

**Case File Number:** PLN18490-REV02, PLN18490-REV01-PUDF01, PLN18490-REV01-PUDF02, PLN18490-REV01-PUDF03

**August 12, 2020**

<b>Location:</b>	<b>West Oakland BART 1451 7<sup>th</sup> St.</b>
<b>Assessor's Parcel Number(s):</b>	<b>APN 004 007700300, 004 007100300</b>
<b>Proposal:</b>	Revision to Preliminary Development Plan (PDP), including changes to residential unit allocation, reduction in office, parking, open space, and removal of under-track retail. Updated State Density Bonus request with additional waivers/concessions. Vesting Tentative Parcel Map with 3 parcels and a designated remainder. Final Development Plan for Horizontal Improvements, including final design for sidewalks, bike lanes, and plazas. Final Development Plan for development of parcel T3, an affordable housing project with 240 affordable residential units, 15,957 sf of retail, and 50 parking spaces. Final Development Plan for development of parcel T1, a market rate residential tower with 522 residential units, 14,350 sf of retail, and 137 parking spaces.
<b>Applicant:</b>	China Harbour Engineering (CHEC)
<b>Contact Person/ Phone Number:</b>	Ronnie Turner (510) 395-27664
<b>Owner:</b>	San Francisco Bay Area Rapid Transit District (BART)
<b>Case File Number:</b>	PLN18490-REV02, PLN18490-REV01-PUDF01, PLN18490-REV01-PUDF02, PLN18490-REV01-PUDF03
<b>Planning Permits Required:</b>	PDP Revision, Variance for commercial loading, Three FDPs, Design Review, Vesting Tentative Parcel Map, compliance with CEQA
<b>General Plan:</b>	Community Commercial
<b>Zoning:</b>	Transit-Oriented Development Commercial Zone (S-15W)
<b>Environmental Determination:</b>	TBD
<b>Historic Status:</b>	Non-Historic Property
<b>City Council District:</b>	CCD1
<b>Finality of Decision:</b>	NA
<b>For Further Information:</b>	Contact case planner <b>Dara O'Byrne</b> at <b>510-238-6983</b> or by e-mail at <a href="mailto:dobyrne@oaklandca.gov">dobyrne@oaklandca.gov</a>

## 1. SUMMARY

The proposed project is a revision to a previously approved Preliminary Development Plan (PDP) for a Planned Unit Development at the West Oakland BART station (1451 7<sup>th</sup> St.). The PDP was initially entitled on February 6, 2019. The proposed revision includes the following changes to the entitlement:

- Changes the single, shared 400-car parking garage to three separate garages with separate entrances, adding driveways on Mandela Parkway with a total of 397 parking spaces.
- Requests additional concessions/waivers for parking, open space, and additional stories, specifically:
  - Reduction of overall residential usable open space from 77,123 square feet to 35,149 square feet
  - Reduction of overall parking spaces from 400 spaces to 397 spaces, with individual parking garages.
  - Increase in number of stories on T1 from 30 to 31.
- Changes to each building include the following:

- T1:
  - Eliminates the 82,460 square feet of office;
  - Moves 22 market rate units from T3 to T1;
  - Reduces retail from 17,185 square feet to 14,350 square feet; and
  - Increases the number of stories from 30 to 31.
- T2: Eliminates retail located under the BART tracks
- T3: Reduces residential units from 262 to 240 units
- T4: Reduces the retail from 30,800 square feet to 23,184 square feet.

The overall project consists of 762 residential units (of which 240 are affordable units), 300,000 square feet of office space, 53,491 square feet of retail, and 397 parking spaces. The project includes plazas and public right of way improvements.

The Final Development Plan for development area T1 (PLN18490-REV01-PUDF01) at the corner of 7<sup>th</sup> St and Mandela Parkway includes 522 residential market-rate units with 14,350 square feet of retail and 137 parking spaces in a 320 ft tall tower.

The Final Development Plan for Horizontal Improvements (PLN18490-REV01-PUDF02) includes final designs for public plazas, sidewalks, bike lanes, and associated phasing of the improvements.

The Final Development Plan for development area T3 (PLN18490-REV01-PUDF03) at 5<sup>th</sup> St. and Chester St. includes 240 affordable residential units (79 units at 30% AMI, 11 units at 50% AMI, 148 units at 60% AMI, and 2 units at 120% AMI) 15,957 square feet of retail, and 50 parking spaces in an 80 foot tall mid-rise building.

The project also includes a revision to the Vesting Tentative Parcel Map for 3 new parcels and a Designated Remainder.

## **2. PROJECT SITE AND SURROUNDING AREA**

The West Oakland BART TOD site encompasses 5.58-acres and is bounded by 7th Street to the north, 5<sup>th</sup> Street to the south, Chester Street to the west, and Mandela Parkway to the east. The project site consists of two parcels at 1451 7<sup>th</sup> Street (Assessor's Parcel Number: 004-007-700-300 and 004-007-100-300).

## **3. PROJECT BACKGROUND**

### ***3.1 Project History***

The proposed project is located at the site of the West Oakland BART station, which currently includes BART parking and a BART station. The San Francisco Bay Area Rapid Transit District (BART) owns the land to be developed and will have a long term lease with the proposed developer. The BART station will remain, but the BART parking and vehicular access will be removed from the site.

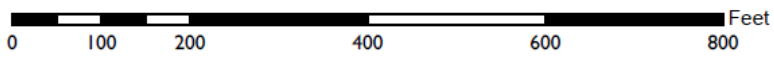
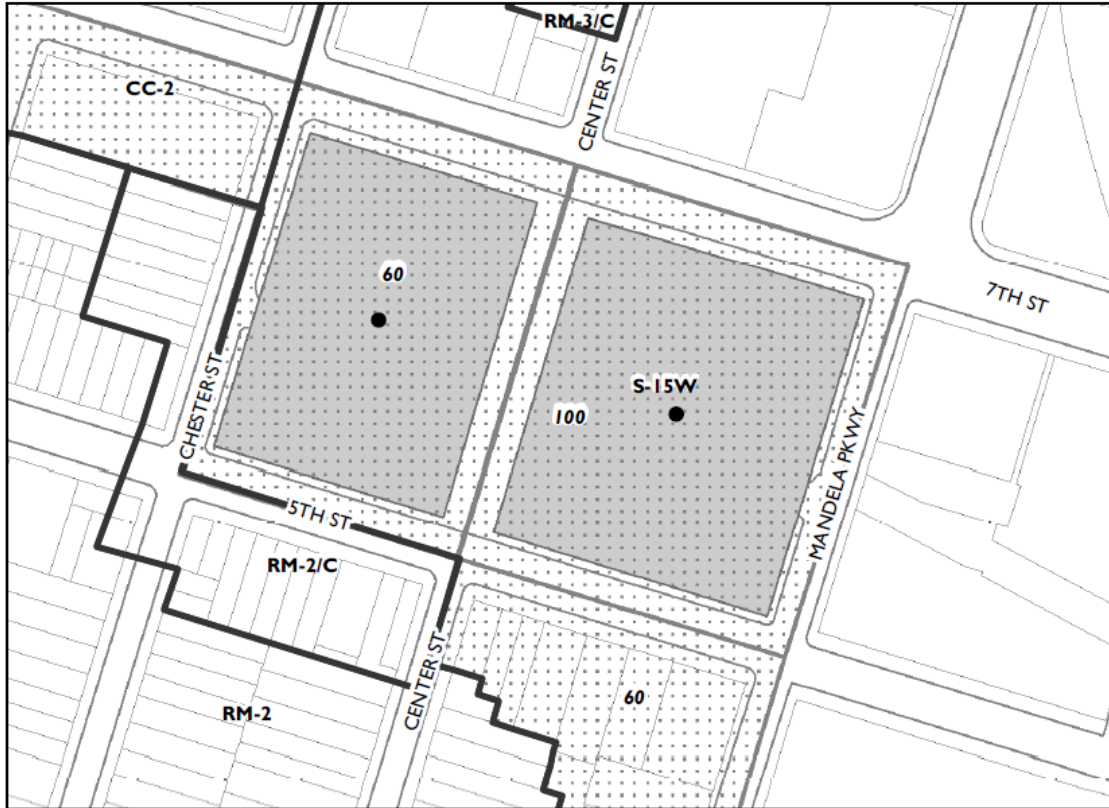
The project applicant first submitted a pre-application to the City of Oakland in October 2017 to receive initial feedback on preliminary site layout and design. The Preliminary Development

Plan for a Planned Unit Development was submitted in November 2018. The project went through an expedited review process in order to compete for an Affordable Housing Sustainable Communities (AHSC) funding award and was approved in February 2019.

In June 2019, a revision to the Preliminary Development Plan was submitted with three Final Development Plans. In December 2019, a separate revision to the Preliminary Development Plan was submitted.

The project applicant has been working with the community, through an established Community Advisory Council, to receive input on the proposed project.

# CITY OF OAKLAND PLANNING COMMISSION



Case File: PLN18490-REV02, PLN18490-REV01-PUDF01,  
PLN18490-REV01-PUDF02, PLN18490-REV01-PUDF03  
Applicant: China Harbour Engineering (CHEC)  
Address: West Oakland BART | 451 7th St  
Zone: S-15W  
Height Area: 60 ft, 100 ft

### ***3.2 Previous Public Hearings***

The previously approved PDP bypassed the Design Review Committee due to expedited review. During the February 2019 Planning Commission Hearing, Commissioners added two conditions of approval to the project based on commitments the applicant made during the hearing:

- **Condition of Approval 74: Below market rate retail.** If feasible, a minimum of 20% of the total retail square footage or number of tenants shall be provided below market rate.
- **Condition of Approval 75: Level of affordable housing provided.** Provide one third (1/3) of all affordable residential units below 30% Area Median Income (AMI)

## **4. PROJECT DESCRIPTION**

The proposed West Oakland BART TOD project is a revision to a previously approved preliminary development plan for a mixed-use project surrounding the West Oakland BART station. The project is located at the approximately 5.58- acre site encompassing the West Oakland BART station bounded by 7th Street to the north, 5th Street to the south, Chester Street to the west, and Mandela Parkway to the east. The project site consists of two parcels at 1451-7th Street (Assessor's Parcel Number: 004-007-700-300 and 004-007-100-300). The proposal also includes three Final Development Plans associated with the PDP revision.

### ***4.1 Existing Conditions and Surrounding Land Uses***

The project site is a rectangular lot occupied by the West Oakland BART station and associated surface parking and circulation. Vegetation onsite is currently limited to some street and parking lot landscaping and trees. Existing land uses in the vicinity include multi- story commercial and residential development to the north, parking/fuel station/vacant lot to the east, light industrial and low- rise residential to the south, and low- rise residential to the west.

### ***4.2 Project Overview for PDP Revision***

The Project is a multi-phase Planned Unit Development that will include 3 parcels and a designated remainder:

- T1 (Lot 1): 522 market-rate residential units, 14,350 square feet of retail, and 137 parking spaces in a 320 foot tall tower
- T3 (Lot 3): 240 affordable residential units, 15,957 square feet of retail, and 50 parking spaces in an 80 foot tall building
- T4 (Lot 2): 300,000 square feet of office space, 23,184 square feet of retail, and 210 parking spaces in a 100 foot tall building.
- Designated Remainder: Includes the West Oakland BART station, BART tracks, a bike station, public plazas, and Emergency Vehicle Access.

The entire project includes 762 residential units, 300,000 square feet of office, 53,491 square feet of retail, and 397 parking spaces. The proposed revision includes the following changes to the entitlement:

- Changes the single, shared 400-car parking garage to three separate garages with separate entrances, adding driveways on Mandela Parkway with a total of 397 parking spaces.
- Requests additional concessions/waivers for parking, open space, and additional stories, specifically:

- Reduction of overall residential usable open space from 77,123 square feet to 35,149 square feet
- Reduction of overall parking spaces from 400 spaces to 397 spaces, with individual parking garages.
- Increase in the number of stories on T1 from 30 to 31.
- Changes to each building include the following:
  - T1:
    - Eliminates the 82,460 square feet of office;
    - Moves 22 market rate units from T3 to T1;
    - Reduces retail from 17,185 square feet to 14,350 square feet; and
    - Increases the number of stories from 30 to 31.
  - T2: Eliminates retail located under the BART tracks
  - T3: Reduces residential units from 262 to 240 units
  - T4: Reduces the retail from 30,800 square feet to 23,184 square feet.

The associated project specific Design Guidelines remain unchanged. Plans, elevations, design guidelines, and illustratives for the revision to the Preliminary Development Plan are provided in Attachment A to this report.

#### ***4.4 Project Overview for T1***

The Final Development Plan for the T1 building includes 522 market-rate residential units, 14,350 square feet of retail, and 137 parking spaces in a 320-foot tall tower located at 7<sup>th</sup> St. and Mandela Parkway. This is a prominent corner in West Oakland and is the project's primary frontage on 7<sup>th</sup> St. Retail faces 7<sup>th</sup> Street on the ground floor with retail wrapping around to Mandela Parkway. The parking garage access, loading, and utility access are ganged on Mandela Parkway.

The breakdown by residential unit type includes:

- 80 3-bedroom units;
- 96 2-bedroom units;
- 228 1 bedroom units; and
- 118 studio units.

The following requested waivers/concessions apply to this project: maximum height at the street frontage, parking, open space, building height, and number of stories. Whether a current State Affordable Housing Bonus request will be deemed a waiver or concession is still under consideration by staff in conjunction with the City Attorney's Office.

#### ***4.5 Project Overview for Horizontal Improvements***

The Final Development Plan for Horizontal Improvements includes final designs for plazas and streetscape improvements. The project includes a plaza at 7<sup>th</sup> St. and Chester St. (Mandela Plaza), an undertrack plaza off of Chester St., and an Art Alley that runs along the south side of the BART tracks from Chester St. to Mandela Pkwy. A plaza is also provided at Center and 5<sup>th</sup> St. (Center Square). The following streetscape improvements are included in the FDP:

- 7<sup>th</sup> Street Improvements:
  - Raised Class IV one-way separated bikeways on both sides of 7<sup>th</sup> St. between Chester St. and Mandela Pkwy.;

- Minimum 8 ft pedestrian through zone on the sidewalk between Chester St. and Mandela Pkwy.;
- Approximately 270-foot extended bus stop on eastbound 7<sup>th</sup> St. at Mandela Pkwy.
- Approximately 130-foot bus stop on westbound 7<sup>th</sup> St. just west of Center St.
- Approximately 250-feet of linear curb designated for passenger loading and unloading on eastbound 7<sup>th</sup> St. between Chester St. and Center St., with about 50 feet of curb on eastbound 7<sup>th</sup> St. just west of Center St. designated as blue accessible loading zone.
- Blues Walk of Fame Plaques relocated to a new position on the sidewalk.
- Existing Dancing Lights removed to accommodate the new streetscape with bikelane and extended sidewalk. OakDOT to review and approve permit for removal of Dancing Lights.
- Mandela Parkway Improvements
  - Class IV one-way separated bikeways on both sides of Mandela Pkwy. between 7<sup>th</sup> St. and 5<sup>th</sup> St.
  - Minimum 8 ft pedestrian through zone on the sidewalk between 7<sup>th</sup> St. and 5<sup>th</sup> St.
  - Parking removed on the west side of Mandela Parkway between 5<sup>th</sup> St. and 7<sup>th</sup> St.
- 5<sup>th</sup> Street Improvements
  - Minimum 8 ft pedestrian through zone on the sidewalk between Chester Street and Mandela Parkway
  - Approximately 170-foot long bus stop and layover zone with a concrete bus pad on 5th Street just west of Mandela Pkwy.
  - Approximately 100-feet of linear curb designated for passenger loading and unloading east of Center St. and about 200 feet west of Center St.
- Chester Street Improvements
  - Minimum 8 ft pedestrian through zone on the sidewalk between 7<sup>th</sup> St. and 5<sup>th</sup> St.
  - Centerline redesigned to facilitate northbound bus turning movements.
  - Prohibit parking on the east side of Chester St. between 5<sup>th</sup> St. and 7<sup>th</sup> St. and on the west side of Chester St. for about 100 feet south of 7<sup>th</sup> St.

#### Intersection Improvements

- 5<sup>th</sup> St. and Center St.
  - All-way stop control
  - High-visibility crosswalks and directional ramps
  - Curb extensions
- 5<sup>th</sup> St. and Chester St.
  - High-visibility crosswalks and directional ramps
- 5<sup>th</sup> St. and Mandela Pkwy.
  - High-visibility crosswalks and directional ramps
  - Curb extensions at the NE, SE, and SW corners
- 7<sup>th</sup> St. and Center St.
  - Pedestrian scramble signal phasing
  - Protected intersection with designated pedestrian and bicycle crossing
- 7<sup>th</sup> St. and Chester St.
  - Curb extensions at the NW, NE, and SW corners
- Mandela Pkwy. and 7<sup>th</sup> St.

- Dedicated bicycle signal phase
- Signal timing for bus-only phase
- Mandela Pkwy. between 5<sup>th</sup> St. and 7<sup>th</sup> St.
  - High-visibility, mid-block pedestrian crossing

The bike station on the east side of the existing BART station will be reviewed and approved by BART, under their jurisdiction. It is proposed to accommodate at least 300 bicycles.

The horizontal improvements do impact previously implemented streetscape improvements, including the Blues Walk of Fame and the 7<sup>th</sup> Street Streetscape Improvements Dancing Lights, as discussed below:

#### 4.5.1 Blues Walk of Fame

“The Music They Played on 7th Street Oakland” Walk of Fame lines the Seventh Street sidewalk in front of the West Oakland BART station and features 88 plaques of honorees, including Aretha Franklin, Ike and Tina Turner, the Pointer Sisters, B.B. King, John Lee Hooker and Ray Charles. The Bay Area Blues Society implemented the Blues Walk of Fame, with funding and support from the City of Oakland, Bay Area Rapid Transit, and private donations.

The FDP for Horizontal Improvements proposes the removal of the plaques during construction and placing the plaques back in the sidewalk along 7<sup>th</sup> St. when the final sidewalks are constructed.

#### 4.5.2 Seventh Street Concept and Urban Design Plan

In 2004, a California Department of Transportation (Caltrans) grant for Environmental Justice funded a streetscape design concept for 7<sup>th</sup> St., which included a streetscape design and urban design elements that were implemented by the City of Oakland. The purpose of the plan was to:

- Reduce the adverse impacts of transportation on the immediate Seventh Street project site.
- Recommend amenities conducive to future transit oriented development and activity with a strong civic character that creates a “place” identity

The implemented improvements included a landscaped median, sidewalk and intersection improvements, as well as a number of urban design elements. The urban design elements included Dancing Lights, which were placed in front of the BART station between Chester St. and Mandela Parkway, a transit canopy structure at the BART station, and a Gateway Feature near Union St. The urban design concept contemplated the inclusion of the Blues Walk of Fame and associated plaques.

The FDP for Horizontal Improvements proposes removal of the Dancing Lights to accommodate a Class IV bike lane and widened sidewalk where the dancing lights are currently located. The matching transit canopy structure would also be removed to accommodate the redesigned bus stop and streetscape design. These proposed changes to the public Rights-of-Way will be overseen by OakDOT.



#### ***4.6 Project Overview for T3***

The Final Development Plan for T3 includes a mixed use building at the corner of 5<sup>th</sup> St. and Chester St. The proposed project includes 240 affordable residential units, at the following affordability levels:

- 79 units at 30% AMI;
- 11 units at 50% AMI;
- 148 units at 60% AMI; and
- 2 units at 120% AMI.

The breakdown by unit type includes:

- 14 3-bedroom units;
- 47 2-bedroom units; and
- 137 1-bedroom units.

The project also includes 15,957 square feet of retail fronting 5<sup>th</sup> St., Center Square, and the Art Alley. The project includes 50 parking spaces. Residential-oriented uses, including a residential lobby, are located along Chester St., and the building steps down from 85 feet to 38 ft tall along Chester St. to transition to the single-family scale of the Prescott Neighborhood. The project includes an art wall along the north elevations, facing the BART tracks.

The following requested waivers/concessions apply to this project: maximum height at the street frontage, parking, open space, and building height. As noted throughout this report, whether a current State Affordable Housing Bonus request will be deemed a waiver or concession is still under consideration by staff in conjunction with the City Attorney's Office.

## **5. GENERAL PLAN ANALYSIS**

The West Oakland BART TOD project site is located in the Community Commercial (CC) General Plan (GP) land use designation. The intent of the CC land use designation is to “identify, create, maintain, and enhance areas suitable for a wide variety of commercial and institutional operations along the City’s major corridors and in shopping districts or centers.” In terms of desired character, future development may include “neighborhood center uses and larger scale retail and commercial uses, such as auto related businesses, business and personal services, health services and medical uses, educational facilities, and entertainment uses. Community Commercial areas can be complemented by the addition of urban residential development and compatible mixed use development.” The maximum allowed intensity is 125 residential units per gross acre and the maximum FAR for this classification is 5.0. At this time, the total count of residential units considered under the Preliminary Development Permit is 762 units, within the General Plan allowance. The proposed commercial FAR is within the General Plan maximum FAR.

The West Oakland BART site is also designated as a Transit-oriented district, which are “designated to take advantage of the opportunities presented by Oakland’s eight BART stations...” “Easy pedestrian and transit access to mixed use development characterize these areas. A strong identity is to be created through careful design and mix of activity.” West Oakland BART is “uniquely situated as the first station linking San Francisco and Oakland, and the only station serving four BART routes.”

The following is an analysis of how the proposed project meets applicable General Plan objectives (staff analysis in indented, italicized text below each objective):

- Policy T2.1 Encouraging Transit-Oriented Development
  - *The proposed project is a transit-oriented development with a mix of office, residential, and ground floor, pedestrian-oriented retail.*
- Policy T2.3 Promoting Neighborhood Services
  - *The proposed project includes up to 55,000 square feet of neighborhood-serving commercial, including a proposed food market and other local retail.*
- Policy N1.1 Concentrating Commercial Development
  - *The proposed project concentrates neighborhood-oriented retail around the West Oakland BART station. There are many smaller scale retail locations that will serve residents and BART riders alike.*
- Policy N3.1 Facilitating Housing Construction
  - *The proposed project includes the creation of 762 new housing units, of which 240 are affordable units.*
- Policy N3.2 Encouraging Infill Development
  - *The proposed project converts an existing parking lot into a mixed-use infill development, consistent with the General Plan.*
- N6.1 Mixing Housing Types
  - *The proposed project includes a mix of housing types and unit sizes, which are available to households with a range of incomes. The project includes studios, 1-bedroom, 2-bedroom, and 3-bedroom types.*
- Policy N8.1 Developing Transit Villages
  - *The proposed project creates a Transit Village at the West Oakland BART station.*
- Policy N8.2 Making Compatible Interfaces Between Densities
  - *The proposed project includes a step-down to 38 feet along Chester Street, as the project transitions to the South Prescott neighborhood.*
- Policy N9.5 Marking Significant Sites
  - *The proposed project will work to incorporate public art and installations that identify locations of interest and historic significance, including the Blues Walk of Fame.*

### **5.1 West Oakland Specific Plan Analysis**

The proposed project is located in the West Oakland Specific Plan (WOSP) area. The WOSP provides specific land use scenarios and policies for West Oakland BART, as follows:

- Intent: Implement the City's long-term vision for a Transit-Oriented Development (TOD) project at the West Oakland BART station, in the area generally coinciding with the boundaries of the City's existing S-15 Transit Oriented Development Zone.
  - *The proposed project implements the vision for a TOD project at the West Oakland BART station.*
- 7th Street TOD Land Use-1: Select a site with immediate proximity to the West Oakland BART Station which can serve as the catalyst, first-phase development of the TOD.

- *The proposed project will serve as a catalyst for future development in West Oakland. The proposed project will be developed in phases.*
- Intent: Ensure a mix of land uses in the BART Station TOD to ensure the site becomes an integral and fully integrated component of West Oakland.
  - *The proposed project includes residential, office, and retail land uses. The project proposes neighborhood serving retail and flexible commercial spaces for small scale retail so it becomes an integral component in West Oakland.*
- 7th Street TOD Land Use-6: Plazas and open space should contribute to a secure and aesthetically pleasing pedestrian experience at and around the BART Station TOD.
  - *The proposed project includes a public plaza at 7<sup>th</sup> St. and Chester St. as well as pedestrian walkways and a 'Center Square' that provides public access to the BART station.*
- 7th Street TOD Env-2: The new buildings envisioned to surround the West Oakland BART station as part of the TOD project are expected to provide a noticeable and significant noise buffer between portions of both the freeway and the BART tracks, and existing residential neighborhoods. The noise attenuation benefits from the proposed new buildings should be fully considered in final designs for these structures.
  - *The conditions of approval ensure that this project will support noise attenuation for the surrounding neighborhood and not aggravate noise conditions for the community.*
- TOD-2: Create a high-density residential, commercial, and mixed-use development which might also include the artistic economic enterprises within the West Oakland district with creative working space for technology, innovative science activities drawing upon the desires of some in those industries for creative space with room for innovative production.
  - *The proposed project provides a high-density mixed-use development at the West Oakland BART site. Local retail spaces are proposed as part of the project, providing opportunities for creating working spaces.*
- TOD-3: Ensure a safe and pleasant pedestrian environment near the West Oakland BART Station.
  - *The proposed project provides safe and pleasant pedestrian connections both along the streets surrounding the project, but also through the site to connect to the BART station.*
- TOD-5: Provide amenities such as benches, kiosks, lighting, public art, high quality pavement materials, drought tolerant landscaping, and specialty uses such as outdoor cafes.
  - *The proposed project includes conceptual designs for improved streetscapes and public plazas with high quality pavement materials, pedestrian-scaled lighting, and pedestrian-oriented retail with outdoor seating throughout the site.*
- TOD-6: Limit conflicts between vehicles and pedestrians, and add urban infrastructure as demonstrations of the best of the new industry standards.
  - *The proposed project removes all vehicular access on the site (except emergency vehicles and service vehicles), creating pedestrian walkways that are separated from vehicular travel. The project also proposes improved pedestrian crossings at the surrounding intersections.*

- TOD-7: New residential and commercial development along the perimeter of the TOD site, nearest to the South Prescott neighborhood, should include a gradual transition in height and density to the surrounding lower-density residential neighborhoods, with building heights stepping down to as low as 2-stories immediately adjacent to existing homes
  - *The proposed project includes 38-ft tall residential activities along Chester Street adjacent to the South Prescott neighborhood, providing a transition to the residential units across the street.*

### Circulation

- Streetscape-4: Prioritize the development of local streetscape improvement plans, including lights, trees, bulb outs, sidewalks, etc., on: Mandela Parkway from 7th to 8th Street;
  - *The project team developed an Access Study for the proposed project, which includes recommendations for improvements to the intersection at 7<sup>th</sup> Street and Mandela Parkway.*
- Pedestrian-1: Promote street right-of-way design standards that make walking convenient and enjoyable.
  - *The proposed project incorporates streetscape designs that will improve the pedestrian environment and improve pedestrian connections to and from the BART station. Improvements include improved sidewalks, improved crosswalks, and internal walkways, connecting pedestrians to the development and the BART station.*
- Bicycle-3: Make bicycle riding more safe, secure and convenient.
  - *The proposed project incorporates Class IV separated bikeways along 7<sup>th</sup> Street and Mandela Parkway, creating safer bicycle access to the development and to the BART station. In addition, the project is providing a 600-space bike station with valet surface on site. The project is also providing long-term bike parking within the buildings and short-term bike parking around the site.*
- Existing Transit Enhance-1: Seek and identify funding mechanisms to increase the frequency of AC Transit bus service and make other transit improvements in and through West Oakland.
  - *The approved TDM plan provides an option for the developer to invest in increased AC Transit service to the project site by funding the implementation of AC Transit's ACgo plan. In particular, lines 14, 29, 36 and 62 serve to connect West Oakland BART and nearby destinations. Operations investments should serve to increase frequency of these routes to meet ACgo full implementation goals (weekday peak and off peak: 15 minute headways for lines 14 and 62, and 20 minute headways for lines 29 and 36; weekend: 20 minute headways for lines 14, 29, 36 and 62).*

### Neighborhood-Serving Retail & Services

- Intent: Support community-based efforts to improve West Oakland residents' access to everyday goods and services.
  - *The proposed project would greatly increase the amount of neighborhood-serving retail, including retail along 7<sup>th</sup> St., 5<sup>th</sup> St., and at the interior of the site with up to*

*55,000 square feet of retail proposed. The project proposes the potential for uses such as a food market, cafes, and numerous local retail spaces.*

- EED-13: Support the expansion of an existing grocery store and/or the establishment of new grocery stores in West Oakland.
  - *The proposed project includes space for a small food market on 7<sup>th</sup> St. near Center St.*
- EED-14: Promote the development of community-based, neighborhood-serving retail and service businesses.
  - *The project is required, if feasible, to provide 20% of the retail square footage below market rate.*

## 6. ZONING ANALYSIS OVERVIEW

The West Oakland BART TOD project is located within the Transit-Oriented Development Commercial Zone (S-15W). The intent of the S-15W Zone is to create, preserve and enhance areas devoted primarily to serve multiple nodes of transportation and to feature high-density residential, commercial, and mixed-use developments to encourage a balance of pedestrian-oriented activities, transit opportunities, and concentrated development; and encourage a safe and pleasant pedestrian environment near transit stations by allowing a mixture of Residential, Civic, Commercial, and Light Industrial Activities, allowing for amenities such as benches, kiosks, lighting, and outdoor cafes; and by limiting conflicts between vehicles and pedestrians, and is typically appropriate around transit centers such as Bay Area Rapid Transit (BART) stations, AC Transit centers, and other transportation nodes. The following discussion outlines the purpose of the S-15W zone, with staff analysis provided below in indented, italicized text:

- Create, preserve, and enhance areas devoted primarily to serve multiple nodes of transportation and to feature high-density residential, commercial, and mixed-use developments.
  - *The proposed project is a mixed-use project at the West Oakland BART station, with high-density residential, office, and retail.*
- Encourage a balance of pedestrian-oriented activities, transit opportunities, and concentrated development
  - *The proposed project provides pedestrian-oriented retail along the ground floor of the development as well as a series of pedestrian-oriented public plazas and walkways. The proposed project integrates the BART station and AC Transit bus stops into the overall development. The proposed project incorporates concentrated development at the site with 762 residential units, 300,000 square feet of office, and up to 55,000 square feet of retail.*
- Encourage a safe and pleasant pedestrian environment near transit stations by allowing a mixture of Residential, Civic, Commercial, and Light Industrial Activities, allowing for amenities such as benches, kiosks, lighting, and outdoor cafes.
  - *The proposed project provides a safe and pleasant pedestrian environment by improving sidewalks along the perimeter of the site as well as creating pedestrian walkways through the site to the BART station. The pedestrian walkways include pedestrian-scaled lighting, seating, and cafes.*
- Limit conflicts between vehicles and pedestrians

- *The proposed project removes vehicles (except Emergency Vehicles and Service Vehicles) from the site and creates a pedestrian-oriented environment.*

### **17.97.035 - Special regulations applying to mixed-use developments on Bay Area Rapid**

#### **Transit (BART) stations on sites with one (1) acre or more land area.**

No mixed-use developments that include Bay Area Rapid Transit (BART) stations located on sites with one (1) acre or more land area shall be permitted except upon the granting of a conditional use permit pursuant to Section 17.97.025 and the conditional use permit procedure in Chapter 17.134 or upon the granting of a Planned Unit Development permit pursuant to Chapters 17.140 and 17.142, and shall be subject to the following special regulations:

- A. Intermodal Activities and Pedestrian Plaza. Developments should incorporate multiple forms of public transportation and a pedestrian plaza.

*The project has incorporated a pedestrian plaza at the corner of 7<sup>th</sup> and Chester St. as well as at 5<sup>th</sup> St. and Center St. The project is accommodating multiple forms of public transportation by providing AC Transit bus stops along 7<sup>th</sup> St. and 5<sup>th</sup> St.*

- B. Professional Design. The application shall utilize the following professionals in the design process for the development:

1. An architect licensed by the state of California; and
2. A landscape architect licensed by the state of California, or an urban planner holding or capable of holding membership in the American Institute of Certified Planners.

*The project is utilizing a licensed architect and a licensed landscape architect.*

- C. Undergrounding of Utilities. All electric and telephone facilities; fire alarm conduits; street light wiring; and other wiring, conduits, and similar facilities shall be placed underground by the developer as required by the city. Electric and telephone facilities shall be installed in accordance with standard specifications of the serving utilities. Street lighting and fire alarm facilities shall be installed in accordance with standard specifications of the Electrical Department.

*All utilities are underground or located inside the building.*

- D. Performance Bonds. The City Planning Commission or, on appeal, the City Council may, as a condition of approval of any said development, require a cash bond or surety bond for the completion of all or specified parts of the development deemed to be essential to the achievement of the purposes set forth in Section 17.97.010. The bond shall be in a form approved by the City Attorney, in a sum of one hundred fifty percent (150%) of the estimated cost of the work, and conditioned upon the faithful performance of the work specified within the time specified. This requirement shall not apply if evidence is provided to the city which indicates that alternative bonding or other assurances have been secured by the Bay Area Rapid Transit District.

*To be determined at the time of permitting for public improvements.*

The applicable zoning regulations include land use and density allowances. The project size is split into two zoning designations:

- Development Areas T2, T3 have a height limit of 60 ft, Commercial FAR maximum of 3.0, and a maximum residential density of 375 square feet of lot area required per dwelling unit.
- Development Areas T1, T4 have a height limit of 100 ft, commercial FAR maximum of 5.0, and a maximum residential density of 225 square feet of lot area required per dwelling unit.

## 7. PROJECT SPECIFIC ZONING ANALYSIS AND DESIGN REVIEW

### 7.1 Revision to Preliminary Development Plan (PDP) for the Planned Unit Development (PLN18490 – REV02)

#### 7.1.1 Zoning Analysis for PDP Revision

Development Standard	Required S-15W – 60’ 004007700300 (T2, T3)	Required S-15W – 100’ 004007100300 (T1, T4)	Proposed 111,230 (T2, T3)	Proposed 131,900 (T1, T4)	Consistency Analysis
Use					
Multifamily	P		P	P	Complies
Administrative	P		P	P	Complies
General Retail	P		P	P	Complies
Community Assembly	P		P		Complies
Height	60 ft	100 ft	80 ft	320 ft, 100 ft	Does not comply. Waiver requested
Max height within 10’ of the front property line	25 ft	75 ft	38 ft	320 ft	Does not comply. Concession requested
Residential Density	375 sq ft (296 units before commercial calculation)	225 sq ft (586 units before commercial calculation)	240 units	522 units	With PUD bonus and State Affordable Housing Bonus, Complies
Max Nonresidential FAR	3.0	5.0	0.16	2.62	Complies
Open Space					
Group usable open space per regular unit	150 sf per unit  150x240 = 36,000	75 sf per unit  75x522 = 39,150	Total required = 75,150 sf  Total open space provided = <b>35,149 sf</b>		Does not comply. Concession/Waiver request under consideration as part of PDP revision
17.142.110 Development standards. F. Usable Open Space	IF PUD bonus requested, 200 sf/du of group usable open space required = 152,400 sf		Total open space provided = <b>35,149</b>		Does not comply Concession/Waiver request under consideration as part of PDP revision
17.116 Parking - Multifamily	Min: 0.50 space per dwelling unit Max: 1.25 spaces per dwelling unit		140		Does not comply

Development Standard	Required S-15W – 60’ 004007700300 (T2, T3)	Required S-15W – 100’ 004007100300 (T1, T4)	Proposed 111,230 (T2, T3)	Proposed 131,900 (T1, T4)	Consistency Analysis
	381 parking spaces required  Can take advantage of up to 50% reduction in parking requirement if meet criteria. 191 required				Concession/Waiver request under consideration as part of PDP revision
17.116 Parking - Office	Min: No parking required Max: 1 space per 300 sq ft		190 parking spaces provided		Complies
17.116 Parking - Commercial	1 space for each 600 sq ft of floor area on the ground floor of a building; 1 space for each 1,000 sq ft of floor area not on the ground floor of a building =53,491/600 = 89 T1: 24 =12 T3: 27 = 14 T4: 39 = 20		55		Does not comply Concession/Waiver request under consideration as part of PDP revision
Total Parking	Total Parking Required: 470 spaces required. If comply with reduction requirements, 280 spaces		385 total parking spaces provided		Complies
<b>Loading</b>					
17.97.060 Loading	Driveway cannot be located within 20 ft from walkways and plazas or need CUP		Complies		Complies
Residential Loading	1 berth for T1 and T3		Loading has been reduced from 2 to 1 on T3, and 2 to 1 on T1		Complies
Commercial Loading	160,000 sq ft or more provide 3 berths		Loading has been reduced from 2 to 1 loading spaces on T4, reduction does not comply.		Does not comply. Variance will be needed
<b>Bike Parking</b>					
17.117 Multifamily Bike Parking	Long-term: 1 space/4 du = 191 Short-term: 1 space/20 du = 39		Complies Long-term: 220 spaces: T3: 70; T1: 150; 191 spaces for residential Short-term: 90 total spaces provided, 39 applied for residential		Complies
17.117 Retail Bike Parking	Long-term: 1 space/12,000 sq ft = 5 spaces required Short-term: 1 space/2,000 sq ft = 27 spaces required		Complies Long-term: 29 Short-term: 27		Complies
17.117 Office Bike Parking	Long-term: 1 space/10,000 sq ft = 30 Short-term: 1 space/20,000 sq ft = 15		Complies Long-term: 55 Short-Term: 24		Complies
TOTAL Bike Parking	Long-term: 226 spaces required Short-term: 81 spaces required		Complies Long-term: 220+55 spaces provided		Complies



Development Standard	Required S-15W – 60’ 004007700300 (T2, T3)	Required S-15W – 100’ 004007100300 (T1, T4)	Proposed 111,230 (T2, T3)	Proposed 131,900 (T1, T4)	Consistency Analysis
			Short-term: 90 short-term spaces provided		
Lockers/Showers	A minimum of two (2) showers per gender plus one (1) shower per gender for each 150,000 sf. above 150,000 sf. = 3 showers per gender Four (4) lockers per shower = 20 lockers		Meets requirement. 3 showers and 12 lockers per gender provided on the ground floor of T4		Complies

**7.1.2 Design Review for Preliminary Development Plan (PLN18490-REV02)**

The Preliminary Development Plan provides conceptual designs for the overall project with supporting Design Guidelines generally remain unchanged, except for the following:

- Removal of under track retail. The removal of the undertrack retail creates a larger public plaza but also removes activating uses and creates an undertrack space that creates design challenges
- Parking redesign. The change to the parking arrangement impacts Mandela Parkway by adding curb cuts, driveways, and garage entries that were not there under the previous entitlement. The added garage access for T1 and T4 means that the majority of Mandela is now dedicated to back of house activities.

**7.2 Zoning Analysis and Design Review for Final Development Plan (FDP) for T1 (PLN18490-REV01-PUDF01)**

**7.2.1 Zoning Analysis for T1**

Development Standard	Required S-15W – 100’ (T1: 32,054 sf)	Proposed (T1)	Consistency Analysis
<b>Use</b>			
Multifamily	P	P	Complies
General Retail	P	P	Complies
Maximum Height	100 ft	320 ft	Does not comply, Waiver as part of PDP approval
Max height within 10’ of the front property line	75 ft	320 ft	Does not comply, Concession as part of PDP approval
Residential Density	225 sq ft (586 units before commercial calculation)	522 units	Does not comply, PUD bonus and State Affordable Housing Bonus approved as part of PDP approval
Max Nonresidential FAR	5.0	0.45	Complies
Max number of stories	9	31	Does not comply, Waiver as part of PDP approval for 30 stories, request to extend to 31 stories
Open Space			

Development Standard	Required S-15W – 100’ (T1: 32,054 sf)	Proposed (T1)	Consistency Analysis
Group usable open space per regular unit	75 sf per unit  75x522 = 39,150	17,648 sf	Does not comply, Concession/Waiver request under consideration as part of PDP revision
17.142.110 Development standards. F. Usable Open Space	200 sf/unit = 104,400, but variance granted for PDP allowing 50% reduction = 52,200 sf	17,648 sf	Does not comply Concession/Waiver request under consideration as part of PDP revision
<b>Parking and Loading</b>			
17.116 Parking - Multifamily	Min: 0.50 space per dwelling unit Max: 1.25 spaces per dwelling unit 261 parking spaces required Can take advantage of up to 50% reduction in parking requirement if meet criteria = 130 spaces	106	Does not comply Concession/Waiver request under consideration as part of PDP revision
17.116 Parking - Commercial	1 space for each 600 sq ft of floor area on the ground floor of a building; (first 3000 sf not counted) 19 spaces required,	19	Does not comply Concession/Waiver request under consideration as part of PDP revision
Total Parking	149 spaces required	125	Does not comply Concession/Waiver request under consideration as part of PDP revision
17.97.060 Loading	Driveway cannot be located within 20 ft from walkways and plazas or need CUP		Complies
Residential Loading	1 berth required	1 berth provided	Complies
17.117 Multifamily Bike Parking	Long-term: 1 space/4 du = 131 Short-term: 1 space/20 du = 27	131 LT spaces 27 ST spaces	Complies
17.117 Retail Bike Parking	Long-term: 1 space/12,000 sq ft = 2 spaces required Short-term: 1 space/2,000 sq ft = 8 spaces required	2 LT spaces 8 ST spaces, covered by PDP	Complies
TOTAL Bike Parking	Long-term: 133 spaces required Short-term: 35 spaces required	252 LT spaces 36 ST spaces	Complies

7.2.2 Design Review for FDP for T1 (PLN18490-REV01-PUDF01)

The Final Development Plan is required to be consistent with the Preliminary Development Plan and the associated Design Guidelines. Staff feedback is indented and in italics below.

West Oakland BART Design Guidelines

***Activated 7<sup>th</sup> Street Corridor***

2. The minimum depth of retail space from storefront to rear should be at least 40 feet to promote viable uses
  - *Most of the proposed retail along 7<sup>th</sup> Street meets this design guideline.*

3. The retail frontage should be built with adequate set-back to the property line to provide public transit use of the sidewalk and possible café tables and other retail uses at the sidewalk edge.
  - *Proposed setbacks do allow for minimum required clear pedestrian through-zone, with some space for café tables or other retail uses.*
5. Building entries should be oriented to the street or promenade with intervals that promote active street experience, except for major anchor tenants such as grocery or drug stores, which could be a greater interval.
  - *The building entries are oriented to the street. .*
6. Shop fronts should be designed with a high level of transparency – at least 75% glass storefront along the 7th Street edge.
  - *The retail fronting 7<sup>th</sup> St. provides a high level of transparency, meeting or exceeding this guideline.*

### **Mandela Parkway**

Building facades along Mandela Parkway will be designed to become lively and delightful edges to streets and open spaces through the variation of building materials and facade typologies.

- *The retail space at 7<sup>th</sup> and Mandela does a good job of wrapping the corner and providing entrances on Mandela Parkway. The rest of Mandela Parkway is dedicated to back of house activities that are not active, but the applicant claims that they cannot be further minimized or located anywhere else.*
2. Service and Loading areas should be well designed and visually protected from pedestrian view. Service and loading areas should be designed to minimize pedestrian disruption and to ensure pedestrian safety.
    - *The service and loading areas that have to be located on Mandela should have improved design, with high quality doors that are integrated into the overall design of the building*
  5. Retail and other public uses should be provided where possible. Shop fronts should be designed with a high level of transparency and visual interest.
    - *The only retail provided on Mandela Pkwy is at the corner of 7<sup>th</sup> St. The rest of the Mandela Pkwy frontage is dedicated to back of house activities.*
  6. Service and loading areas should be designed to minimize pedestrian disruption and to ensure pedestrian safety.
    - *Design of garage and loading doors as well as doors for transformers and gas meter could be higher quality.*

### ***7.3 Zoning Analysis and Design Review for Final Development Plan (FDP) for Horizontal Improvements (PLN18490-REV01-PUDF02)***

#### **7.3.1 Zoning Analysis for FDP for Horizontal Improvements**

The Final Development Plan for Horizontal Improvements does not require a zoning analysis because the improvements are related to portions of the project that are non-development related.

**7.3.2 Design Review for FDP for Horizontal Improvements**

The streets and open spaces proposed in the Preliminary Development Plan are further refined in the Final Development Plan and should be consistent with the PDP and the West Oakland BART Design Guidelines. Staff feedback is indented and in italics below.

**West Oakland BART TOD Design Guidelines**

Public Space Goals: The following urban design principles are intended to support public space strategy:

- Establish a continuous, diverse and active network of public open spaces, including plazas, activated streetscapes and pedestrian paseos that connect site and BART station to the surrounding community.
  - *The project provides a plaza at 7<sup>th</sup> St. and Chester St. (Mandela Plaza), a plaza at 5<sup>th</sup> St. and Center St. (Center Square), and an Art Alley along the BART tracks. The public spaces are well connected and activated with retail and design elements.*
- Configure and design the open space system to serve as center for the West Oakland community and a destination for Oakland and the East Bay.
  - *The applicant has added landscaping and design features to the open space system since the first submittal, but it is still unclear if the design elements are creating a true destination where people will want to stop and spend time. The Conditions of Approval require programming, which will help activate the space and create a destination.*
- Create walkable and lively public streets, open spaces and pedestrian ways that provide a delightful, safe and activated 7 day-a-week destination for the neighborhood, transit riders and East Bay.
  - *The design of the public streets and pedestrian ways provide critical multimodal access to the West Oakland BART station. .*
- Provide a range of cultural, recreational and commercial activities that reinforce the public destination appeal and civic role of the Station site as a whole.
  - *The project provides a wide range of commercial activities and will retain the Blues Walk of Fame. The Public Art component of the project has the potential to provide further cultural destinations to the site, but the content of the Public Art is still unknown at this time.*

**7.4 Zoning Analysis and Design Review for Final Development Plan (FDP) for T3 (PLN18490-REV01-PUDF03)**

**7.4.1 Zoning Analysis for FDP for T3 (PLN18490-REV01-PUDF03)**

Development Standard	Required S-15W – 60’ (T3:53,652sf)	Proposed T3	Consistency Analysis
Use			
Multifamily	P	P	Complies
Administrative	P	P	Complies
General Retail	P	P	Complies 15,944 sf

Development Standard	Required S-15W – 60’ (T3:53,652sf)	Proposed T3	Consistency Analysis
Community Assembly	P	P	Complies 1,719 sf
Maximum Height	60 ft	80 ft	Does not comply. Previous waiver for height approved for 80’.
Max height within 10’ of the front property line	25 ft	38 ft	Does not comply, Concession as part of PDP approval
Residential Density	375 sq ft (296 units before commercial calculation)	240 units	Complies with approved PDP (with 35% SAHB)
Max Nonresidential FAR	3.0	0.33	Complies
Max number of stories	5	8	Does not comply, Waiver as part of PDP approval
<b>Open Space</b>			
Group usable open space per regular unit	150 sf per unit  150x240 = <b>36,000 sf</b>	Total open space provided = <b>17,501 sf</b>	Does not comply. Concession/Waiver request under consideration as part of PDP revision
17.142.110 Development standards. F. Usable Open Space	PUD bonus requested, 200 sf/du of group usable open space required = <b>48,000 sf</b>	Total open space provided = <b>17, 501 sf</b>	Does not comply. Concession/Waiver request under consideration as part of PDP revision
<b>Parking and Loading</b>			
17.116 Parking - Multifamily	Min: 0.50 space per dwelling unit Max: 1.25 spaces per dwelling unit 120 parking spaces required Can take advantage of up to 50% reduction, 60 parking spaces	34 provided.	Does not comply Concession/Waiver request under consideration as part of PDP revision
17.116 Parking - Commercial	1 space for each 600 sq ft of floor area on the ground floor of a building; Subtract first 3000 sf. 22 spaces required, apply 50% reduction for 11 spaces required	13 provided	Complies
Total Parking	Total Parking Required: 141 spaces required. If comply with reduction requirements, 71 spaces required	50 spaces provided (ADA plus Car Share spaces)	Does not comply Concession/Waiver request under consideration as part of PDP revision
17.97.060 Loading	Driveway cannot be located within 20 ft from walkways and plazas or need CUP	Driveways are more than 20 ft from a walkway or plaza	Complies
Residential Loading	1 berth	1 berth provided	Complies.
Commercial Loading	Not required	None	Complies
17.117 Multifamily Bike Parking	Long-term: 1 space/4 du = 60 Short-term: 1 space/20 du = 12	Long-term: 64 Short-term: 12	Complies
17.117 Retail Bike Parking	Long-term: 1 space/12,000 sq ft = 2 spaces required Short-term: 1 space/2,000 sq ft = 1 spaces required	Long-term: 2 Short-term: 8	Complies

Development Standard	Required <b>S-15W – 60’ (T3:53,652sf)</b>	Proposed <b>T3</b>	Consistency Analysis
TOTAL Bike Parking	Long-term: 62 spaces required Short-term: 13 spaces required	Long-term: 66 spaces provided Short-term: 20 spaces provided	Complies

7.4.2 Design Review for FDP for T3 (PLN18490-REV01-PUDF03)

The Final Development Plan is required to be consistent with the Preliminary Development Plan and the associated Design Guidelines. Staff response to the relevant design guidelines are indented and italicized below.

West Oakland BART TOD Design Guidelines

**Activated 5<sup>th</sup> Street Corridor**

1. The ground floor-to-floor dimension should promote viable retail uses that are welcoming and transparent in nature, with a minimum floor-to-floor height of 20’
  - *The floor to floor height is increased to 20ft along 5<sup>th</sup> St., complying with this guideline.*
2. The minimum depth of retail space from storefront to rear should be at least 25-30 feet to promote viable uses
  - *Some of the retail spaces along the Art Walk Plaza are only 20 ft deep and therefore do not comply with this requirement.*
5. Building entries should be oriented to the street or promenade with intervals that promote active street experience, except for major anchor tenants such as grocery or drug stores, which could be a greater interval.
  - *Building entries along 5<sup>th</sup> St. are oriented to the street with intervals that promote active street experience. Along the Art Alley, there are two areas that front the parking garage with windows, so it is unclear what will be visible to the pedestrian. It would be better to relocate the car-share spaces and extend the retail space.*
6. Shop fronts should be designed with a high level of transparency – at least 60% glass storefront along the 5th Street edge.
  - *Retail has been moved back to 5<sup>th</sup> St. and storefront windows have a high level of transparency.*
7. Use architectural details on residential structures such as balconies, railings, lighting, canopies, and other elements that enliven the facade and reinforce the human scale of the development
  - *Architectural details are provided to reinforce the human scale of the development. The bay projections facing Chester Street encroach over the property line into the City right-of-way. This will require City Council approval.*

***Chester Street***

2. Residential uses should have individual entries that activate the street and provide a more traditional neighborhood scale.

- *Residential lobby, community assembly, and other residential amenity space faces Chester. There are no ground floor units.*

## 8. ZONING AND DESIGN RELATED ISSUES

### 8.1 Design

Staff has worked with the applicant to refine the design of the project to maximize access to the BART Station, activate the public spaces, and create pedestrian-oriented design at the ground floor. The building designs for T1 and T3 have been refined through the design review process.

### 8.2 Issues

In general, staff finds the project to be well-designed and generally consistent with the previously approved PDP and associated Design Guidelines. That said, staff has a few remaining design concerns and asks the DRC to consider the following:

#### 8.2.1 Preliminary Development Plan Revision (PLN18490-REV02):

- Removal of under track retail. The approved PDP included retail under the BART tracks, providing a continuation of retail along 7<sup>th</sup> St. and activating the plaza. The applicant is proposing to remove the retail under the track and expanding the plaza space. As part of the February 6, 2019 entitlement, Condition of Approval #68 requires:

“The project applicant shall provide approximately 7,610 square feet of retail under the BART tracks between Chester Street and the BART Station, consistent with the approved PDP. If retail uses under the BART tracks are deemed infeasible by the City of Oakland, then an equivalent amount of retail shall be provided along 7<sup>th</sup> St. between Chester Street and Center Street. Retail can be provided in kiosks, containers, or other small-scale retail buildings. At the same time, if retail cannot be provided under the BART tracks on Development Area T2, then the applicant shall design the space under the tracks as an attractive, delightful space for residents, community members, and BART riders to spend time with adequate provisions for safety such as security and lighting improvements.”

The retail under the tracks has been deemed infeasible, due to BART safety requirements, which made the retail too expensive to provide. The applicant is also unable to provide an equivalent amount of retail along 7<sup>th</sup> St., and is therefore seeking a revision to this Condition. The applicant is proposing a continuous plaza from 7<sup>th</sup> St. to the T3 development. The space under the track is a difficult space to activate and to create a safe, attractive, and delightful space. The applicant has continued to improve and refine the proposed design for the under track plaza. Please see Attachment C: L1.03, L-1.12, A-9.01, A-9.05.

- *Does the Design Review Committee feel that this alternative for the under track space has created an attractive, delightful space for residents, community members, and BART riders?*

#### 8.2.2 FDP for T1 (PLN18490-REV01-PUDF01):

- Mandela Pkwy. All of the back of house activities for the T1 project are ganged on Mandela Parkway, including the garage access, loading access, and utility areas. This lack of activation isn't consistent with the design guidelines for Mandela Parkway, but this is a better location for these back of house activities than 7<sup>th</sup> St. The only option would be to somehow reduce the width of the transformer room or gas meter room fronting Mandela Pkwy.

- *Does the DRC support the concentration of back of house activities on Mandela Parkway?*

### 8.2.3 FDP for Horizontal Improvements (PLN18490-REV01-PUDF02):

- The project specific conditions of approval require that the ground floor public plazas and walkways “be high-quality, well-designed spaces that include excellent pedestrian-scaled lighting, extensive furnishings, and interactive art or other amenities for children.” The applicant is proposing a striped maze and space for future Public Art, but no other interactive amenities or amenities for children.
  - *Does the DRC feel that these plazas are high-quality and interactive?*
- Blues Walk of Fame. The “The Music They Played on 7th Street Oakland” Walk of Fame lines the 7th Street sidewalk in front of the West Oakland BART station and features 88 plaques. The Bay Area Blues Society implemented the Blues Walk of Fame, with funding and support from the City of Oakland, Bay Area Rapid Transit, and private donations. The applicant proposes removing the plaques to protect them during construction and putting them back in the sidewalk along 7<sup>th</sup> St. when the streetscape improvements are finalized.
  - *Does DRC support the proposal to remove and replace the plaques as part of the finished streetscape design on 7<sup>th</sup> St.?*
- Dancing Lights. The dancing lights were part of a 2004 Caltrans grant, which funded the concept for the urban design elements called the Dancing Lights. The lights are located in front of the BART station between Chester St. and Mandela Parkway along 7<sup>th</sup> St. The City of Oakland implemented this streetscape improvement. The new multimodal streetscape design does not provide room for these urban design features, with a loading zone, a sidewalk grade bike lane, and a wide pedestrian through zone. The applicant is proposing removal of these urban design features to allow for the streetscape improvements.
  - *Does the DRC support the multimodal streetscape design that does not include the Dancing Lights?*

### 8.2.4 FDP for T3 (PLN18490-REV01-PUDF03):

- Art Wall: The project contains a 16,000 square foot blank wall with very little glazing facing the BART tracks, which is envisioned as an art wall with rotating art. The wall acts as a sound barrier, with a corridor facing the tracks and units facing away from the BART tracks. Staff requested more details on the approach to the public art on this wall before going to public hearing, to help provide more details about how the concept will work, including who manages the rotating art, how its paid for, how often it is updated, who decides on the art, etc. The details of the Public Art are not technically required until the building permit stage, but because this wall is a significant feature of the overall design of the building, it is a significant feature as viewed from BART as people leave and enter Oakland, and it is the prominent design feature viewed from across 7<sup>th</sup> St., staff feels it is important for the DRC and the Planning Commission to consider it. The applicant does not yet have an art consultant or a plan to curate the art wall and is asking that the design without the public art be considered and approved (see page A-00.04 of Attachment D). The development team still needs to establish a process for selecting the art; establish



funding; and establish a process for commissioning the art. The applicant also anticipates time lags between art displays when the ‘non-art’ façade will be exposed.

- *Does the DRC think the wall without art (non-art wall) is a satisfactory design solution for this prominent facade, considering that the public art process and approach won't be decided until building permit and there will still be lags between art installations?*
- Façade facing Art Alley. There are two areas where parking is located along the ground floor façade facing the Art Alley with full height glazing. The type of glazing and what will be visible to the pedestrian is uncertain. Staff would prefer to see this parking removed or relocated and the retail spaces extended along the façade to better activate the space.
  - *Does the DRC support removing visible parking from the ground floor façade along the Art Alley?*

### **8.3 On-going, Non-design Related Issues**

The following issues will need to be resolved prior to advancing to Planning Commission:

- Off-street loading in T4. T4 does not provide the required amount of off-street loading with the required dimensions. The code compliant loading will need to be provided or a variance needs to be requested.
- Phasing of Project. The phasing of the project requires that the affordable housing portion of the project (T3) be built before or concurrently with the market rate portion of the project (T1) because the market rate tower is dependent on the State Affordable Housing Bonus, which provides a 35% density bonus plus concessions and waivers for height, open space, and parking for the T1 market rate tower. The applicant is requesting that the City allow them to build the market rate tower first. This issue will need to be resolved before the project can advance to Planning Commission.

In addition, the phasing associated with implementation of the public infrastructure improvements still needs to be resolved, particularly around key public access points to the BART station, including 7<sup>th</sup> Street improvements.
- State Affordable Housing Bonus requests. As part of the PDP revision, the applicant has requested new concessions/waivers for parking and residential open space as well as an increase in the number of stories for T1. Whether a current State Affordable Housing Bonus request will be deemed a waiver or concession is still under consideration by staff in conjunction with the City Attorney's Office.
- Major Encroachment. The proposed T3 building has livable space that extends three feet over the property line into the public right-of-way. This will require a Major Encroachment Permit and will require City Council approval.
- Incident Command Center. The Incident Command Center for the BART station is currently proposed in the T3 building along the Art Alley. The Oakland Fire Department has requested that it be located inside the BART station as approved in the current Preliminary Development Plan.
- Bike Station under the BART track. The jurisdiction for the review and approval of the under track Bike Station is still being considered between BART and the City of

Oakland. Conceptual designs of the bike station are included by the applicant for context purposes.

**RECOMMENDATION**

Staff recommends the DRC review and comment on the proposed West Oakland BART PDP Revision (PLN18490-REV02), the FDP for Parcel T1 (PLN18490-REV01-PUDF01), the FDP for Horizontal Improvements (PLN18490-REV01-PUDF02), and the FDP for Parcel T3 (PLN18490-REV01-PUDF03) with attention to the issues raised by staff in this report.

Prepared by:

*Dara O'Byrne*  
\_\_\_\_\_  
Dara O'Byrne, Acting Planner IV

Reviewed by:

*Catherine Payne*

\_\_\_\_\_  
Catherine Payne, Acting Development Planning Manager  
Bureau of Planning

**Attachment A:** Proposed West Oakland BART TOD PDP Revision, Vesting Tentative Tract Map, dated July 24, 2020

**Attachment B:** Proposed T1 FDP, dated July 24, 2020

**Attachment C:** Proposed Horizontal Improvements FDP, dated July 24, 2020

**Attachment D:** Proposed T3 FDP, dated July 24, 2020

## **ATTACHMENT A:**

Proposed West Oakland BART TOD PDP Revision,  
Vesting Tentative Tract Map, dated July 24, 2020

# MANDELA STATION

@ West Oakland BART

## PRELIMINARY DEVELOPMENT PLAN

July 24, 2020



HENSEL PHELPS



**GENERAL**

G100 TITLE SHEET, DRAWING INDEX

**CONTEXT INFO**

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A-00.17

A-01.00 SUMMARY OF PROPOSED ENTITLEMENT REVISIONS

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A-01.02 ZONING SUMMARY  
A-01.03 OPEN SPACE ANALYSIS  
A-01.04 PHASING PLAN  
A-01.06 LEED CHECK LIST  
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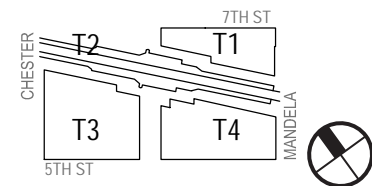
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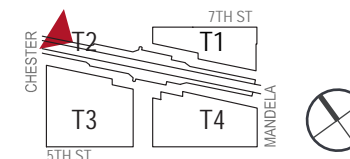


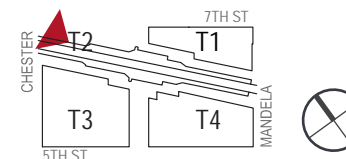
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**DRAWING  
INDEX**

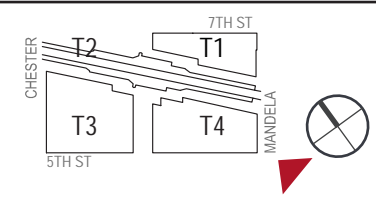
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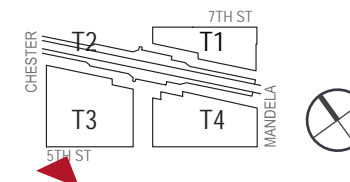
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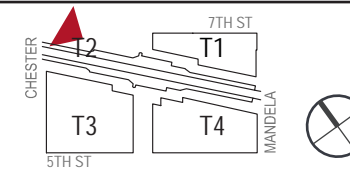


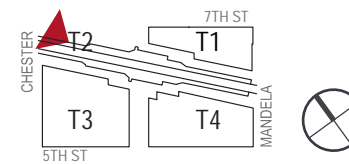


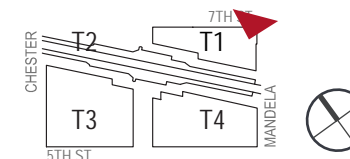


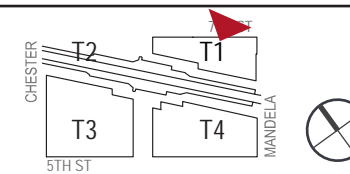


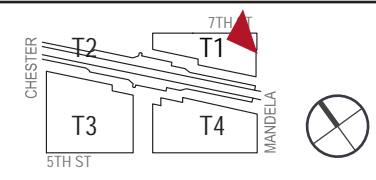


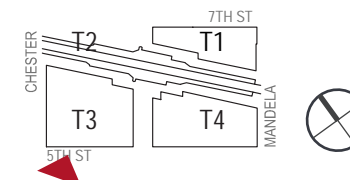




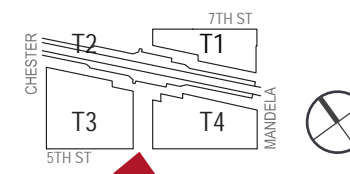


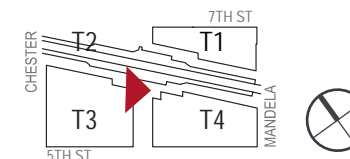


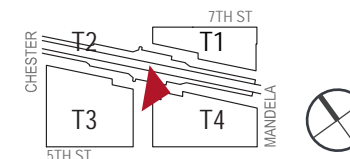


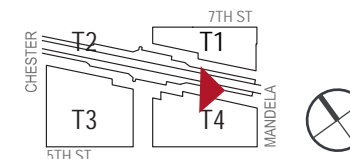


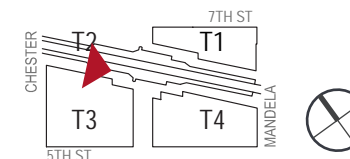


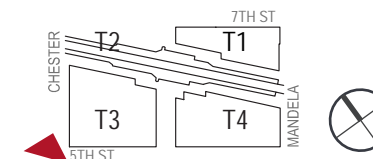


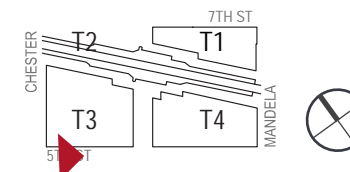












# SUMMARY OF PROPOSED ENTITLEMENT CHANGES

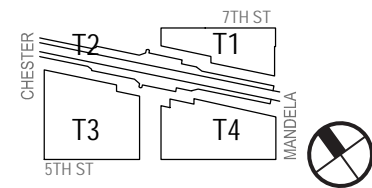
## Development Program

- 1** T1 – eliminated the 82,460sf of office, and added 22 market rate units that have been transferred from T3; reduced retail from 17,185sf to 14,350sf; increased to 31 stories
- 2** T2 – eliminated the under-track retail kiosks
- 3** T3 – moved the 22 market rate units to T1
- 4** T4 – reduced the retail from 30,800sf to 23,184sf
- 5** Parking – changed the shared 400 car parking garage to 3 separate garages: T1-125 spaces; T3-50 spaces; T4- 210spaces
- 6** Requested State Affordable Housing Bonus waivers for parking and open space.

## Revision Cloud Key



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PROJ. # 168-153 WO BART  
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## SUMMARY OF PROPOSED ENTITLEMENT REVISIONS

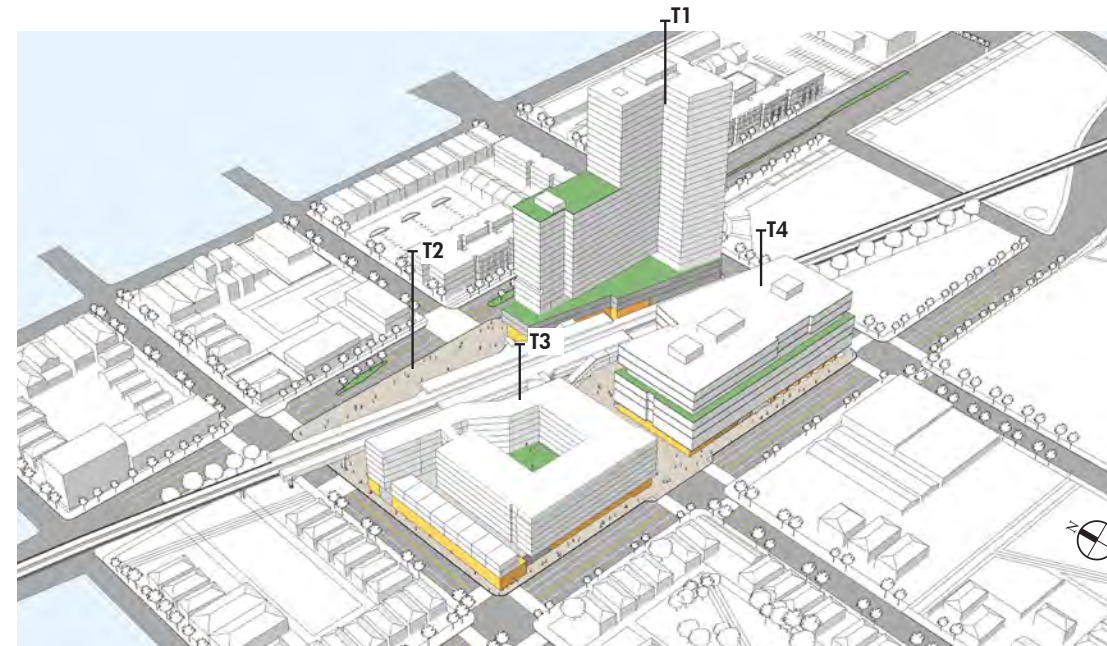
SHEET: A-01.00



**Proposed Program**

USES	T1	T2	T3	T4	Program Total
OFFICE				300,000 sf	300,000 sf
RETAIL	14,350 sf	Flexible Kiosk Program	15,944 sf	23,184 sf	53,478 sf
OTHER NON-RESIDENTIAL			2,057 sf	8,540 sf	10,597 sf
RESIDENTIAL	522 units		240 units*		762 units (20% min. affordable)
PARKING	125		50	210	385 spaces

\*79 units at 30% AMI, 11 units at 50% AMI, 148 units at 60% AMI, 2 units at 120% AMI



**Note:** Program is based on the previously approved Conditions of Approval, As approved by the Planning Commission February 6, 2019.

**Open Space Calculation**

<b>Residential Open Space Req</b>		
240 Units	200	48,000 SF
522 Units	200	104,400 SF
<b>Required Open Space</b>		<b>152,400 SF</b>
<b>Open Space Provided</b>		
T1 Open Space		17,648 SF
T3 Open Space		17,501 SF
		<b>35,149*</b> SF

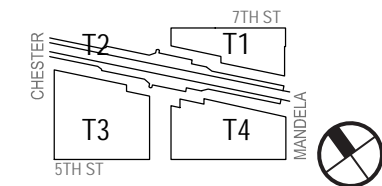
\*Open space waiver requested for 117,251 sf for T1 and T3 per Density Bonus Letter.

