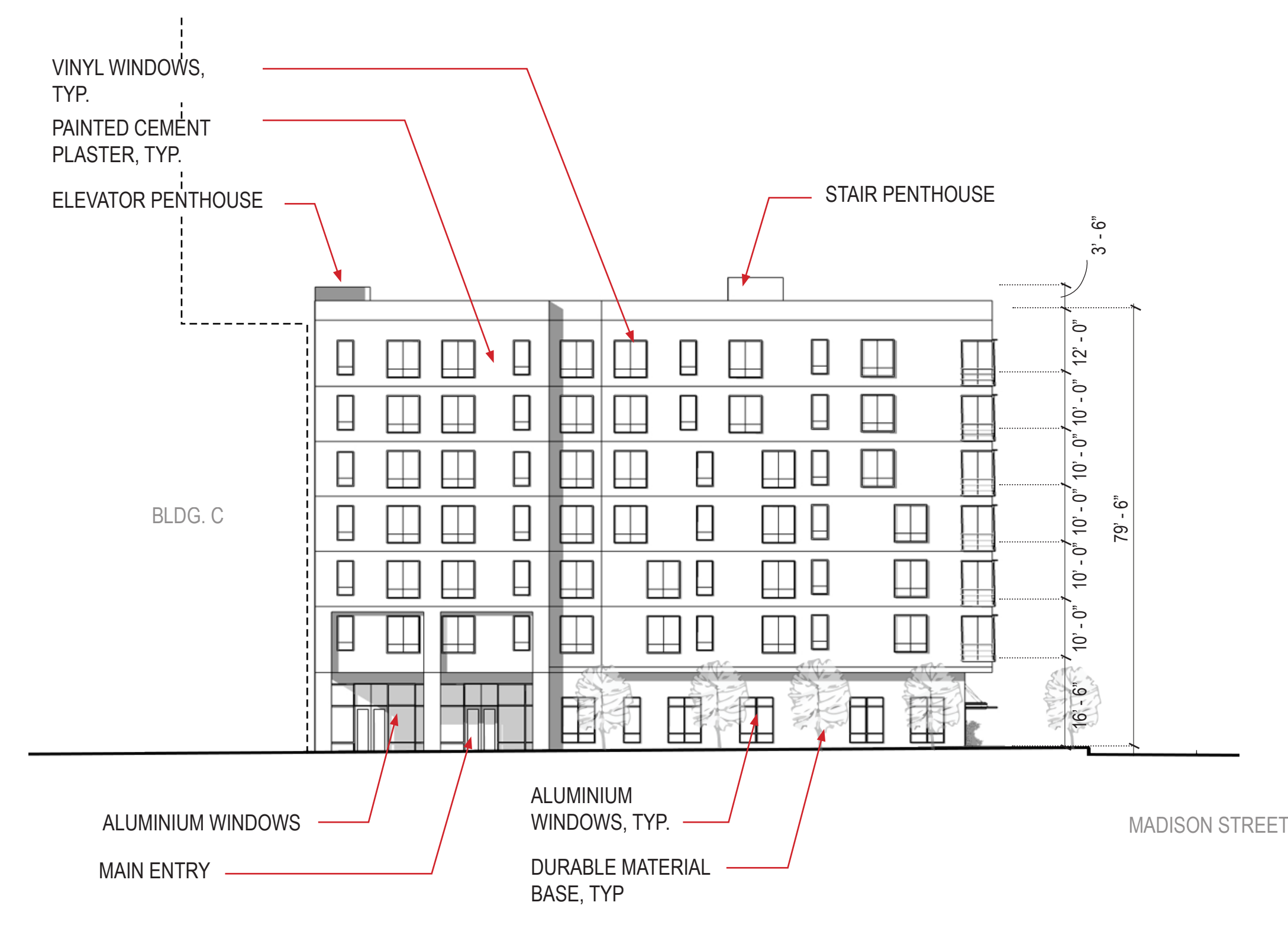
**NORTH-SOUTH SECTION**



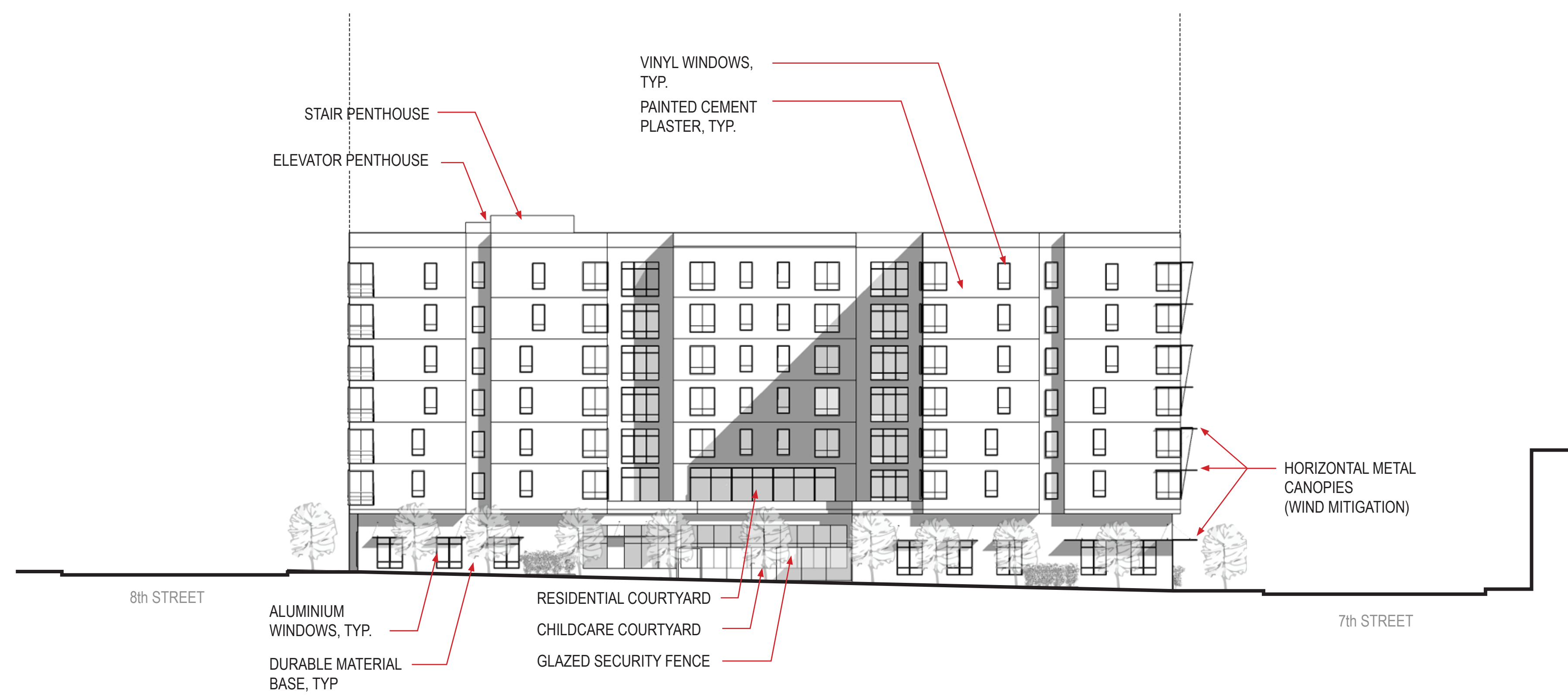
NOTE: COLORS AND MATERIALS TO BE FINALIZED AT FDP PHASE.



2. BUILDING D SOUTH ELEVATION



1. BUILDING D NORTH ELEVATION



3. BUILDING D WEST ELEVATION

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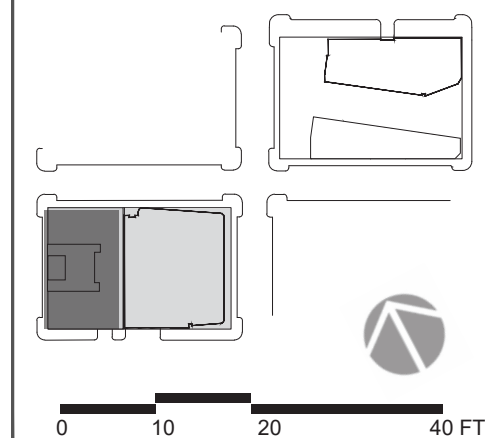
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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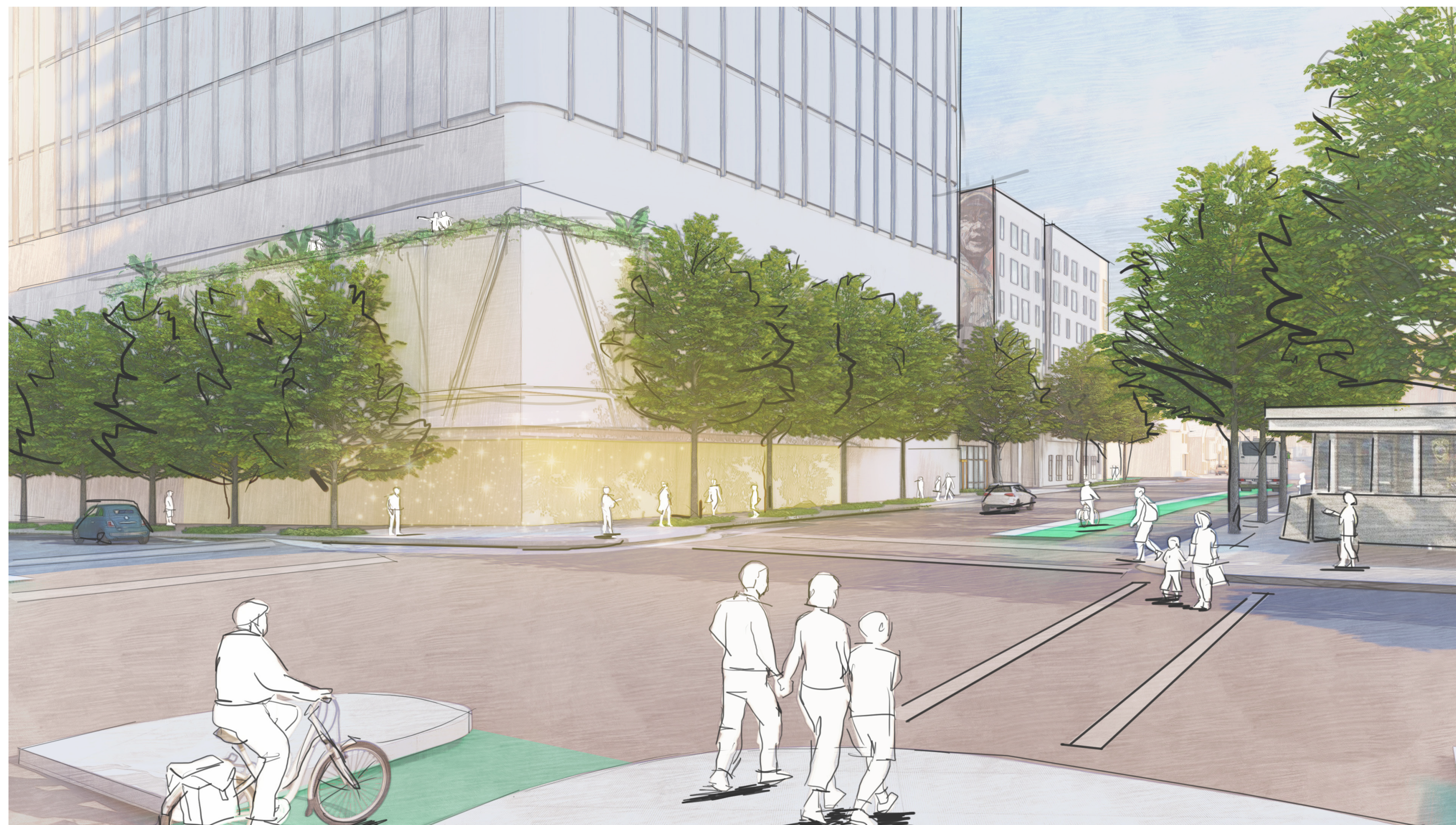


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG D ELEVATIONS**





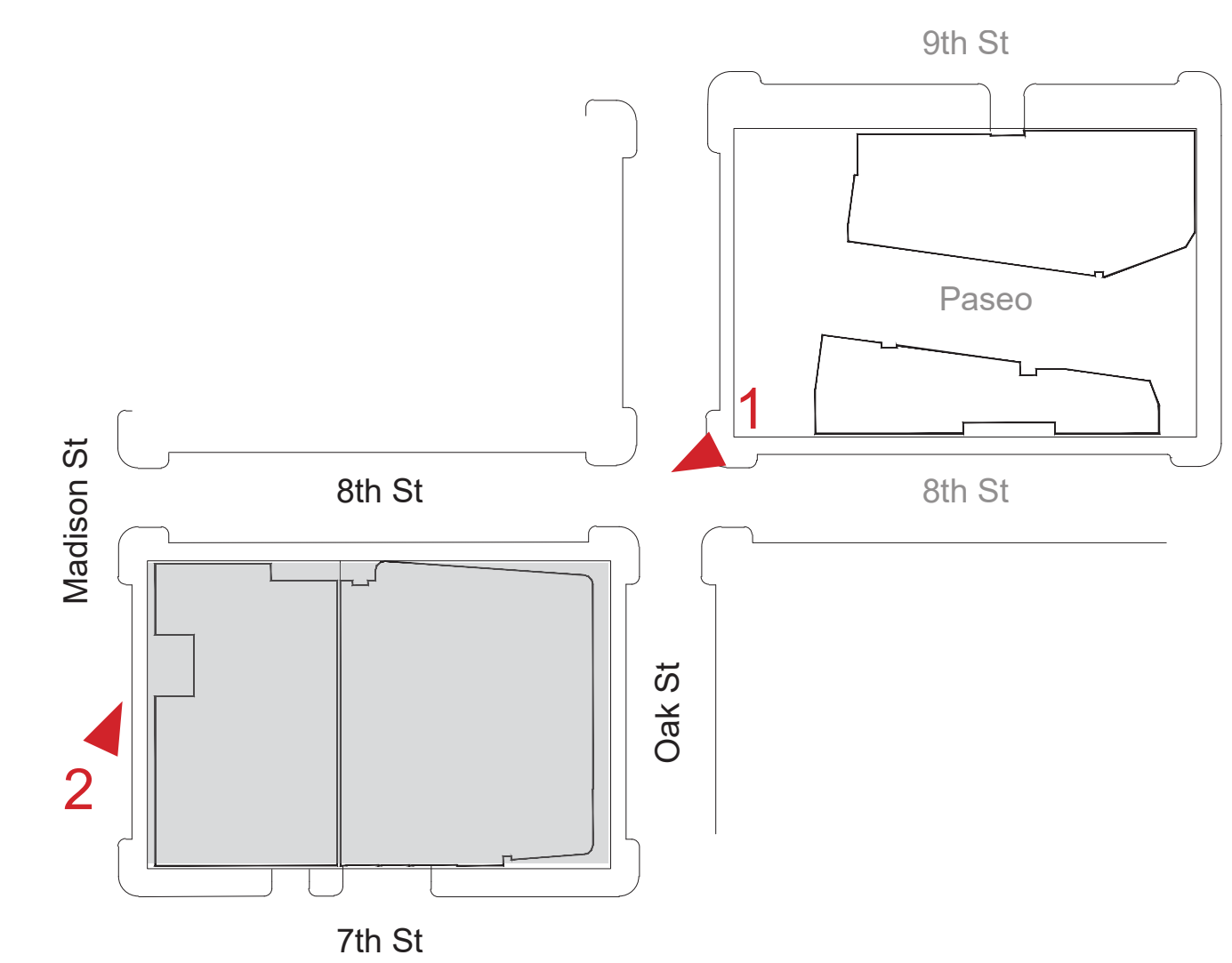




8TH & OAK STREET  
VIEW TOWARDS SOUTH WEST



MADISON STREET  
VIEW TOWARDS THE DAYCARE



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OAKLAND, CA 94612

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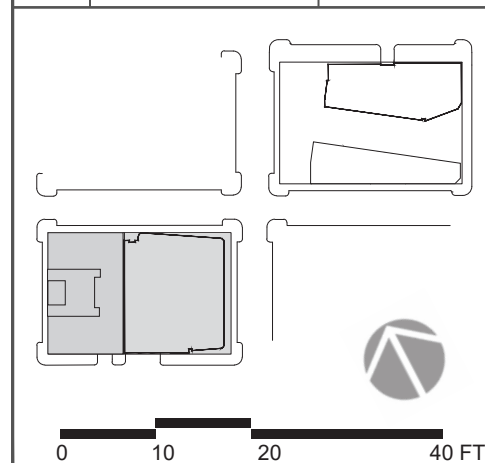
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Oakland, CA 94607

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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / CONCEPT 3D VIEWS**



**NOTE:**

- SEE C2.1, C2.2, C6.1, C6.2 FOR FIRE HYDRANT LOCATIONS

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

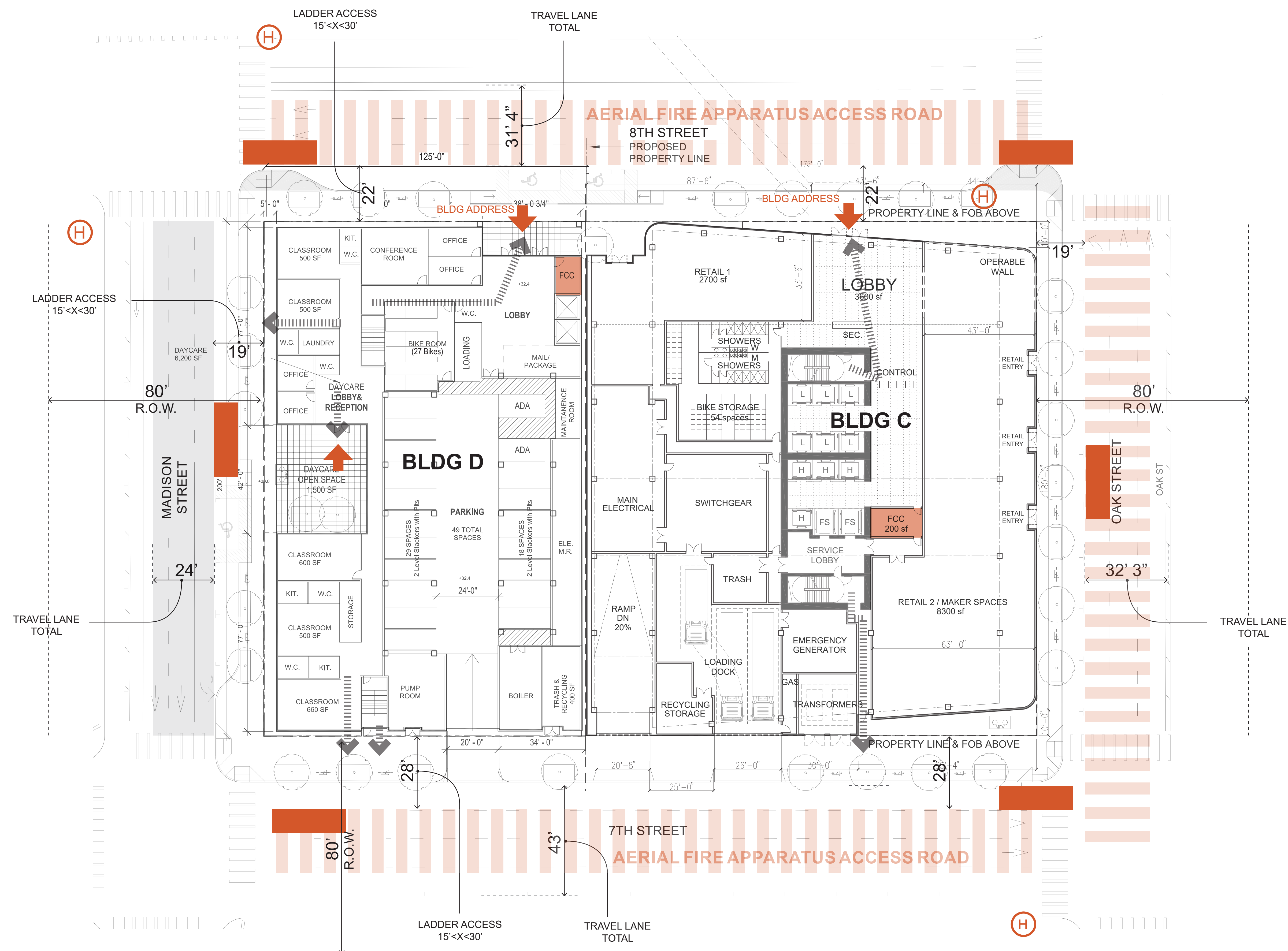
**STRADA**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com

SOLIMON CORDWELL BUENZ ARCHITECTS  
335 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
415/761-2600  
www.scb.com

INWILLERUEHL  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/851-1686  
www.inwilleruehl.com

**BKF100+**  
ENGINEERS, SURVEYORS, PLANNERS  
225 BUCKLEUP DR. SUITE 200  
REDDING, CA 96001  
(530) 463-4300  
www.bkf.com



**BUILDING C:**  
19 STORIES/ 275' HEIGHT  
OFFICE  
TYPE I  
FULLY SPRINKLERED

**BUILDING D:**  
7 STORIES/ 85' HEIGHT  
RESIDENTIAL/MIXED USE  
TYPE III OVER TYPE I  
FULLY SPRINKLERED

**KEY LEGEND**

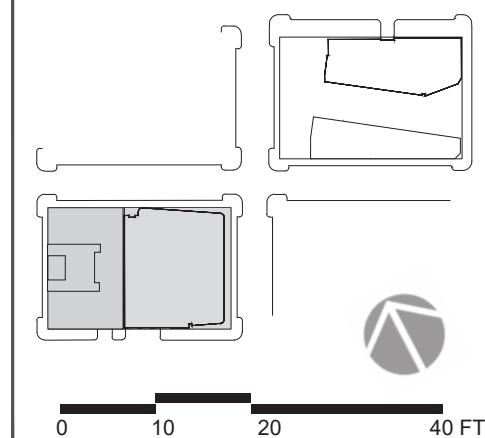
- (H) EXISTING HYDRANT LOCATION
- 150' HOSE DISTANCE
- ORANGE RECTANGLE FIRE TRUCK
- ORANGE STRIPES AERIAL FIRE APPARATUS ACCESS ROAD (26 FEET WIDE)
- GREY STRIPES NON-AERIAL FIRE APPARATUS ACCESS ROAD (24 FEET WIDE)
- ORANGE ARROW PRIMARY BLDG ENTRANCE
- HATCHED ARROW EXIT

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPK COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPK COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPK COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPK COMMENTS	03/19/2021



**BLOCK 2 FIRE ACCESS DIAGRAM**



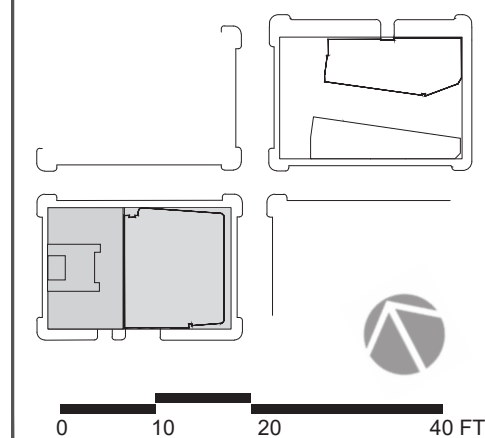
**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

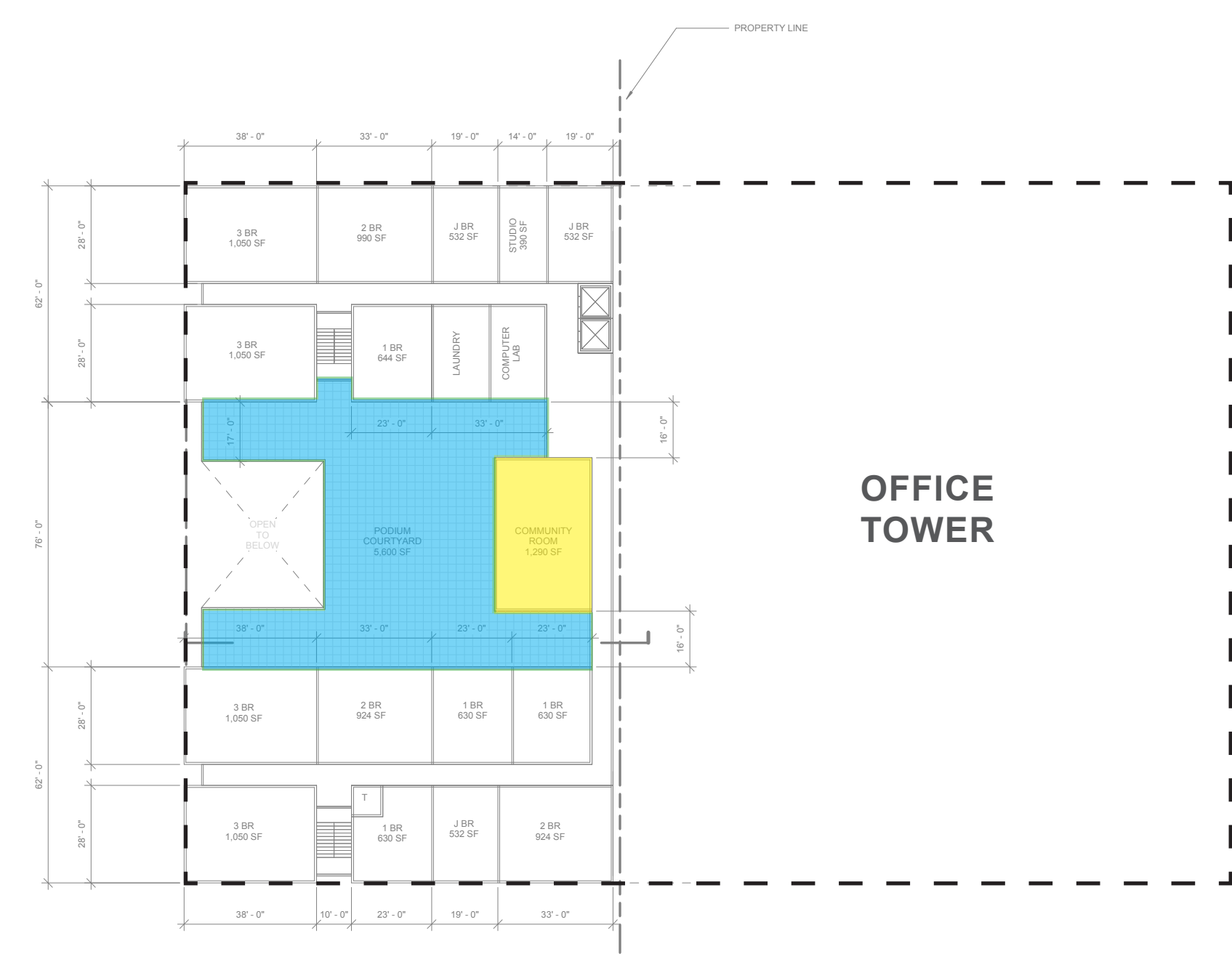
- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE

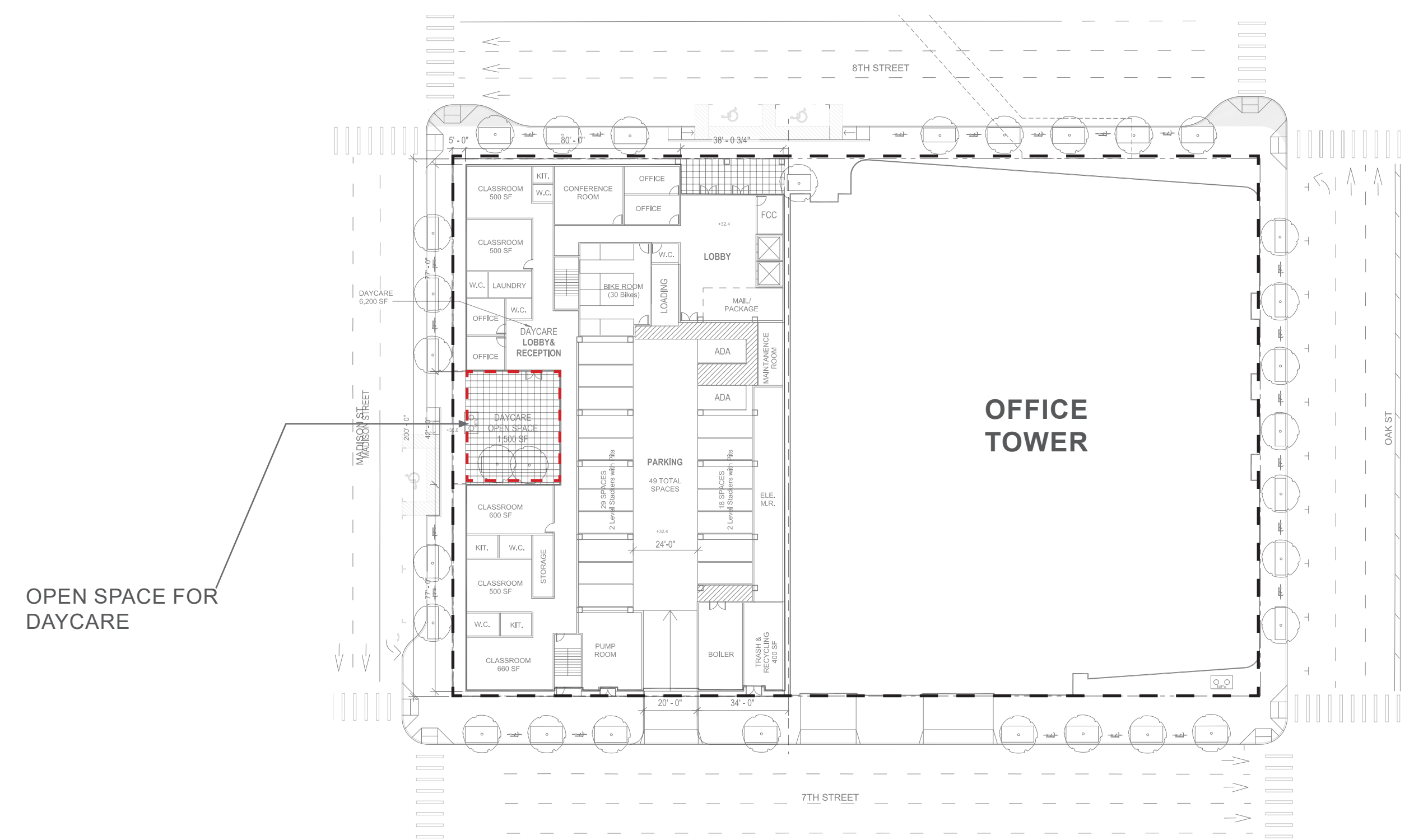
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP'S COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP'S COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP'S COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 OPEN SPACE EXHIBIT**



**BUILDING D (COURTYARD/PODIUM)**



**BLOCK 2 - GROUND LEVEL**

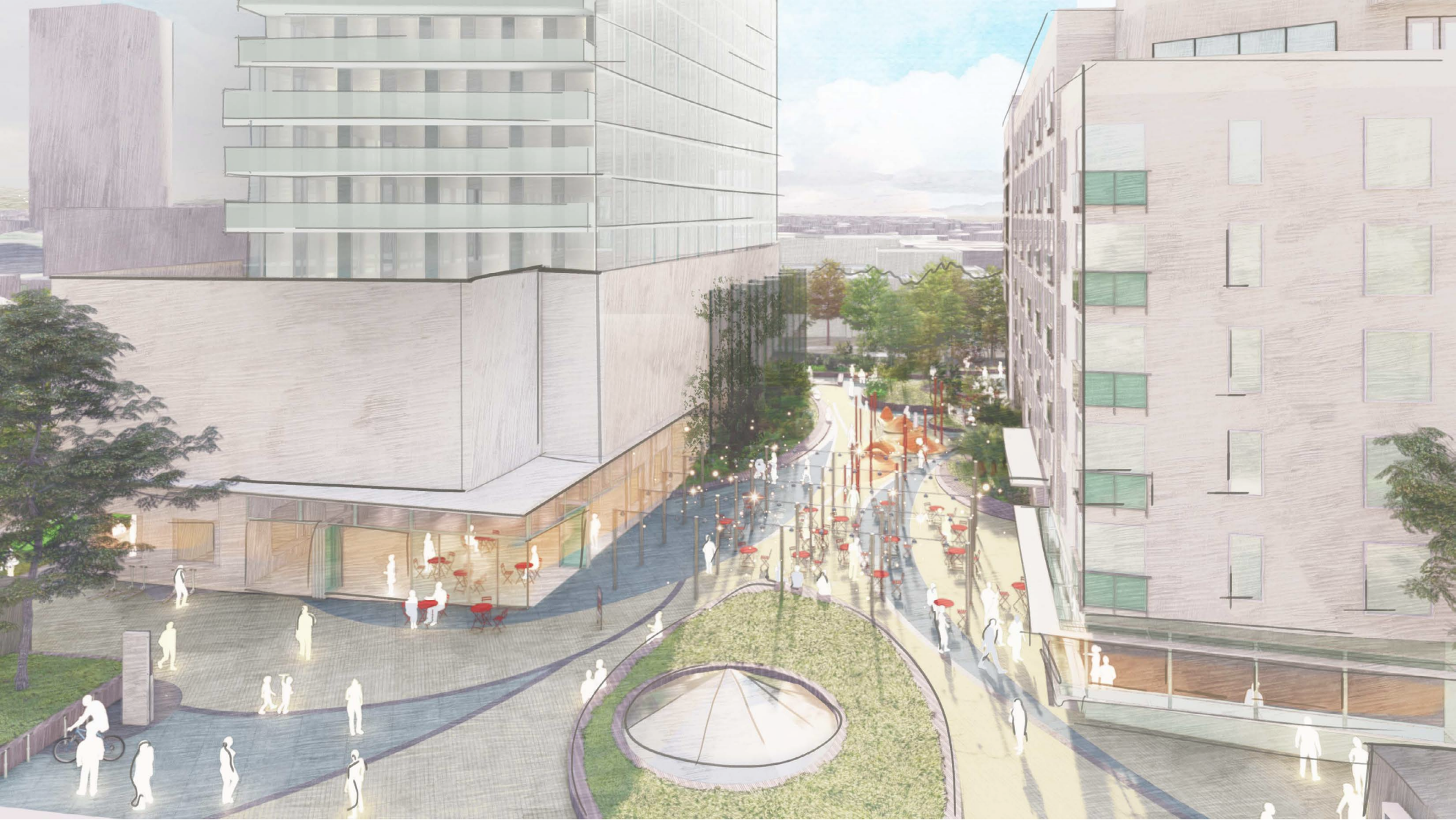
- PUBLICLY ACCESSIBLE OPEN SPACE
- GROUP USEABLE INDOOR OPEN SPACE (FOR BLDG OCCUPANTS)
- GROUP USEABLE OPEN SPACE (FOR BLDG OCCUPANTS)
- PRIVATE USEABLE OPEN SPACE (FOR UNIT OCCUPANTS)

	BUILDING D
PUBLICLY ACCESSIBLE OPEN SPACE	-
GROUP USEABLE INDOOR OPEN SPACE	1,200
GROUP USEABLE OPEN SPACE	5,600
PRIVATE USEABLE OPEN SPACE	-
<b>TOTAL</b>	<b>6,800</b>

**NOTE:**

- \* Covered entry plaza (setback at ground level) is not included in Publicly Accessible Open Space calculations
- \*\* open space is not required for Bldg C (office building)





# Design Guidelines for Lake Merritt BART

March 23, 2021







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# Design Guidelines for Lake Merritt BART

## APPLICABILITY

The Lake Merritt BART Design Guidelines (LMBDG), in conjunction with the development standards set forth in the D-LM-2 Lake Merritt Station Area Zoning District and the Lake Merritt Station Area Plan Design Guidelines (LMSAPDG), will serve as the basis for Design Review Findings for the Final Development Plans for the Lake Merritt BART Transit-Oriented Development (LMBTOD) Project. These guidelines build on the goals and intent set forth in The Lake Merritt Station Area Plan (LMSAP) and in LMSAPDG. The LMB Design Guidelines generally follow the format of the LMSAPDG, and provide more specific guidance for the design of this project site.

## RELATION TO PRELIMINARY DEVELOPMENT PLAN (PDP)

The Lake Merritt BART Design Guidelines are one component of the Preliminary Development Plan (PDP) for the Lake Merritt BART project. The graphic components of the PDP establish the general arrangement and scale of building elements and the preliminary design for streets and public spaces. The LMB Design Guidelines provide supplementary guidance for the development of the detailed design of buildings, streetscape and open space.

## 1. SITE DESCRIPTION

The Project spans two complete city blocks adjacent to the Lake Merritt BART Station. The Project's Block 1 is bounded by 9th Street, Fallon Street, 8th Street, and Oak Street. Currently this block serves the station as a surface parking lot owned and operated by BART, and the BART tunnel runs underneath the center of the site from Oak to Fallon. Additionally, this block has two station headhouse entrances on Oak, at the corners of 8th Street and 9th Street, which are not part of the LMBTOD project. The Project's Block 2 is bounded by 8th Street, Oak Street, 7th Street, and Madison Street. Block 2 is currently occupied by the existing office building and its private parking lot.

## 2. COMMUNITY VISION

The Lake Merritt BART Station Transit-Oriented-Development (the "Project") consists of high-rise and mid-rise buildings with a diverse mix of residential, retail, community space, and office uses, along with new publicly accessible open space. Each building brings a unique design while all working in concert to activate the ground-level experience and provide a common feeling of vibrancy, sense of arrival, and community. The Project is rooted in the design parameters and land use goals of the Lake Merritt Station Area Plan, and the City of Oakland's standards for quality, sustainable development. The Project aims to provide an equitable approach to Transit-Oriented Development, increasing transit ridership, delivering inclusive community benefits, and serving as a hub for the surrounding Chinatown, Jack London, Eastlake, and Lakeside neighborhoods.

The Bay Area Rapid Transit system (BART) moves hundreds of thousands of people across the Bay Area each day,

including over 14,000 people coming in and out of the Lake Merritt BART Station. Over the past century, the Oakland Chinatown neighborhood has been a residential, commercial, and cultural home to a community, a place of local and international exchange.

The redevelopment of the Lake Merritt BART Station seeks to create a sense of arrival in this historic context, and to better connect the wider Bay Area region to Oakland Chinatown and the many other nearby neighborhood assets including Laney College, the Oakland Museum of California, Jack London Square, and Lake Merritt. Our development concept will enable the BART Station to become a dynamic TOD destination that complements, rather than competes with the commercial core of Oakland Chinatown, and will help it more equitably share in and contribute to the prosperity of Oakland and the entire Bay Area. The design of the buildings will be modern and angular. The design of the public spaces will feature smooth lines that flow more organically, to encourage movement and remind visitors of the natural landscapes that are just a few blocks away.

The Project will include two phases and four buildings that feature a high-density mix of market-rate, moderate-income, and affordable apartments; Class A office and community space; active ground-floor retail and restaurant-ready space; and a reinvigorated and attractive public realm. The Project design also accommodates BART's existing infrastructure, including headhouse entrances to the station, as well as the underground tunnel and surrounding ventilation structures. The design must account for load restrictions, access needs, and other technical requirements to ensure safe operations for BART.



The first phase of development will occur on Block 1, the current BART parking lot site. Block 1 includes:

- Market-rate and moderate-income rental housing in a high-rise building.
- Affordable rental housing in a mid-rise building.
- A generous mid-block pedestrian thoroughfare is anchored by transit uses, neighborhood-scale retail designed for food service, interactive art or play, and a garden setting. Taken as a whole the publicly accessible space creates a layered identity for the site that connects to the surrounding neighborhoods. This publicly accessible space also presents an opportunity for public art that will contribute to the identity of the Project.

The second phase of development will occur on Block 2, after the existing office building is demolished. Block 2 will include:

- An iconic, high-rise, office building, including Class A market-rate space targeted to serve community organizations, nonprofits, and small business tenants that provide important services to the area’s historically immigrant, working class, and low-income residents. The building also includes active ground-floor retail and community-facing spaces.
- Affordable senior rental housing in a mid-rise building. This building will also have a childcare center located at the corner of 8th and Madison Street, facing Madison Square Park.

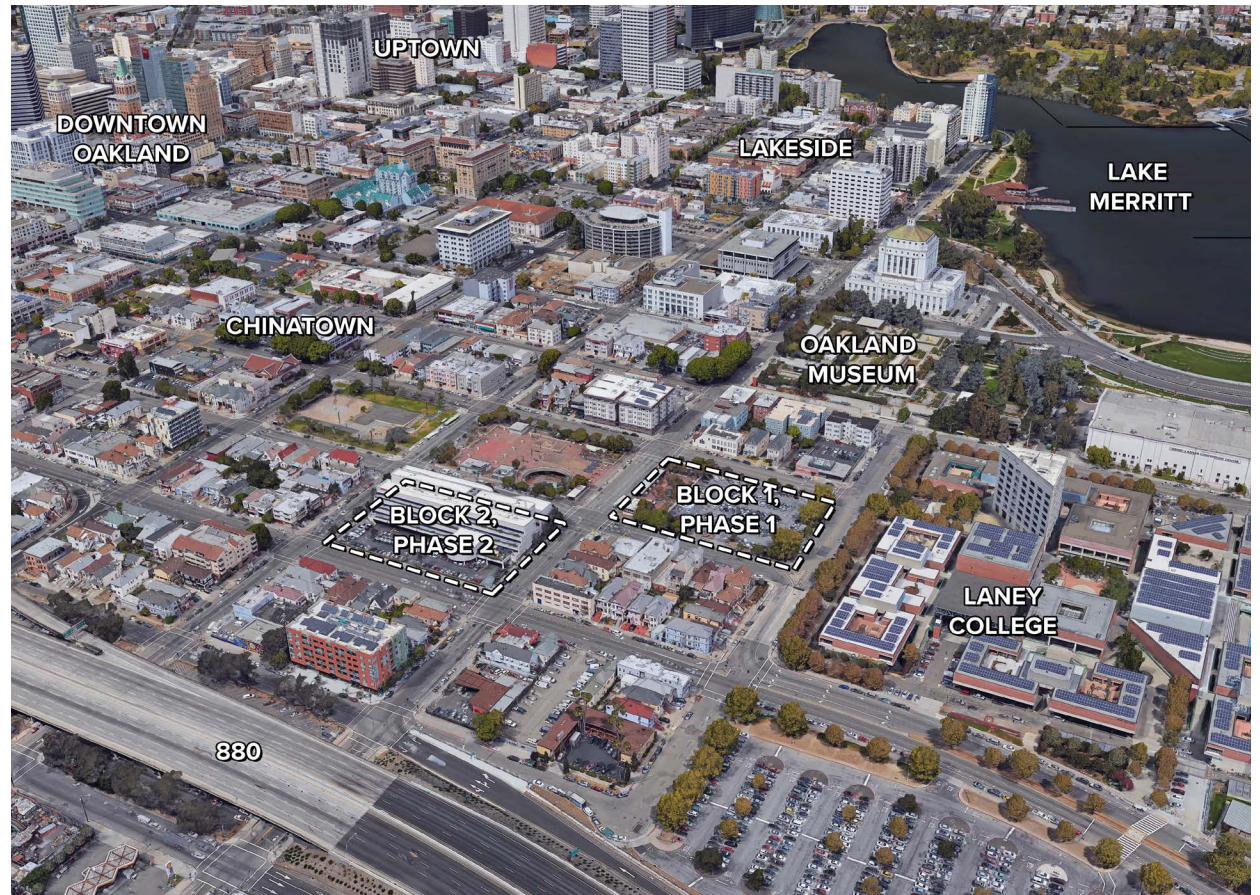


Figure 1: 3D Context View



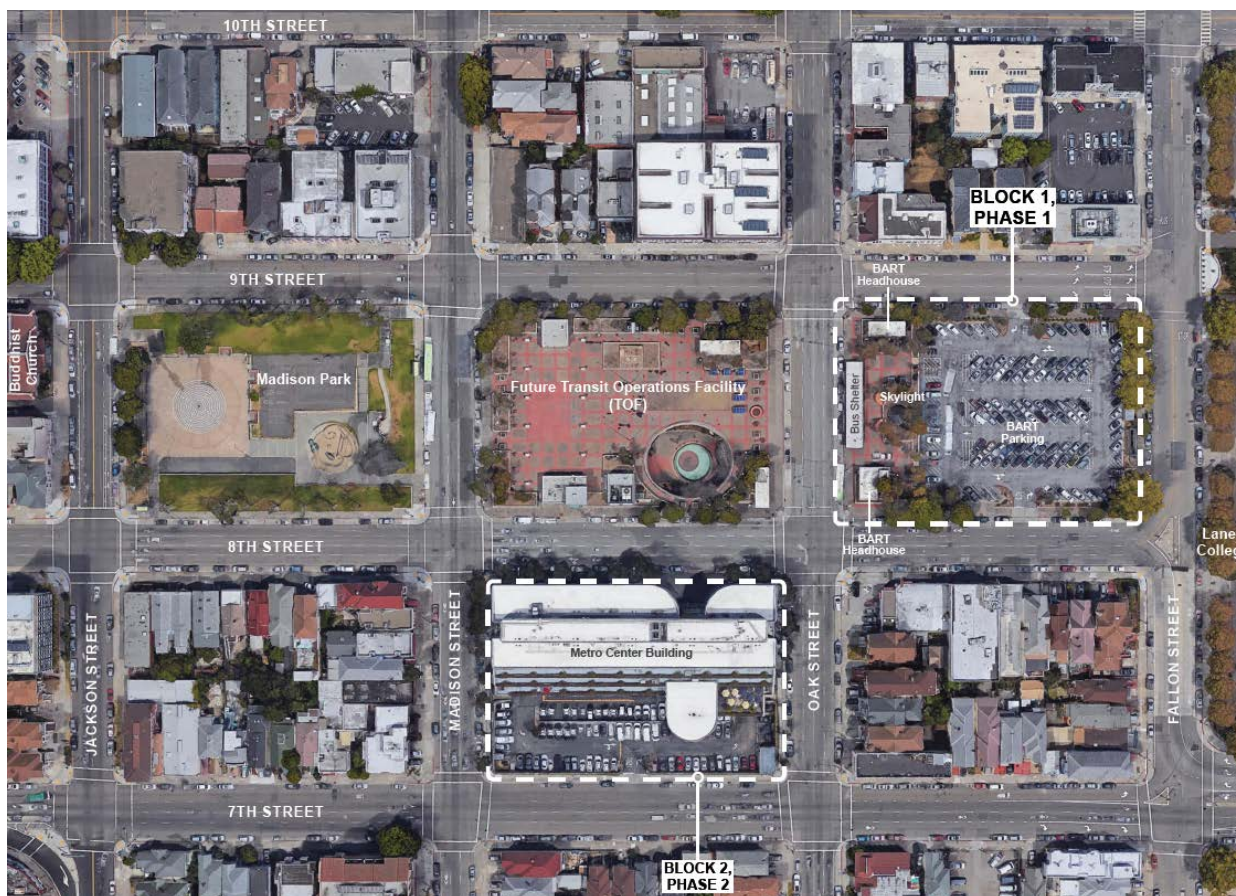


Figure 2: Context Map

### 3. GUIDING PRINCIPLES

The Project design will be guided by principles that address the unique opportunities presented by the redevelopment of the Lake Merritt BART Station. These principles build on the underlying goals set forth in the Lake Merritt Station Area Plan (LMSAP).

#### A. Cultural Identity and Sense of Arrival

The Project will create a regional destination that announces an arrival at the BART Station located in Oakland Chinatown, while also featuring prominent way-finding measures that highlight the surrounding civic and cultural assets. The Project seeks to celebrate Chinatown as a regional community destination and distinctive place, while also serving as a launching point to connect to the Oakland Museum of Art, Laney College, the Jack London Square Waterfront, Lake Merritt and the other nearby neighborhoods. A dynamic mix of high-rise and mid-rise improvements will create a new and identifiable mark on the skyline while simultaneously addressing the scale of the surrounding neighborhood context. Thoughtfully interspersed open space and an active pedestrian environment will promote a continuous flow between the Station and the surrounding neighborhood. The inflow and outflow at this core regional hub is a primary guiding principle of site programming.

#### B. Transit-Oriented Development

Transit-oriented development can enhance the arrival experience of riders and be a tool to connect people to places, cultures, and ecology. It also enhances sustainability by encouraging multimodal transportation and less reliance on automobile use. The Project's proposed design has been coordinated with a BART sponsored Access Study, which focused on better non-



automobile connections to this important transit node from the surrounding neighborhoods and institutions. The site is a 10-minute walk to the Amtrak Jack London Square Station, is surrounded by various AC Transit bus lines and bike lanes, and the Lake Merritt BART Station is at the center of the Project. The proposed Project program will deliver improvements that increase transit usage, improve pedestrian and bicycle safety, and provide a more sustainable transit environment for the community.

### C. Publicly Accessible Gathering Space

The BART Station is already a hub of activity in Oakland, surrounded by formal and informal gathering spaces. A key goal for the Project is to enhance the attractiveness of existing spaces and add new opportunities for recreation, outdoor eating, contemplative space, public events, public art, wayfinding, and other communal activities. On Block 1, the engineering constraints and load limitations above BART's existing tunnel allow the Project to create an internal open space that connects to the BART Station entrances and will be a welcoming passageway for public access. The Project design focuses on making this public space a community destination, with landscaping, art, and neighborhood-scale retail. The Project will also include improvements to the public realm around the BART Station and on the surrounding streets and sidewalks to enhance the street-level experience for pedestrians and cyclists.

### D. Community of Opportunity

The Project provides a mixture of market-rate and affordable housing units for a diverse mix of residents; brings a diverse job center to the neighborhood with office space targeted to community and non-profit

organizations; and creates new opportunities for community-focused retail and gathering spaces. Food retail has helped to spark a commercial renaissance in many parts of Oakland, and food is a historically important part of the identity of Chinatown. The project's retail scale, design, and orientation will be particularly well suited to local and "pop-up" restaurants, including new entrepreneurs who may have less access to startup capital.

## 4. DESIGN CONSIDERATIONS

### Block 1 Design Considerations

The BART tunnel structural requirements inform much of the site plan concept on Block 1. No primary building structures can be feasibly located over the BART tunnel and platform infrastructure because of load and access restrictions. However, this challenge also creates an opportunity to divide Block 1 into two separate buildings on opposing sides of the BART tunnel, allowing for variation in architecture and massing on the site. The north side of Block 1 adjacent to 9th Street is envisioned as a high-rise residential building with ground-floor commercial uses. The south side adjacent to 8th Street is envisioned as a mid-rise affordable housing building. This arrangement of building elements maximizes solar access at the public open spaces and provides appropriate spacing between the high-rise elements on Block 1 and Block 2. As a result of the mid-block passageway, both buildings on Block 1 have highly visible public frontages on all sides. In this circumstance special attention must be paid to creating an attractive ground level experience while accommodating normal building service functions. Residential lobbies are located further away from the BART headhouses to support

resident privacy and security, while commercial lobbies and retail spaces are oriented towards the BART headhouses.

The pedestrian walkway between the two buildings on Block 1 creates a new pedestrian space above the tunnel as a defining feature of the Project and key gathering space for the community. The Project connects pedestrian energy and activity to the center of the block, while also including active uses on key corners on Oak Street and Fallon Street. This orientation allows for a single sense of place with critical mass, as Buildings A and B can support each other with a cohesive retail program. It also allows for a more comfortable "outdoor room" for people to gather, with weather protection and active building frontages on both sides. The design draws on successful precedents throughout the world for two-sided retail or "night market" experiences with a series of small booths or stalls. The scale and diversity of uses – especially well-suited for food-related uses – encourages people to walk through, browse, and interact.

The publicly accessible open space will include movable outdoor seating for the retail at the west end and transition to interactive sculpture or play and quieter garden rooms at the east end towards Fallon Street. Furnishing and landscaping will redirect pedestrian flow and break the wide space into smaller "rooms", with active building uses at both the Oak Street and Fallon Street ends of the block. The open space will also include pedestrian-scaled lighting that may offer opportunities for hanging art or lanterns (similar to the cables that are used in Old Oakland between Broadway and Washington Streets). This will help to bring down the scale and create a more exciting visual and pedestrian-oriented environment as well as potentially providing a way to connect to art and culture, particular during festivals and events.



## Block 2 Design Considerations

The Block 2 plan will create a complementary mix of high-rise and mid-rise buildings, while adding a wider variety of affordable housing options focused on families, along with new high-rise office space. The office building will be located directly across from the BART headhouses on the eastern side of the block to support reverse-commute ridership for office tenants that will include community and non-profit organizations. The office building will also feature an active lobby and ground-floor retail uses to help activate the important corner at 8th and Oak Street. The mid-rise family affordable building will be located kitty corner from Madison Square Park, an amenity for families in the new building. Ground floor community-facing spaces (including potentially a daycare) will have direct visibility and access to the Park. Active and transparent frontages on 8th Street and Madison Street will support the larger area public realm improvement plan and build stronger connections to the Park.

## Project Wide Design Considerations

### ● Landscape Design

Landscape design will enhance the pedestrian experience, and will also help to soften the hardness of the urban environment. Greening is an important part of improving the walking experience and some species provide practical benefits by removing particulate matter from the air (which the Chinatown Coalition has demonstrated using US EPA studies as a major health risk in the neighborhood), and reducing urban heat islands that will likely grow in severity over the coming decades. The Project design proposes a plant palette that evokes seasonal change and cultures.

### ● BART Station and BART Access

The Project aims to maintain strong visual connection and physical access to the BART Station, supporting an improved and safer transit rider experience. This includes improvements to the pedestrian experience and safety of the pedestrian walk-ways adjacent to the BART headhouses and Plaza, and incorporating these areas into active, safe and delightful urban places.

To accommodate the multitude of users of the Project (BART riders, residents, workers, visitors, etc.), careful planning for access to the site through various modes has been coordinated between BART, the City, AC Transit, and the Alameda County Transportation Commission (ACTC).

### ● Enhanced Pedestrian and Bicycle Access

The Project will provide improved pedestrian access to the BART Station Plaza and the development blocks from all directions, with activation on all sides. To accommodate the variety of one-way and two-way streets around our site, the Project will be designed to accommodate bike lanes, protected bike lanes, and two-way cycle tracks that will offer added safety for cyclist commuters. Secure bike parking will also be provided.

Key pedestrian improvements will include sidewalk upgrades such as repaving/regrading throughout the sites, planting of street trees, addition of corner bulb-outs, and sidewalk widening in some areas. Bicycle improvements include the incorporation of raised two-way protected bicycle lane on the south side of 9th Street at Block 1. This will serve as the major bicycle facility for

accessing the station area, and serve as a connection for neighborhoods to the east of the Development to the station area, Chinatown, and Downtown. The Project would also include a one-way protected bicycle lane on the west side of Fallon Street at Block 1.

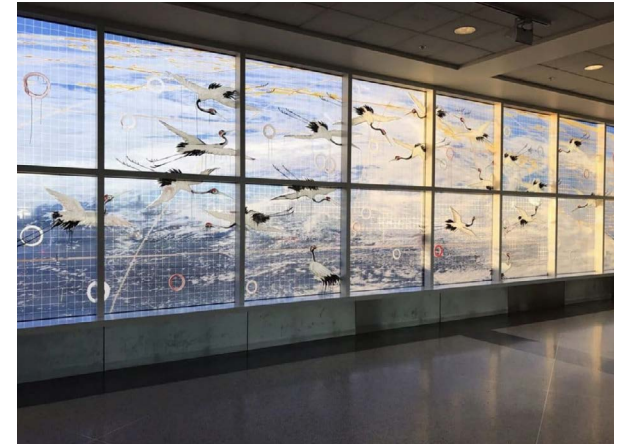


● **Public Art/Programming**

Public art, particularly located in the mid-block pedestrian walkway/public realm, will help create a sense of place and potentially serve as a landmark and wayfinding tool for the TOD Project and potentially the BART Station. Specific locations and concepts will be developed in consultation with local artists during the Final Development Phase of the Project.



**Figure 3:** Tommy Wong + Civic Design Center: Chinatown Banner, Oakland



**Figure 5:** Hung Liu: "Take-off" SFO



**Figure 4:** Posts as Play Space Light Sculptural



**Figure 6:** Playful Furnishing



## 5. BUILDING DESIGN GUIDELINES

The Design Guidelines set forth in the following sections are based on the Guiding Principles and shall be applied and interpreted in the context of those Principles as Project Specific Guidelines for the Lake Merritt BART development. Where guidelines are similar in content to guidelines in the Design Guidelines for the Lake Merritt Station Area these guidelines are cross-referenced with the notation of (LMSAP/DG-#).