**Approximate Public Open Space Provided**

Mandela Plaza	30,032 SF
Under Track Plaza	8,464 SF
Art Alley	20,923 SF
Center Square	15,949 SF
<b>75,368 SF</b>	



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PROJ. # 168-153 WO BART  
DATE: July 24, 2020

**ZONING  
INFO**

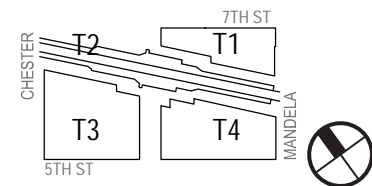
SHEET: A-01.01

GENERAL ZONING INFORMATION	REFERENCE	NOTES
ASSESORS BLOCK	Survey	See Survey; Tract 8046, Blocks 494 and 493
ZONING USE DISTRICT	Oak GIS	S-15W
PERMITTED AND/OR CONDITIONAL USES	17.97.010-.020	Residential, Community Assembly, General Retail, Full +Limited Service Retail, Parking, Commercial Office permitted
HEIGHT & BULK DISTRICT	17.97	S-15W 60' and S-15W 100'; master plan consistent with zoning requirements
GENERAL PLAN / POLICY PLAN	OAK-GIS	General Commercial; West Oakland Specific Plan
HISTORIC OR LANDMARK STATUS	OAK-GIS	None
LIQUEFACTION HAZARD ZONE	OAK-GIS	Severity 4
CONDO CONVERSION IMPACT AREA	OAK-GIS	None
<b>HEIGHT AND BULK CONTROLS</b>		
SITE AREA	Survey	241,282 SF
FLOOR AREA RATIO (FAR)	17.97.130	S-15W 60' - 3.0; S-15W 100' - 5.0; Master Plan within zoning density requirements
HEIGHT LIMIT	17.97	60' and 100' (Height limit modified to allow 80' and 320' tall building pursuant to State Affordable Housing Exemption)
REQUIRED SETBACKS	17.97.060	No Front Yard Setbacks Required; Interior Lot subject to PUD
REAR YARDS / COURTS		None Required
ADJACENCIES		None Significant
UNIT SEPARATION / EXPOSURE REQUIREMENTS	17.108.080	8' minimum at living room window +2' for each floor above = maximum 10% of It width
<b>DETAILED CONTROLS &amp; REQUIREMENTS</b>		
RESIDENTIAL DENSITY LIMITS	17.97.130	S-15W 60' - 375sf/unit; S-15W100' - 225sf/unit; Density increase per State Affordable Housing and PUD density bonus
OPEN SPACE REQUIREMENTS	17.97.130	S-15W 60' - 150sf/unit; and S-15W 100' - 75sf/unit; Overall master plan within zoning limites
SCREENING & SETBACK OF PARKING & LOADING	17.116.290	All parking garages are screened per zoning requirements
OFF-STREET PARKING - RESIDENTIAL	17.116.060	0.5 parking space per dwelling unit required; Parking meets zoning requirements with approved reductions
OFF-STREET PARKING - RETAIL	17.116.080	1 space/600 Sf of ground floor; Parking meets zoning requirements with approved reductions
OFF-STREET PARKING - COMMERCIAL	17.116.080	None required
OFF-STREET PARKING DIMENSIONS	17.116.200	50-50 compact / standard; or 75% intermediate + 12.5% compact
OFF-STREET DRIVE AISLE DIMENSIONS	17.116.210	21'-24' two way aisle widths
OFF-STREET LOADING - RESIDENTIAL	17.116.120	Loading per zoning requirements
OFF-STREET LOADING - RETAIL	17.116.150	Loading per zoning requirements
LOADING BERTH DIMENSIONS	17.116.220	10'x23', 12' high for residential activities, 10'x25', 8' high for undertaking service commercial activities
BICYCLE REQUIREMENTS - RESIDENTIAL+COMMERCIAL	17.117.090	LT: total 221 spaces; ST: total 54 spaces LT: 1 per 10,000 sf of commercial; ST: 1 per 20,000 sf of commercial LT: 0.25 spaces per dwelling unit; ST: 0.05 per dwelling unit; bicycle parking provide per zoning code
BICYCLE REQUIREMENTS - RETAIL	17.117.110	LT: total 6 spaces; ST: total 28 spaces LT: 1 per 12k; ST: 1 per 2k; bicycle parking provided per zoning code



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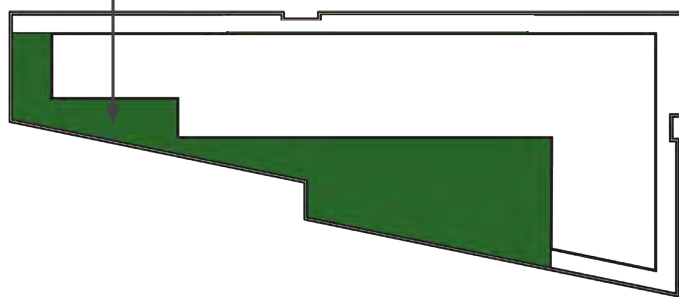
PROJ. # 168-153 WO BART  
DATE: July 24, 2020

**ZONING  
SUMMARY**

SHEET: A-01.02

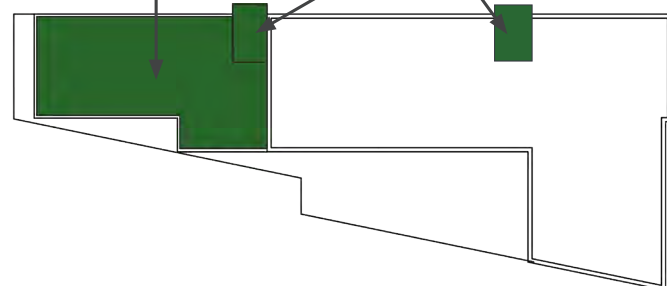
# BUILDING T1

Landscaped Terrace  
Common Open Space



BUILDING T1 - LEVEL 4 - AMENITY

Common Use Terrace  
Level 5-31 - Common Use Decks

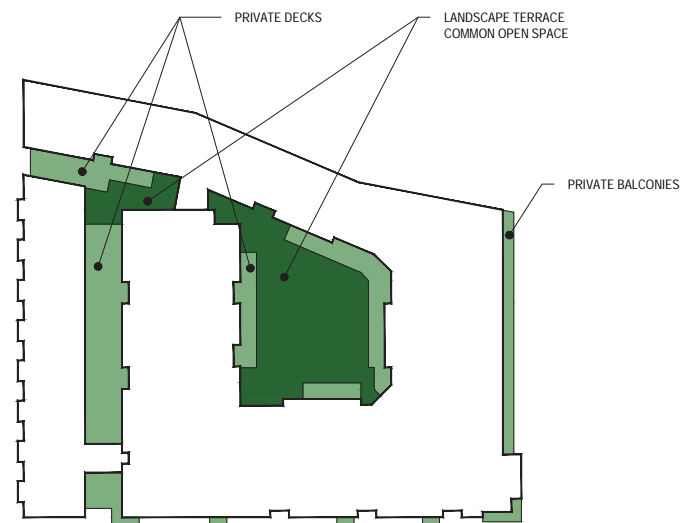


BUILDING T1 - LEVEL 19 - AMENITY

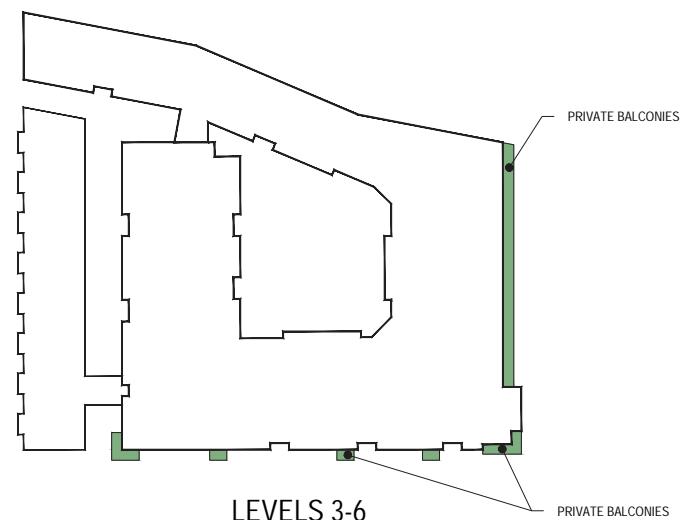
Common Use Decks	2,334 SF
Roof Terraces	15,314 SF
<b>Total</b>	<b>17,648* SF</b>

\*Open space waiver of 86,752 sf requested for T1 per Density Bonus Letter.

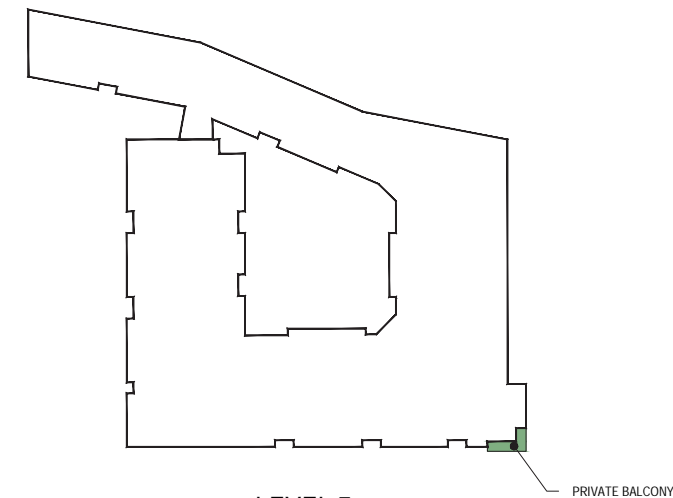
# BUILDING T3



LEVEL 2



LEVELS 3-6



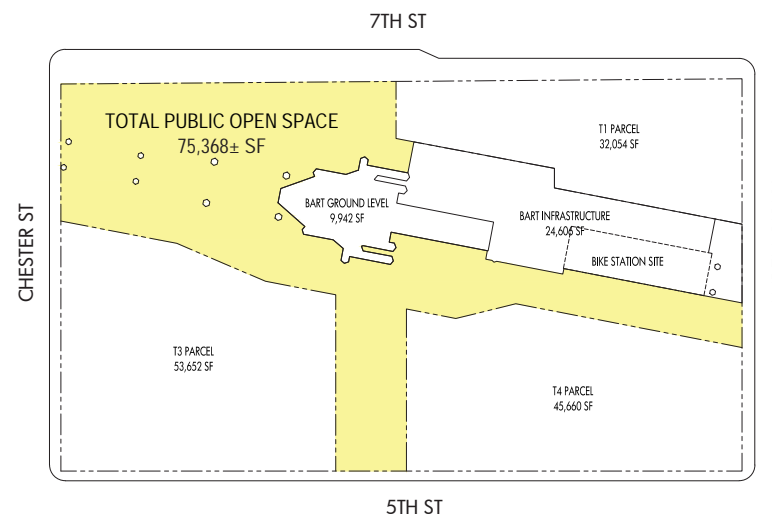
LEVEL 7

LEVEL	COMMON	PRIVATE
LV2	6,048 SF	6,667 SF
LV3		1,161 SF
LV4		1,161 SF
LV5		1,161 SF
LV6		1,161 SF
LV7		142 SF
<b>SUBTOTAL</b>	<b>6,048 SF</b>	<b>11,453 SF</b>
<b>TOTAL T3 OPEN SPACE</b>	<b>17,501 SF *</b>	

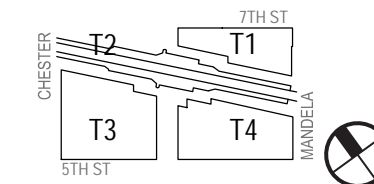
\*Open space waiver of 30,499 sf requested for T3 per Density Bonus Letter.

# PUBLIC OPEN SPACE

GROUND LEVEL - PUBLIC OPEN SPACE 75,368± SF



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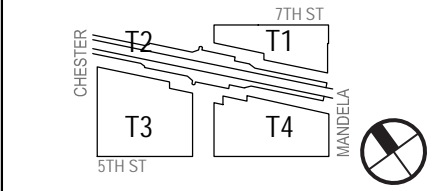


LANDSCAPE ARCHITECTS

PROJ. # 168-153 WO BART  
DATE: July 24, 2020

SHEET: OPEN SPACE ANALYSIS  
A-01.03

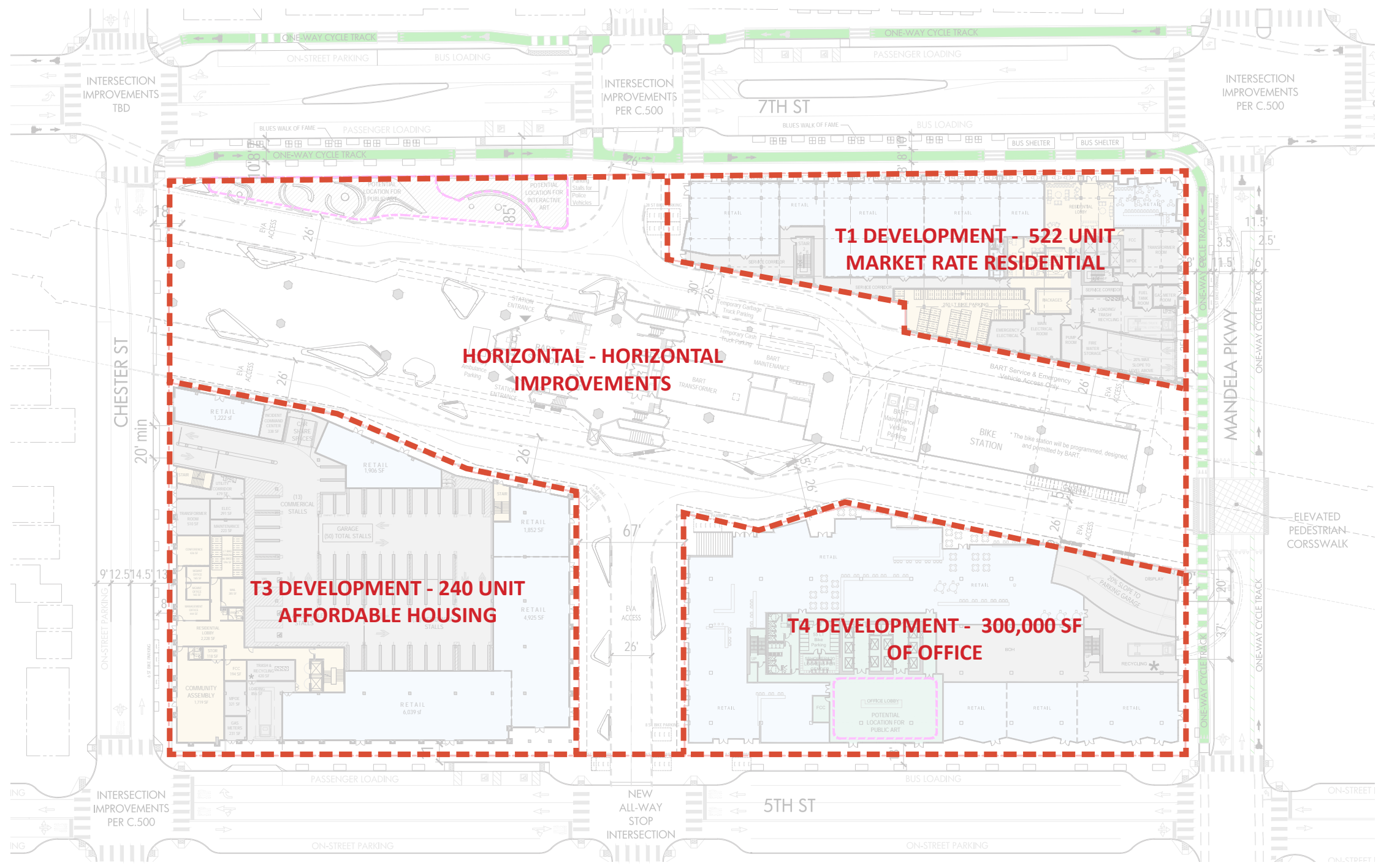
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**PROJECT SCHEDULE**

1. Horizontal				3. Phase II – T1 Development & T2 Development (plus PZ/PX for that phase)			
i.	Submit application for Final Development Plan	1 year following PDP approval		i.	Submit application for Final Development Plan	3 years following PDP approval	
ii.	Anticipated FDP approval date by Agency	1 year following submittal of Horizontal FDP application		ii.	Anticipated FDP approval date by Agency	1 year following submittal of Phase II FDP application	
iii.	Commence construction – Initial PX and/or PZ job (additional PX and PZ jobs will be tied to each later phase)	2 years following FDP approval		iii.	Commence construction	The latter of 2 years following FDP approval or 2 years following Phase I commencement of construction	
2. Phase I – T3 Development (plus PZ/PX for that phase)				4. Phase III – T4 Development (plus PZ/PX for that phase)			
i.	Submit application for Final Development Plan	1 year following PDP approval		i.	Submit application for Final Development Plan	5 years following PDP approval	
ii.	Anticipated FDP approval date by Agency	1 year following submittal of Phase I FDP application		ii.	Anticipated FDP approval date by Agency	1 year following submittal of Phase III FDP application	
iii.	Commence construction	2 years following FDP approval (allowing time to secure affordable financing)		iii.	Commence construction	The latter of 2 years following FDP approval or 2 years following Phase II commencement of construction	

PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

**PHASING PLAN**

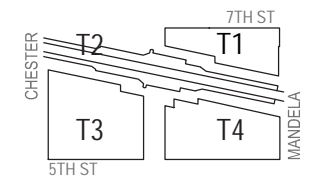
SHEET: A-01.04

# LEED CS v4 SCORECARD

## West Oakland Station T4



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PROJ. # 168-153 WO BART  
DATE: June 6, 2020

SHEET:

A-01.06

IP	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
1	1				D	Credit	Integrative Process - In design phases, achieve synergies between building, energy AND water related systems	1
<b>Totals</b>								<b>1</b>
LOCATION & TRANSPORTATION	20				D	Credit	LEED for Neighborhood Development Location - Locate within LEED ND certified development site boundary	20
	2				D	Credit	Sensitive Land Protection - Develop on previously developed land or follow criteria for non-sensitive	2
	2		1		D	Credit	High Priority Site - Locate project on infill location in historic district, priority designation or brownfield	3
	2	2		2	D	Credit	Surrounding Density & Diverse Uses - Site within 1/4 mile of surrounding density criteria and/or a 1/2 mile of diverse uses	6
	6				D	Credit	Access to Quality Transit - Locate functional entries within 1/4 mile of existing transit or 1/2 mile of planned transit services	6
	1				D	Credit	Bicycle Facilities - Provide a bike network and storage areas	1
	1				D	Credit	Reduced Parking Footprint - Don't exceed minimum local code requirements for parking capacity	1
	1				D	Credit	Green Vehicles - 5% of spaces or 20% discount for parking and electric car charging OR liquid, gas or battery facilities	1
<b>Totals</b>								<b>20</b>

SUSTAINABLE SITES	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
Yes					C	Prereq	Construction Activity Pollution Prevention - Implement an erosion control plan, per the EPA CGP v2012	NA
			1		D	Credit	Site Assessment - Complete site survey including: topography, hydrology, climate, vegetation, soils, human use, human health	1
			2		D	Credit	Site Development - Protect or Restore Habitat - Preserve 40% of greenfield AND on-site restoration OR financial support	2
1					D	Credit	Open Space - Provide outdoor space greater than or equal to 30% of total site area, 25% of which is vegetated	1
			3		D	Credit	Rainwater Management - Manage runoff for at least the 85th percentile of local rainfall events	3
	1				D	Credit	Heat Island Reduction - Meet nonroof and roof criteria OR place a minimum of 75% parking spaces under cover	2
1					D	Credit	Light Pollution Reduction - Backlight-uplight-glare method or calculation method, exterior luminaires and signage requirements	1
1					D	Credit	Tenant Design and Construction Guidelines - Provide Manual or automative glare control devices	1
<b>Totals</b>								<b>11</b>

WATER	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
Yes					D	Prereq 1	Outdoor Water Use Reduction - Permanent non-irrigated landscape OR reduce landscape water use 30% for peak watering month	NA
Yes					D	Prereq 2	Indoor Water Use Reduction - Reduce aggregate water use by 20% for fixtures and fittings	NA
Yes					D	Prereq 3	Building-Level Water Metering - Install permanent water meters that measure potable water use, share data with USGBC	NA
1			1		D	Credit	Outdoor Water Use Reduction - Reduce water use no irrigation or reduced irrigation 50% - 100%	2
3			3		D	Credit	Indoor Water Use Reduction - Reduce fixture and fitting water use by 25% - 50%	6
			2		D	Credit	Cooling Tower Water Use - Conduct a one-time potable water analysis, measure control parameters in Table 1	2
			1		D	Credit	Water Metering - Meters for 2 or more water subsystems: irrigation, indoor plumbing, hot water, boiler, reclaimed water, or other	1
<b>Totals</b>								<b>11</b>

ENERGY & ATMOSPHERE	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
Yes					C	Prereq 1	Fundamental Commissioning and Verification - Commissioning for ASHRAE 0-2005 and 1.1-2007	NA
Yes					D	Prereq 2	Minimum Energy Performance - Whole building energy simulation OR ASHRAE 50% Design Guide OR ABCPG	NA
Yes					D	Prereq 3	Building-Level Energy Metering - Use building-level energy meters or submeters that can aggregate building-level data	NA
Yes					D	Prereq 4	Fundamental Refrigerant Management - Do not use CFC-based refrigerants in HVAC&R systems, or have a phase out plan	NA
3	1		2		C	Credit	Enhanced Commissioning - Implement systems commissioning or monitor-based commissioning	6
5	2		11		D	Credit	Optimize Energy Performance - Whole building energy simulation or follow ASHRAE Advanced Energy Design Guide	18
			1		D	Credit	Advanced Energy Metering - Install advanced energy metering for whole building and individual energy sources	1
			2		C	Credit	Demand Response - Participate in existing demand response program or provide infrastructure for demand response programs	2
			3		D	Credit	Renewable Energy Production - Use renewable energy system to meet 1-10% of usage	3
	1				D	Credit	Enhanced Refrigerant Management - Refrigerants with ODP of 0 and GWP of less than 50 OR calculate refrigerant impact	1
			2		C	Credit	Green Power and Carbon Offsets - Use 50-100% green power or carbon offsets	2
<b>Totals</b>								<b>33</b>

MATERIALS & RESOURCES	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
Yes					D	Prereq	Storage and Collection of Recyclables - Dedicated areas for waste collection, collection and storage	NA
Yes					D	Prereq	Construction and Demolition Waste Management Planning - Establish C&D waste diversion goals	NA
	3		3		C	Credit	Building Life-Cycle Impact Reduction - Historic building reuse, renovate blighted buildings OR whole building LCA	6
	1		1		C	Credit	Building Product Disclosure and Optimization - Environmental Product Declarations	2
	1		1		C	Credit	Building Product Disclosure and Optimization - Sourcing of Raw Materials	2
	1		1		C	Credit	Building Product Disclosure and Optimization - Material Ingredients	2
	1		1		C	Credit	C&D Waste Management - Divert 50% (3 streams), 75% (4 streams) OR 2.5 lbs. waste per square foot	2
<b>Totals</b>								<b>14</b>

INDOOR ENVIRONMENTAL	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
Yes					D	Prereq	Minimum Indoor Air Quality Performance - Meet ASHRAE 62.1-2010	NA
Yes					D	Prereq	Environmental Tobacco Smoke Control - Prohibit smoking indoors, restrict outdoor smoking within 25 feet	NA
1	1				D	Credit	Enhanced Indoor Air Quality Strategies - Comply with enhanced IAQ strategies	2
1	1		1		C	Credit	Low-Emitting Materials - Achieve level of compliance for product categories or use budget calculation method	3
			3		C	Credit	Construction IAQM Plan - Implement IAQMP & protect materials and equipment during construction	1
			3		D	Credit	Daylight - Install glare control devices, spatial daylight autonomy, illuminance calculations OR daylight floor area measurement	3
	1				D	Credit	Quality Views - Vision glazing for 75% of regularly occupied floor area, with at least two kinds of view types	1
<b>Totals</b>								<b>10</b>

INNOVATION*	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
			1		D	Credit	Innovation: Low Mercury Lighting	1
			1		D	Credit	Innovation: Greenbuilding Education	1
			1		D	Credit	TBD	1
			1		D	Credit	TBD	1
			1		D	Credit	TBD	1
1			1		C	Credit	LEED Accredited Professional	1
<b>Totals</b>								<b>6</b>

\*Innovation in Design includes Exemplary Performance credits

REGIONAL**	Confirmed				Phase	Credit Number	Credit Name	Points Available
	Likely	Maybe	No	Confirmed				
			1		D	Credit	Optimize Energy Performance (10 Points)	1
1					D	Credit	Access to Quality Transit (5 Points)	1
			1		D	Credit	BPDO - Raw Materials (1 point)	1
			1		D	Credit	Rainwater Management (3 Points)	1
			1		D	Credit	Outdoor Water Use Reduction (2 points)	1
			1		D	Credit	Indoor Water Use Reduction (1 point)	1
<b>Totals</b>								<b>4</b>

\*\*only 4 Regional Credits are Applicable

Confirmed Certification Level:	Not SILVER
Confirmed + Likely Certification Level:	Silver
Confirmed + Likely + Maybe Certification Level:	
Total Confirmed Points	32
Total Confirmed + Likely Points	58
Total Confirmed + Likely + Maybe Points	58

LEED CHECK LIST



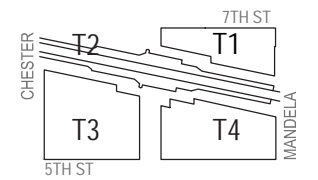
NEW HOME RATING SYSTEM, VERSION 7.0

Blueprint Scoresheet

Mandela Station T1		Points Targeted	Community	Energy	IAQ/Health	Resources	Water
		Possible Points					
<b>CALGreen</b>							
Yes	CALGreen Res (REQUIRED)	4		1	1	1	1
<b>A. SITE</b>							
Yes	A6. Stormwater Control: Prescriptive Path						
	A6.3 Non-Leaching Roofing Materials	1					1
<b>C. LANDSCAPE</b>							
<b>C3. Resource Efficient Landscapes</b>							
Yes	C3.3 Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species	3					3
<b>E. EXTERIOR</b>							
Yes	E4. Durable and Non-Combustible Cladding Materials	1				1	
Yes	E5. Durable Roofing Materials						
	E5.2 Roofing Warranty for Shingle Roofing	Y	R	R	R	R	R
<b>F. INSULATION</b>							
<b>F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content</b>							
Yes	F1.1 Walls and Floors	1				1	
Yes	F1.2 Ceilings	1				1	
<b>F2. Insulation that Meets the CDPH Standard Method—Residential for Low Emissions</b>							
Yes	F2.1 Walls and Floors	1			1		
Yes	F2.2 Ceilings	1			1		
<b>G. PLUMBING</b>							
<b>G2. Install Water-Efficient Fixtures</b>							
Yes	G2.1 WaterSense Showerheads 1.8 gpm with Matching Compensation Valve	2					2
Yes	G6. Submeter Water for Tenants	2					2
<b>H. HEATING, VENTILATION, AND AIR CONDITIONING</b>							
<b>H6. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality</b>							
Yes	H6.1 Meet ASHRAE Standard 62.2-2010 Ventilation Residential Standards	Y	R	R	R	R	R
Yes	H8. High Efficiency HVAC Filter (MERV 13+)	1			1		
<b>J. BUILDING PERFORMANCE AND TESTING</b>							
<b>J5. Building Performance Exceeds Title 24 Part 6</b>							
Option 1: Compliance Over Title 24	J5.1 Home Outperforms Title 24	25		25+			
<b>K. FINISHES</b>							
<b>K1. Entryways Designed to Reduce Tracked-In Contaminants</b>							
Yes	K1.2 Entryways to Buildings	1			1		
<b>L. FLOORING</b>							
≥75%	L2. Low-Emitting Flooring Meets CDPH 2010 Standard Method—Residential	3			3		
<b>M. APPLIANCES AND LIGHTING</b>							
Yes	M1. ENERGY STAR® Dishwasher	1					1
Yes	M2. Efficient Clothes Washing and Drying						
	M2.2 Energy Star Dryer	1		1			
<20 cubic feet	M3. Size-Efficient ENERGY STAR Refrigerator	2		2			
Full Circuit	M6. Electric Vehicle Charging Stations and Infrastructure	2		2			
Yes	M8. Gearless Elevator	1		1			
<b>N. COMMUNITY</b>							
<b>N1. Smart Development</b>							
Yes	N1.1 Infill Site	2	1			1	
>35	N1.3 Conserve Resources by Increasing Density	4		2		2	
	N1.5 Home Size Efficiency	8				10	
740	Enter the area of the home, in square feet						
2	Enter the number of bedrooms						
Yes	N2. Home(s)/Development Located Near Transit						
	N2.2. Within 1/2 mile of a Major Transit Stop	2	2				
<b>N3. Pedestrian and Bicycle Access</b>							
Yes	N3.2 Connection to Pedestrian Pathways	1	1				
Yes	N3.3 Traffic Calming Strategies	2	2				
Yes	N3.5 Bicycle Storage for Residents	1	1				
1 space per unit	N3.7 Reduced Parking Capacity	2	2				
<b>N4. Outdoor Gathering Places</b>							
Yes	N4.1 Public or Semi-Public Outdoor Gathering Places for Residents	1	1				
Yes	N4.2 Public Outdoor Gathering Places with Direct Access to Tier 1 Community Services	1	1				
Yes	N9.2 Community Location	2	1		1		
<b>N11. Mixed-Use Developments</b>							
Yes	N11.2 At Least 2% of Development Floor Space Supports Mixed Use	1	1				
<b>O. OTHER</b>							
Yes	O1. GreenPoint Rated Checklist in Blueprints	Y	R	R	R	R	R
Yes	O2. Pre-Construction Kickoff Meeting with Rater and Subcontractors	2		0.5		1	0.5
Yes	O7. Green Appraisal Addendum	Y	R	R	R	R	R
Yes	O11. Tobacco Free Buildings	2			2		
<b>Summary</b>							
Total Available Points in Specific Categories		375.5	46	110.5	70	95	54
Minimum Points Required in Specific Categories		50	2	25	6	6	6
<b>Total Points Targeted</b>		<b>85</b>	<b>15</b>	<b>32.5</b>	<b>11</b>	<b>16</b>	<b>10.5</b>



MANDELA STATION  
@WEST OAKLAND  
BART  
1451 7th St, Oakland,  
CA 94607



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PROJ. # 168-153 WO BART  
DATE: June 6, 2020

GREENPOINTRATED

SHEET: A-01.07

# WEST OAKLAND STATION T3



## NEW HOME RATING SYSTEM, VERSION 7.0

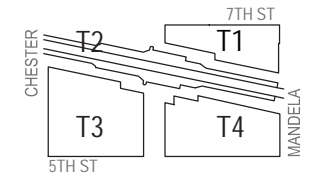
## Blueprint Scoresheet



		Points Targeted	Community	Energy	IAQ/Health	Resources	Water	Responsible Party	Blueprint Page No.	
		Possible Points								
<b>CALGreen</b>										
Yes	CALGreen Res (REQUIRED)	4		1	1	1	1			
<b>C. LANDSCAPE</b>										
Yes	C1. Plants Grouped by Water Needs (Hydrozoning)	1					1			
<b>C3. Resource Efficient Landscapes</b>										
Yes	C3.1 No Invasive Species Listed by Cal-IPC	1				1				
Yes	C3.3 Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species	1					3			
<b>E. EXTERIOR</b>										
Yes	E4. Durable and Non-Combustible Cladding Materials	1				1				
<b>E5. Durable Roofing Materials</b>										
Yes	E5.2 Roofing Warranty for Shingle Roofing	Y	R	R	R	R	R			
<b>F. INSULATION</b>										
<b>F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content</b>										
Yes	F1.1 Walls and Floors	1				1				
<b>F2. Insulation that Meets the CDPH Standard Method—Residential for Low Emissions</b>										
Yes	F2.1 Walls and Floors	1			1					
Yes	F2.2 Ceilings	1			1					
<b>G. PLUMBING</b>										
<b>G2. Install Water-Efficient Fixtures</b>										
Yes	G2.1 WaterSense Showerheads 1.8 gpm with Matching Compensation Valve	2					2			
Yes	G2.2 WaterSense Bathroom Faucets with 1.0gpm or less	1					1			
1.28 gpf	G2.3 WaterSense Toilets with a Maximum Performance (MaP) Threshold of No Less Than 500 Grams 1.28gpf OR 1.1 gpf	1					2			
<b>H. HEATING, VENTILATION, AND AIR CONDITIONING</b>										
<b>H6. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality</b>										
Yes	H6.1 Meet ASHRAE Standard 62.2-2010 Ventilation Residential Standards	Y	R	R	R	R	R			
<b>J. BUILDING PERFORMANCE AND TESTING</b>										
<b>J5. Building Performance Exceeds Title 24 Part 6</b>										
Option 1: Compliance Over Title 24	J5.1 Home Outperforms Title 24	25		25+						
<b>N. COMMUNITY</b>										
<b>N1. Smart Development</b>										
Yes	N1.1 Infill Site	2	1			1				
<b>N2. Home(s)/Development Located Near Transit</b>										
Yes	N2.2. Within 1/2 mile of a Major Transit Stop	2	2							
<b>N3. Pedestrian and Bicycle Access</b>										
	N3.1 Pedestrian Access to Services Within 1/2 Mile of Community Services	2	2							
10	Enter the number of Tier 1 services									
10	Enter the number of Tier 2 services									
Yes	N9.2 Community Location	2	1		1					
<b>O. OTHER</b>										
Yes	O1. GreenPoint Rated Checklist in Blueprints	Y	R	R	R	R	R			
Yes	O2. Pre-Construction Kickoff Meeting with Rater and Subcontractors	2		0.5		1	0.5			
Yes	O7. Green Appraisal Addendum	Y	R	R	R	R	R			
<b>P. DESIGN CONSIDERATIONS</b>										
<b>P3. Commissioning</b>										
Yes	P3.1 Design Phase	2		1	1					
Yes	P3.2 Construction Phase	3		2	1					
Yes	P3.3 Post-Construction Phase	3		2	1					
<b>Summary</b>										
Total Available Points in Specific Categories		375.5	46	110.5	70	95	54			
Minimum Points Required in Specific Categories		50	2	25	6	6	6			
<b>Total Points Targeted</b>		<b>58</b>	<b>6</b>	<b>31.5</b>	<b>7</b>	<b>6</b>	<b>7.5</b>			



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PROJ. # 168-153 WO BART  
DATE: June 6, 2020

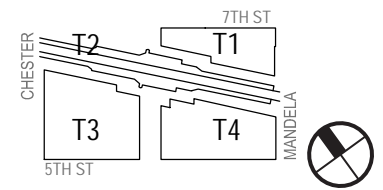
GREENPOINT RATED

SHEET: A-01.08



**MANDELA STATION  
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BART**

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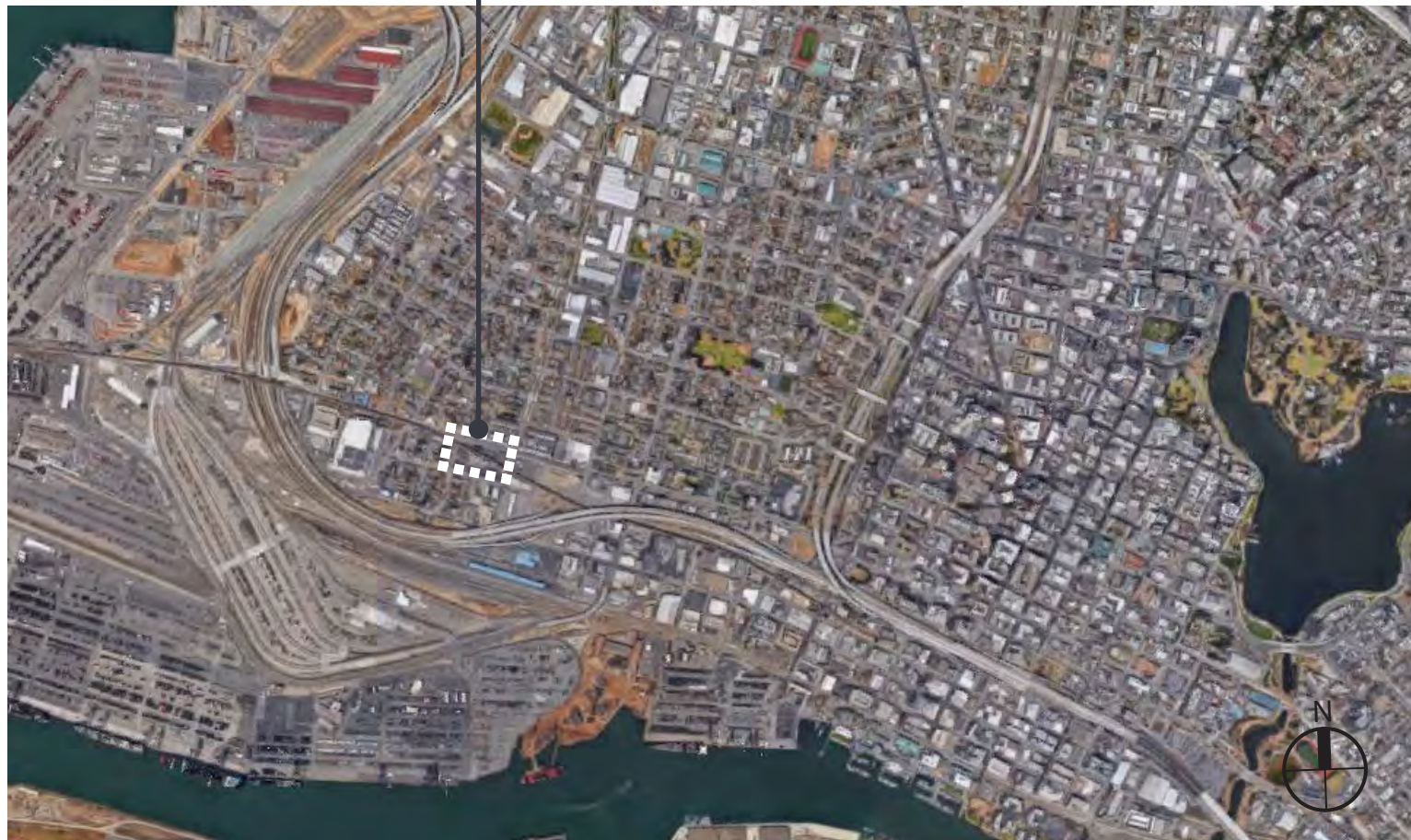


PROJ. # 168-153 WO BART  
DATE: July 24, 2020

**CONTEXT  
MAP**

SHEET: A-01.09

SITE LOCATION

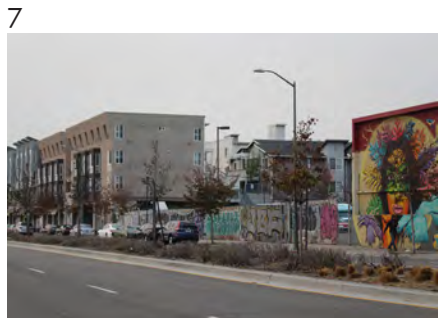
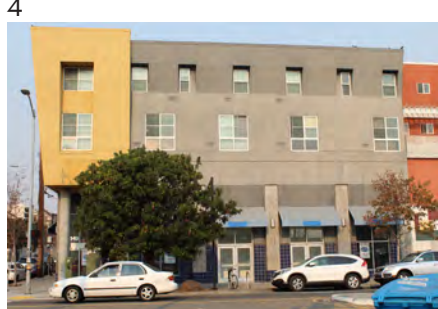


LOCATION OF PROJECT SITE WITHIN WEST OAKLAND NEIGHBORHOOD

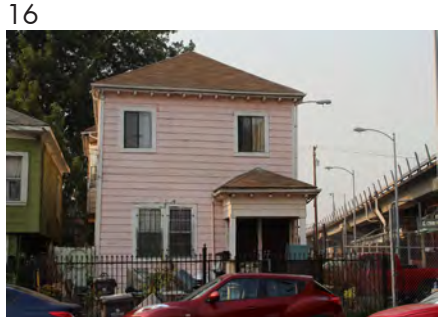
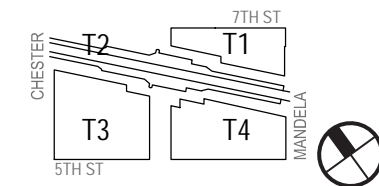


5 ACRE PROJECT SITE





SITE



**MANDELA STATION  
@WEST OAKLAND  
BART**  
1451 7th St, Oakland,  
CA 94607

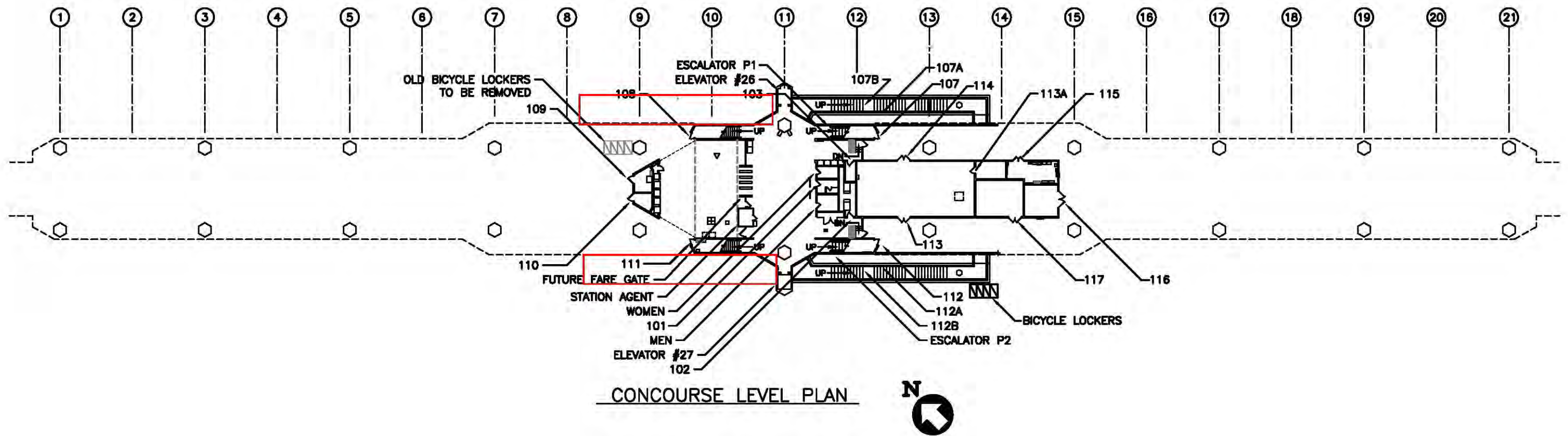
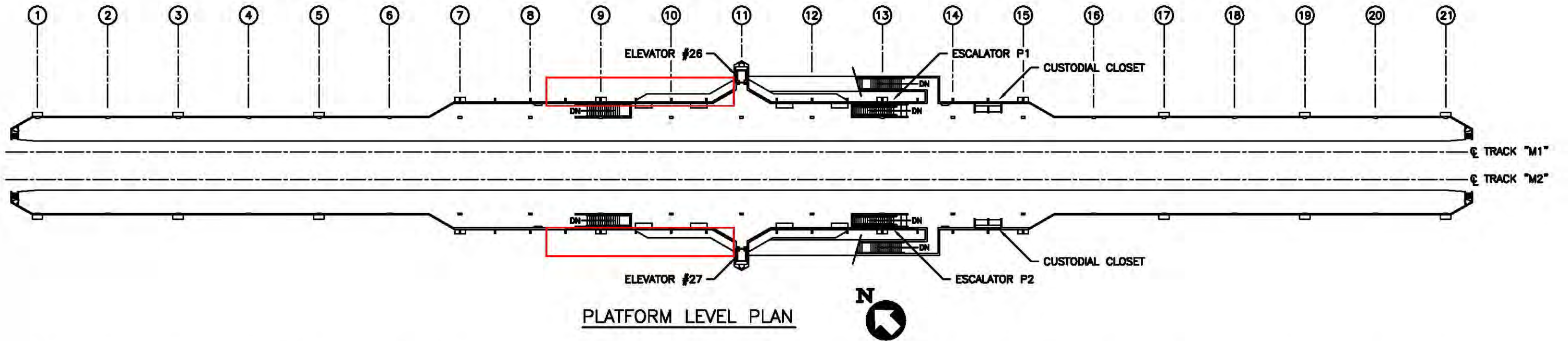
**JRDV URBAN  
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LANDSCAPE ARCHITECTS

PROJ. # 168-153 WO BART  
DATE: July 24, 2020

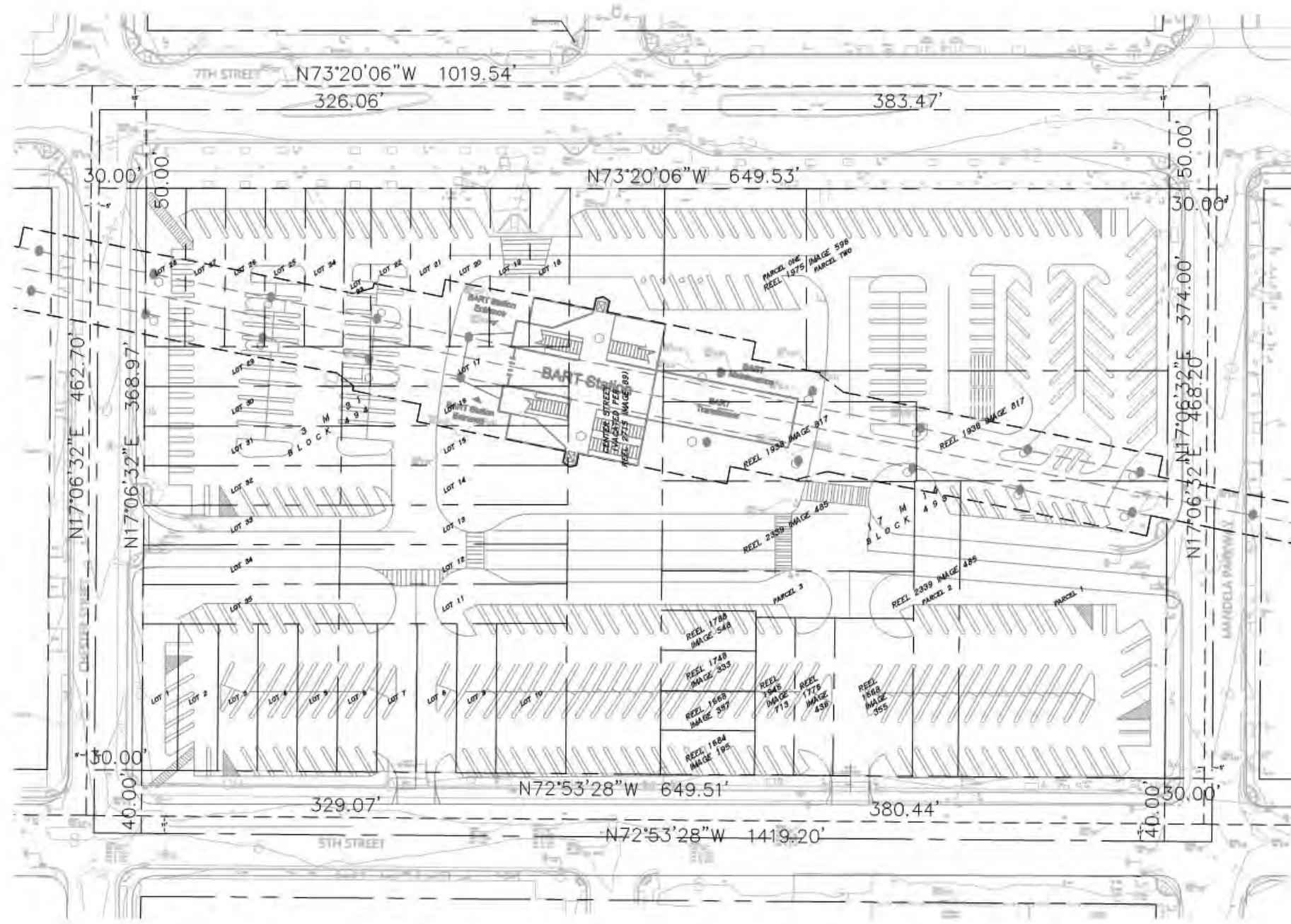
**CONTEXT  
PHOTOS**

SHEET: A-01.10

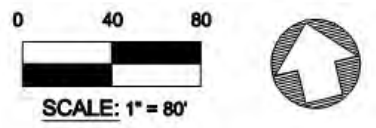


DOOR/ROOM LEGEND					
DR.NO.	USE	DR.NO.	USE	DR.NO.	USE
101	STAFF RESTROOM	109	CUSTODIAN	113A	BATTERY RM.
102	ELEV. EQUIP. RM.	110	STAFF BREAKROOM	114	TRAIN CONTROL
103	STORAGE	111	ELECT. EQUIP.	115	ELECT. EQUIP.
107	STORAGE	112	STORAGE	116	TRANSFORMER
107A	GATE	112A	GATE	117	FAN ROOM
107B	STORAGE	112B	STORAGE		
108	STORAGE	113	TRAIN CONTROL		

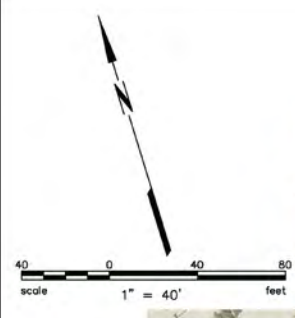
**WEST OAKLAND STATION - OWS  
"M" LINE - STATION M-10  
FLOOR PLANS**



**NOTE:**  
 EXISTING BOUNDARY FROM RECORD INFORMATION FOR PLANNING PURPOSES ONLY



Date:	12/06/2018	No.:	
Scale:	AS SHOWN	Design:	N/A
Drawn:	ADD	Approved:	KS
Job No.:	180384	Job No.:	180384
Revisions:		Drawing Number:	180384
			1 OF 5



**BASIS OF BEARINGS**

THE BEARING OF NORTH 72°53'28" WEST BETWEEN FOUND MONUMENTS ON 5TH AVENUE BETWEEN HENRY STREET AND KIRKHAM STREET AS SHOWN ON THAT RECORD OF SURVEY, R/S 1687, FILED AUGUST 7, 2000 IN BOOK 25 OF RECORDS OF SURVEY AT PAGES 58-69, INCLUSIVE, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.

**BENCHMARK**

THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H." ELEVATION 10.784 FEET (DATUM = MEAN SEA LEVEL).

**NOTES**

1. THE SURVEY WAS CONDUCTED IN NOVEMBER OF 2018.
2. ALL UNITS ARE IN US SURVEY FEET AND DECIMALS THEREOF.
3. THE TOPOGRAPHIC MAPPING SHOWN HEREON IS A WORKING DOCUMENT. THE TOPOGRAPHIC SURVEY AND MAPPING IS IN PROGRESS AND HAS NOT BEEN COMPLETED OR FINALIZED.

**SURVEYOR'S STATEMENT:**

THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE PROFESSIONAL LAND SURVEYOR'S ACT.

KEVIN STEIN, P.L.S. NO. 9028 DATE: \_\_\_\_\_

**PROFESSIONAL LAND SURVEYOR**  
**PRELIMINARY**  
**PROGRESS PRINT**  
DATE: 12/06/2018

DRAWING NAME: K:\WORK\180384\180384\_0001\180384\_0001\_0001.dwg  
DATE: 12/06/2018  
DRAWN BY: KS



TOPOGRAPHIC AND BOUNDARY SURVEY  
WEST OAKLAND BART

Revisions	
No.	Description

Date:	12/06/2018
Scale:	AS SHOWN
Design:	N/A
Drawn:	ADD
Approved:	KS
Job No.:	180384

**SYMBOLS & LEGEND**

**EXISTING**

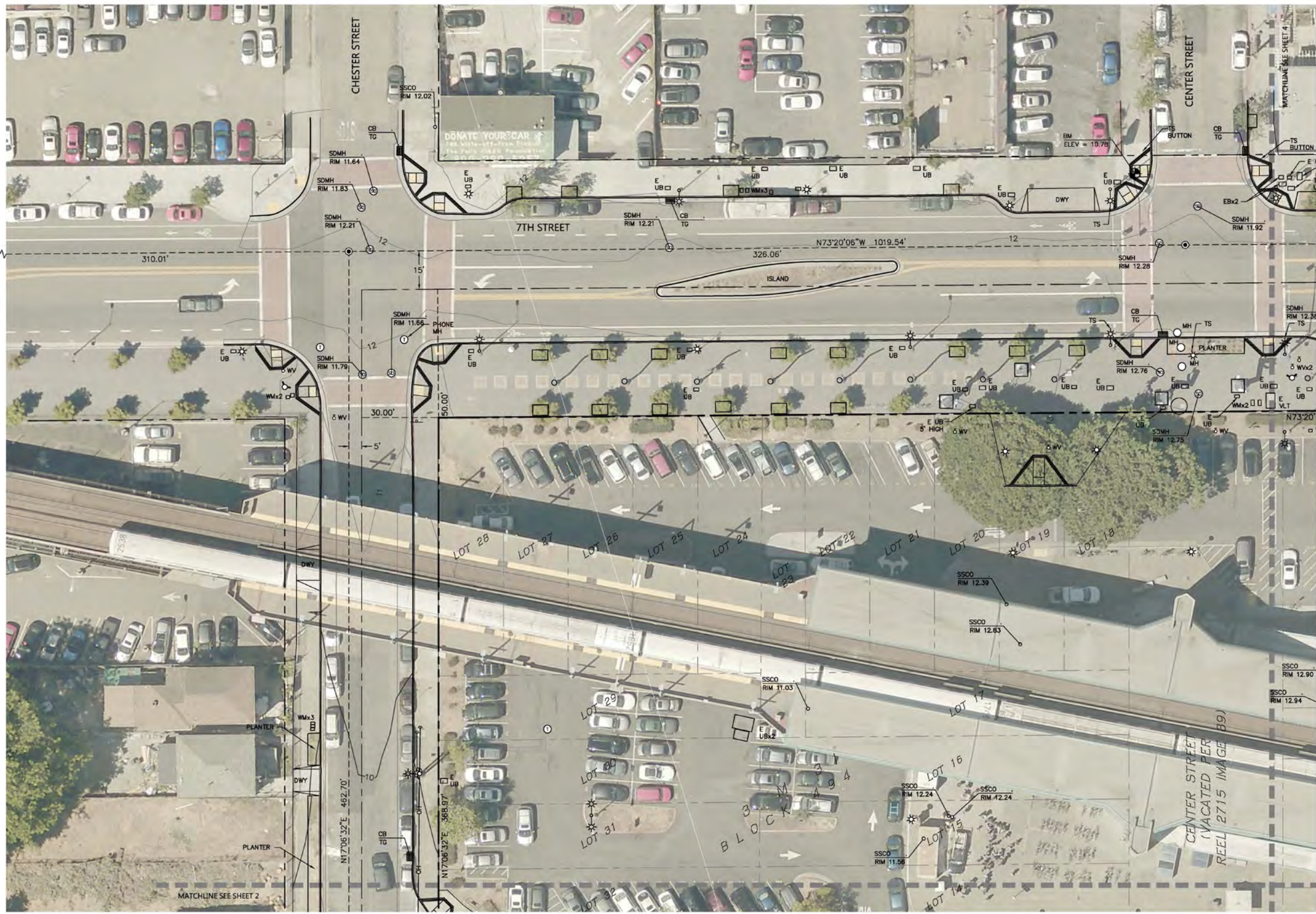
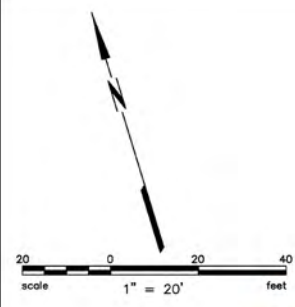
- FOUND CITY OF OAKLAND MONUMENT
- ⊕ VALVE
- ⊕ FIRE HYDRANT
- ⊕ BACKFLOW PREVENTION DEVICE
- ⊕ RISER
- ⊕ SIGN
- ⊕ STREET LIGHT (UNLESS NOTED OTHERWISE)
- ⊕ LIGHT POLE (UNLESS NOTED OTHERWISE)
- ⊕ GUY ANCHOR
- ⊕ UTILITY POLE
- ⊕ TELEPHONE MANHOLE
- ⊕ MANHOLE

**SUBJECT PROPERTY**

- ▬ ADJOINING LOT CENTERLINE
- ▬ SURVEY TIE
- ▬ STORM DRAIN
- ▬ SANITARY SEWER
- ▬ OVERHEAD UTILITY LINE
- ▬ CONCRETE
- ▬ DETECTABLE WARNING

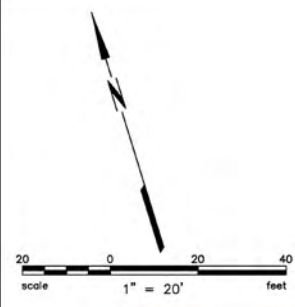
**ABBREVIATIONS**

- AC ASPHALT CONCRETE
- CA CABLE
- CB CATCH BASIN
- CO CLEAN OUT
- COMM COMMUNICATION
- CONC CONCRETE
- DI DROP INLET
- DWY DRIVEWAY
- E ELECTRIC
- EP EDGE OF PAVEMENT
- FL SURFACE FLOWLINE
- G GAS
- GYP GUY POLE
- INV BOTTOM INSIDE OF PIPE
- LG LIP OF GUTTER
- M METER
- MH MANHOLE
- OH OVERHEAD UTILITY LINE
- SD STORM DRAIN
- SL STREETLIGHT
- SS SANITARY SEWER
- TC TOP FACE OF CURB
- TEL TELECOMMUNICATION LINE
- TG TOP OF GRATE
- TS TRAFFIC SIGNAL
- TV TELEVISION
- UB UTILITY BOX
- V VALVE
- VLT VAULT
- W WATER
- WM WATER METER



DRAWING NAME: K:\2018\180384\_001\_001\_001.dwg; Drawing Title: TOPOGRAPHIC AND BOUNDARY SURVEY; User: J. B. BAKER; Date: 12/06/2018 10:00:00 AM

- SYMBOLS & LEGEND**
- EXISTING**
- FOUND CITY OF OAKLAND MONUMENT
  - VALVE
  - FIRE HYDRANT
  - BACKFLOW PREVENTION DEVICE
  - RISER
  - SIGN
  - STREET LIGHT (UNLESS NOTED OTHERWISE)
  - LIGHT POLE (UNLESS NOTED OTHERWISE)
  - GUY ANCHOR
  - UTILITY POLE
  - TELEPHONE MANHOLE
  - MANHOLE
- ABBREVIATIONS**
- AC ASPHALT CONCRETE
  - CA CABLE
  - CB CATCH BASIN
  - CO CLEAN OUT
  - COMM COMMUNICATION
  - CONC CONCRETE
  - DI DROP INLET
  - DWY DRIVEWAY
  - E ELECTRIC
  - EP EDGE OF PAVEMENT
  - FL SURFACE FLOWLINE
  - G GAS
  - GYP GUY POLE
  - INV BOTTOM INSIDE OF PIPE
  - LG LIP OF GUTTER
  - M METER
  - MH MANHOLE
  - OH OVERHEAD UTILITY LINE
  - SD STORM DRAIN
  - SL STREETLIGHT
  - SS SANITARY SEWER
  - TC TOP FACE OF CURB
  - TEL TELECOMMUNICATION LINE
  - TG TOP OF GRATE
  - TS TRAFFIC SIGNAL
  - TV TELEVISION
  - UB UTILITY BOX
  - V VALVE
  - VLT VAULT
  - W WATER
  - WM WATER METER



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TOPOGRAPHIC AND BOUNDARY SURVEY  
WEST OAKLAND BART

CALIFORNIA  
COUNTY OF ALAMEDA  
OAKLAND

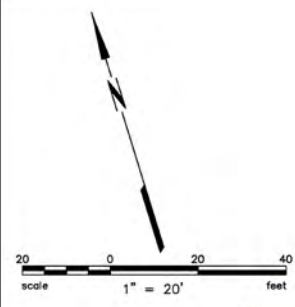
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Scale:	AS SHOWN	Revisions:	
Design:	N/A		
Drawn:	ADD		
Approved:	KS		
Job No.:	180384		
Drawing Number:	180384		

**SYMBOLS & LEGEND**

	FOUND CITY OF OAKLAND MONUMENT
	VALVE
	FIRE HYDRANT
	BACKFLOW PREVENTION DEVICE
	RISER
	SIGN
	STREET LIGHT (UNLESS NOTED OTHERWISE)
	LIGHT POLE (UNLESS NOTED OTHERWISE)
	GUY ANCHOR
	UTILITY POLE
	TELEPHONE MANHOLE
	MANHOLE
	SUBJECT PROPERTY
	ADJOINING LOT
	CENTERLINE
	SURVEY TIE
	STORM DRAIN
	SANITARY SEWER
	OVERHEAD UTILITY LINE
	CONCRETE
	DETECTABLE WARNING

**ABBREVIATIONS**

AC	ASPHALT CONCRETE
CA	CABLE
CB	CATCH BASIN
CO	CLEAN OUT
COMM	COMMUNICATION
CONC	CONCRETE
DI	DROP INLET
DWY	DRIVEWAY
E	ELECTRIC
EP	EDGE OF PAVEMENT
FL	SURFACE FLOWLINE
G	GAS
GYP	GUY POLE
INV	BOTTOM INSIDE OF PIPE
LG	LIP OF GUTTER
M	METER
MH	MANHOLE
OH	OVERHEAD UTILITY LINE
SD	STORM DRAIN
SL	STREETLIGHT
SS	SANITARY SEWER
TC	TOP FACE OF CURB
TEL	TELECOMMUNICATION LINE
TG	TOP OF GRATE
TS	TRAFFIC SIGNAL
TV	TELEVISION
UB	UTILITY BOX
V	VALVE
VL	VAULT
W	WATER
WM	WATER METER



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TOPOGRAPHIC AND BOUNDARY SURVEY  
WEST OAKLAND BART

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Scale:	AS SHOWN
Design:	N/A
Drawn:	KS
Approved:	KS
Job No.:	180384

Revisions

No.	
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Drawing Number:  
**180384**

5 OF 5

DRAWING NAME: K:\Users\BKF\My Documents\BART\_TDC\_Survey\BART\Maping\Topo\180384.dwg  
DATE: 12/06/2018 10:00:00 AM  
USER: BKF



# VESTING TENTATIVE PARCEL MAP NO. 10940

## FOR CONDOMINIUM PURPOSES

**SHEET INDEX**

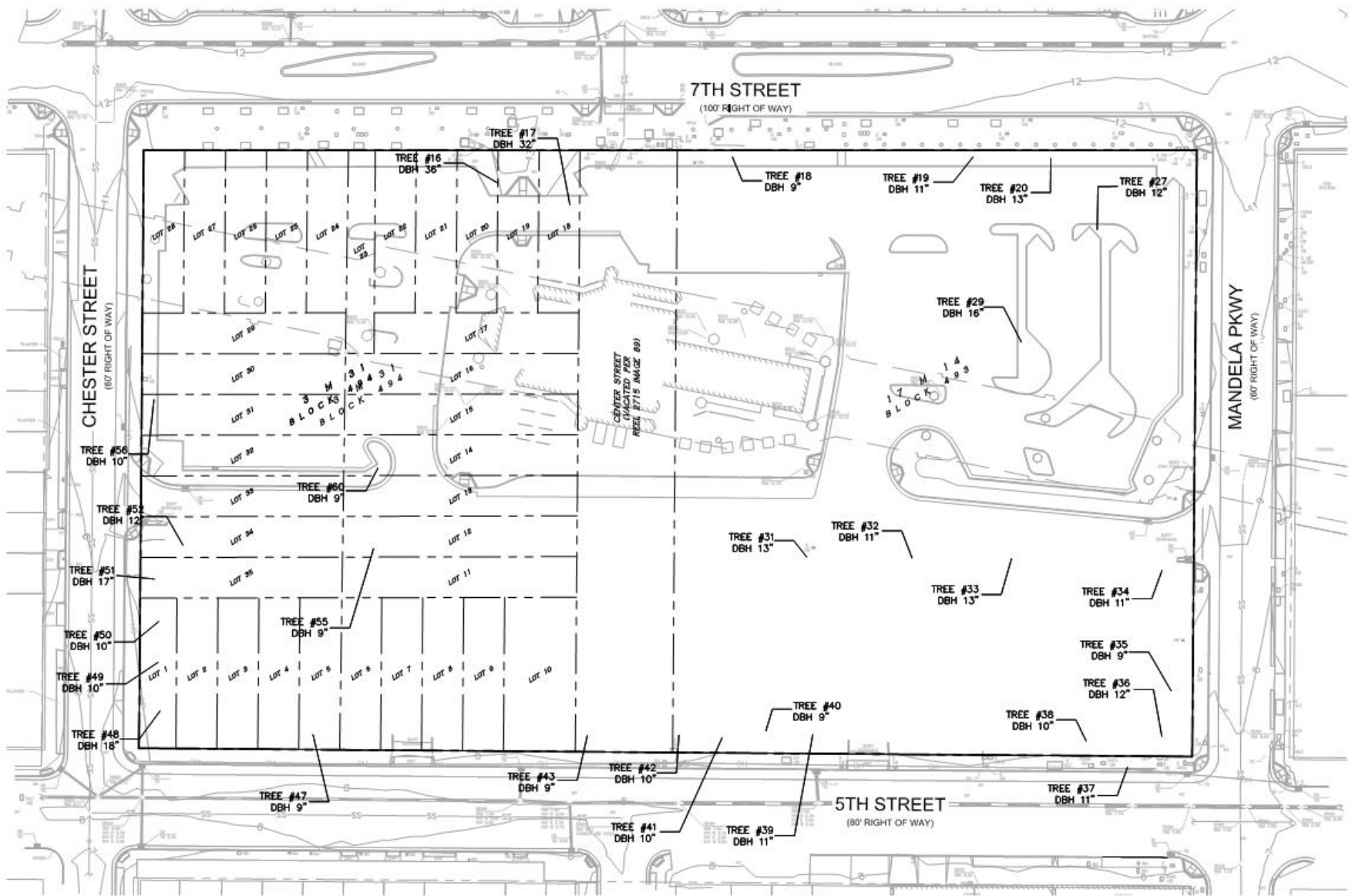
SHEET #	SHEET TITLE
1	TITLE SHEET/EXISTING SITE PLAN
2	PROPOSED SITE PLAN
3	EMERGENCY VEHICLE ACCESS EASEMENT

**SYMBOLS & LEGEND**

EXISTING	SYMBOL	DESCRIPTION
VALVE		VALVE
FIRE HYDRANT		FIRE HYDRANT
BACKFLOW PREVENTION DEVICE		BACKFLOW PREVENTION DEVICE
RISER		RISER
SIGN		SIGN
STREET LIGHT		STREET LIGHT
LIGHT POLE		LIGHT POLE
GUY ANCHOR		GUY ANCHOR
UTILITY POLE		UTILITY POLE
TELEPHONE MANHOLE		TELEPHONE MANHOLE
MANHOLE		MANHOLE
EXISTING PROPERTY LINE		EXISTING PROPERTY LINE
ADJOINING LOT CENTERLINE		ADJOINING LOT CENTERLINE
SURVEY TRIE		SURVEY TRIE
STORM DRAIN		STORM DRAIN
SANITARY SEWER		SANITARY SEWER
OVERHEAD UTILITY LINE		OVERHEAD UTILITY LINE
CONCRETE		CONCRETE
DETECTABLE WARNING		DETECTABLE WARNING
BOUNDARY LINE		BOUNDARY LINE

**ABBREVIATIONS**

AC	ASPHALT CONCRETE
CA	CABLE
CB	CATCH BASIN
CO	CLEAN OUT
COMM	COMMUNICATION
CONC	CONCRETE
DBH	DIAMETER AT BREAST HEIGHT
DI	DROP INLET
DWY	DRIVEWAY
E	ELECTRIC
EP	EDGE OF PAVEMENT
EVAE	EMERGENCY VEHICLE ACCESS EASEMENT
FL	SURFACE FLOWLINE
G	GAS
GYP	GUY POLE
INV	BOTTOM INSIDE OF PIPE
LG	LIP OF GUTTER
M	METER
MH	MANHOLE
MIN	MINIMUM
OH	OVERHEAD UTILITY LINE
PL	PROPERTY LINE
R	RADIUS
SD	STORM DRAIN
SF	SQUARE FEET
SL	STREETLIGHT
SS	SANITARY SEWER
(T)	TOTAL
TC	TOP FACE OF CURB
TEL	TELECOMMUNICATION LINE
TG	TOP OF GRATE
TV	TRAFFIC SIGNAL
TS	TELEVISION
UB	UTILITY BOX
V	VALVE
VT	VAULT
W	WATER
WM	WATER METER



**PROPERTY ADDRESS:**  
1451 7TH STREET  
OAKLAND, CA 94607

**OWNER AND SUBDIVIDER:**  
SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT  
300 LAKESIDE DRIVE, 22ND FLOOR  
OAKLAND, CA 94612  
PHONE: 510-597-6300

**BASIS OF BEARINGS**

THE BEARING OF NORTH 72°53'28" WEST BETWEEN FOUND MONUMENTS ON 5TH STREET BETWEEN HENRY STREET AND KIRKHAM STREET AS SHOWN ON THAT RECORD OF SURVEY, R/S 1687, FILED AUGUST 7, 2000 IN BOOK 25 OF RECORDS OF SURVEY AT PAGES 58-59, INCLUSIVE, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.

**BENCHMARK**

THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H," ELEVATION 10.784 FEET (DATUM = MEAN SEA LEVEL).

**NOTES**

- BASED ON INFORMATION CONTAINED IN THE PRELIMINARY TITLE REPORT, ORDER NUMBER NCS-891862-SC, DATED JANUARY 31, 2018, PROVIDED BY FIRST AMERICAN TITLE COMPANY, THE SUBJECT PROPERTY IS OWNED BY SFBART BEING ASSESSOR PARCEL NUMBERS 004-0077-003, 004-0071-003, AND THE VACATION OF CENTER STREET; THERE ARE 36 PARCELS, NOT INCLUDING THE VACATION OF CENTER STREET, THE EXISTING PARCELS WILL BE RECONFIGURED TO CREATE 3 LOTS AND ONE REMAINDER PARCEL VIA PARCEL MAP TO BE PROCESSED.
- THE INTENT OF THE MAP IS TO MERGE THE 36 PARCELS AND THE VACATED CENTER STREET INTO THE FOUR CONSOLIDATE PARCELS DEFINING THE KEY COMPONENTS OF THE NEW DEVELOPMENT: LOT 1 MARKET RATE RESIDENTIAL TOWER, LOT 2 OFFICE BUILDING, LOT 3 AFFORDABLE HOUSING, AND THE DESIGNATED REMAINDER PARCEL FOR THE EXISTING WEST OAKLAND BART STATION AND NEW ENTRY PLAZAS.
- THE CONTROL SURVEY WAS CONDUCTED IN NOVEMBER OF 2018.
- ALL UNITS ARE IN US SURVEY FEET AND DECIMALS THEREOF.
- THE TOPOGRAPHIC SURVEY IS BASED ON A FIELD SURVEY COMPLETED IN DECEMBER 2018.
- ALL BUILDINGS TO THE NORTH OF THE BART STATION SHALL BE 30' FROM THE BART PLATFORM, AND ALL BUILDINGS TO THE SOUTH OF THE BART STATION SHALL BE 33' FROM THE BART PLATFORM.
- ALL EMERGENCY VEHICLE ACCESS EASEMENTS SHALL BE 26' WIDE.
- LOTS 1, 2 AND 3 ARE ALLOWED UP TO FOUR COMMERCIAL CONDOMINIUMS ON EACH LOT.
- MULTIPLE MAPS MAY BE FILED FOR THIS PROJECT.

**SURVEYOR'S STATEMENT:**

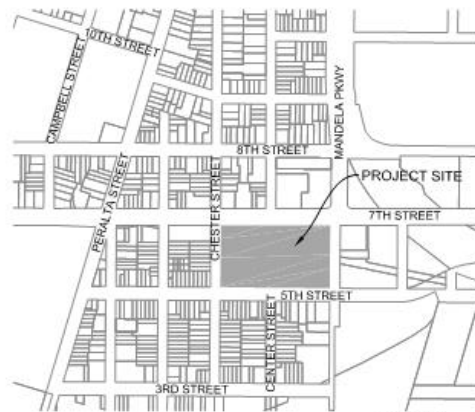
THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE PROFESSIONAL LAND SURVEYOR'S ACT.

PAUL KITTRIDGE  
PROJECT MANAGER  
P.L.S. # 5790



DATE:

EXISTING CONDITIONS  
SCALE 1"=40'



KEY MAP  
SCALE 1"=500'



Tree number	DBH (in)	Species Name
16	36	Pinus Pinea
17	32	Pinus Pinea
20	13	Liriodendron Tulipifera
28	12	Maytenus Boaria
29	16	Maytenus Boaria
31	13	Liriodendron Tulipifera
33	13	Liriodendron Tulipifera
36	12	Liriodendron Tulipifera
48	18	Platanus X Acerifolia
51	17	Platanus X Acerifolia
52	12	Liriodendron Tulipifera

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PLOT DATE: 04-06-20 PLOTTED BY: mitr

VESTING TENTATIVE PARCEL MAP NO. 10940  
TITLE SHEET

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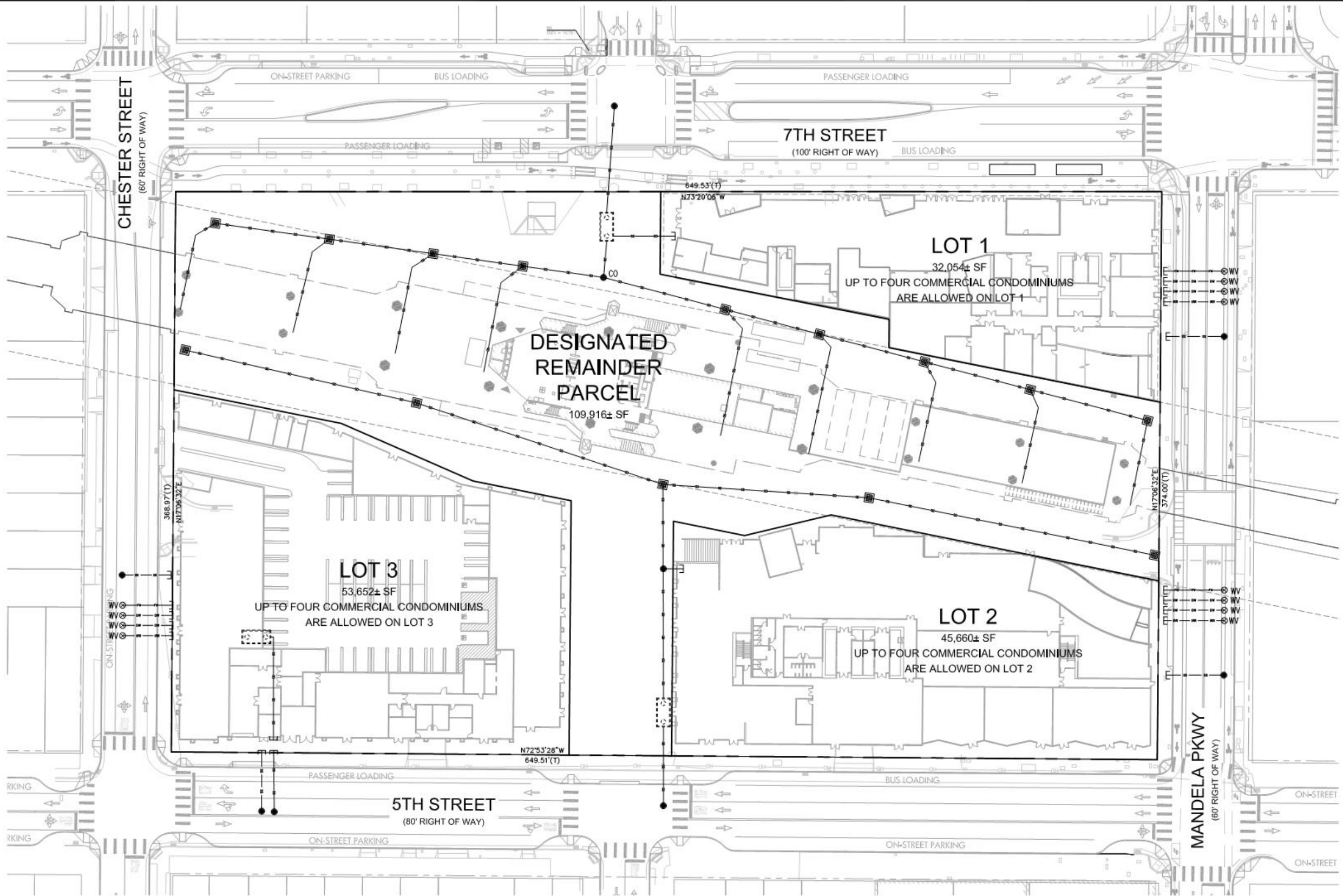
Revisions	No.	DATE	BY	DESCRIPTION
AS SHOWN		04/06/2020		
CC				
ES				

TENTATIVE  
PARCEL MAP  
C-1.01A

Revisions	No.	Date
AS SHOWN		04/06/2020
BY CC		
BY CC		
BY ES		
No. 20180384		

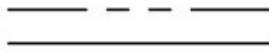
TENTATIVE  
PARCEL MAP  
C-1.01B

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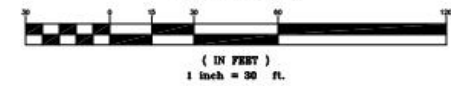


**LEGEND**

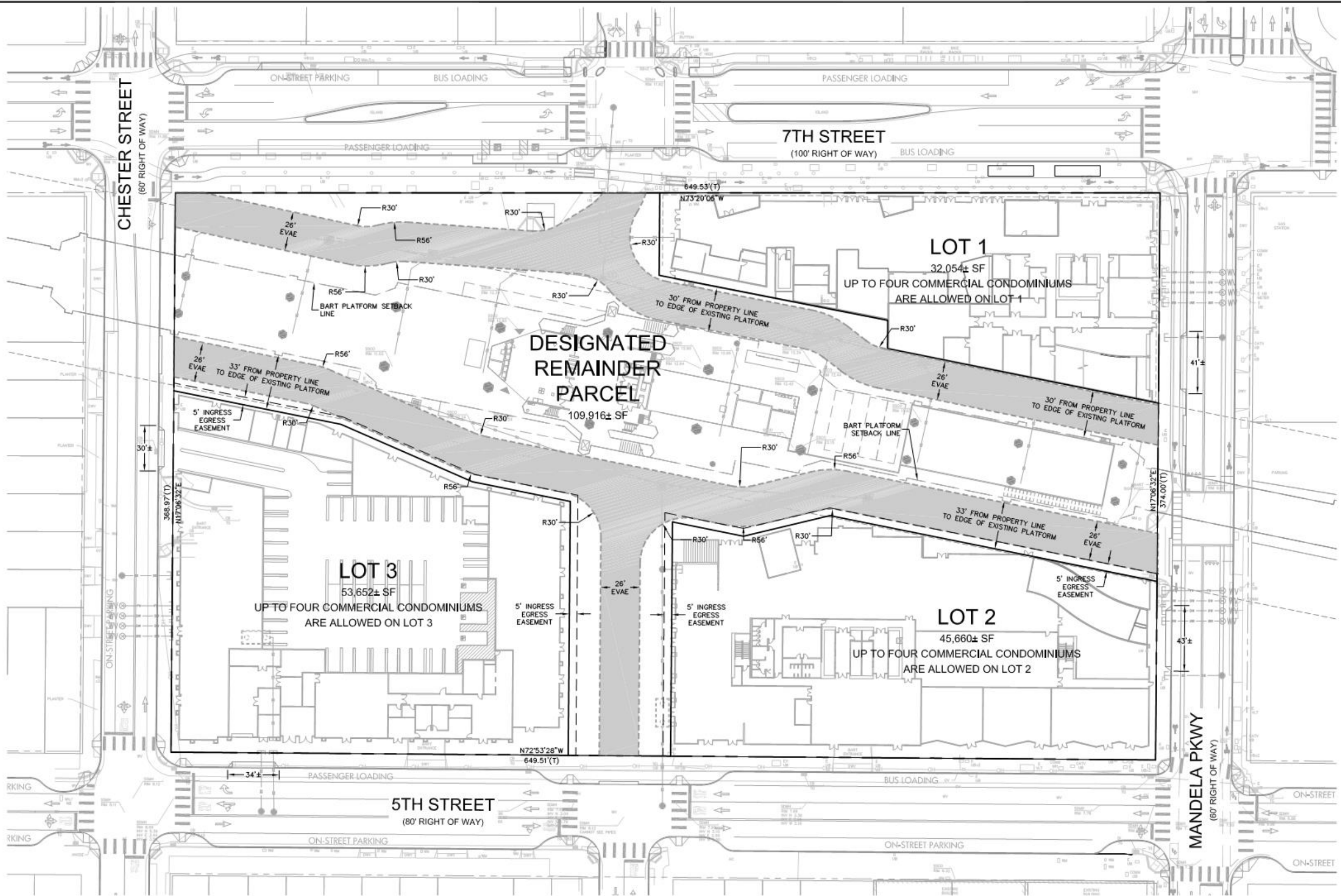
BOUNDARY LINE  
PROPOSED PARCEL LINE



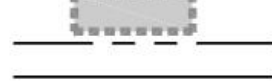
GRAPHIC SCALE



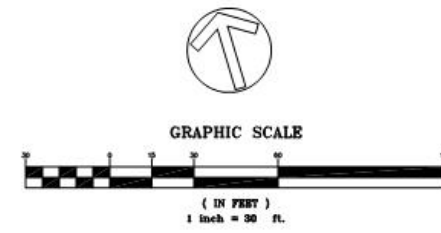
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PLOT DATE: 04-06-20 PLOTTED BY: mitr



- LEGEND**
- PROPOSED EVAE
  - BOUNDARY LINE
  - PROPOSED PARCEL LINE

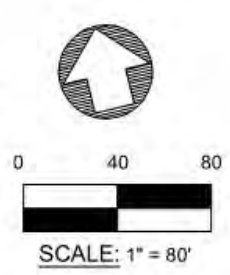
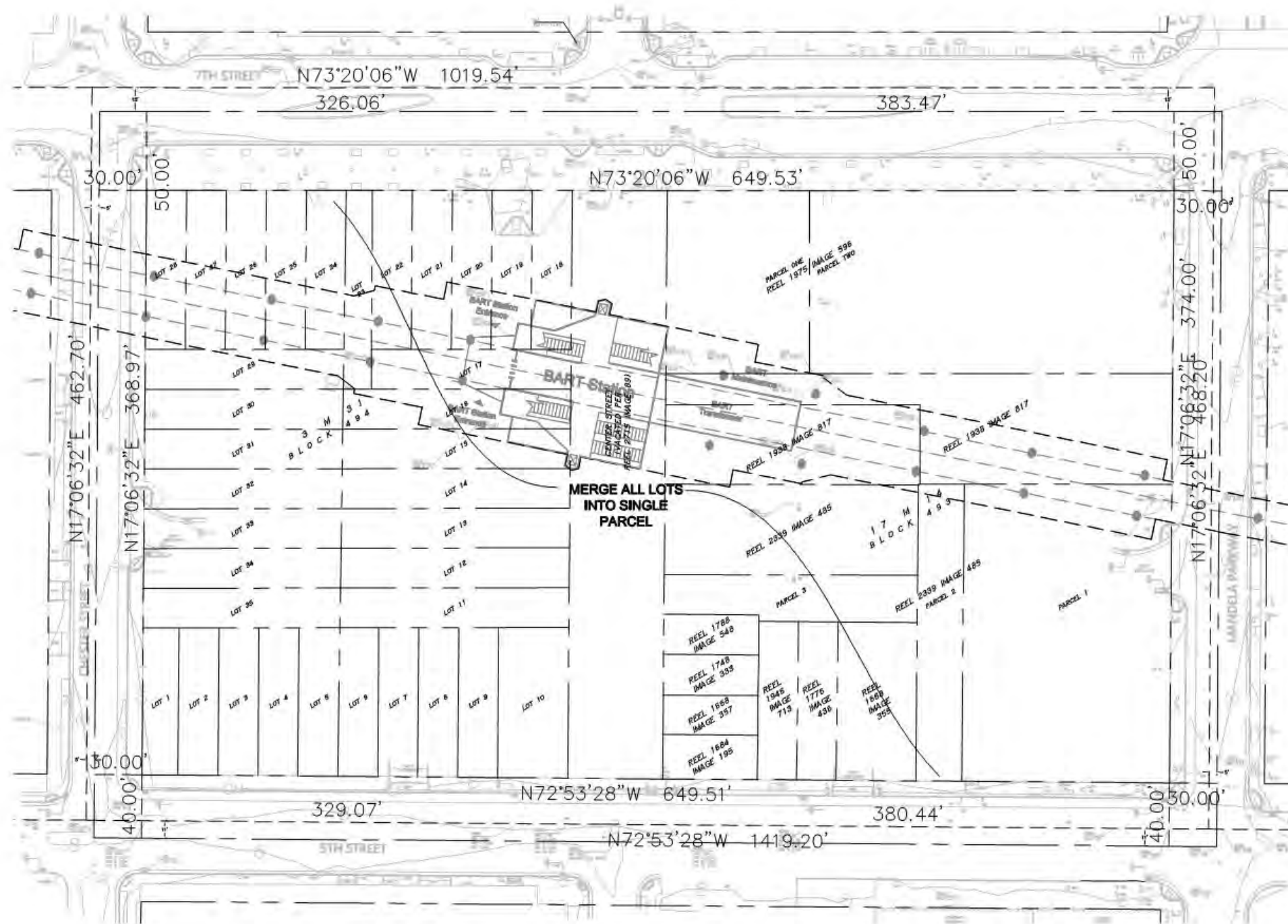


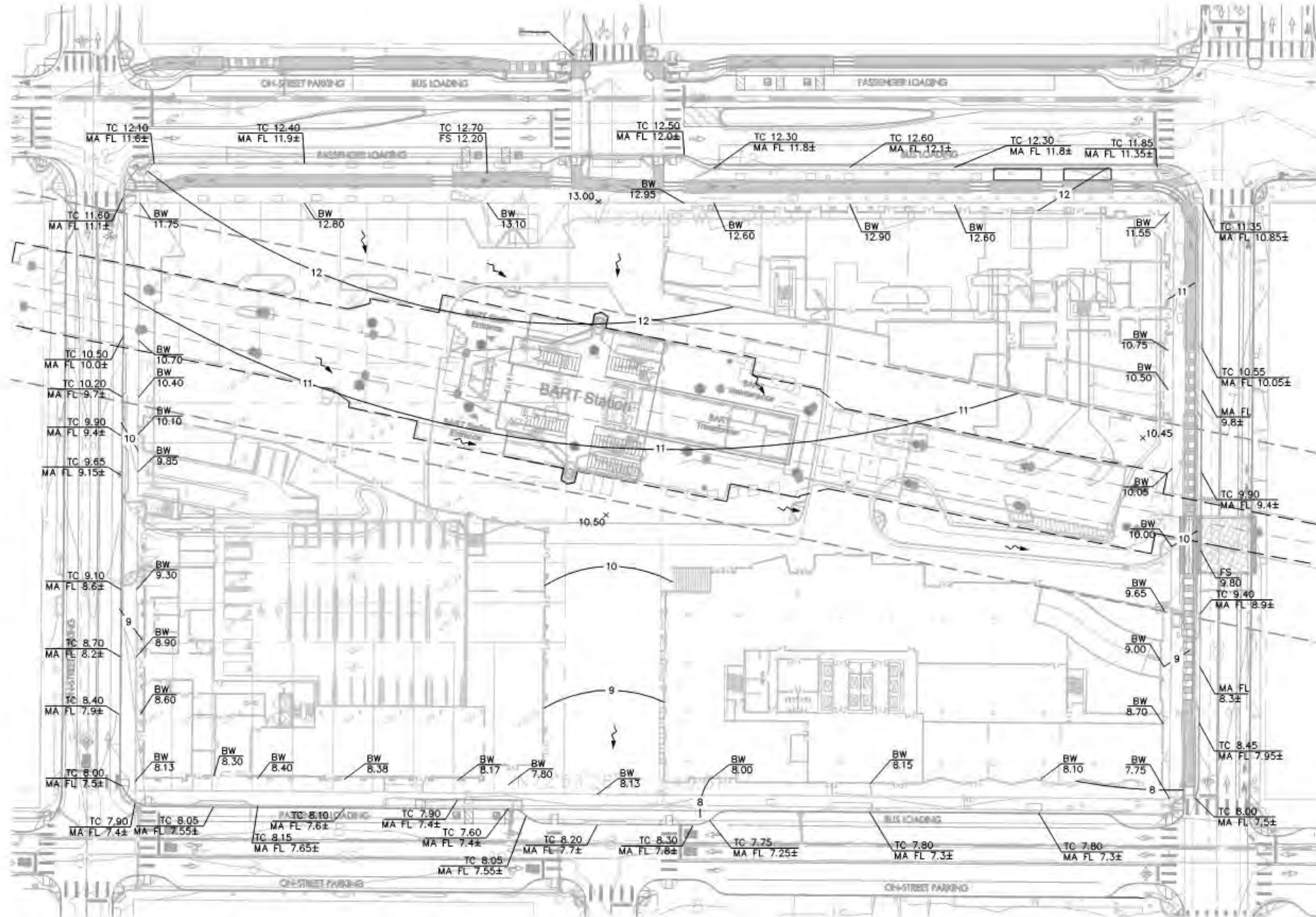
**EMERGENCY VEHICLE ACCESS EASEMENT**



Revisions	No.	Date
AS SHOWN		04/06/2020
CC		
CC		
ES		
ES		
ES		

TENTATIVE  
PARCEL MAP  
C-1.01C





**ABBREVIATIONS:**

FF	FINISHED FLOOR
FG	FINISHED GROUND
FS	FINISHED SURFACE
FL	FLOW LINE
MA	MATCH
TC	TOP OF CURB
TS	TOP OF STEP
TW	TOP OF WALL

**LEGEND:**

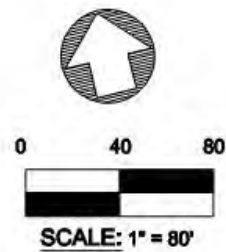
- DIRECTION OF PROPOSED DRAINAGE
- GRADE BREAK
- 5.00 CONTOUR

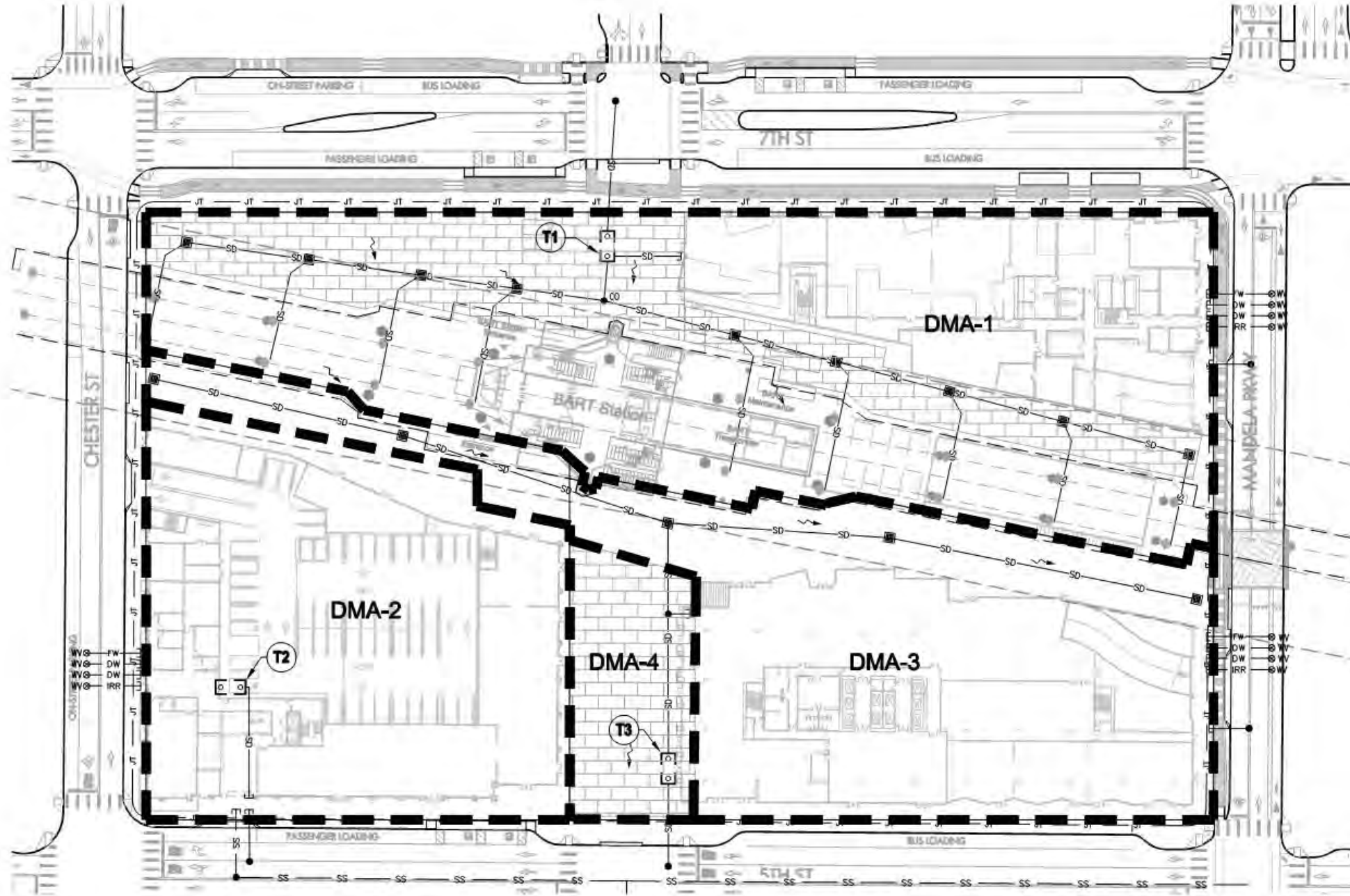
**GRADING NOTES:**

- TC ELEVATIONS ARE 6" ABOVE ADJACENT PAVEMENT FL ELEVATIONS, UNLESS OTHERWISE NOTED ON PLAN.

**EARTHWORK SUMMARY:**

TOTAL EXCAVATION: 51,303 CUBIC YARDS  
 TOTAL FILL: 0 CUBIC YARDS  
 TOTAL OFFHAUL: 51,303 CUBIC YARDS



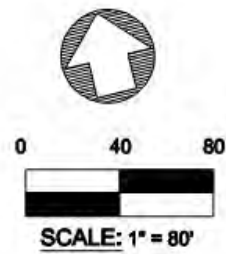


**LEGEND:**

- DRAINAGE MANAGEMENT AREA (DMA)
- PERMEABLE PAVING
- FILTER VAULT  
SEE SHEET C3.01
- TREATMENT AREA LABEL

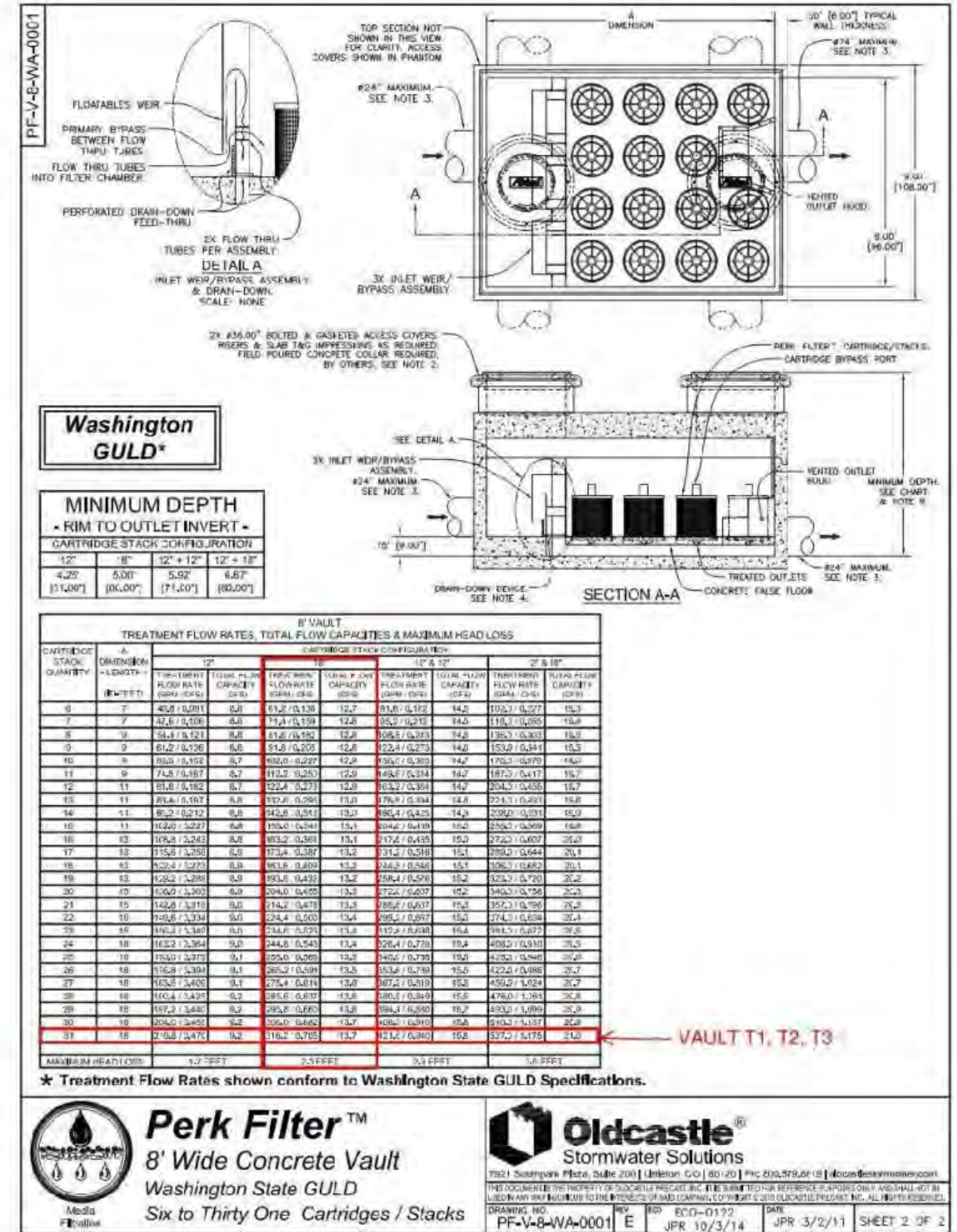
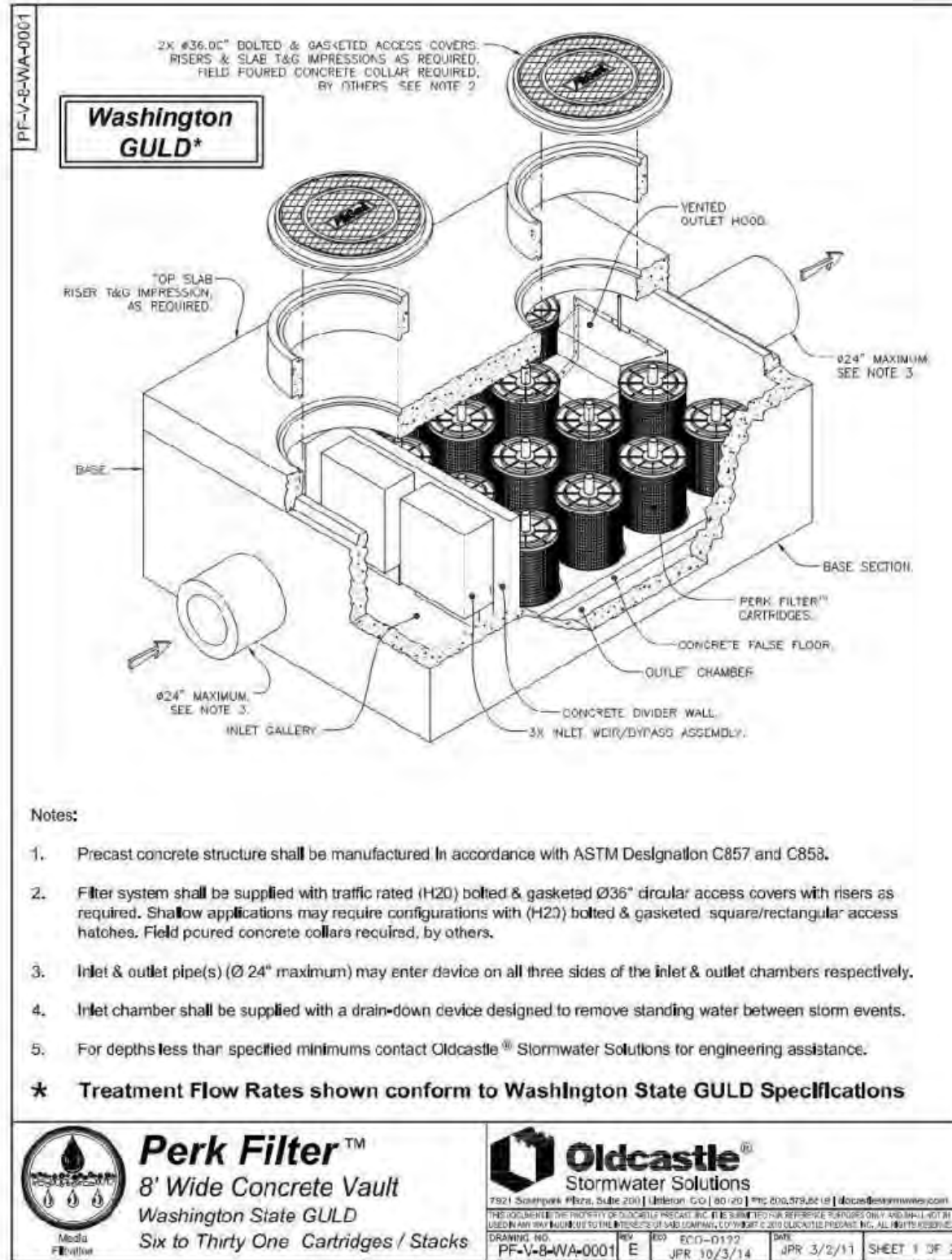
**STORMWATER MANAGEMENT NOTES:**

1. THE TREATMENT VAULTS WERE SELECTED BASED ON 243,131 SF OF IMPERVIOUS SURFACE AND A TREATMENT INTENSITY OF 0.2 IN/HR. SEE OLD CASTLE WASHINGTON CULD PF-V-8-WA-0001 PERK FILTER DETAIL ON SHEET C3.01. ALL FILTER VAULTS SHOWN ARE COMPRISED OF (31) - 18" CARTRIDGES.



TOTAL SITE AREA (SF)	TOTAL LAND AREA DISTURBED (SF)	TOTAL EXISTING/PRE-PROJECT IMPERVIOUS SURFACE (SF)	REPLACED IMPERVIOUS SURFACE (SF)	NEW IMPERVIOUS SURFACE (SF)	TOTAL POST-PROJECT IMPERVIOUS SURFACE (SF)
243,130	243,130	212,865	177,410	25,135	202,545

DMA	Impervious Area (SF)	Pervious Area (SF)	Treatment Type	Designated Treatment Area
1	74,600	28,445	Filter Vault	T1
2	58,278	0	Filter Vault	T2
3	69,667	0	Filter Vault	T3
4	0	12,140	Permeable Paving	T4

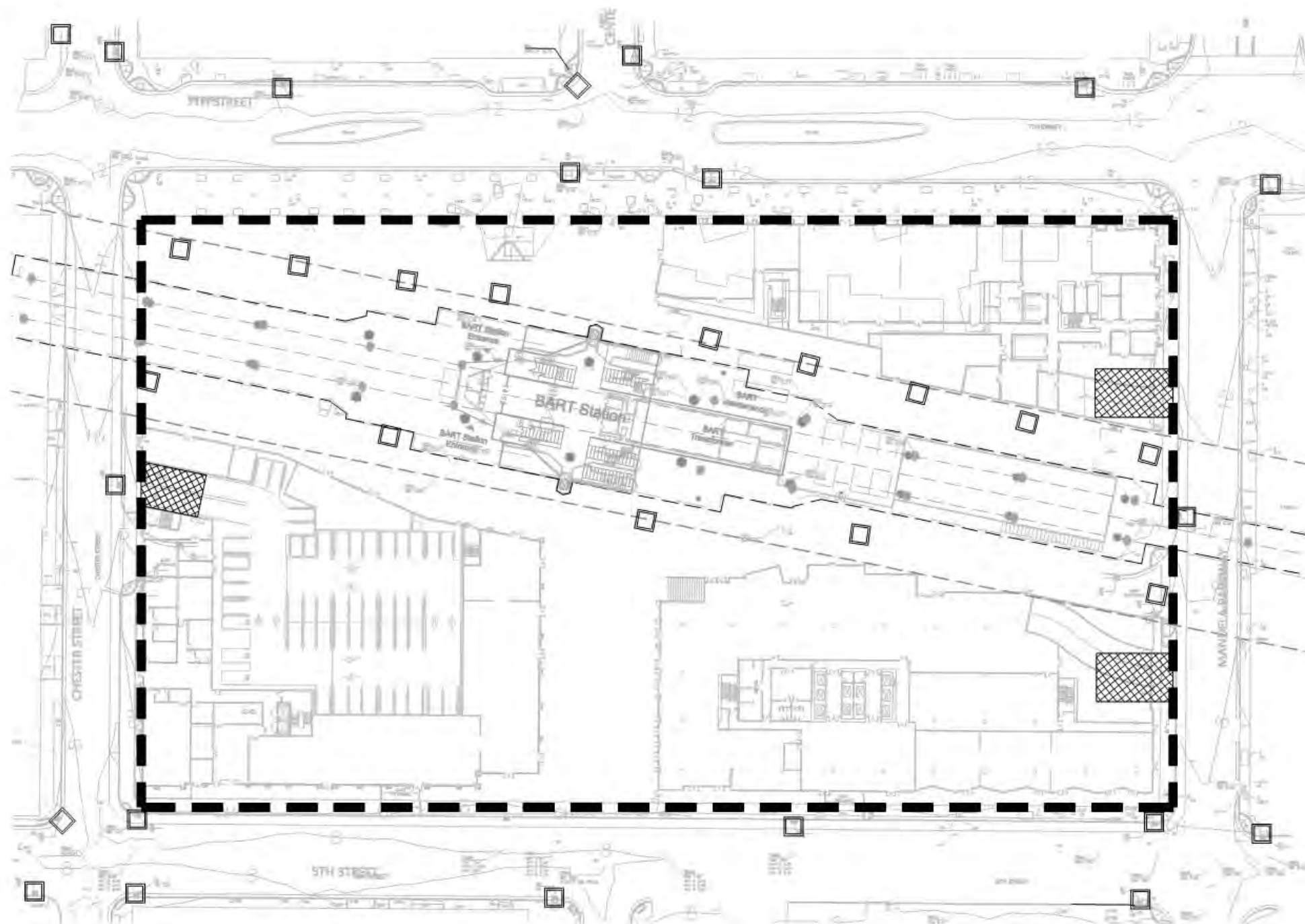


## Special Projects

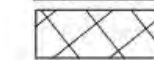


### Non-LID Treatment Summary

Category	Impervious Area Created/Replaced (acres)	Site Coverage (%)	Project Density or FAR	Density/Criteria	Allowable Credit (%)	Applied Credit (%)
A			N.A.	N.A.	100%	
B				Res $\geq$ 50 DU/ac or FAR $\geq$ 2:1	50%	
				Res $\geq$ 75 DU/ac or FAR $\geq$ 3:1	75%	
				Res $\geq$ 100 DU/ac or FAR $\geq$ 4:1	100%	
C				<b>Location credit (select one)<sup>5</sup>:</b>		
				Within 1/4 mile of transit hub	50%	50%
				Within 1/2 mile of transit hub	25%	
				Within a planned PDA	25%	
				<b>Density credit (select one):</b>		
				Res $\geq$ 30 DU/ac or FAR $\geq$ 2:1	10%	
				Res $\geq$ 60 DU/ac or FAR $\geq$ 4:1	20%	
				Res $\geq$ 100 DU/ac or FAR $\geq$ 6:1	30%	30%
				<b>Parking credit (select one):</b>		
				$\geq$ 10% at-grade surface parking <sup>6</sup>	10%	
No surface parking	20%	20%				
<b>TOTAL TOD CREDIT =</b>					<b>100%</b>	





**EROSION CONTROL LEGEND:**

-  STABILIZED CONSTRUCTION ENTRANCE (TC-1)\*\* WITH ENTRANCE/OUTLET TIRE WASH (TC-3)\*\*
-  STORM DRAIN INLET PROTECTION (SC-10)\*\*
-  FIBER ROLL (SC-05)\*\*

\*\*REFER TO SHEET C4.01 FOR DETAILS

**EROSION CONTROL NOTES:**

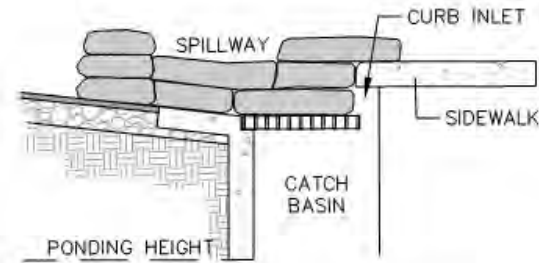
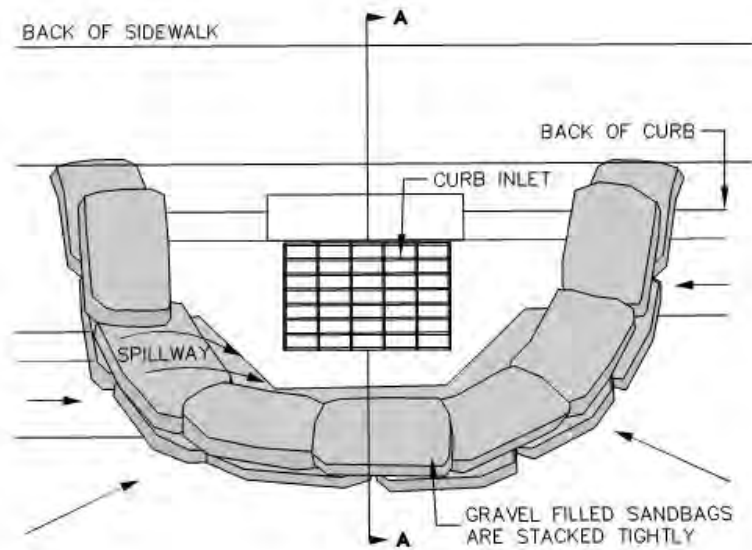
1. SITE ACCESS SHOWN ON THIS PLAN IS PROVIDED FOR INFORMATION PURPOSES ONLY. CONTRACTOR SHALL LOCATE CONSTRUCTION ACCESS DRIVEWAYS AS NECESSARY.
2. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN EFFECT AND MAINTAINED BY THE CONTRACTOR ON A YEAR-ROUND BASIS UNTIL ALL DISTURBED AREAS ARE STABILIZED UNLESS OTHERWISE PERMITTED BY THE COUNTY INSPECTOR.
3. ALL INLETS RECEIVING STORM WATER RUNOFF FROM THE PROJECT AREA MUST BE EQUIPPED WITH REQUIRED INLET PROTECTION.
4. ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIALS AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT LADEN RUNOFF ENTERING THE STORM DRAIN SYSTEM.
5. STOCKPILED EARTHEN MATERIAL SHALL BE EITHER COVERED WITH A TARP OR WATERED SUFFICIENTLY TO ELIMINATE DUST.
6. REFERENCE: "CALIFORNIA STORM WATER BEST MANAGEMENT PRACTICE (BMP) HANDBOOK", JANUARY 2015.



0 40 80



SCALE: 1" = 80'

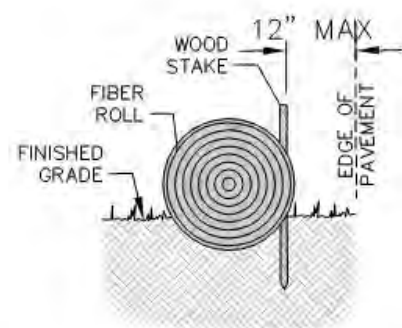


SECTION A-A

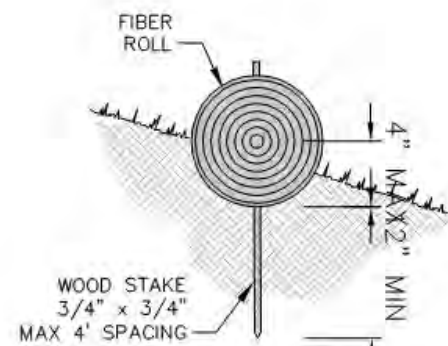
**NOTES:**

1. PLACE CURB TYPE SEDIMENT BARRIERS ON GENTLY SLOPING STREET SEGMENTS, WHERE WATER CAN POND AND ALLOW SEDIMENT TO SEPARATE FROM RUNOFF.
2. SANDBAGS OF EITHER BURLAP OR WOVEN 'GEOTEXTILE' FABRIC, ARE FILLED WITH GRAVEL LAYERED AND PACKED TIGHTLY.
3. LEAVE A ONE SANDBAG GAP IN THE TOP ROW TO PROVIDE A SPILLWAY FOR OVERFLOW.
4. INSPECT BARRIERS AND REMOVE SEDIMENT AFTER EACH STORM EVENT. SEDIMENT AND GRAVEL MUST BE REMOVED FROM THE TRAVELED WAY IMMEDIATELY.

**1 CURB INLET SEDIMENT BARRIER**  
NOT TO SCALE



ENTRENCHMENT DETAIL  
IN FLAT AREA

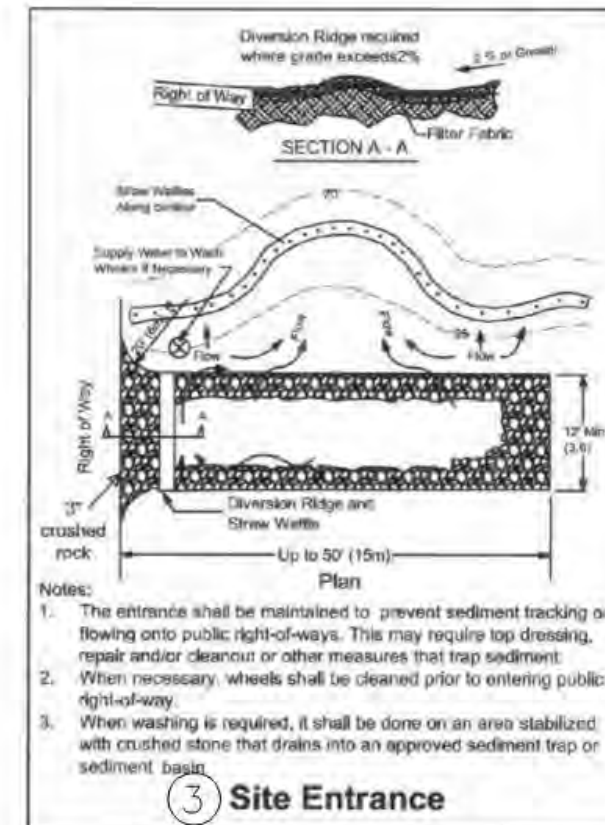


ENTRENCHMENT DETAIL  
IN SLOPED AREA

**NOTES**

1. FIBER ROLLS ARE TUBES MADE FROM POROUS BIODEGRADABLE FIBER STUFFED IN A PHOTO-DEGRADABLE OPEN WEAVE NETTING. THEY ARE APPROXIMATELY 8" DIAMETER.
2. FIBER ROLL INSTALLATION REQUIRES THE PLACEMENT AND SECURE STAKING OF THE ROLL IN A TRENCH; 2"-4" DEEP, DUG ON CONTOUR. RUNOFF MUST NOT BE ALLOWED TO RUN UNDER OR AROUND ROLL. ROLLS SHOULD BE ABUTTED SECURELY TO PROVIDE A TIGHT JOINT, NOT OVERLAPPED.

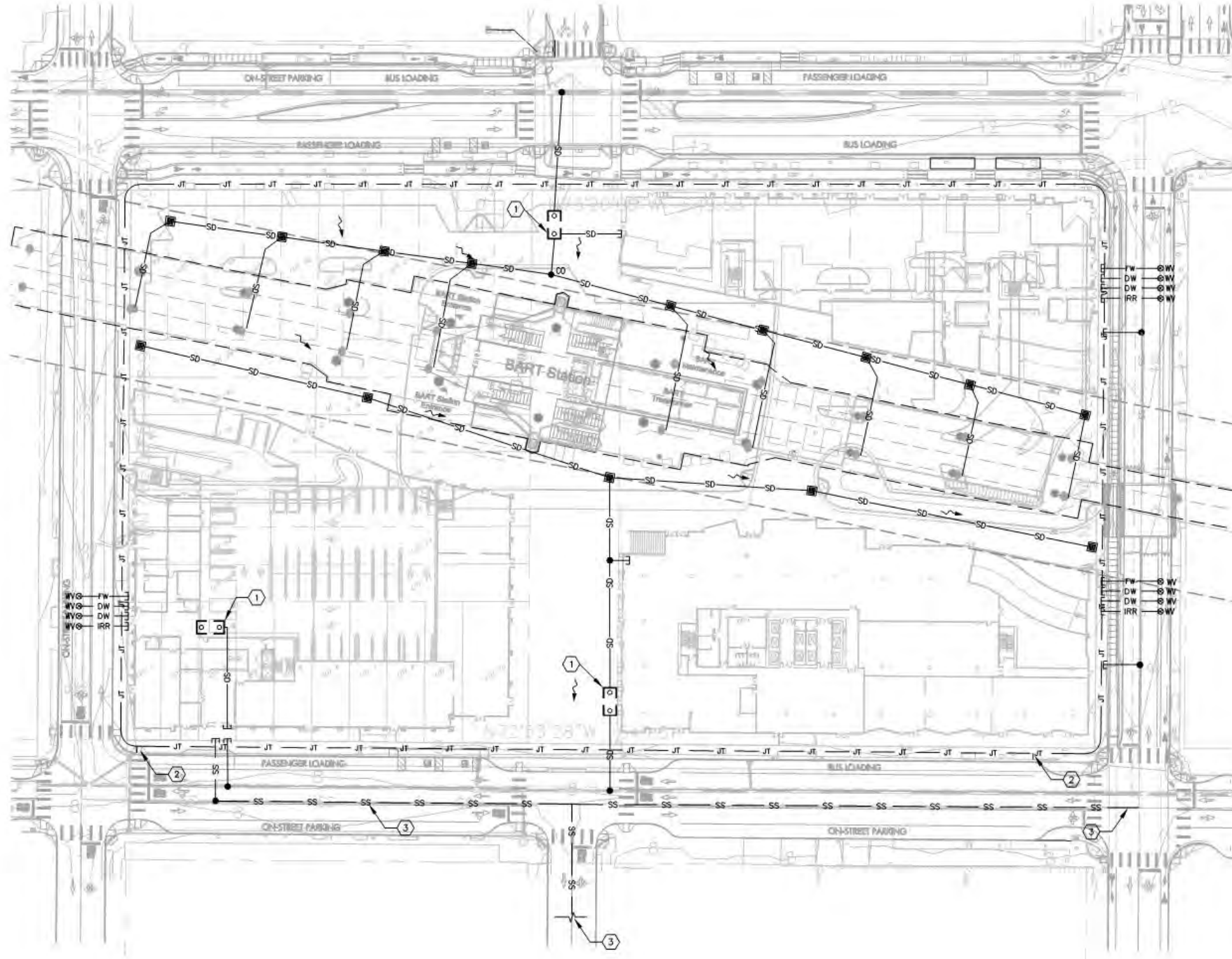
**2 FIBER ROLL**  
NOT TO SCALE



**Notes:**

1. The entrance shall be maintained to prevent sediment tracking or flowing onto public right-of-ways. This may require top dressing, repair and/or cleanout or other measures that trap sediment.
2. When necessary, wheels shall be cleaned prior to entering public right-of-way.
3. When washing is required, it shall be done on an area stabilized with crushed stone that drains into an approved sediment trap or sediment basin.

**3 Site Entrance**

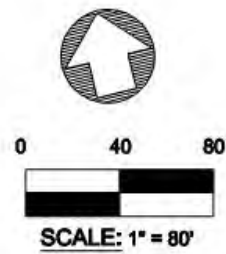


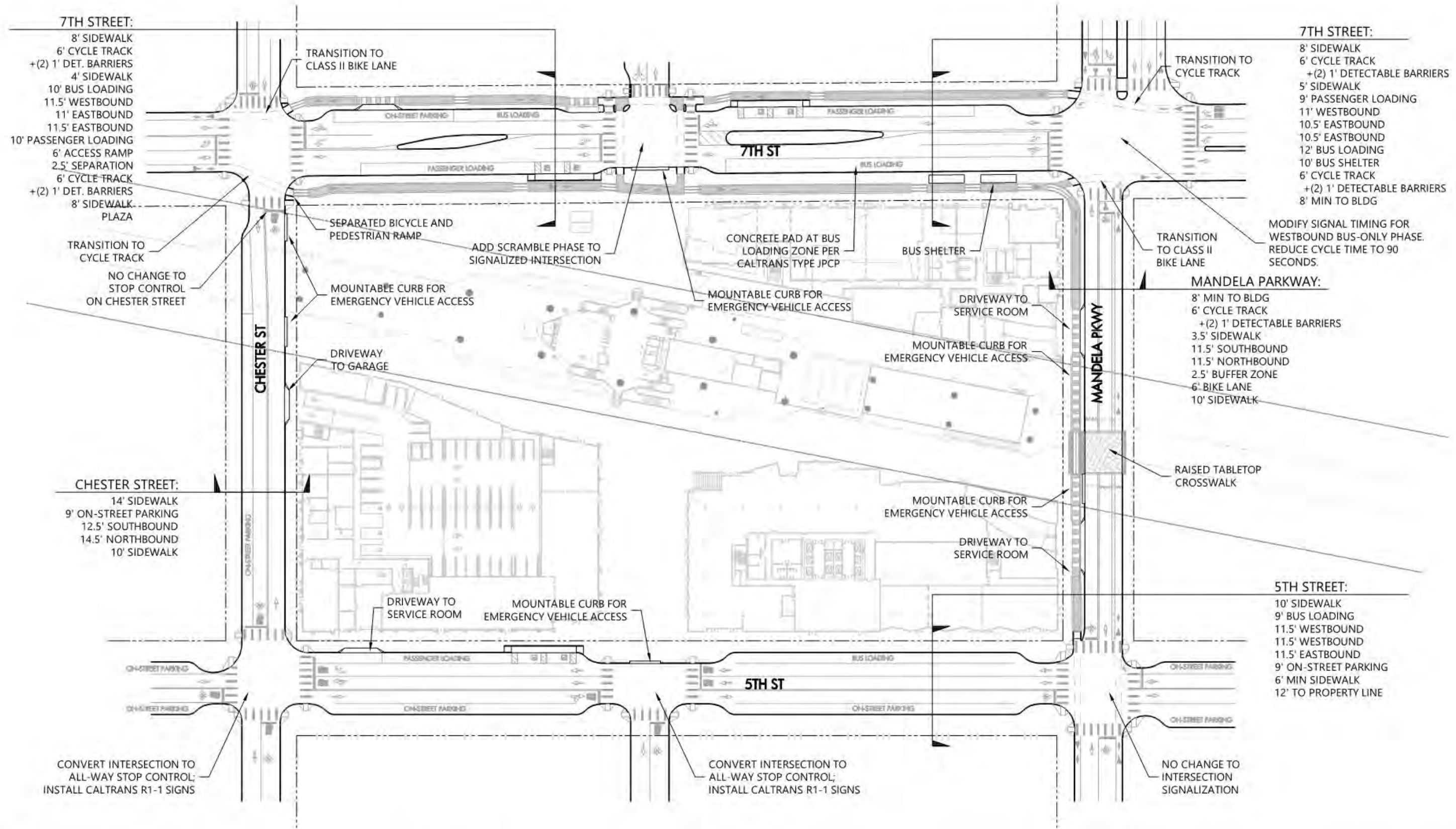
**UTILITY KEYNOTES:**

- ① NEW OLD CASTLE WASHINGTON GULD PV-V-8-WA-0001 PERK FILTER OR APPROVED EQUAL SEE DETAIL ON SHEET C3.01
- ② POTENTIAL CONNECTION LOCATION FOR ELECTRICAL AND TELECOM
- ③ REBUILD/UPSIZE SANITARY SEWER LINE TO CONNECTION AT CENTER ST x 3RD ST

**UTILITY LEGEND:**

- SD STORM DRAIN LINE
- IRR IRRIGATION LINE
- FW FIRE WATER LINE
- DW DOMESTIC WATER LINE
- SS SANITARY SEWER LINE
- JT JOINT TRENCH: ELEC + TELECOM
- DOMESTIC WATER MANHOLE PER CITY OF OAKLAND STANDARD DETAIL D-11
- STORM DRAIN MANHOLE
- WV WATER VALVE
- DRAIN INLET







CYCLE TRACK CASE STUDY



CYCLE TRACK CASE STUDY

**Rendering of Better Market Street with trapezoidal delineator between pedestrians and bicycles**

Source:  
San Francisco Public Works  
Better Market Street Project

Trapezoidal delineator

Enhancing Mobility, Access and Safety for Pedestrians (Part II)

**Recommendation**  
Use a trapezoidal indicator between bicycle and pedestrian sides of a separated bikeway at sidewalk level

PLAN, Regular Tile: 12.00" (300 mm) x 12.00" (300 mm)

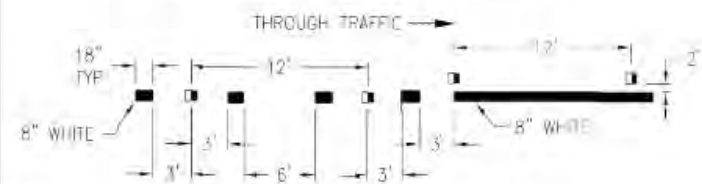
PLAN, End Tile: 12.00" (300 mm) x 12.00" (300 mm)

SECTION: 10.00" (250 mm) x 6.25" (150 mm)

DETAIL: 1.875" (48 mm) x 1.00" (25 mm) x 0.75" (20 mm) at 22° angle

**DETAIL 1 - LANE DROP**

CONTINUE 1-WAY CLEAR REFLECTIVE MARKERS AT 12'



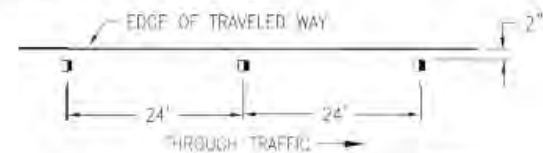
**DETAIL 2 - LANE LINE EXTENSIONS THROUGH INTERSECTIONS**



**DETAIL 3 - CROSSWALKS**



**DETAIL 4 - LEFT EDGE LINE**



CONTINUE 1-WAY YELLOW REFLECTIVE MARKERS AT 24'

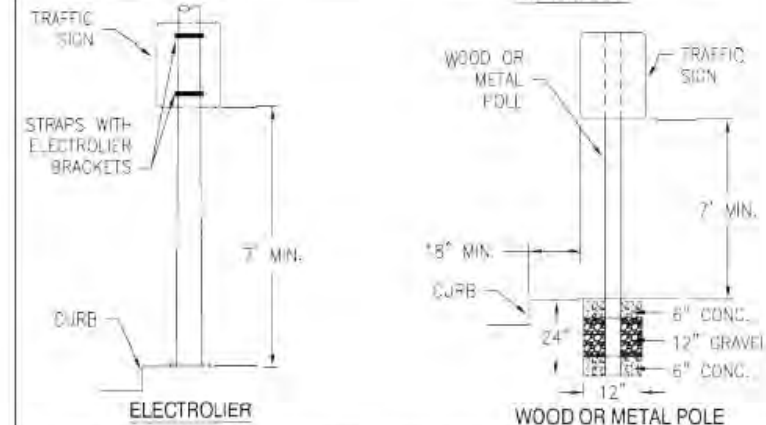
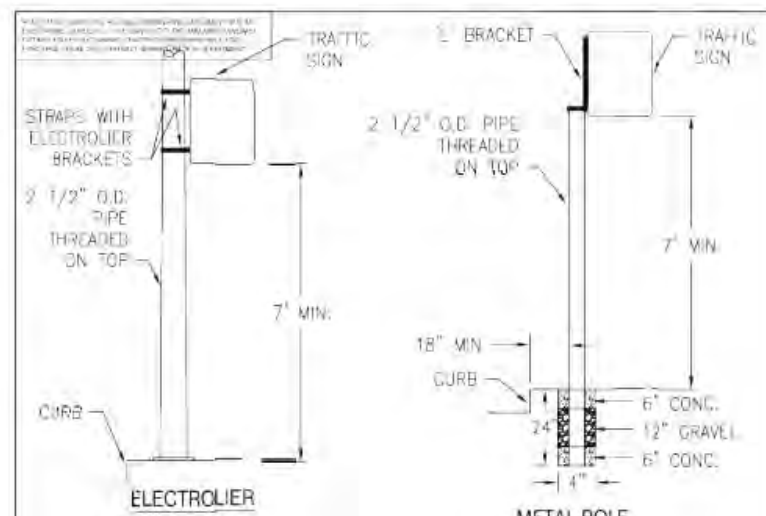
NOTE: REFLECTIVE MARKERS SHALL BE PLACED AT THE EDGE OF THE TRAVELED WAY AND SHALL BE PLACED AT THE EDGE OF THE TRAVELED WAY AND SHALL BE PLACED AT THE EDGE OF THE TRAVELED WAY.

CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT



**PAVEMENT MARKING  
DETAIL**

TRANSPORTATION SERVICES MANAGER		
DATE	APPROVED	DATE
REV. DATE		T-3

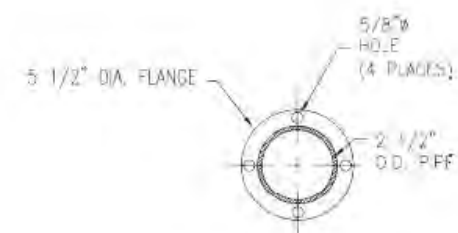
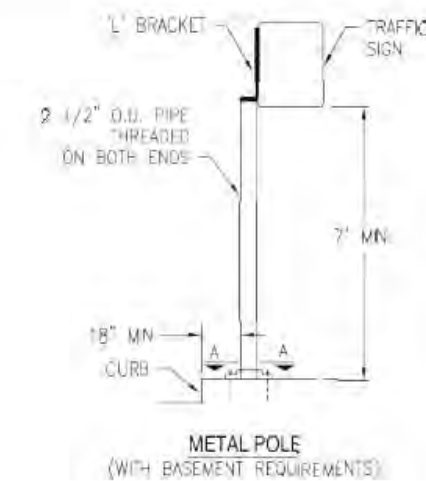


CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT



**SIGN INSTALLATION  
DETAILS**

TRANSPORTATION SERVICES MANAGER		
DATE	APPROVED	DATE
REV. DATE		T-4



**SECTION A-A**  
1/4" THICK FLANGE  
USE 3/8" DIA. X 3" LENGTH  
ANCHOR BOLTS W/LO. NUTS OR  
EQUAL

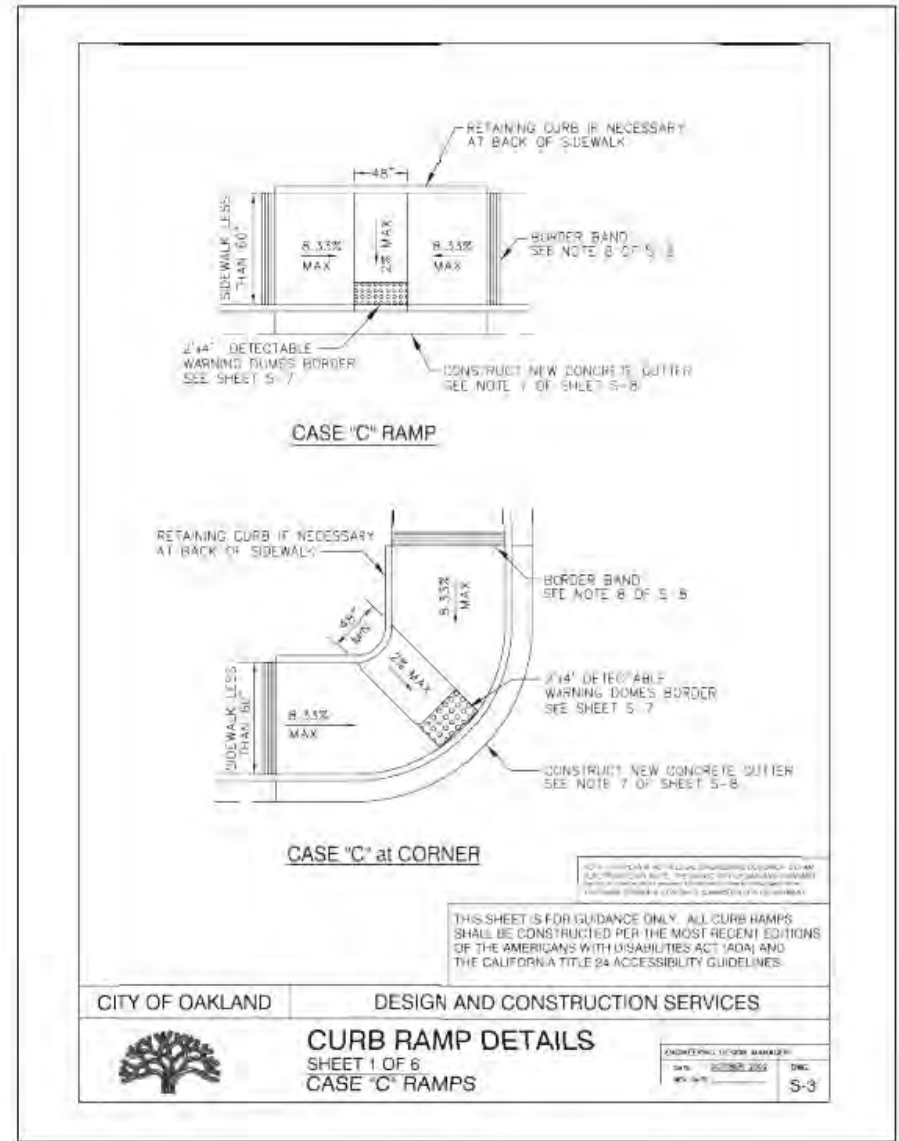
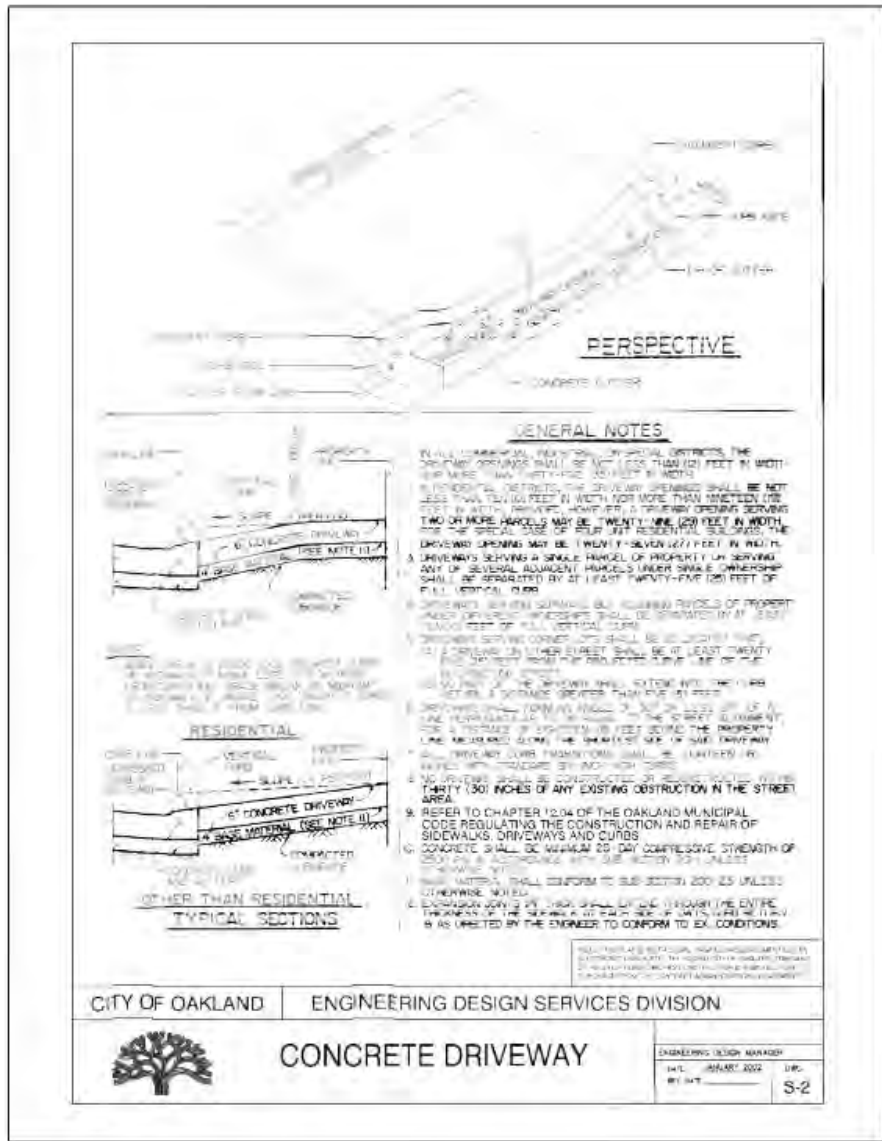
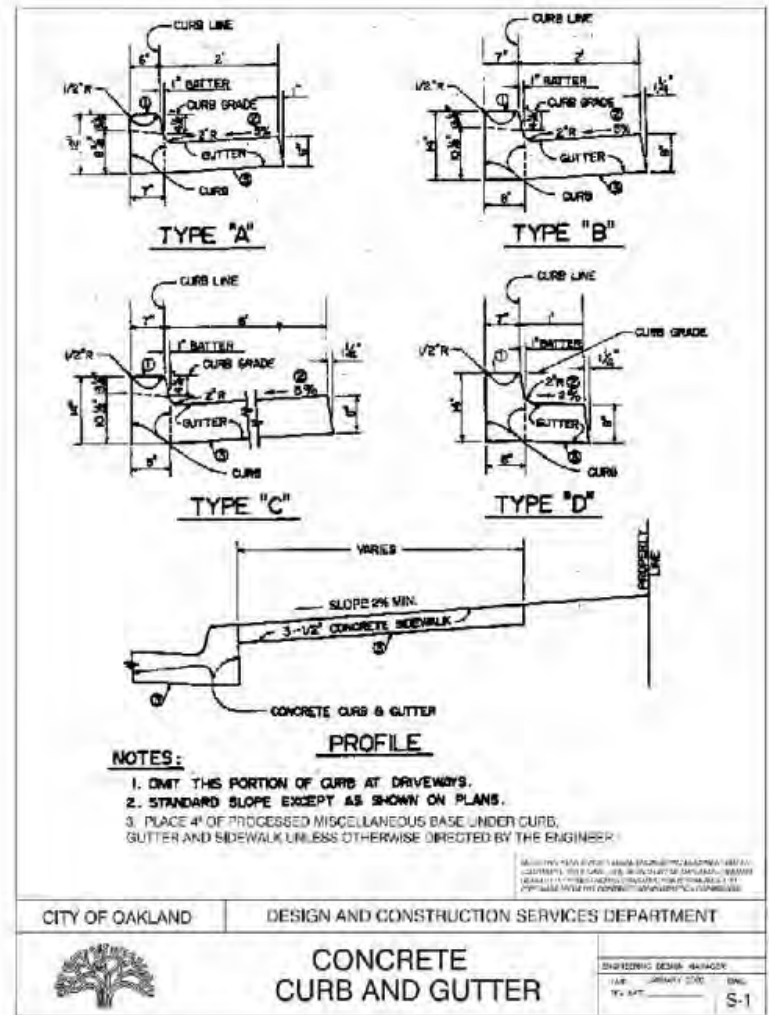
NOTE: REFLECTIVE MARKERS SHALL BE PLACED AT THE EDGE OF THE TRAVELED WAY AND SHALL BE PLACED AT THE EDGE OF THE TRAVELED WAY AND SHALL BE PLACED AT THE EDGE OF THE TRAVELED WAY.

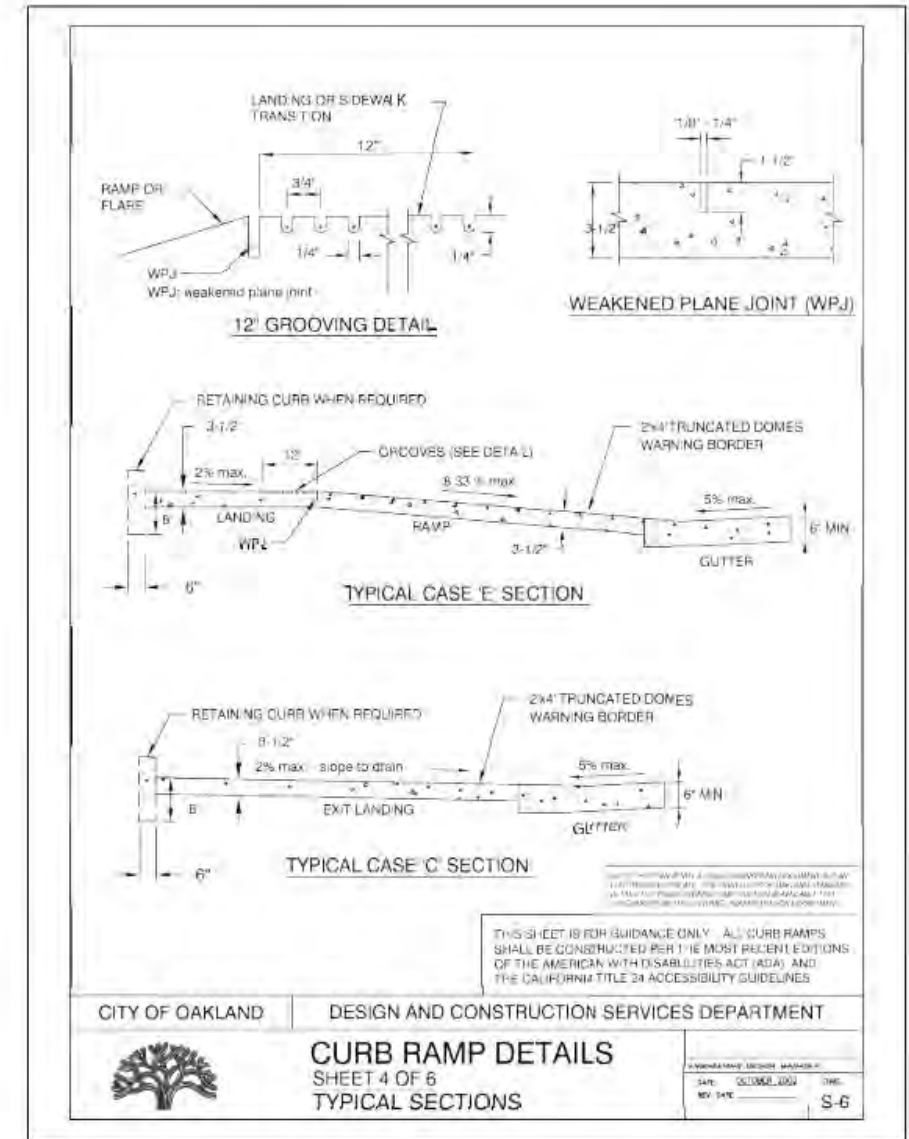
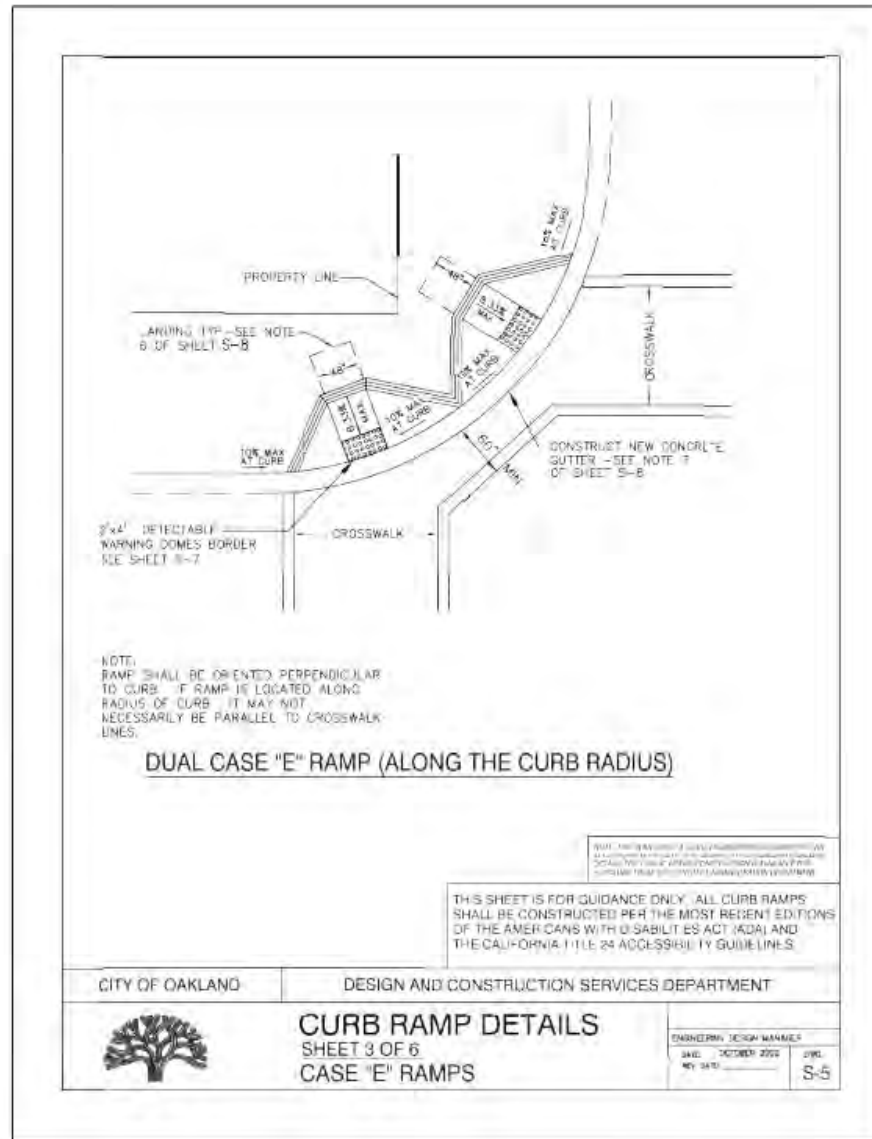
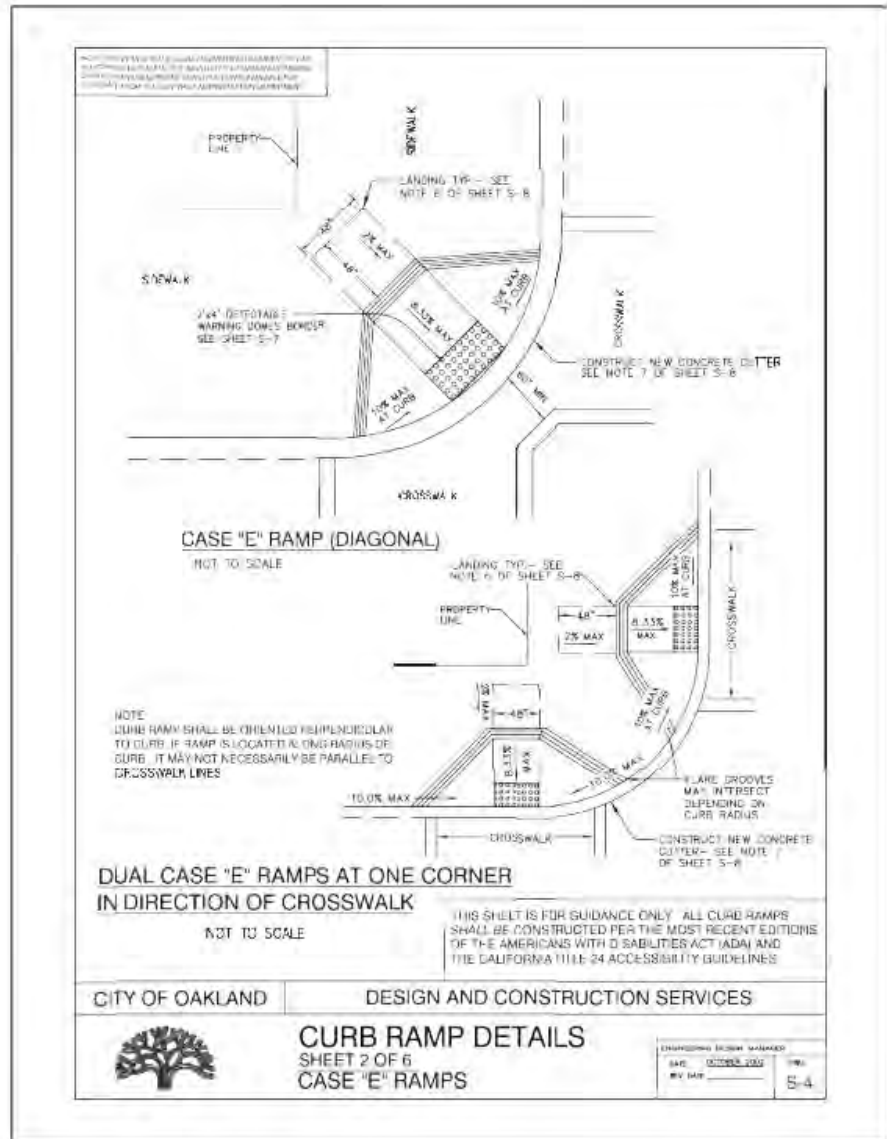
CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT



**SIGN INSTALLATION  
DETAILS**

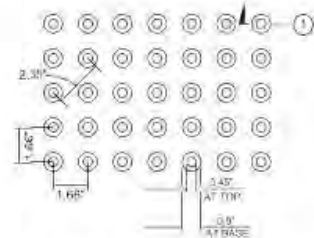
TRANSPORTATION SERVICES MANAGER		
DATE	APPROVED	DATE
REV. DATE		T-5







1. Curb ramps shall have 4 ft by 2 ft long detectable warning border centered and squared at the ramp bottom.
2. The detectable warning border shall contrast visually with the adjoining surfaces, either light or dark. The material used to provide contrast shall be an integral part of the walking surface and shall contrast by at least 70 percent. A fully cured 2 ft x 2 ft sample of both the cast-in-place detectable warning border and the adjacent curb ramp shall be submitted to the Engineer for review and approval prior to installation.
3. Dome orientation shall conform to the latest ADA / Title 24 regulations.
4. The closest corner of the truncated dome tile to the street shall be set 6" from the flowline.
5. Detectable dome warning tiles or strips made of materials other than concrete shall only be used with the written approval of the Director of Public Works.
6. Precast concrete detectable warning dome pavers shall be installed on top of a 4 inch thick concrete pad. Mortared joints shall be flush with top surface and finished to provide a smooth surface. Pavers shall be laid such that joints are level with adjoining joints to provide a smooth transition from paver to paver and from paver to concrete surface.