### A. Building Design – General Guidelines for Blocks 1 and 2

- 1. Coordinated Design.** The design of the high-rise Buildings (A and C) and the mid-rise Buildings (B and D) should be coordinated to create a cohesive frontage and to reinforce the overall sense of identity for each Block. This coordinated design may take the form of a similar vocabulary of forms, openings, materials and colors. This should include consideration of how buildings work together to achieve the Design Principles for Lake Merritt BART, contribute to the public realm and the overall quality of life. (LMSAP/DG-1: Public Perception)
- 2. Transition-in-Scale.** Mid-rise Buildings (B and D) on each block should be designed to provide a visual transition between the scale and rhythm of the neighboring buildings and high-rise Buildings (A and C). (LMSAP/DG-17: Reinforce the Existing Rhythm)
- 3. Service Areas at Ground Level.** Service areas at the first level should be articulated with pedestrian-scaled facade articulation such as panels, contrasting textures, high-quality and interesting building materials, blind windows, doors, planting treatments, murals or other public art, and/or exterior detailing are recommended in order to create visual interest and diversity and to reinforce the pedestrian scale.



Figure 7: Artwork at Blank Walls



Figure 8: Contrasting Textures and Interesting Materials at Blank Walls



**4. Family Friendly Housing.** At residential units intended to accommodate families, especially in Building D as an affordable family building, the building design should include a variety of unit sizes to accommodate households of various sizes. Building design on both Block 1 and Block 2 should also incorporate other family friendly elements including outdoor play space designed to allow supervision and easy access. (LMSAP/DG-52: Family Friendly Housing)

**5. Shared Outdoor Spaces.** Shared outdoor spaces should include plantings, benches, lighting and other appropriate elements to create an inviting and useful space for the residents. Shared outdoor spaces should be designed to accommodate children where feasible by incorporating play structures and play areas. Shared outdoor spaces such as the publicly accessible open space Paseo, the courtyard, the open space at the

upper floor step backs and/or the rooftops, should be designed to have ample daylight and to be sheltered from the wind. (LMSAP/DG-57: Shared Spaces)

**6. Residential Community Rooms.** Where community rooms are provided they should be located to promote active use by residents. Community rooms are encouraged to be located adjacent to shared outdoor spaces or public open areas to create strong connections between indoor and outdoor activities. (LMSAP/DG-57: Shared Spaces)

**7. Exterior Color.** The exterior color palette should be coordinated to provide a cohesive overall appearance and to reinforce the design intent. The color palette at adjacent buildings should be coordinated to achieve the desired balance of cohesion and variety. (LMSAP/DG-73: Color)



Figure 9: Family Friendly Environment

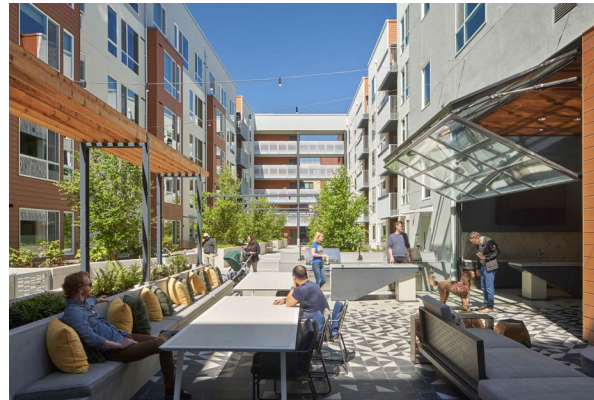


Figure 10: Shared Outdoor Space



Figure 11: Residential Community Room



## B. Building Design – Street Frontage Guidelines for Block 1

The guidelines below apply to portions of buildings fronting on the referenced street or public open space. These frontage guidelines are intended to supplement the Building Design General Guidelines. The character and design intent for each frontage is outlined at the beginning of each section.

### B.1. Oak Street Frontage (Buildings A and B)

Oak Street is the “front door” to Block 1 and also provides the main access to Lake Merritt BART Station with BART entries located at the 8th Street and 9th Street corners. The east-west, mid-block Paseo above the BART tunnel creates an 80 foot-wide visual break between Buildings A and B. The wide BART Plaza at Oak Street accommodates the BART entries (headhouses) and creates a welcoming entry to the Paseo. The high-rise Building A and mid-rise Building B shall work together to create a cohesive sense of place and an active public realm at the BART Plaza and the Paseo. Oak Street is a highly visible frontage and its orientation toward Downtown Oakland provides an opportunity for active upper-stories with roof deck/gardens. Due to the adjacent BART Plaza, no service areas or parking access points are located on the Oak Street frontage.

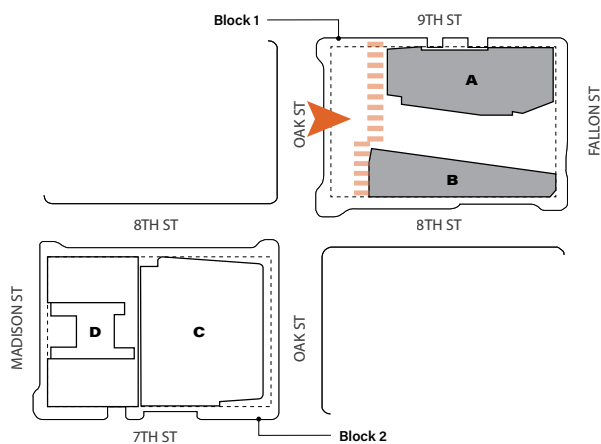


Figure 12: Oak Street Frontage



### Oak Street Frontage: Design Standards

- The Building A tower shall be setback from the base building a minimum of 20 feet.
- Parking uses located in the podium at Building A shall include 25% openings for natural ventilation.
- The corner of the podium at the Paseo and Oak Street shall be notched a minimum of 10 feet by 10 feet.
- A canopy shall be provided over entrances to the ground-floor commercial spaces. See Figure 13.

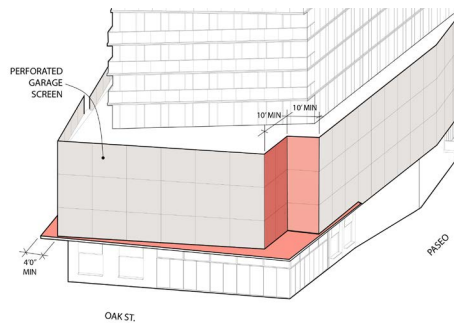


Figure 13: Building A Corner at Oak Street and the Paseo



Figure 14: Building Corners and Defined Open Space

### Oak Street Frontage: Design Guidelines

- Architectural articulations and landmark features may be used to highlight and emphasize the building corners facing Oak Street, to visually define and animate the mid-block pedestrian thoroughfare entry and to facilitate pedestrian flow.
- Active commercial uses may be provided at the ground floor of all four building corners along Oak Street with visual transparency to create an active and inviting public realm and to connect the street life to the Paseo. (LMSAP/DG-39: Storefronts, DG-32: Views of Indoor Spaces)
- Open spaces and/or articulated roof shapes may be provided at step backs and the rooftop facing Oak Street to emphasize the gateway elements and to provide shared outdoor spaces overlooking the BART Plaza and Downtown.
- The design of the high-rise Building A and the mid-rise Building B may be coordinated to create a cohesive frontage and to reinforce the overall



Figure 15: Active Roof at Step Backs

identity for the block. This coordinated design may take the form of a similar vocabulary of forms, openings and materials.

- Use of canopies, awnings or other projecting elements are recommended to provide shelter and shade at west facing active uses. At Building B, horizontal awnings above the ground floor may wrap around both corners of Oak Street and the Paseo and at Oak and 8th Street. (LMSAP/DG-35: Awnings)
- The visual prominence of the west facade of the Building A tower suggests incorporating three-dimensional elements such as balconies, recesses and similar architectural features that create deep shadows and provide shading for interior uses. (LMSAP/DG-24: Distinguished Tower Design)
- Varying materials, texture and scale may be used to differentiate the building base from the tower above. (LMSAP/DG-18: Transitions in Building Height)



Figure 16: Active Commercial Uses





Figure 17: Shaping West Tower Facade

### Oak Street Block 1: Streetscape

This block of Oak is a dynamic area for multi-modal transfers by passengers and should function for efficiency and safety.

#### Business Frontage Zone

- Located in part above the BART tunnel, the design in this area must comply with the BART Facilities Standards.
- Furniture should include benches which orient in multiple directions and provide a variety of ways to sit including tiered, backed, non-backed, communal, etc.
- Furniture should also accommodate all needs of transit users and include bike lockers, bike racks, lighting, waste stations, and information kiosks/maps.
- The Paseo pavements shall extend to surround the skylight.
- The skylight should be visible and iconic and at night be lit for interest.

#### Pedestrian Pathway Zone

- Sidewalk concrete should be colored with the maximum amount of lamp black allowable by the

City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.

- Corners of sidewalks should be stamped with letters to show name of street.
- Located in part above the BART tunnel, the furnishings in this area must comply with BART Facility Standards and should not block sightlines into the Paseo.
- Furniture should include benches which orient towards both the Paseo and Oak Street.
- The expression of Oak Street as a “green street” encourages emphasizing public transit and where possible planting that does not conflict with the transit program. (LMSAP/DG-128: Streets with Special Functions)
- Street furnishings may be provided in transit waiting areas and as needed to facilitate multi-modal transfers from bike or scooter or bus to BART. (LMSAP/DG-131: Furnishings)

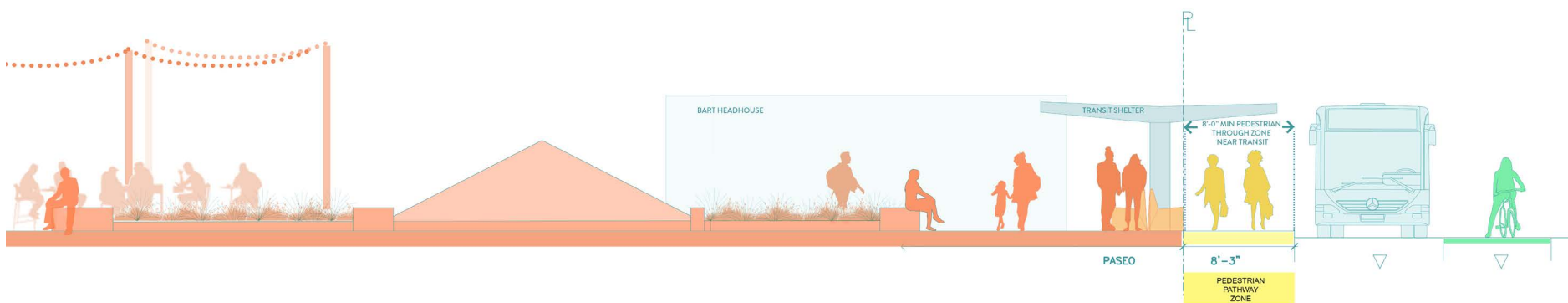


Figure 18: Oak Street Block 1



## B.2. Fallon Street Frontage (Buildings A and B)

Fallon Street borders the Laney College campus on its east side and connects directly to the Oakland Museum of California (OMCA) on the north end. The mid-block Paseo creates a visual break through the site while providing a physical connection between Laney College and OMCA on the east and BART Plaza and Madison Square Park on the west. The primary residential entries and associated amenity space for Buildings A and B are located on Fallon Street to create an active frontage and a vibrant entry to the Paseo. Similar to the Oak Street frontage, the high-rise Building A and mid-rise Building B shall work together to create a cohesive sense of place and an active public realm on Fallon and at the Paseo. Due to the limited street frontage, no service areas or parking access points shall be located on the Fallon Street frontage.

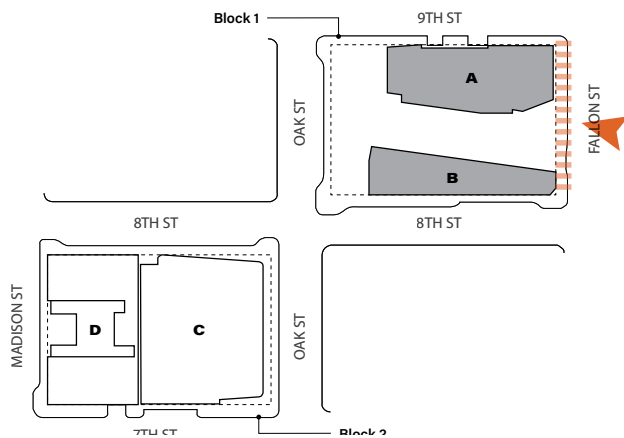


Figure 20: Fallon Street Frontage and the Paseo Entry



### Fallon Street Frontage: Design Standards

- Parking uses shall include a minimum of 25% openings for natural ventilation.
- A minimum six-foot deep canopy shall be provided on Building A at the corner of Fallon and the Paseo above the pedestrian level.
- Horizontal fins with a minimum depth of one foot shall be provided on alternating upper floors. See Figure 21.

### Fallon Street Frontage: Design Guidelines

- Emphasizing the corner of 9th and Fallon Street as a prominent corner with architectural features and main residential entry is recommended. (LMSAP/DG-7: Corner Building Design, LMSAP/DG-30: Ground Floor Entries)
- Emphasizing the corners of Fallon Street and 8th and 9th Streets as gateways to the Paseo and main residential entrances is recommended as well as providing architectural details to define the corner and a vibrant pedestrian entry. (LMSAP/DG-7: Corner Building Design, DG-30: Ground Floor Entries)
- Visual transparency may be used to create an active and inviting public realm and to connect the street life to the Paseo. (LMSAP/DG-32: Views of Indoor Spaces)
- Residential entries may be designed as prominent elements that reinforce and activate street frontage. (LMSAP/DG-30: Ground Floor Entries)
- The narrow corner at Building B creates a dramatic frontage that may be enhanced with a recessed entry or other significant and welcoming element that provides a visual landmark for Block 1 and relates to the scale of the Laney Campus.

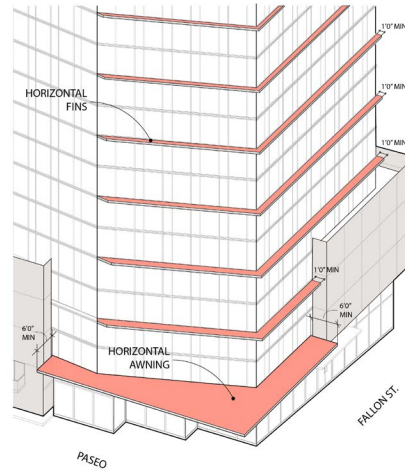


Figure 21: Building A Corner at Fallon Street and the Paseo

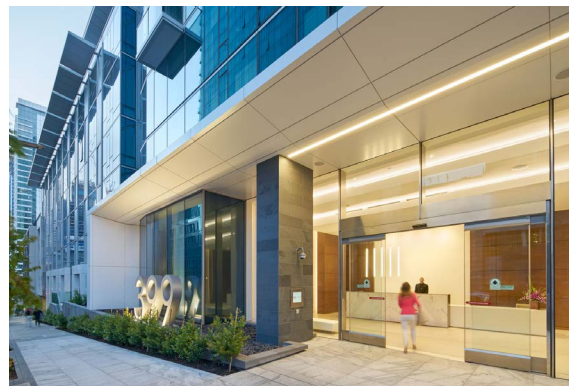


Figure 22: Residential Tower Entry

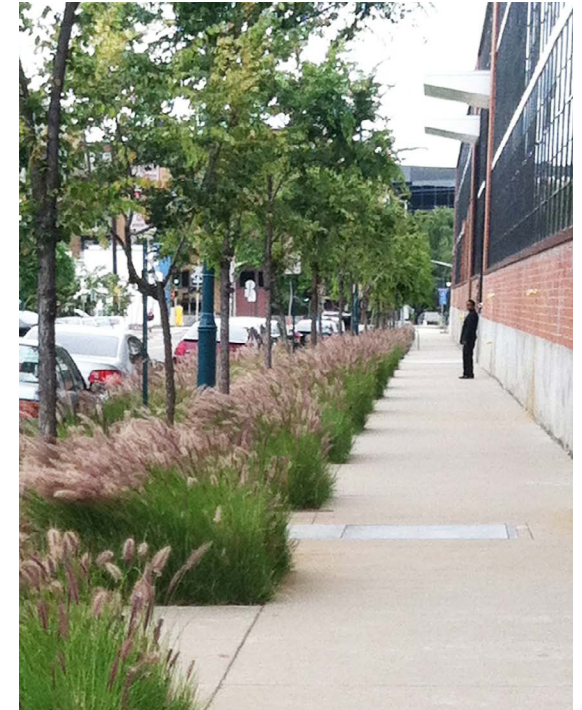


Figure 23: Robust Under Planting and Cohesive Street Tree Character



Figure 24: Building Corner at 8th and Fallon Streets



## Fallon Street: Streetscape

Anchoring the Paseo's east end, this block connects to Laney and the Oakland Museum and embraces a campus character.

### Business Frontage Zone

- a. Small urban furniture elements including benches, container plants, and extension of the lobby to the outside should be encouraged.

- b. Elegant vertical circulation with clear sitelines to the Paseo should be maintained at entry points mid-block in the business frontage zone.
- c. Plantings that screen BART structures and utilities are desirable within the constraints of the BART Facilities Standards.

### Pedestrian Pathway Zone

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland

standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.

- b. Corners of sidewalks should be stamped with letters to show name of street or text connecting to cultural identity of site such as poetry or history.
- c. Public seating that can be located at back of sidewalk or near lobbies is encouraged.
- d. New street trees and lush plantings should be provided along Fallon to connect the Paseo with the campuses of Laney College and the OMCA. (LMSAP/DG-110: Tree Planting and Preservation)

### Landscape Furniture Zone

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- b. Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, and concrete.

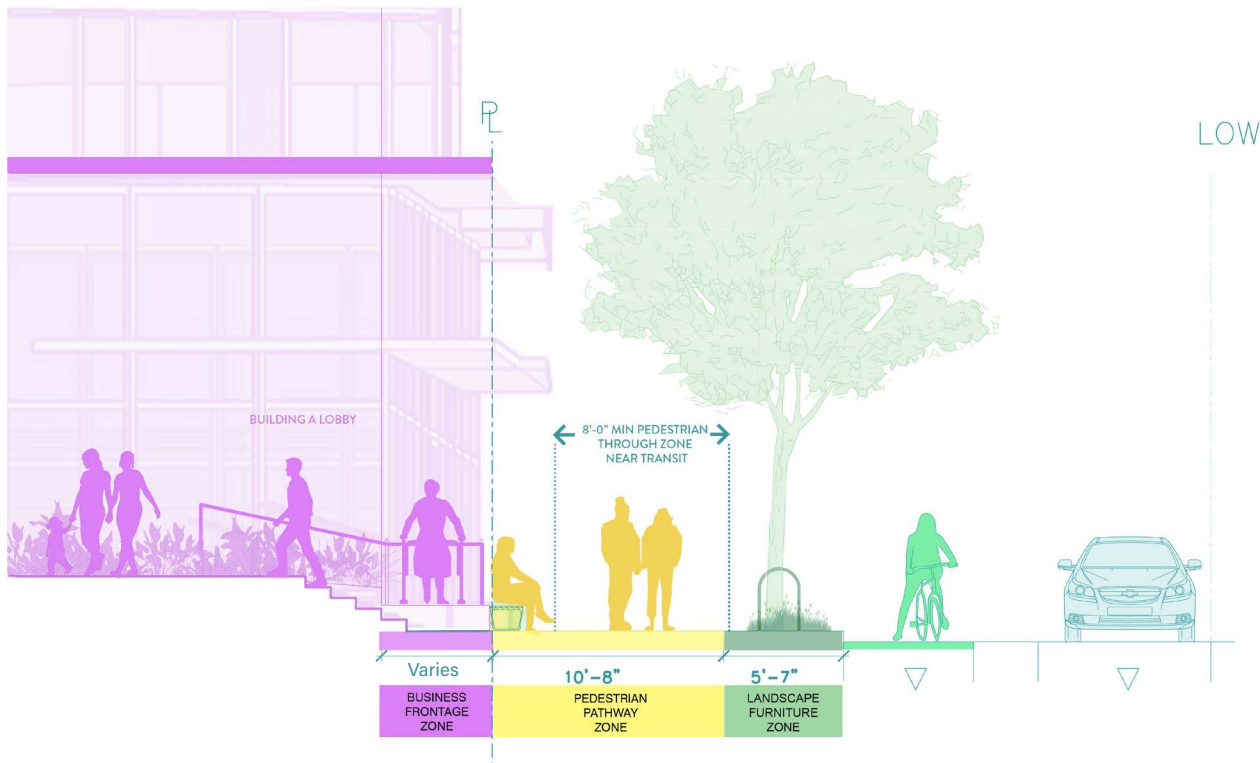


Figure 25: Fallon Street Block 1



### B.3. 9th Street Frontage (Building A)

9th Street is considered a Principal Street and serves as an important connector for auto, bicycle and pedestrian traffic traveling between Chinatown and Laney College. Active ground floor uses are encouraged on the 9th Street frontage with a focus on creating active corner elements. Access to parking and loading, and necessary ground floor services are located on the 9th Street frontage to avoid impacting the narrower Fallon Street frontage. Where non-active uses occur at the ground level street frontage, architectural and planting treatments should be used to maintain an attractive and lively pedestrian experience.



Figure 26: Special Paving between Tree Wells



Figure 27: Festival Lighting Infrastructure Cables

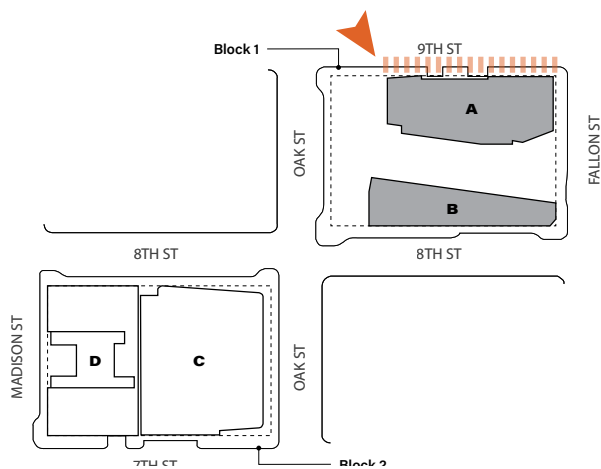


Figure 28: 9th Street and Oak Street Corner



### 9th Street Frontage: Design Standards

- a. The Building A tower shall be setback from the base building a minimum of 10 feet along the 9th Street frontage. See Figure 29.
- b. Parking uses located in the Building A podium shall be wrapped in a porous screen with minimum 25% openings to allow for natural ventilation. Screening element to be integrated with overall building design, or consist of high-quality, contrasting material to create an architectural feature. (LMSAP/DG-98: Integral Design)

### 9th Street Frontage: Design Guidelines

- a. It is recommended that the ground floor be set back two to four feet from the public right-of-way to create a wider sidewalk with a minimum of eight foot clearance. Upper levels of the building may extend over the ground floor set back to the public right-of-way, maintaining a 15-foot minimum height from the sidewalk. (LMSAP/DG-125: Sidewalk Elements)

- b. Active ground floor uses may be provided at the building corners at Fallon Street and Oak Street. The minimum length of the active frontage from each corner may be not less than 30 feet. (LMSAP/DG-8: Primary Lot Frontage)
- c. Transparent openings may be provided at active uses such that windows allow views of indoor space between two and nine feet above the sidewalk. (LMSAP/DG-32: Views of Indoor Space)
- d. The access for parking and loading areas may be as narrow as functionally possible. (LMSAP/DG-92: Vehicular Access)
- e. Garage and other service access doors are recommended to be set back from the public right-of-way two to four feet to reduce prominence. It is also recommended that access doors be fabricated from high-quality and easily maintained materials, with glazing incorporated where possible. (LMSAP/DG-93: Site Design)

- f. Non-active ground floor uses including service areas and parking facilities may be articulated with architectural elements such as a change in material and/or texture, screening elements, translucent windows and plantings (LMSAP/DG-93: Site Design)
- g. At the 9th Street frontage the tower may be set back 10 feet from the Building Base to reduce apparent scale and modulation of the podium facade. (LMSAP/DG-19: Step Back Above the Podium Height)
- h. Step backs above the Building Base may be utilized as roof gardens and active outdoor space. (LMSAP/DG-27: Active Upper Stories)

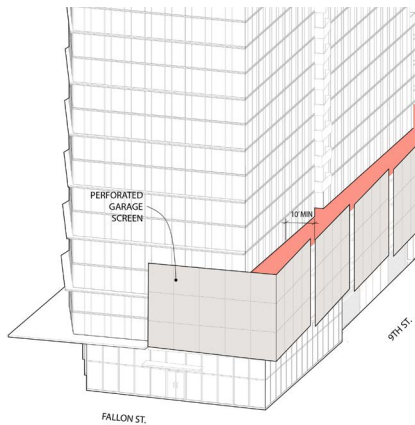


Figure 29: Building A Corner at 9th and Fallon Street.

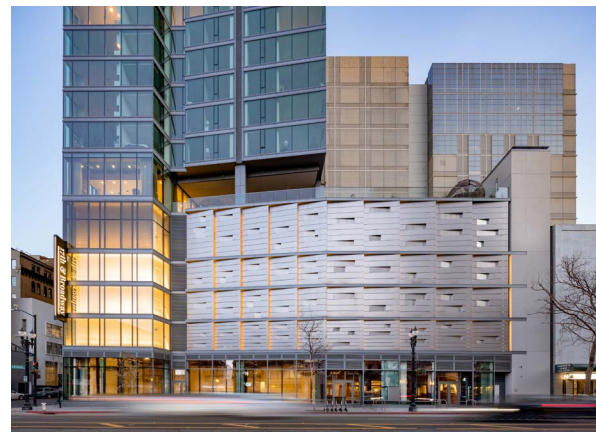
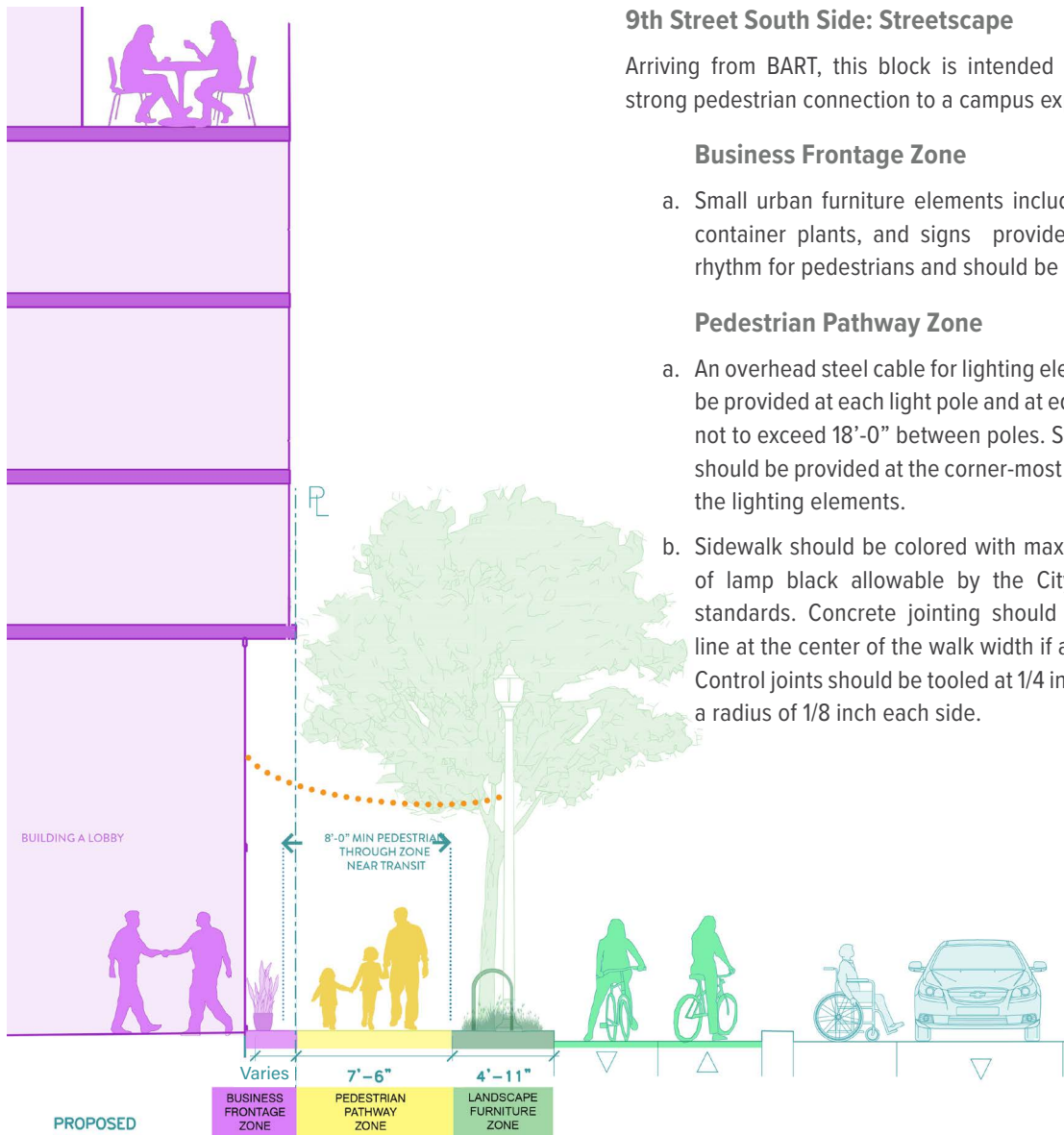


Figure 30: Porous Screen at Parking Uses



Figure 31: Active Use near Oak Street / BART





### 9th Street South Side: Streetscape

Arriving from BART, this block is intended to feel like a strong pedestrian connection to a campus experience.

#### Business Frontage Zone

- a. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

#### Pedestrian Pathway Zone

- a. An overhead steel cable for lighting elements should be provided at each light pole and at equal distances not to exceed 18'-0" between poles. Secured power should be provided at the corner-most light poles for the lighting elements.
- b. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.

- c. Corners of sidewalks should be stamped with letters to show name of street and or text connecting to cultural identity of site such as poetry or history.
- d. New street trees that could be carried west to Madison Park should be provided along 9th to connect to the campuses of Laney College and OMCA, strengthen its identity as a green street, and increase livability. (LMSAP/DG-110: Tree Planting and Preservation)

#### Landscape Furniture Zone

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- b. Between tree wells concrete unit pavers, stamped concrete, or cobblestones should be use to unify the three blocks of 8th and 9th at the curb edge. Stamp design could be created with an artist to be culturally relevant to Chinatown.
- c. Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials. For 8th and 9th these selections should extend to three block open space and may be colorful or otherwise stand out to reinforce the frame of the open space on 8th and 9th Streets.

Figure 32: 9th Street Block 1



### B.4. 8th Street Frontage (Building B)

8th Street is considered a Principal Street and serves as an important connector for auto, bicycle and pedestrian traffic. It is designated as a pedestrian-oriented spine, a priority lighting corridor, a commercial corridor and transit preferential street. The Oak Street corner is the priority location for active retail uses on Block 1 and provides an important gateway to the public open spaces at the BART Plaza and the Paseo. The Fallon Street corner is also highly prominent due to the narrow building footprint and also provides an important gateway to the Paseo. Necessary ground floor services are located on 8th Street frontage to avoid impacting the Fallon Street frontage and public spaces at the Paseo and BART entrance areas.



Figure 33: Bus Bulb Out



Figure 34: Sculptural Skylight Wayfinding

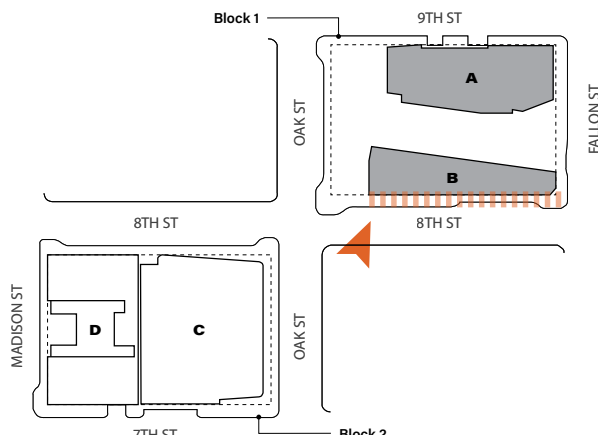


Figure 35: 8th Street Frontage



### 8th Street Frontage: Design Guidelines

a. To allow for an eight-foot minimum clear width at sidewalk, the ground floor may be setback an average depth of two to four feet from the public right-of-way for the entire frontage length. Upper levels of the building may extend over the ground floor setback to the public right-of-way, maintaining a 15-foot-minimum height from the sidewalk. The ground floor setback may also provide room for planting treatments to reduce blank wall impacts where blank walls are unavoidable. See Figure 36. (LMSAP/DG-29: Distinct Ground Floor, DG-38: Blank Wall Limitations)

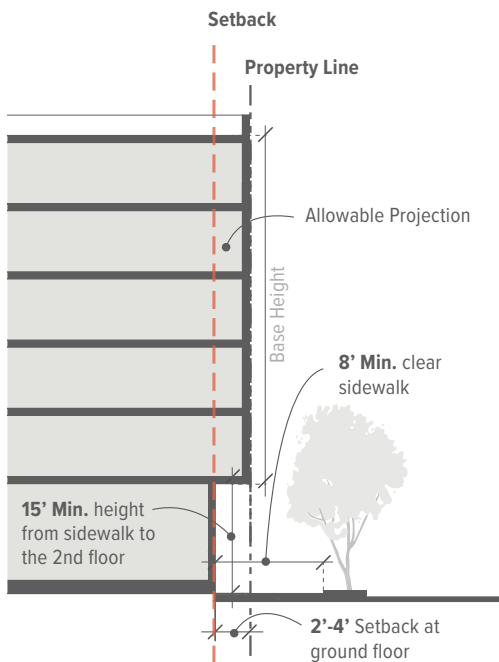


Figure 36: Ground Floor Setback

b. Massing breaks and architectural elements may be used to reduce the apparent building bulk along 8th Street. The massing breaks could correspond to the internal function of the building. (LMSAP/DG-16: Three-dimensional Articulation)

c. Building design may take advantage of the south facing frontage on 8th Street to create visually interesting patterns of lights and shadows. Proving regular rhythms of elements such as awnings, metal canopies, sunshades and bays, and/or recessed windows to create playful light and shadow patterns is recommended. Consider use of awnings, canopies, and other details over 'back of house' activities to

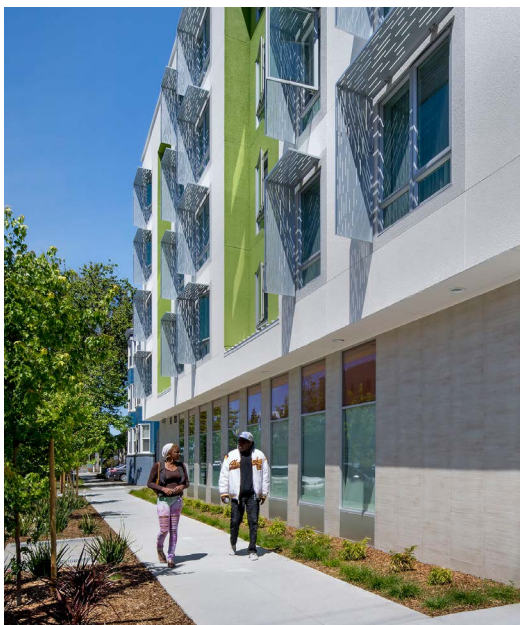


Figure 37: Planting at Ground Floor Setback

create a more interesting facade. (LMSAP/DG-16: Three-dimensional Articulation)

d. Active ground floor commercial spaces may be provided at the Oak Street corner with a recommended minimum length on 8th Street of not less than 30 feet, utilizing large glazing and horizontal awnings that fully wrap the corner of this façade. The minimum height of the ground floor commercial space from the 8th Street sidewalk may be no less than 15 feet and no colonnades are allowed in front of the commercial space. (LMSAP/DG-32: Views of Indoor Spaces, DG-7: Corner Building Design, DG-6: Avoid Colonnades)

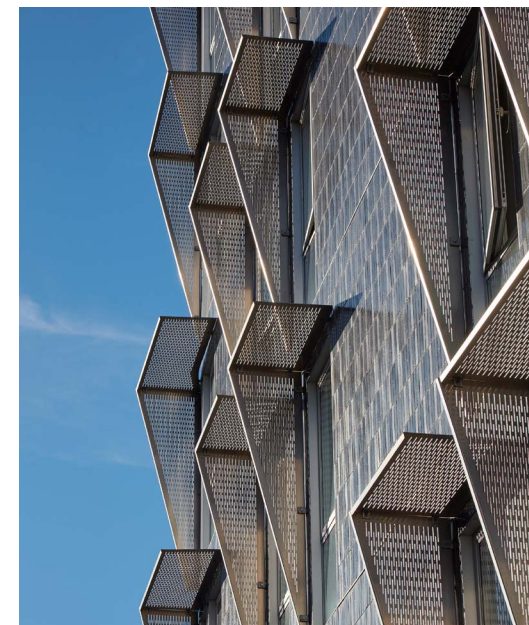


Figure 38: South Facing Frontage



- e. It is recommended to emphasize and highlight the building corner at Oak Street and 8th Street by architectural forms and features such as change in the height, roof form, different material, and recessed or projected architectural elements, to visually define and animate the intersection. (LMSAP/DG-7: Corner Building Design)
- f. Active ground floor uses may be provided at Fallon Street corner with a minimum length on 8th Street of not less than 30 feet. Active uses at this location may include residential lobby entrance or residential amenity spaces. (LMSAP/DG-32: Views of Indoor Spaces)
- g. Given the narrow building footprint at Building B, ground floor uses such as the community room and the lobby are encouraged to provide windows on both sides of the space to allow views from 8th Street frontage to the Paseo. This would activate the street frontage and also create visual connections between 8th Street and the Paseo. (LMSAP/DG-1: Public Perception, DG-32: Views of Indoor Spaces)
- h. It is recommended that the total active frontage be less than 50% of the ground floor frontage.
- i. A ground floor setback is encouraged at the Fallon Street corner to enhance public access and views to the Paseo from 8th Street. Upper levels of the building may extend over the corner setback to

- the public right-of-way. It is recommended that the corner ground floor setback be a minimum of two stories. (LMSAP/DG-29: Distinct Ground Floor, DG-26: Pedestrian Scale)
- j. It is recommended that blank walls, service doors and other non-active elements be limited to a maximum of 30% of the linear ground floor frontage on 8th Street. Consider use of windows, architectural details, landscaping, or art details at these non-active elements. (LMSAP/DG-38: Blank Walls)
- k. New street trees may be provided along 8th to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)

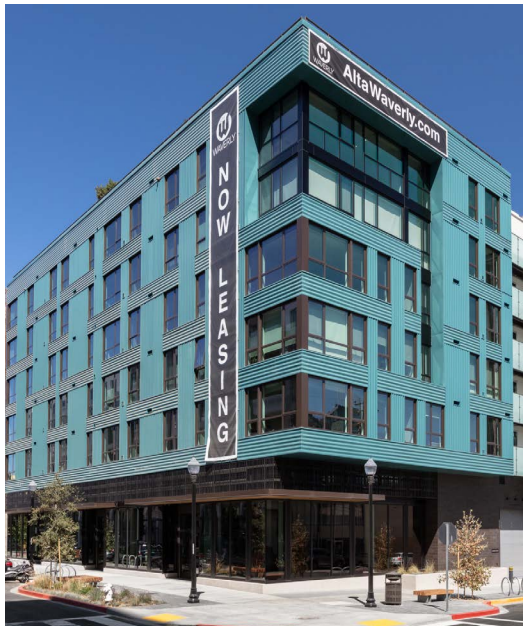


Figure 39: Building Corner and Active Uses



Figure 40: Main Residential Entry at the Corner

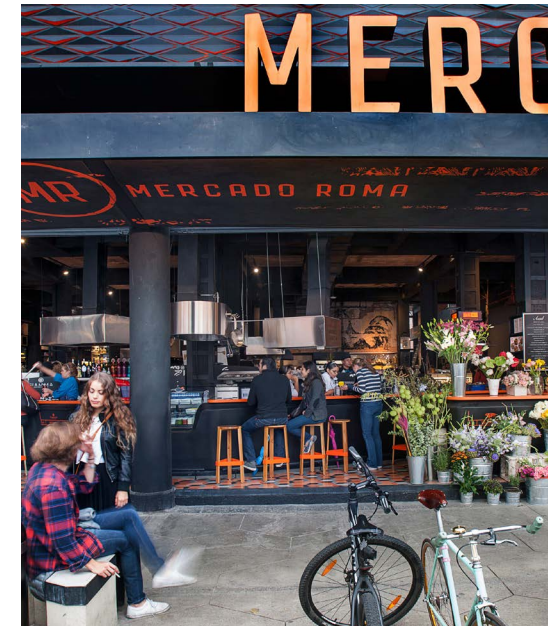


Figure 41: Transparency at Active Frontage



### 8th Street (North Side): Streetscape

An important corridor for arriving in Oakland from the freeway and points east on bicycle, this block is an important threshold for arrival in Chinatown.

#### Business Frontage Zone

- a. The building wall of 8th Street is experienced head on by those arriving from the south and east and as such should be memorable.
- b. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

#### Pedestrian Pathway Zone

- a. An overhead steel cable for lantern elements should be provided at each light pole and at equal distances not to exceed 18'-0" between poles. Secured power should be provided at the corner-most light poles for the lantern elements
- b. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- c. Corners of sidewalks should be stamped with letters to show name of street.
- d. Extended space for pedestrians and transition from shuttles and cars to BART is provided outside the landscape furnishing zone

#### Landscape Furniture Zone

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints

of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.

- b. Between tree wells concrete unit pavers, stamped concrete, or cobblestones should be use to unify the three blocks of 8th and 9th at the curb edge. Stamp design could be created with an artist to be culturally relevant to Chinatown.

- c. Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials. For 8th and 9th these selections should extend to a three block open space and may be colorful or otherwise stand out to reinforce the frame of the open space on 8th and 9th.

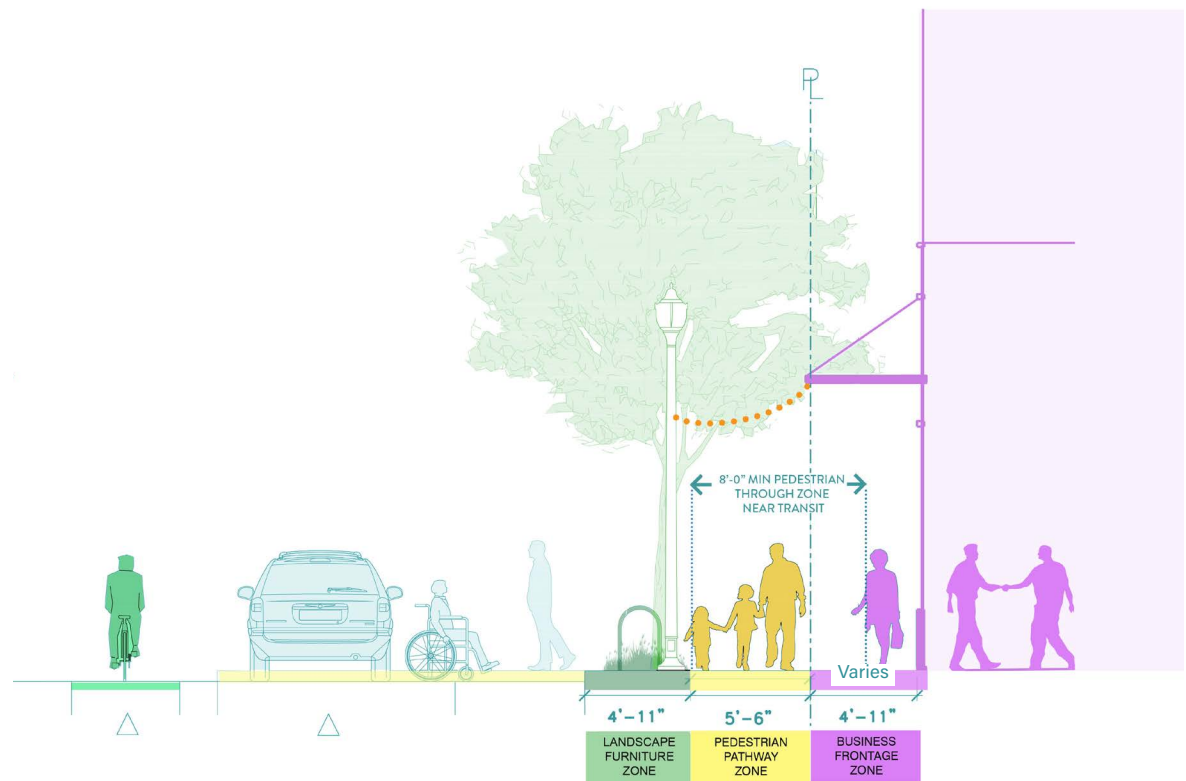


Figure 42: 8th Street Block 1



### B.5. Paseo Frontage (Buildings A and B)

The Paseo will provide a new pedestrian-oriented connection between the BART entrances located on Oak Street and Laney College, Lake Merritt, and the Oakland Museum of California (OMCA) to the northeast. Active ground floor uses including retail and dining storefronts should be located near the BART headhouses and Oak Street. Building articulation, new landscape elements, and public art should enhance the pedestrian experience of this new public amenity. The high-rise Building A and mid-rise Building B should use a complementary and cohesive design language to create a vibrant sense of place.

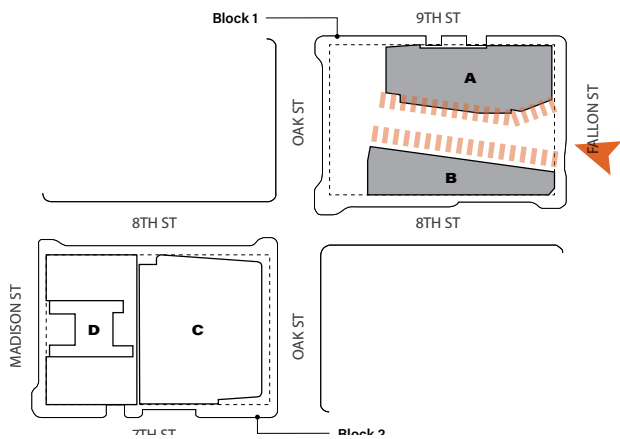


Figure 43: Paseo Frontage



**Paseo Frontage: Design Standards**

- a. Building A tower shall be setback from the building base.
- b. Parking uses located in the Building A podium shall be wrapped in a porous screen with minimum 25% openings to allow for natural ventilation. Screening element to be integrated with overall building design, or consist of high-quality, contrasting material to create an architectural feature. (LMSAP/DG-98: Integral Design)
- c. A canopy at least four feet in width shall be located above pedestrian level at the corner of the Paseo and Oak Street. See Figure 13.

**Paseo Frontage: Design Guidelines**

- a. At the Paseo-fronting facades, visually interesting architectural features may be employed to encourage interactions with passersby and highlight focal points. (LMSAP/DG-26: Pedestrian Scale)
- b. Active uses such as storefronts, dining and building entrances may be located at the ground floor

frontage on both sides of the Paseo to create a destination and active public space. (LMSAP/DG-39: Storefronts)

- c. It is recommended that pedestrian-scale articulation such as overhangs, recesses, enhanced lighting, and increased transparency are used to create an active and inviting public scale. (LMSAP/DG-26: Pedestrian Scale, DG-78: Building Lighting)
- d. It is recommended that awnings and canopies be in scale with the building and complement the overall design while providing protection from weather and sun. (LMSAP/DG-35: Awnings)
- e. Where blank walls are unavoidable, their facades may be articulated with architectural elements such as a change in material and/or texture, screening elements, translucent windows, public art, and/or plantings. (LMSAP/DG-38: Blank Walls)
- f. It is recommended to activate the roofs of the adjacent podium building(s) on the Paseo with shared amenities and other tenant uses to increase vibrancy of the pedestrian experience.



Figure 44: Use of Awnings and Indoor Outdoor Relation



Figure 45: Active Corners along the Paseo



Figure 46: Community Room along Paseo

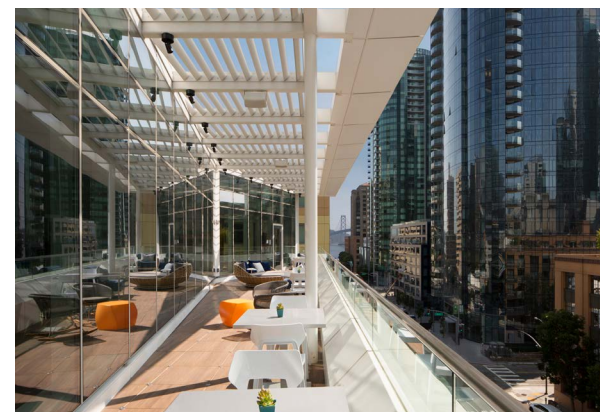


Figure 47: Active Use at Step Back



## C. Building Design – Street Frontage Guidelines for Block 2

### C.1. 8th Street Frontage (Buildings C and D)

8th Street is considered as a Principal Street and the “front door” for Block 2 as well as the primary address for both the high-rise Building C and the mid-rise Building D. 8th Street serves as an important connector for auto, bicycle and pedestrian traffic and it is designated as a pedestrian-oriented spine and Commercial Corridor. This segment of 8th Street links the commercial core of Chinatown with Madison Square Park, Lake Merritt BART Station, and Laney College. It is designated as a priority lighting corridor and transit preferential street. The BART Plaza on the north side of 8th Street is directly opposite Block 2. Active uses are encouraged to face 8th Street to enhance the pedestrian experience. Setbacks at the ground floor are encouraged to provide wider sidewalks and to provide a strong sense of arrival at the primary entries.

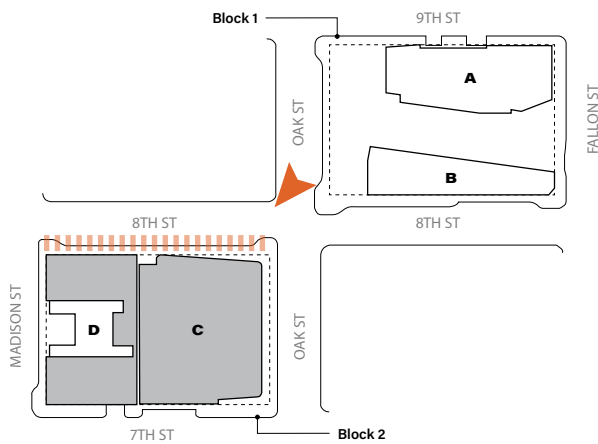


Figure 48: 8th Street Frontage



### 8th Street Frontage: Design Guidelines

- a. To allow for a five-foot six-inch minimum clear width at sidewalk, it is recommended that the ground floor be setback an average depth of two to four feet from the public right-of-way for the entire frontage length. The depth of this ground floor step back may vary. Upper levels of the building may extend over the ground floor set back to the public right-of-way. (LMSAP/DG-29: Distinct Ground Floor)
- b. Architectural articulations and landmark features may be used to highlight and emphasize the building corners at Madison and Oak Street to define and animate the intersections. (LMSAP/DG-7: Corner Building Design)
- c. Active ground floor uses with high transparency may be provided for the entire 8th Street frontage. Active uses may include building entries, residential amenities, childcare space or other community serving uses and retail. Office and administrative uses at the ground floor may not exceed 25% of the length of the frontage. (LMSAP/DG-32: Views of Indoor Spaces)
- d. At the Oak Street corner, Building C may provide an active use that wraps the corner on to Oak Street with large windows and other architectural features that create a strong relationship to the BART entrances and to Block 1. A flexible retail use with possible connection to the office lobby is recommended. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)
- e. A small plaza, building recess or other street level open space may be provided at or near the juncture of Building C and Building D. It is recommended that this mid-block break may be designed as a welcoming “front porch” to accommodate a primary entry or entries for one or both buildings. (LMSAP/DG-32: Views of Indoor Spaces, DG-48: Entry)

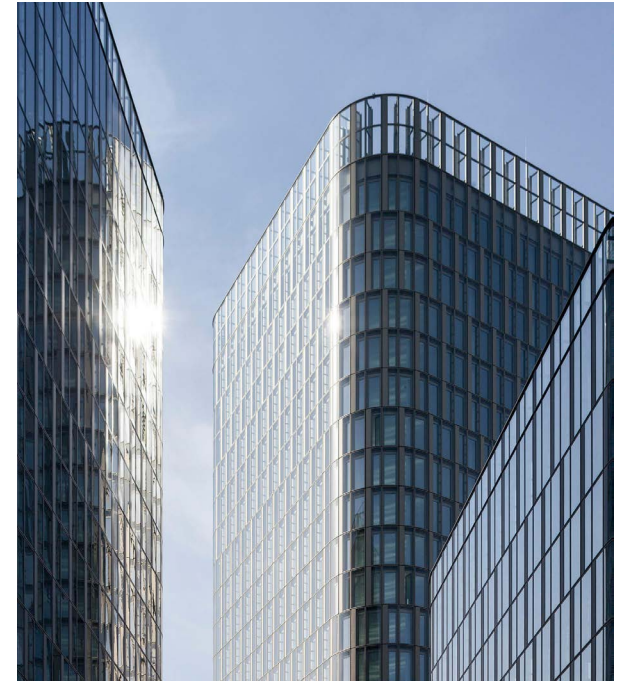


Figure 49: Tower Articulation



Figure 50: Active Corners



- f. The Tower at Building C may be stepped back from the interior lot line at Building D to provide a mid-block massing break between Building C and Building D. It is recommended that this setback be a minimum 10 feet for a minimum of 50% of tower perimeter, measured from the public right-of-way. It is recommended that this massing break be coordinated with the street level open space.
- g. It is recommended that the design of the high-rise Building C and the mid-rise Building D be coordinated to create a cohesive frontage and to reinforce the overall sense of identity for the block. This coordinated design may take the form of a similar vocabulary of forms, openings and materials.
- h. At the 8th Street frontage the tower may be stepped back up to 10 feet minimum from the building base to reduce the apparent scale of the tower.
- i. The tower massing may employ articulation strategies such as massing breaks, tapering and sculpted corners and roofline to reduce apparent mass and create a distinctive profile on the skyline. (LMSAP/DG-22: Slender Towers, DG-24: Distinguished Tower Design)
- j. Open spaces may be provided at step backs and roof tops facing 8th Street to take advantage of views to BART Plaza, Lake Merritt and to downtown Oakland.

- k. Blank walls, service doors and other non-active elements may be limited to a maximum of 15% of the linear ground floor frontage on 8th Street. (LMSAP/DG-38: Blank Walls)
- l. Where blank walls or service doors occur at the ground level, consider incorporating artwork to enhance the 8th Street as the primary frontage for Block 2.

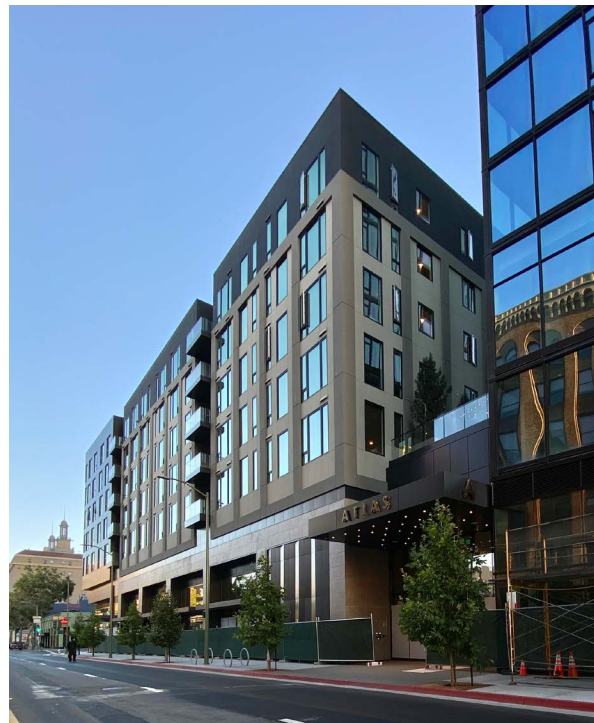


Figure 51: Mass Break between Buildings C and D

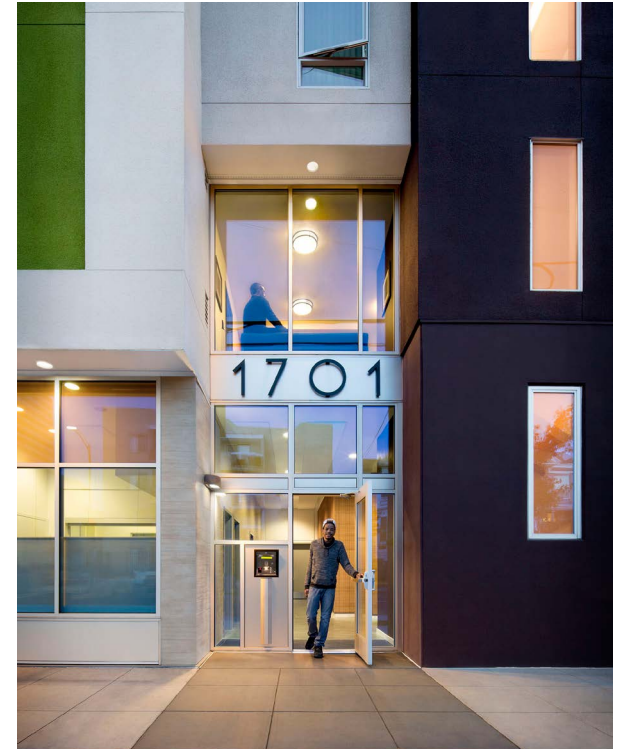


Figure 52: Prominent Residential Entry



Figure 53: Artwork on Blank Walls



## 8th Street (South Side): Streetscape

Located on an important corridor this block creates a street wall for framing the three block open space.