Bottom Diameter = 0.9 inches  
 Top Diameter = 0.4 inches  
 Height = 0.2 inches  
 Center-to-Center Spacing = 2.35 inches

PLAN VIEW OF DETECTABLE WARNING DOMES  
 NTS



THIS SHEET IS FOR GUIDANCE ONLY. ALL CURB RAMPS SHALL BE CONSTRUCTED FOR THE MOST RECENT EDITIONS OF THE AMERICAN WITH DISABILITIES ACT (ADA) AND THE CALIFORNIA TITLE 24 ACCESSIBILITY GUIDELINES.

CITY OF OAKLAND	DESIGN AND CONSTRUCTION SERVICES
<b>CURB RAMP DETAILS</b>	
SHEET 5 OF 6	
DETECTABLE WARNINGS	
	ENGINEERING DESIGN DIVISION CIVIL ENGINEER DATE: OCTOBER 2012 BY: [Signature]

**CASE "C" CURB RAMP NOTES**

1. If site conditions do not accommodate a Case E ramp, a Case "C" Curb Ramp shall be constructed.
2. If the sidewalk is less than 5-feet wide, a Case C ramp shall be installed.
3. The maximum slope of the lower landing of a Case C ramp shall be 2%. In all cases the lower landing shall be sloped to drain.
4. No vertical obstructions shall be allowed within a Case C curb ramp.

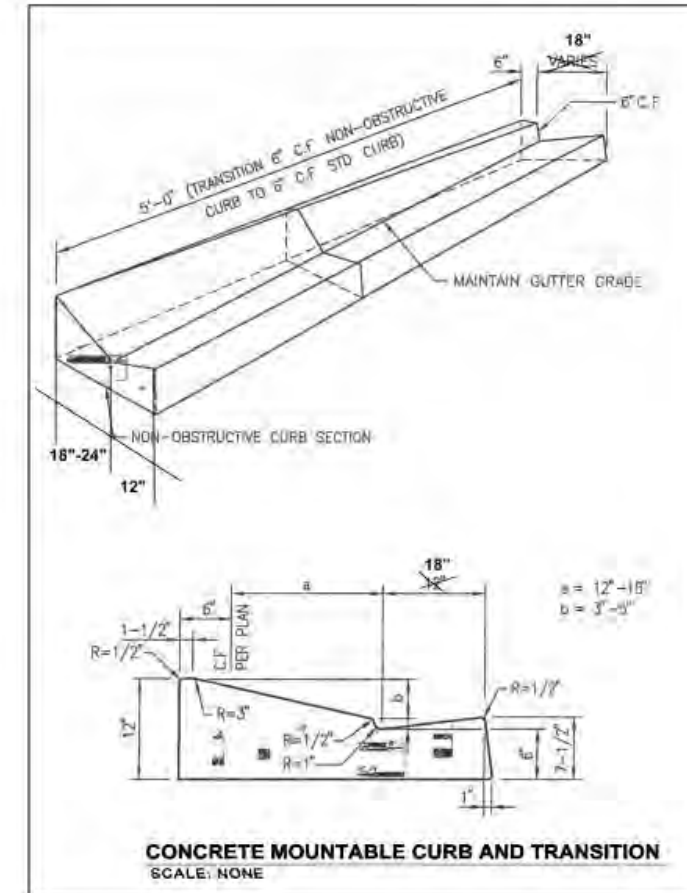
**CASE "E" CURB RAMP NOTES:**

5. Two ramps shall be provided at each corner of an intersection, unless otherwise allowed by the plans or by the Engineer. The centerline and path of travel of both curb ramps must be perpendicular to the gutter.
6. A level landing (4-ft deep with 2% max. slope) shall be provided at the upper end of each Case E curb ramp over its full width to permit safe exit from the ramp surface. If a level landing cannot be installed, then the side flares of the ramp shall slope less than or equal to 8.33%.

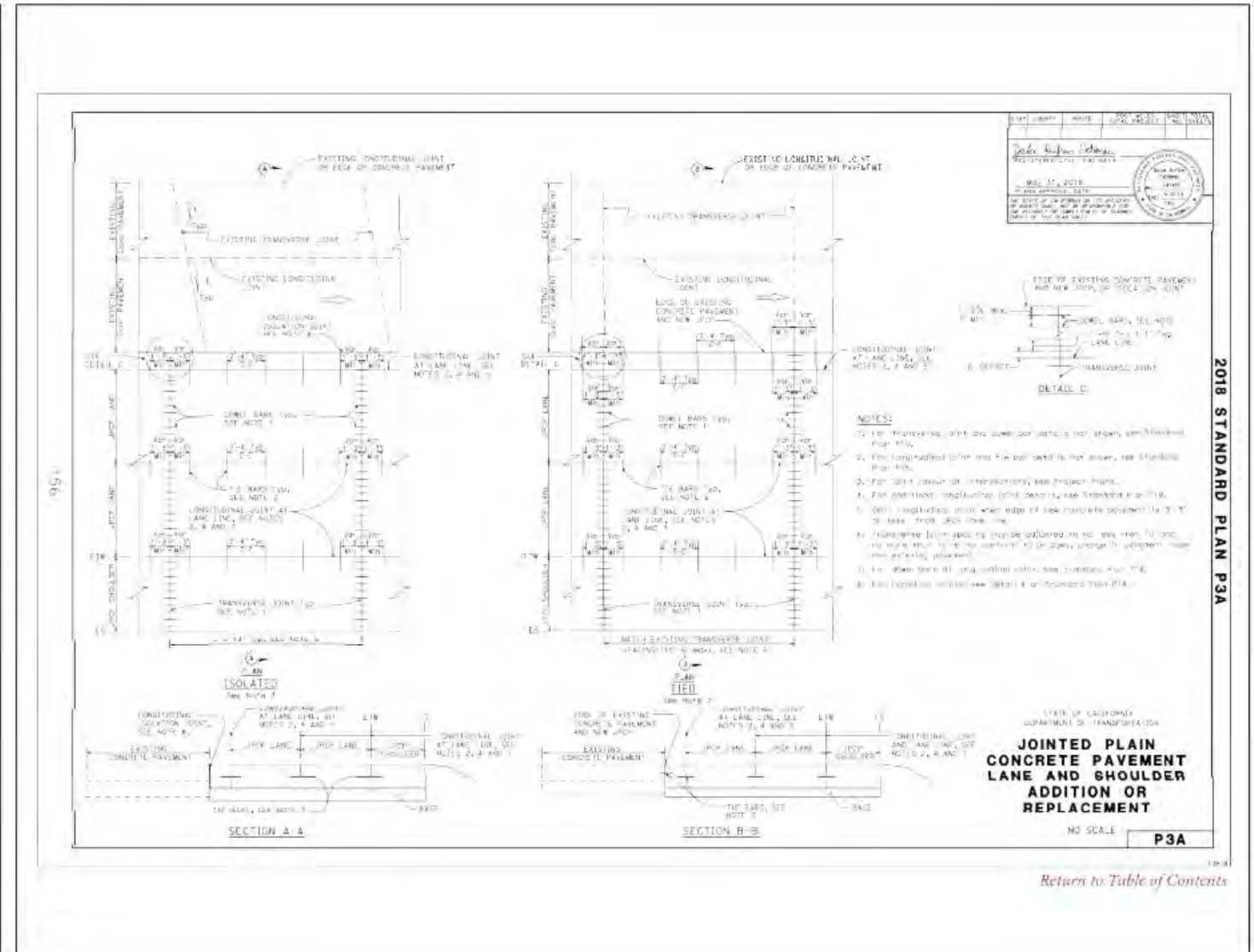
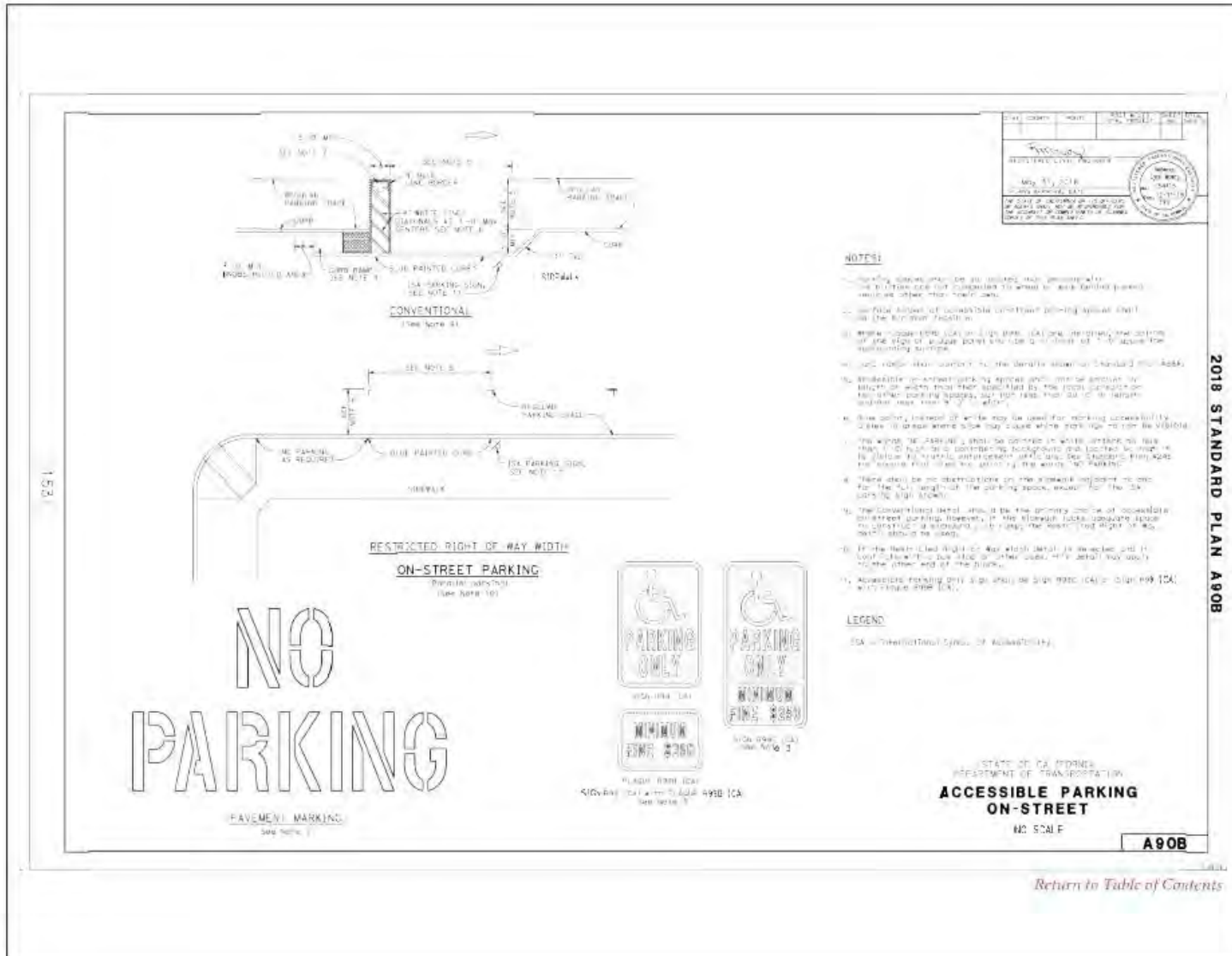
**GENERAL CURB RAMP NOTES:**

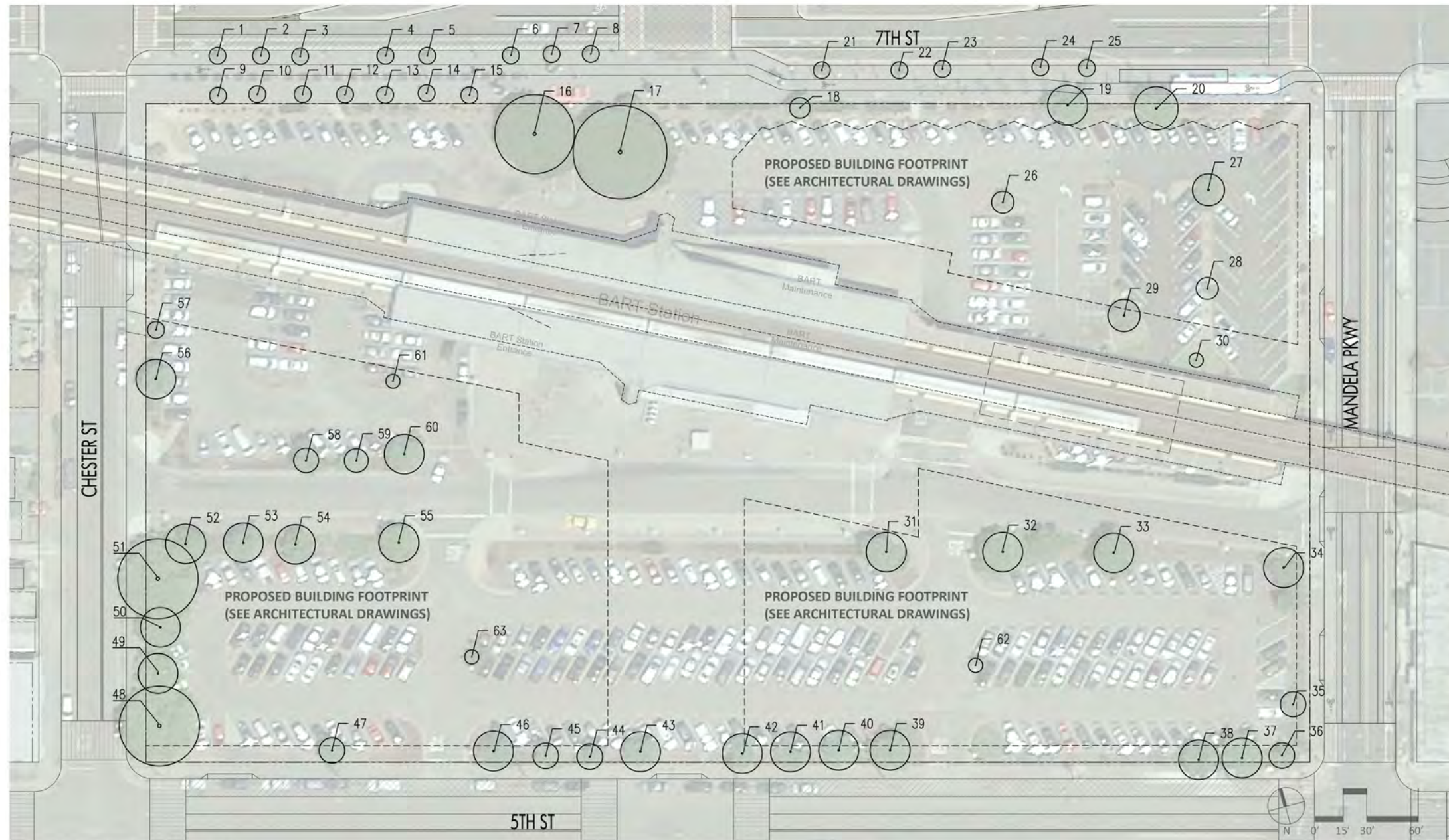
7. When constructing curb ramps, construct concrete gutter to match existing gutter widths. The new gutter shall be flush with the bottom of the curb ramp and the new gutter shall be considered part of the curb ramp construction.
8. Each curb ramp shall be bounded by a 12- inch border band with 1/4 inch deep grooves scored 3/4 inches apart except in the curb and gutter area.
9. Vertical obstructions, such as utility poles and traffic signals, shall only be allowed with the approval of the Engineer.
10. Existing utility boxes and covers shall be adjusted to conform flush with the curb ramp surface. All utility boxes and covers located within the detectable warning border area shall be relocated.

CITY OF OAKLAND	ENGINEERING DESIGN DIVISION
<b>CURB RAMP DETAILS</b>	
SHEET 6 OF 6	
GENERAL NOTES	
	ENGINEERING DESIGN DIVISION CIVIL ENGINEER DATE: OCTOBER 2012 BY: [Signature]



**CONCRETE MOUNTABLE CURB AND TRANSITION**  
 SCALE: NONE





**Trees Proposed for Removal**

Key	Species	DBH (in)
1	<i>Pistacia chinensis</i>	4
2	<i>Pistacia chinensis</i>	4
3	<i>Pistacia chinensis</i>	4
4	<i>Pistacia chinensis</i>	3
5	<i>Pistacia chinensis</i>	5
6	<i>Pistacia chinensis</i>	4
7	<i>Pistacia chinensis</i>	4
8	<i>Pistacia chinensis</i>	4
9	<i>Pistacia chinensis</i>	4
10	<i>Pistacia chinensis</i>	4
11	<i>Pistacia chinensis</i>	4
12	<i>Pistacia chinensis</i>	4
13	<i>Pistacia chinensis</i>	4

Key	Species	DBH (in)
14	<i>Pistacia chinensis</i>	4
15	<i>Pistacia chinensis</i>	4
16	<i>Pinus pinea</i>	36
17	<i>Pinus pinea</i>	32
18	<i>Liriodendron tulipifera</i>	9
19	<i>Liriodendron tulipifera</i>	11
20	<i>Liriodendron tulipifera</i>	13
21	<i>Pistacia chinensis</i>	4
22	<i>Pistacia chinensis</i>	4
23	<i>Pistacia chinensis</i>	4
24	<i>Pistacia chinensis</i>	4
25	<i>Pistacia chinensis</i>	5
26	<i>Maytenus boaria</i>	7

Key	Species	DBH (in)
27	<i>Maytenus boaria</i>	12
28	<i>Maytenus boaria</i>	5
29	<i>Maytenus boaria</i>	16
30	<i>Maytenus boaria</i>	2
31	<i>Liriodendron tulipifera</i>	13
32	<i>Liriodendron tulipifera</i>	11
33	<i>Liriodendron tulipifera</i>	13
34	<i>Liriodendron tulipifera</i>	11
35	<i>Liriodendron tulipifera</i>	9
36	<i>Liriodendron tulipifera</i>	12
37	<i>Liriodendron tulipifera</i>	11
38	<i>Liriodendron tulipifera</i>	10
39	<i>Liriodendron tulipifera</i>	11

Key	Species	DBH (in)
40	<i>Liriodendron tulipifera</i>	9
41	<i>Liriodendron tulipifera</i>	10
42	<i>Liriodendron tulipifera</i>	10
43	<i>Liriodendron tulipifera</i>	9
44	<i>Liriodendron tulipifera</i>	7
45	<i>Liriodendron tulipifera</i>	8
46	<i>Liriodendron tulipifera</i>	8
47	<i>Liriodendron tulipifera</i>	9
48	<i>Platanus X acerifolia</i>	18
49	<i>Platanus X acerifolia</i>	10
50	<i>Platanus X acerifolia</i>	10
51	<i>Platanus X acerifolia</i>	17
52	<i>Liriodendron tulipifera</i>	12

Key	Species	DBH (in)
53	<i>Liriodendron tulipifera</i>	8
54	<i>Liriodendron tulipifera</i>	8
55	<i>Liriodendron tulipifera</i>	9
56	<i>Liriodendron tulipifera</i>	10
57	<i>Liriodendron tulipifera</i>	5
58	<i>Liriodendron tulipifera</i>	8
59	<i>Liriodendron tulipifera</i>	7
60	<i>Liriodendron tulipifera</i>	9
61	<i>Maytenus boaria</i>	5
62	<i>Maytenus boaria</i>	5
63	<i>Maytenus boaria</i>	7

**Bolded trees are protected trees.**



BUILDING T1  
FDP PACKAGE

PROJECT

**WEST OAKLAND BART**  
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM

- BKF ENGINEERS  
1560 N. CALIFORNIA  
BLVD, SUITE 400  
WALNUT CREEK  
CA 94596  
(925) 940-2200
- PGA DESIGN  
444 17th Street  
Oakland  
CA 94612  
(510) 455-1284
- STOK  
9408 Front Street  
San Francisco  
CA 94111  
(415) 329-7100

STAMP

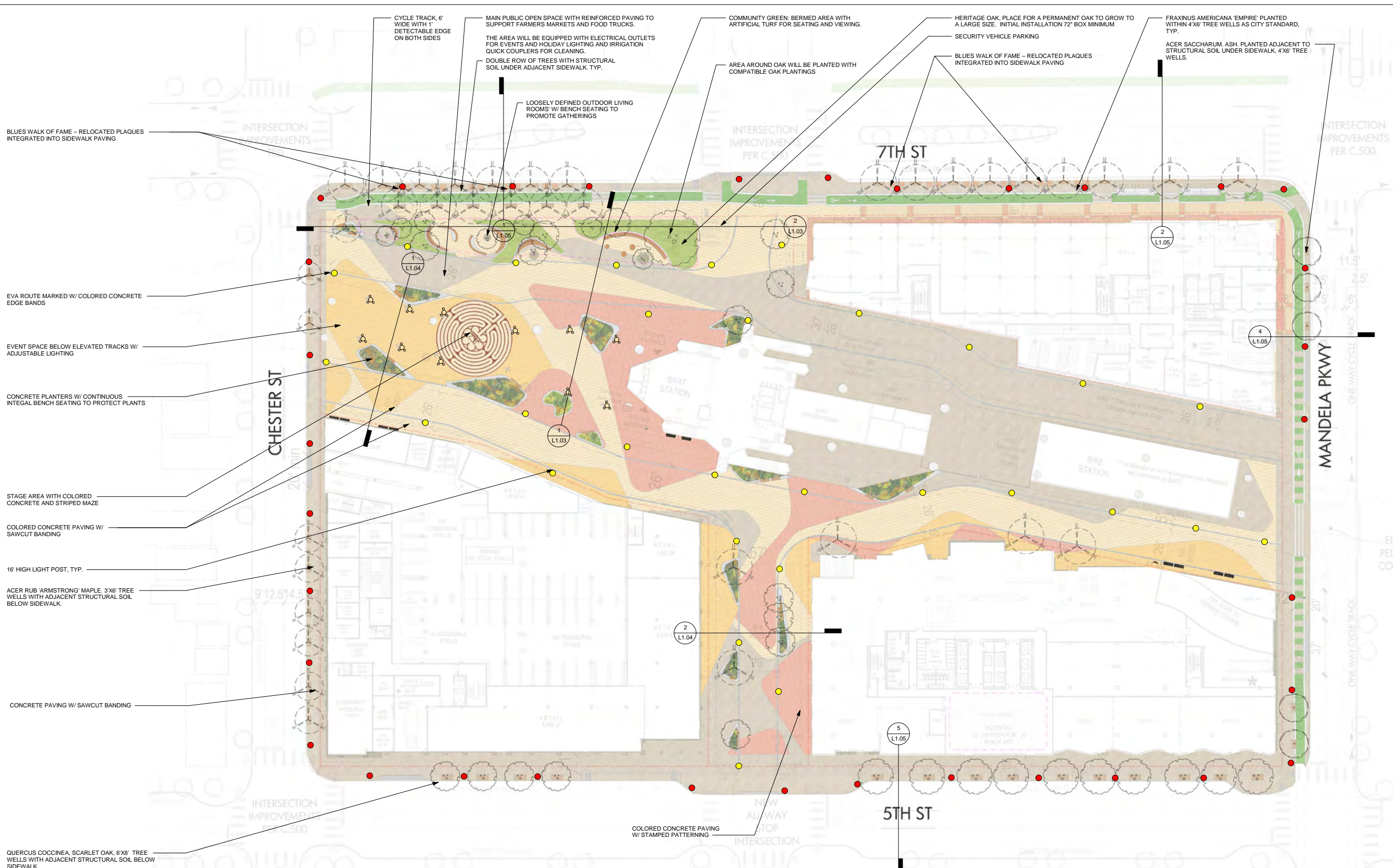
KEY

PROJ #: 168-153 WO BART  
DATE: 06/04/20  
SCALE: 1"=60' - 0"

TITLE:  
**TREE  
REMOVAL  
PLAN**

SHEET:  
**L1.00**

OWNER  
**HEC**  
**HENSEL PHELPS**  
**suda**  
 BUILDING T1 FDP PACKAGE  
 PROJECT  
**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607  
 ARCHITECT  
**jrdrv**  
 PROJECT TEAM  
**BKF ENGINEERS**  
**BKF100**  
**PGA DESIGN**  
**stok**  
 STAMP  
 KEY  
 PROJ #: 168-153 WO BART  
 DATE: 03/26/20  
 SCALE: 1"=60' - 0"  
 TITLE: **LAYOUT & MATERIALS PLAN**  
 SHEET: **L1.01**



THE PLAZA IS DESIGNED TO SUPPORT AND BALANCE THE NEEDS OF FUTURE USERS. THESE PATRONS WILL VARY FROM COMMUTERS TO THOSE COMING TO ATTEND LARGE EVENTS OR SIMPLY TO VISIT THE UNIQUE MARKETS. VARIOUS SIZED GATHERING SPACES ACCOMMODATE DIVERSE USES.

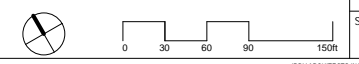
**IRRIGATION METHOD & WATER EFFICIENT LANDSCAPE ORDINANCE**

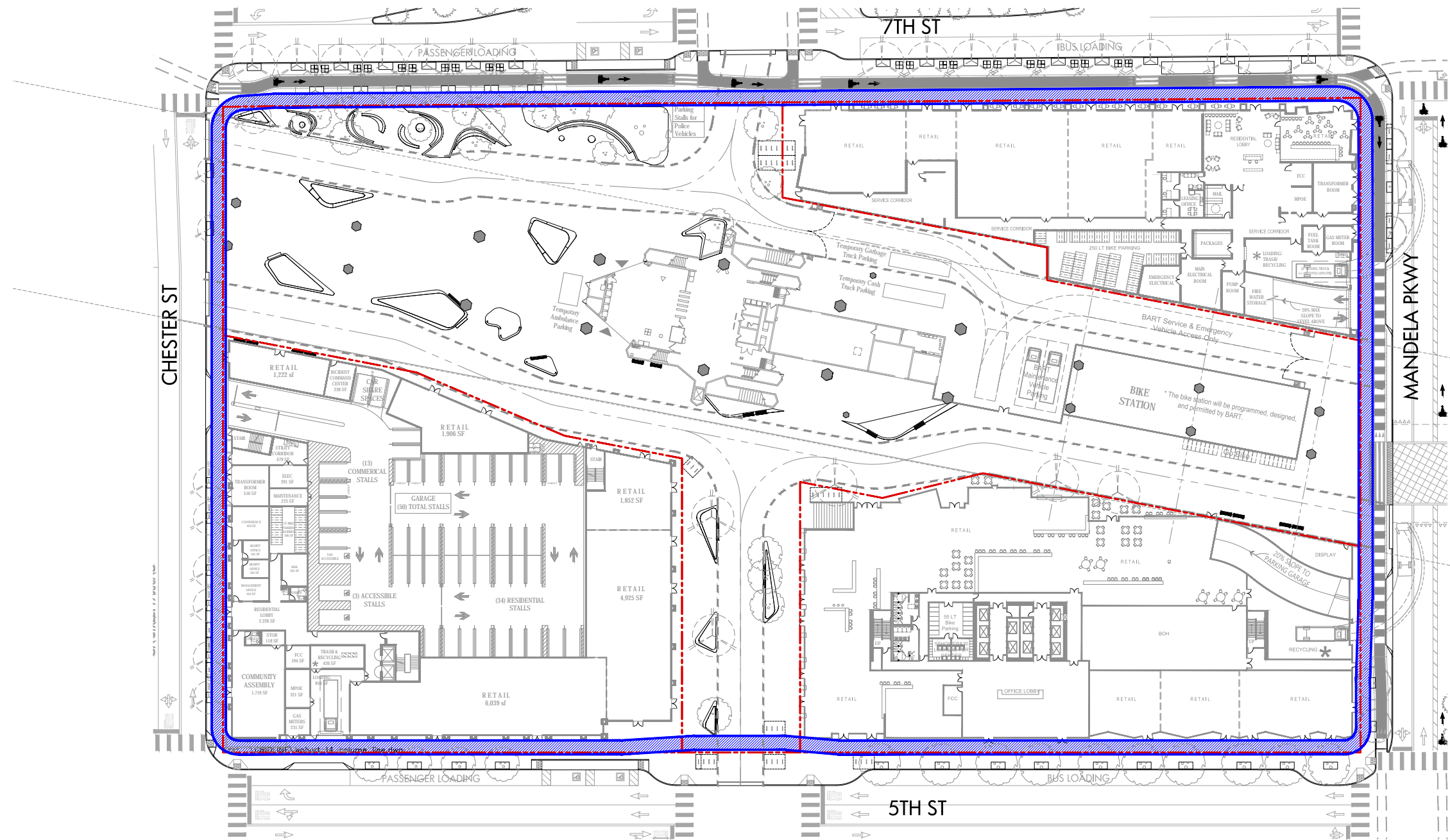
- IRRIGATION SYSTEM WILL BE DESIGNED TO PROVIDE THE MINIMUM AMOUNT OF WATER NECESSARY TO SUSTAIN GOOD PLANT HEALTH. ALL SELECTED COMPONENTS TO BE COMMERCIAL GRADE, SELECTED FOR DURABILITY, VANDAL RESISTANCE AND MINIMUM MAINTENANCE REQUIREMENT.
- THE SYSTEM WILL BE DRIPLINE IRRIGATION AS APPROPRIATE TO PLANT TYPE, EXPOSURE AND SLOPE CONDITIONS.
- CONTROL OF THE SYSTEM WILL BE VIA A WEATHER-ENABLED CONTROLLER CAPABLE OF DAILY SELF-ADJUSTMENT BASED ON REAL-TIME WEATHER CONDITIONS AS MEASURED BY AN ON-SITE WEATHER SENSOR.
- THE SYSTEM WILL INCLUDE A MASTER CONTROL VALVE AND FLOW SENSING CAPABILITY WHICH WILL SHUT DOWN ALL OR PART OF THE SYSTEM IF LEAKS ARE DETECTED.
- THE LANDSCAPE IS OVER 500 SQUARE FEET OF NEW LANDSCAPING AND WILL THEREFORE MEET THE REQUIREMENTS OF THE WATER EFFICIENCY LANDSCAPE ORDINANCE (WELO): [HTTPS://WATER.CA.GOV/LEGACYFILES/WATERUSEEFFICIENCY/LANDSCAPEORDINANCE/DOCS/TITLE%2023%20EXTRACT%20-%20OFFICIAL%20CCR%20PAGES.PDF](https://water.ca.gov/legacy/files/wateruseefficiency/landscapeordinance/docs/title%2023%20extract%20-%20official%20CCR%20PAGES.PDF)

**WEST OAKLAND STATION CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN STRATEGIES**

- AREA LIGHTING:**
- ALL LOADING, UNLOADING, KISS AND RIDE, DROP OFF, PICK UP AREAS WILL BE LIT TO 5 CANDLE FOOT. SIDEWALKS AROUND THE SITE WILL BE LIT TO 1.25 CANDLE FOOT (CITY STANDARD). BIKE LOCKER AREAS WILL BE LIT TO 10 CANDLE FOOT, AND EXTERIOR PATRON WAITING AREAS OUTSIDE THE BART FAIR GATES WILL BE LIT TO 10 CANDLE FOOT. SEE SHEET L1.07 FOR MORE DETAIL REGARDING THE LOCATION OF THESE AREAS.
  - POLE LIGHTS ARE KEPT 20' CLEAR OF TREES AND LANDSCAPING. LIGHT POLES ARE LESS THAN 22' HIGH. OTHER LIGHTS WILL BE MOUNTED ON BUILDING, ON BOLLARDS OR ALONG OVERHEAD WIRES (<20' HIGH).
  - THE LIGHTING WILL USE A FULL COLOR INDEX IN ORDER TO BETTER DISTINGUISH FORM AND MOVEMENT.
- AREA PLANTING:**
- PLANTINGS, INCLUDING THE RAISED PLANTERS AND BERMS THEY ARE IN, WILL NOT EXCEED 36" IN HEIGHT. TREES WILL BE LIMBED UP TO 7' HIGH. THE INTENTION IS TO AVOID BLIND SPOTS CAUSED BY HIGH PLANTS AND PLANTERS. TREES WILL BE THE TYPE THAT RETAIN A THIN TRUNK AS THEY GROW AND MATURE. THE TREE CANOPIES WILL NOT BE OVERLY DENSE AND WILL ALLOW LIGHT IN FROM ABOVE.

- AREA SITE FURNISHINGS:**
- SITE FURNISHINGS, EARTH FORMS, AND ARTWORK WILL BE DESIGNED NOT TO OBSCURE VISIBILITY. BENCHES HAVE SEPARATORS/ARMRESTS, SO THEY ARE USED FOR SITTING ONLY. PLANTER BOXES AND LANDSCAPE WALLS WILL HAVE GROOVES OR ELEMENTS TO DETER SKATEBOARDERS. FENCES ARE DESIGNED TO RESIST TEMPERING AND CLIMBING.
- AREA SIGNAGE:**
- DIRECTIONAL AND WAYFINDING SIGNAGE WILL BE PLACED AT APPROPRIATE LOCATIONS TO HELP DIRECT THE VISITOR TO THEIR DESTINATION.
- SURVEILLANCE CAMERAS:**
- A SERIES OF SURVEILLANCE CAMERAS WILL BE PLACED TO COVER THE ENTIRE SITE. THESE CAMERAS WILL COMPLY WITH BART FACILITY STANDARDS.

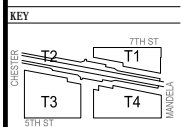




**LEGEND**

- 8' CLEAR ZONE FOR PEDESTRIAN
- PROPERTY LINE

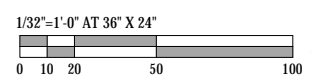
STAMP



PROJ # 168-153 WO BART  
 DATE July 23, 2020  
 SCALE 1/32"=1'-0"  
 TITLE

PUBLIC ACCESS

SHEET: **L1.01.02**



OWNER



BUILDING T1  
FDP PACKAGE

PROJECT

WEST OAKLAND BART  
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM

BNF ENGINEERS  
1548 N CALIFORNIA  
BLVD, SUITE 400  
WALNUT CREEK  
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(925) 940-2200

PGA DESIGN  
444 17th Street  
Oakland  
CA 94612  
(510) 465-1284

STOK  
949B Front Street  
San Francisco  
CA 94111  
(415) 329-7100

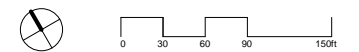
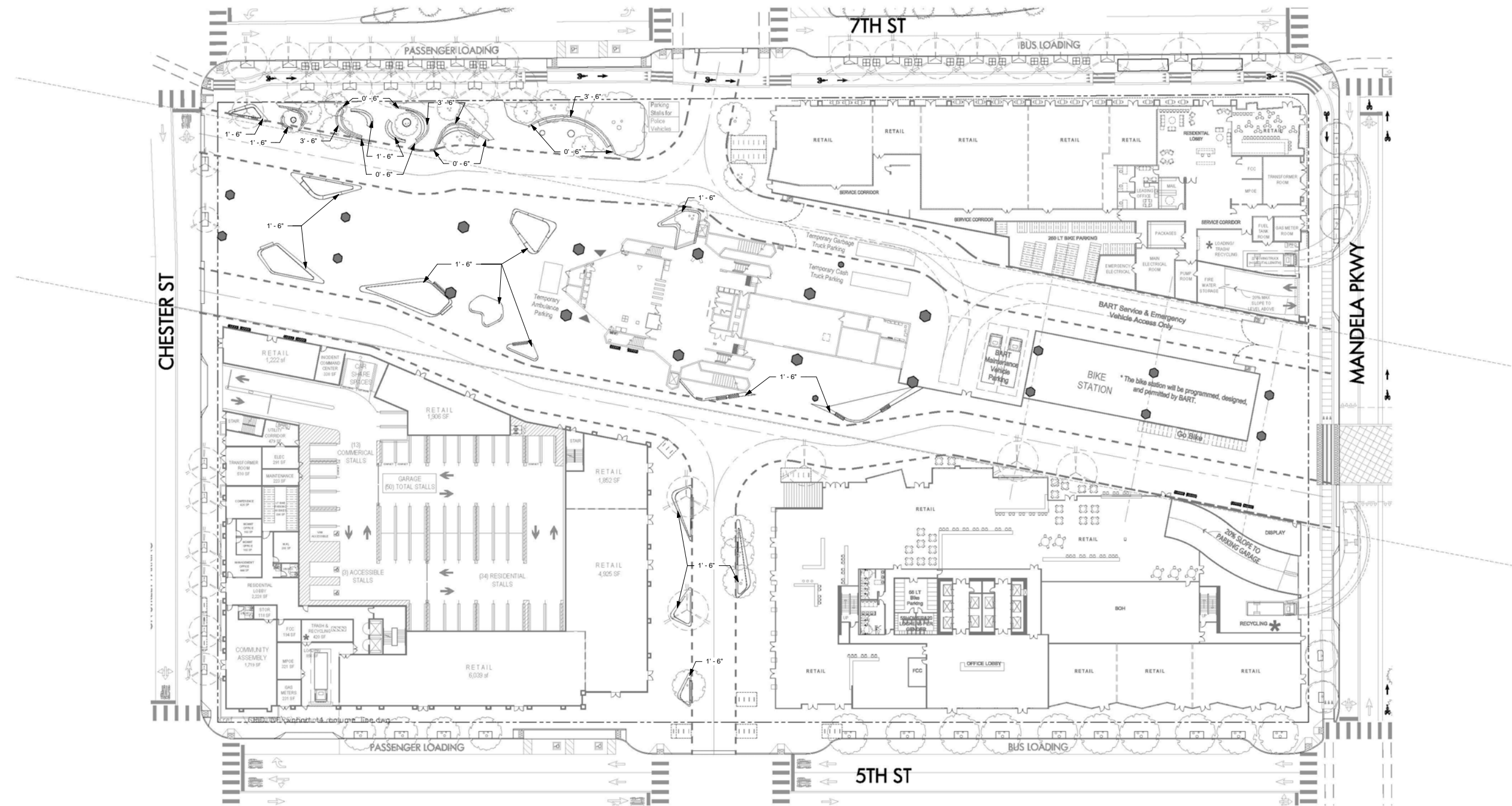
STAMP

KEY

PROJ #: 168-153 WO BART  
DATE: 03/26/20  
SCALE: 1"=60' - 0"

TITLE:  
LANDSCAPE  
WALL HEIGHT  
PLAN

SHEET:  
L1.02





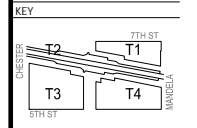
1 SECTION 1  
L1.03 1/16" = 1'-0"



2 SECTION 2  
L1.03 1/16" = 1'-0"



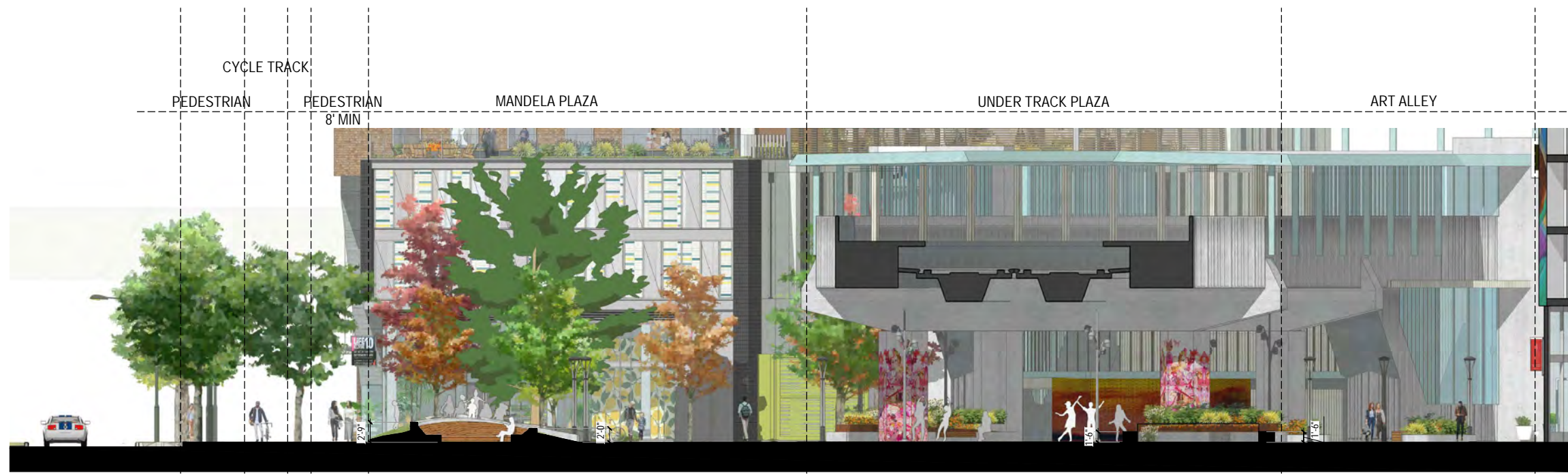
STAMP



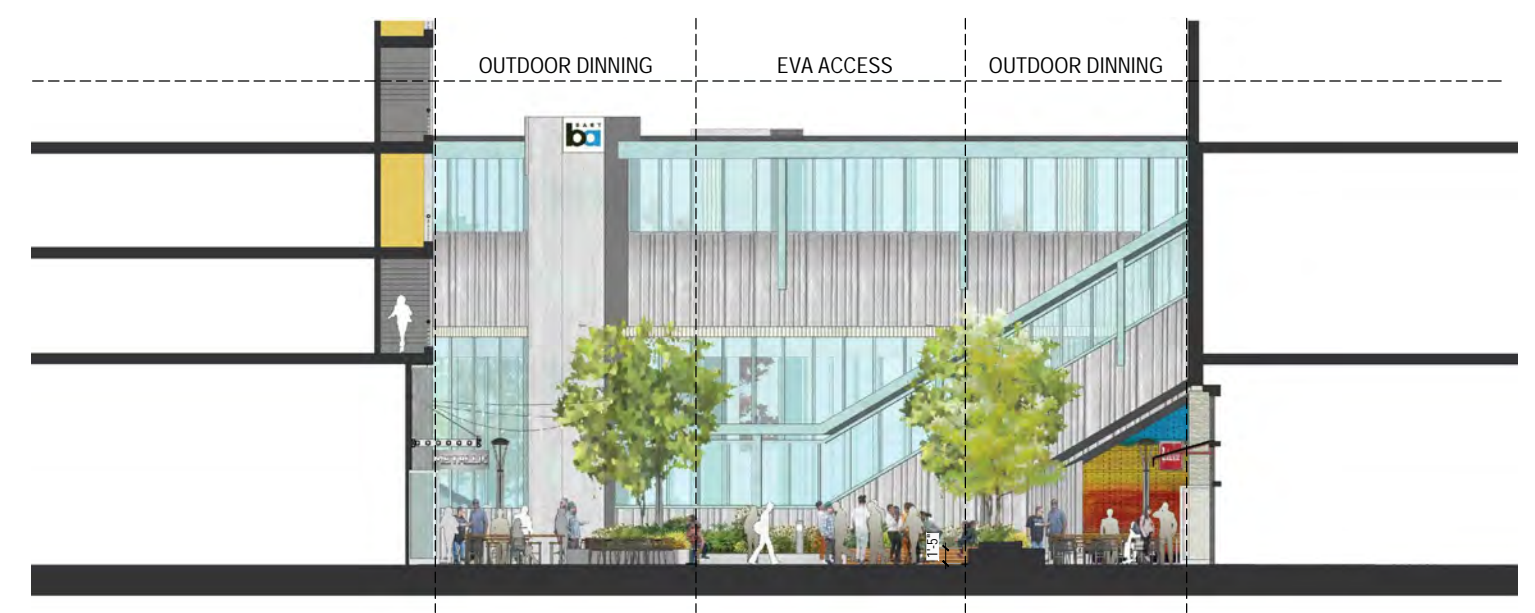
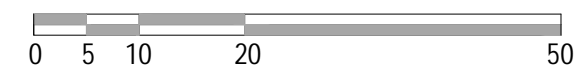
PROJ #: 168-153 WO BART  
 DATE: July 24, 2020  
 SCALE: 1/16" = 1'-0"  
 TITLE:

SECTIONS

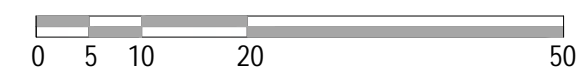
SHEET: L1.03



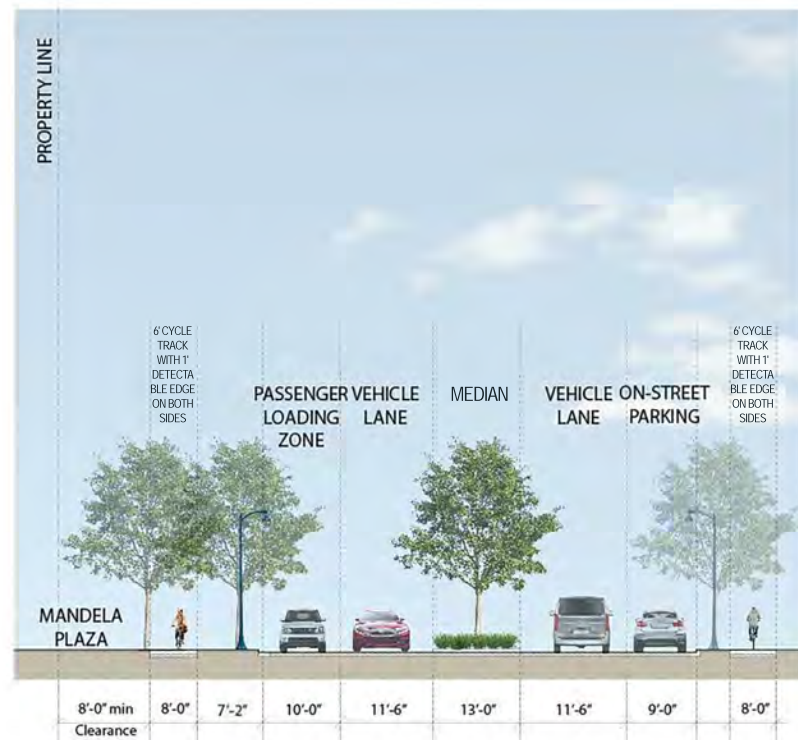
1 SECTION 3  
L1.04 1/16" = 1'-0"



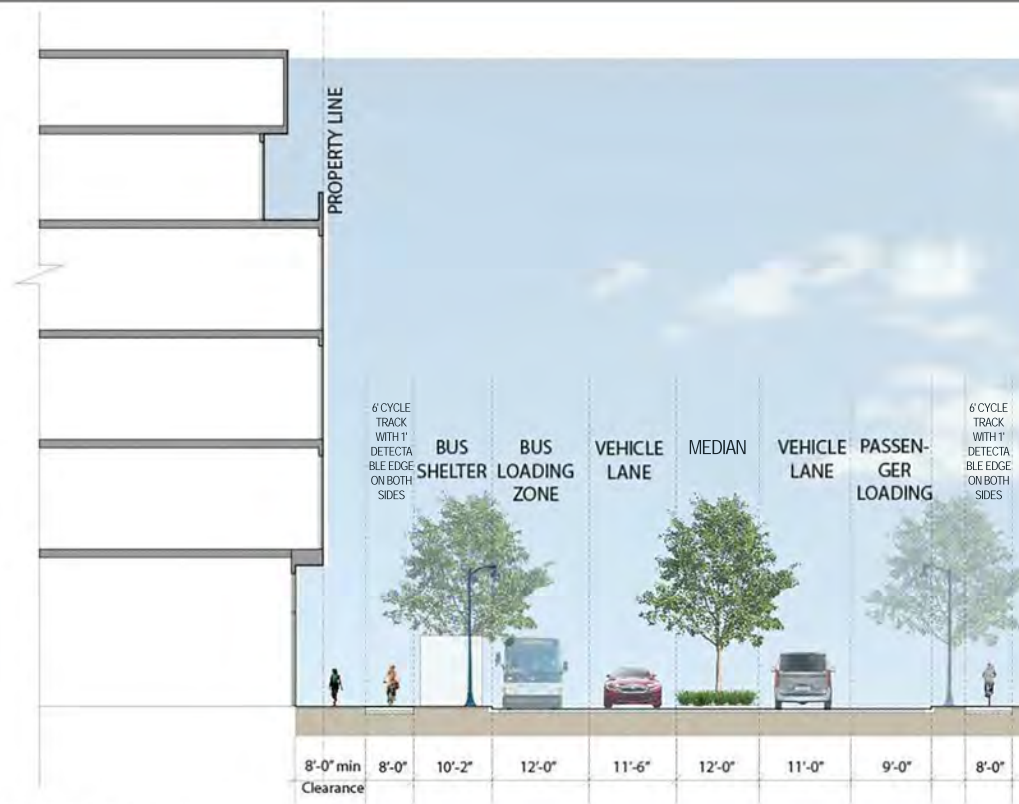
2 SECTION 4  
L1.04 1/16" = 1'-0"



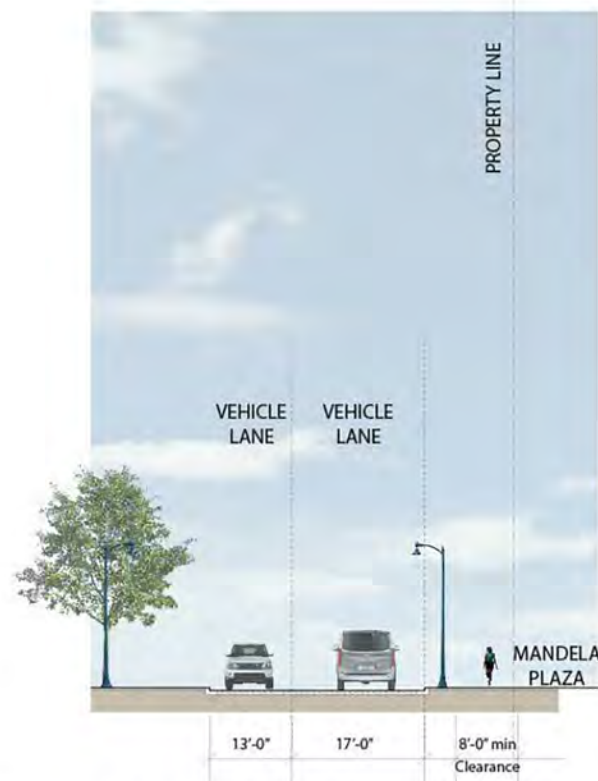




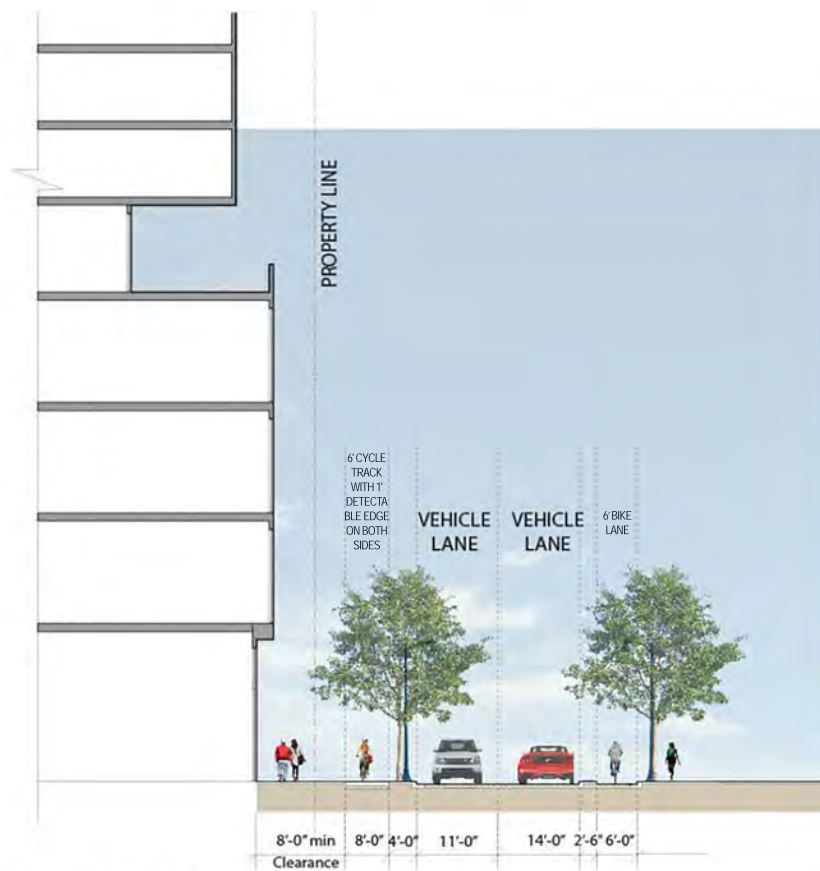
1 7TH STREET WEST SECTION  
L1.05 1" = 20'-0"



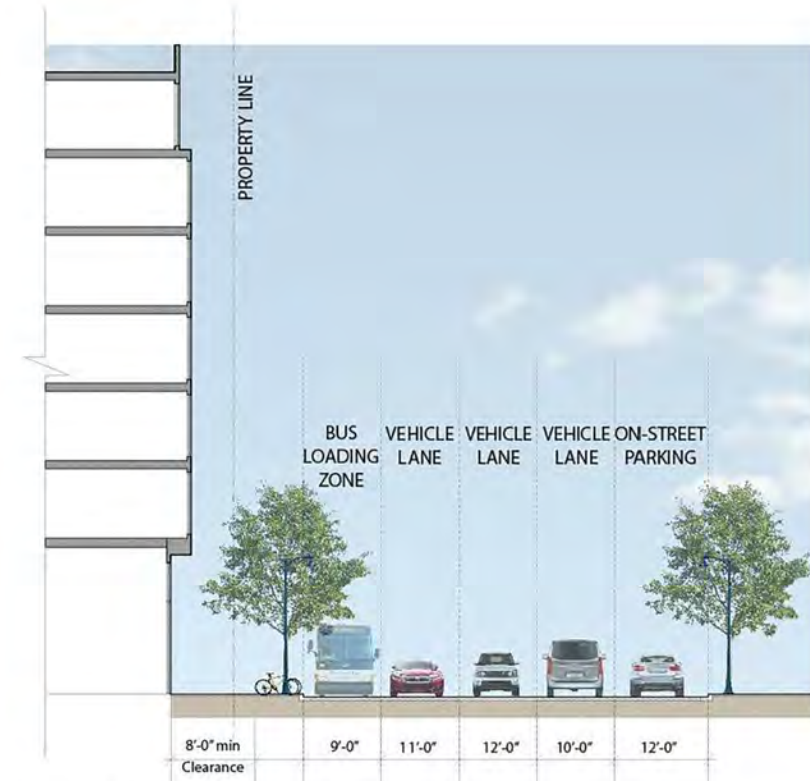
2 7TH STREET EAST SECTION  
L1.05 1" = 20'-0"



3 CHESTER STREET SECTION  
L1.05 1" = 20'-0"



4 MANDELA PKWY SECTION  
L1.05 1" = 20'-0"



5 5TH STREET SECTION  
L1.05 1" = 20'-0"

OWNER  
**HEC**  
HENSEL PHELPS

**suda**  
BUILDING T1 FDP PACKAGE

PROJECT  
**WEST OAKLAND BART**  
1451 7th St, Oakland, CA 94607

ARCHITECT  
**jrdv** ARCHITECTS

PROJECT TEAM  
BIG BROWERS  
H&K/SOPHIA  
BUTTS, BUTTS AND  
VON STORCH  
CA 94606  
415 740 2222

PGAdesign  
PGAdesign  
CA 94602  
415 450 0214

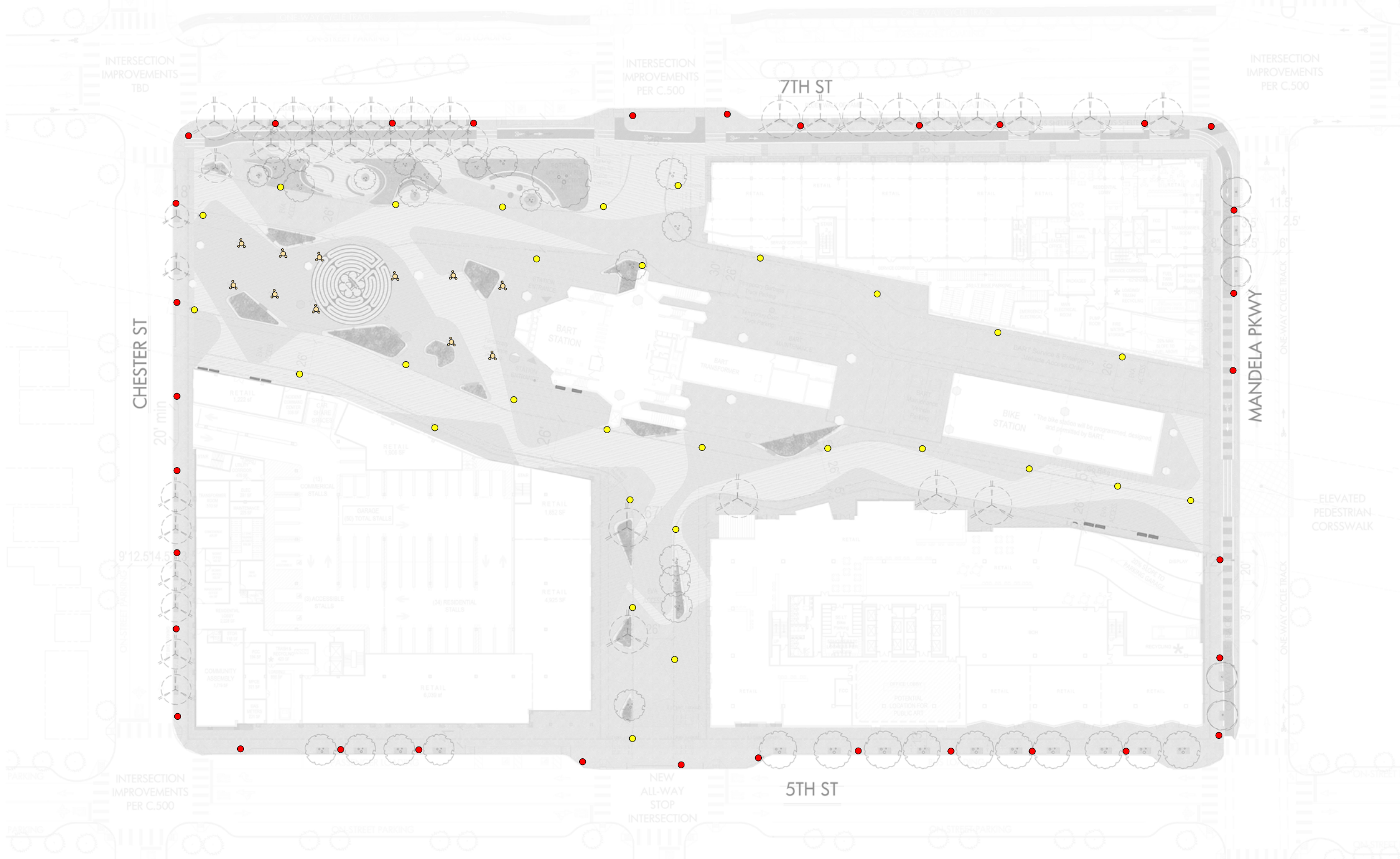
STOK  
948 11-01311  
101 1000000  
CA 94611  
415 220-7100

STAMP

KEY

PROJ # 100-150-000-BART  
DATE 06/04/20  
SCALE 1"=20'-0"  
TITLE **STREET SECTIONS**

SHEET: **L1.05**



**LEGEND - LIGHTING PLAN**

- AREA LIGHTS FOR PLAZA
- AREA LIGHTS WITH MULTIPLE HEADS FOR EVENT PLAZA
- STREET LIGHTS AS CITY REQUIRED



OWNER  
 HENSEL PHELPS

BUILDING T1  
 FDP PACKAGE

PROJECT  
**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607

ARCHITECT  
 ARCHITECTS

PROJECT TEAM  
 BKF ENGINEERS  
 1546 N CALIFORNIA BLVD, SUITE 400  
 WALNUT CREEK CA 94596  
 (925) 940-2200

PGA DESIGN  
 444 17th Street  
 Oakland CA 94612  
 (510) 465-1284

STOK  
 845B Front Street  
 San Francisco CA 94111  
 (415) 329-7100

STAMP

KEY

PROJ #: 168-153 WO BART  
 DATE: 03/26/20  
 SCALE: 1"=60' - 0"  
 TITLE: **LIGHTING PLAN**

SHEET: **L1.06**



**PLANTING LEGEND**

- |   |  |   |
|---|--|---|
| <p><b>PLANTING ZONE 1: MANDELA PLAZA ENTRY</b></p> <ul style="list-style-type: none"> <li>CAREX DIVULSA</li> <li>NANDINA DOMESTICA ALBA 'LEMON-LIME'</li> <li>BERBERIS 'ORANGE ROCKET'</li> <li>IRIS DOUGLASIANA 'CANYON SNOW'</li> <li>MUHLENBERGIA DUBIA 'PINE MUHLY'</li> <li>PITTOSPORUM TOBIRA 'MOJO'</li> <li>PHORMIUM 'YELLOW WAVE'</li> <li>CORDYLINE 'DESIGN-A LINE'</li> <li>DIGIPLIXIS ILLUMINATION 'APRICOT'</li> </ul> | <p><b>PLANTING ZONE 2: SHADE TOLERANT PLANTS BELOW BART TRACKS</b></p> <ul style="list-style-type: none"> <li>MAHONIA 'SOFT CARESS'</li> <li>MUHLENBERGIA DUBIA 'PINE MUHLY'</li> <li>BLECHNUM SPICANT 'DEER FERN'</li> <li>HELLEBORUS ARGUTIFOLIUS</li> <li>WOODWARDIA FIMBRIATA 'GIANT CHAIN FERN'</li> <li>CORDYLINE 'DESIGN-A LINE'</li> <li>EUPHORBIA CHARACIAS SSP.</li> <li>PHORMIUM 'YELLOW WAVE'</li> </ul> | <p><b>PLANTING ZONE 3: SOUTH FACING ALLEY</b></p> <ul style="list-style-type: none"> <li>CAREX DIVULSA</li> <li>MUHLENBERGIA DUBIA 'PINE MUHLY'</li> <li>IRIS DOUGLASIANA 'CANYON SNOW'</li> <li>MONARDELLA VILLOSA 'RUSSIAN RIVER'</li> <li>SALVIA SPATHACEA 'HUMMINGBIRD SAGE'</li> <li>SALVIA 'DARA'S CHOICE'</li> </ul> |
|---|--|---|

PLEASE SEE RELATED IMAGES ON THE NEXT PAGE

OWNER  
**HEC**  
 HENSEL PHELPS  
 suda  
 BUILDING T1  
 FDP PACKAGE  
 PROJECT

**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607

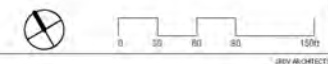
ARCHITECT  
**jrdr**  
 ARCHITECTS

PROJECT TEAM  
 CIVIL ENGINEERS: **BRT100**  
 FIRM IN CALIFORNIA: 8100 SUTTER AVENUE, SUITE 400, EMERYVILLE, CA 94608, (925) 885-2200  
 PLS DESIGN: 244 15th Street, Oakland, CA 94612, (510) 465-1284  
 STON: 8489 First Street, San Francisco, CA 94111, (415) 224-7100

STAMP

KEY

PROJ # 198-031 W/O BART  
 DATE 05/24/20  
 SCALE 1"=60' - 0"  
 TITLE **PLANTING ZONE**  
 SHEET **L1.07**



# TREES

## STREET TREES



ACER RUBRUM 'ARMSTRONG'



QUERCUS COCCINEA



AFROCARPUS FALCATUS



PISTACIA CHINENSIS

## PLAZA TREES



POPULUS DELTOIDES



CHITALPA TASHKENTENSIS



ALBIZIA JULIBRISSIN

# SHRUBS & GROUNDCOVER

## PLANTING ZONE 1 : ON THE PLAZA



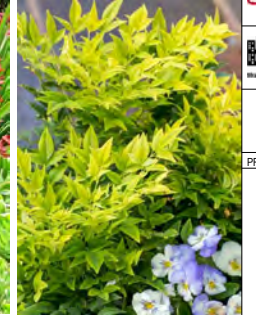
CAREX DIVULSA



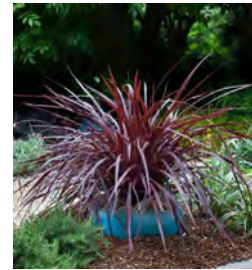
PHORMIUM 'YELLOW WAVE'



BERBERIS 'ORANGE ROCKET' 4'H X 2'W



NANDINA DOMESTICA ALBA 'LEMON-LIME' 4'H X 3'W



CORDYLIN 'DESIGN-A LINE' 3'X3'



PITTIOSPORUM TOBIRA 'MOJO' 3'X3'



DIGIPLEXIS ILLUMINATION 'APRICOT' 2'H X 18"W



IRIS DOUGLASIANA 'CANYON SNOW'



MUHLENBERGIA DUBIA 'PINE MUHLY'

## PLANTING ZONE 2 : UNDER THE TRUCK



MAHONIA 'SOFT CARESS' 4'X4'



PHORMIUM 'YELLOW WAVE'



CORDYLIN 'DESIGN-A LINE' 3'X3'



EUPHORBIA CHARACIAS SSP.