### Business Frontage Zone

- Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

### Pedestrian Pathway Zone

- Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- Corners of sidewalks should be stamped with letters to show name of street.

### Landscape Furniture Zone

- Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

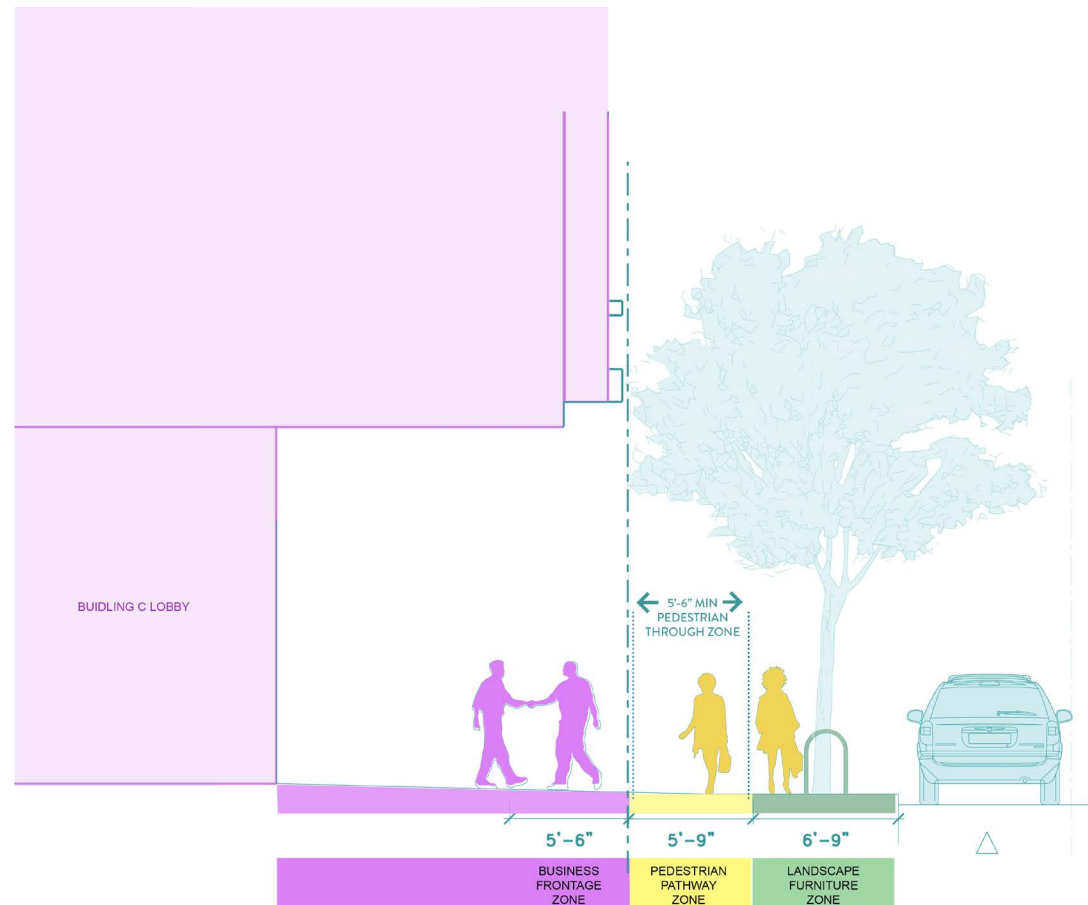
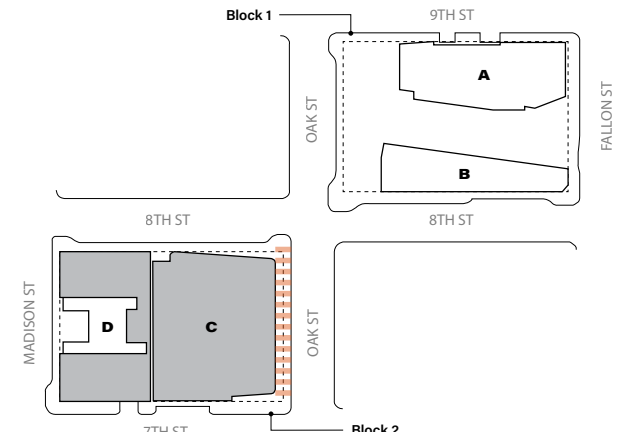


Figure 54: 8th Street Block 2



## C.2. Oak Street Frontage (Building C)

Oak Street is the southern gateway to the Lake Merritt BART Project and is identified as a green street in the LMSAP Design Guidelines. The three north bound lanes provide auto, bike and pedestrian connections from the Jack London neighborhood to Lake Merritt BART Station, the Oakland Museum and to Lake Merritt. Oak Street also provides a direct connection to the Lake Merritt BART Station from the I-880 Freeway. The east side of Oak Street is occupied by two-story residential buildings with varying setbacks. Oak Street slopes down approximately five feet from 8th to 7th Street. This slope will impact the arrangement of ground floor uses and the feasibility of entries to ground floor active uses. Wide sidewalks, active ground floor uses and upper floor step backs should be utilized to provide a welcoming pedestrian experience and a transition from the residential buildings on the south side of Oak Street to the high-rise Building C.



### Oak Street Frontage: Design Guidelines

- It is recommended that the ground floor be setback to allow for additional width at the sidewalk. The depth of the ground floor setback may vary but cannot exceed five feet. Where upper levels of the building extend over the ground floor setback, it is recommended that a generous 15-foot minimum clear height be provided and the setback be free of columns to increase visibility between the interior and the public way. (LMSAP/DG-6: Avoid Colonnades, DG-32: Views of Indoor Spaces, DG-128: Streets with Special Functions)
- Active ground floor uses may be provided along the entire Oak Street frontage. Active uses may include building entries, a maker space or retail use. Multiple entries are encouraged where feasible. Office and administrative uses at the ground floor may not exceed 25% of the length of the frontage. (LMSAP/DG-30: Ground Floor Entrances)



Figure 55: Active Frontage



- c. Architectural articulations and landmark features may be used to highlight and emphasize the building corners at 8th Street and 7th Street, including sculpted corners, changes in material and other features. (LMSAP/DG-7: Corner Building Design)
- d. At the 8th Street corner, Building C may provide an active ground floor use that wraps the corner onto 8th Street with large windows and other architectural features to create a strong relationship to the BART Plaza and to Block 1. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)
- e. At the 7th Street corner, it is recommended that Building C provide an active ground floor use that wraps the corner onto 7th Street with architectural features to create a welcoming gateway element and a transition in scale to adjacent residential uses. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)
- f. The tower at Building C may be stepped back from the building base a minimum of 10 feet to reduce the apparent scale of the tower. (LMSAP/DG-19: Step Back Above the Podium Height)
- g. It is recommended that blank walls, service doors and other non-active elements be limited to a maximum of 15% of the linear ground floor frontage on Oak Street. (LMSAP/DG-38: Blank Wall Limitation)
- h. Where blank walls or service doors are unavoidable at the ground level, consider incorporating artwork to reinforce Oak Street as a gateway to the Lake Merritt BART Project. (LMSAP/DG-38: Blank Wall Limitation)
- i. New street trees and lush planting may be provided along Oak to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)



Figure 56: Active Corners

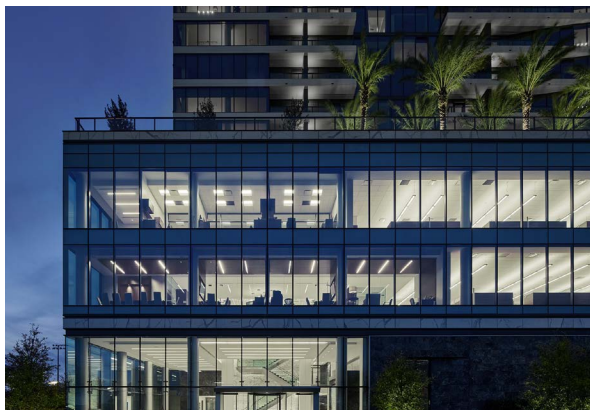


Figure 57: Step back above Building Base



### Oak Street (Block 2): Streetscape

An important corridor for arriving in Oakland from the freeway and points south, this block is an important threshold for arrival in Chinatown and heading towards the Lake, the OMCA, or the Courthouse beyond.

#### Business Frontage Zone

- A large setback at the ground floor provides space for the extension of the lobby program outside.
- Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

#### Pedestrian Pathway Zone

- Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- Corners of sidewalks should be stamped with letters to show name of street.

#### Landscape Furniture Zone

- Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.

- Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

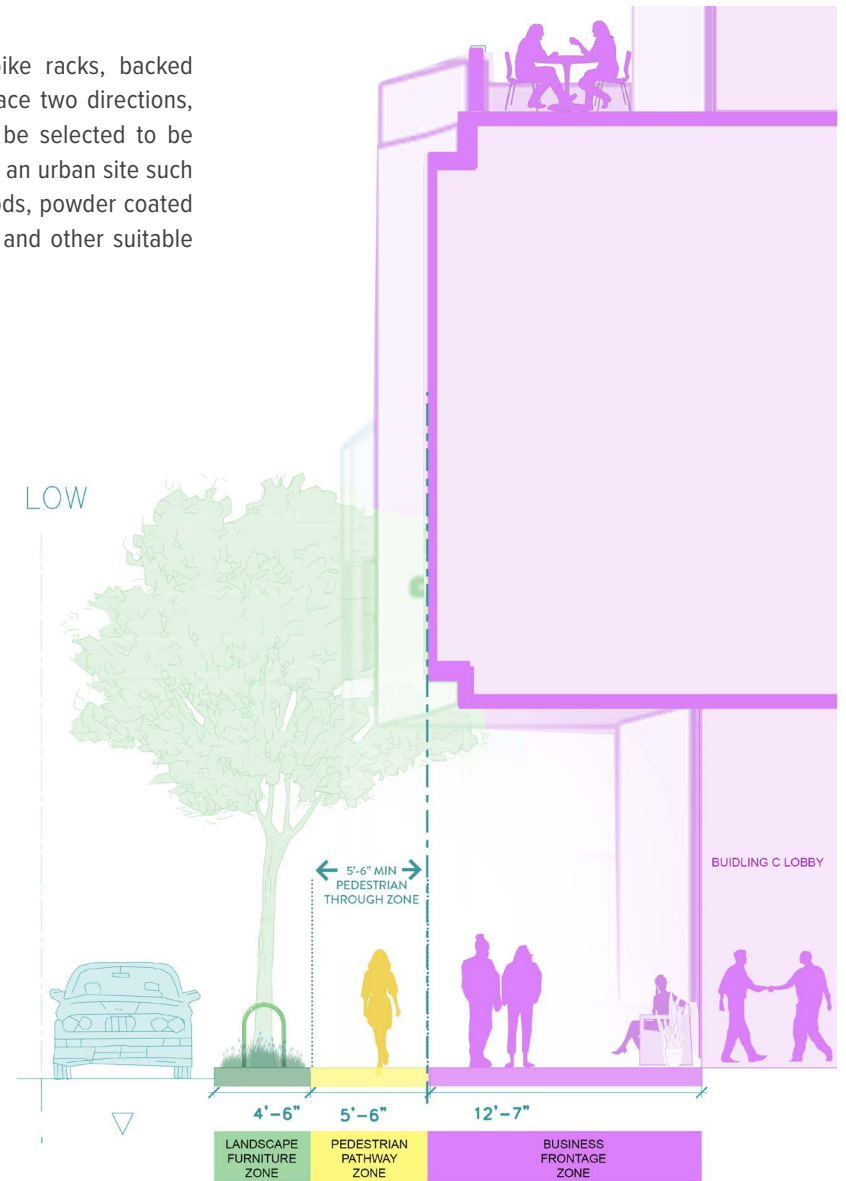


Figure 58: Oak Street Block 2



### C.3. 7th Street Frontage (Buildings C and D)

7th Street is a one-way street with three lanes of east bound traffic connecting the commercial and residential districts of Chinatown with south side of the Laney College campus and with the East Lake neighborhoods to the south. The south side of 7th Street is occupied by two- to three-story residential buildings with varying setbacks. Active uses should be provided at corner locations to create an attractive frontage. Parking entries and services for Building C and Building D should be consolidated mid-block at the 7th Street frontage to maximize the opportunity for active ground floor uses at the other frontages.

#### 7th Street Frontage: Design Guidelines

- To allow for additional width at sidewalk, the ground floor may be setback an average depth of two feet from the public right-of-way for the entire frontage length. The depth of the ground floor setback may vary but cannot exceed five feet. Upper levels of the building may extend over the ground level setback. See Figure 36.
- It is recommended that architectural articulation be used to highlight the building corners and active ground floor uses at Oak Street and Madison Street.
- Active ground floor uses may be provided at the Oak and Madison Street corners with a minimum length on 7th Street of 30 feet. Active uses may include building entries, childcare classrooms, maker space or retail use. Entries to active space from 7th Street are encouraged where feasible.
- It is recommended that parking access and service areas be located mid-block. Curb cuts for parking access, loading and waste rooms may be consolidated where feasible. (LMSAP/DG-92: Vehicular Access)
- It is recommended that the design of parking entries and service areas at Building C and Building D be coordinated to provide an integrated design and an attractive pedestrian frontage.
- Where the Building C tower is stepped back from the building base, occupied roof terraces may be provided to activate and improve the pedestrian experience. (LMSAP/DG-27: Active Upper-Stories)
- It is recommended that the tower at Building C be stepped back from the interior lot line at Building D to provide a mid-block massing break between Building C and Building D. It is recommended that this setback be at least 10 feet in width and at least 30 feet in depth, measured from the public right-of-way.

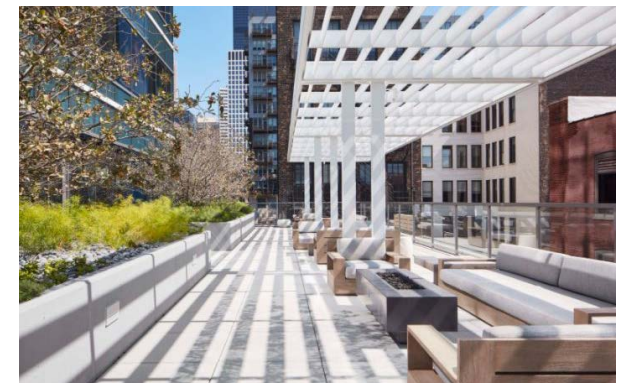
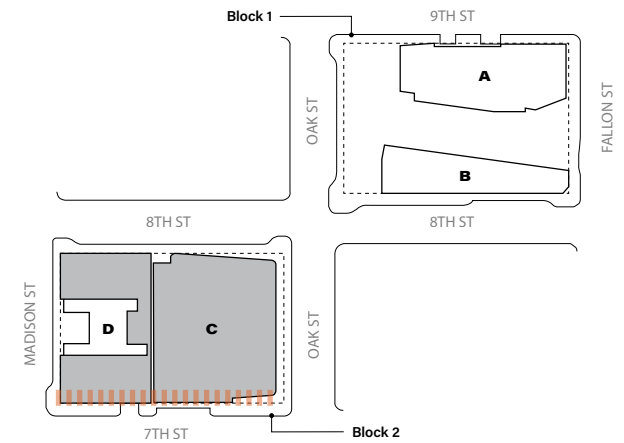


Figure 59: Active Roof Terrace at Step Backs



Figure 60: Highlight Building Corners



## 7th Street: Streetscape

A transition from larger buildings to smaller scaled houses, this street uses planting to soften and connect to neighbors gardens.

### Business Frontage Zone

- a. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

### Pedestrian Pathway Zone

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- b. Corners of sidewalks should be stamped with letters to show name of street.
- c. New street trees and lush planting along 7th are encouraged to reinforce its role as a green street and provide additional livability. (LMSAP/ DG-110: Tree Planting and Preservation)

### Landscape Furniture Zone

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.

- b. Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

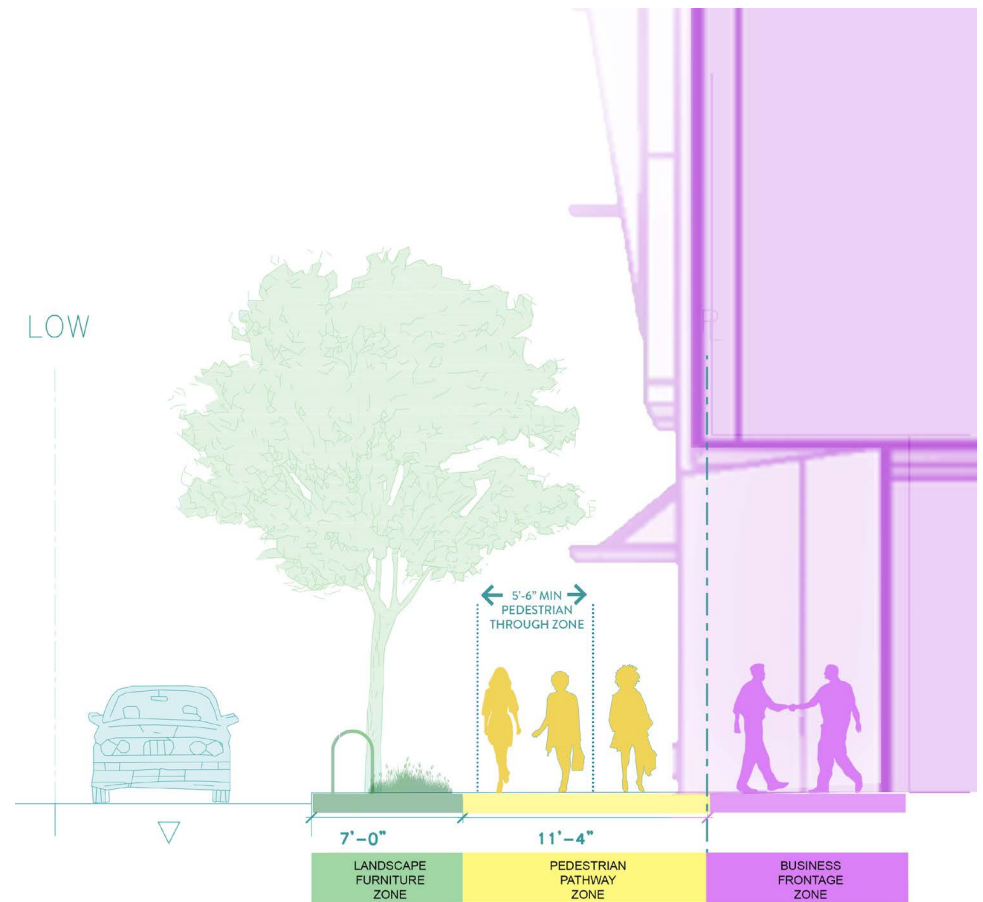


Figure 61: 7th Street Block 2



### C.4. Madison Street Frontage (Building D)

Madison Street is the western boundary of the Lake Merritt BART Project and provides auto, bike and pedestrian connections from the Gold Coast neighborhood to Madison Square Park and the Jack London neighborhood. The west side of Madison Street is occupied by two- and three-story residential buildings with varying setbacks. Madison Street slopes down approximately five feet from 8th Street to 7th Street. This slope will impact arrangement of ground floor uses and the feasibility of street level entries. Wide sidewalks, active ground floor uses and significant massing breaks at the mid-rise building should be utilized to provide a transition from the existing residential scale on the west side of Madison Street to the mid-rise Building D.

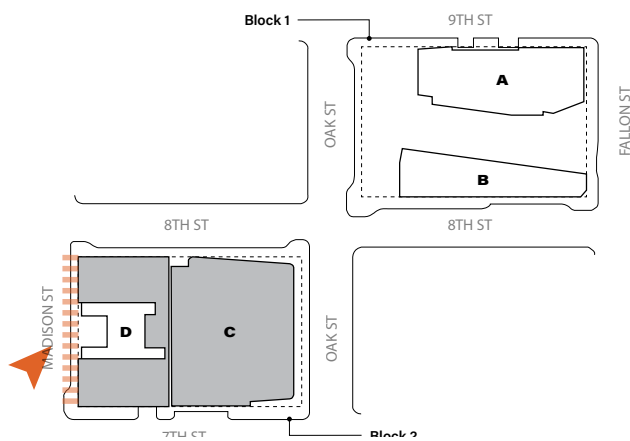


Figure 62: Madison Street Frontage



### Madison Street Frontage: Design Standards

- a. A six-foot deep and a 40-foot long awning along 7th Street and a 10-foot long along Madison Street shall be provided at the southwest corner of Building D.
- b. Upper level horizontal awnings shall be included at the second and third floors of the 7th and Madison Street corners. These upper floor awnings shall be four-foot deep and 30-foot long along 7th Street from the 7th and Madison Street. Additional horizontal awnings above the third floor are not required but encouraged. See Figure 63.

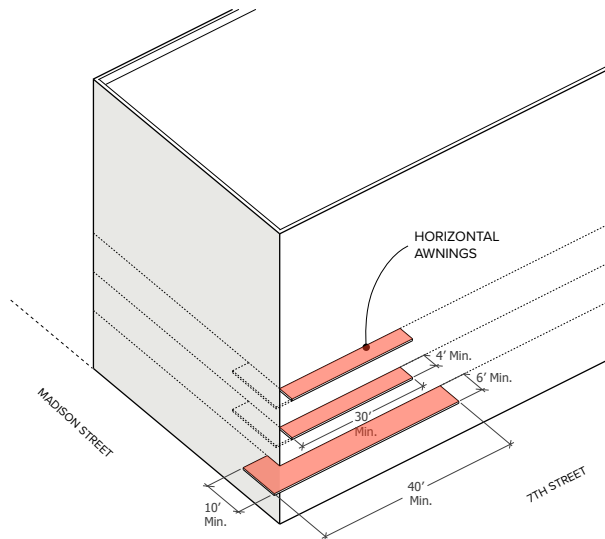


Figure 63: Awnings at Building D, South-West Corner

### Madison Street Frontage: Design Guidelines

- a. To allow for an eight foot minimum clear width at sidewalk, it is recommended that the ground floor be setback a maximum depth of five feet from the public right-of-way for the entire frontage length. The depth of the ground floor setback may vary. Upper levels of the building may extend over the ground level set back to the public right-of-way. Architectural and landscape features such as awnings and planting may be used to enhance the pedestrian experience and to reduce the impact of upper floor overhang. See Figure 30. (LMSAP/DG-29: Distinct Ground Floor)
- b. Entries, transparent windows and glazing may be provided at the 8th Street and 7th Street corners with a minimum length on Madison Street of 30 feet. (LMSAP/DG-32: Views of Indoor Spaces)
- c. Active uses are encouraged at the remainder of the frontage. These uses may include offices, community amenity and childcare care facilities. (LMSAP/DG-42: Flexible Commercial Space)

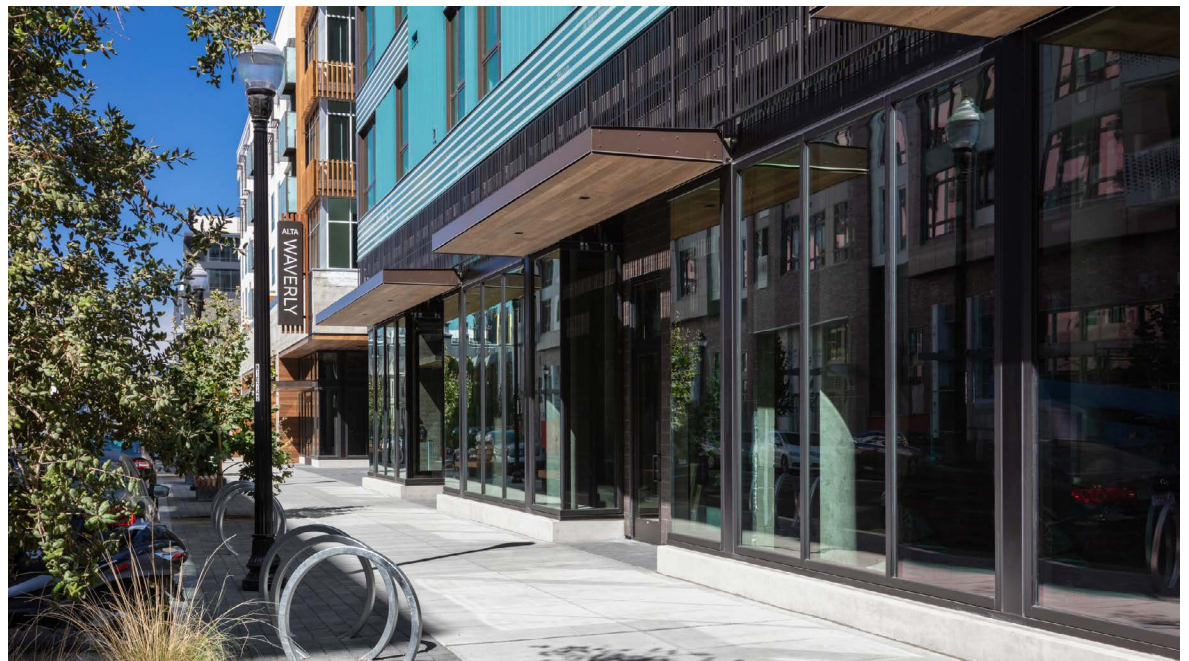
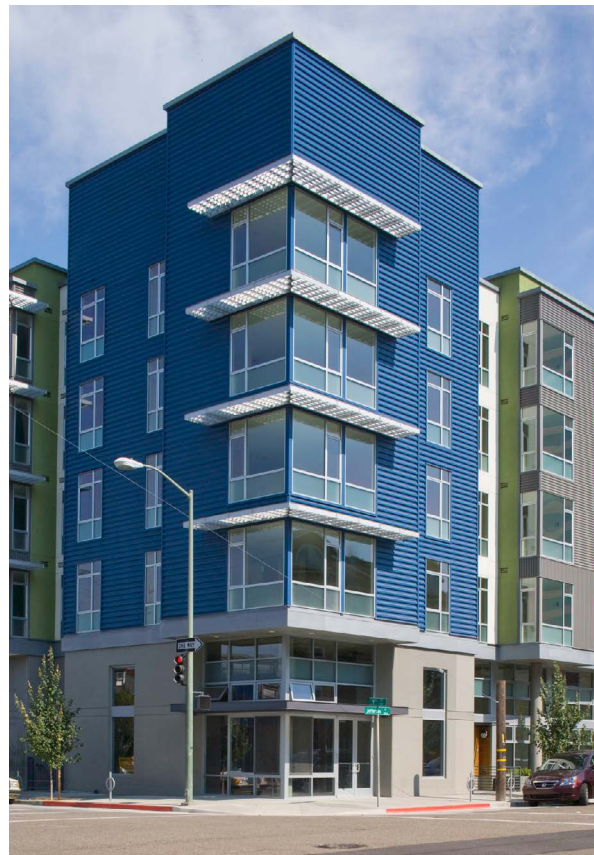


Figure 64: Ground Floor Setback and Use of Awnings



- d. Architectural articulations may be used to highlight the building corners and to active ground floor uses at 7th Street and 8th Street. (LMSAP DG-26: Pedestrian Scale)
- e. A street level plaza or courtyard is encouraged to create a mid-block break at the street level. It is recommended that this space be at least 30 feet in width and at least 20 feet in depth. This space may serve as a building entry, outdoor space for residents or outdoor space for a potential childcare space or other community serving use. A decorative fence may be provided to secure street level open space provided the fence is at least 75% transparent with a maximum eight-foot height. (LMSAP/DG-46: Street Wall Openings)
- f. If a mid-block residential courtyard is provided at Building D at the street level or at an upper level, an opening to Madison Street is encouraged to provide additional sunlight into the courtyard and to provide a massing break at the upper floors. This opening may be at least 20 feet in width. Bridging elements are acceptable within this opening provided, they are open-air and allow significant visual transparency into the courtyard. (LMSAP/DG-46: Street Wall Openings)



**Figure 65:** Building Corner Design at 7th and Madison Streets



**Figure 66:** Massing Break and Street Level Plaza



## Madison Street: Streetscape

A transition from larger buildings to smaller-scaled houses, this street uses planting to soften and connect to neighbors' gardens.

### Business Frontage Zone

- Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

### Pedestrian Pathway Zone

- Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- Corners of sidewalks should be stamped with letters to show name of street.
- New street trees are encouraged along Madison to provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)

### Landscape Furniture Zone

- Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet

- Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

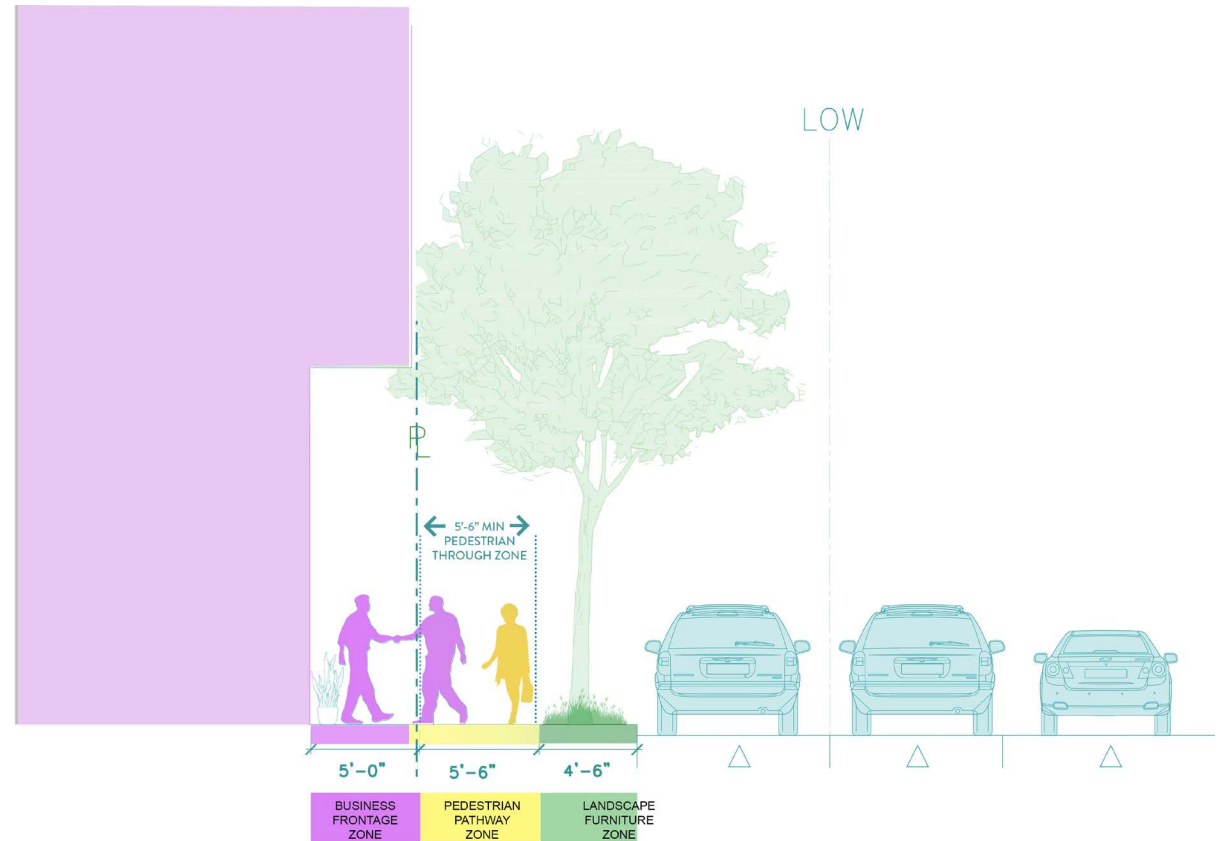


Figure 67: Madison Street Block 2



## 6. OPEN SPACE

The LMBTOD Project has an opportunity to extend the linear public realm that currently exists flowing from Madison Square Park to the BART Plaza, and into Block 1 of our redevelopment. This totally unique condition is one reason we sought to embrace the Paseo concept between Buildings A and B. It allows us to create a three block long publicly accessible open space, something that is quite uncommon in dense urban areas, and can be celebrated and highlighted by our project.

- a. The following recommendations are guidelines for the larger open spaces, not the sidewalk open space that is described above in the street frontages.
- b. In order to create a public realm that not only meets the needs of the community, but does so with the highest attention to health, safety, and environmental considerations the design may collocate public or active ground floor programs and adjacent open space as an important partnership in the success of both. (LMSAP/DG-11: Crime Prevention through Environmental Design)
- c. Maximize planting opportunities are encouraged to create identity, buffer, to create strong edges and to generally increase greening that can be experienced by users of the open space. It is recommended that planting selections meet WELO criteria and emphasize native and drought tolerant tough urban plants. Plants may also be selected for cultural resonance with Chinatown. (LMSAP/DG-84 through DG-87: Landscaping)
- d. It is encouraged to maximize a variety of conditions that include, sunny, shady, partially sunny, partially shady in well-scaled and hospitable locations for seating. (LMSAP/DG-137: Sun Exposure)

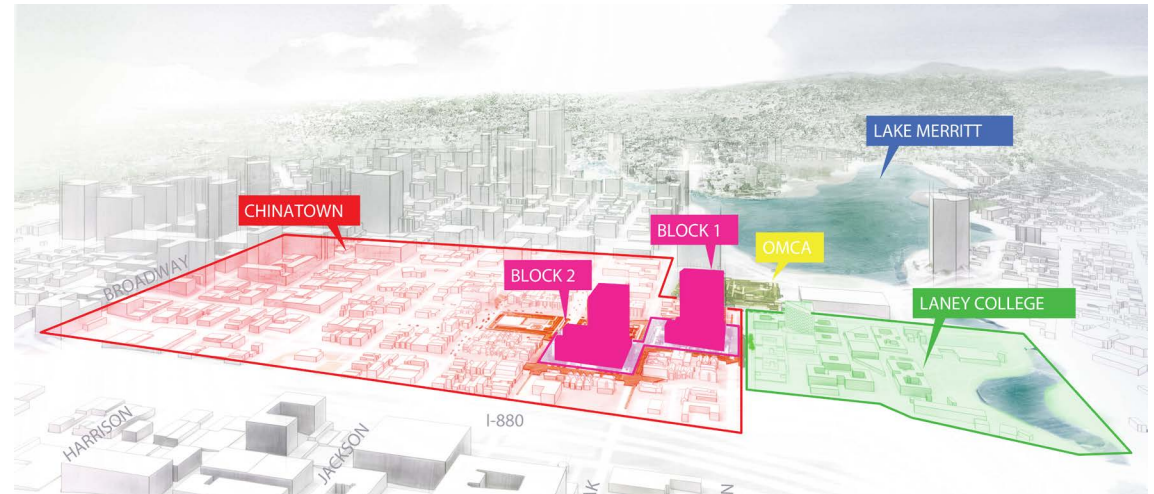


Figure 68: Adjacent Open Space Coordination

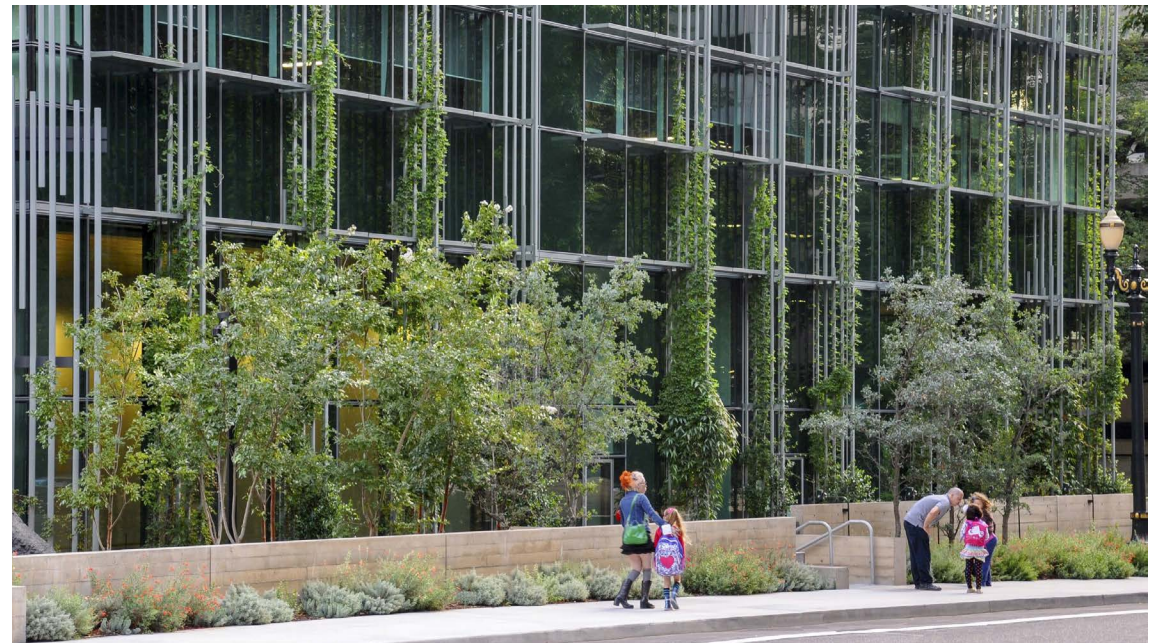


Figure 69: Maximize Greening by Utilizing the Vertical Plane



## A. Block 1

### A.1. The Paseo

- a. Concentrate outdoor dining at the west end of the Paseo to create a destination with multiple co-located options for dining.
- b. Create a layered program of use within the Paseo that is attractive at different times of day and attracts multigenerational users. (LMSAP/DG-142: Amenities and DG-146 through 148)
- c. Utilize materials that are appropriate to the civic and public nature of this location and meet BART’s Facilities Standards. (LMSAP/DG-143: Surfaces)
- d. Paseo design shall accommodate BART maintenance truck accessibility for regular maintenance of BART infrastructure.”



Figure 70: Dining

### Planting

- a. Planting in the Paseo must conform with BART Facilities Standards for work over existing BART tunnels. The planting must be hand watered and drainage must be directed off-site. Structural load criteria for the tunnel may limit soil volume and therefore the size or type of plants that could be planted here. The community process has resulted in a strong desire to maximize greening, so the design should try to achieve as much planting as possible within these above stated constraints.
- b. Plant selected should be low water, durable in a heavily used urban realm, and provide evergreen lush planting year round. Where possible with sun exposure, native plants, pollinator species, seasonal change, and culturally relevant plants should take priority in selections.
- c. Adequate soil volume and drainage should be provided for the long-term health of the plants.



Figure 71: Interactive Art



Figure 72: Under-story Pollinator Planting



Figure 73: Culturally Relevant Planting as Seasonal Change



Figure 74: BART Facility Standards



## Furniture

- Seating should be provided in a variety of types including: movable, fixed, communal, tiered, face to face, lounging, and back to back.
- Locations of seating should consider microclimate including wind, sun, and shade.
- Furniture should be high quality, durable, and beautiful. Color palette and material for furniture elements should be composed with both Paseo materials and architectural facade materials.
- Waste stations should provide for trash, recycling, and compost.
- No ash urns should be provided.

## Lighting

- Site lighting should be designed to provide a consistent level of lighting for faces across the Project site and at transitions off the Project area.
- The Paseo identity during early evening gathering and dining should be strengthened with lighting that creates and celebrates the cultural identity of this site. Lanterns, color, and other lighting should be used to create a ceiling and sense of place.



Figure 75: Movable Furniture



Figure 76: Fixed Furniture

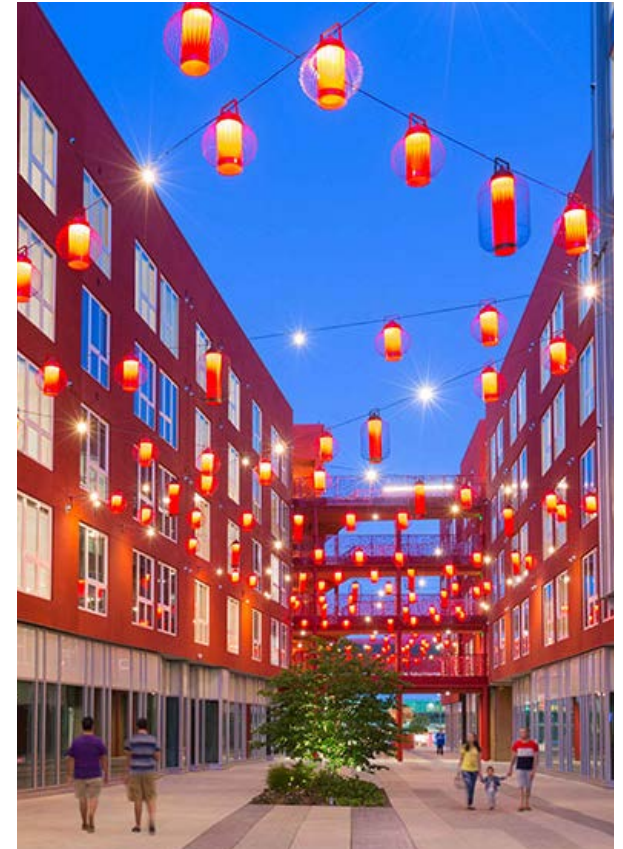


Figure 77: Modern Lighting



### Pavement

- Pavement in the Paseo must conform with BART Facilities Standards for work over existing BART tunnels.
- Pavement must be easily cleaned and durable for the heavy uses that are anticipated at most urban transit locations.
- Cast-in-place concrete with Lithocrete finish or other durable materials may be used for strong graphic pattern.
- Trench grate will integrate with ground floor pattern.

### A. 2. Roof Deck

- In consideration of local ecology of Lake Merritt and the open water, roof deck plantings should provide ecosystem services in the form of a pollinator garden. (LMSAP/DG-84 through DG-87)
- Program and utilization of roof decks should provide activation and diverse opportunities by multigenerational users.



Figure 78: *Lithocrete Activates Ground Plane*



Figure 80: *Trench Grate*



Figure 79: *Roof Top Pollinator Garden*



Figure 81: *Variety of Roof Top Programming*



## B. Block 2

### B.1. Entry Plaza

- Entry plaza pavement should use high quality materials and be differentiated from adjacent sidewalks. (LMSAG/DG-145: High Quality Materials)
- Visual connection to interiors of the building should be maintained for people arriving or departing from the building. (LMSAP/DG-11: Crime Prevention through Environmental Design)

### B.2. Residential Courtyard

- Create a layered program of use within the courtyard that is attractive at different times of day and attracts multigenerational users. (LMSAP/DG-142: Amenities, DG-146 through DG-148)
- Provide low-water and native planting that contributes to the local ecology. (LMSAP/DG-84 through DG-87)



Figure 82: Distinguished Plaza Pave Material

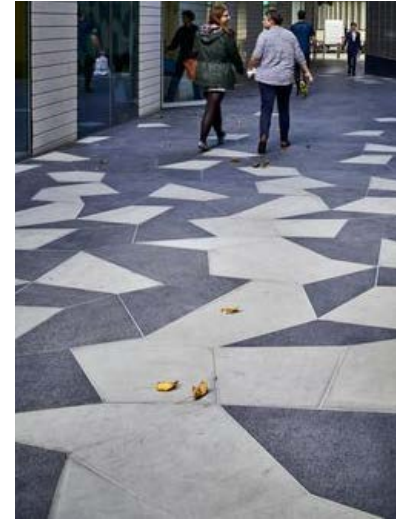


Figure 84: Distinguished Plaza Pave Material

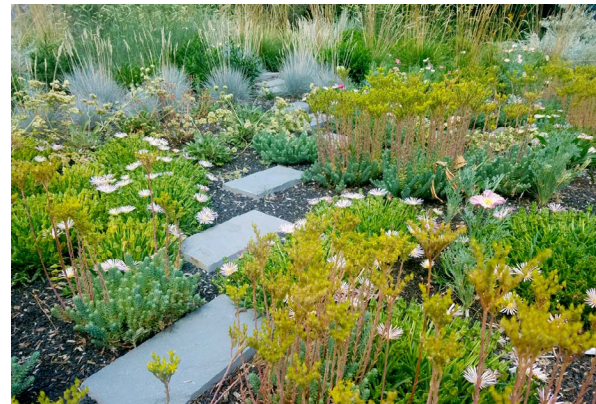


Figure 83: Native Planting Low-Water Use



Figure 85: Space for Different Activities, Ages



### B.3. Daycare Open Space

- a. Children's play environments must meet the required codes of play spaces, but should also connect children with the natural world through materials, planting selections, and art.



Figure 86: Nature Exploration



Figure 87: Nature Exploration

## 7. SIGNAGE

- a. **Consistency.** Signage will be consistent with the guidelines set forth in the Lake Merritt Station Area Plan Design Guidelines (LMSAP/DG-79 through DG-83).
- b. **Integrated Design.** Signage should be designed to reinforce the overall design character of the Lake Merritt Project. Signs and mounting systems should be integrated into the exterior design and should be constructed of high quality materials that complement the exterior material and color palette. (LMSAP/DG-79 through DG-82)
- c. **Visibility and Illumination.** Signage should be located and designed to be readily visible by pedestrians. Graphics should be designed to be highly legible and consistent with the exterior design intent. Illumination should be provided to ensure signage is visible in the evening hours. (LMSAP/DG-79: Illumination, DG-83: Legibility and Readability)

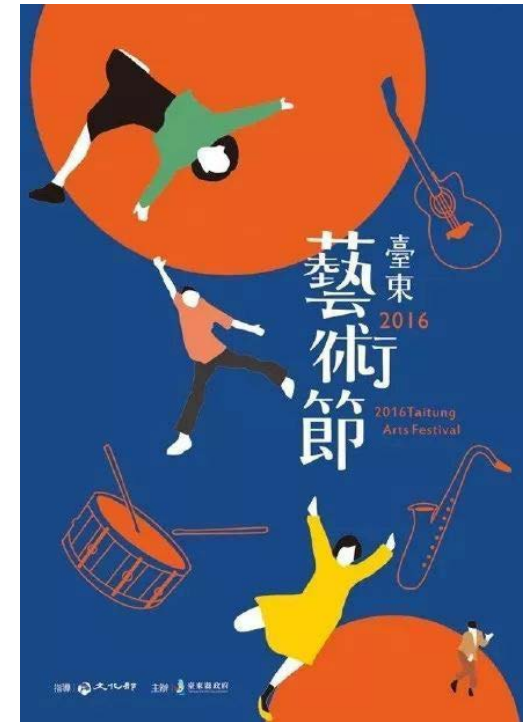


Figure 88: Multilingual Graphic



## 8. LIGHTING

- a. Exterior site lighting will be consistent with most of the guidelines set forth in the Lake Merritt Station Area Plan Design Guidelines (LMSAP/ DG-123: Lighting and Safety, DG-124: Pedestrian-Oriented Lighting, DG-150: Lighting) However, the additional of light as a goal without consideration of overall levels is not recommended. Lighting design proposed is consistent with best practices and generally focused on evenness of transitions rather than increased brightness.
- b. Provide adequate lighting to provide a safe environment for pedestrian safety while conforming to current best practices to mitigate light pollution.
- c. The lighting plan will be designed to create well lit plazas and pedestrian pathways through the site including surrounding sidewalks.



Figure 89: *Lighting of Local Obon Festival*



Figure 90: *Extend Festival Infrastructure*

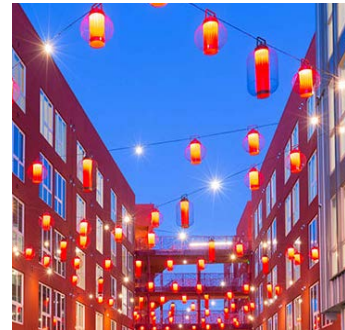


Figure 91: *Modern Light Fixtures*

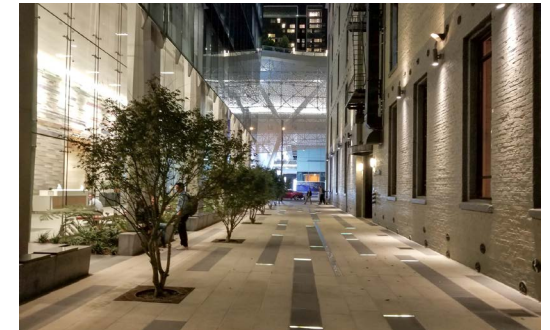


Figure 94: *Integrated Lighting Design*



Figure 92: *Existing Skylight*



Figure 93: *Skylight Becomes a Beacon*



Figure 95: *Integrated Lighting Design*



## 9. SUSTAINABLE DESIGN

This development is inherently sustainable in terms of land use due to the high density residential and commercial development near a major urban transit hub. Additional sustainable design measures can positively contribute to environmental benefits such as promotion of occupant comfort, water conservation, energy conservation, and healthy building environments. A few general sustainability measures are as follows:

### Site Design Measures

The development shall employ architectural strategies that are responsive to the local climate including solar orientation, prevailing winds, and precipitation.

- a. Orient units and/or provide architectural shading treatments to maximize winter solar exposure and minimize summer exposure.
- b. Provide on-site stormwater treatment as appropriate to the scale of the buildings and available open space. The stormwater design shall comply with the Municipal Regional Permit Order No. R2-0074 and the City of Oakland Storm Drainage Design Guidelines, which establish a 25% goal for peak flow reduction compared to existing conditions, to the extent possible.
- c. Provide on-site secure bicycle parking and secure bicycle parking for the most biked to station of the BART network, reducing the carbon footprint.

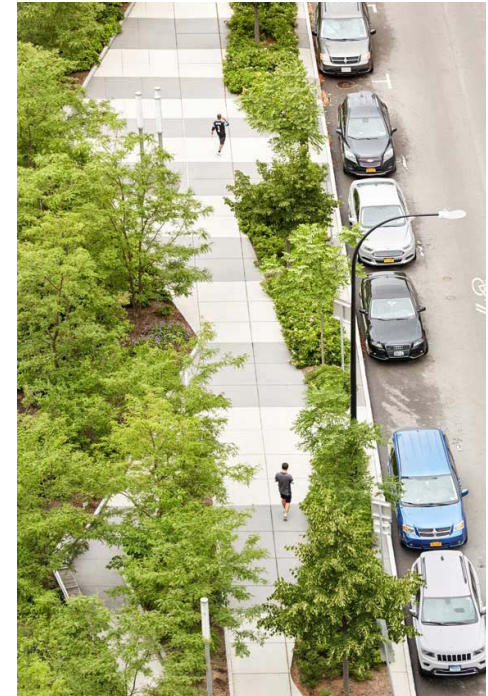


Figure 96: Stormwater Management



Figure 97: Stormwater Management



- d. Reduce parking capacity to a reasonable minimum.
- e. Consider designated parking for carpool vans or car share vehicles.
- f. Use native, drought-tolerant and shade tolerant landscaping to minimize irrigation required.
- g. Provide more vegetated spaces and street trees to reduce the heat island effect.



Figure 98: *Bike Rack*



Figure 99: *Bike Lockers*



Figure 100: *Scooter Corral*



Figure 101: *Kiss and Ride*

### Building Design Measures

- a. Use reflective roofing to minimize heat island effect.
- b. Use water-conserving fixtures and irrigation systems.
- c. Design building envelope, HVAC systems, lighting, and other systems to maximize energy efficiency. Consider fundamental commissioning of development systems.
- d. Consider on-site electrical generation or purchase of off-site renewable energy.
- e. Consider all-electrical building design and reduction of gas.
- f. Provide adequate facilities to allow for recycling by residents.
- g. Where possible, use recycled, salvaged, sustainably harvested, or locally produced materials.
- h. Use low- or no-VOC materials in interior spaces.
- i. Recommend that the development be designed and constructed in accordance with the recommendations of a recognized “Green” rating system such as GreenPoint, Enterprise Green Communities, and USGBC LEED rating.







## **ATTACHMENT B:**

Vesting Tentative Tract Map, Vesting Tentative Tract Map No. 8560 and 8577



# LAKE MERRITT BART DEVELOPMENT

## VESTING TENTATIVE TRACT MAP NO. 8560 (BLOCK 1) AND 8577 (BLOCK 2) TRACT MAP NO. 8560, A 3 LOT SUBDIVISION AND REMAINDER LOT (2 RESIDENTIAL UNITS, 2 COMMERCIAL UNITS, AND 2 NON-CONDOMINIUM PARCELS) TRACT MAP NO. 8577, A 2 LOT SUBDIVISION (1 RESIDENTIAL UNIT AND 5 COMMERCIAL UNITS) CITY OF OAKLAND ALAMEDA COUNTY STATE OF CALIFORNIA

### PROJECT DESCRIPTION

#### BLOCK 1 (TRACT MAP NO. 8560)

THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 1, BOUND BY 8TH STREET, FALLON STREET, 9TH STREET, AND OAK STREET. A NEW MARKET RATE RESIDENTIAL BUILDING (BUILDING A), A NEW SENIOR HOUSING BUILDING (BUILDING B), AND A NEW PEDESTRIAN PASEO WILL BE CONSTRUCTED ON THE PROPERTY SITE AND WILL MAINTAIN AND IMPROVE ACCESS TO THE EXISTING LAKE MERRITT BART STATION. THE PROJECT WILL REMOVE THE 11 EXISTING LOTS AND WILL BE SUBDIVIDED INTO 3 NEW LOTS AND 1 REMAINDER PARCEL.