BLECHNUM SPICANT 'DEER FERN'



WOODWARDIA FIMBRIATA 'GIANT CHAIN FERN'



HELLEBORUS ARGUTIFOLIUS 3'X3'



MUHLENBERGIA DUBIA 'PINE MUHLY'

## PLANTING ZONE 3 : IN THE ALLEY



MUHLENBERGIA DUBIA 'PINE MUHLY'



MONARDELLA VILLOSA 'RUSSIAN RIVER'



IRIS DOUGLASIANA 'CANYON SNOW'



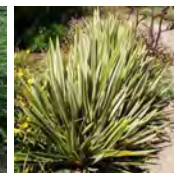
SALVIA SPATHACEA 'HUMMINGBIRD SAGE'



SALVIA "DARA'S CHOICE"



CAREX DIVULSA



PHORMIUM TONY TIGER' 2.5'X2.5

OWNER  
 HENSEL PHELPS

BUILDING T1 FDP PACKAGE

PROJECT

**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607

ARCHITECT  
 ARCHITECTS

PROJECT TEAM  
 BKT ENGINEERS  
 1566 K CALIFORNIA BLVD, SUITE 400  
 WALNUT CREEK CA 94599  
 (925) 940-2200  
 © Robert Perry

PGA DESIGN  
 444 17th Street  
 Oakland CA 94612  
 (510) 465-1284

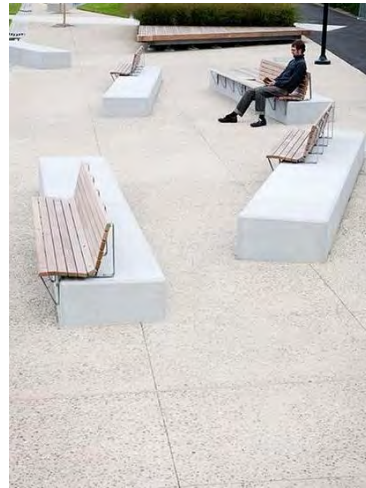
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 San Francisco CA 94111  
 (415) 329-7100

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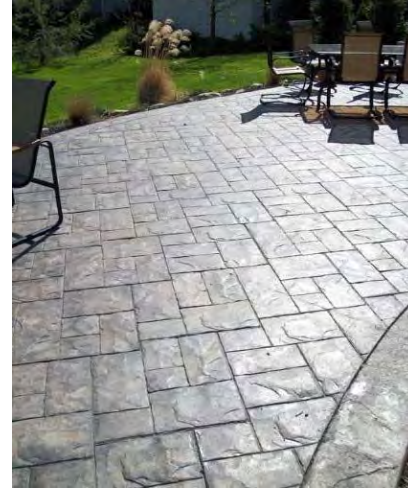
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 DATE: 06/04/20  
 SCALE:  
 TITLE: LANDSCAPE PLANTS  
 SHEET: L1.08

**CAST-IN-PLACE CONCRETE PAVING**



COLORED C.I.P w/ SAWCUT BANDING



COLORED C.I.P w/ SAWCUT BANDING



C.I.P w/ SAWCUT BANDING

NOTE: ALL PAVING SHALL BE ADA COMPLIANT AND MINIMIZE UNDULATING AFFECTS WHEN TRAVELING OVER SURFACE

**PLANTER WALL**



POUR-IN-PLACE CONCRETE WITH SACK FINISH

**SITE FEATURES**



SLOPED TURF



EVENT PLAZA

**SITE FURNITURES**



BENCH: MODEL: NEW STANDARD BENCH, MANUFACTURER: STREETLIFE, TEL: 215.247.0148, WEBSITE: <https://www.streetlife.nl/us>



BIKE RACK: MODEL: BOLA BIKE RACK, MANUFACTURER: LANDSCAPEFORMS, TEL: 800.430.6209, WEBSITE: <https://www.landscapeforms.com/en-US>



TRASH BIN: MODEL: CHASE PARK LITTER, MANUFACTURER: LANDSCAPEFORMS, WEBSITE: <https://www.landscapeforms.com/en-US>



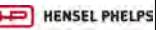
PICNIC TABLE SET: MODEL: GRETCHEN PICNIC TABLE, ADA COMPLIANT STYLE, MANUFACTURER: LANDSCAPE FORMS, TELEPHONE: 800.430.6209, WEBSITE: [WWW.LANDSCAPEFORMS.COM](http://WWW.LANDSCAPEFORMS.COM)



DINING TABLE & CHAIR SET: 21 CHAIR WITH ARMS BY LANDSCAPE FORMS, 4 CHAIRS PER TABLE, CATENA TABLE, 48" DIAMETER, NO UMBRELLA HOLE, ADA COMPLIANT QUAD SUPPORT LEGS, ADJUSTABLE GLIDES. FINISH: LANDSCAPE FORMS PROPRIETARY PANGARD II® POLYESTER POWDERCOAT COLOR: TBD FROM MANUFACTURER'S STANDARD PALETTE MANUFACTURER: LANDSCAPE FORMS, TELEPHONE: 800.430.6209, WEBSITE: [WWW.LANDSCAPEFORMS.COM](http://WWW.LANDSCAPEFORMS.COM)



OWNER



BUILDING T1  
FDP PACKAGE

PROJECT

**WEST OAKLAND BART**  
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM



STAMP

KEY

PROJ #: 168-153 WO BART

DATE: 06/04/20

SCALE:

TITLE:

**LANDSCAPE MATERIALS**

SHEET:

**L1.09**

STREET LIGHT

# Domus Luminaire



Manufacturer(s): Lumec

Catalog #: DM550-HPS-SC3M-5MB-SCITX (Large Domus)  
DOSHPS-SG3-5MB-SCITX-LMS (Small Domus)

Photometric File: 50105312.ies (Domus DM550, 200-250W)  
50206072.ies (Domus DOS, 35-150W)

Application: Domus DM550 - Arterial and/or collector street lighting  
Domus DOS - Residential street lighting and/or sidewalk side pedestrian lighting

Lamp Type: 35W, 100W, 150W, 250W HPS

Optical System: Type III

Ballast Voltage: Multi-tap 120/240V

Color: Teal Green only

Special Requirements:

Luminaire Cost: \$\$\$

As shown: Domus DOS luminaire w/ 3' arm on 16" octagonal pole.

Color: Teal Green

Location: Coliseum Gardens



Page 19 of 71

PEDESTRIAN LIGHTING



PLAZA LIGHTS



landscape lighting

landscape lighting

## ALCOTT AREA LIGHT



The Alcott Area Light is a contemporary interpretation of a traditional lantern. Its gently curved form is inspired by a classic energy-efficient LED luminaire paired with a translucent lens. The Alcott Area Light is constructed with multiple energy-efficient side directional lighting, reducing light pollution and wasted energy.

### General Description

- Offered in 3 standard mounting heights (12', 14' and 16')
- Asymmetrical and symmetrical distribution sizes available
- Mounting hardware included (bracket, hardware, and cable)
- Accurate, complete, and consistent product line for installation
- Durable light, maintenance-free, and easy to clean
- UL Listed, ETL Listed, and CE Marked

### Electrical

Single pole, 100V-277V (100V) HPS, LED (2-ohm driver) enclosed with a rugged, cast aluminum enclosure. LED lighting with a variety of optics (available) available with 100V, 277V, and 480V ballasts.

### Alcott Type 3

Task 3 (3000K): Structure Housing, LED luminaire and cabinet are cast aluminum with a powder-coated finish. Lamp: 48" Dia (18" H) LED. CCT: 3000K, 3500K, 4000K. Color: Amber, White, Cyan, Amber, Cyan. Lens: Diffused Acrylic. Power Supply: 100V-277V. LED Driver: 100V-277V. Dimensions: 12" Dia, 100V-277V. IP Rating: IP68 for LED luminaire.

### Alcott Type 5

Task 5 (3000K): Structure Housing, LED luminaire and cabinet are cast aluminum with a powder-coated finish. Lamp: 48" Dia (18" H) LED. CCT: 3000K, 3500K, 4000K. Color: Amber, White, Cyan, Amber, Cyan. Lens: Diffused Acrylic. Power Supply: 100V-277V. LED Driver: 100V-277V. Dimensions: 12" Dia, 100V-277V. IP Rating: IP68 for LED luminaire.



## ALCOTT AREA LIGHT



### Finish

Finished in a standard landscape finish, a 10-step program of cleaning, priming, and painting that ensures durability, longevity, and resistance to corrosion. The finish is available in a variety of colors to match your landscape design.

### To Order

Product	Lamp	Mounting	Color	Height	Weight
AC	48" LED (Type 3)	12' (144")	Amber	12' (144")	12' (144")
AC	48" LED (Type 5)	12' (144")	Amber	12' (144")	12' (144")

### Product Modifications

Consult your local distributor for more information. Product modifications are subject to change without notice. ©2015 Luminaire Systems, Inc. All rights reserved.

### Warranty

LED lighting is warranted for 5 years.



PEDESTRIAN ACCENT LIGHT



OVERHEAD CABLE LIGHTS

**Lomax LED luminaire**

**Specifications**

**Luminaire Housing/Plunger**  
Overhead luminaire housing made from low-iron, clear polycarbonate. Plunger design allows for easy installation and removal. Housing is made from polycarbonate with a clear lens and a clear lens cap. Housing is made from polycarbonate with a clear lens and a clear lens cap.

**LED Driver**  
LEDs are driven by a built-in constant current driver with a built-in surge protector. The driver is made from high-quality components and is designed for long life.

**Surge Protection**  
The luminaire includes a built-in surge protector to protect the LEDs from voltage spikes.

**Power Cord**  
The luminaire includes a built-in power cord with a standard AC plug. The power cord is made from high-quality materials and is designed for long life.

**LED Optics**  
The luminaire includes a built-in LED optic that provides a wide beam spread. The optic is made from high-quality materials and is designed for long life.

**Settings and Ratings**  
The luminaire is designed for use in outdoor environments. It is rated for use in wet locations and is made from high-quality materials.

**Warranty**  
The luminaire is warranted for 5 years.

UNDERTRACK LIGHTING

**UZA-20001 Zaab 2 Single Side Cluster Column**

**Construction**  
The Zaab 20001 is constructed from high-quality materials and is designed for long life. It features a built-in surge protector and a power cord with a standard AC plug.

**Adjustable surface spotlight family**  
The Zaab 20001 is part of the adjustable surface spotlight family, which offers a wide range of options and accessories. The family includes various beam spreads and mounting options.

**Mounting Details**  
The Zaab 20001 is designed for easy installation. It features a built-in mounting bracket that allows it to be mounted to a variety of surfaces.

**Additional Options**  
The Zaab 20001 is available in a variety of colors and finishes. It also offers a range of accessories, including different beam spreads and mounting options.



OWNER: **HEC** HENSEL PHELPS

PROJECT: **suda** BUILDING T1 FDP PACKAGE

ARCHITECT: **jrdrv** ARCHITECTS

PROJECT TEAM: **BKF ENGINEERS**, **BKT 100**, **PGA DESIGN**, **PGAdesign**, **STOK**

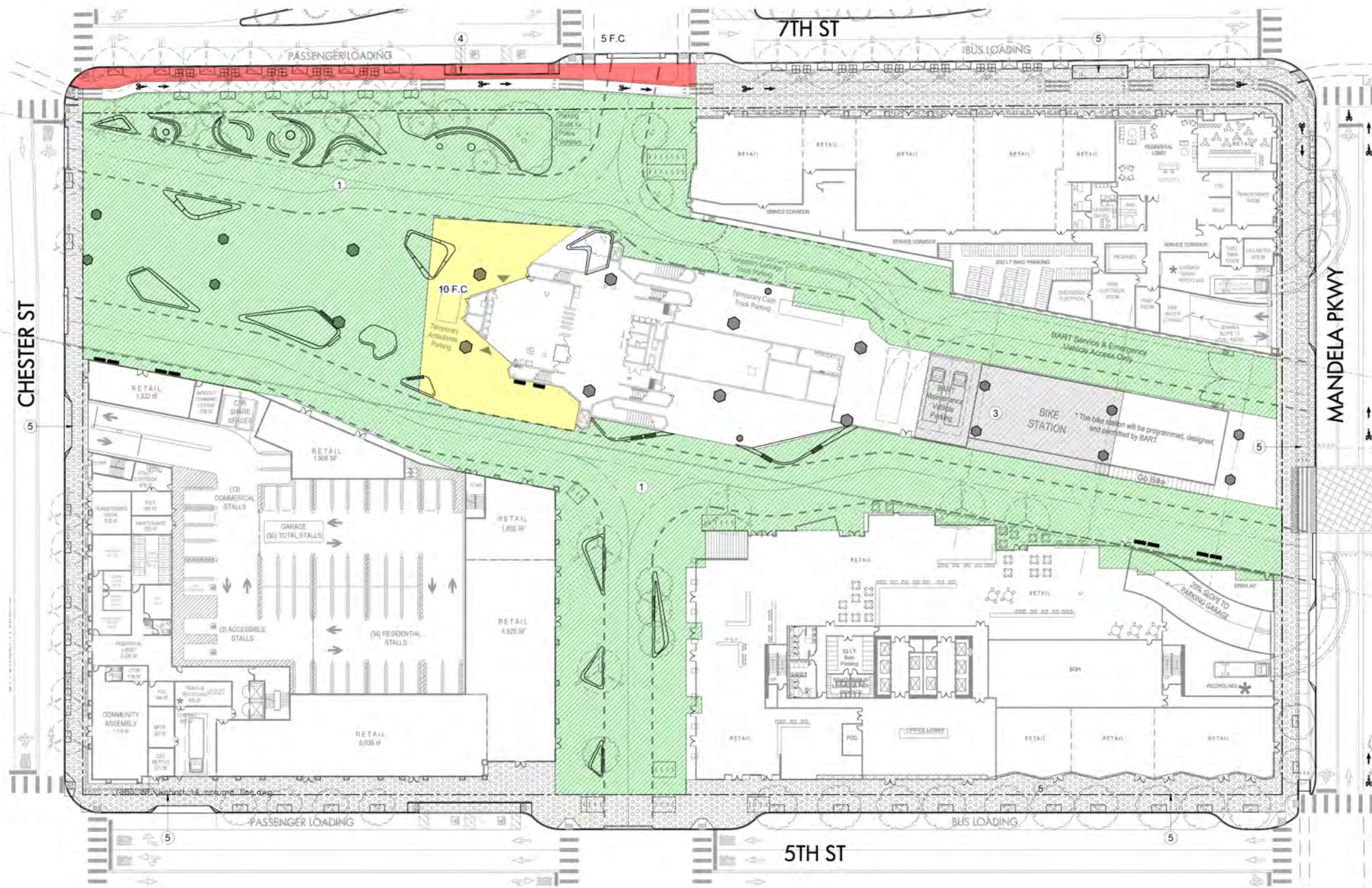
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KEY: \_\_\_\_\_

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DATE: 06/04/20  
SCALE: \_\_\_\_\_  
TITLE: LIGHTING IMAGES

SHEET: L1.10

WEST OAKLAND BART  
1451 7th St, Oakland, CA 94607



**LIGHTING PHOTOMETRIC GOALS**

AREA	FOOT CANDLE	LIGHT TYPE	CONTROLS
1. PEDESTRIAN WALKWAY	2 MIN	LED	POWER-LINE CARRIER TECHNOLOGY
2. EXTERIOR PATRON WAITING AREA	10	LED	DIMMABLE, TO REDUCE CONTRAST
3. BIKE LOCKER AREA	10	LED	SAME AS SURROUNDING SPACE
4. LOADING, UNLOADING, KISS-AND-RIDE AREAS, AND PEDESTRIAN WALKWAYS	5	LED	PHOTOCELL AND POWER-LINE CARRIER TECHNOLOGY OR WIRELESS CONTROL
5. OTHER EXTERIOR SPACES	1.25	LED	POWER-LINE CARRIER

OWNER  
**HEC**  
**HENSEL PHELPS**  
**suda**  
 BUILDING T1  
 FDP PACKAGE  
 PROJECT

**WEST OAKLAND BART**  
 4784-R-#R-#R-#R-#R-#R-D-#-793

ARCHITECT  
**irdv**  
 ARCHITECTS  
 irdv Architects, Inc.  
 The Commercial Building  
 Architects and Engineers  
 1000 W. CALIFORNIA  
 OAKLAND, CA 94612 USA  
 510.546.4327  
 www.irdv.com

PROJECT TEAM  
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 CA 94612  
 (510) 940-2200

PGA DESIGN  
 444 17th Street  
 Oakland  
 CA 94612  
 (510) 465-1214

STOK  
 9420 Front Street  
 San Francisco  
 CA 94111  
 (415) 329-7100

STAMP

KEY

PROJ # 168.153 WO BART  
 DATE 03/26/20  
 SCALE 1"=60'-0"  
 TITLE LIGHT LEVEL GOALS

SHEET L1.11



1 - Mandela Plaza Center Street Entrance



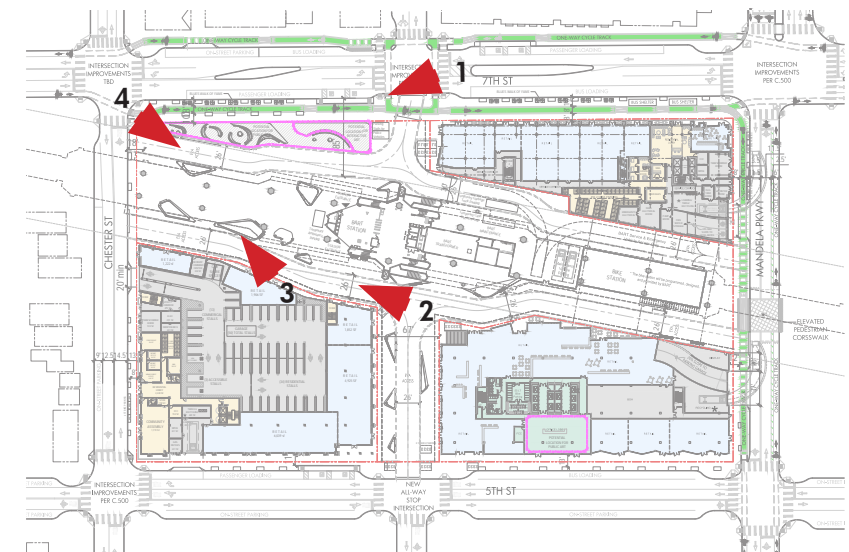
2 - Artwalk from Center Street



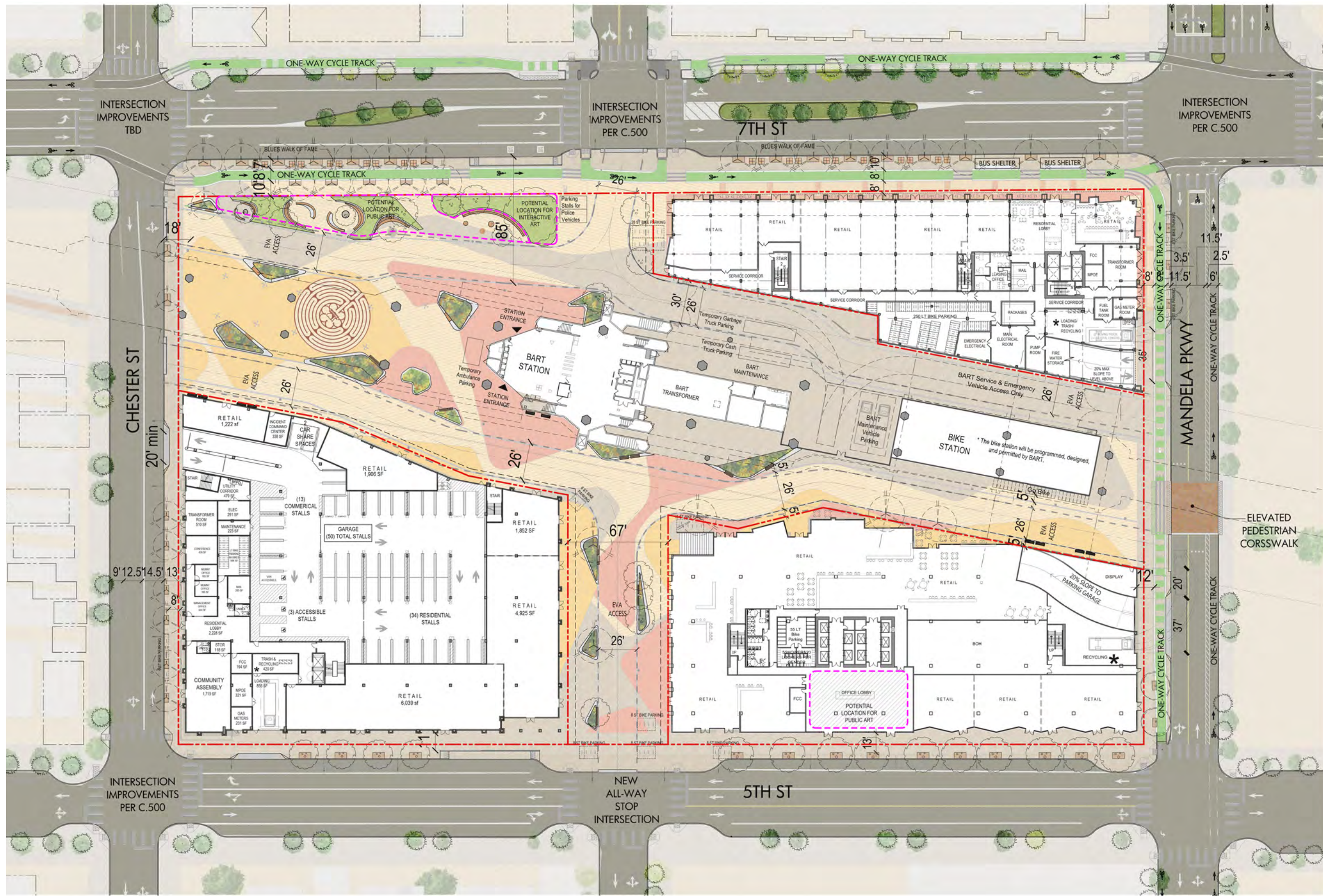
3 - Undertrack Plaza from Artwalk



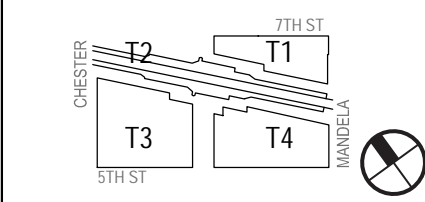
4 - Mandela Plaza Chester Street Entrance and Blues Walk of Fame







**MANDELA STATION  
 @WEST OAKLAND  
 BART**  
 1451 7th St, Oakland,  
 CA 94607



**JRDV URBAN  
 INTERNATIONAL**  
 JRDV Architects, Inc  
 The Cathedral Building  
 Broadway and Telegraph  
 PO Box 70126  
 Oakland, CA 94612 USA  
 510 295 4392 T  
 www.jrdv.com

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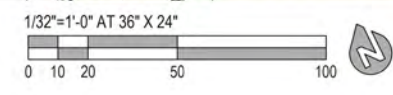
PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

**SITE PLAN**

SHEET:

**A-9.00**

The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.



# Public Space Design

## Public Space Improvements:

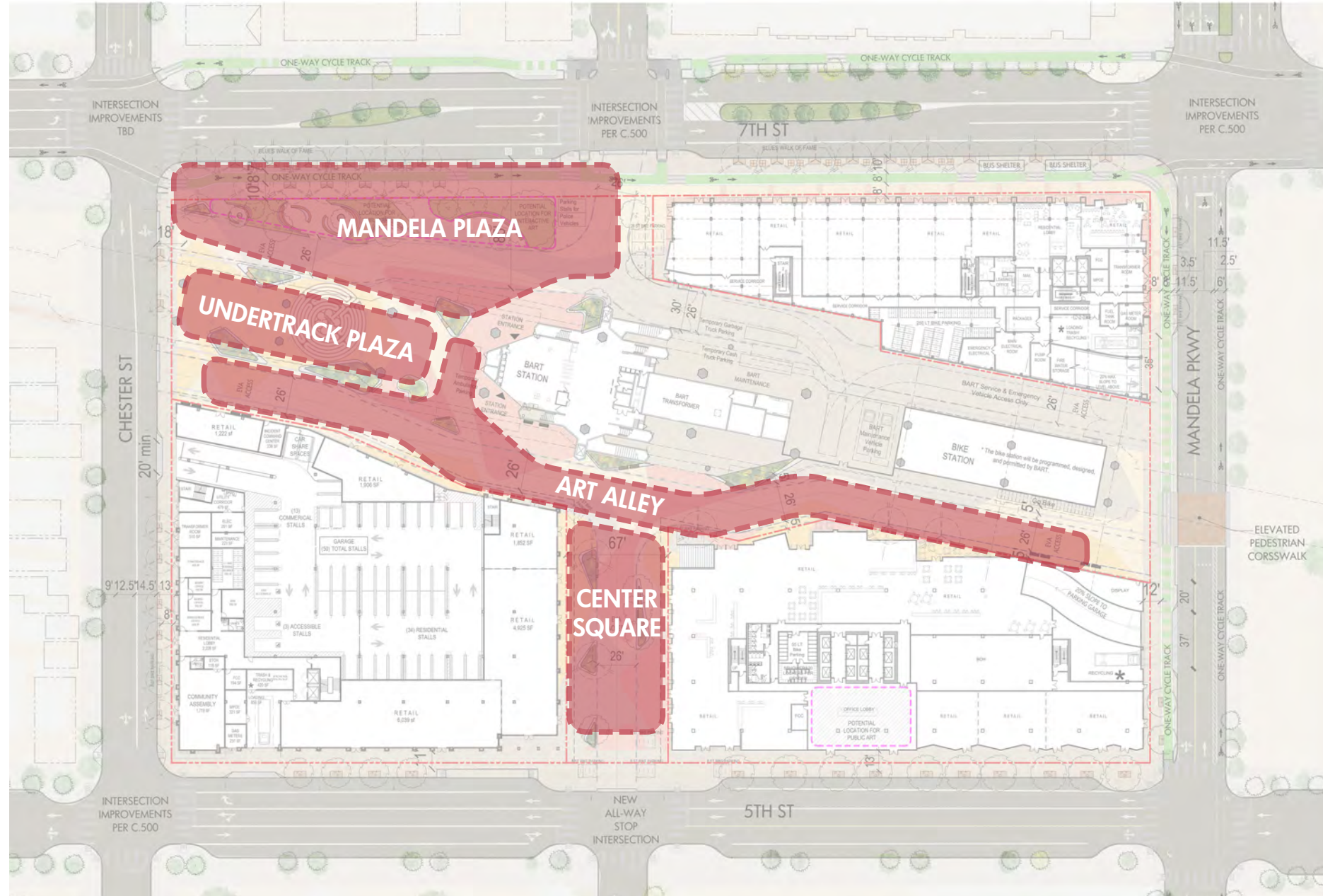
The public spaces for the Mandela Station site are designed to reinforce the vibrant mixed-use development that furthers Oakland's efforts to promote urban living at key transit sites, and provides an active and delightful center for the West Oakland community.

The site is designed with a series of important civic open spaces, including:

- **Mandela Plaza**, a major community civic plaza at 7th Street fronting BART Station entrance. This plaza is designed with varied paving and green spaces. It is designed with a delightful series of sculptural seating areas arranged to promote community interaction – a “Living Room” for the residents and surrounding neighborhood.
- **Center Square**, a pedestrian plaza replacing the vacated Center Street. This plaza is focused on providing food related tenants along T3 and T4, and will be designed with fixed and permanent seating to become a destination food hub for the neighborhood, BART patrons and the on-site residents.

• **Art Alley**, a pedestrian paseo that activates the area south of the BART tracks. This area is lined with small gallery spaces and will have a expansive “art wall” along the north façade of T3 framing the alley space.

• **Undertrack Area**, a flexible event space that will provide a venue for the programs that activate Mandela Plaza and Art Alley. The under track will feature movable food kiosks, pop-up events, farmers markets, art fairs, live music and other programmed events that will help make Mandela Station a destination location.



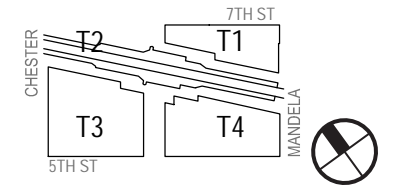
**Hardscape and Green-scape Design:** Landscape plan is designed to enhance the pedestrian public spaces, and to create a high quality of pedestrian experience and civic prominence. The existing trees will be replaced because of conflicts with the access plan. The new tree planting complements the overall landscape strategy of the 7th and 5th Street corridors to ensure a continuous, interesting and varied visual experience.

**Landscape Materials:** The landscape materials are designed with pavers or stamped concrete and other decorative materials to create a high quality public pedestrian experience and to maximize the types of uses that can occur on site. The landscape is designed to become a catalyst to a larger vision for the 7th Street corridor. The new tree planting complements the overall landscape strategy of the 7th Street corridor to ensure a continuous, interesting and varied visual experience.

**Site Lighting:** The Lighting plan is designed to create well lit plazas and pedestrian pathways through the site. The visual security of all pedestrian spaces within the site is facilitated by locating retail and other public activities along all edges of the development. The landscape plan provides adequate lighting to provide a safe environment while conforming to current best practices to mitigate light pollution.



**MANDELA STATION  
@WEST OAKLAND  
BART**  
1451 7th St, Oakland,  
CA 94607



**JRDV URBAN INTERNATIONAL**  
JRDV Architects, Inc  
The Cathedral Building  
Broadway and Telegraph  
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Oakland, CA 94612 USA  
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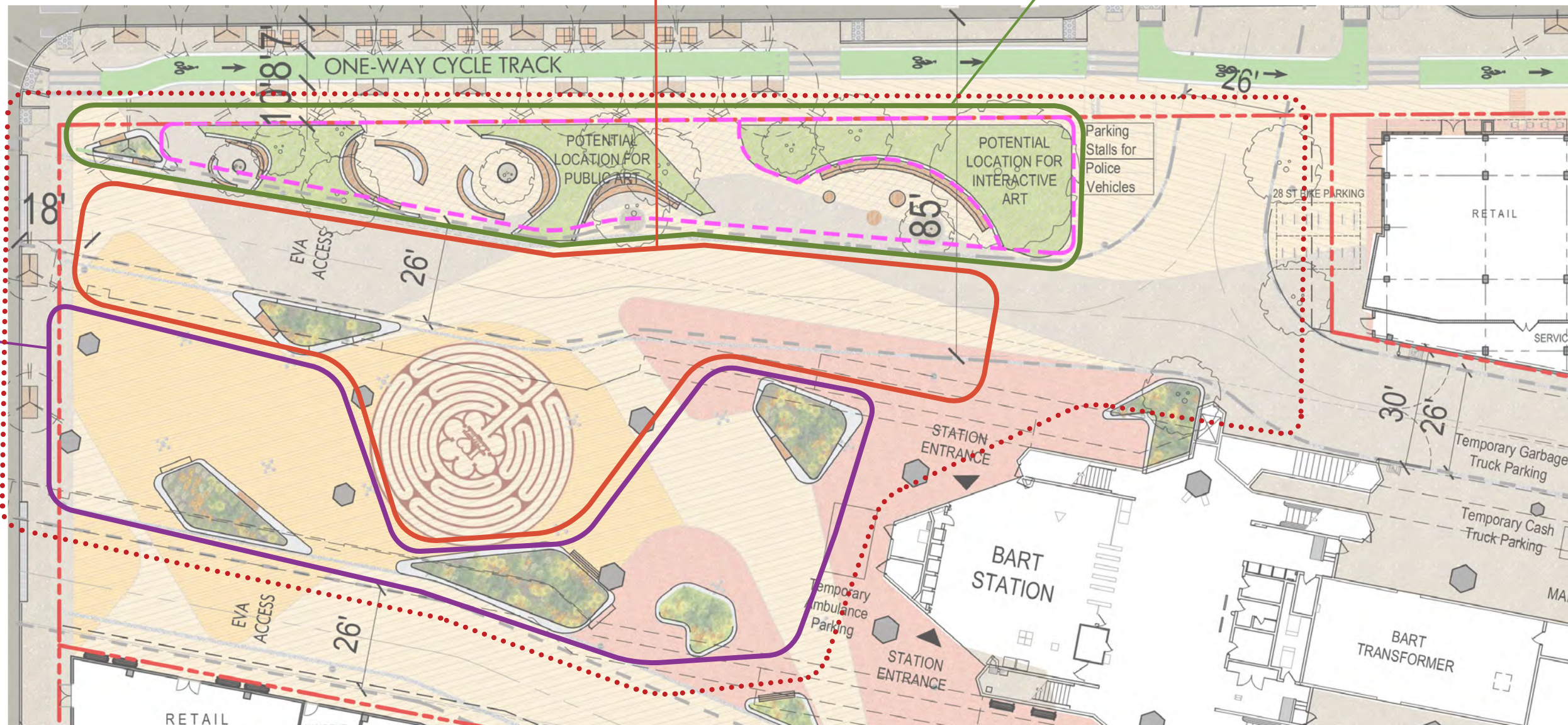


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DATE: July 24, 2020

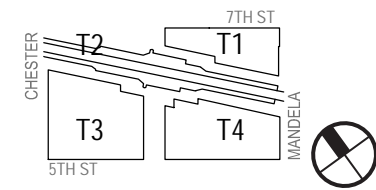
## PUBLIC SPACE IMPROVEMENT: PUBLIC SPACE DESIGN

SHEET: A-9.01

# Mandela Plaza



**MANDELA STATION  
 @WEST OAKLAND  
 BART**  
 1451 7th St, Oakland,  
 CA 94607



**JRDV URBAN INTERNATIONAL**  
 JRDV Architects, Inc  
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 Broadway and Telegraph  
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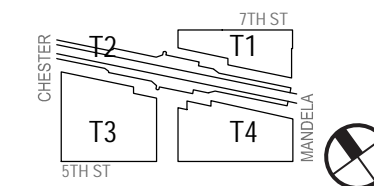
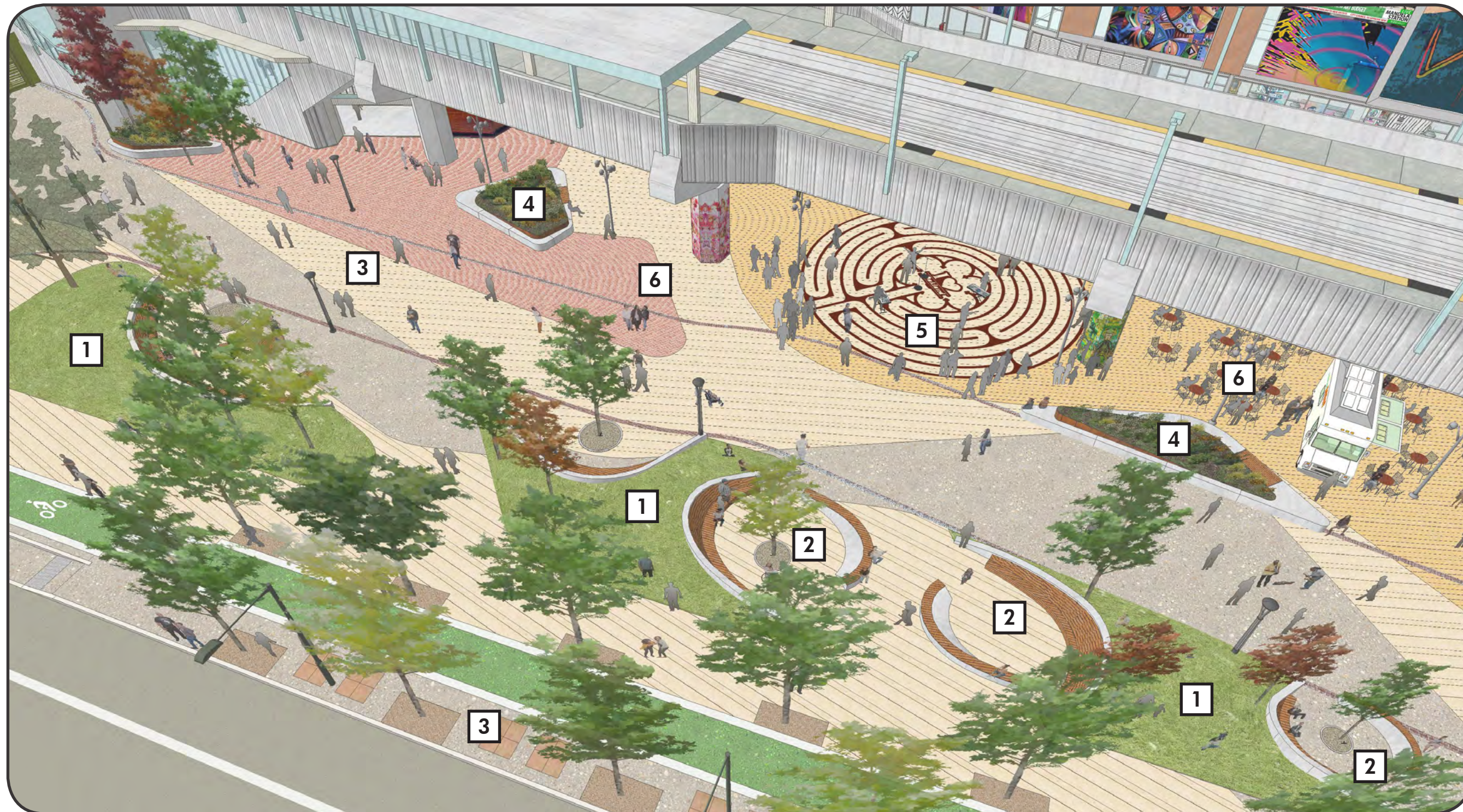
PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

## PUBLIC SPACE IMPROVEMENT: MANDELA PLAZA

SHEET:

A-9.02

Note: Undertrack painting is optional pending BART's approval.



## Mandela Plaza Features

- 1** **SCULPTED BERMS** - Artificial turf berms to promote informal seating and interactive play options for a wide range of age groups.
- 2** **OUTDOOR 'LIVING ROOMS'** - Nodes framed with integral bench seating for small gatherings.
- 3** **BLUES WALK OF FAME** - Relocated plaques cast into 7th Street sidewalk paving north of Mandela Plaza and the T1 building.

- 4** **BENCH PLANTERS** - Concrete planters with consistent, bench-height edge to provide fixed seating option for plaza events.
- 5** **MAZE STRIPING** - Maze for public interaction. This open space is flexible for performances and other event activities.
- 6** **ADDITIONAL OPEN SPACE** - Framed open space for food trucks, outdoor dining, and other temporary programming.

# Public Space Programming and Activation

The Developer will provide regular programming in the public plazas of the proposed Mandela Station at West Oakland BART Station as required in the Conditions of Approval (COA #66) approved on February 6, 2019 by the City of Oakland Planning Commission. The purpose of the programming is to provide future BART riders, project-site residents, shoppers and workers, and the larger community of West Oakland with excellent opportunities to relax and enjoy as well as providing local businesses and non-profits to financially support themselves and promote their services and products.

Programmed events may include:

1. Concert events will be held on a monthly schedule in co-operation with the Developer and local non-profit Zoo Labs among others. Zoo Labs empowers music artists as leaders and directs resources toward their ventures.
2. Farmers Market events will be held monthly in collaboration with the Mandela Grocery Cooperative, a worker-owned grocery store that provides nourishing foods and enhanced opportunities for food vendors, including the Freedom Farmers Market weekly from July to November.
3. Local vendor Pop-Ups events will be held monthly and co-sponsored by PopUp Village. Popupvillage.org features opportunities including:
  - Arts and Culture classes and exhibits
  - Food & Agriculture
  - Health & wellness
  - Youth Activities
  - Highlights local retail vendors
  - Performers of music, dance, spoken word
  - Education
  - These are a few of the collaborator of existing programs on 7th Street that as developers we intend to support and expand in collaboration with these organizations.

All program schedules and events will be advertised widely using: BART Station Public Information System, the networks of organizations that participated in the West Oakland Advisory Committee process and the local media outlets.



Community Art Fair



Local Farmers Market



Undertrack Program Events



Food Truck Events



Undertrack Program Events



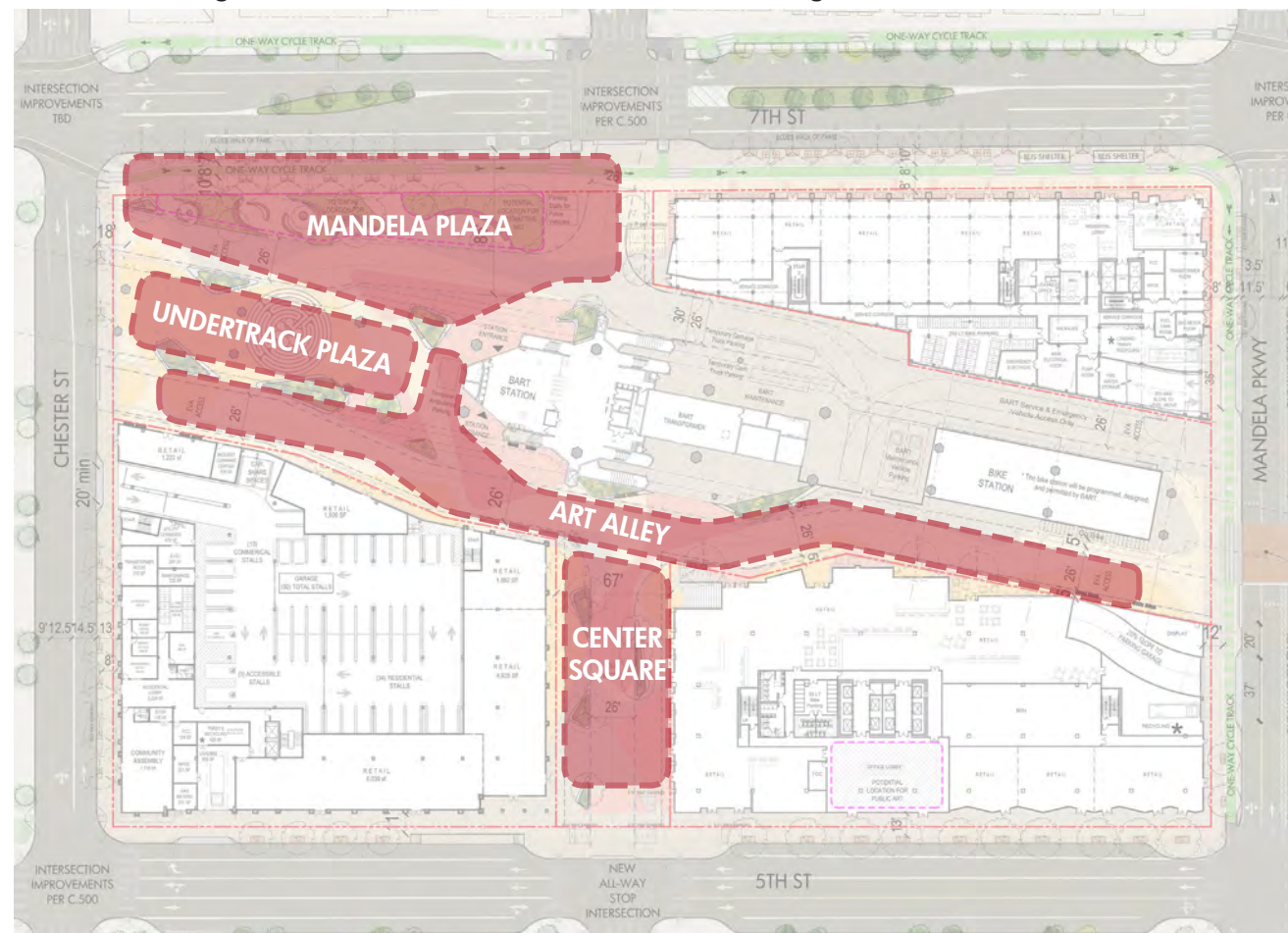
Undertrack Program Events



Food Truck Events



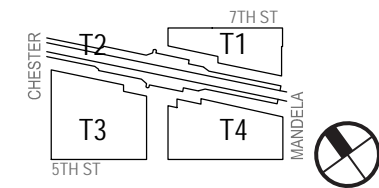
Sunday Music Show



Public Open Space Map



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PROJ. # 168-153 WO BART  
DATE: July 24, 2020

## EVENT SPACE PROGRAM

SHEET:

A-9.04

# Under Track Strategy

The Under-Track area is designed to integrate with both Mandela Plaza on 7th Street and Art Alley to the south. These combination of these spaces are planned to be programmed and perceived both separately and as linked spaces.

**Programmed Activation.** Under-Track area is designed as a flexible and activated space that will support food kiosks, exhibits and a wide range of staged events. The physical space of the Under-Track area serves these programmed activities by creating a defined space with high “ceiling” and significant scale, that is visually connected to the surrounding Public Spaces. The space also functions as an interesting urban plaza when no events are staged. It becomes an delightful and safe passage through the site.

**Landscape Paving.** The paving patterns are designed to define the space and encourage pedestrians to flow through the space. The variety of paving materials is designed to reduce the scale of the space, by implying nodes and clusters that can contain smaller groups or events. The paving pattern and variety of materials will also create a visually interesting plaza surface that does not require any activation or events.

**Seating.** Seating is provided with a combination of fixed-seating that is integrated into the planters. And, movable café-seating and tables to be placed and managed by specific events. The goal is to promote this space as both safe pedestrian passage through the site, and as a place to rest or participate in a planned event. The number of movable seats will be managed by the client and will respond to the actual demand.

**Lighting.** The lighting of the Under-Track area is designed to provide a high level of ambient light and “decorative” feature lighting. The ambient light is provided by a double row of 10’high light standards with a fixture that provide directional up-lighting and ground-lighting. Feature lighting will be provided with either fixtures attached to the BART structure, or fixtures attached to the light standards. This lighting will provide changeable artistically colored lighting that creates a powerful night presence that extends to the plaza and 7th Street. Lighting levels are designed to ensure visual safety for this space at all times.

**Decorative Surface Painting.** The columns of the BART structure may be painted with a surface graphic that is colorful and visually animates the space. This painted surface will create a delightful backdrop that will enhance the space for events and for the enjoyment of the public.

Note: Undertrack painting is optional pending BART’s approval.



1 - From BART Station North Entrance



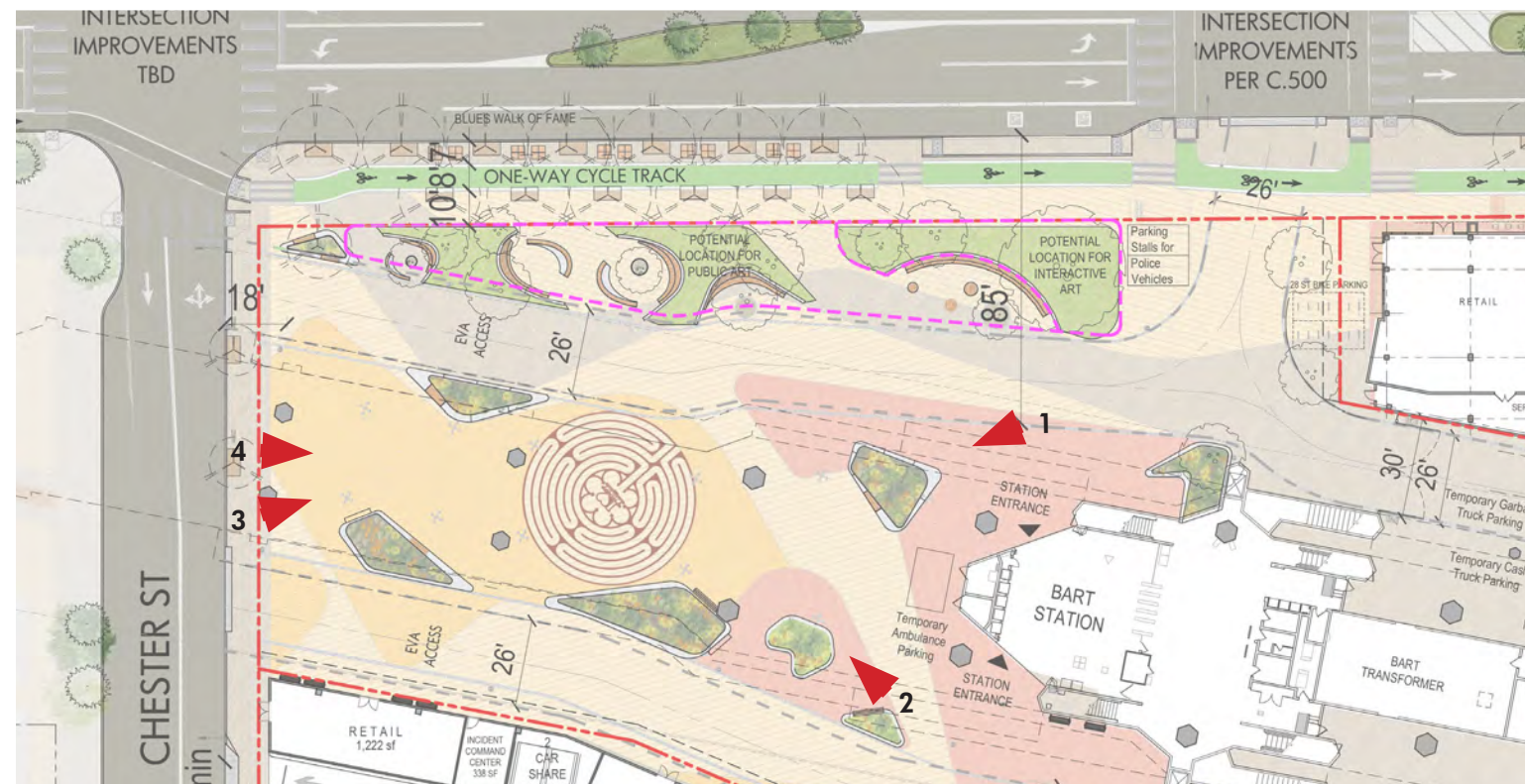
2 - From Art Walk



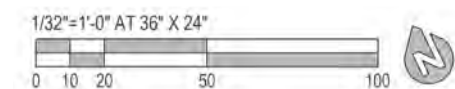
3 - From Mandela Plaza



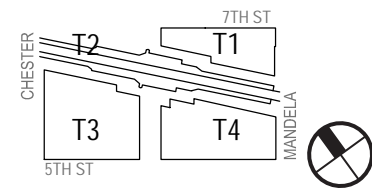
4 - From Under Track Plaza



Partial Site Plan



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UNDER TRACK  
SPACE

SHEET:

A-9.05

# Under-Track Activation



Farmers Markets



Food Trucks and Mobile Enterprise



Music and Staged Events



Off The Grid Events



Night Markets

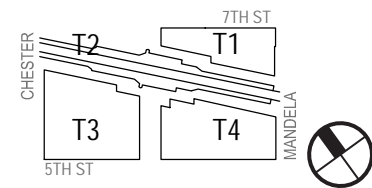


Art Fairs and Pop-up Events



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UNDER TRACK  
SPACE

SHEET:

A-9.07

## Mandela Plaza - Community Civic Space:

This urban plaza creates a major community civic plaza at 7th Street fronting BART Station entrance. It is designed with a delightful series of sculptural seating and green-berm areas arranged to promote community use – a “Living Room” for the residents and surrounding neighborhood. The plaza provides access to the BART station, and also celebrates the civic importance of this site in the community. This plaza is located centrally to the overall site in order to increase its public importance, public access, and public use for community, arts and cultural events. This central plaza is designed to enhance the 7th Street corridor activation and overall public experience.

This plaza is designed with varied paving and green spaces. The Landscape plan is designed to complement the importance and use of the space by the neighborhood and residents on site. The arced cluster seating and green berms along 7th Street create a welcoming pedestrian and community experience.

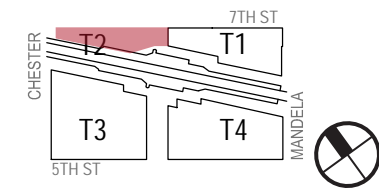
The new tree planting complements the overall landscape strategy of the 7th Street corridor to ensure a continuous, interesting and varied visual experience. Paving materials are quality pavers or stamped colored concrete and are aesthetically designed to differentiate unique spaces within the pedestrian plazas, promote visual access to the BART station entrance, and to create opportunities for cultural, community and arts events. The landscape plan is designed to create a delightful destination for the West Oakland community and users of the transit hub.



VIEW FROM 7TH STREET



VIEW FROM 7TH STREET





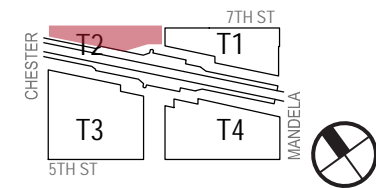


VIEW OF BART ENTRANCE AT 7TH STREET



VIEW FROM CHESTER STREET

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**MANDELA PLAZA**

SHEET:

**A-9.09**



VIEW FROM 7TH STREET

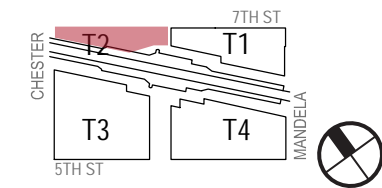


VIEW FROM 7TH STREET



VIEW OF 7TH STREET SIDEWALK AND LANDSCAPED BERM

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 LANDSCAPE ARCHITECTS

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**MANDELA PLAZA**

SHEET:

A-9.10



VIEW FROM 7TH STREET

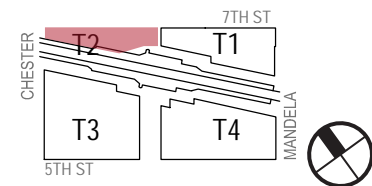


VIEW OF MANDELA PLAZA



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MANDELA PLAZA

SHEET:

A-9.11

# Art Alley



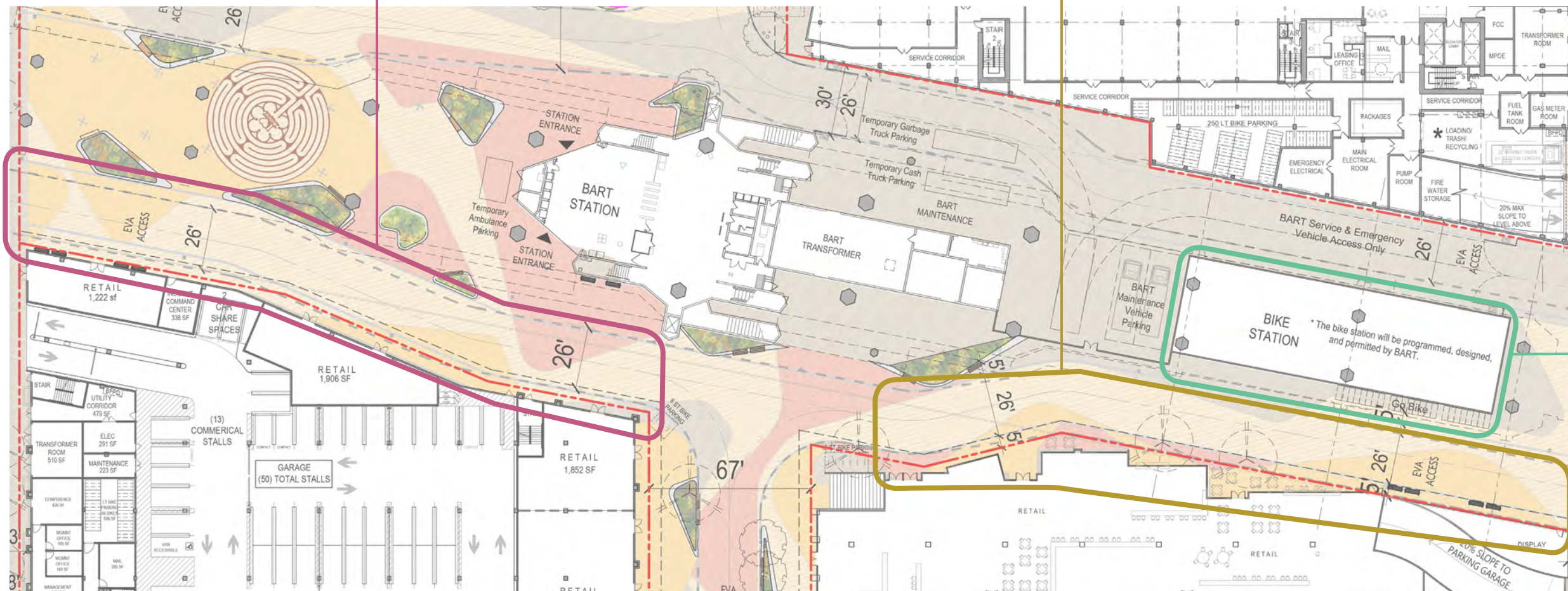
Art Walk Events



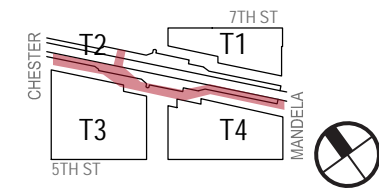
Art Walk Activated Edges



Places for Music and Poetry Events



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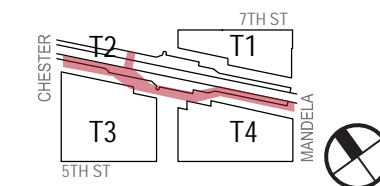
PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

**PUBLIC SPACE  
 IMPROVEMENT:  
 ART ALLEY**

SHEET:  
**A-9.12**



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**ART ALLEY**

SHEET:  
**A-9.14**

# Center Square

**Center Square:** The vacated Center Street has been transformed into an active urban destination for the neighborhood. This space is both an important pedestrian connection to the BART station and a significant public plaza for the surrounding community. It is lined with a wide variety of local food offerings that showcase the cultural and ethnic diversity of West Oakland. The space creates an inviting place for the neighborhood to shop, dine, and relish a quality community experience.

The landscape materials are designed with high quality paver, brick, finished concrete and other materials to create a high quality public pedestrian experience and to maximize the types of uses that can occur on site.

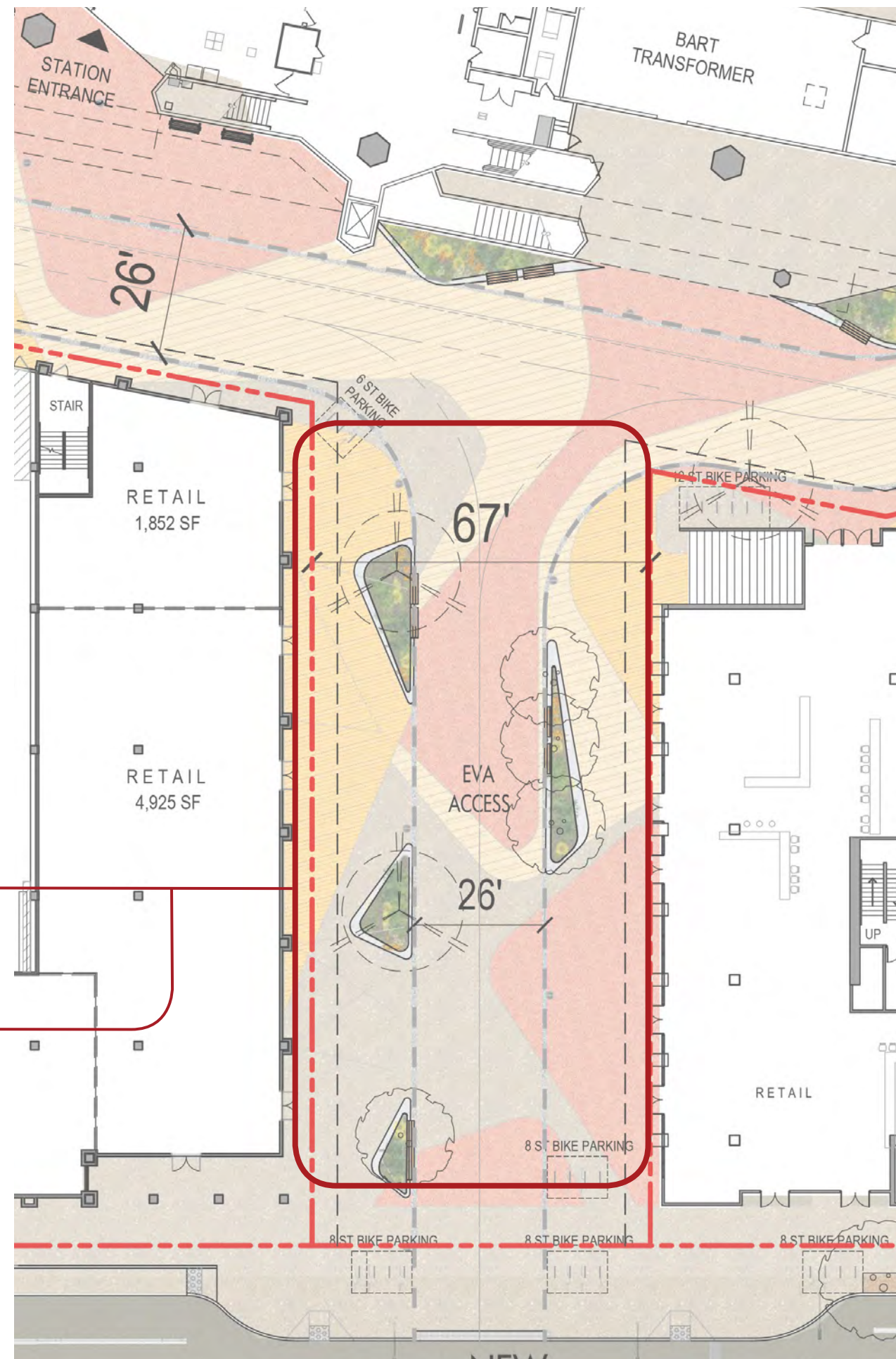
**Site Lighting:** The Lighting plan has been designed to create a well lit space that connects BART to 5th Street. The visual security of pedestrians within this space is facilitated by locating retail and other public activities along all edges of the development. The landscape plan is designed with adequate lighting to provide a safe environment while conforming to current best practices to mitigate light pollution.



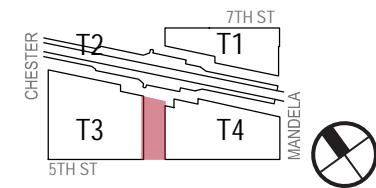
Evening Food Festivals



Neighborhood Food Destination



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**PUBLIC SPACE**  
**IMPROVEMENT:**  
**CENTER SQUARE**

SHEET:

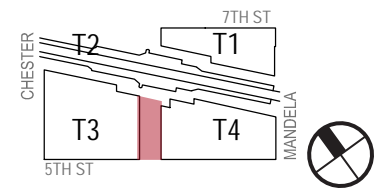
A-9.15



BIRDSEYE OF CENTER SQUARE

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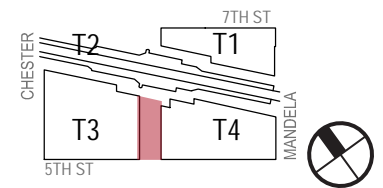
CENTER  
SQUARE

SHEET:  
A-9.16



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**VIEW OF CENTER SQUARE**



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**CENTER  
SQUARE**

SHEET: **A-9.17**



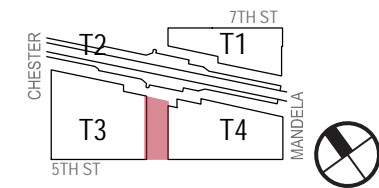


VIEW FROM 5TH STREET



VIEW OF CENTER SQUARE

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**CENTER**  
**SQUARE**

SHEET: **A-9.18**

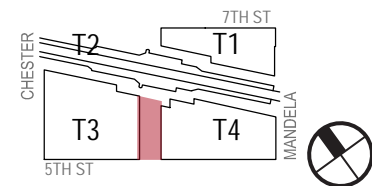


VIEW OF EXTERIOR STAIR



VIEW OF ART ALLEY FROM CENTER SQUARE

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SHEET: **CENTER SQUARE**  
**A-9.19**

# Crosswalks & Intersections

## 5th Street/Center Street and 5th Street/Chester Street Intersection:

High-visibility crosswalks will be installed along with directional, ADA compliant curb ramps.

## Chester Street/7th Street Intersection:

To facilitate safe pedestrian, bicycle and vehicle movements, the curbs and crosswalks have been engineered to separate pedestrian and bicycle movements with clear non-conflicting pathways.

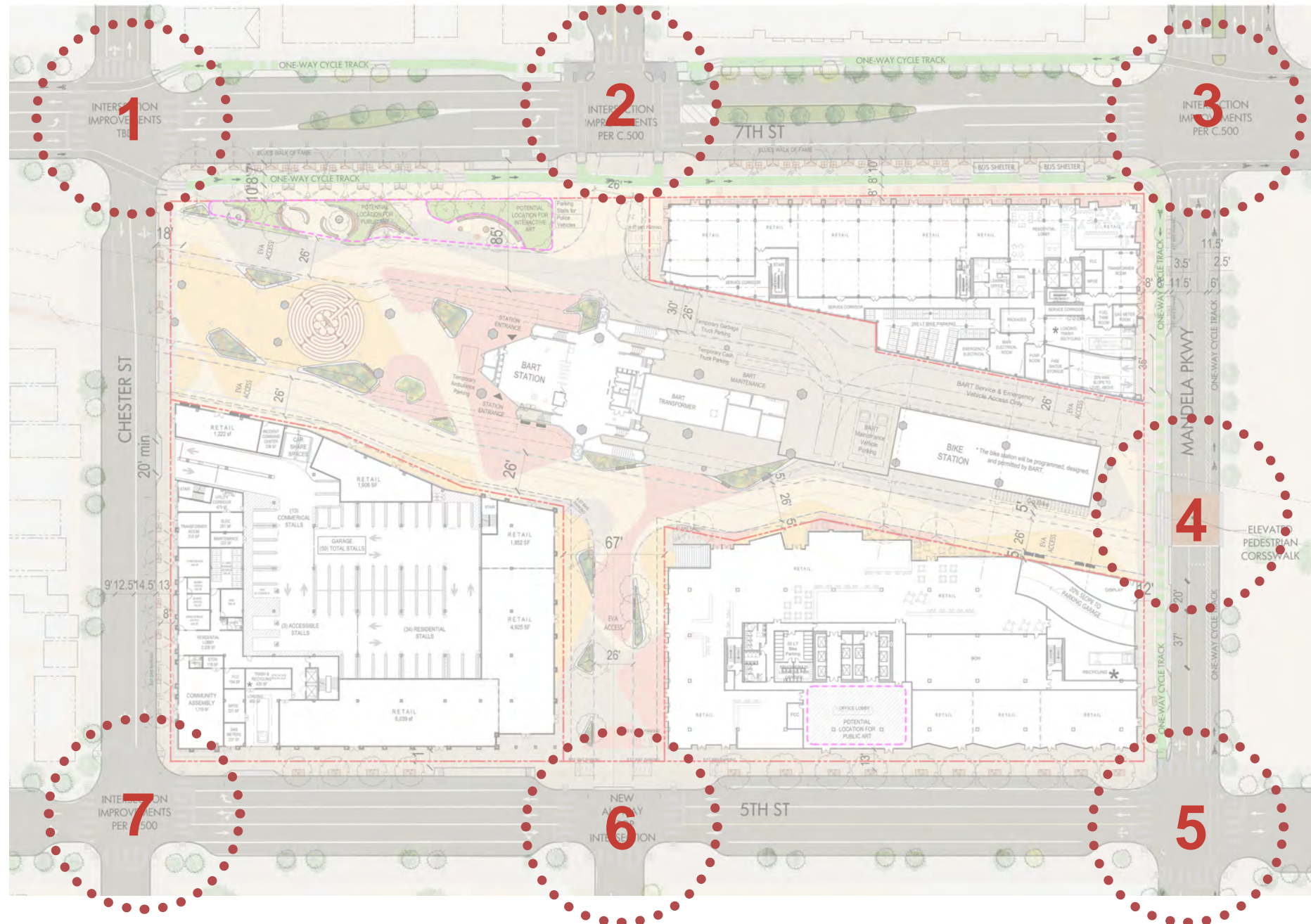
Bulb-outs have been provided where possible, including the NW, NE, and SW corners. Bus turning requirements prevent this on the SE corner.

## Mandela Parkway/7th Street Intersection:

This is a significant bike and pedestrian access point. To facilitate safe pedestrian, bicycle and vehicle movements, the curbs and crosswalks have been engineered to clearly separate pedestrian and bicycle movements with clear non-conflicting pathways. A dedicated bicycle signal phase will be evaluated for further consideration.

## Mandela Parkway/5th Street Intersection:

The NE, SE and SW crosswalks at this intersection have been bulbed out to minimize pedestrian crossing distances and ADA-compliant, directional curb ramps will be installed. Crosswalks will be reconfigured to create more directional connectivity.



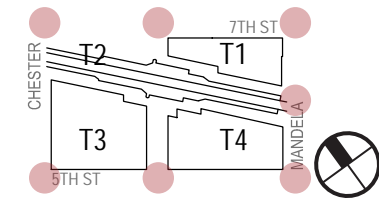
**Mid-Block Crossing on Mandela:** A high-visibility, mid-block pedestrian crossing will be added on Mandela Parkway between 7th and 5th Streets. The crossing design is raised and coordinated with the design of the two-way cycle track to facilitate safe pedestrian crossing of both the roadway and the cycle track.

**7th Street/Center Street Intersection:** The intersection crossing will be designed with a scramble pedestrian crossing to facilitate access. Bike and pedestrian access will be designed to prevent conflicts and promote ease of use. A protected intersection design will be provided with separated pedestrian crossing and bikeway crossing.



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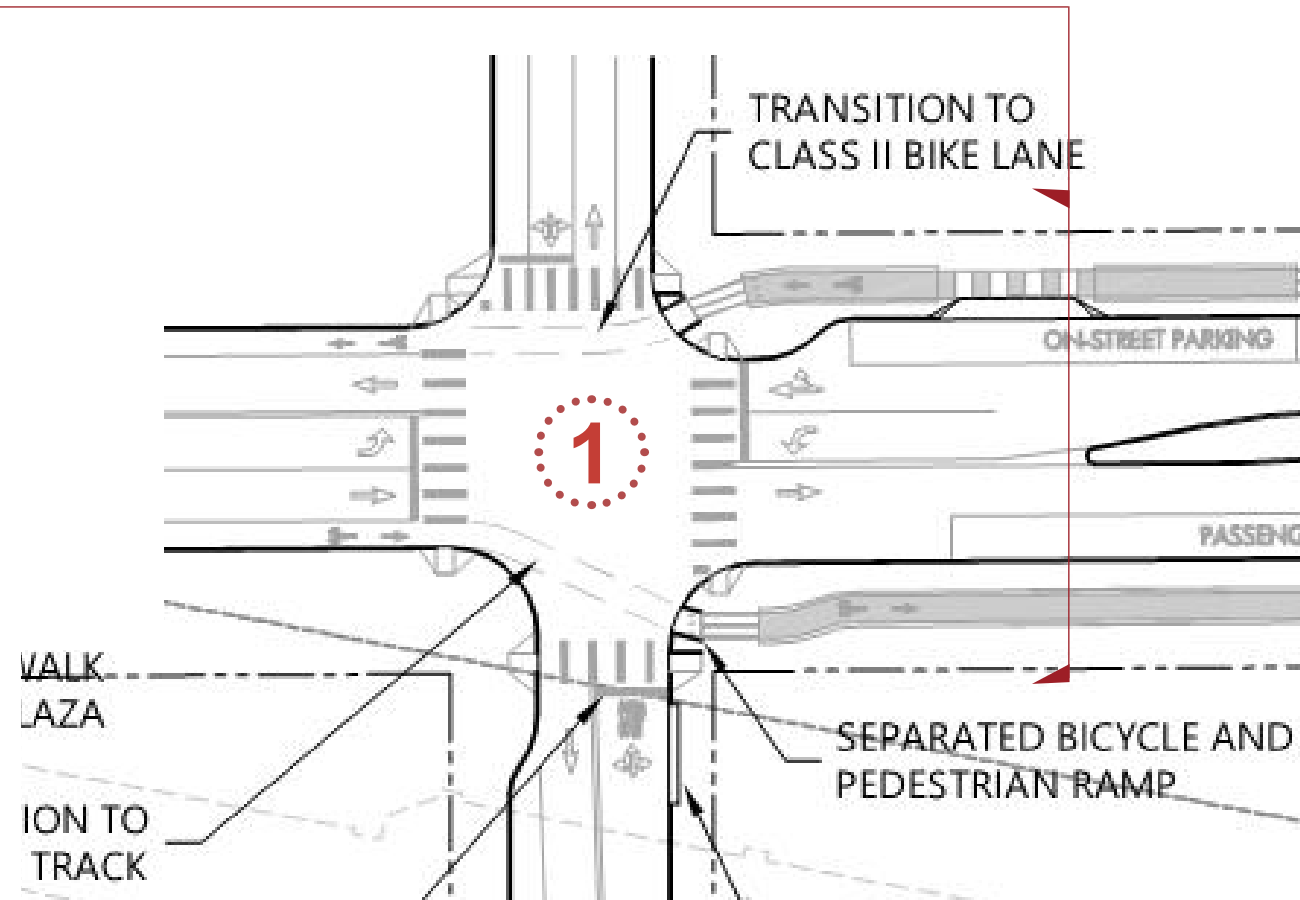
PUBLIC SPACE  
IMPROVEMENT:  
CROSSWALKS &  
INTERSECTIONS

SHEET:

A-10.04

## 7TH STREET

- 8' SIDEWALK
- 6' CYCLE TRACK
- +(2) 1' DET. BARRIERS
- 4' SIDEWALK
- 10' BUS LOADING
- 11.5' WESTBOUND
- 11' EASTBOUND
- 11.5' EASTBOUND
- 10' PASSENGER LOADING
- 8.5' SEPARATION
- 6' CYCLE TRACK
- +(2) 1' DET. BARRIERS
- 8' SIDEWALK
- PLAZA

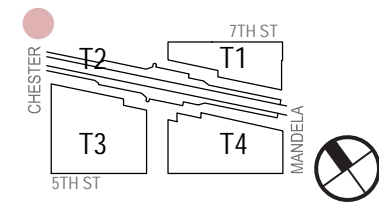


### RAMP NOTES:

1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.

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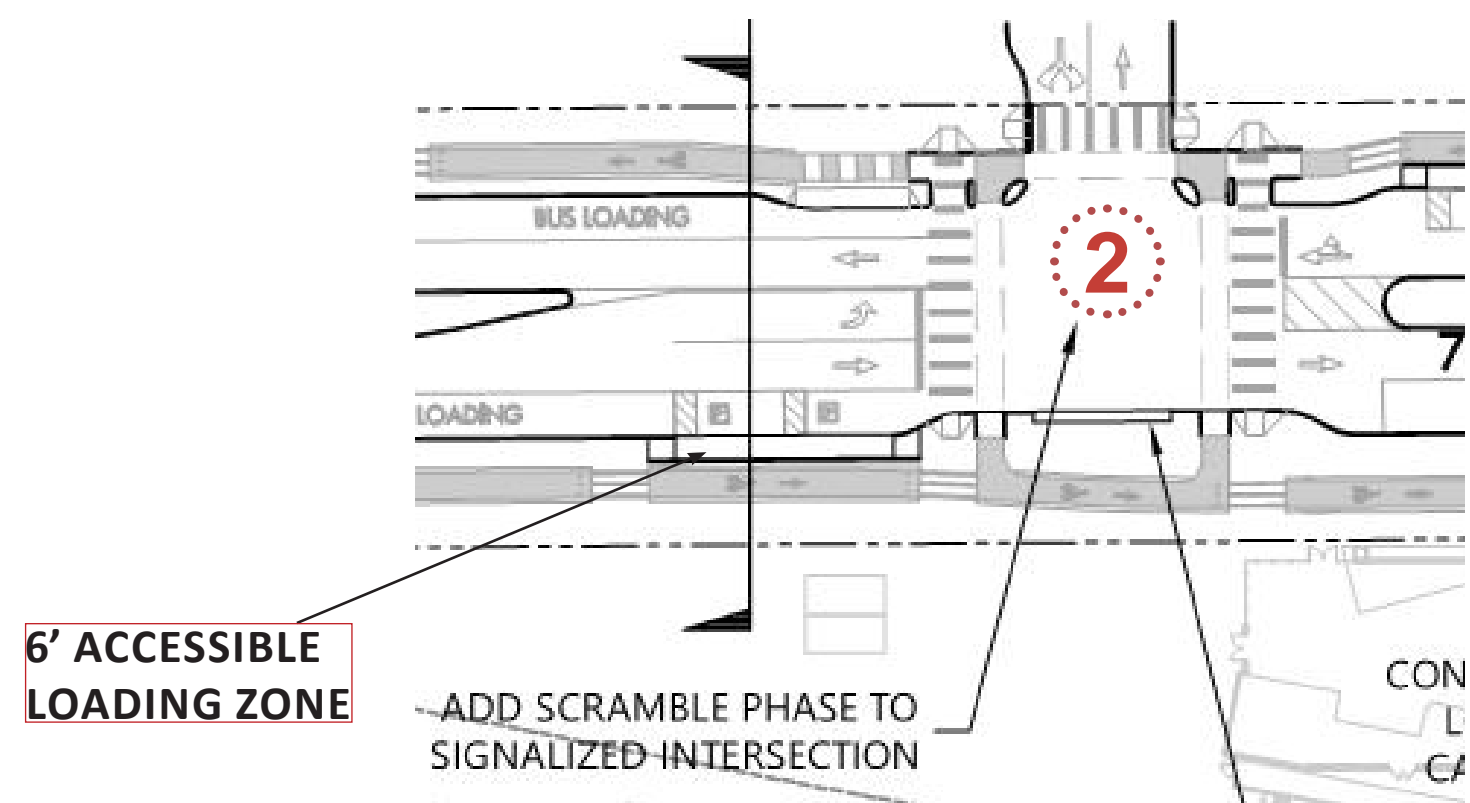
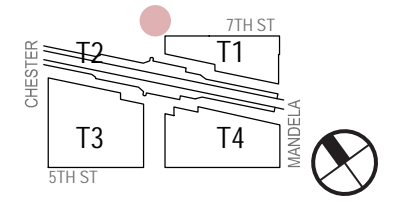
PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

## CROSSWALKS & INTERSECTIONS

SHEET:

A-10.04.01

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**6' ACCESSIBLE LOADING ZONE**

ADD SCRAMBLE PHASE TO SIGNALIZED INTERSECTION



**RAMP NOTES:**

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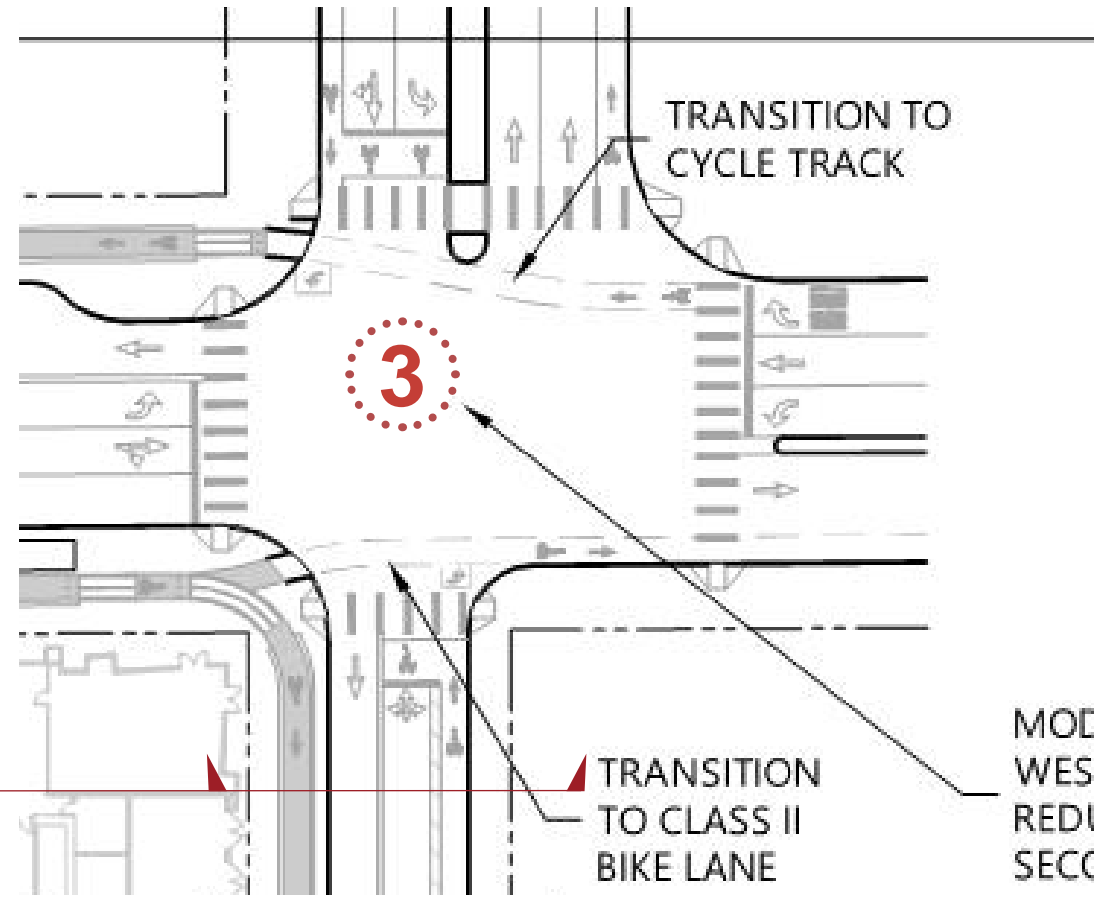
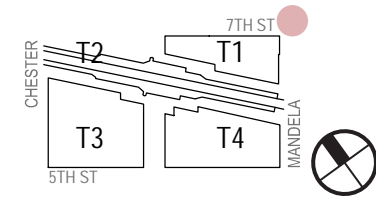
PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

**CROSSWALKS &  
 INTERSECTIONS**

SHEET:

A-10.04.02

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MODIFY SIGNAL TIMING FOR  
 WESTBOUND BUS-ONLY PHASE.  
 REDUCE CYCLE TIME TO 90  
 SECONDS.

**MANDELA PARKWAY**

- 8' MIN. TO BUILDING
- 6' CYCLE TRACK
- +(2) 1' DET. BARRIERS
- 3.5' SIDEWALK
- 11.5' SOUTHBOUND
- 11.5' NORTHBOUND
- 2.5' BUFFER ZONE
- 6' BIKE LANE
- 10' SIDEWALK LANE

**RAMP NOTES:**

1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.




  
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**CROSSWALKS & INTERSECTIONS**

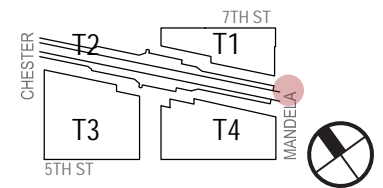
SHEET:

A-10.04.03



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**DETECTABLE WARNING  
DOMES BOTH SIDES**



**RAMP NOTES:**

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**CROSSWALKS &  
INTERSECTIONS**

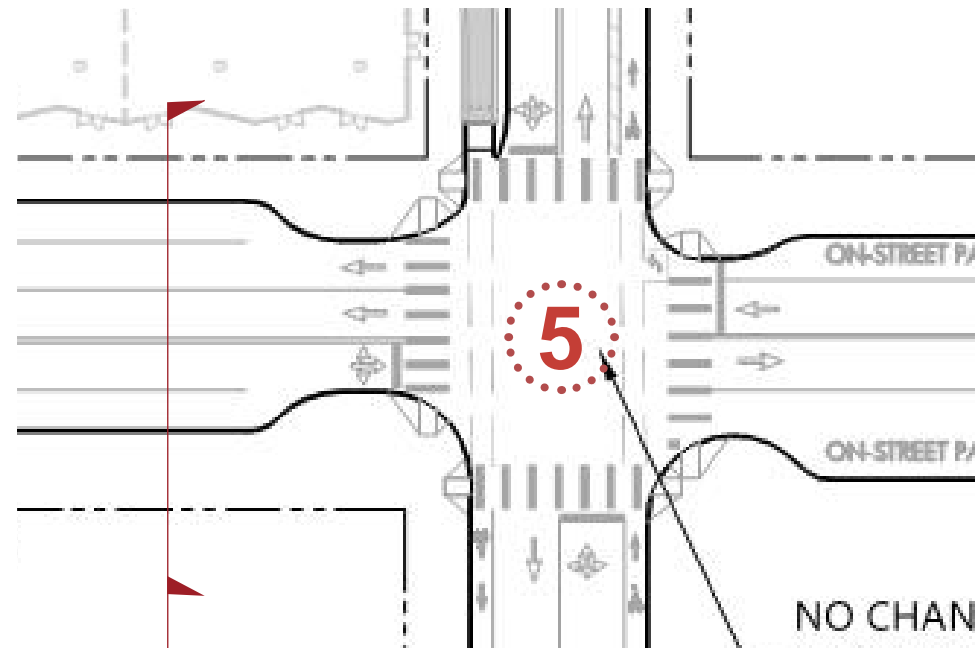
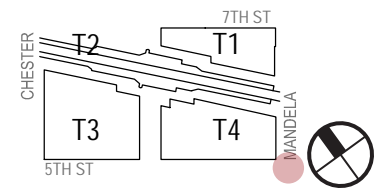
SHEET:

**A-10.04.04**



MANDELA STATION  
@WEST OAKLAND  
BART

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CA 94607



**5TH STREET**

- 10' SIDEWALK
- 9' BUS LOADING
- 11.5' WESTBOUND
- 11.5' WESTBOUND
- 11.5' EASTBOUND
- 9' ON-STREET PARKING
- 6' MIN SIDEWALK
- 12' TO PROPERTY LINE



**RAMP NOTES:**

1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.



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PROJ. # 168-153 WO BART

DATE: July 24, 2020

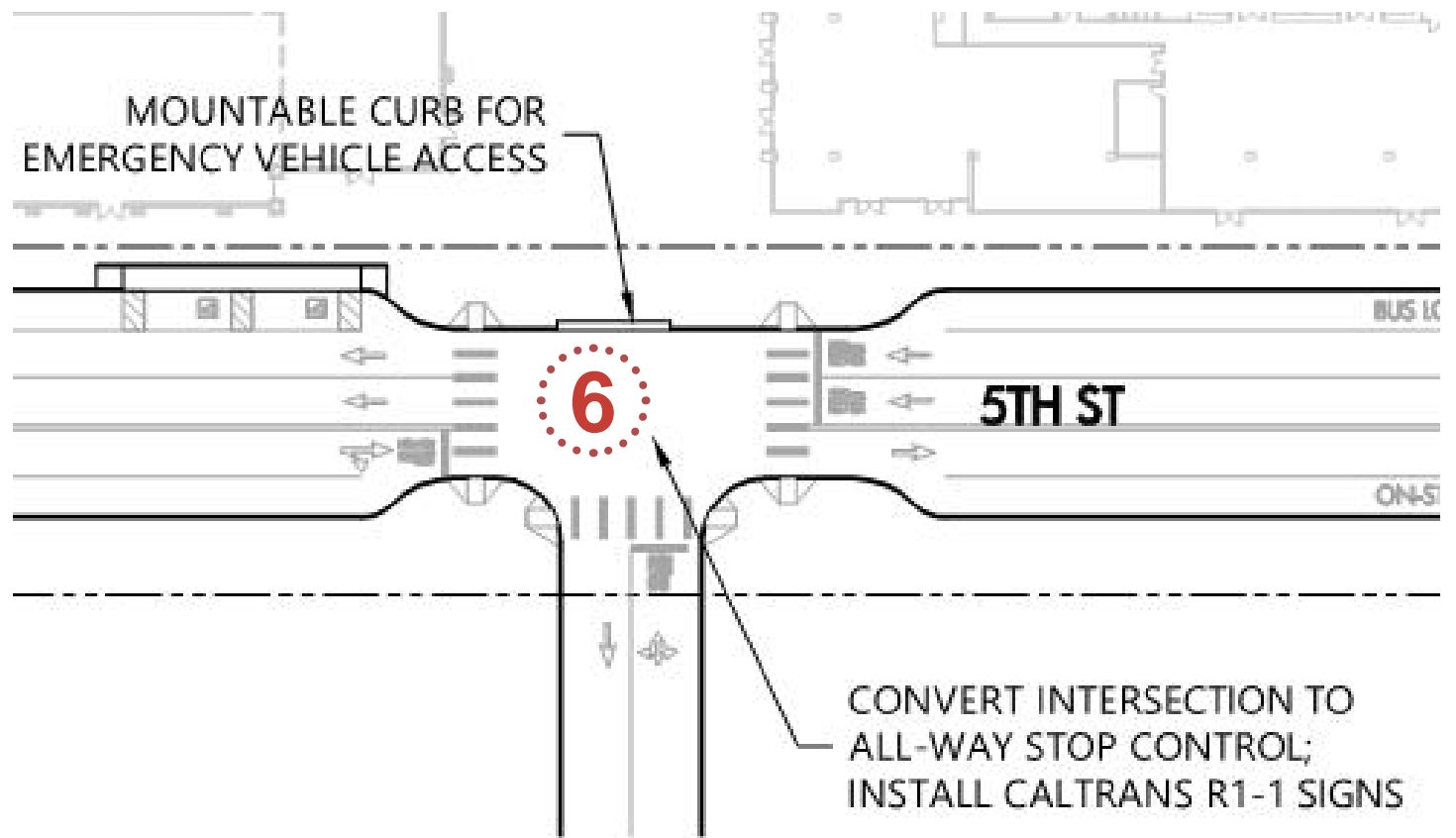
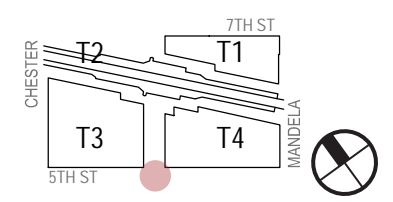
CROSSWALKS &  
INTERSECTIONS

SHEET:

A-10.04.05



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PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

**CROSSWALKS &  
 INTERSECTIONS**

SHEET:

**A-10.04.06**

**RAMP NOTES:**

1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.



**CHESTER STREET**

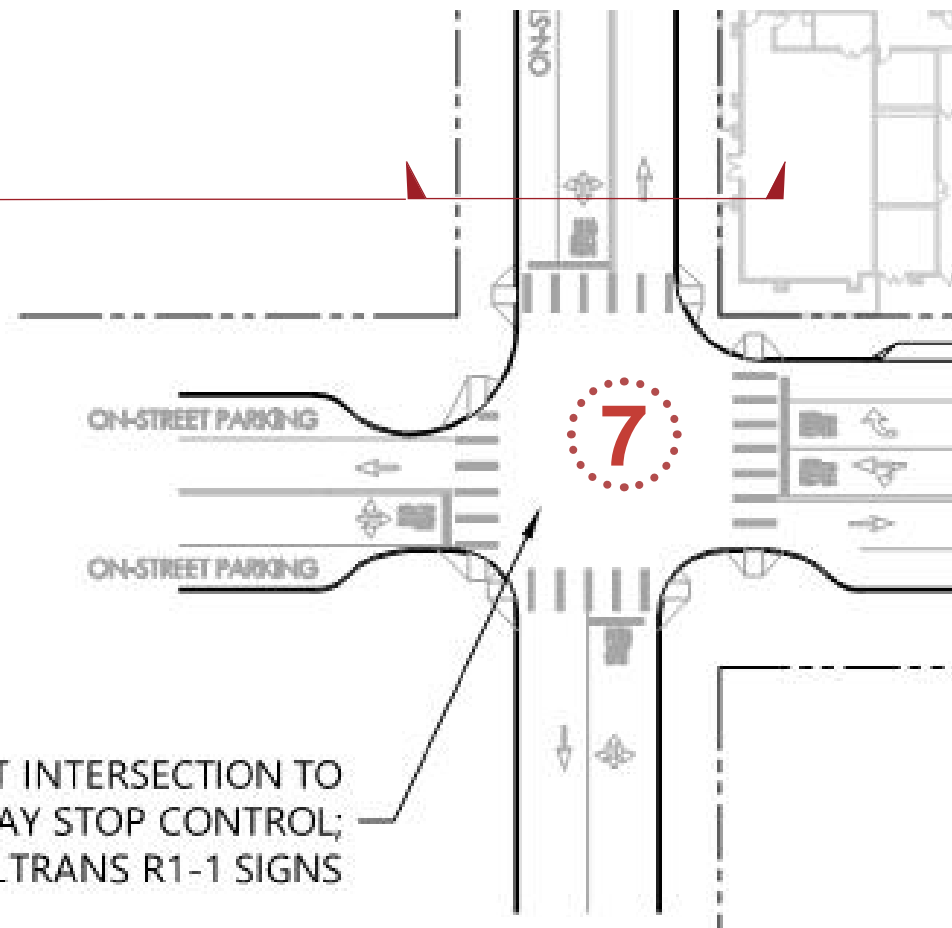
14' SIDEWALK

9' ON-STREET PARKING

12.5' SOUTHBOUND

14.5' NORTHBOUND

10' SIDEWALK

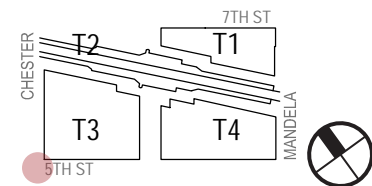


CONVERT INTERSECTION TO ALL-WAY STOP CONTROL; INSTALL CALTRANS R1-1 SIGNS



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DATE: July 24, 2020

**CROSSWALKS &  
INTERSECTIONS**

SHEET:

A-10.04.07



**RAMP NOTES:**

1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.

# Sidewalks

## 7th Street (between Mandela Parkway and Chester Street):

The sidewalk widths are range from 10'-15', and are designed to allow adequate clearances in order to accommodate the needs of pedestrians, bus passengers, and curbside passenger loading. The sidewalk is clear space for passanger loading at the curb, and a protected class 4 cycle track with clearly marked pedestrian crossing indicators.

Street lighting and street trees are located to reinforce the visual experience of the sidewalk space.

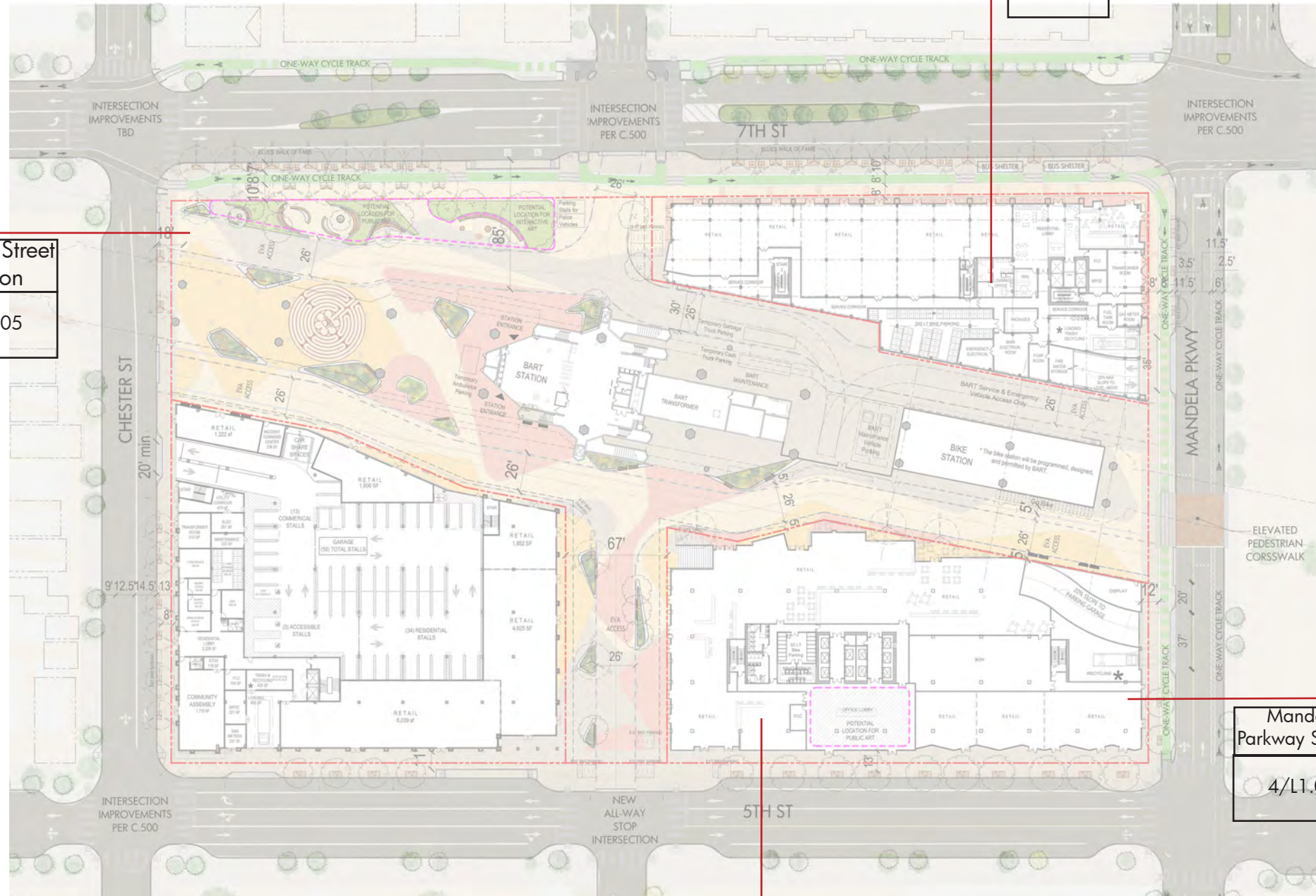
## 5th Street (between Mandela Parkway and Center Street):

The sidewalk width will be 10-15 feet min. wide in order to accommodate the needs of pedestrians, bus passengers, and curbside passenger loading.

## 5th Street (between Center and Chester Street):

Between 5th and 7th Streets, sidewalks adjacent to the project site are a minimum of 10 feet wide, and provide 8' clear path. The sidewalk is buffered from adjacent loading and street traffic by street pedestrian-scale lighting and street trees.

Chester Street Section  
3/L1.05



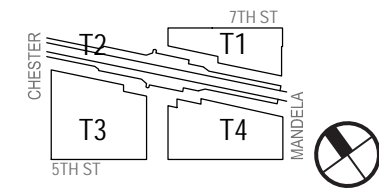
7th Street Section  
2/L1.05

Mandela Parkway Section  
4/L1.05

5th Street Section  
5/L1.05



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**PUBLIC SPACE  
IMPROVEMENT:  
SIDEWALKS**

SHEET: **A-10.05**

**Parking and Truck Loading Entrances:** for the project are designed to provide clear sight lines, and include ADA features such as tactile warning strips, and audible warnings for when vehicles cross the pedestrian path of travel.

# Bikeways & Bike Parkings

## Mandela Cycle Tracks:

To facilitate bicycle access from the bicycle lanes on Mandela Parkway south of 7th Street and north of 5th Street, two raised, one-way cycle tracks are located along the west and east sides of Mandela Parkway between 7th and 5th Streets.

These one-way cycle tracks are 6 feet wide, with an 8' and 10' separation from face of curb to the edge of the cycle track. In addition to the cycle track, sidewalks with a minimum width of 8' clear are provided.

## 7th St Cycle Tracks Eastbound:

To facilitate bicycle access in conjunction with passenger loading from vehicles and buses along eastbound 7th Street between Chester Street and Mandela Parkway, a raised, one-way cycle track is provided.

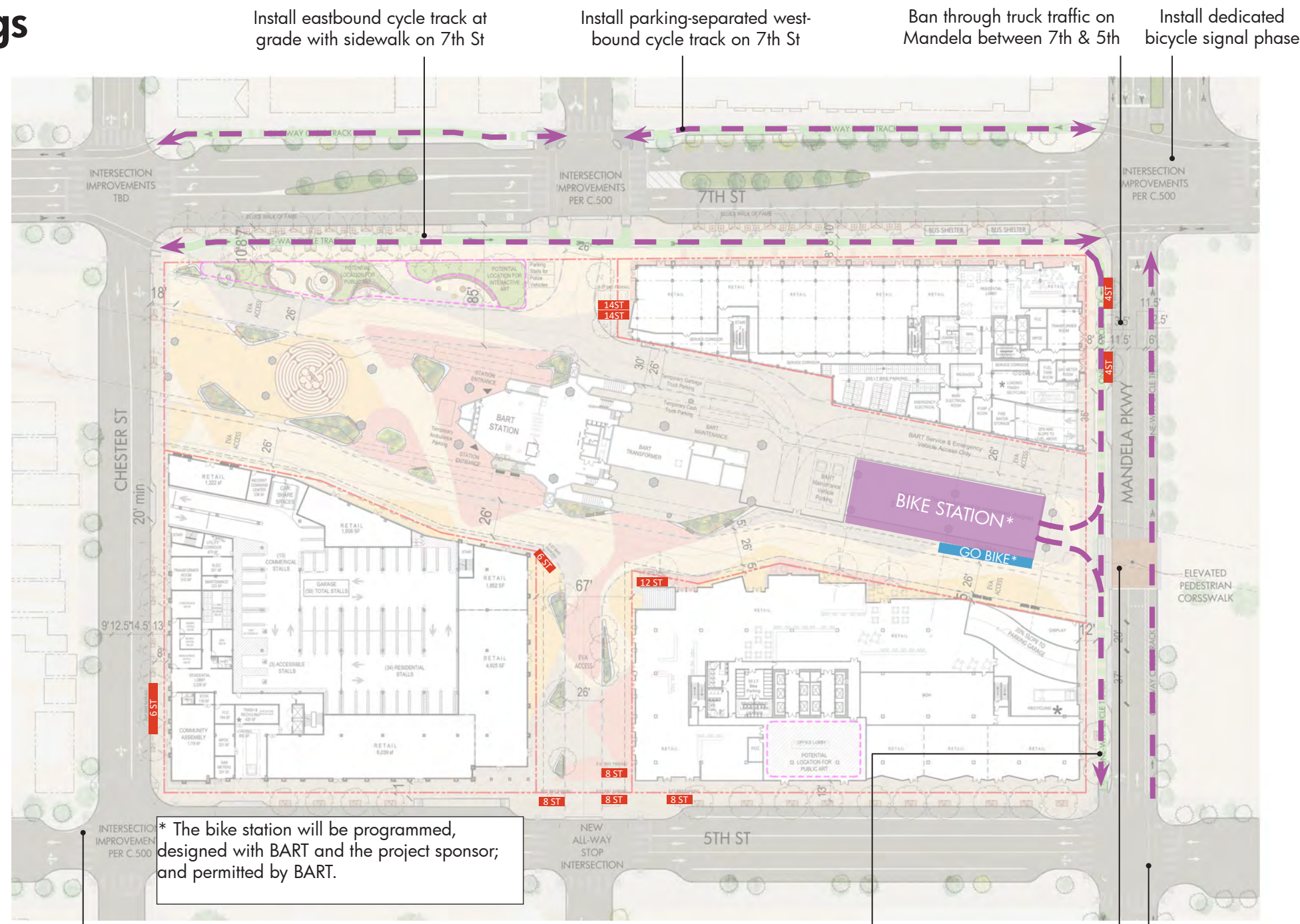
The cycle track travel surface is 6 feet, with an 8'-10' foot setback from sidewalk to the face of curb.

## 7th St Cycle Tracks Westbound:

On westbound 7th Street between Mandela Parkway and Chester Street, a raised, one-way cycle track is located with a width of 6 feet.

The cycle track is provides an 8' setback sidewalk from the face of curb to the edge of the cycle track to provide adequate space for pedestrian loading.

Ford Go-bike: has been co-relocated with the Bike Station, to provide ease of access, prevent plaza use conflicts and to reinforce the bike station as a central hub for bike users.



Install high-visibility crosswalks, directional curb ramps & pedestrian refuges on 5th St

Install 2 one-way cycle tracks on Mandela between 7th & 5th on grade with sidewalk

Install high-visibility mid-block crosswalk

Install dedicated bicycle signal phase

## Bike Station:

The project will include an enhanced bicycle station located on the east side of the station structure. The bike station will be programmed, designed with BART and the project sponsor; and permitted by BART. The city will be responsible for design review.

Additional bicycle parking for BART and commercial patrons is provided throughout the site in the form of bicycle racks. Bicycle parking for the TOD's commercial and residential tenants will also be provided in accordance with City of Oakland requirements.

## Bike Parking Account:

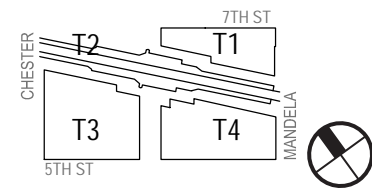
**ON SITE SHORT TERM PARKING: 92**

**BIKE STATION:** The bike station will be programmed, designed with BART and the project sponsor; and permitted by BART. The city will be responsible for design review.

**GO BIKE PARKING NUMBER: 21**



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DATE: July 24, 2020

**PUBLIC SPACE  
IMPROVEMENT:  
BIKES**

SHEET:

**A-10.06**

# Passenger Loading

## 7th Street Passenger Loading:

Will be located on the south side of 7th Street west of Center Street. A portion (50 linear feet) of this loading area closest to the intersection with Center Street is designated as a loading space for passengers with disabilities. This area provides the most direct access to the station entrance.

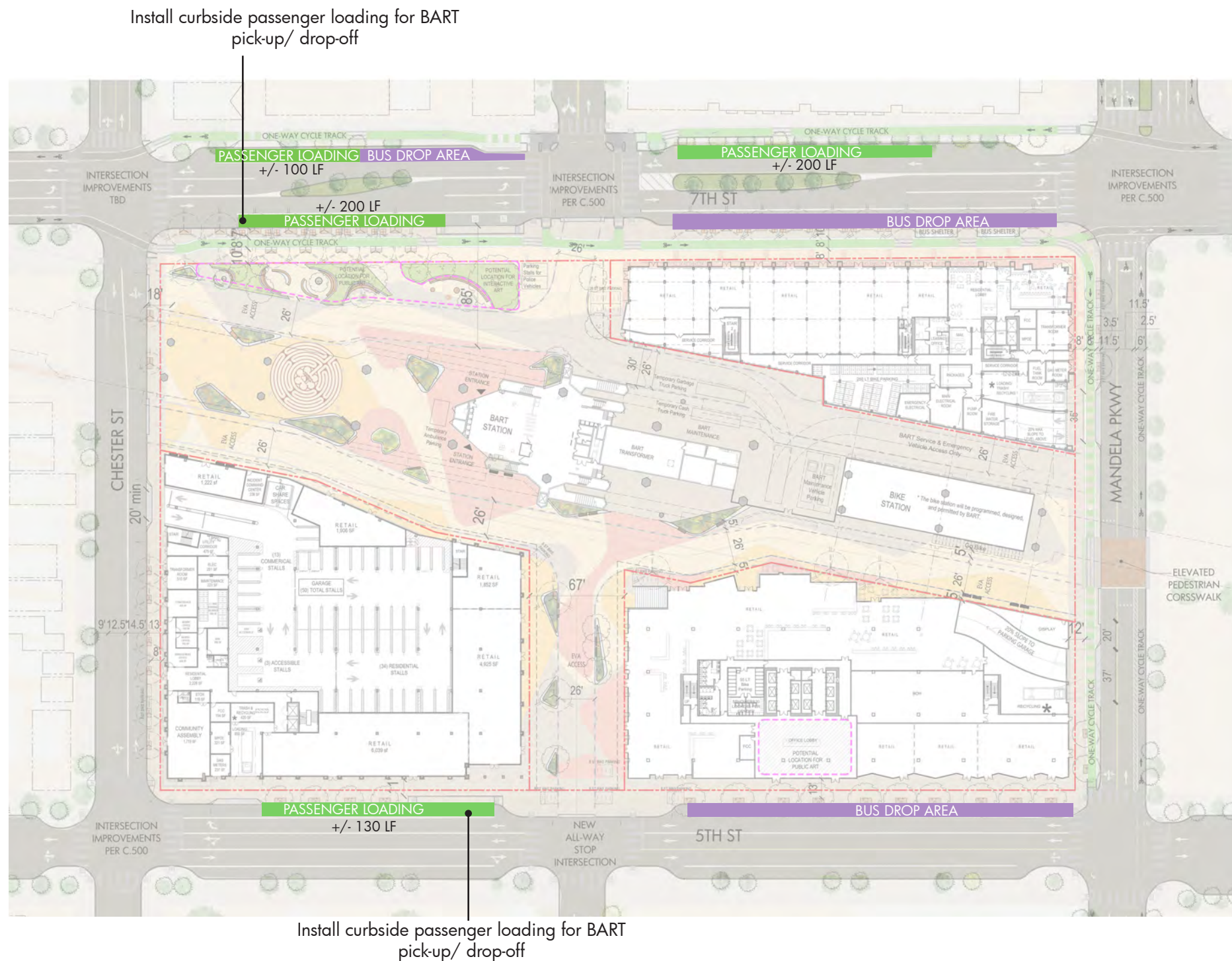
Another 200 feet of linear curb is on north side of 7th Street between Center Street and Mandela Parkway for passenger loading and unloading and 100 feet of linear curb near the Chester Street intersection.

## 5th Street Passenger Loading:

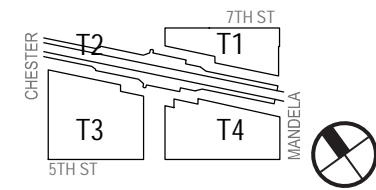
130 feet of linear curb is proposed along westbound 5th Street on the west side of the intersection with Center Street.

## Wayfinding:

Signage directing vehicles to loading zones will be provided at key decision points like the Mandela Parkway and 7th Street intersection and the 5th Street and Kirkham or Union Street intersections. Loading zone locations will be incorporated into smartphone mapping and TNC apps to facilitate safe and efficient circulation and access.



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DATE: July 24, 2020

**PUBLIC SPACE  
IMPROVEMENT:  
PASSENGER  
LOADING**

SHEET:

A-10.07

# Bus Stops

Install bus stop for intercity coaches (Bolt, Megabus) - optional location east of Mandela

Install bus stop for line 29 and Emery-Go-Round

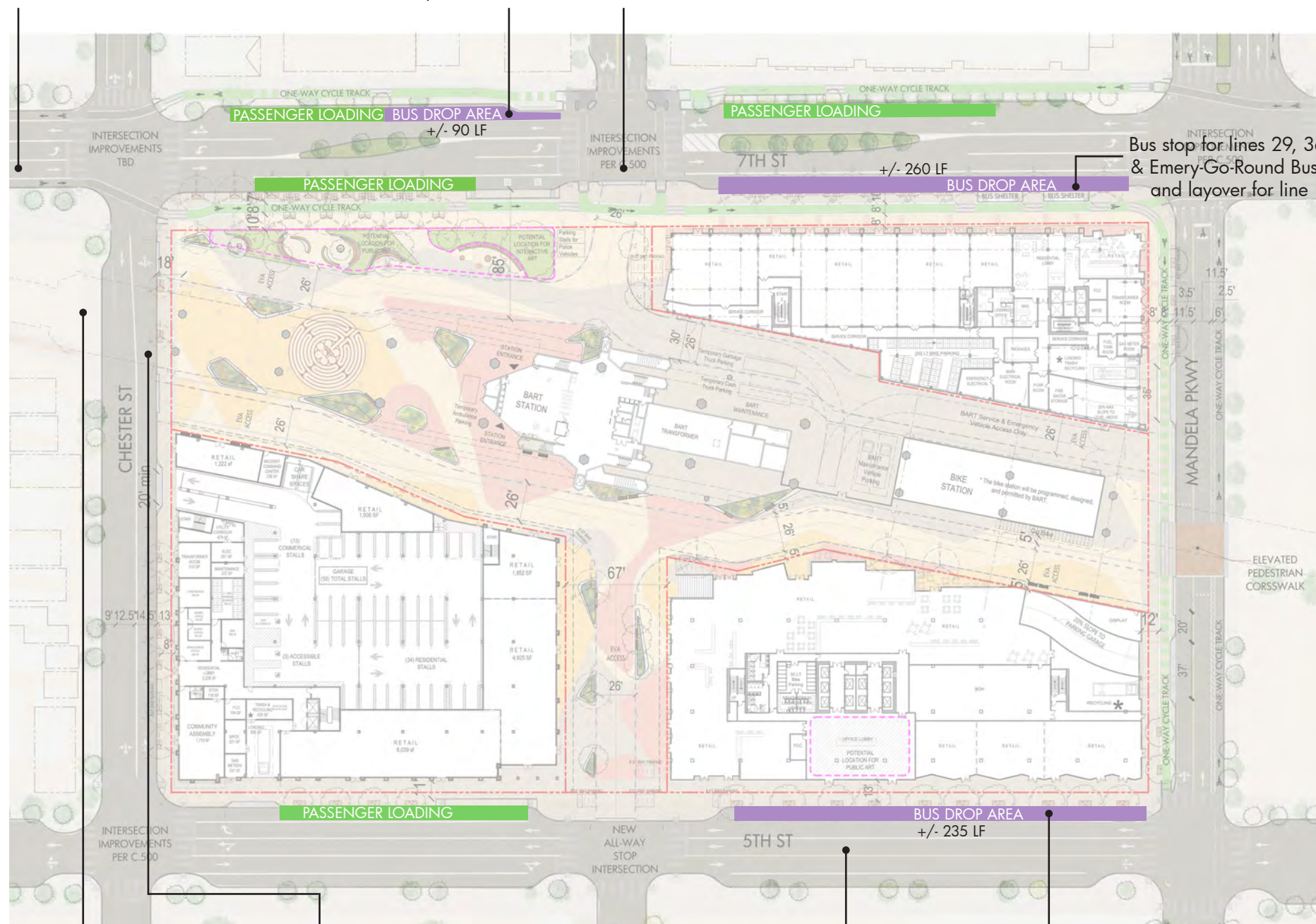
Start sidewalk taper immediately after crosswalk to better accommodate bus access

## 7th Street Eastbound:

The existing bus stop on eastbound 7th Street will be retained and extended to the intersection with Center Street for an approximate total length of 260 linear feet. This stop would serve AC Transit Lines 29, 36, and 62 and will serve as both a stop and layover space for AC Transit Line 14. If Emery-Go-Round service is extended to the West Oakland Station, this stop could serve Emery-Go-Round vehicles as well.

**7th Street Westbound:** A new bus stop will be installed on westbound 7th Street to serve AC Transit Line 29 and Emery-Go-Round, if the service is extended to the West Oakland Station. 90 feet of linear curb is provided at this location to accommodate transit vehicles, as well as a concrete bus pad in the roadway.

**5th Street Westbound:** The bus stop and layover for lines 36 and 62 will be relocated to westbound 5th Street on the far side of the intersection with Mandela Parkway. 235 feet of curb length is provided to accommodate the bus stop and layovers, and a concrete bus pad will be installed in the roadway.



Potential layover space for lines 36 & 62, requires further evaluation

Redesign Chester St cross-section to facilitate buses turning right

Install median barrier to prevent jaywalking to/from bus stop

Bus stop and layover for lines 62 & 36

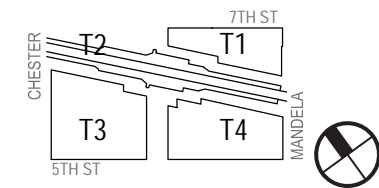
**Intercity & Private Bus Stops:** A bus stop for intercity coaches (e.g., Megabus and Bolt) could be installed on 7th Street between Henry and Chester Streets. Private employer shuttle could also utilize this stop. The existing BART surface parking immediately adjacent to this curb could be utilized for bus and shuttle transit passenger pick-up and drop-off (if not utilized as a layover space for AC Transit buses). A option to locate this bus stop east of Mandela Parkway is under consideration.

## Passenger Amenities:

All bus stops are provide with a high level of passenger amenities, including shelters with seating, maps and other information, and real-time bus arrival information; trash receptacles; and lighting.



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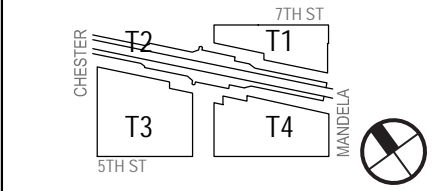
PROJ. # 168-153 WO BART  
DATE: July 24, 2020

**PUBLIC SPACE  
IMPROVEMENT:  
BUS STOPS**

SHEET:

A-10.08

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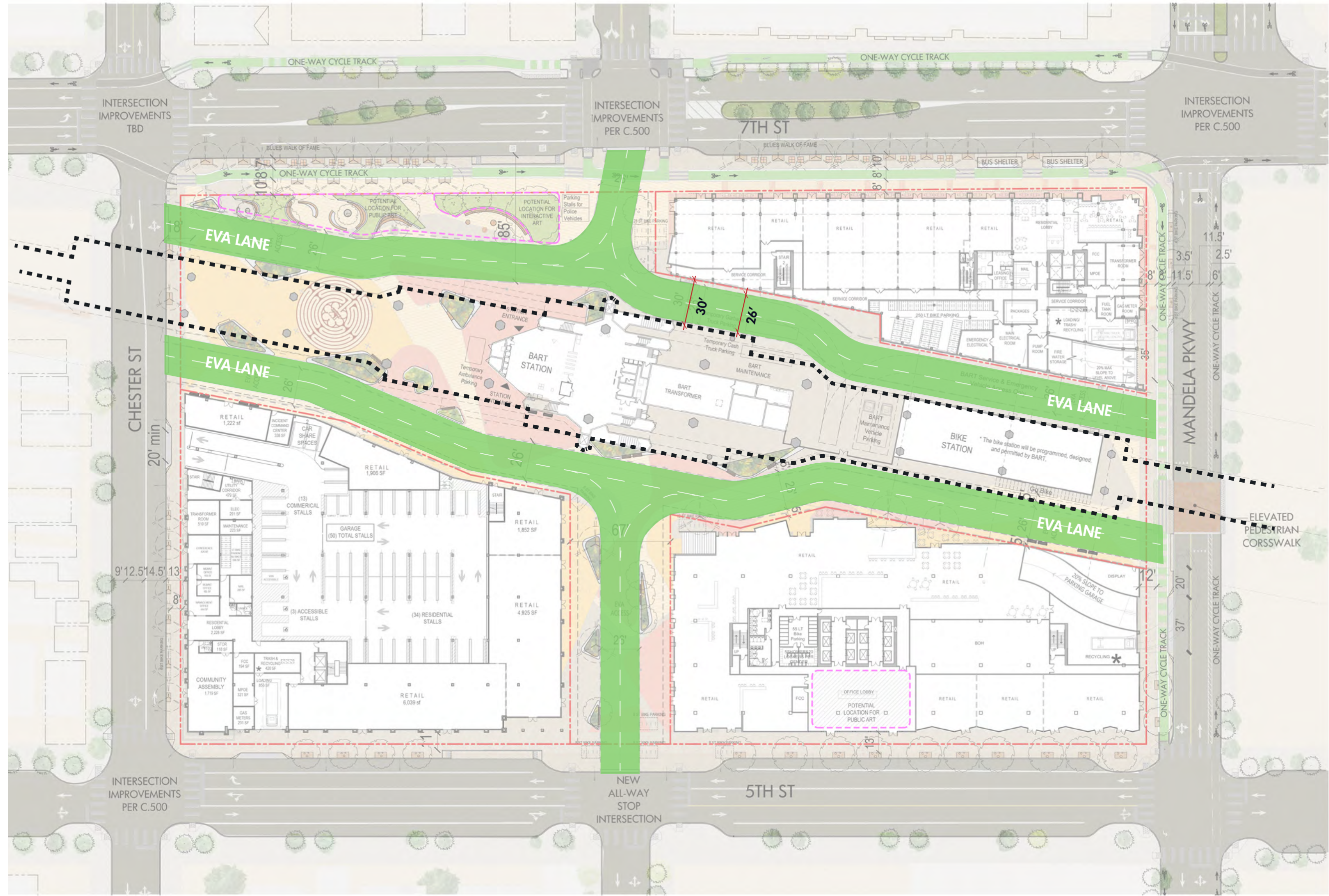

  

  
 PGAdesign  
 LANDSCAPE ARCHITECTS

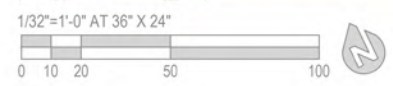
PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

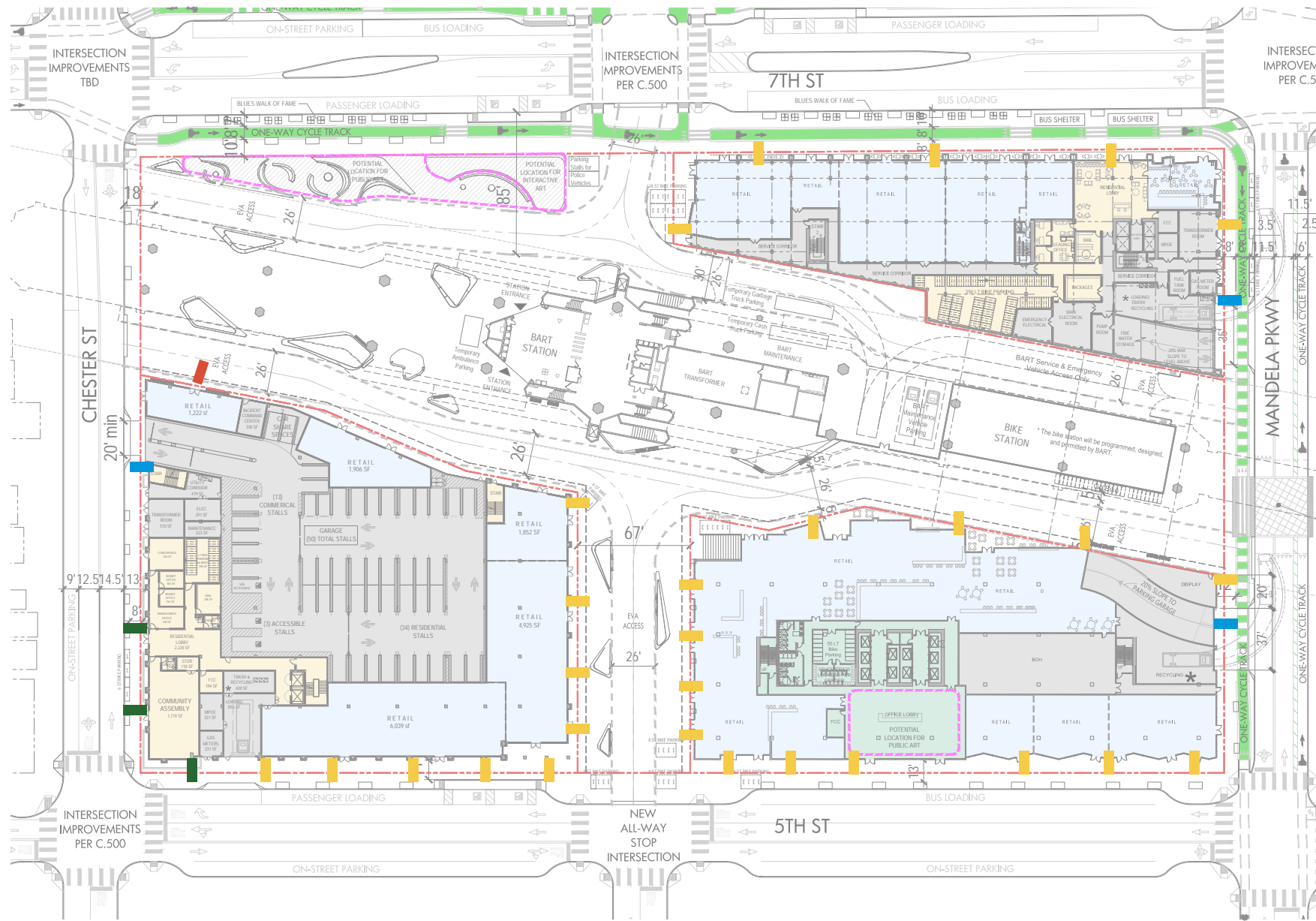
**EMERGENCY**  
**VEHICLE**  
**ACCESS**

SHEET:



The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.





- COMMERCIAL BLADE SIGN
- GARAGE ENTRANCE SIGN
- ART DISTRICT SIGN
- COMMUNITY BLADE SIGN



ART DISTRICT SIGN

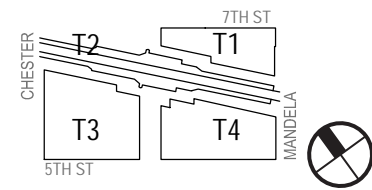


COMMERCIAL BLADE SIGN



COMMERCIAL BLADE SIGN

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**SIGN PLAN**

SHEET:



# T1 RESIDENTIAL TOWER

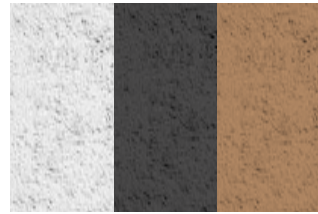
## UPPER FLOORS

### TOP FLOOR



**CHANNEL  
METAL  
MECHANICAL  
SCREEN**

### ALL ELEVATION



**ARCHITECTURAL  
PRECAST PANELS**

### EAST ELEVATION

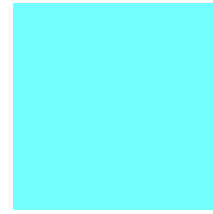


**BEIGE  
PLASTER**

### WINDOW SYSTEM

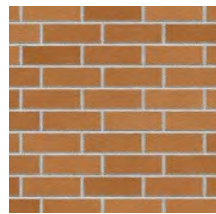


**ALUMINUM  
WINDOW  
MULLION  
SYSTEM**

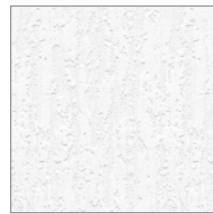


**HIGH PERFORMANCE  
INSULATED GLAZING  
UNIT - LIGHT GREEN**

### PODIUM



**LIGHT GAUGE  
MTL WALLS  
w/ THIN  
BRICK FACE**

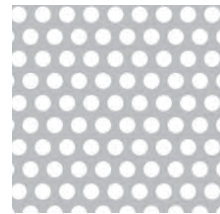


**LIGHT GAUGE MTL /  
MASONRY WALLS  
w/ WHITE 3-COAT  
CEMENT PLASTER**

### NORTH, WEST, SOUTH ELEVATION PODIUM



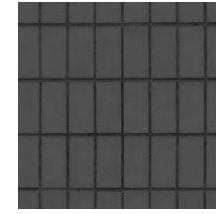
**DECORATIVE METAL PANEL**



**PERFORATED  
METAL PANEL**



**GREY  
PAINTED  
STEEL PANEL  
FRAME**



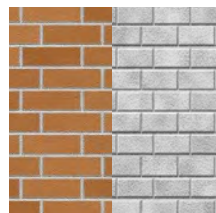
**DARK GRAY  
TILE**



**3"X3"WOVEN  
WIRE MESH IN  
MTL FRAMES**

## GROUND FLOOR

### EAST AND NORTH ELEVATION

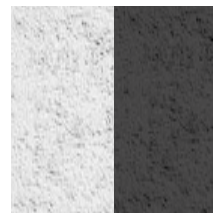


**LIGHT GAUGE  
MTL WALLS  
w/ THIN  
BRICK FACE**



**EXPOSED  
CONCRETE  
COLUMNS**

### SOUTH ELEVATION

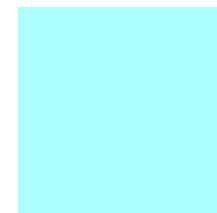


**MASONRY  
WALLS w/  
PLASTER**

### STOREFRONT SYSTEM



**WINDOW  
MULLION  
w/ KYNAR  
FINISH**

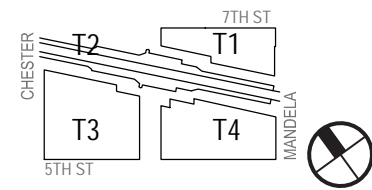


**CLEAR GLASS  
STOREFRONT  
SYSTEM**



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**T1 RESI. TOWER  
MATERIAL  
BOARD**

SHEET:



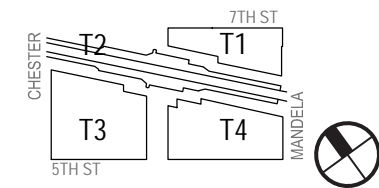
PODIUM  
LIGHT GAUGE MTL WALLS W/ THIN BRICK



UPPER FLOORS  
CLARK COMPOSITE ARCHITECTURAL PRECAST PANELS (C-CAPP)



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T1 RESI. TOWER  
MATERIAL  
BOARD

SHEET:

# T3 RESIDENTIAL MID-RISE

## UPPER FLOORS

### SOUTH AND WEST ELEVATION



**MULTIPLE COLOR, 3-COAT CEMENT PLASTER SYSTEM**

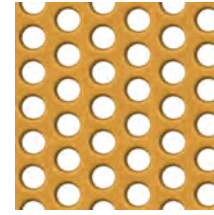


**DECORATIVE PANEL**

### RAILING SYSTEM



**PAINTED STEEL RAILING**



**PERFORATED METAL PANEL**

### WINDOW SYSTEM



**ALUMINUM WINDOW MULLION SYSTEM**



**HIGH PERFORMANCE INSULATED GLAZING UNIT - LIGHT GREEN**

### EAST AND NORTH ELEVATION



**METAL PANEL WALL SYSTEM**



**MULTIPLE COLOR, 3-COAT CEMENT PLASTER SYSTEM**



**DECORATIVE PANEL**

### EAST AND SOUTH ELEVATION



**FIBER CEMENT WITH WOOD TEXTURE**

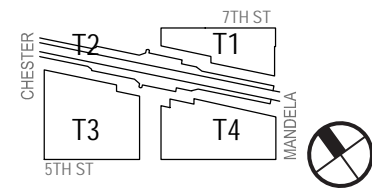
### TOP FLOOR



**2 COLOR METAL PANEL WALL SYSTEM**



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## GROUND FLOOR

### SOUTH AND WEST ELEVATION



**BRICK PILASTERS**



**PRECAST CONC BASE**

### EAST, SOUTH, AND WEST ELEVATION

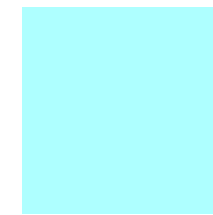


**FIBER CEMENT W/ WOOD TEXTURE**

### STOREFRONT SYSTEM



**WINDOW MULLION W/ KYNAR FINISH**



**CLEAR GLASS STOREFRONT SYSTEM**

### VINE TRELLIS WEST ELEVATION



PROJ. # 168-153 WO BART  
DATE: July 24, 2020

**T3 RESI. MID-RISE  
MATERIAL  
BOARD**

SHEET:

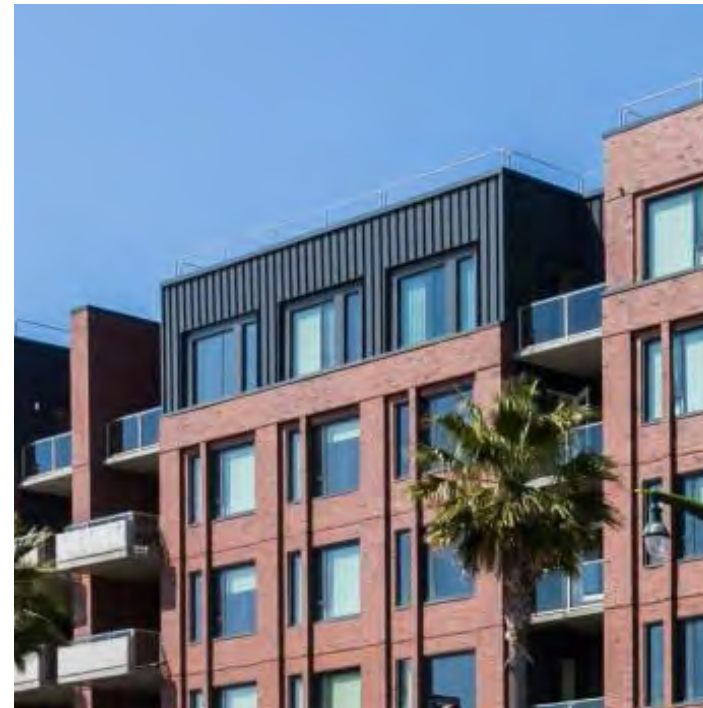
# T3 RESIDENTIAL MID-RISE



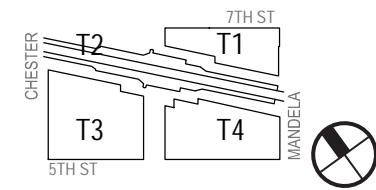
GLASS WALL + HIGH TRANSPARENCY WINDOW SYSTEM



WALL SYSTEM  
FIBER CEMENT LAP SIDING + CORRUGATED METAL PANEL



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JRDV Architects, Inc  
The Cathedral Building  
Broadway and Telegraph  
PO Box 70126  
Oakland, CA 94612 USA  
510 295 4392 T  
www.jrdv.com



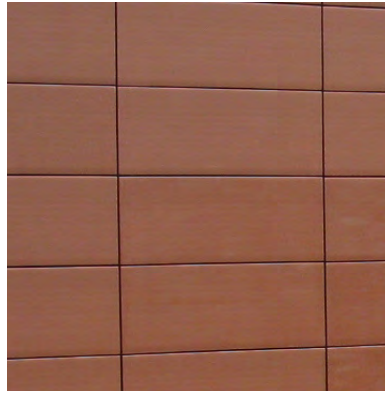
PROJ. # 168-153 WO BART  
DATE: July 24, 2020

T3 RESI. MID-RISE  
MATERIAL  
BOARD

SHEET:

A-10.11

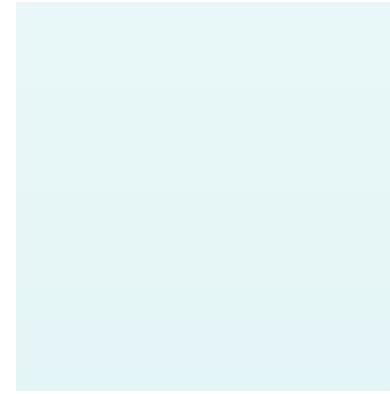
# T4 OFFICE BUILDING



PODIUM  
CONC. PRE-CAST W/  
INTEGRATED COLOR &  
TEXTURE



PODIUM  
CONC. PRE-CAST W/  
INTEGRATED COLOR &  
TEXTURE

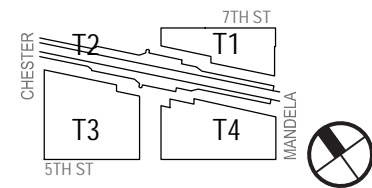


CURTAIN WALL  
HIGH SOLAR PERFORMANCE  
WINDOW SYSTEM



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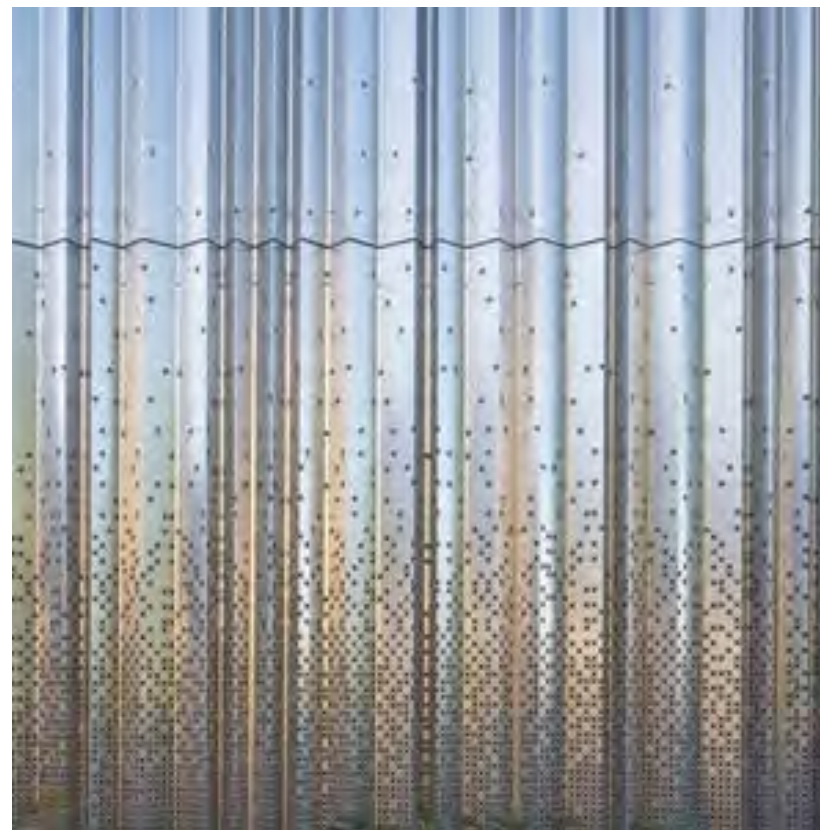
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## T4 OFFICE MATERIAL BOARD

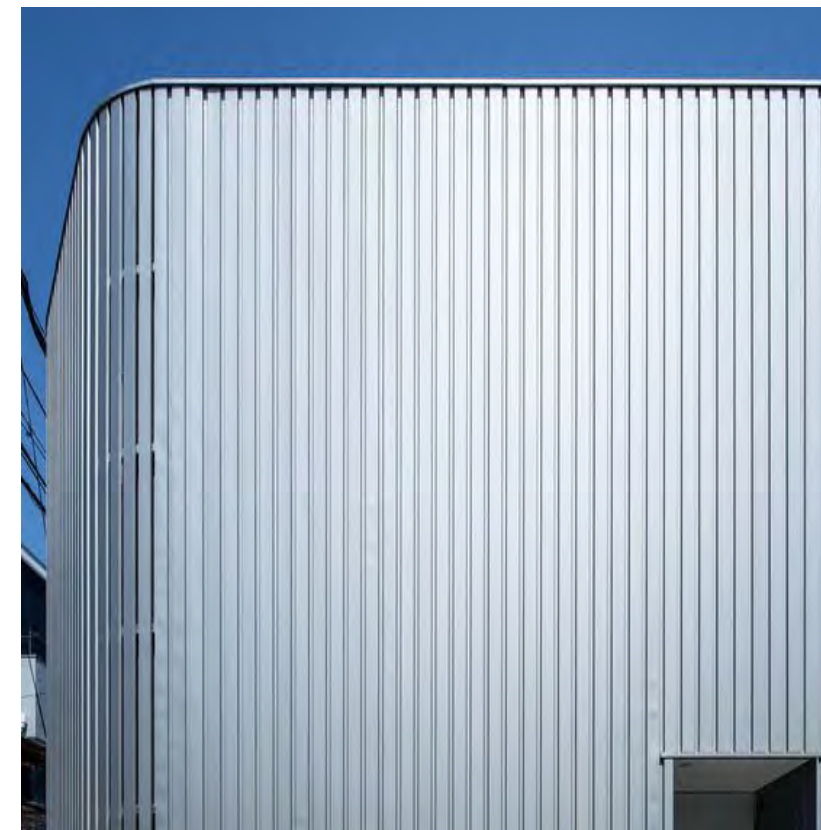
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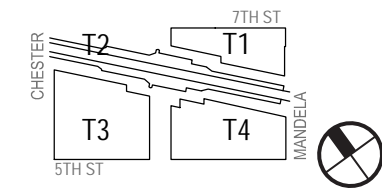
CURTAIN WALL  
HIGH SOLAR PERFORMANCE WINDOW SYSTEM



ROOF MECHANICAL SCREEN  
METAL CLADDING WITH STANDING SEAM



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BART  
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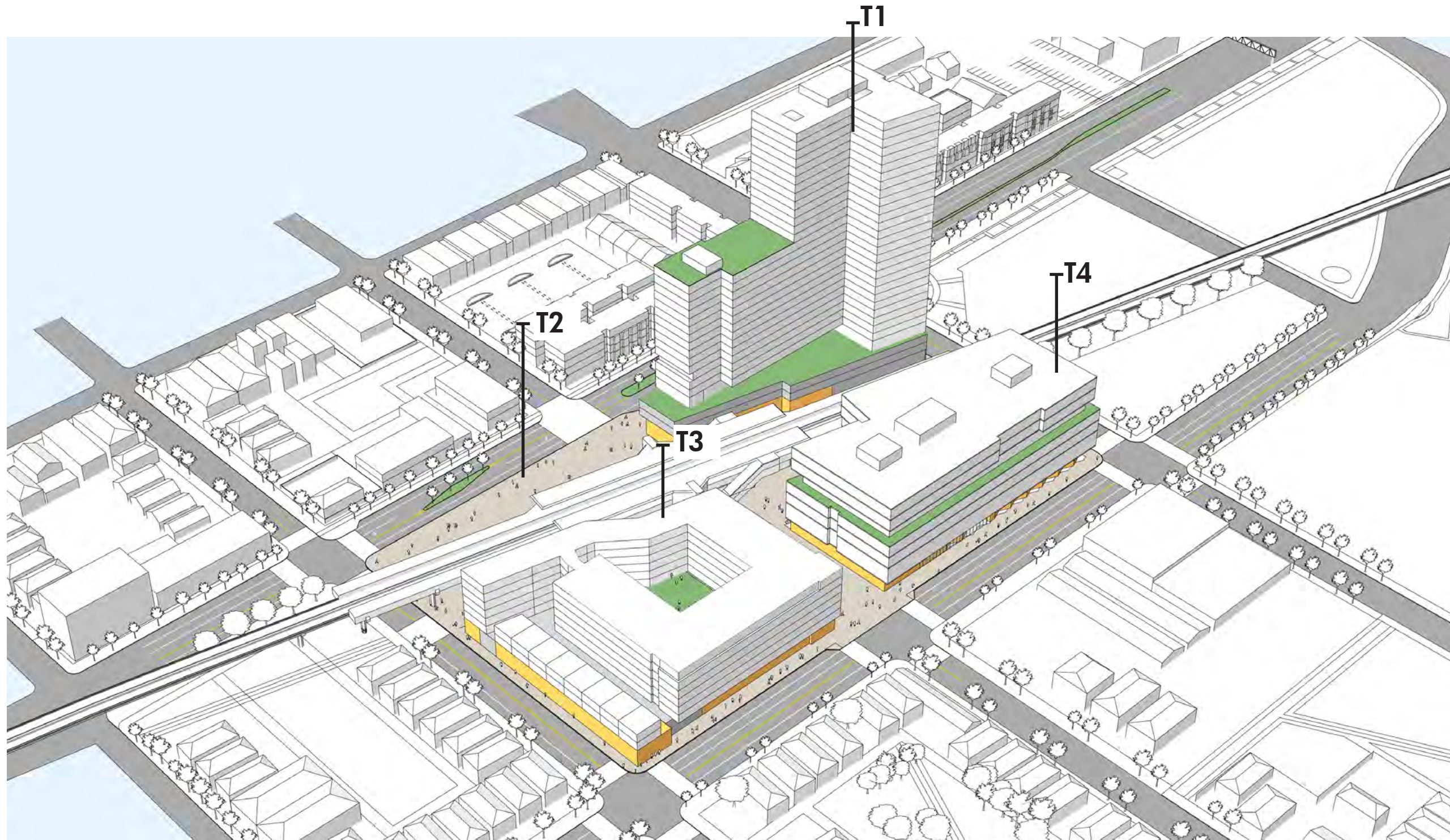
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DATE: July 24, 2020

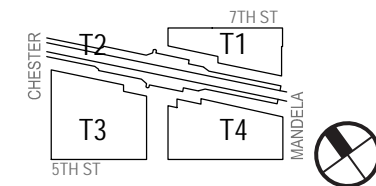
T1 / T4  
MATERIAL  
BOARD

SHEET:



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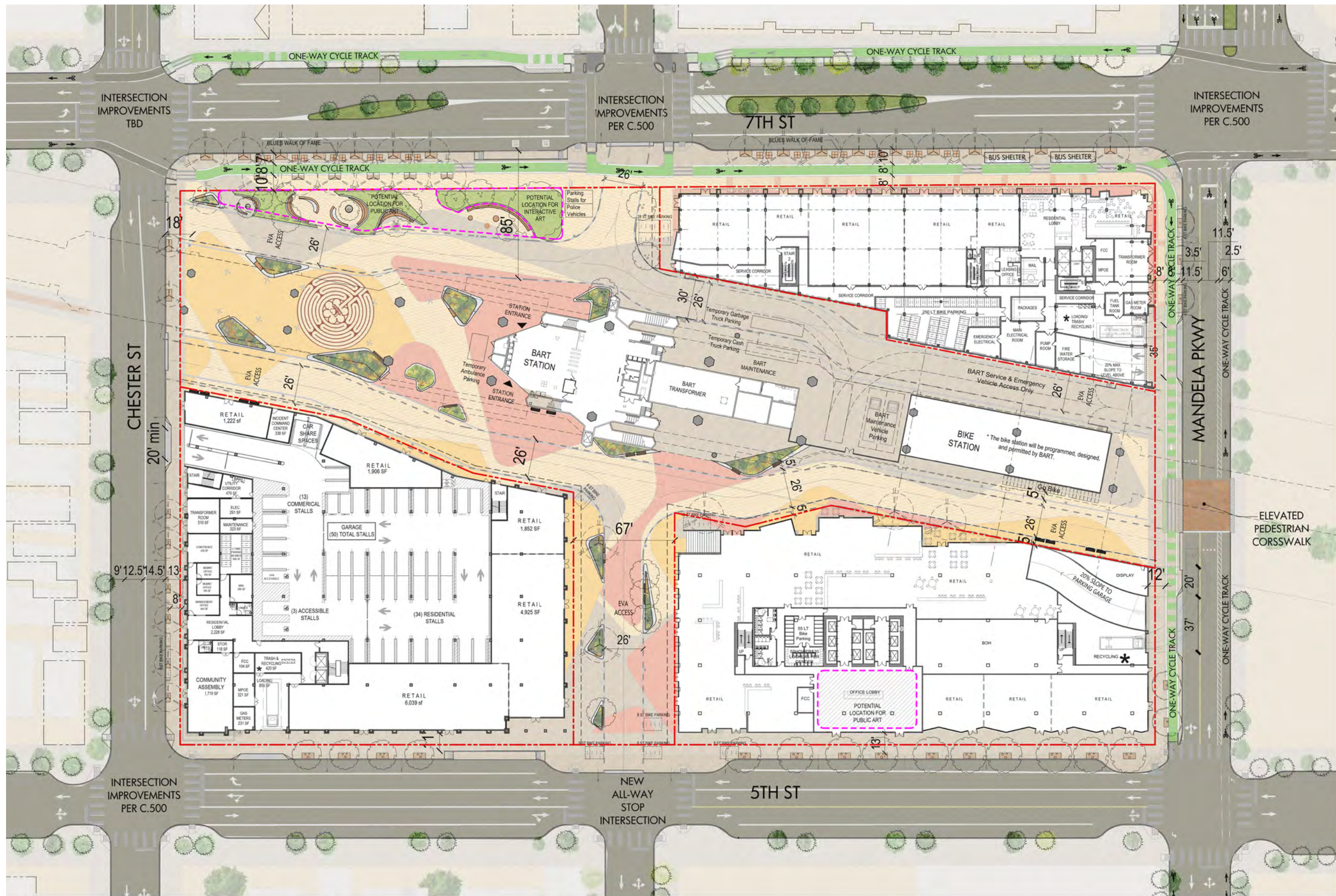


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DATE: July 24, 2020

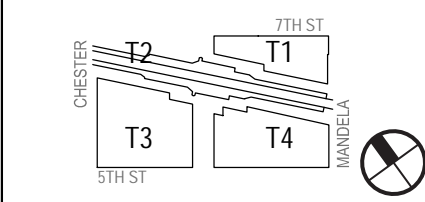
**MASSING  
DIAGRAM**

SHEET:





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ENGINEERS · SURVEYORS · PLANNERS

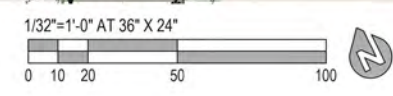
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 LANDSCAPE ARCHITECTS

PROJ. # 168-153 WO BART  
 DATE: July 24, 2020

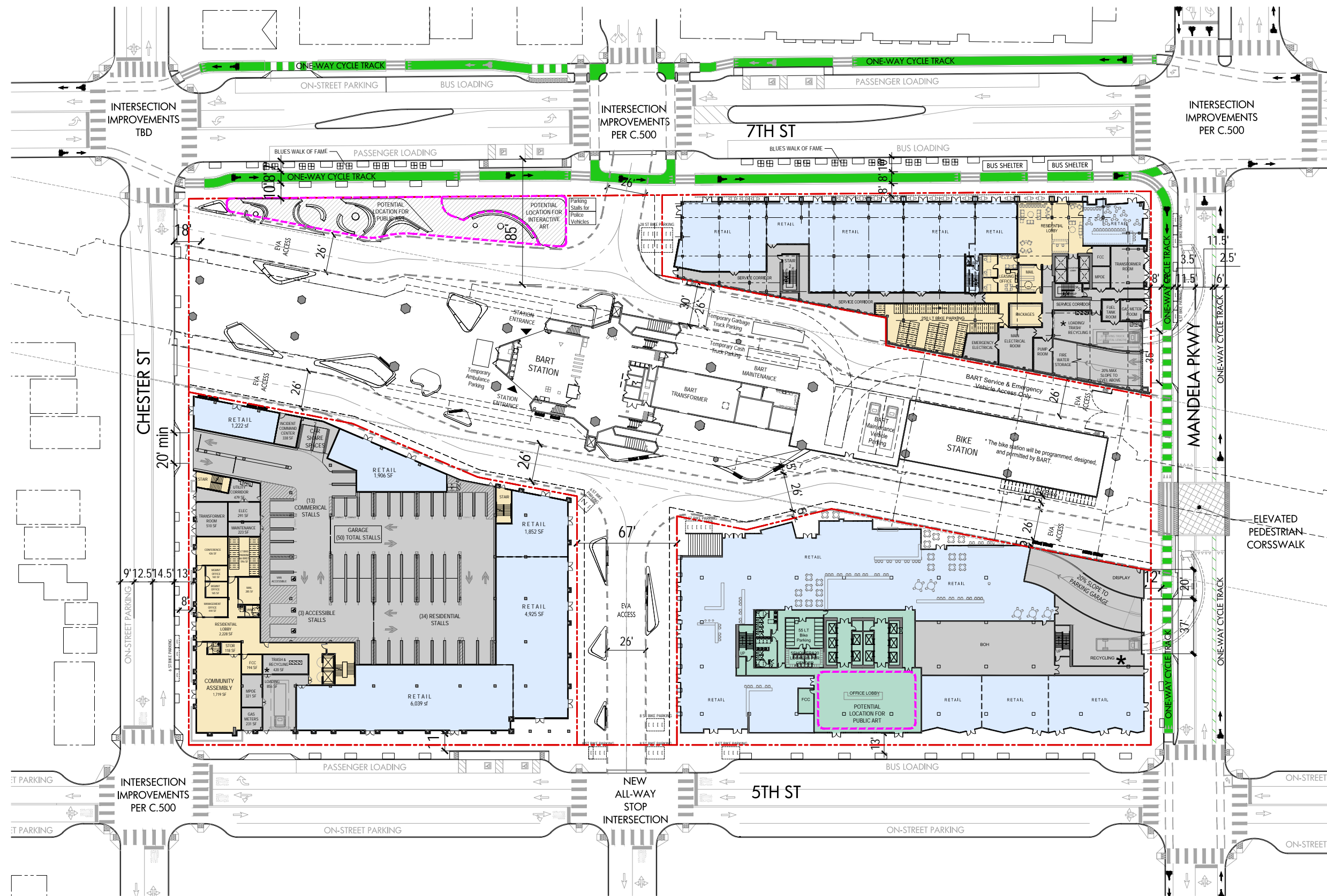
**SITE PLAN**

SHEET:

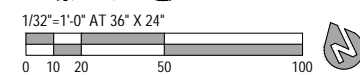
The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.