#### BLOCK 2 (TRACT MAP NO. 8577)

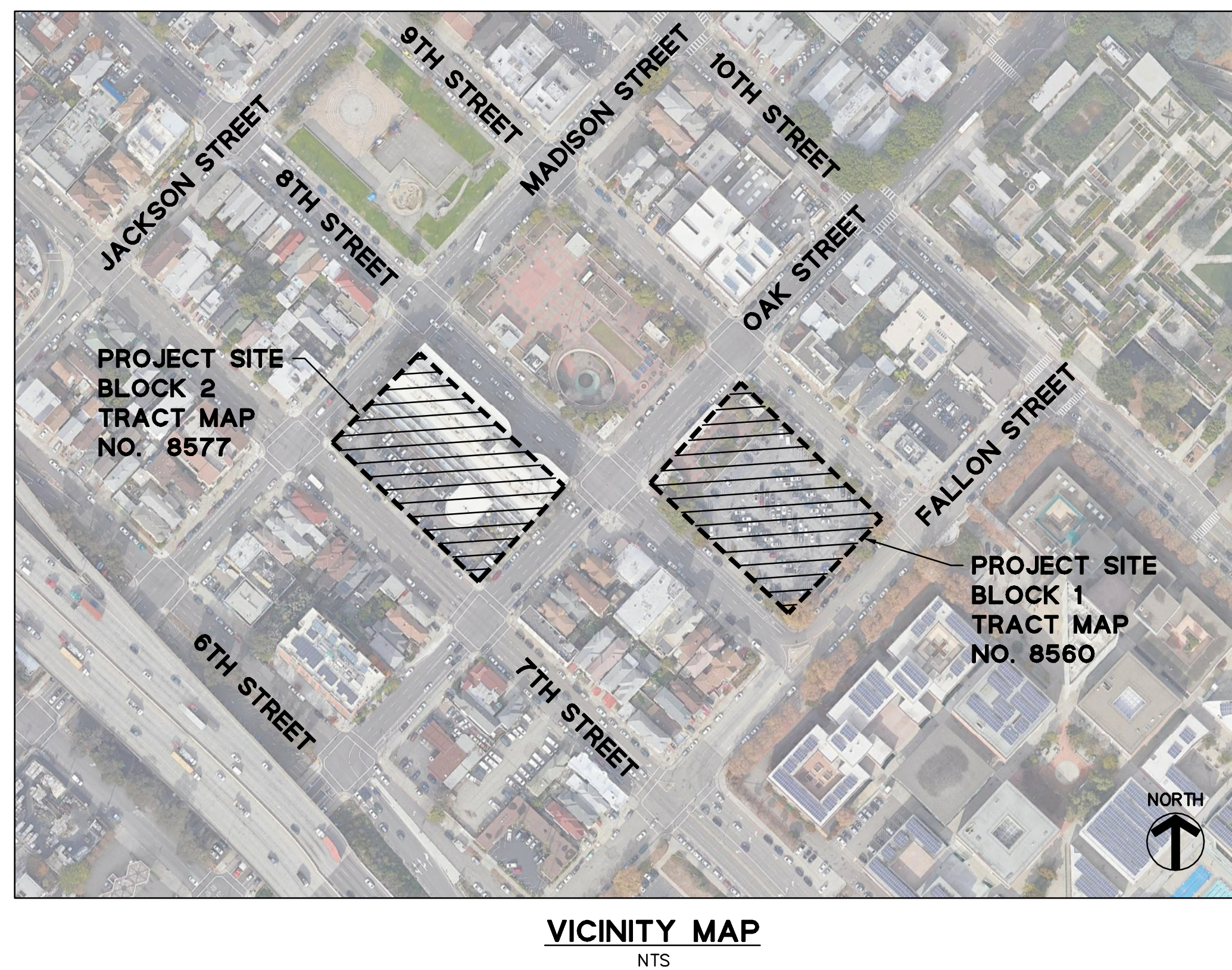
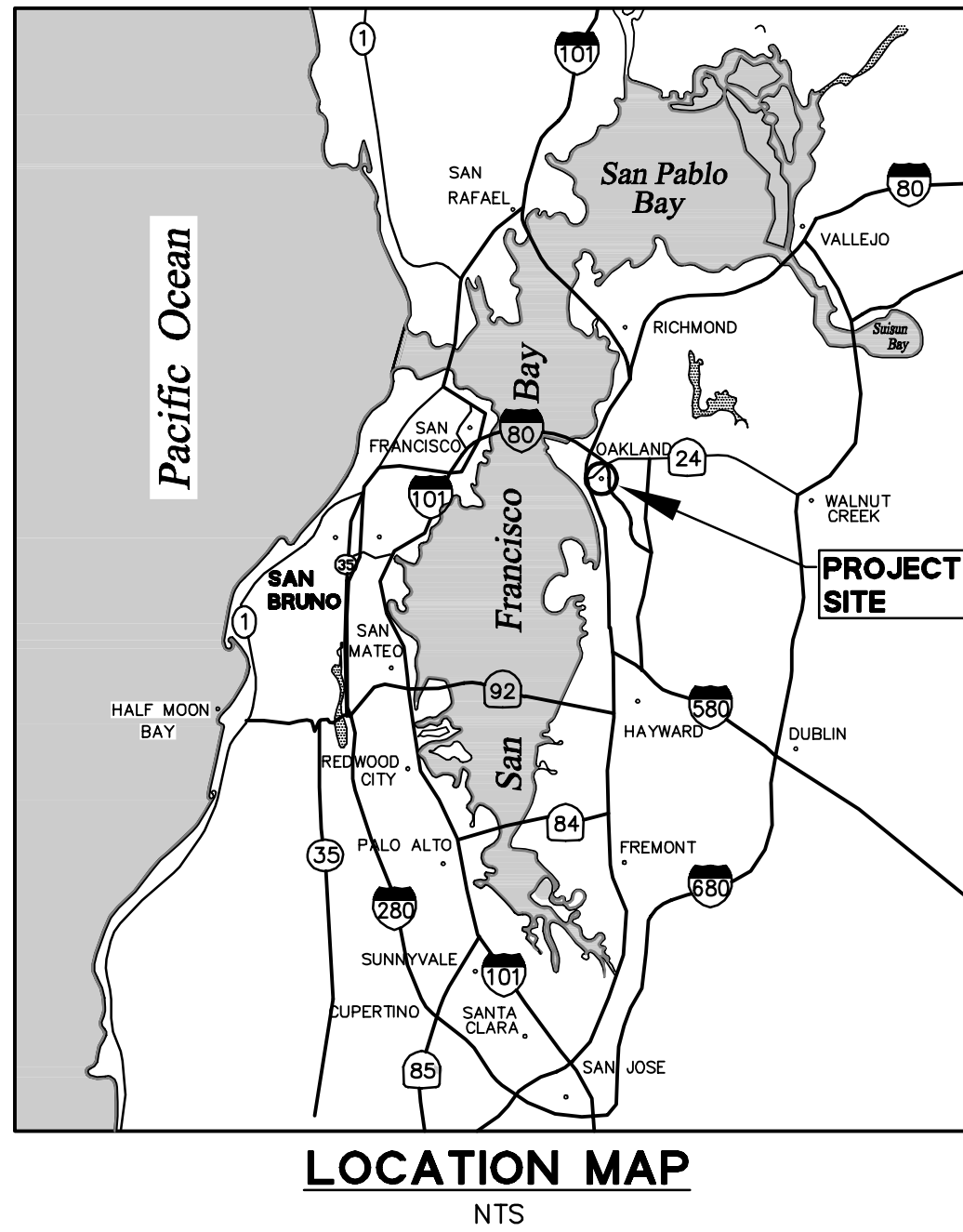
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 2, BOUND BY 7TH STREET, OAK STREET, 8TH STREET, AND MADISON STREET. A NEW OFFICE BUILDING (BUILDING C) AND A NEW AFFORDABLE HOUSING BUILDING (BUILDING D) WILL BE CONSTRUCTED ON THE PROPERTY SITE. THE PROJECT WILL SUBDIVIDE THE EXISTING LOT INTO 2 NEW LOTS.

### GENERAL NOTES

- VESTING TENTATIVE TRACT MAP:** THESE VESTING TENTATIVE TRACT MAPS ARE BEING FILED IN ACCORDANCE WITH CHAPTER 3, ARTICLE 2, SECTION 66452 AND WITH CHAPTER 2, ARTICLE 1, SECTION 66426 OF THE SUBDIVISION MAP ACT.
- CONDOMINIUM UNITS AND NON-CONDOMINIUM PARCELS:**
  - VESTING TRACT MAP NO. 8560 (BLOCK 1) - A 3 LOT SUBDIVISION WITH 1 REMAINDER PARCEL BEING: 1 RESIDENTIAL CONDOMINIUM UNIT AND 1 COMMERCIAL CONDOMINIUM UNIT AS TO LOT 1, 1 RESIDENTIAL CONDOMINIUM UNIT AND COMMERCIAL CONDOMINIUM UNIT AS TO LOT 2, AND 2 NON-CONDOMINIUM PARCELS AS TO LOT 3 AND A REMAINDER PARCEL.
  - VESTING TRACT MAP NO. 8577 (BLOCK 2) - A 2 LOT SUBDIVISION BEING: 4 COMMERCIAL CONDOMINIUM UNITS AS TO LOT 1, AND 1 RESIDENTIAL CONDOMINIUM UNIT AND 1 COMMERCIAL CONDOMINIUM UNIT AS TO LOT 2.
- MULTIPLE TRACT MAPS:** THE DEVELOPER INTENDS TO FILE TWO TRACT MAPS PURSUANT TO CHAPTER 3, ARTICLE 4, SECTION 66456.1 OF THE SUBDIVISION MAP ACT.
- SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY UNDER THE SUPERVISION OF DAVIS THRESH, PLS #6868, PERFORMED ON MAY 13TH, MAY 15TH, AND MAY 22ND, 2019. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
- FEMA DESIGNATED FLOOD ZONE:** PURSUANT TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY, NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, COMMUNITY NO. 06001C0067H, EFFECTIVE DATE DECEMBER 21, 2018, THE SUBJECT PROPERTY LIES WITHIN FLOOD ZONE 'X' - AREAS DETERMINED OF MINIMAL FLOOD HAZARD.
- UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES, AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
- BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HEREON IS BASED UPON RESOLUTIONS OF RECORD STREET AND LOT DIMENSIONS AND COLLECTED STREET MONUMENT LOCATIONS WITHIN THE SURROUNDING STREETS. MONUMENT COLLECTION WAS CONDUCTED ON APRIL 19, 2019. NO CURRENT MAP OR RECORD OF SURVEY CURRENTLY EXISTS FOR THE MAPPED BLOCKS; DEEDS MAKE REFERENCE TO KELLERSBERGER'S MAP OF OAKLAND FILED IN BOOK 7 OF MISCELLANEOUS MAPS AT PAGE 3, ALAMEDA COUNTY RECORDS.
- HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.
- BENCHMARK:** FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD88)  
**ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COOVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COOVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COOVD ELEVATIONS. (ALL FUTURE IMPROVEMENT PERMIT PLANS WILL REFERENCE THE CITY OF OAKLAND VERTICAL DATUM)**

### PROJECT DATA

<b>OWNERS:</b>	STRADA INVESTMENT GROUP 101 MISSION STREET, SUITE 420 SAN FRANCISCO, CA 94105 PHONE: (415) 263-9151 CONTACT: WILLIAM GOODMAN	EAST BAY ASIAN LOCAL DEVELOPMENT CORP 1825 SAN PABLO AVENUE, SUITE 200 OAKLAND, CA 94612 PHONE: (510) 287-5353 CONTACT: ANDREW MATSAS
<b>ARCHITECTS:</b>	PYATOK ARCHITECTS 1611 TELEGRAPH AVENUE, SUITE 200 OAKLAND, CA 94612 PHONE: (510) 465-7010 CONTACT: PETER WALLER	SOLOMON CORDWELL BUENZ ARCHITECTS 255 CALIFORNIA STREET, 3RD FLOOR SAN FRANCISCO, CA 94111 PHONE: (415) 216-2450 CONTACT: CYRIL CHONG
<b>CIVIL ENGINEER:</b>	BKF ENGINEERS 255 SHORELINE DRIVE, SUITE 200 REDWOOD CITY, CA 94065 PHONE: (650) 482-6377 CONTACT: SIMON NORTH	
<b>LANDSCAPE ARCHITECT:</b>	EINWILLERKUEHL LANDSCAPE ARCHITECTURE 318 HARRISON STREET, SUITE 301 OAKLAND, CA 94607 PHONE: (510) 891-1696 CONTACT: SARAH KUEHL	
<b>ASSESSOR PARCEL NO.:</b>	001-0169-001 (BLOCK 1) 001-0171-002 (BLOCK 2)	
<b>EXISTING LAND USE:</b>	COMMERCIAL	
<b>PROPOSED LAND USE:</b>	MIXED USE	
<b>LAND AREA:</b>	BLOCK 1: 60,031 SF (1.378 ACRES) BLOCK 2: 60,028 SF (1.378 ACRES)	
<b>UTILITY INFORMATION:</b>	WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD) FIRE PROTECTION: CITY OF OAKLAND / EBMUD SEWAGE DISPOSAL: CITY OF OAKLAND STORM DRAIN: CITY OF OAKLAND GAS: PACIFIC GAS & ELECTRIC (PG&E) ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E) TELEPHONE: AT&T CABLE TELEVISION: COMCAST	



### ABBREVIATIONS

AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BACK OF WALK
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EV	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
HCR	=	HANDICAP RAMP
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
P	=	POST
PKM	=	PARKING METER
SDCO	=	STORM DRAIN CLEANOUT
SDDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UB	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	LOT LINE
---	---	EASEMENT LINE
---	---	RECORD LOT LINE TO BE REMOVED
---	---	CURB AND GUTTER
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWCUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	CATCH BASIN
---	---	SSMH
---	---	SDMH
---	---	SDDI
---	---	WATER VALVE
---	---	GAS VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DDCA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	STREET LIGHT
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE

### SHEET INDEX

SHEET NO	DESCRIPTION
C1.0	TITLE SHEET
C2.1	EXISTING PARCELIZATION (BLOCK 1)
C2.2	EXISTING PARCELIZATION (BLOCK 2)
C3.1	PROPOSED PARCELIZATION PLAN (BLOCK 1)
C3.2	PROPOSED PARCELIZATION PLAN (BLOCK 2)
C4.1	PROPOSED GRADING PLAN (BLOCK 1)
C4.2	PROPOSED GRADING PLAN (BLOCK 2)
C5.1	PROPOSED UTILITY PLAN (BLOCK 1)
C5.2	PROPOSED UTILITY PLAN (BLOCK 2)

### ENGINEER'S STATEMENT

THIS TENTATIVE MAP HAS BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

*Simon North*  
SIMON R. NORTH, P.E. CA 64657  
VICE PRESIDENT  
BKF ENGINEERS

03/17/2021  
DATE

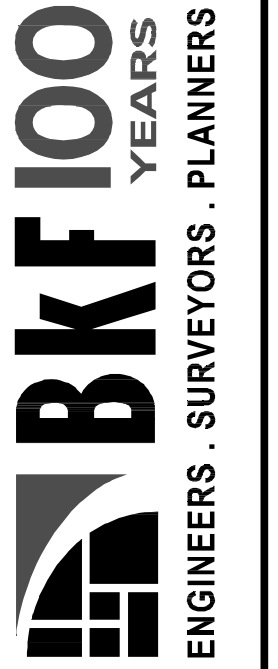
### SURVEYOR'S STATEMENT

THIS TENTATIVE MAP HAS BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD SURVEY PRACTICE.

*Davis Thresh*  
DAVIS R. THRESH, P.L.S. CA 6868  
PRINCIPAL  
BKF ENGINEERS

03/17/2021  
DATE

LAKE MERRITT BART DEVELOPMENT  
 VESTING TENTATIVE TRACT MAP NO. 8560/8577  
 TITLE SHEET  
 CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA

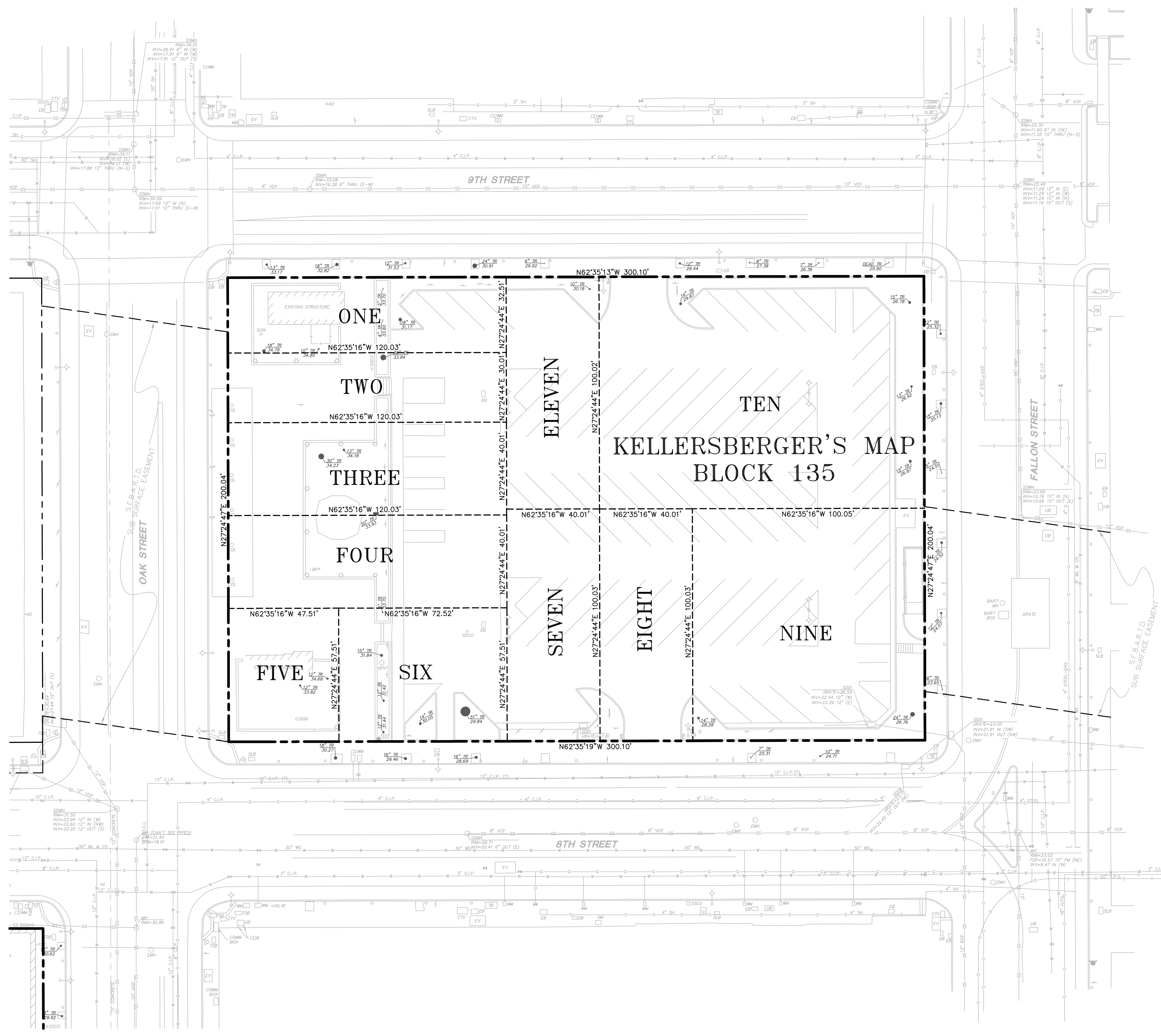


255 SHORELINE DR.,  
SUITE 200  
REDWOOD CITY, CA 94065  
(650) 482-6300  
www.bkf.com

Revisions	No.	Date	Scale	AS NOTED
		03/19/2021		
			Design	DFS
			Drawn	DFS
			Approved SRN	
			Job No	20190110
Drawing Number: <b>C1.0</b>				
1 OF 9				



DRAWING NAME: \\BKF-r-c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C2.1-C2.2\_ExistingParcelization.dwg  
PLOT DATE: 03-17-21



**ANNOTATION & LEGEND**

- AD = AREA DRAIN
- B = BOLLARD
- BFP = BACKFLOW PREVENTOR
- BR = BIKE RACK
- BW = BACK OF WALK
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- ⊙ = SIGN
- ⊙ = STREET LIGHT
- ⊙ = GAS VALVE
- ⊙ = WATER VALVE
- ⊙ = SIGNAL LIGHT
- = ELECTRICAL LINE
- = GAS LINE
- = STORM DRAIN LINE
- = SANITARY SEWER LINE
- = UNKNOWN UTILITY LINE
- = DOMESTIC WATER LINE
- = BOUNDARY LINE
- = RECORD LOT LINE TO BE REMOVED
- = EASEMENT LINE

**NOTES**

FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

**HORIZONTAL CONTROL**

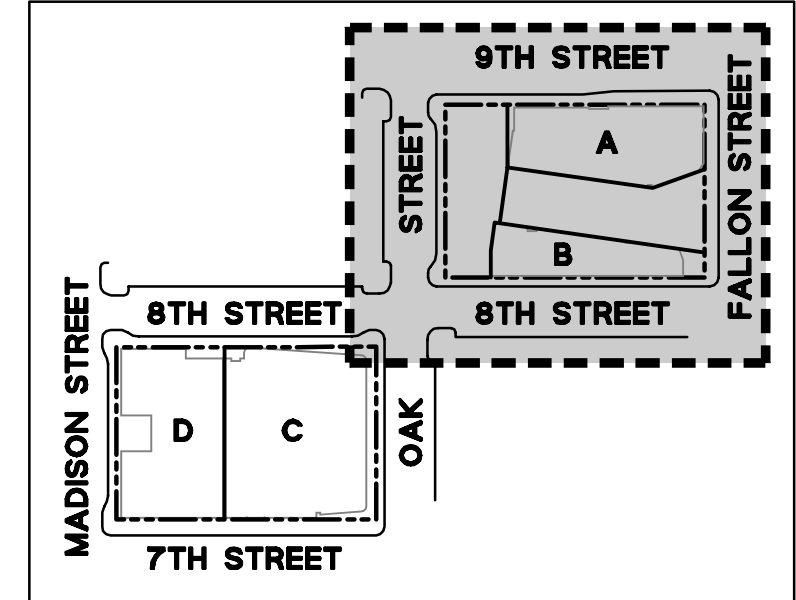
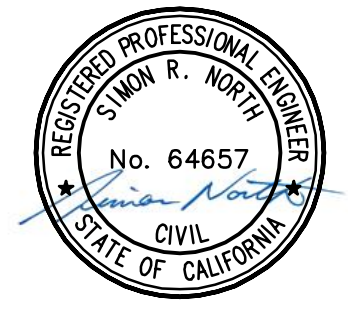
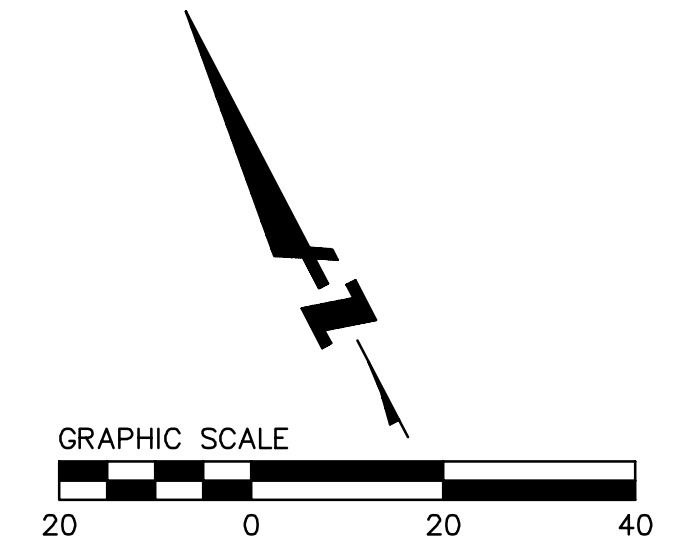
HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS83), EPOCH 2017.00

**BENCHMARK**

FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET.

ELEVATION = 23.062 (NAVD88)

**ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.**



**LAKE MERRITT BART DEVELOPMENT (BLOCK 1)**  
**VESTING TENTATIVE TRACT MAP NO. 8560**  
**EXISTING PARCELIZATION (BLOCK 1)**

**BKF100**  
YEARS  
ENGINEERS . SURVEYORS . PLANNERS

ALAMEDA COUNTY  
CITY OF OAKLAND  
255 SHORELINE DR.,  
REDWOOD CITY, CA 94065  
(650) 482-6300  
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Revisions	Date	By	Checked	Approved
No.	02/22/2021			
Scale	1" = 20'			
Design	DFS			
Drawn	DFS			
Approved SRN				
Job No	20190110			

Drawing Number: **C2.1**

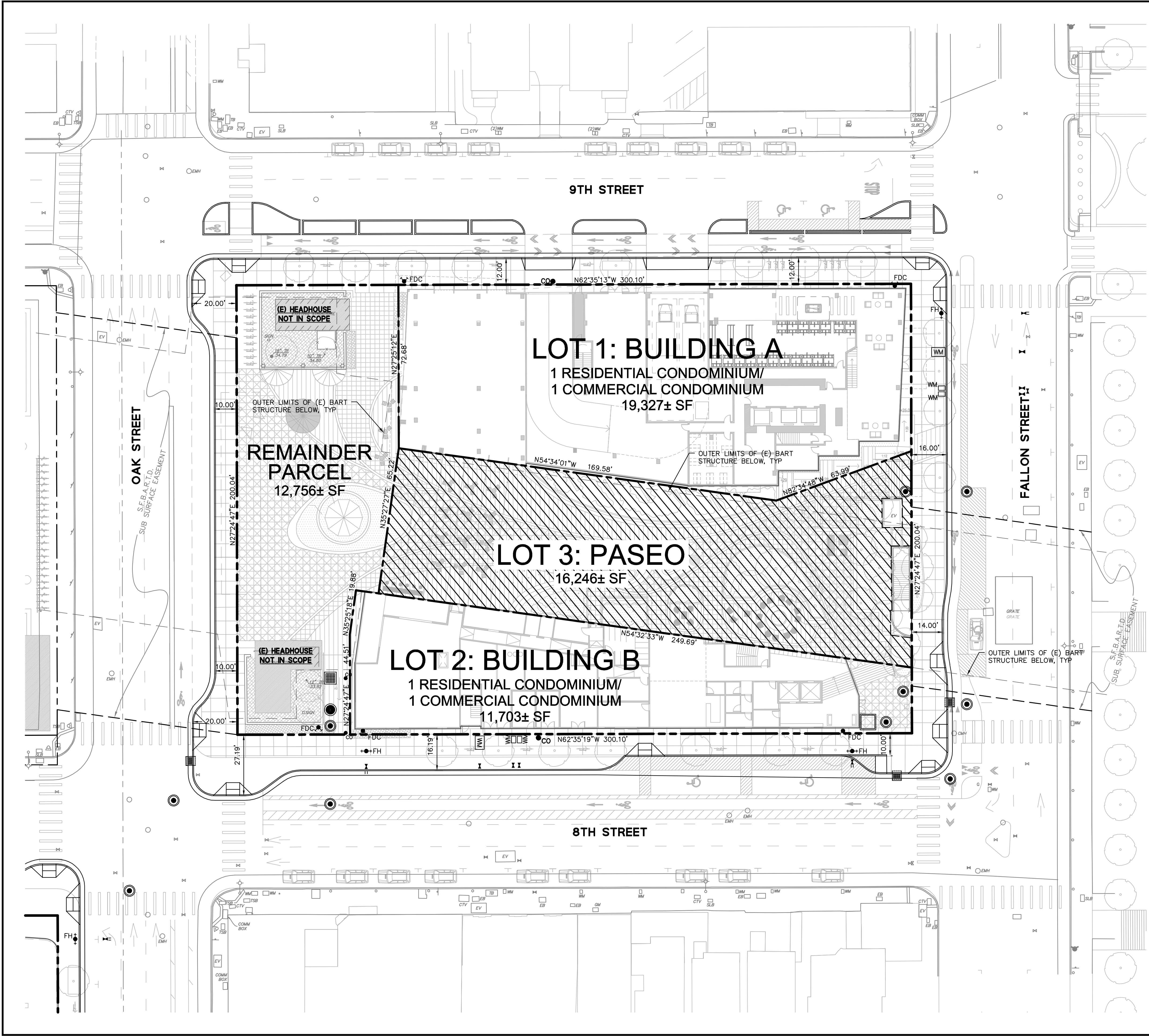
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DRAWING NAME: \\BKF-r\c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C3.1-C3.2\_ProposedParcelization.dwg  
PLOT DATE: 03-17-21  
PLOTTED BY: suid



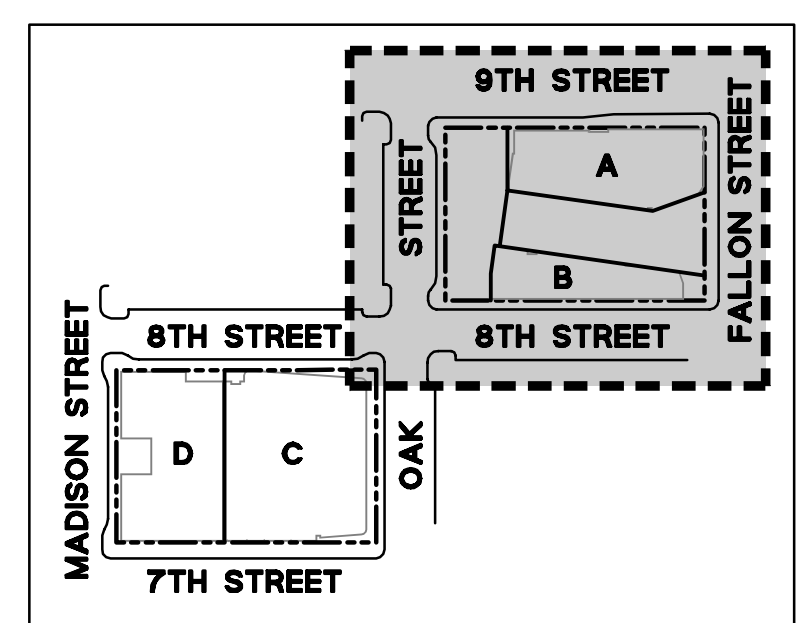
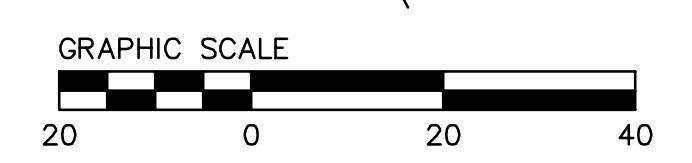
**SITE PLAN NOTES:**

- ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
- SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
- THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
- ONSITE PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS. PAVING WITHIN THE PUBLISH RIGHT OF WAY SHALL BE PER THE LATEST STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION ("GREENBOOK") AND CITY OF OAKLAND STANDARDS.
- COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
- SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
- FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.



**LAKE MERRITT BART DEVELOPMENT (BLOCK 1)**  
**VESTING TENTATIVE TRACT MAP NO. 8560**  
**PROPOSED PARCELIZATION PLAN (BLOCK 1)**

CITY OF OAKLAND  
ALAMEDA COUNTY  
CALIFORNIA

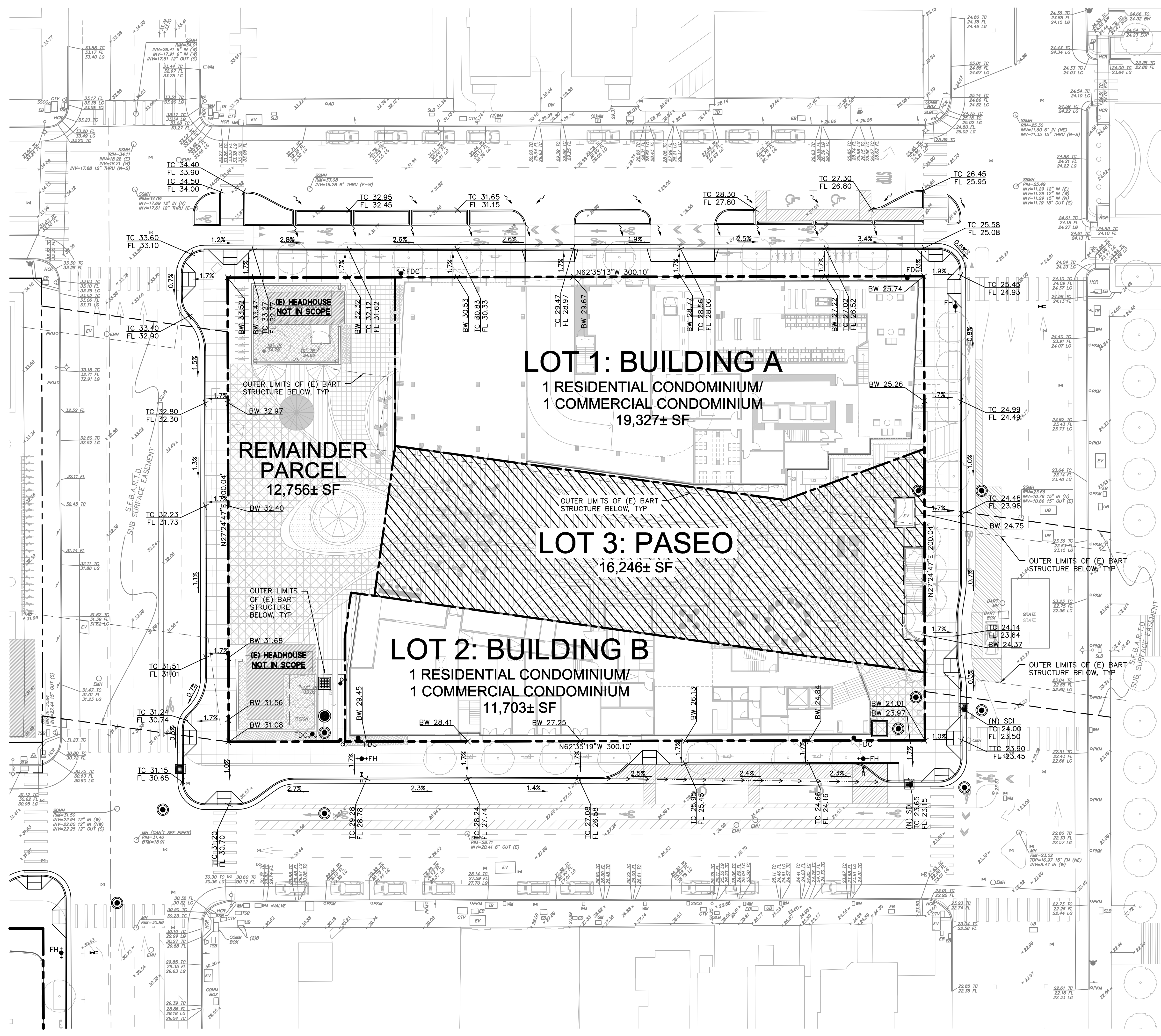
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Drawing Number:	C3.1		
Job No	20190110		







DRAWING NAME: \\BKF-r-c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C4\_1-C4\_2\_GradingPlan.dwg  
PLOT DATE: 03-17-21  
PLOTTED BY: suid



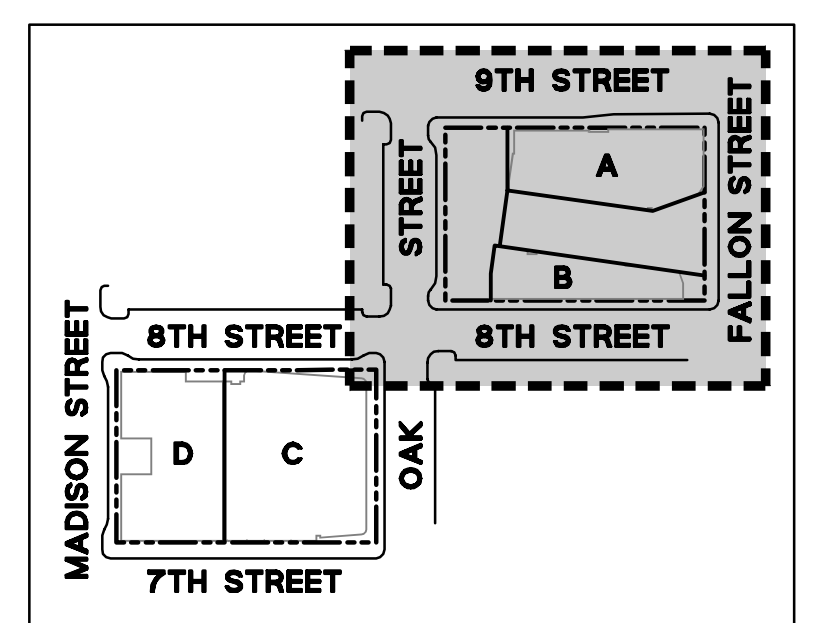
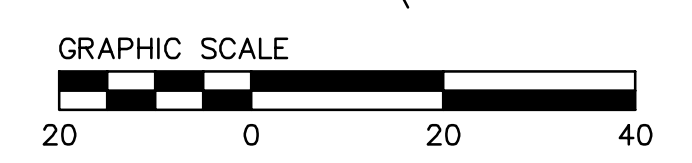
**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05', HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

**LEGEND:**

- BOUNDARY LINE
- - - LOT LINE
- - - EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.



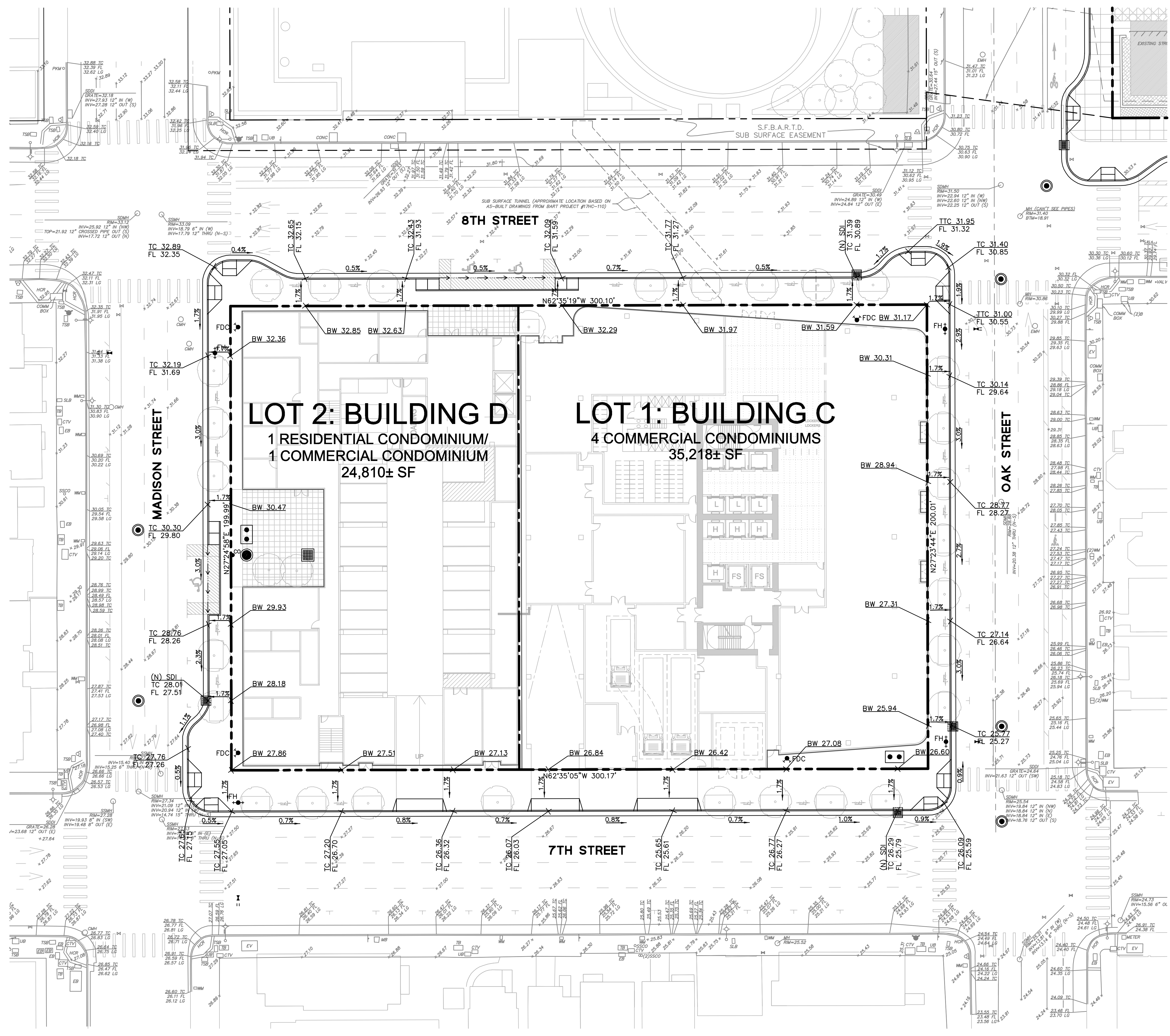
**LAKE MERRITT BART DEVELOPMENT (BLOCK 1)**  
**VESTING TENTATIVE TRACT MAP NO. 8560**  
**PROPOSED GRADING PLAN (BLOCK 1)**  
 CALIFORNIA  
 ALAMEDA COUNTY  
 CITY OF OAKLAND

**BKF100**  
**YEARS**  
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Date	02/22/2021
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Job No	20190110
Drawing Number:	<b>C4.1</b>
Revisions	



DRAWING NAME: \\BKF-r-c\o\2019\190110.BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C4.1-C4.2\_GradingPlan.dwg  
PLOT DATE: 03-17-21  
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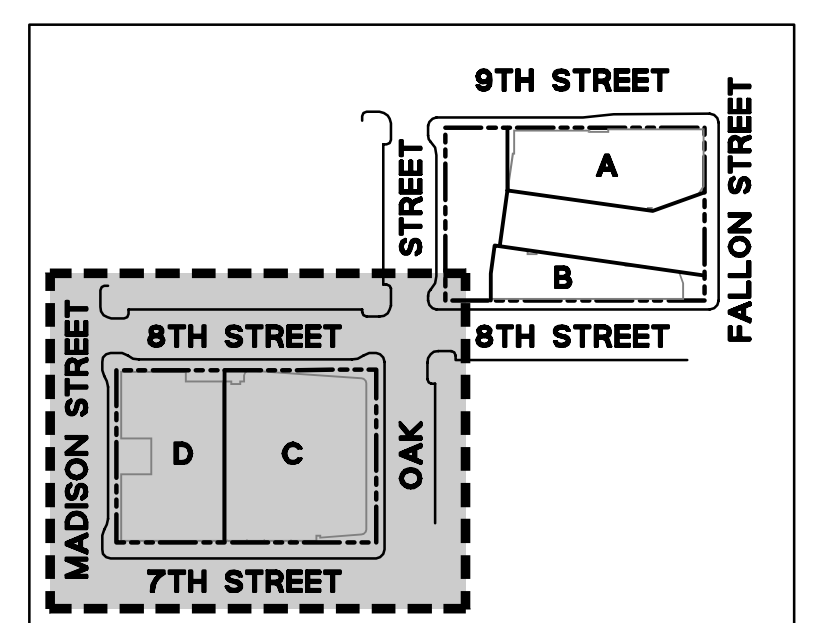
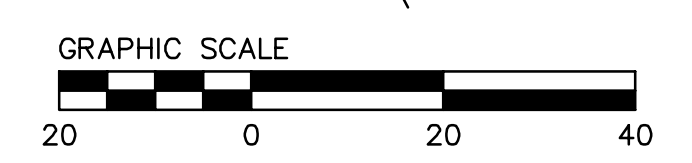
**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05'. HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.



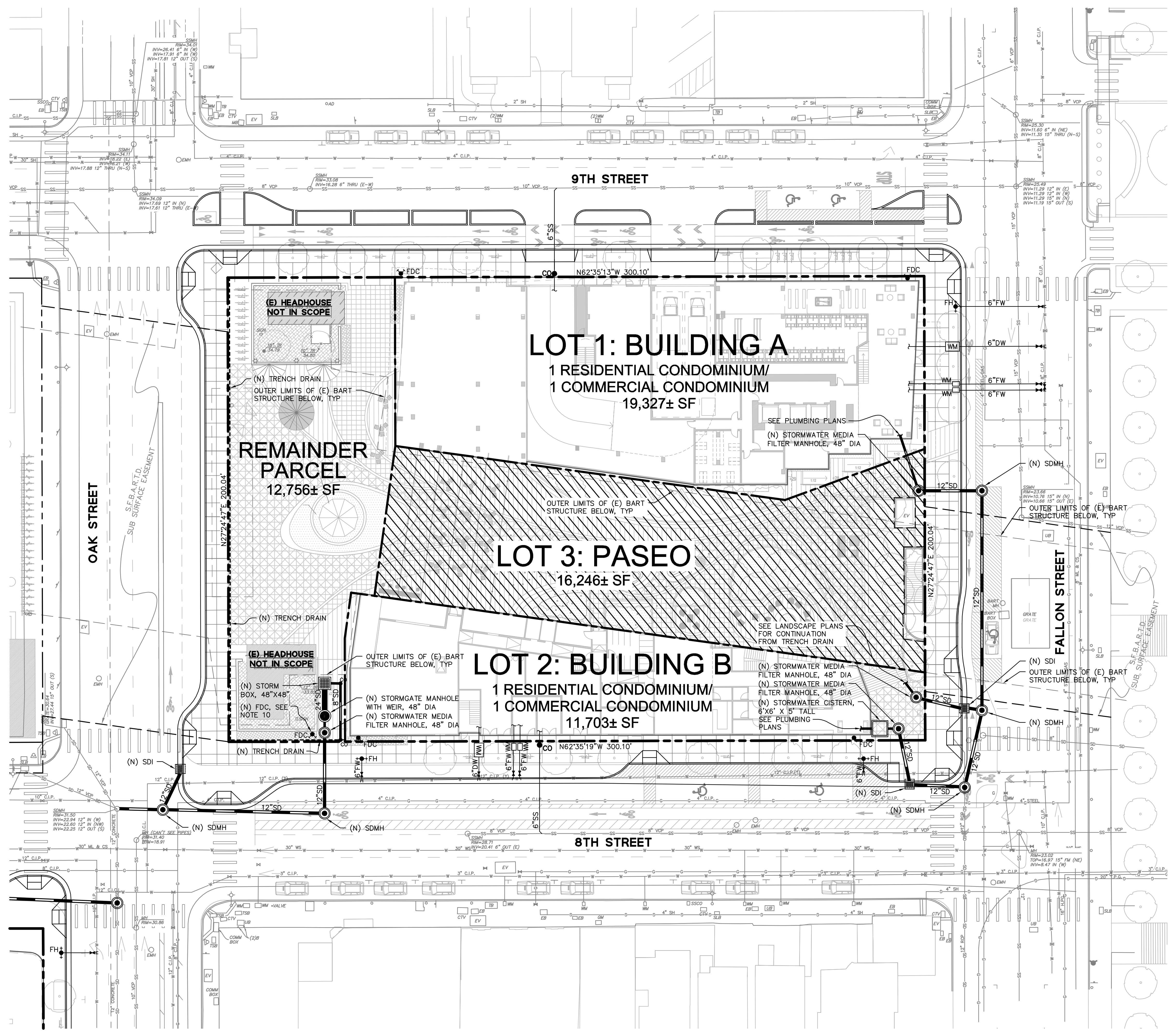
**LAKE MERRITT BART DEVELOPMENT (BLOCK 2)**  
**VESTING TENTATIVE TRACT MAP NO. 8577**  
**PROPOSED GRADING PLAN (BLOCK 2)**  
 CALIFORNIA  
 ALAMEDA COUNTY  
 CITY OF OAKLAND

**BKF100**  
**YEARS**  
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Date	02/22/2021	No.	
Scale	1" = 20'	Design	DFS
Drawn	DFS	Approved SRN	
Drawing Number:	<b>C4.2</b>		
	7 OF 9		



DRAWING NAME: \\BKF-r\c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C5.1-CS.2\_UtilityPlan.dwg  
 PLOT DATE: 03-17-21  
 PLOTTED BY: suid



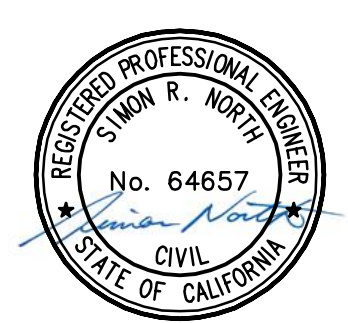
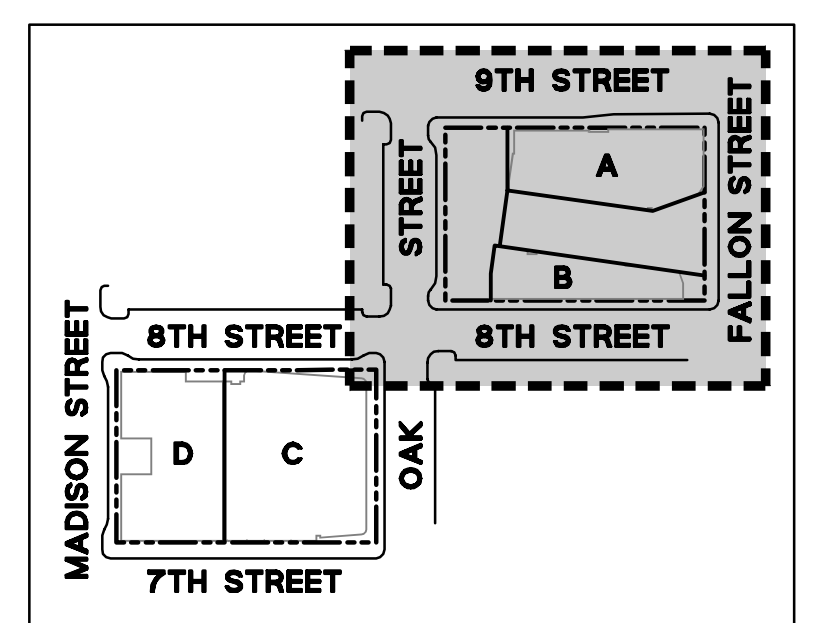
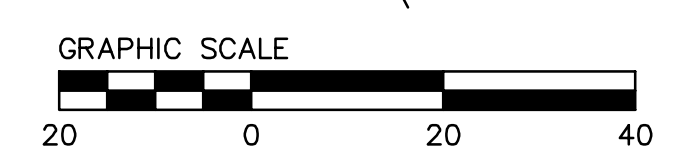
**UTILITY NOTES:**

1. INFORMATION REGARDING EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM RECORD DATA AND IS NOT MEANT TO BE A FULL CATALOG OF EXISTING CONDITIONS. CONTRACTOR SHALL CONDUCT FIELD INVESTIGATIONS, SUCH AS POT-HOLING, AS REQUIRED TO VERIFY THE LOCATIONS, ELEVATIONS, AND CONNECTION POINTS OF ALL EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES, WHETHER SHOWN ON THESE PLANS OR NOT, PRIOR TO THE COMMENCEMENT OF WORK AND UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
2. DOMESTIC WATER, FIRE WATER, AND SANITARY SEWER LATERAL SIZES TO BE CONFIRMED DURING THE DETAILED DESIGN PHASE.
3. CONTRACTOR SHALL COORDINATE GRAVITY UTILITY WORK WITH ALL JOINT TRENCH/RULE 20 UNDERGROUND WORK. IF JOINT TRENCH SCOPE OF WORK IS TO BE PERFORMED FIRST, CONTRACTOR SHALL STAKE LOCATIONS AND ELEVATIONS OF ALL PROPOSED GRAVITY UTILITY CROSSINGS. JOINT TRENCH TO BE INSTALLED WITH MINIMUM 12" VERTICAL CLEARANCE TO PROPOSED GRAVITY UTILITY AT ALL CROSSINGS.
4. ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
5. ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
6. ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
7. FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POT-HOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR GROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
8. DRAINS SHOWN ON CIVIL PLANS ARE NOT INTENDED TO BE THE FINAL NUMBER AND LOCATION OF ALL DRAINS. PLACEMENT AND NUMBER OF LANDSCAPING DRAINS ARE HIGHLY DEPENDENT ON GROUND COVER TYPE AND PLANT MATERIAL. CONTRACTOR SHALL ADD ADDITIONAL AREA DRAINS AS NEEDED AND AS DIRECTED BY THE LANDSCAPE ARCHITECT OR CIVIL ENGINEER.
9. THE LOCATION OF THE 12" WATER LINE IN 8TH STREET HAS BEEN SHOWN BASED ON RECORD INFORMATION PROVIDED BY EAST BAY MUD. EXACT LOCATION TO BE VERIFIED AND CONFIRMED PRIOR TO INSTALLATION OF THE PROPOSED DOMESTIC AND FIRE WATER SERVICES TO BUILDING B.
10. THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.

**LEGEND:**

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- - - LOT LINE
- . - . EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

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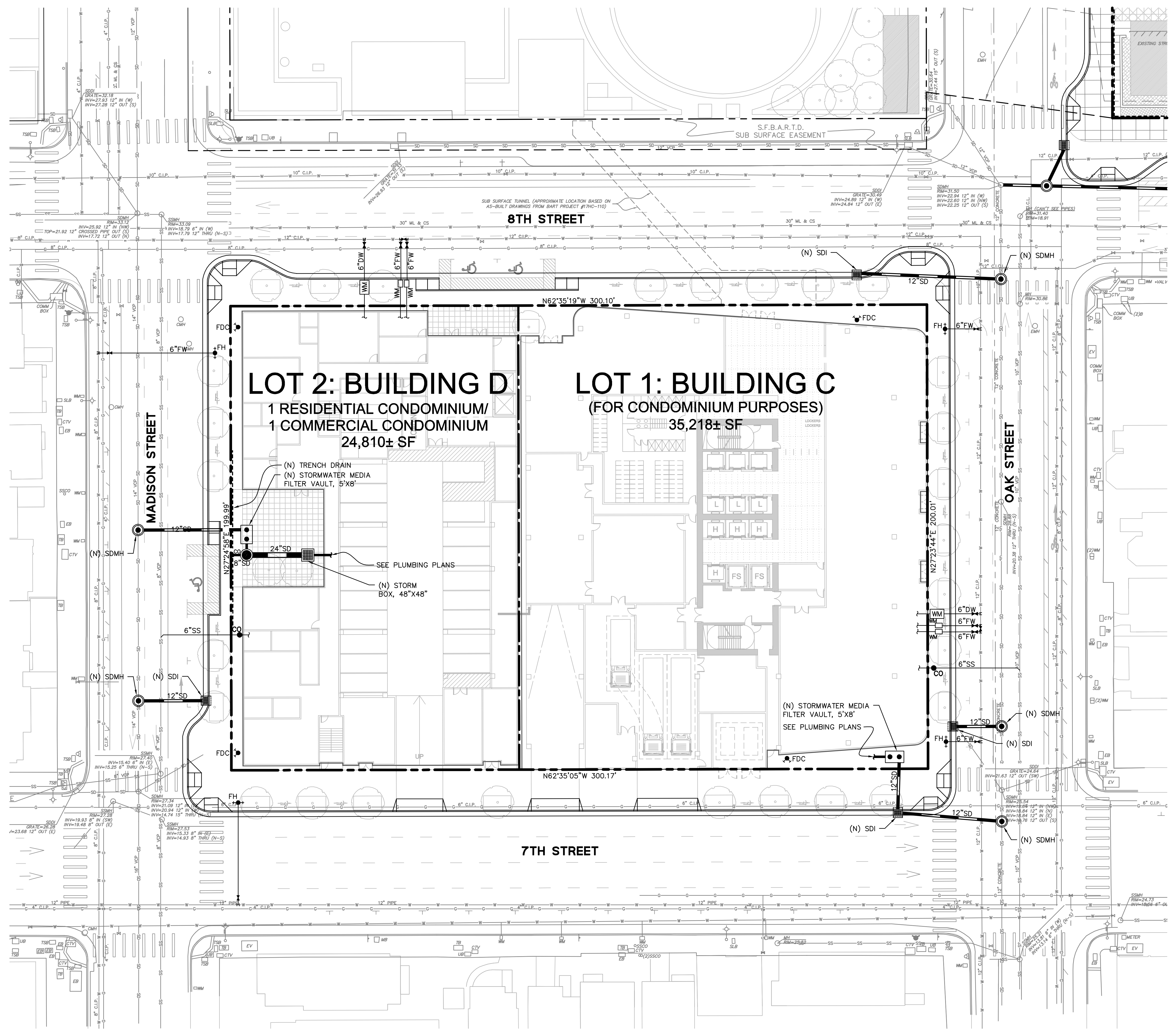
**LAKE MERRITT BART DEVELOPMENT (BLOCK 1)**  
**VESTING TENTATIVE TRACT MAP NO. 8560**  
**PROPOSED UTILITY PLAN (BLOCK 1)**  
 ALAMEDA COUNTY  
 CITY OF OAKLAND  
 CALIFORNIA

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**YEARS**  
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Revisions	No.	Date	02/22/2021
Scale	1" = 20'		
Design	DFS		
Drawn	DFS		
Approved SRN			
Job No	20190110		
Drawing Number:	<b>C5.1</b>		
	8 OF 9		



DRAWING NAME: \\BKF-rvc\o\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C5.1-C5.2\_UtilityPlan.dwg PLOT DATE: 03-17-21



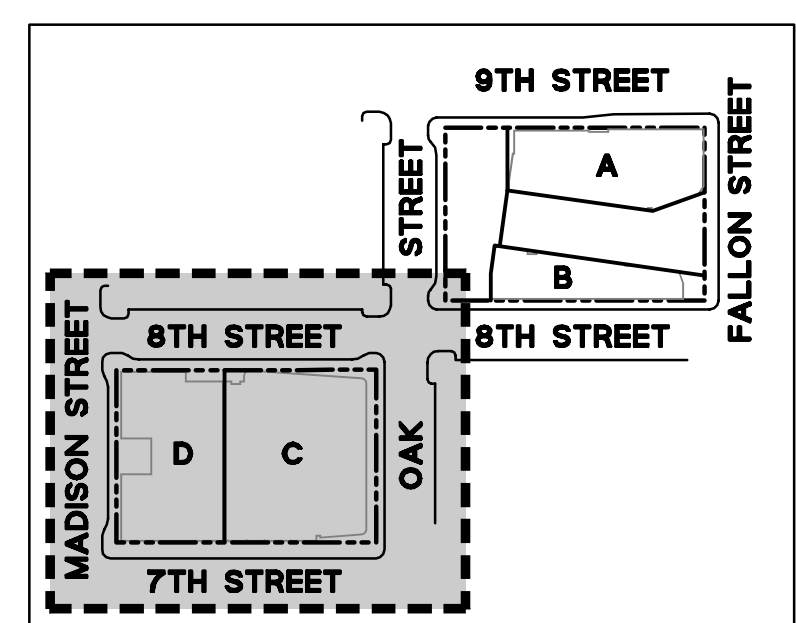
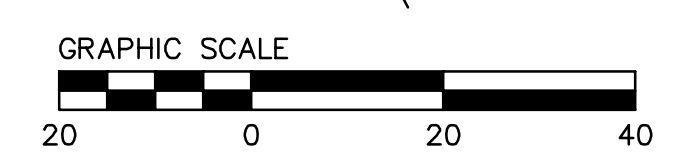
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**LAKE MERRITT BART DEVELOPMENT (BLOCK 2)**  
**VESTING TENTATIVE TRACT MAP NO. 8577**  
**PROPOSED UTILITY PLAN (BLOCK 2)**  
 CITY OF OAKLAND  
 ALAMEDA COUNTY  
 CALIFORNIA

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Date	02/22/2021	No.	
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Drawing Number:	<b>C5.2</b>		
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