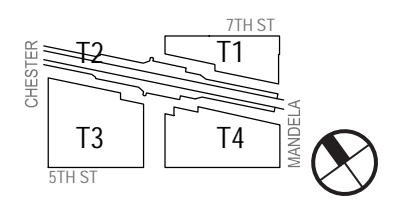




1. The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.



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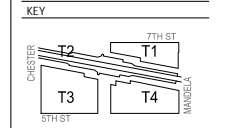
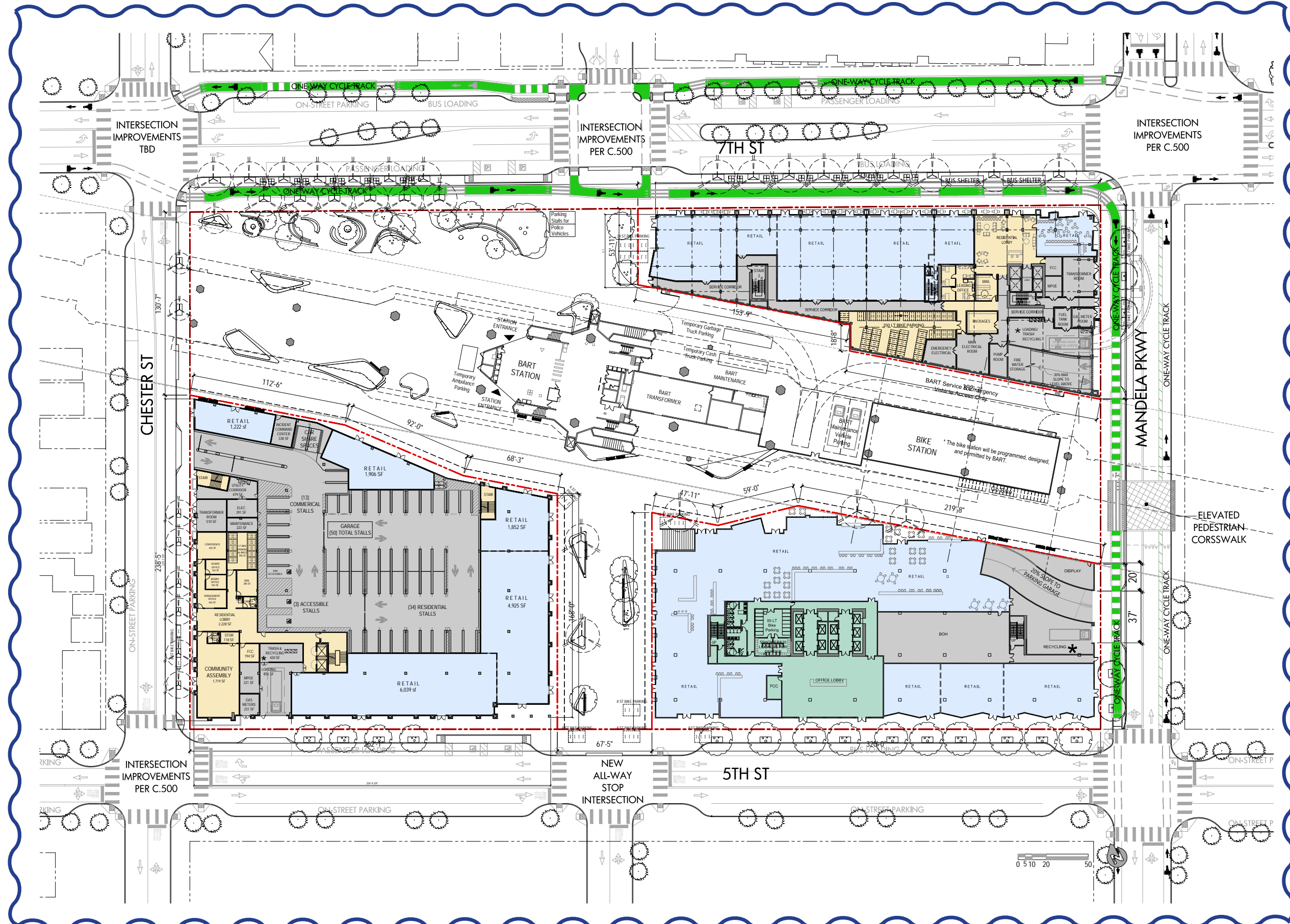


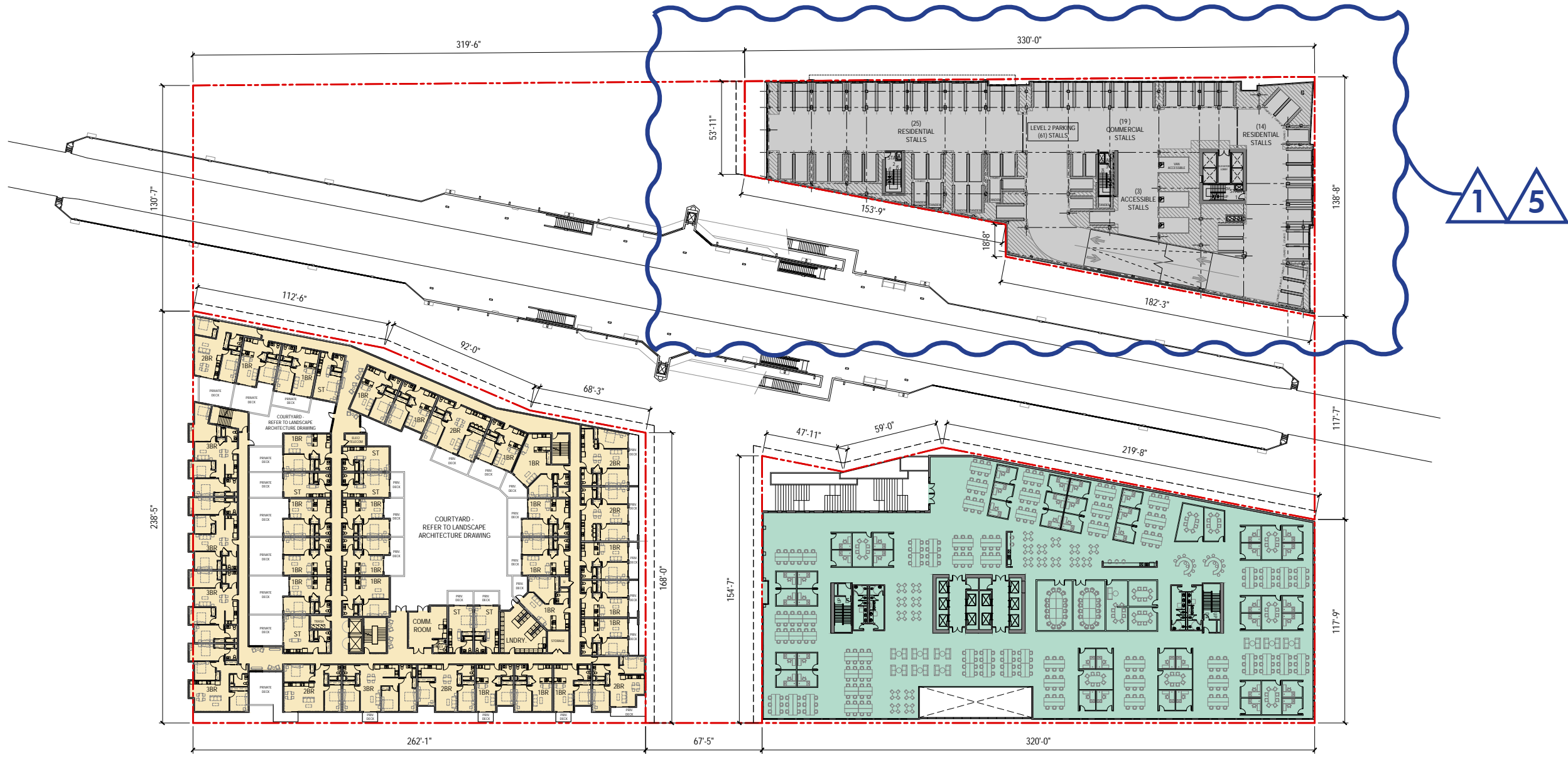
PROJ. # 168-153 WO BART  
DATE: July 24, 2020

**SITE PLAN**

SHEET:







PROJECT

**MANDELA STATION @ WEST OAKLAND BART**  
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ARCHITECT


  
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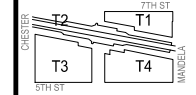

  
**BKF ENGINEERS**  
 1648 N. CALIFORNIA  
 BLVD, SUITE 409  
 WALNUT CREEK  
 CA 94596  
 (925) 949-2200


  
**PGA DESIGN**  
 444 17th Street  
 Oakland  
 CA 94612  
 (510) 465-1284


  
**STOK**  
 848B Front Street  
 San Francisco  
 CA 94111  
 (415) 326-7100

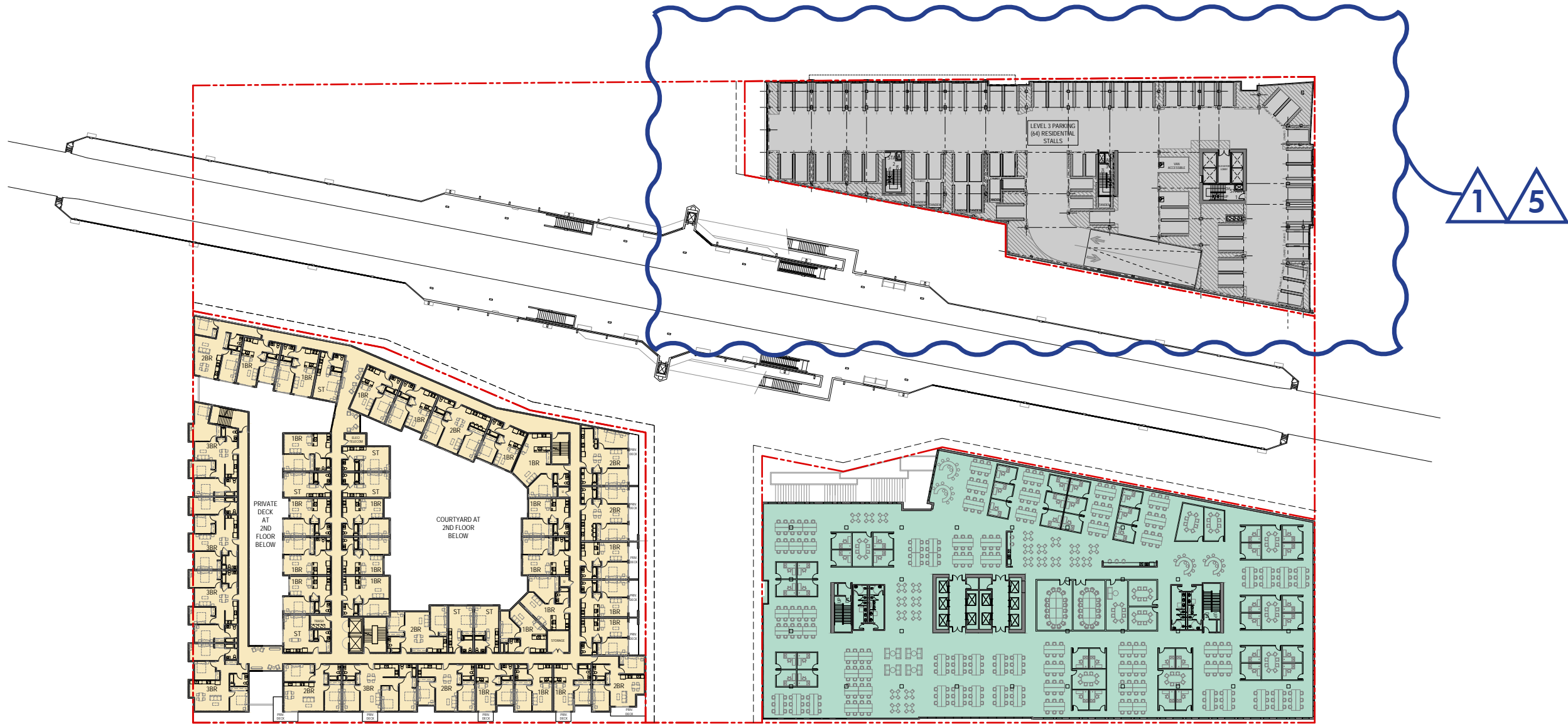
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PROJ #: 168-153 WO BART  
 DATE: July 29, 2020  
 SCALE: 1/32"=1'-0"  
 TITLE:

3RD  
 FLOOR  
 PLAN  
 SHEET: **A11.03**



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PROJECT

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Broadway and  
Telegraph  
PO Box 70126  
Oakland, CA 94612  
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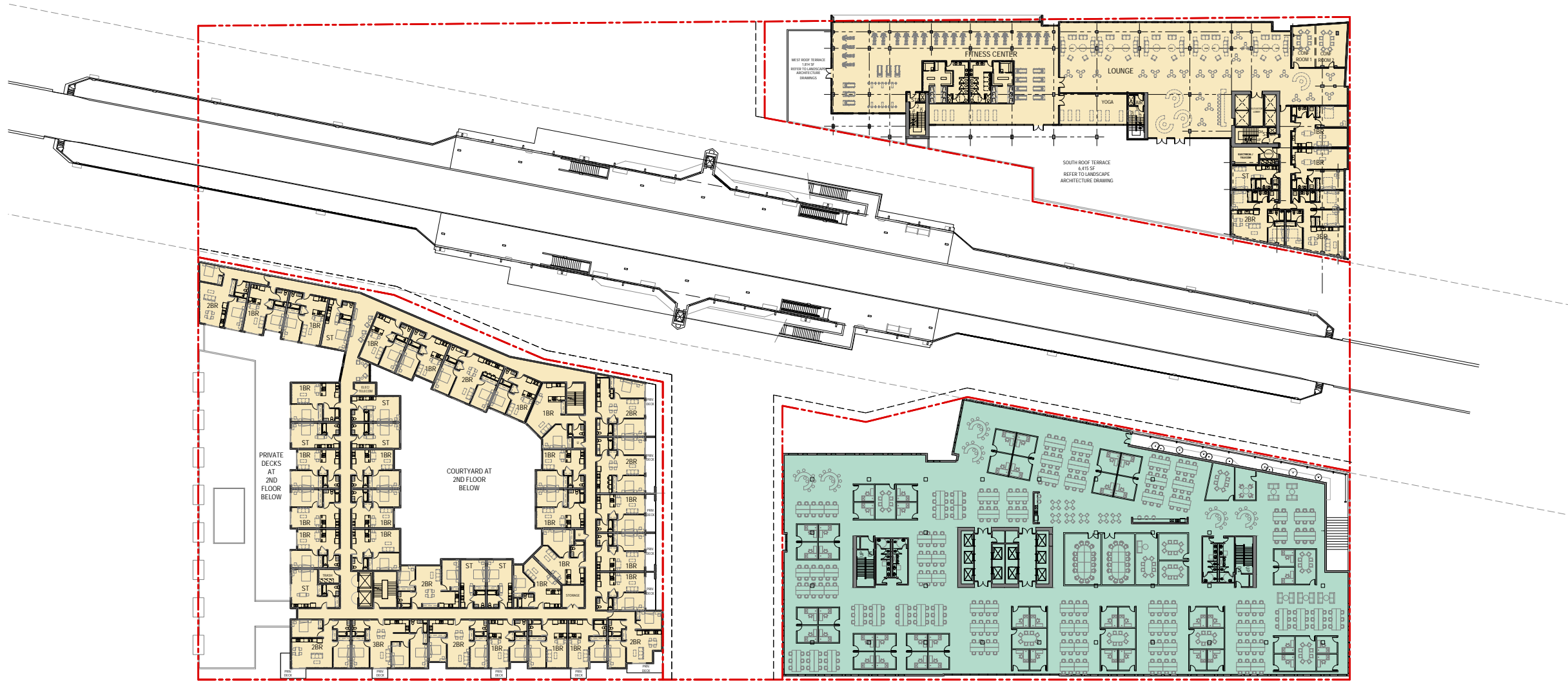
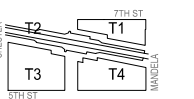
BNF ENGINEERS  
1646 N. CALIFORNIA  
BLVD, SUITE 400  
WALNUT CREEK  
CA 94596  
(925) 945-2200

PGA DESIGN  
444 17th Street  
Oakland  
CA 94612  
(510) 455-1284

STOK  
845B Front Street  
San Francisco  
CA 94111  
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STAMP

KEY



PROJ #: 168-153 WO BART  
DATE: July 22, 20  
SCALE: 1/32"=1'-0"  
TITLE:

4TH  
FLOOR  
PLAN

SHEET: **A11.04**

PDP PACKAGE

PROJECT

MANDELA STATION @ WEST OAKLAND BART  
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ARCHITECT

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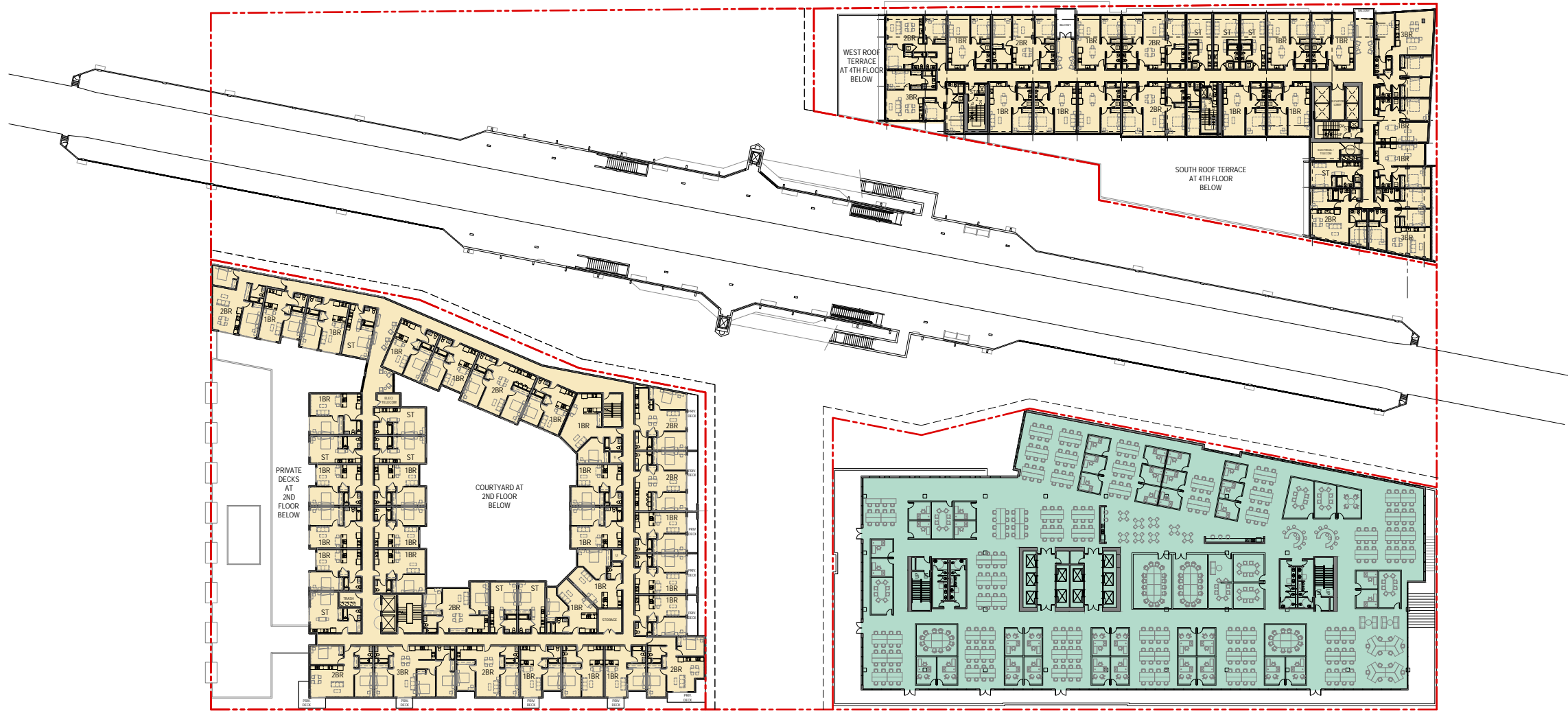
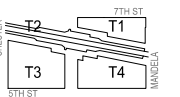
**BKF ENGINEERS**  
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BLVD, SUITE 400  
WALNUT CREEK  
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**PGA DESIGN**  
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KEY



PROJ #: 168-153 WO BART  
DATE: July 22, 20  
SCALE: 1/32"=1'-0"  
TITLE:

5TH  
FLOOR  
PLAN

SHEET: **A11.05**

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PROJECT

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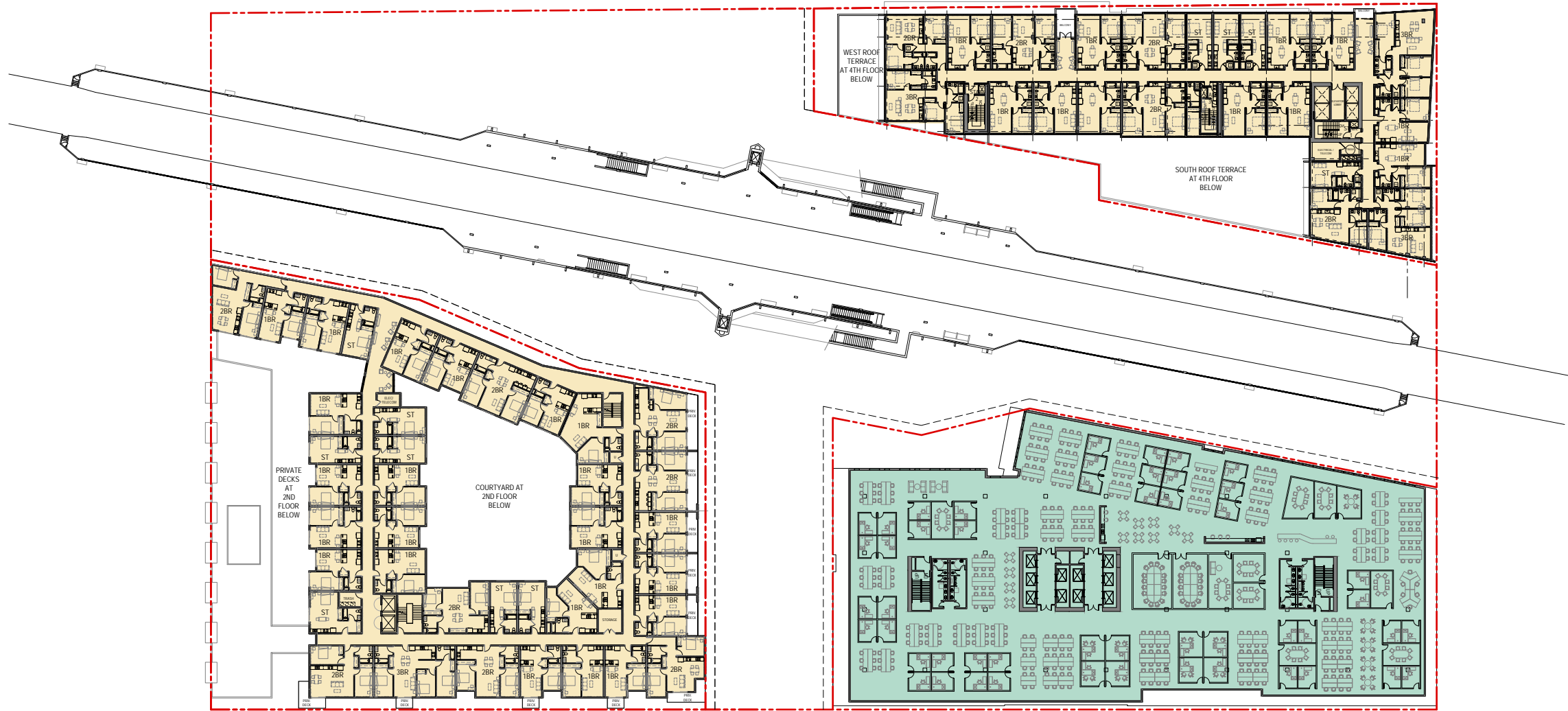
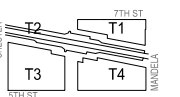
BKF ENGINEERS  
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BLVD, SUITE 400  
WALNUT CREEK  
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KEY



PROJ #: 168-153 WO BART  
DATE: July 22, 20  
SCALE: 1/32"=1'-0"  
TITLE:

6TH-7TH  
FLOOR  
PLAN

SHEET: **A11.06**

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PROJECT

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PROJECT TEAM

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CA 94596  
(925) 940-2200



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Oakland  
CA 94612  
(510) 455-1284

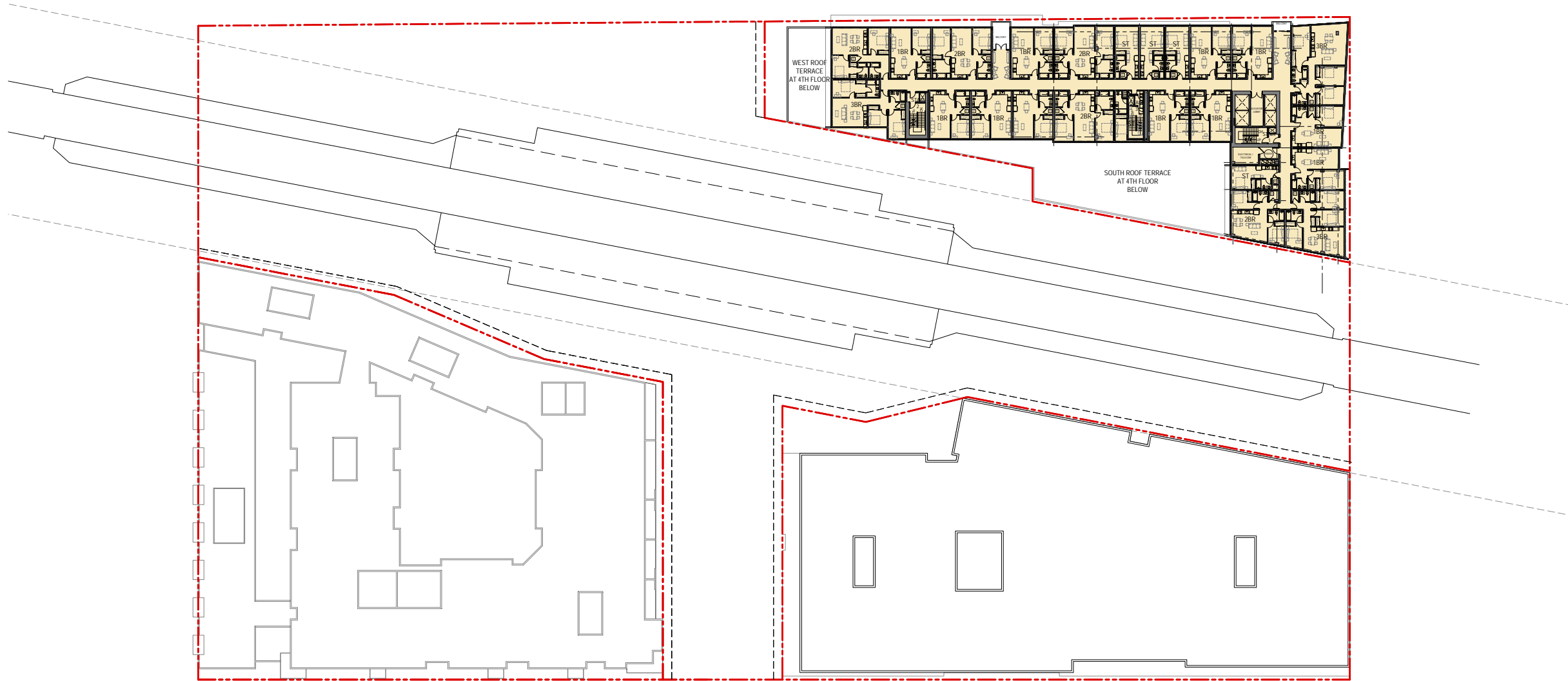
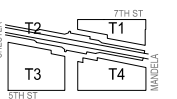


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949B Front Street  
San Francisco  
CA 94111  
(415) 329-7100



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KEY



PROJ #: 168-153 WO BART  
DATE: July 22, 20  
SCALE: 1/32"=1'-0"  
TITLE:

8-18TH  
FLOOR  
PLAN

SHEET: **A11.07**





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PROJECT

MANDELA STATION @ WEST OAKLAND BART  
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CA 94596  
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CA 94612  
(510) 455-1284

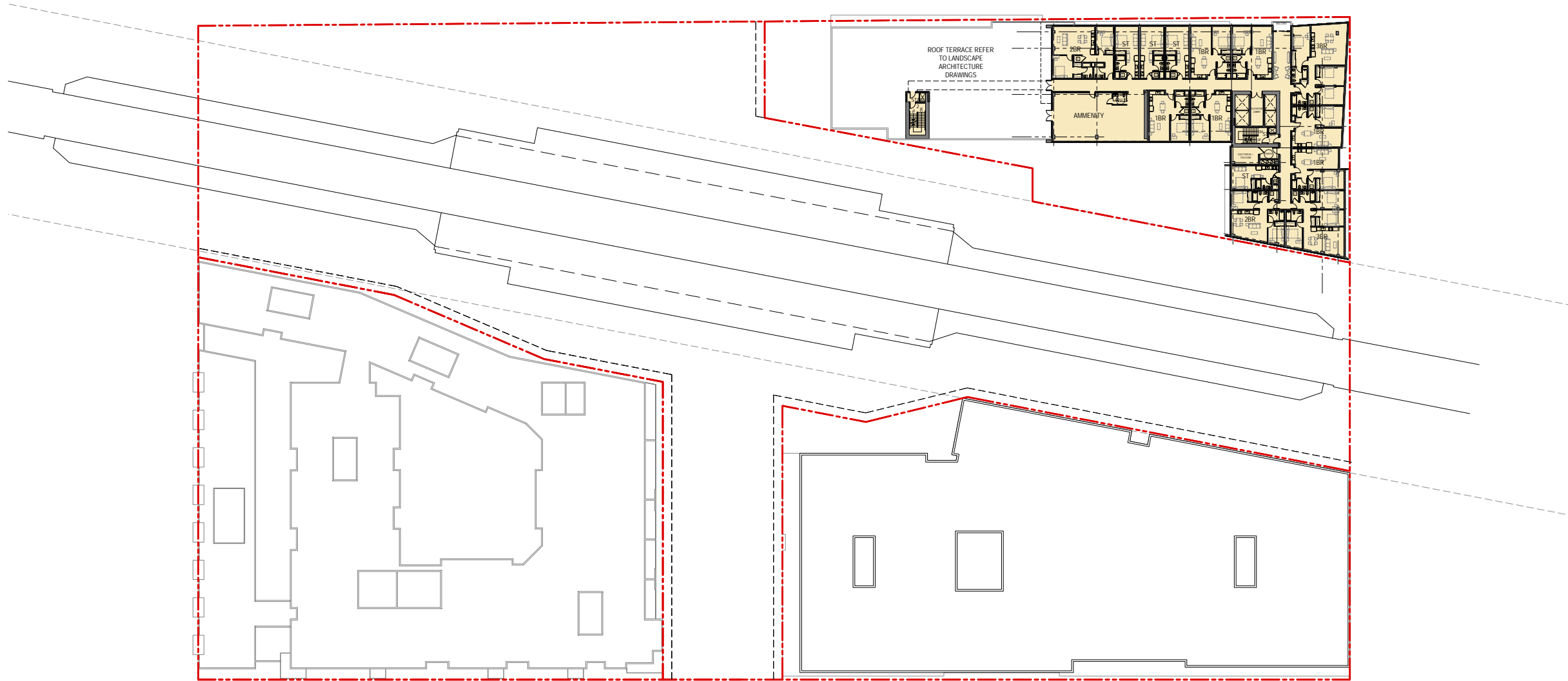
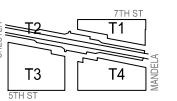


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KEY



PROJ #: 168-153 WO BART  
DATE: July 22, 20  
SCALE: 1/32"=1'-0"  
TITLE:

19TH  
FLOOR  
PLAN

SHEET: **A11.08**



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PROJECT

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ARCHITECT

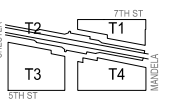


PROJECT TEAM

- BKF ENGINEERS  
 1646 N CALIFORNIA BLVD, SUITE 400  
 WALNUT CREEK, CA 94596  
 (925) 940-2200
- PGA DESIGN  
 444 17th Street  
 Oakland, CA 94612  
 (510) 465-1284
- STOK  
 949B Front Street  
 San Francisco, CA 94111  
 (415) 329-7100

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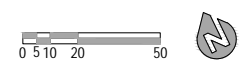
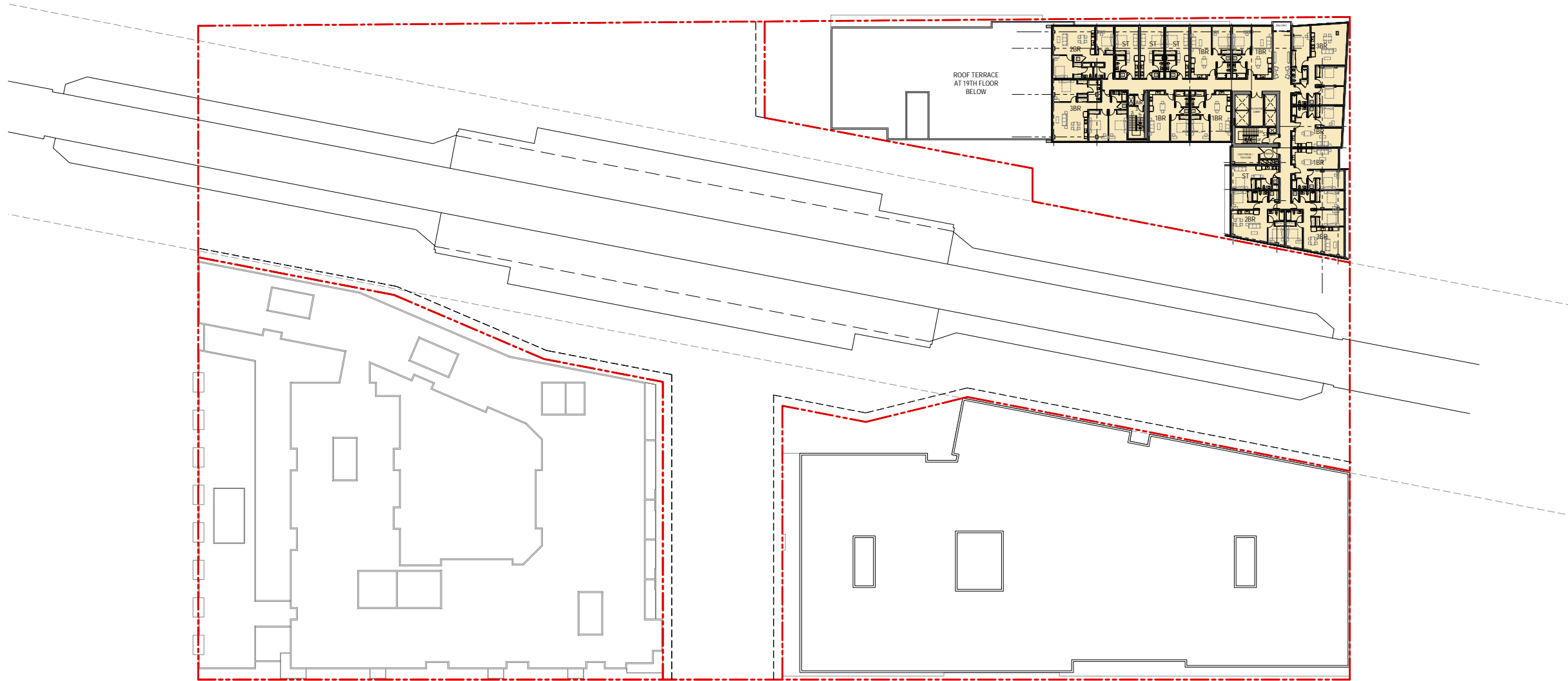
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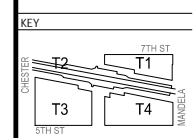
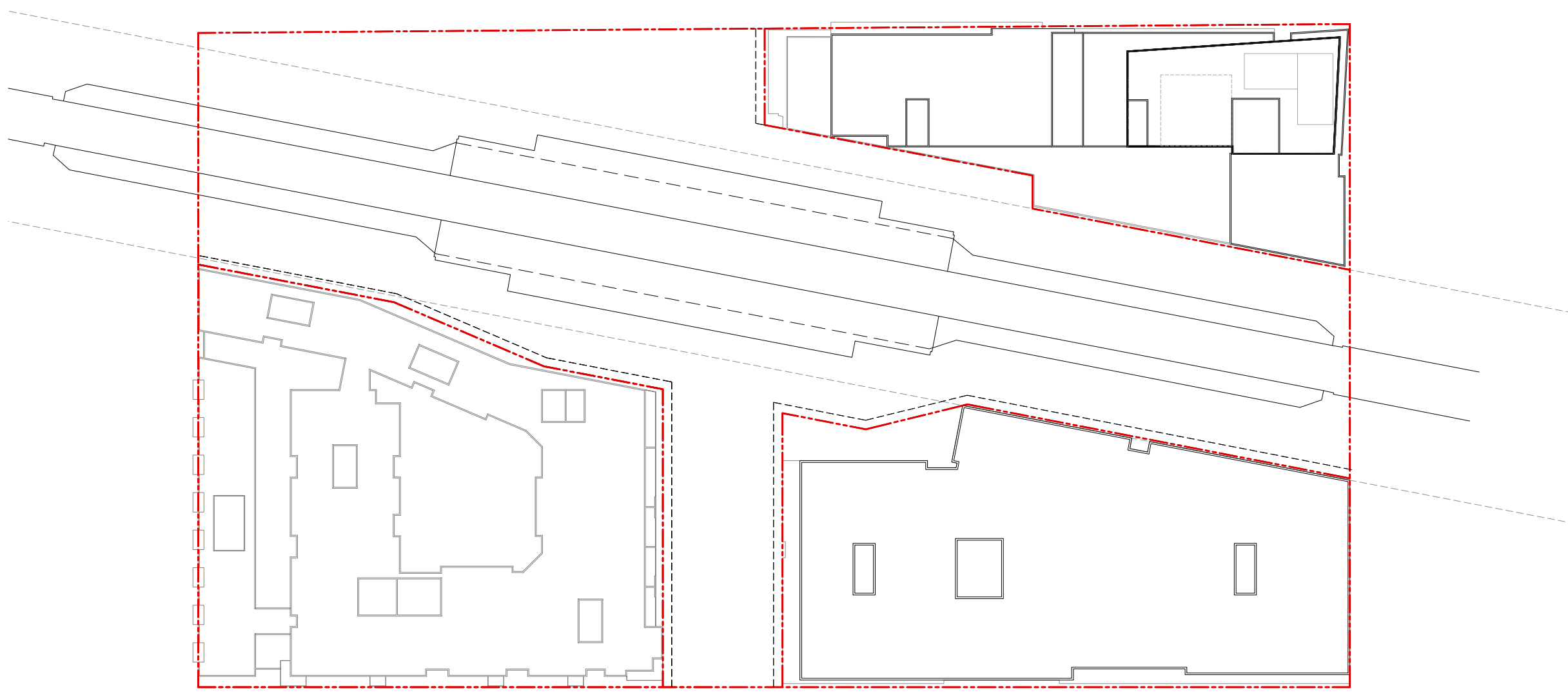


PROJ #: 168-153 WO BART  
 DATE: July 22, 20  
 SCALE: 1/32"=1'-0"  
 TITLE:

20TH-31TH FLOOR PLAN

SHEET: **A11.09**







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ARCHITECT

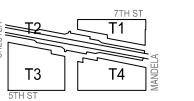


PROJECT TEAM

- BKF ENGINEERS  
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- PGA DESIGN  
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 Oakland, CA 94612  
 (510) 455-1284
- STOK  
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 San Francisco, CA 94111  
 (415) 329-7100

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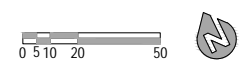
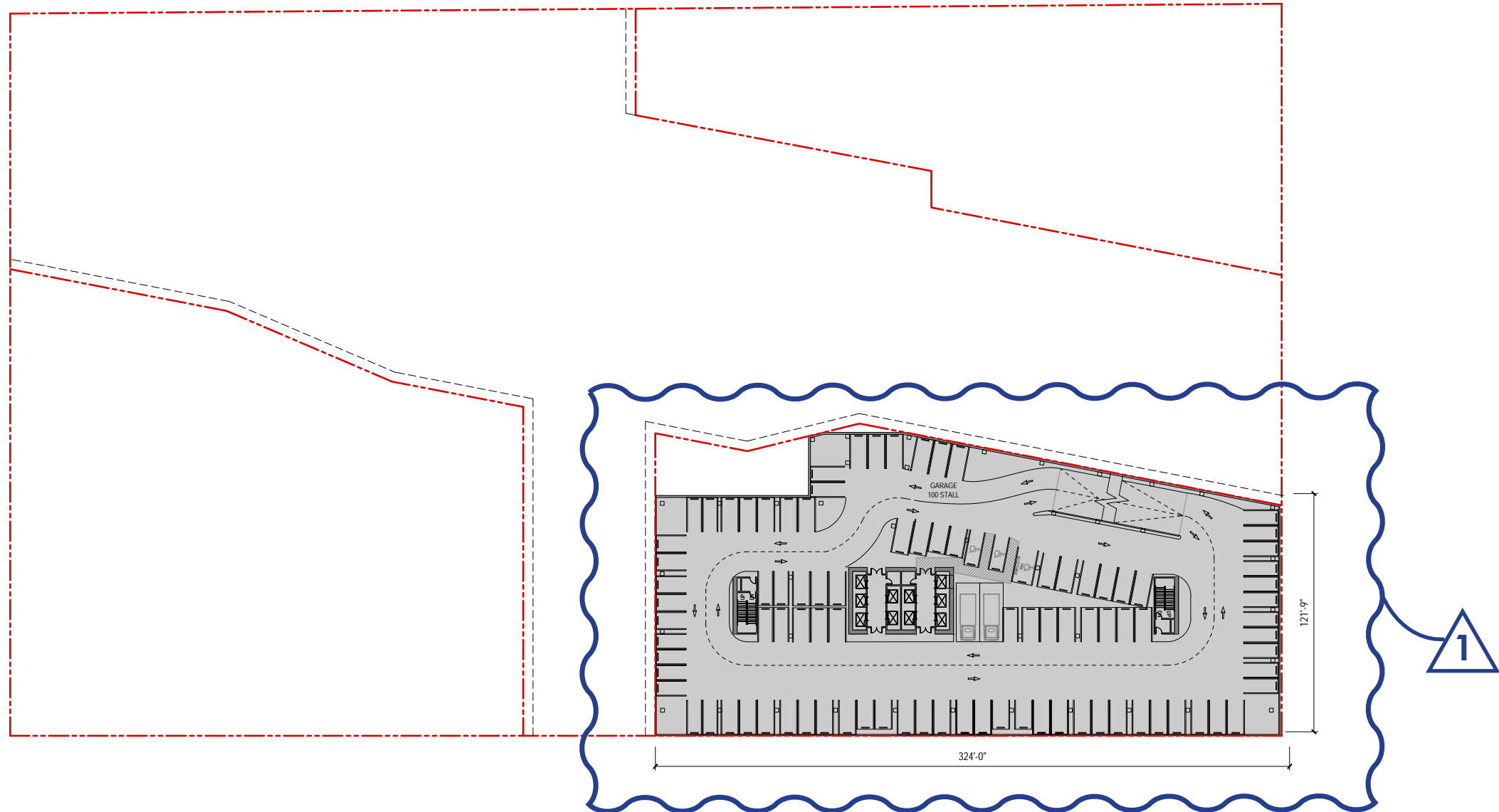
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PROJ #: 168-153 WO BART  
 DATE: July 22, 20  
 SCALE: 1/32"=1'-0"  
 TITLE:

B1  
FLOOR  
PLAN

SHEET: **A11.11**





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ARCHITECT

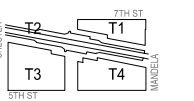


PROJECT TEAM

- INF ENGINEERS  
1646 N CALIFORNIA  
BLVD, SUITE 400  
WALNUT CREEK  
CA 94596  
(925) 940-2200
- Bkt 100
- PGA DESIGN  
444 17th Street  
Oakland  
CA 94612  
(510) 455-1284
- PGAdesign
- STOK  
949B Front Street  
San Francisco  
CA 94111  
(415) 329-7100
- stök

STAMP

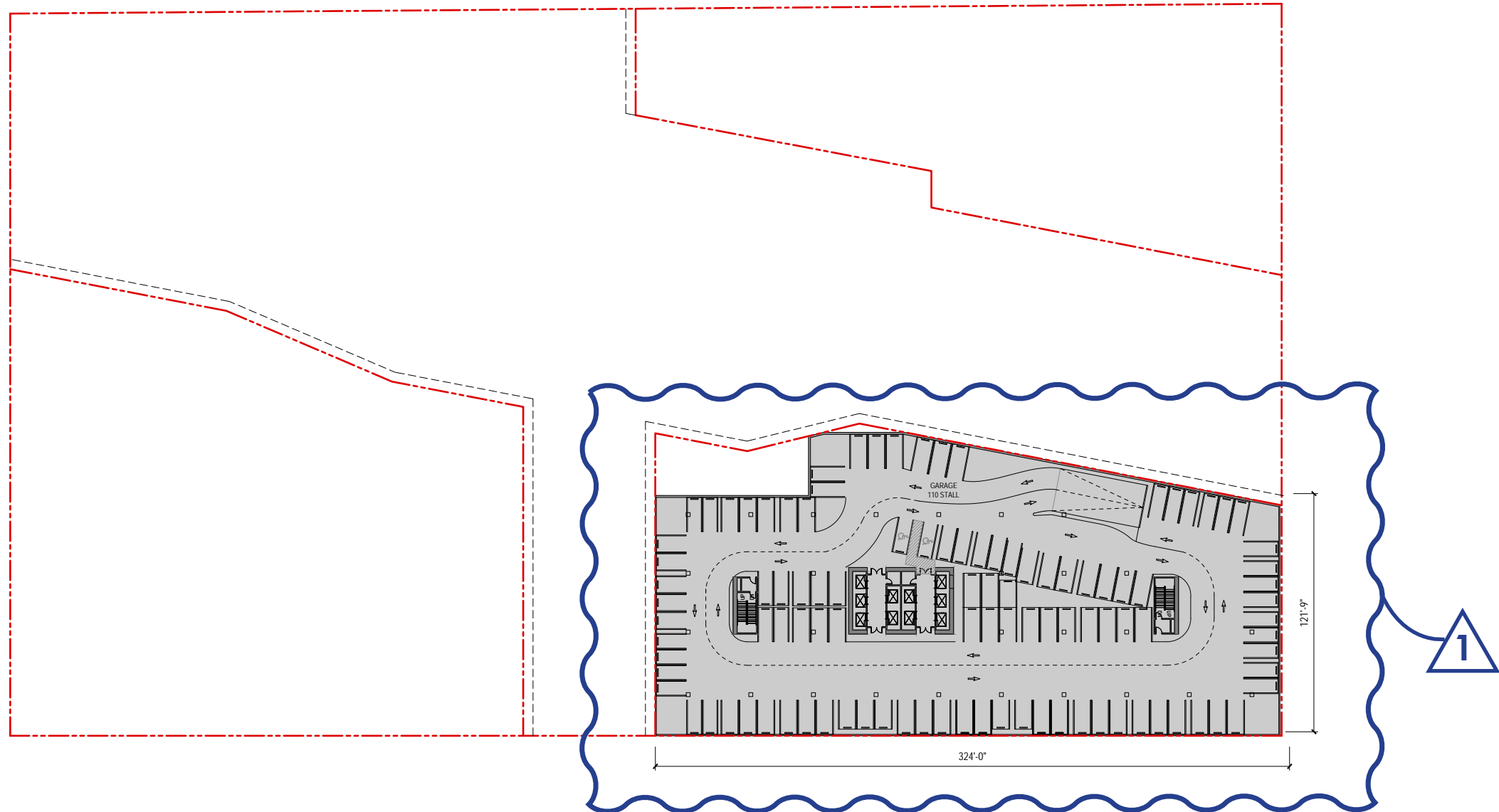
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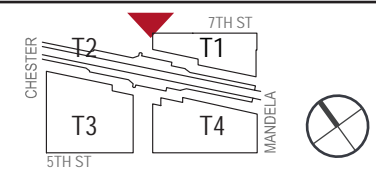
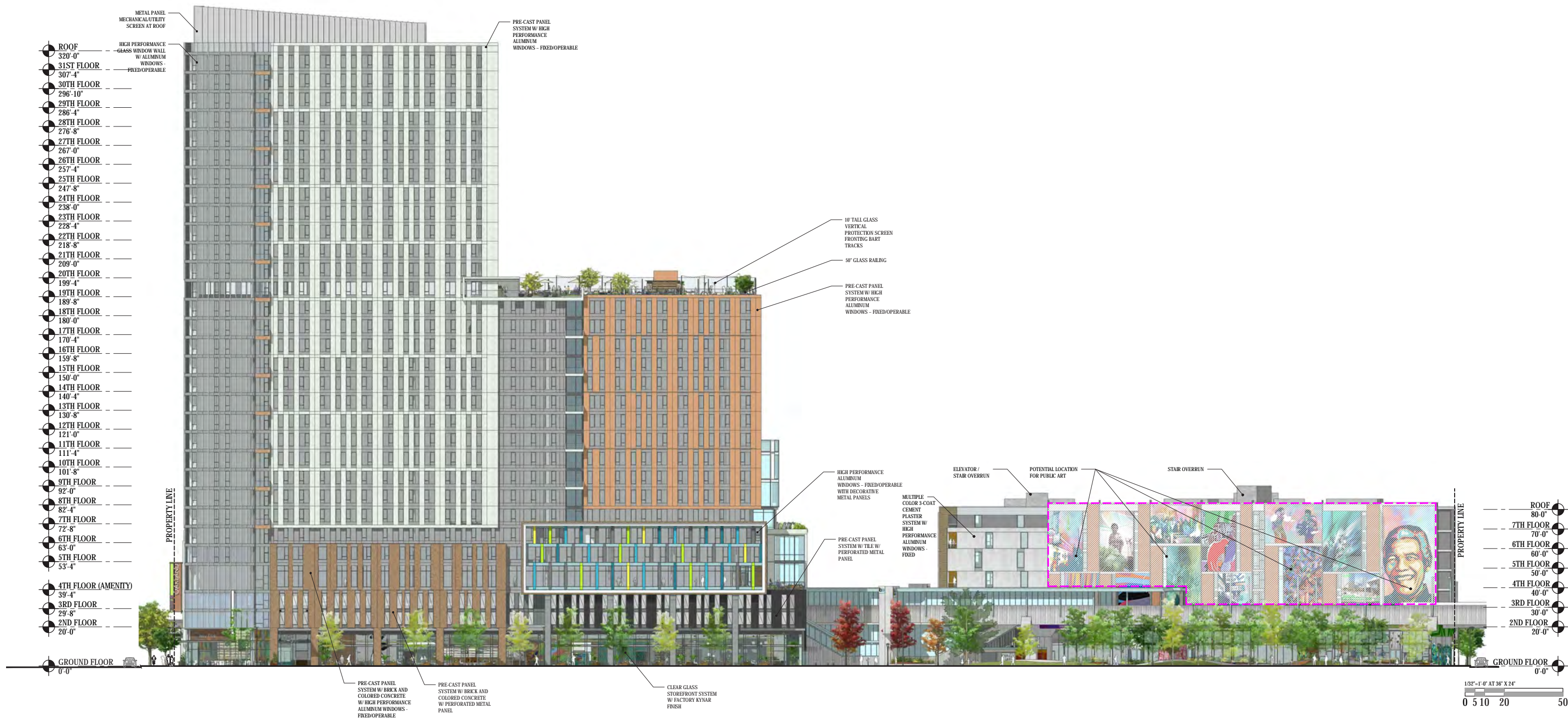


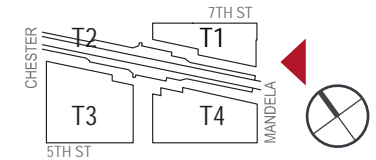
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DATE: July 22, 20  
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TITLE:

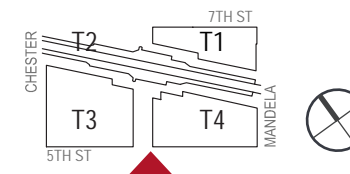
B2  
FLOOR  
PLAN

SHEET: A11.12

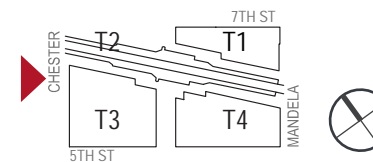


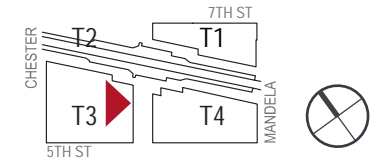


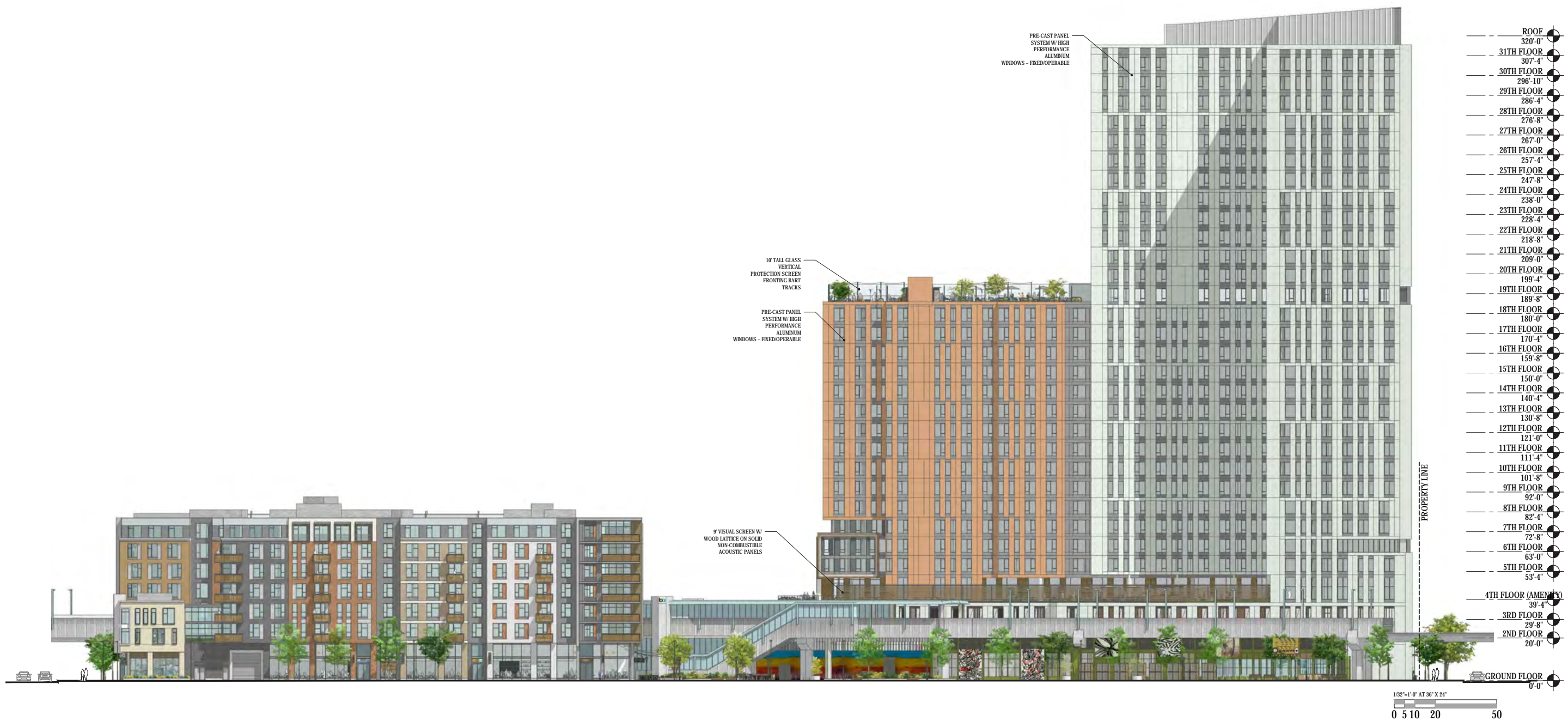








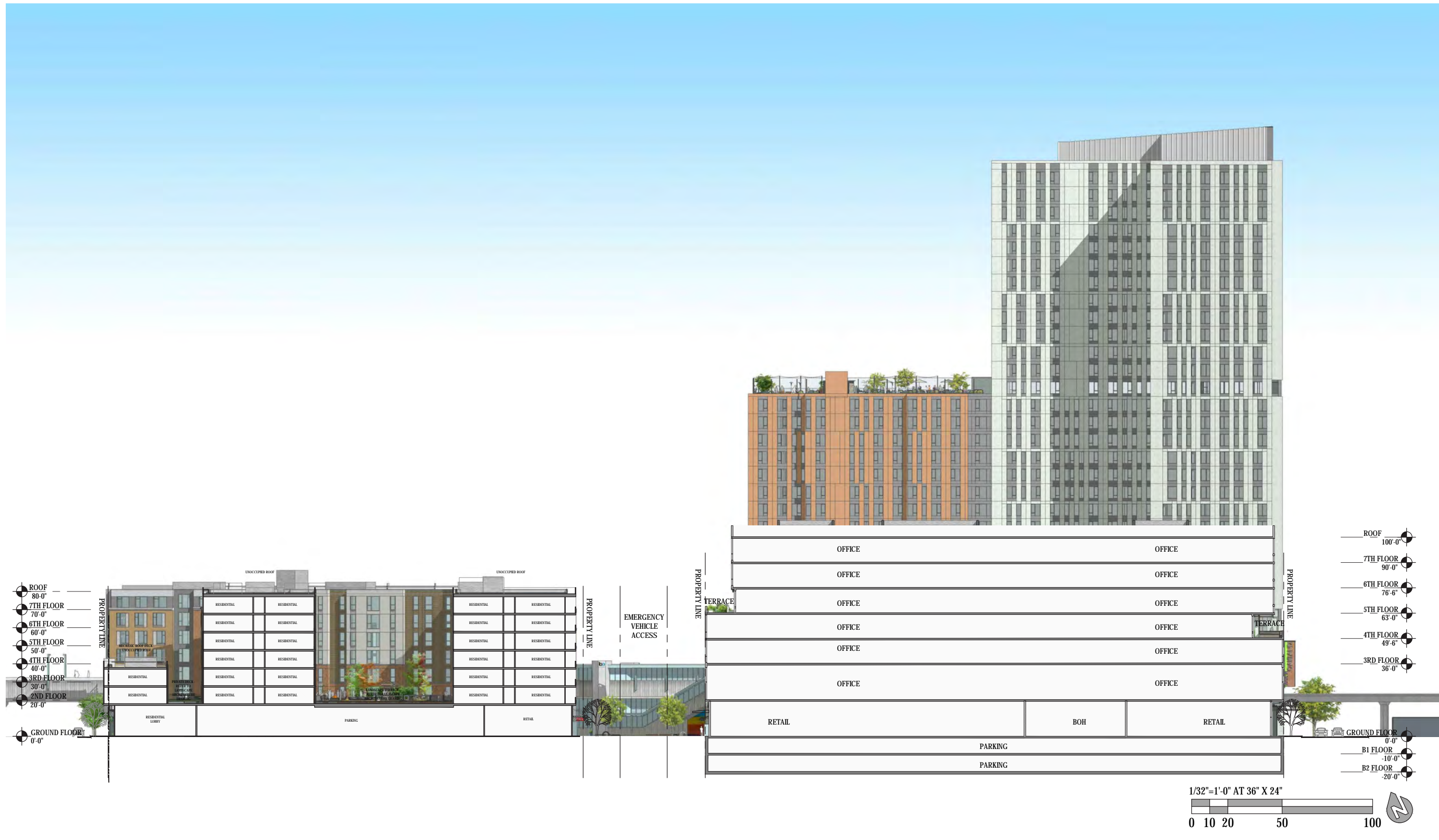




1 5







## **ATTACHMENT B:**

Proposed T1 Final Development Plan, dated July 24, 2020

# MANDELA STATION

@ West Oakland BART

## FINAL DEVELOPMENT PLAN T1

July 24, 2020





## GENERAL

G100	TITLE SHEET, DRAWING INDEX
A-00.01 -	RENDERINGS
A-00.14 -	

## CONTEXT INFO

A-01.01	ZONING INFO
A-01.02	OPEN SPACE ANALYSIS
A-01.03	ZONING SUMMARY
A-01.04	PHASING PLAN
A-01.05	GREENPOINTRATED
A-01.06	CONTEXT MAP
A-01.07	CONTEXT PHOTOS
A-01.08	CONTEXT VIEW
A-01.09	CONTEXT VIEW

## CIVIL PLANS

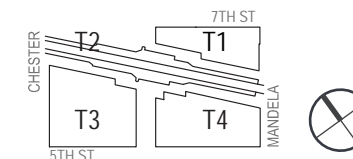
C1.00	EXISTING CONDITIONS PLAN
180384 1-5	TOPOGRAPHIC AND BOUNDARY SURVEY
C-1.01A -	TENTATIVE PARCEL MAP
C-1.01C	
C1.02	LOT MERGER PLAN
C2.00	PROPOSED GRADING PLAN
C3.00	STORMWATER MANAGEMENT PLAN
C4.00	EROSION CONTROL PLAN
C4.01	EROSION CONTROL PLAN DETAILS

## LANDSCAPE PLANS

L1.00	TREE REMOVAL PLAN
L1.01	LAYOUT & MATERIALS PLAN
L1.01.02	PUBLIC ACCESS PLAN
L1.02	LANDSCAPE WALL HEIGHT PLAN
L1.03	LANDSCAPE SECTIONS
L1.04	LANDSCAPE SECTIONS
L1.05	STREET SECTIONS
L1.06	PRELIMINARY LIGHTING PLAN
L1.07	PLANTING ZONES
L1.08	LANDSCAPE PLANTS
L1.09	LANDSCAPE MATERIALS
L1.10	LIGHTING IMAGES
L1.11	LIGHT LEVEL GOALS
L1.13	PODIUM LAYOUT PLAN T1
L1.14	PODIUM LAYOUT PLAN T1
L1.15	PODIUM PALETTE T1

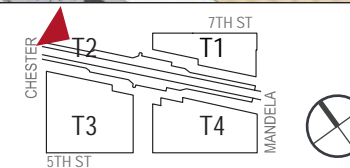
## ARCHITECTURAL PLANS, ELEVATIONS, AND DETAILS

A-02.01	MASSING DIAGRAM
A-02.02	MASSING UPDATE
A-02.03	UNIT MIX
A-02.04	MATERIAL PALETTE
A-02.05	MATERIAL PALETTE
A-02.06	MATERIAL PALETTE
A-51.01	TYPICAL WALL & WINDOW SECTION 1
A-51.02	TYPICAL WALL & WINDOW SECTION 2
A10.01	SITE PLAN
A11.01	T1 GROUND FLOOR PLAN
A11.02	T1 2ND FLOOR PLAN
A11.03	T1 3RD FLOOR PLAN
A11.04	T1 4TH FLOOR PLAN
A11.05	T1 5-18TH FLOOR PLAN
A11.06	T1 19TH FLOOR PLAN
A11.07	T1 20TH-31ST FLOOR PLAN
A11.08	T1 ROOF PLAN
A21.01	T1 NORTH ELEVATION
A21.02	T1 EAST ELEVATION
A21.03	T1 SOUTH ELEVATION
A21.04	T1 WEST ELEVATION
A31.00	SECTION A-A
A31.01	SECTION D-D



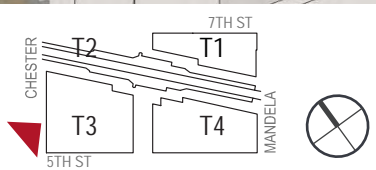


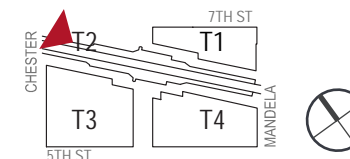
Residential T1

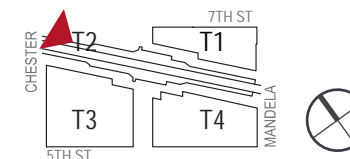


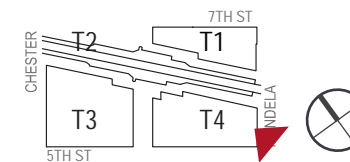


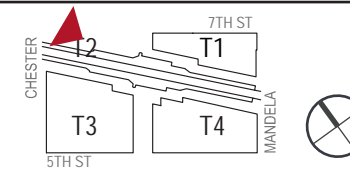
Residential T1

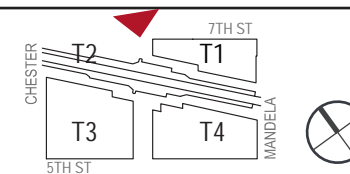




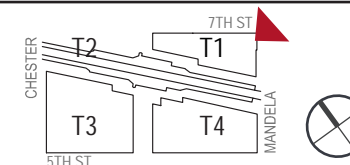


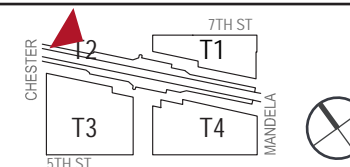


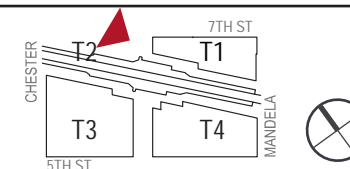


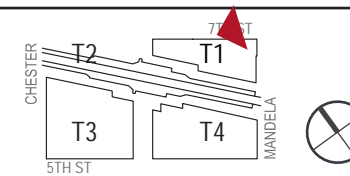


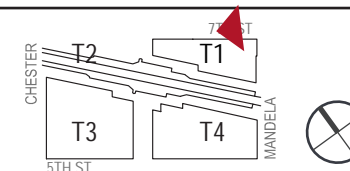


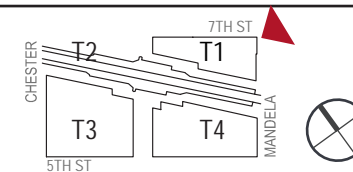










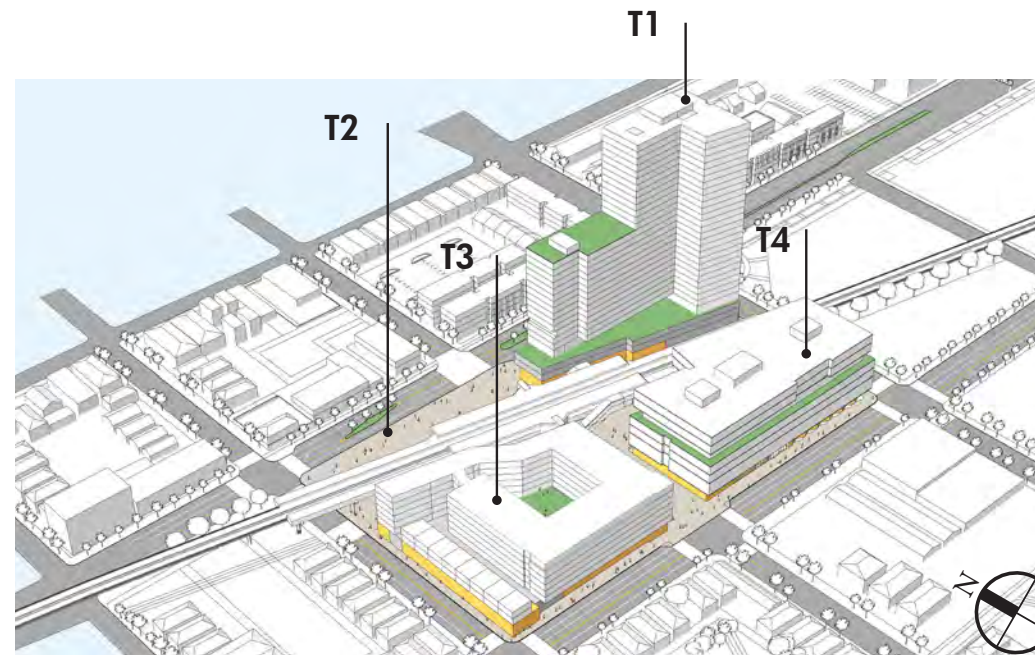


**Proposed Program**

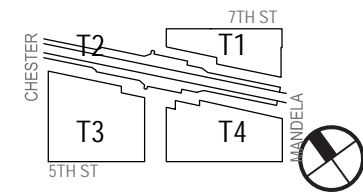
USES	T1	T2	T3	T4	Program Total
OFFICE				300,000 sf	300,000 sf
RETAIL	14,350 sf	Flexible Kiosk Program	15,944 sf	23,184 sf	53,478 sf
OTHER NON-RESIDENTIAL			2,057 sf	8,540 sf	10,597 sf
RESIDENTIAL	522 units		240 units*		762 units (20% min. affordable)
PARKING	125		50	210	385 spaces

\*79 units at 30% AMI, 11 units at 50% AMI, 148 units at 60% AMI, 2 units at 120% AMI

**Note:** Program is based on the previously approved Conditions of Approval, As approved by the Planning Commission February 6, 2019.



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**T1 Open Space Calculation and Parking Analysis**

Residential Open Space Req			
522 Units	200	104,400 SF	
<b>Required Open Space</b>		<b>104,400 SF</b>	
Open Space Provided			
Common Use Decks		2,334 SF	
Roof Terraces		15,314 SF	
		<b>17,648* SF</b>	
*Open space waiver of 86,752 sf requested for T1 per Density Bonus Letter.			
T1 Parking Analysis			
T1 Req. Pkg	522	0.5	261 Pkg
Commercial Pkg	14,350	600	19
<b>Total</b>			<b>280 Spaces</b>
PKG Reductions			
Transit	0.3		84 Spaces
Car Share	0.2		56 Spaces
<b>Residential PKG Req</b>			<b>140 Spaces</b>
Parking Provided			
T1 Parking			<b>125* Spaces</b>
*Parking waiver requested for 15 spaces per Density Bonus Letter.			
Bicycle Parking			
	Multi-family Req	Commerical Req	Total Req
Short Term	27	8	35
Long Term	131	2	133
			<b>Total Provided</b>
			<b>36</b>
			<b>250</b>

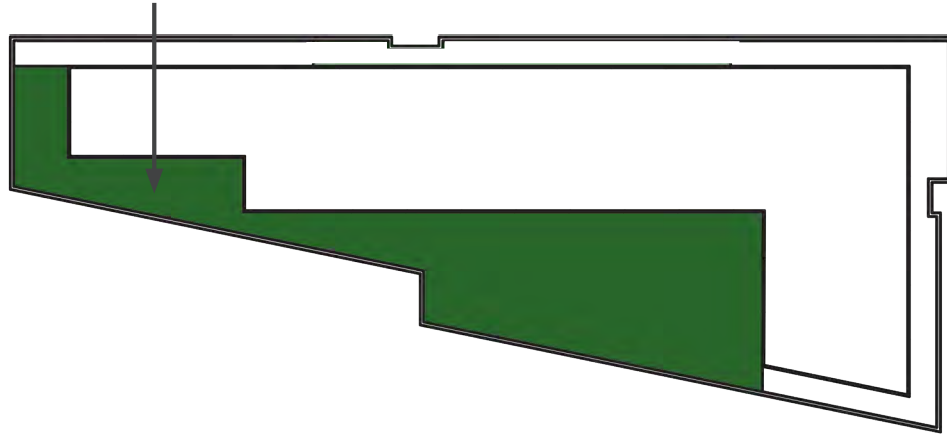
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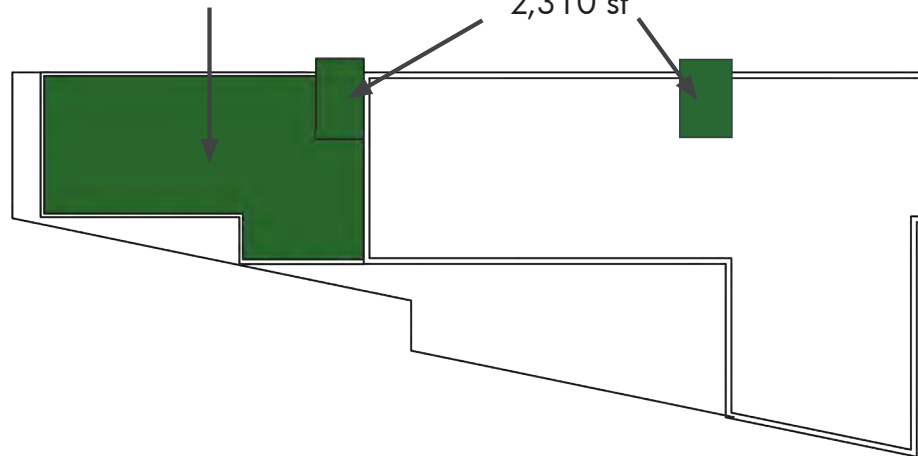
SHEET: ZONING INFO  
A-01.01

Landscaped Terrace  
9,096Sf Common Open Space



**BUILDING T1 - LEVEL 4 - AMENITY**

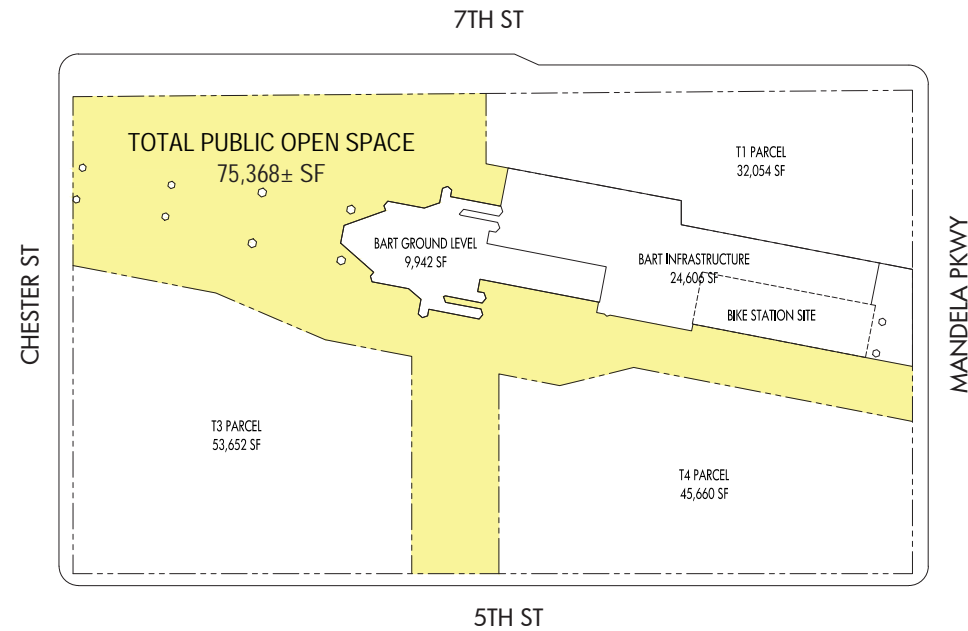
Common Use Terrace 7,961 sf  
Level 5-31 - Common Use Decks 2,310 sf



**BUILDING T1 - LEVEL 19 - AMENITY**

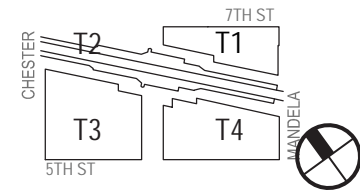
Common Use Decks	2,334 SF
Roof Terraces	15,314 SF
	<b>17,648* SF</b>

\*Open space waiver of 86,752 sf requested for T1 per Density Bonus Letter.



**GROUND LEVEL - PUBLIC OPEN SPACE 75,368± SF**

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LANDSCAPE ARCHITECTS

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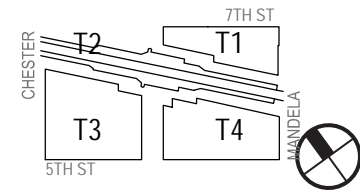
SHEET: OPEN SPACE ANALYSIS  
A-01.02



GENERAL ZONING INFORMATION	REFERENCE	NOTES
ASSESORS BLOCK	Survey	See Survey; Tract 8046, Blocks 493
ZONING USE DISTRICT	Oak GIS	S-15W
PERMITTED AND/OR CONDITIONAL USES	17.97.010-.020	Residential, Community Assembly, General Retail, Full +Limited Service Retail, Parking, Commercial Office permitted
HEIGHT & BULK DISTRICT	17.97	S-15W 100'; master plan consistent with zoning requirements
GENERAL PLAN / POLICY PLAN	OAK-GIS	General Commercial; West Oakland Specific Plan
HISTORIC OR LANDMARK STATUS	OAK-GIS	None
LIQUEFACTION HAZARD ZONE	OAK-GIS	Severity 4
CONDO CONVERSION IMPACT AREA	OAK-GIS	None
<b>HEIGHT AND BULK CONTROLS</b>		
SITE AREA	Survey	31,643 SF
FLOOR AREA RATIO (FAR)	17.97.130	S-15W 100' - 5.0; Master Plan within zoning density requirements
HEIGHT LIMIT	17.97	100' (Height limit modified to allow 320' tall building pursuant to State Affordable Housing Exemption)
REQUIRED SETBACKS	17.97.060	No Front Yard Setbacks Required; Interior Lot subject to PUD
REAR YARDS / COURTS		None Required
ADJACENCIES		None Significant
UNIT SEPARATION / EXPOSURE REQUIREMENTS	17.108.080	8' minimum at living room window +2' for each floor above = maximum 10% of It width
<b>DETAILED CONTROLS &amp; REQUIREMENTS</b>		
RESIDENTIAL DENSITY LIMITS	17.97.130	S-15W100' - 225sf/unit; Density increase per State Affordable Housing and PUD density bonus
OPEN SPACE REQUIREMENTS	17.97.130	S-15W 100' - 75sf/unit; Overall master plan within zoning limites
SCREENING & SETBACK OF PARKING & LOADING	17.116.290	All parking garages are screened per zoning requirements
OFF-STREET PARKING - RESIDENTIAL	17.116.060	0.5 parking space per dwelling unit required; Parking meets zoning requirements with approved reductions
OFF-STREET PARKING - RETAIL	17.116.080	1 space/600 Sf of ground floor; Parking meets zoning requirements with approved reductions
OFF-STREET PARKING - COMMERCIAL	17.116.080	None required
OFF-STREET PARKING DIMENSIONS	17.116.200	50-50 compact / standard; or 75% intermediate + 12.5% compact
OFF-STREET DRIVE AISLE DIMENSIONS	17.116.210	21'-24' two way aisle widths
OFF-STREET LOADING - RESIDENTIAL	17.116.120	Loading per zoning requirements
OFF-STREET LOADING - RETAIL	17.116.150	Loading per zoning requirements
LOADING BERTH DIMENSIONS	17.116.220	10'x23', 12' high for residential activities
BICYCLE REQUIREMENTS - RESIDENTIAL	17.117.090	LT: total 131 spaces; ST: total 27 spaces LT: 1 per 10,000 sf of commercial; ST: 1 per 20,000 sf of commercial LT: 0.25 spaces per dwelling unit; ST: 0.05 per dwelling unit; bicycle parking provide per zoning code
BICYCLE REQUIREMENTS - RETAIL	17.117.110	LT: total 2 spaces; ST: total 8 spaces LT: 1 per 12k; ST: 1 per 2k; bicycle parking provided per zoning code



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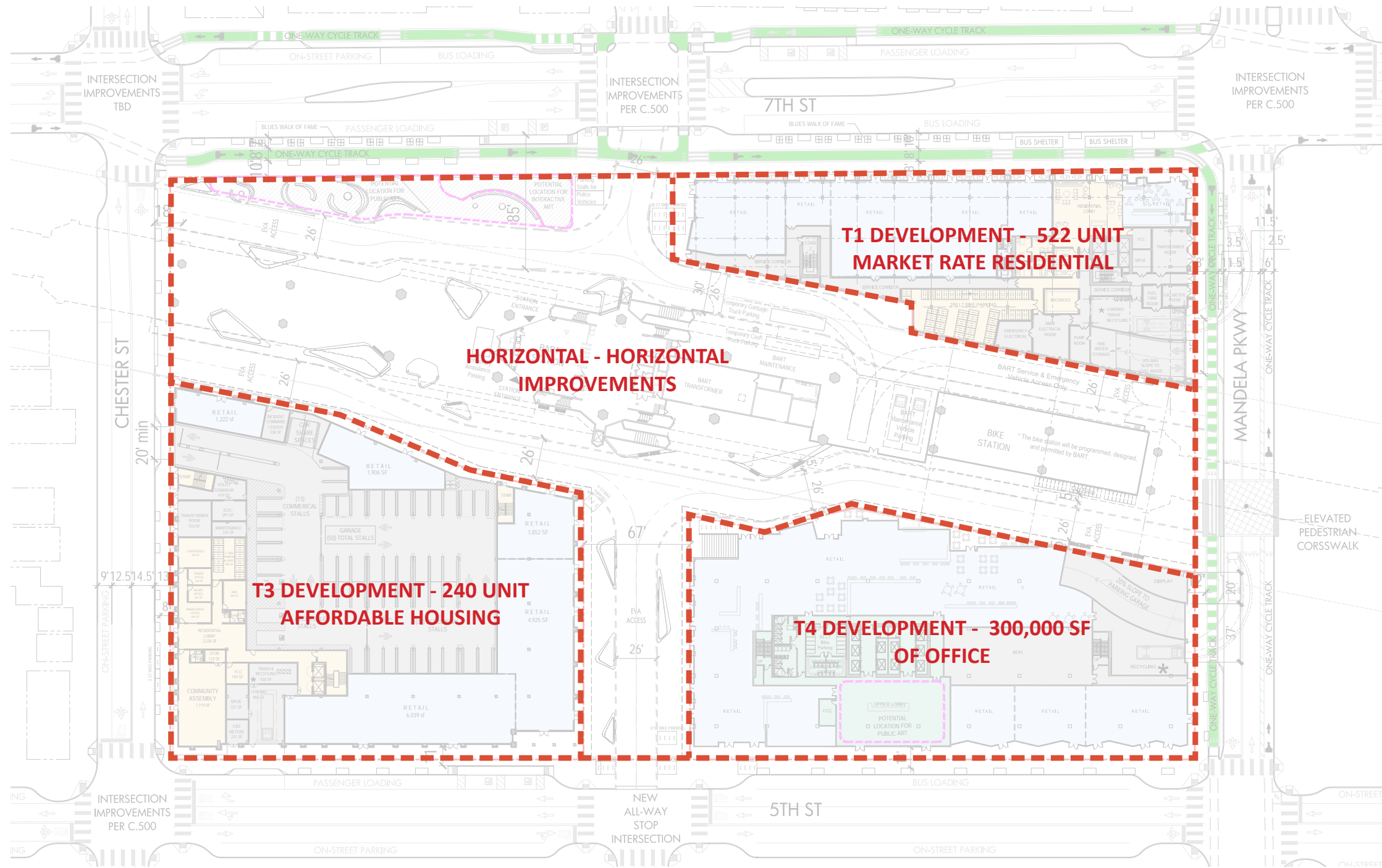


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PROJ. # 168-153 WO BART  
DATE: July 24, 2020

SHEET: **ZONING  
SUMMARY**  
A-01.03



### PROJECT SCHEDULE

1. Horizontal

i.	Submit application for Final Development Plan	1 year following PDP approval	
ii.	Anticipated FDP approval date by Agency	1 year following submittal of Horizontal FDP application	
iii.	Commence construction – Initial PX and/or PZ job (additional PX and PZ jobs will be tied to each later phase)	2 years following FDP approval	

2. Phase I – T3 Development (plus PZ/PX for that phase)

i.	Submit application for Final Development Plan	1 year following PDP approval	
ii.	Anticipated FDP approval date by Agency	1 year following submittal of Phase I FDP application	
iii.	Commence construction	2 years following FDP approval (allowing time to secure affordable financing)	

3. Phase II – T1 Development & T2 Development (plus PZ/PX for that phase)

i.	Submit application for Final Development Plan	3 years following PDP approval	
ii.	Anticipated FDP approval date by Agency	1 year following submittal of Phase II FDP application	
iii.	Commence construction	The latter of 2 years following FDP approval or 2 years following Phase I commencement of construction	

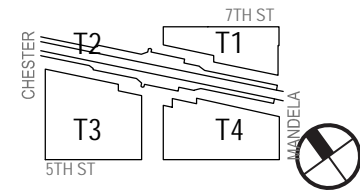
4. Phase III – T4 Development (plus PZ/PX for that phase)

i.	Submit application for Final Development Plan	5 years following PDP approval	
ii.	Anticipated FDP approval date by Agency	1 year following submittal of Phase III FDP application	
iii.	Commence construction	The latter of 2 years following FDP approval or 2 years following Phase II commencement of construction	

All dates are subject to force majeure events



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PHASING  
PLAN  
SHEET: A-01.04



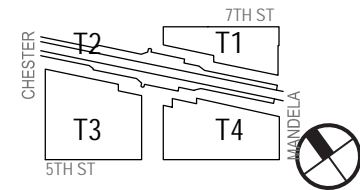
NEW HOME RATING SYSTEM, VERSION 7.0

Blueprint Scoresheet

Mandela Station T1		Points Targeted	Community	Energy	IAQ/Health	Resources	Water
		Possible Points					
<b>CALGreen</b>							
Yes	CALGreen Res (REQUIRED)	4		1	1	1	1
<b>A. SITE</b>							
Yes	A6. Stormwater Control: Prescriptive Path						
	A6.3 Non-Leaching Roofing Materials	1					1
<b>C. LANDSCAPE</b>							
<b>C3. Resource Efficient Landscapes</b>							
Yes	C3.3 Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species	3					3
<b>E. EXTERIOR</b>							
Yes	E4. Durable and Non-Combustible Cladding Materials	1				1	
Yes	E5. Durable Roofing Materials						
Yes	E5.2 Roofing Warranty for Shingle Roofing	Y	R	R	R	R	R
<b>F. INSULATION</b>							
<b>F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content</b>							
Yes	F1.1 Walls and Floors	1				1	
Yes	F1.2 Ceilings	1				1	
<b>F2. Insulation that Meets the CDPH Standard Method—Residential for Low Emissions</b>							
Yes	F2.1 Walls and Floors	1			1		
Yes	F2.2 Ceilings	1			1		
<b>G. PLUMBING</b>							
<b>G2. Install Water-Efficient Fixtures</b>							
Yes	G2.1 WaterSense Showerheads 1.8 gpm with Matching Compensation Valve	2					2
Yes	G6. Submeter Water for Tenants	2					2
<b>H. HEATING, VENTILATION, AND AIR CONDITIONING</b>							
<b>H6. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality</b>							
Yes	H6.1 Meet ASHRAE Standard 62.2-2010 Ventilation Residential Standards	Y	R	R	R	R	R
Yes	H8. High Efficiency HVAC Filter (MERV 13+)	1			1		
<b>J. BUILDING PERFORMANCE AND TESTING</b>							
<b>J5. Building Performance Exceeds Title 24 Part 6</b>							
Option 1: Compliance Over Title 24	J5.1 Home Outperforms Title 24	25		25+			
<b>K. FINISHES</b>							
<b>K1. Entryways Designed to Reduce Tracked-In Contaminants</b>							
Yes	K1.2 Entryways to Buildings	1			1		
<b>L. FLOORING</b>							
≥75%	L2. Low-Emitting Flooring Meets CDPH 2010 Standard Method—Residential	3			3		
<b>M. APPLIANCES AND LIGHTING</b>							
Yes	M1. ENERGY STAR® Dishwasher	1					1
Yes	M2. Efficient Clothes Washing and Drying						
	M2.2 Energy Star Dryer	1		1			
<20 cubic feet	M3. Size-Efficient ENERGY STAR Refrigerator	2		2			
Full Circuit	M6. Electric Vehicle Charging Stations and Infrastructure	2		2			
Yes	M8. Gearless Elevator	1		1			
<b>N. COMMUNITY</b>							
<b>N1. Smart Development</b>							
Yes	N1.1 Infill Site	2	1			1	
>35	N1.3 Conserve Resources by Increasing Density	4		2		2	
	N1.5 Home Size Efficiency	8				10	
740	Enter the area of the home, in square feet						
2	Enter the number of bedrooms						
Yes	N2. Home(s)/Development Located Near Transit						
	N2.2. Within 1/2 mile of a Major Transit Stop	2	2				
<b>N3. Pedestrian and Bicycle Access</b>							
Yes	N3.2 Connection to Pedestrian Pathways	1	1				
Yes	N3.3 Traffic Calming Strategies	2	2				
Yes	N3.5 Bicycle Storage for Residents	1	1				
1 space per unit	N3.7 Reduced Parking Capacity	2	2				
<b>N4. Outdoor Gathering Places</b>							
Yes	N4.1 Public or Semi-Public Outdoor Gathering Places for Residents	1	1				
Yes	N4.2 Public Outdoor Gathering Places with Direct Access to Tier 1 Community Services	1	1				
Yes	N9.2 Community Location	2	1		1		
<b>N11. Mixed-Use Developments</b>							
Yes	N11.2 At Least 2% of Development Floor Space Supports Mixed Use	1	1				
<b>O. OTHER</b>							
Yes	O1. GreenPoint Rated Checklist in Blueprints	Y	R	R	R	R	R
Yes	O2. Pre-Construction Kickoff Meeting with Rater and Subcontractors	2		0.5		1	0.5
Yes	O7. Green Appraisal Addendum	Y	R	R	R	R	R
Yes	O11. Tobacco Free Buildings	2			2		
<b>Summary</b>							
Total Available Points in Specific Categories		375.5	46	110.5	70	95	54
Minimum Points Required in Specific Categories		50	2	25	6	6	6
<b>Total Points Targeted</b>		<b>85</b>	<b>15</b>	<b>32.5</b>	<b>11</b>	<b>16</b>	<b>10.5</b>



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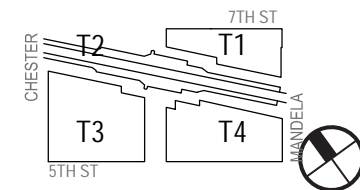
SHEET: GREENPOINTRATED  
A-01.05



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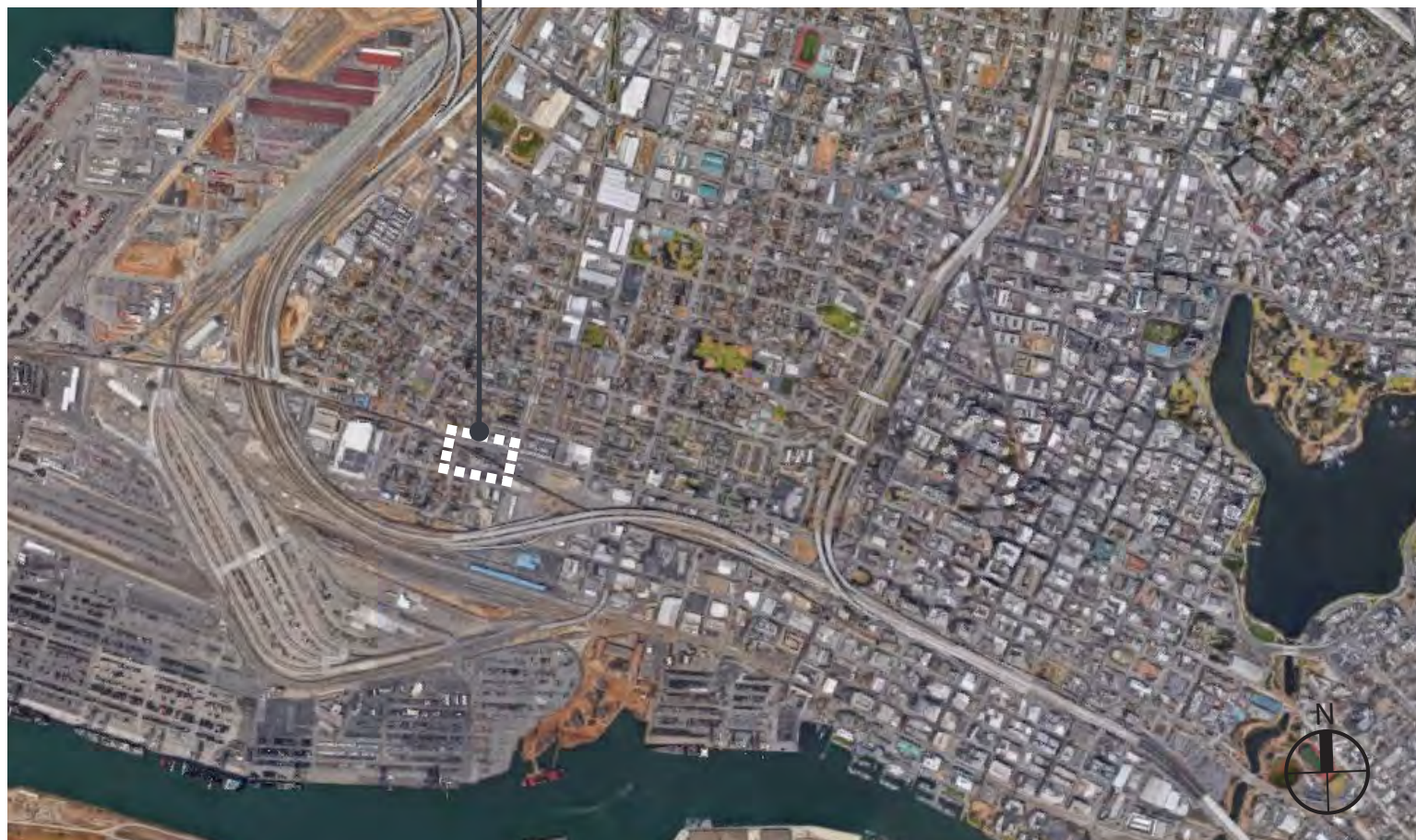
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SHEET: CONTEXT  
MAP  
A-01.06

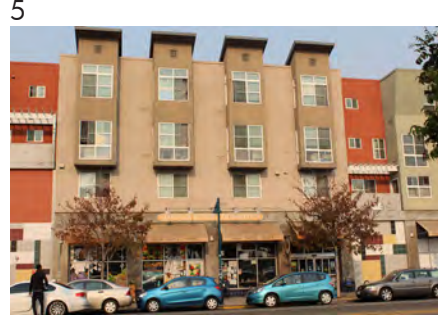
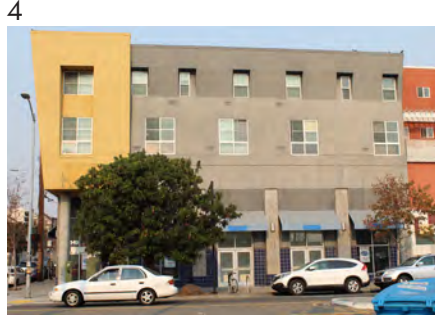
SITE LOCATION



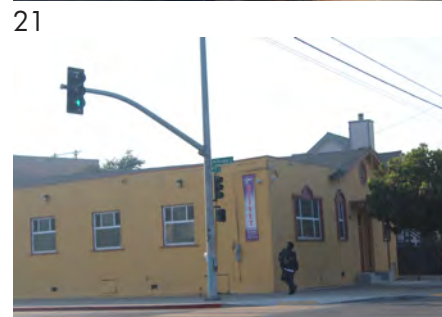
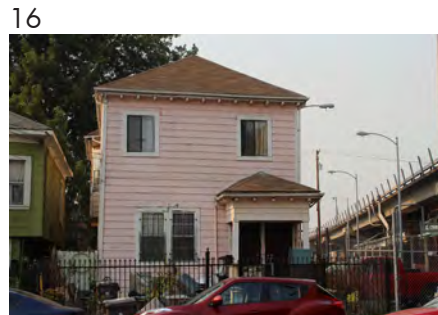
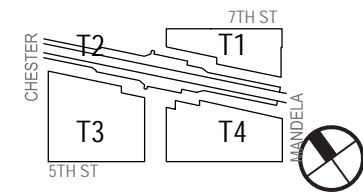
LOCATION OF PROJECT SITE WITHIN WEST OAKLAND NEIGHBORHOOD



5 ACRE PROJECT SITE



SITE



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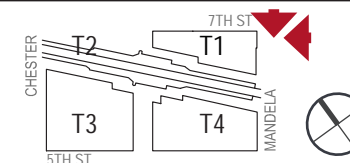
SHEET: CONTEXT  
PHOTOS  
A-01.07



CONTEXT VIEW FROM  
7TH STREET  
LOOKING WEST



CONTEXT VIEW FROM  
MANDELA PKWY  
LOOKING SOUTH

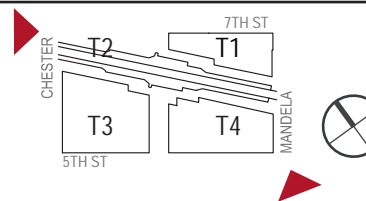


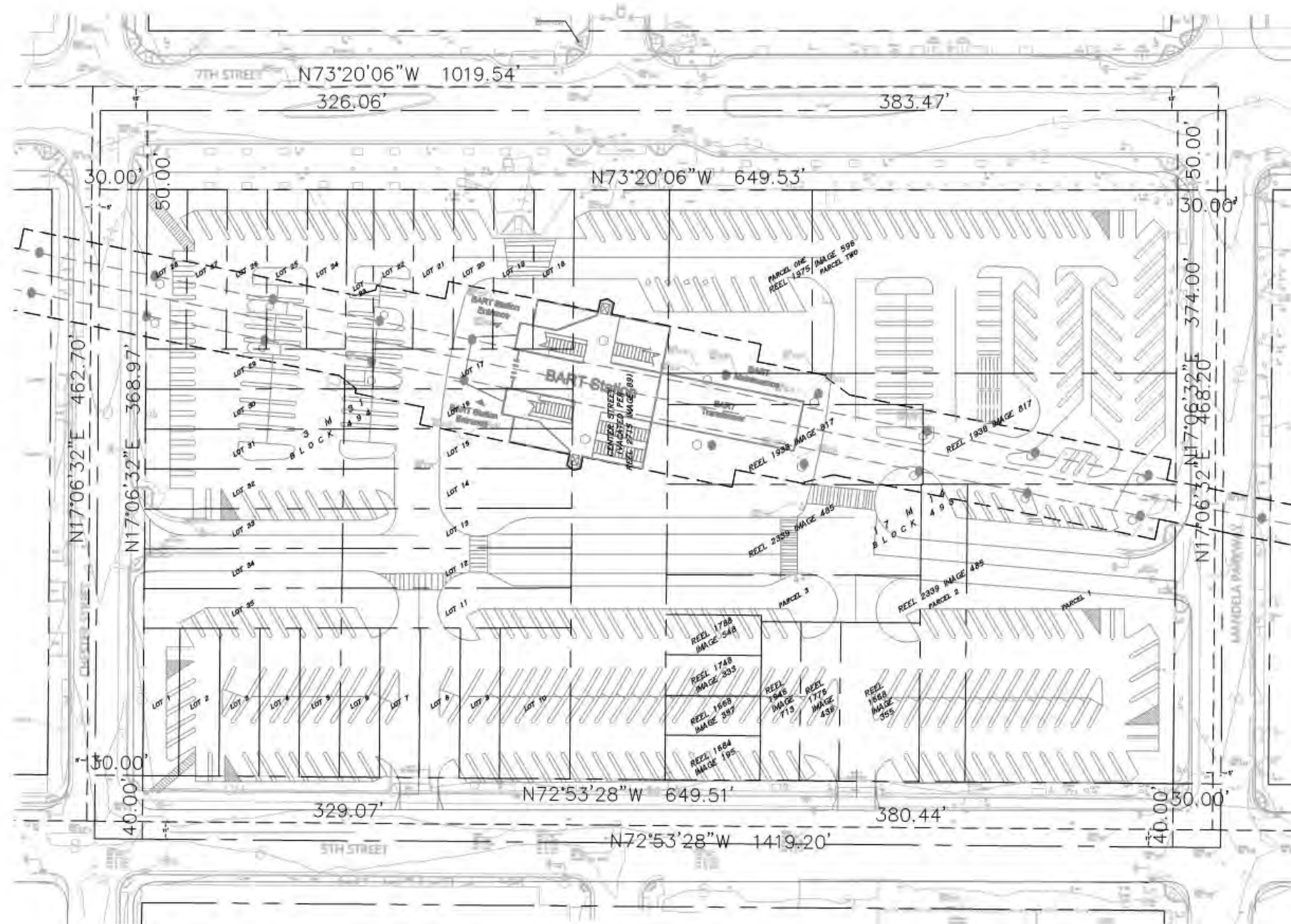


CONTEXT VIEW FROM  
I-880  
LOOKING NORTH

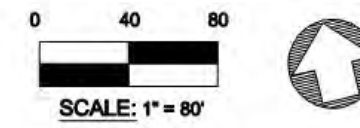


CONTEXT VIEW FROM  
7TH STREET  
LOOKING EAST





**NOTE:**  
 EXISTING BOUNDARY FROM RECORD INFORMATION FOR  
 PLANNING PURPOSES ONLY











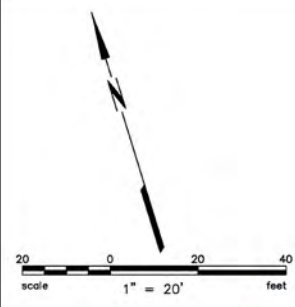


**SYMBOLS & LEGEND**

	FOUND CITY OF OAKLAND MONUMENT
	VALVE
	FIRE HYDRANT
	BACKFLOW PREVENTION DEVICE
	RISER
	SIGN
	STREET LIGHT (UNLESS NOTED OTHERWISE)
	LIGHT POLE (UNLESS NOTED OTHERWISE)
	GUY ANCHOR
	UTILITY POLE
	TELEPHONE MANHOLE
	MANHOLE
	SUBJECT PROPERTY
	ADJOINING LOT
	CENTERLINE
	SURVEY TIE
	STORM DRAIN
	SANITARY SEWER
	OVERHEAD UTILITY LINE
	CONCRETE
	DETECTABLE WARNING

**ABBREVIATIONS**

AC	ASPHALT CONCRETE
CA	CABLE
CB	CATCH BASIN
CO	CLEAN OUT
COMM	COMMUNICATION
CONC	CONCRETE
DI	DROP INLET
DWY	DRIVEWAY
E	ELECTRIC
EP	EDGE OF PAVEMENT
FL	SURFACE FLOWLINE
G	GAS
GYP	GUY POLE
INV	BOTTOM INSIDE OF PIPE
LG	LIP OF GUTTER
M	METER
MH	MANHOLE
OH	OVERHEAD UTILITY LINE
SD	STORM DRAIN
SL	STREETLIGHT
SS	SANITARY SEWER
TC	TOP FACE OF CURB
TEL	TELECOMMUNICATION LINE
TG	TOP OF GRATE
TS	TRAFFIC SIGNAL
TV	TELEVISION
UB	UTILITY BOX
V	VALVE
VL	VAULT
W	WATER
WM	WATER METER



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CALIFORNIA  
COUNTY OF ALAMEDA

TOPOGRAPHIC AND BOUNDARY SURVEY  
WEST OAKLAND BART

Date:	12/06/2018
Scale:	AS SHOWN
Design:	N/A
Drawn:	KS
Approved:	KS
Job No.:	180384

Revisions

No.	
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Drawing Number:  
**180384**

5 OF 5

DRAWING NAME: K:\2018\180384\180384\_001\180384\_001.dwg  
DATE: 12/06/2018 10:00:00 AM  
USER: JSM

# VESTING TENTATIVE PARCEL MAP NO. 10940

## FOR CONDOMINIUM PURPOSES

**SHEET INDEX**

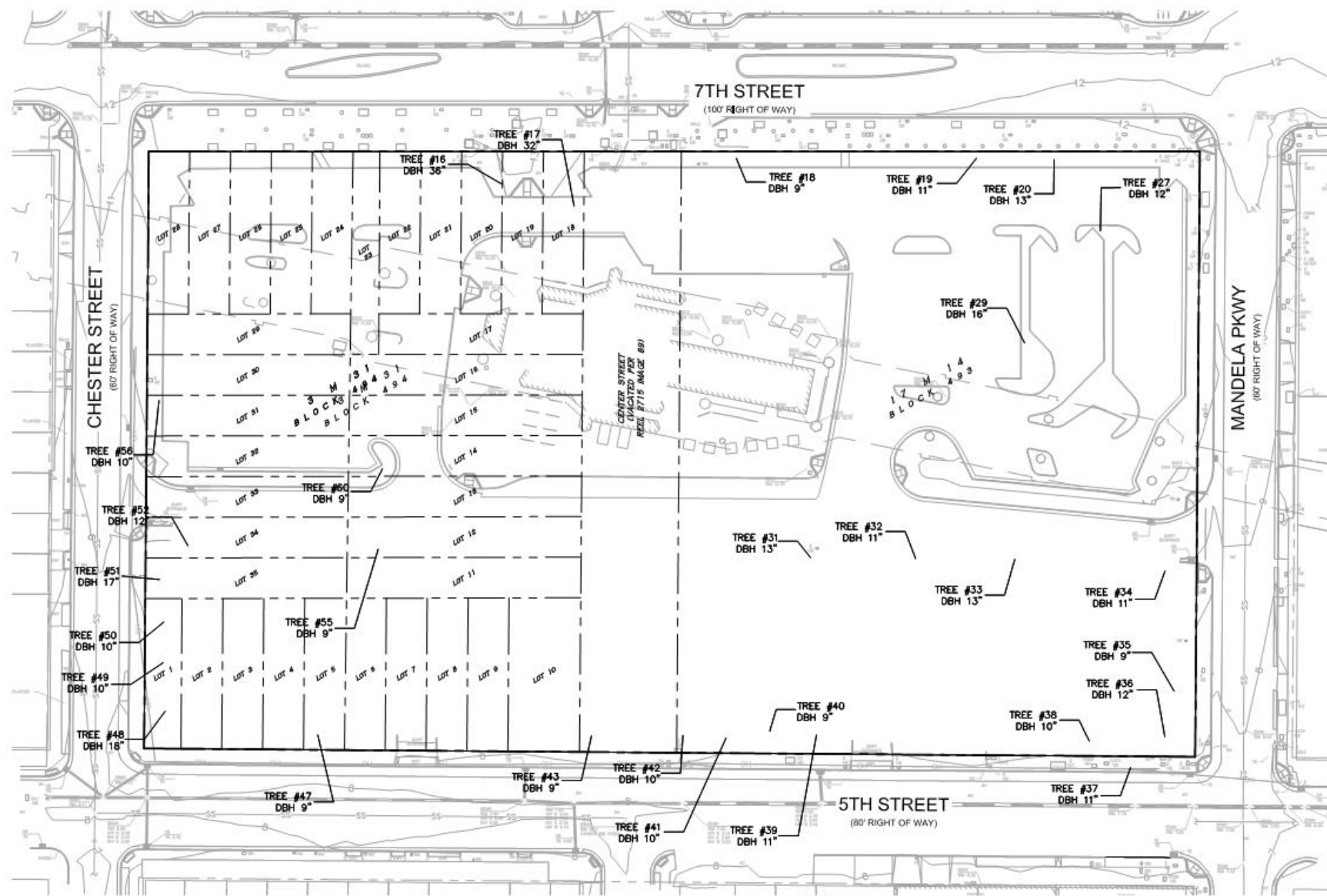
SHEET #	SHEET TITLE
1	TITLE SHEET/EXISTING SITE PLAN
2	PROPOSED SITE PLAN
3	EMERGENCY VEHICLE ACCESS EASEMENT

**SYMBOLS & LEGEND**

EXISTING	SYMBOL	DESCRIPTION
VALVE		VALVE
FIRE HYDRANT		FIRE HYDRANT
BACKFLOW PREVENTION DEVICE		BACKFLOW PREVENTION DEVICE
RISER		RISER
SIGN		SIGN
STREET LIGHT		STREET LIGHT
LIGHT POLE		LIGHT POLE
GUY ANCHOR		GUY ANCHOR
UTILITY POLE		UTILITY POLE
TELEPHONE MANHOLE		TELEPHONE MANHOLE
MANHOLE		MANHOLE
EXISTING PROPERTY LINE		EXISTING PROPERTY LINE
ADJOINING LOT CENTERLINE		ADJOINING LOT CENTERLINE
SURVEY TRIE		SURVEY TRIE
STORM DRAIN		STORM DRAIN
SANITARY SEWER		SANITARY SEWER
OVERHEAD UTILITY LINE		OVERHEAD UTILITY LINE
CONCRETE		CONCRETE
DETECTABLE WARNING		DETECTABLE WARNING
BOUNDARY LINE		BOUNDARY LINE

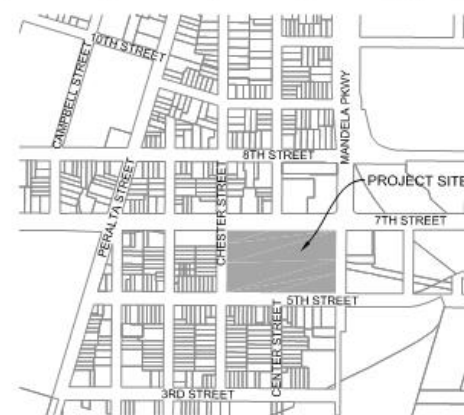
**ABBREVIATIONS**

AC	ASPHALT CONCRETE
CA	CABLE
CB	CATCH BASIN
CO	CLEAN OUT
COMM	COMMUNICATION
CONC	CONCRETE
DBH	DIAMETER AT BREAST HEIGHT
DI	DROP INLET
DWY	DRIVEWAY
E	ELECTRIC
EP	EDGE OF PAVEMENT
EVAE	EMERGENCY VEHICLE ACCESS EASEMENT
FL	SURFACE FLOWLINE
G	GAS
GYP	GUY POLE
INV	BOTTOM INSIDE OF PIPE
LG	LIP OF GUTTER
M	METER
MH	MANHOLE
MIN	MINIMUM
OH	OVERHEAD UTILITY LINE
PL	PROPERTY LINE
R	RADIUS
SD	STORM DRAIN
SF	SQUARE FEET
SL	STREETLIGHT
SS	SANITARY SEWER
(T)	TOTAL
TC	TOP FACE OF CURB
TEL	TELECOMMUNICATION LINE
TG	TOP OF GRATE
TV	TRAFFIC SIGNAL
TS	TELEVISION
UB	UTILITY BOX
V	VALVE
VT	VAULT
W	WATER
WM	WATER METER



Tree number	DBH (in)	Species Name
16	36	Pinus Pinea
17	32	Pinus Pinea
20	13	Liriodendron Tulipifera
28	12	Maytenus Boaria
29	16	Maytenus Boaria
31	13	Liriodendron Tulipifera
33	13	Liriodendron Tulipifera
36	12	Liriodendron Tulipifera
48	18	Platanus X Acerifolia
51	17	Platanus X Acerifolia
52	12	Liriodendron Tulipifera

EXISTING CONDITIONS  
SCALE 1"=40'



KEY MAP  
SCALE 1"=500'

**PROPERTY ADDRESS:**  
1451 7TH STREET  
OAKLAND, CA 94607

**OWNER AND SUBDIVIDER:**  
SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT  
300 LAKESIDE DRIVE, 22ND FLOOR  
OAKLAND, CA 94612  
PHONE: 510-597-6300

**BASIS OF BEARINGS**

THE BEARING OF NORTH 72°53'28" WEST BETWEEN FOUND MONUMENTS ON 5TH STREET BETWEEN HENRY STREET AND KIRKHAM STREET AS SHOWN ON THAT RECORD OF SURVEY, R/S 1687, FILED AUGUST 7, 2000 IN BOOK 25 OF RECORDS OF SURVEY AT PAGES 58-59, INCLUSIVE, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.

**BENCHMARK**

THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H," ELEVATION 10.784 FEET (DATUM = MEAN SEA LEVEL).

**NOTES**

- BASED ON INFORMATION CONTAINED IN THE PRELIMINARY TITLE REPORT, ORDER NUMBER NCS-891862-SC, DATED JANUARY 31, 2018, PROVIDED BY FIRST AMERICAN TITLE COMPANY, THE SUBJECT PROPERTY IS OWNED BY SFBART BEING ASSESSOR PARCEL NUMBERS 004-0077-003, 004-0071-003, AND THE VACATION OF CENTER STREET; THERE ARE 36 PARCELS, NOT INCLUDING THE VACATION OF CENTER STREET, THE EXISTING PARCELS WILL BE RECONFIGURED TO CREATE 3 LOTS AND ONE REMAINDER PARCEL VIA PARCEL MAP TO BE PROCESSED.
- THE INTENT OF THE MAP IS TO MERGE THE 36 PARCELS AND THE VACATED CENTER STREET INTO THE FOUR CONSOLIDATE PARCELS DEFINING THE KEY COMPONENTS OF THE NEW DEVELOPMENT; LOT 1 MARKET RATE RESIDENTIAL TOWER, LOT 2 OFFICE BUILDING, LOT 3 AFFORDABLE HOUSING, AND THE DESIGNATED REMAINDER PARCEL FOR THE EXISTING WEST OAKLAND BART STATION AND NEW ENTRY PLAZAS.
- THE CONTROL SURVEY WAS CONDUCTED IN NOVEMBER OF 2018.
- ALL UNITS ARE IN US SURVEY FEET AND DECIMALS THEREOF.
- THE TOPOGRAPHIC SURVEY IS BASED ON A FIELD SURVEY COMPLETED IN DECEMBER 2018.
- ALL BUILDINGS TO THE NORTH OF THE BART STATION SHALL BE 30' FROM THE BART PLATFORM, AND ALL BUILDINGS TO THE SOUTH OF THE BART STATION SHALL BE 33' FROM THE BART PLATFORM.
- ALL EMERGENCY VEHICLE ACCESS EASEMENTS SHALL BE 26' WIDE.
- LOTS 1, 2 AND 3 ARE ALLOWED UP TO FOUR COMMERCIAL CONDOMINIUMS ON EACH LOT.
- MULTIPLE MAPS MAY BE FILED FOR THIS PROJECT.

**SURVEYOR'S STATEMENT:**

THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE PROFESSIONAL LAND SURVEYOR'S ACT.

PAUL KITTRIDGE  
PROJECT MANAGER  
P.L.S. # 5790



DATE:

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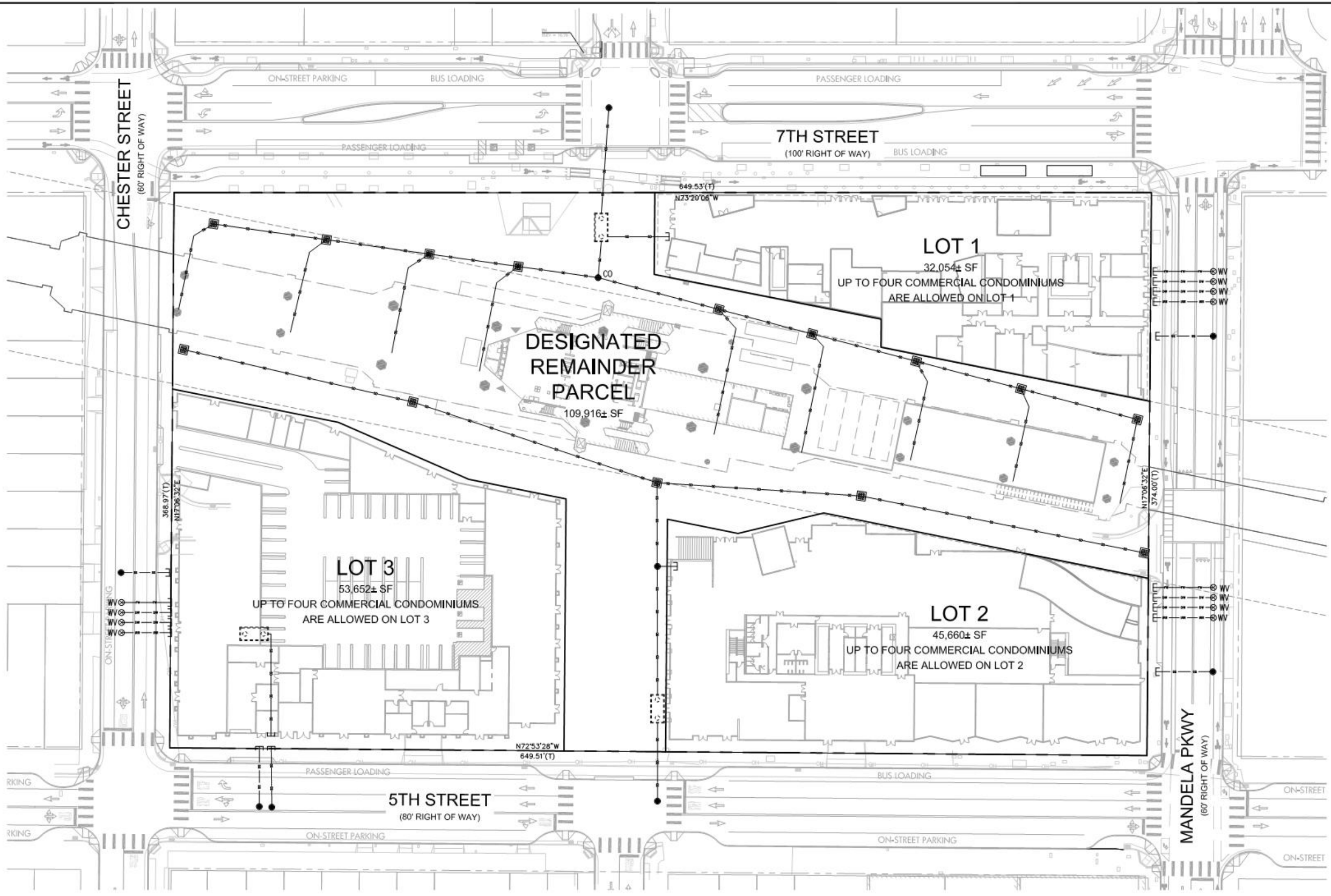
**VESTING TENTATIVE PARCEL MAP NO. 10940**  
**TITLE SHEET**  
 COUNTY OF ALAMEDA  
 CITY OF OAKLAND

DRAWING NAME: K:\2018\190384\_WestOakland\_Bart\_TOD\_Survey\ENG\TENTATIVE PARCEL MAP\SHEETS\01\WOBTMS.dwg  
 PLOT DATE: 04-06-20 PLOTTED BY: mitr

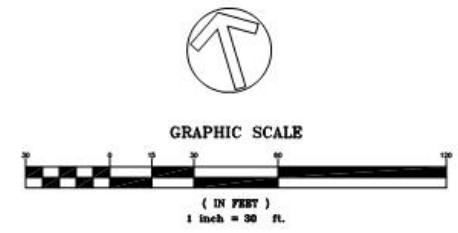
Revisions	No.	DATE	BY	DESCRIPTION
AS SHOWN		04/06/2020		
CC				
ES				

**TENTATIVE**  
**PARCEL MAP**  
**C-1.01A**

DRAWING NAME: K:\2018\190384\_WestOakland\_BART\_TOD\_Survey\ENG\TENTATIVE PARCEL MAP\SHEETS\02\0201MPL.dwg  
PLOT DATE: 04-06-20 PLOTTED BY: mitr



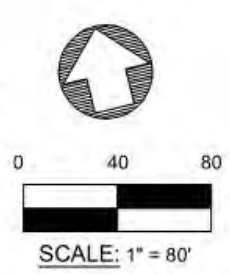
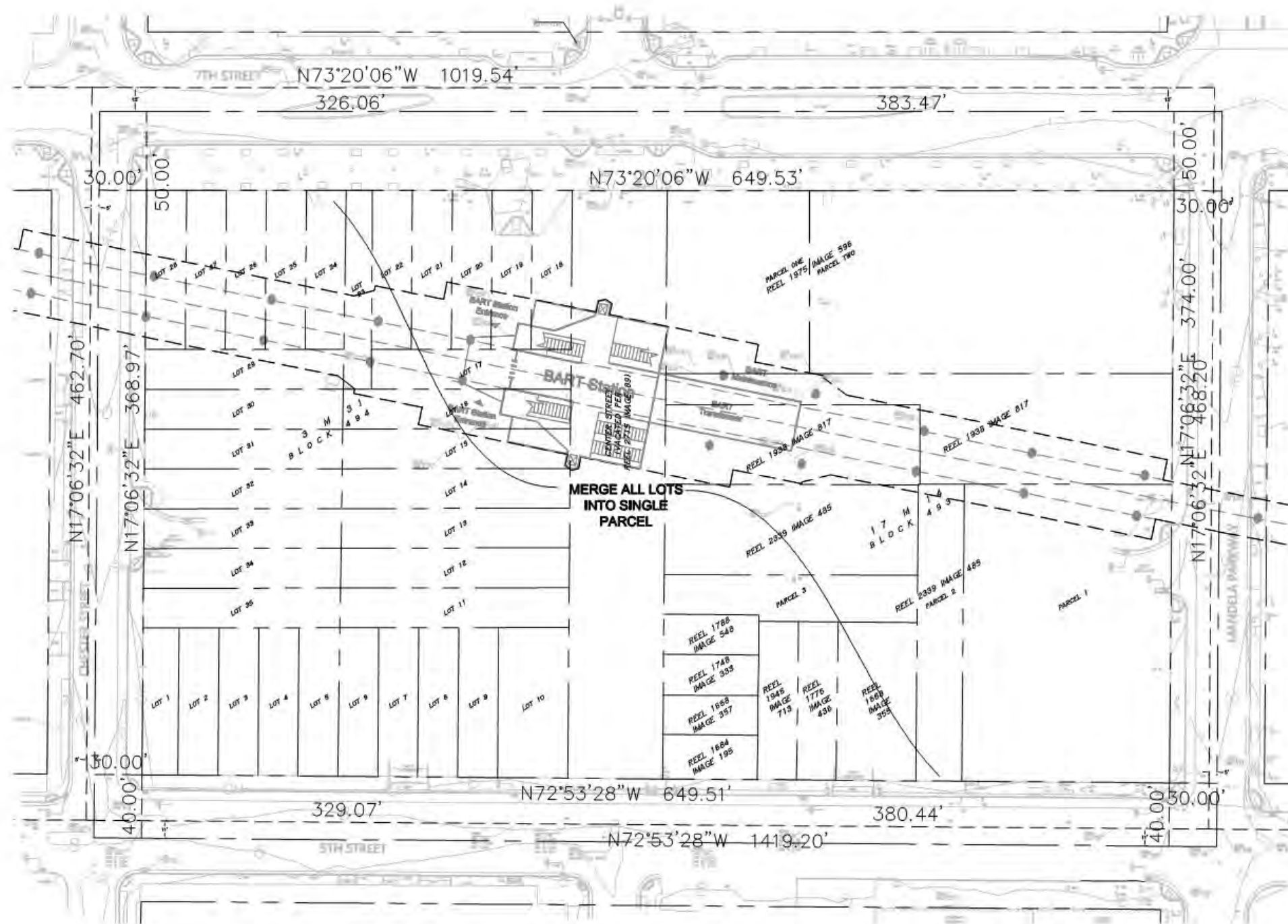
**LEGEND**  
BOUNDARY LINE  
PROPOSED PARCEL LINE

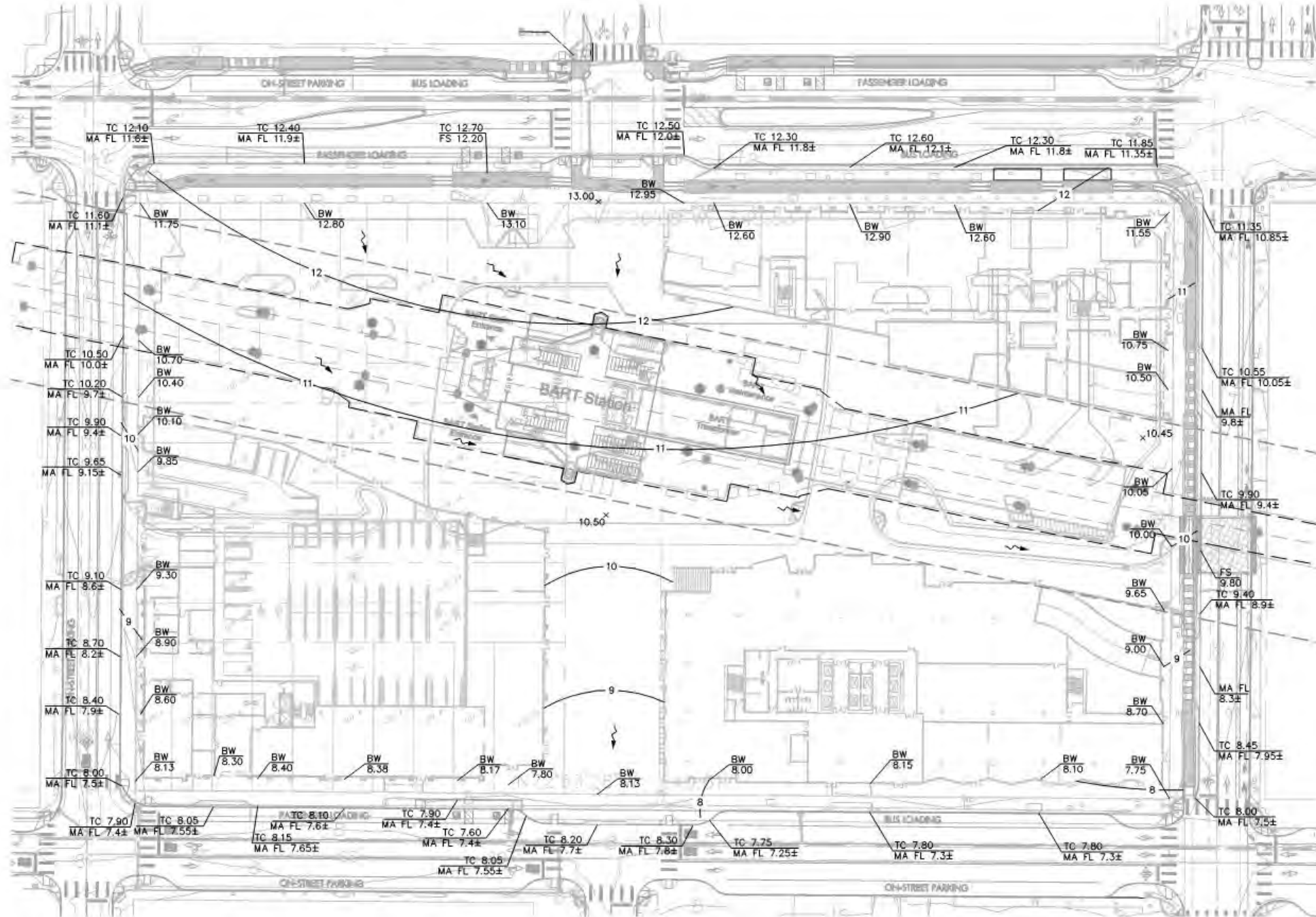


Revisions	No.	Date
AS SHOWN		04/06/2020
BY CC		
BY CC		
BY ES		
No. 20180384		









**ABBREVIATIONS:**

FF	FINISHED FLOOR
FG	FINISHED GROUND
FS	FINISHED SURFACE
FL	FLOW LINE
MA	MATCH
TC	TOP OF CURB
TS	TOP OF STEP
TW	TOP OF WALL

**LEGEND:**

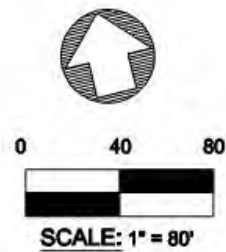
	DIRECTION OF PROPOSED DRAINAGE
	GRADE BREAK
	CONTOUR

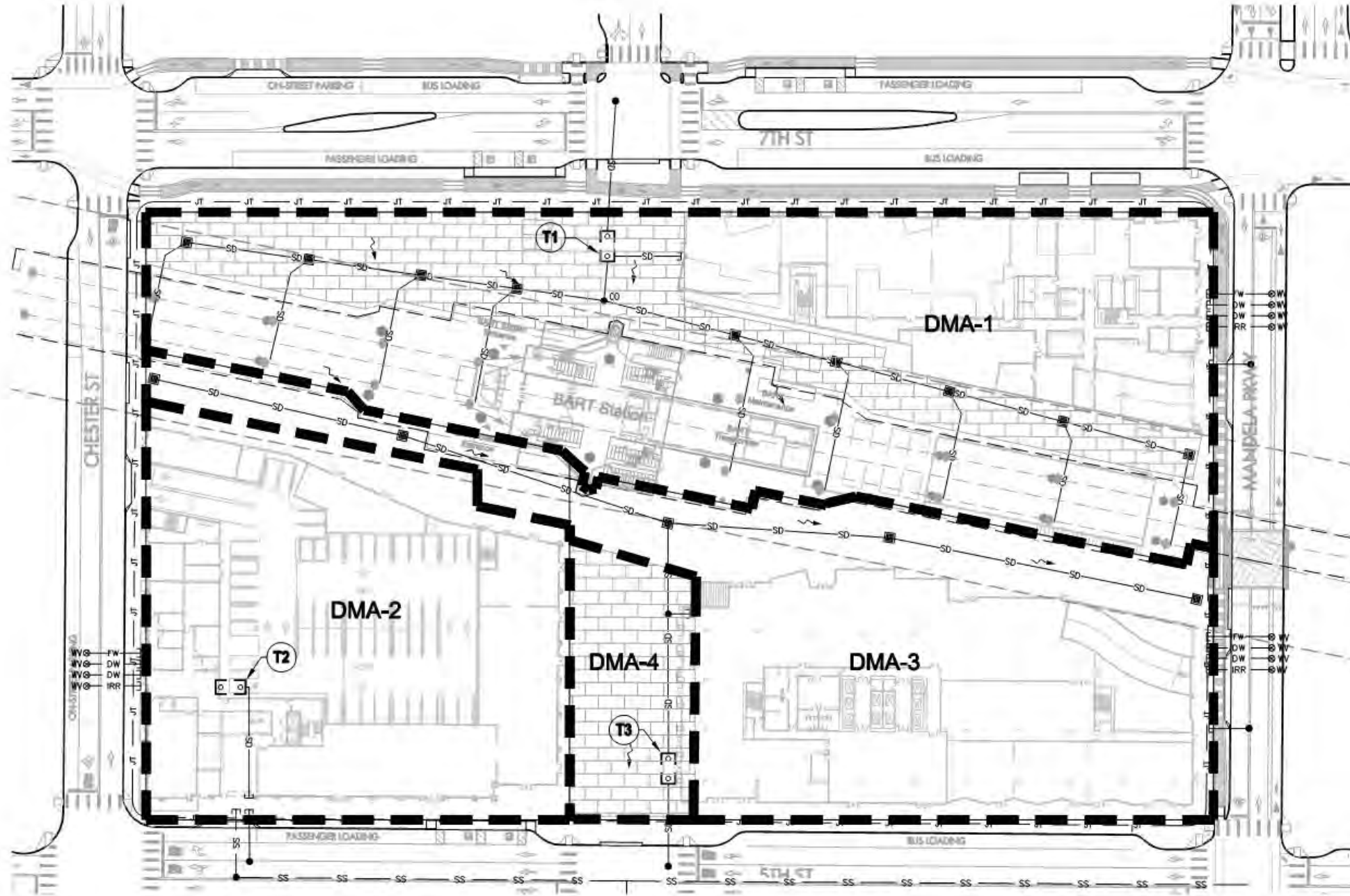
**GRADING NOTES:**

1. TC ELEVATIONS ARE 6" ABOVE ADJACENT PAVEMENT FL ELEVATIONS, UNLESS OTHERWISE NOTED ON PLAN.

**EARTHWORK SUMMARY:**

TOTAL EXCAVATION: 51,303 CUBIC YARDS  
 TOTAL FILL: 0 CUBIC YARDS  
 TOTAL OFFHAUL: 51,303 CUBIC YARDS



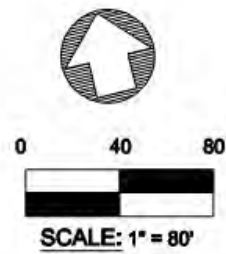


**LEGEND:**

- DRAINAGE MANAGEMENT AREA (DMA)
- PERMEABLE PAVING
- FILTER VAULT  
SEE SHEET C3.01
- TREATMENT AREA LABEL

**STORMWATER MANAGEMENT NOTES:**

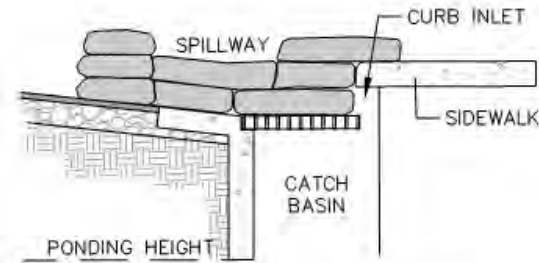
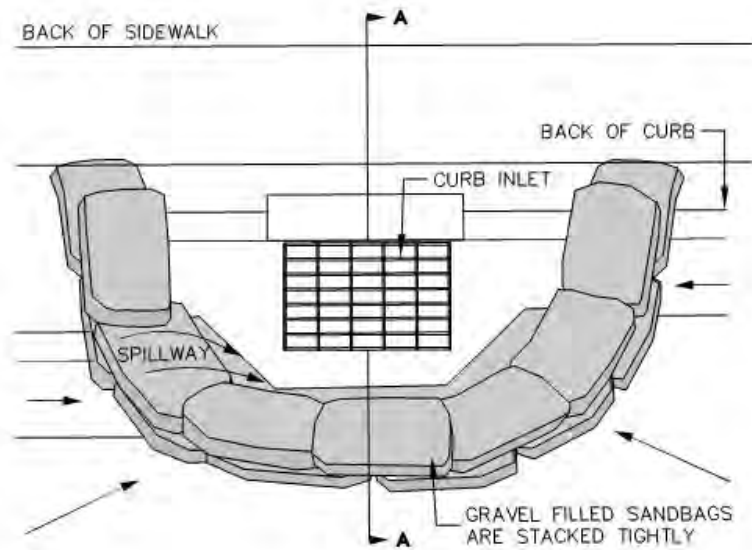
1. THE TREATMENT VAULTS WERE SELECTED BASED ON 243,131 SF OF IMPERVIOUS SURFACE AND A TREATMENT INTENSITY OF 0.2 IN/HR. SEE OLD CASTLE WASHINGTON CULD PF-V-8-WA-0001 PERK FILTER DETAIL ON SHEET C3.01. ALL FILTER VAULTS SHOWN ARE COMPRISED OF (31) - 18" CARTRIDGES.



TOTAL SITE AREA (SF)	TOTAL LAND AREA DISTURBED (SF)	TOTAL EXISTING/PRE-PROJECT IMPERVIOUS SURFACE (SF)	REPLACED IMPERVIOUS SURFACE (SF)	NEW IMPERVIOUS SURFACE (SF)	TOTAL POST-PROJECT IMPERVIOUS SURFACE (SF)
243,130	243,130	212,865	177,410	25,135	202,545

DMA	Impervious Area (SF)	Pervious Area (SF)	Treatment Type	Designated Treatment Area
1	74,600	28,445	Filter Vault	T1
2	58,278	0	Filter Vault	T2
3	69,667	0	Filter Vault	T3
4	0	12,140	Permeable Paving	T4



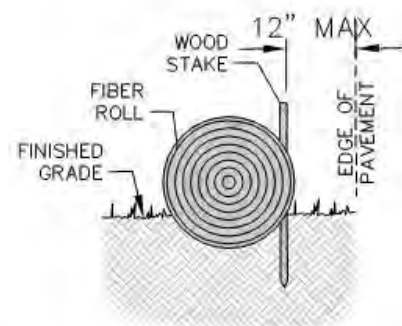


SECTION A-A

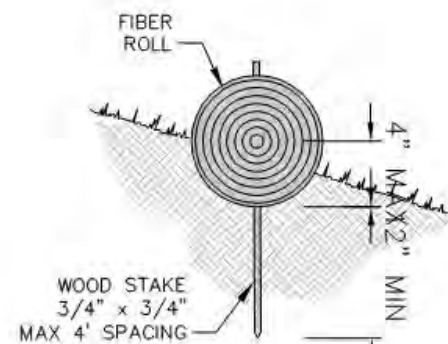
**NOTES:**

1. PLACE CURB TYPE SEDIMENT BARRIERS ON GENTLY SLOPING STREET SEGMENTS, WHERE WATER CAN POND AND ALLOW SEDIMENT TO SEPARATE FROM RUNOFF.
2. SANDBAGS OF EITHER BURLAP OR WOVEN 'GEOTEXTILE' FABRIC, ARE FILLED WITH GRAVEL LAYERED AND PACKED TIGHTLY.
3. LEAVE A ONE SANDBAG GAP IN THE TOP ROW TO PROVIDE A SPILLWAY FOR OVERFLOW.
4. INSPECT BARRIERS AND REMOVE SEDIMENT AFTER EACH STORM EVENT. SEDIMENT AND GRAVEL MUST BE REMOVED FROM THE TRAVELED WAY IMMEDIATELY.

**1 CURB INLET SEDIMENT BARRIER**  
NOT TO SCALE



ENTRENCHMENT DETAIL  
IN FLAT AREA

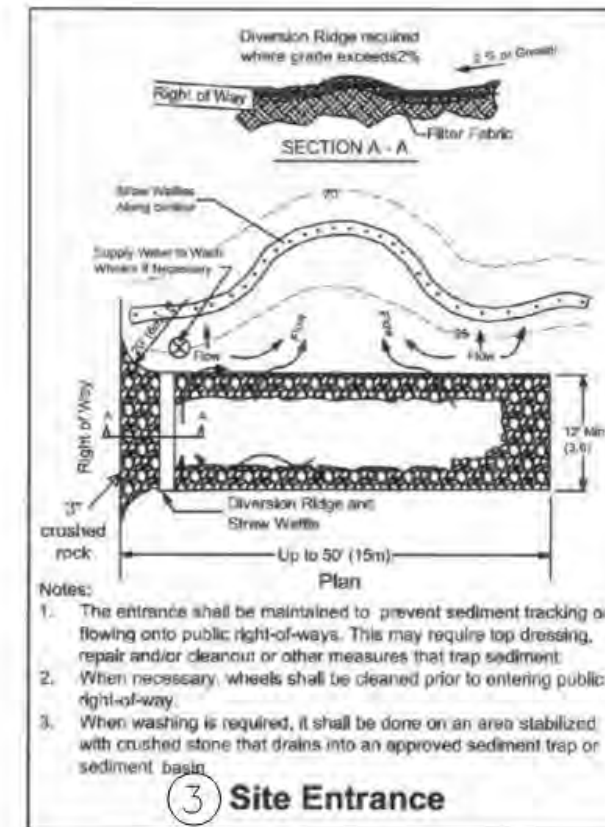


ENTRENCHMENT DETAIL  
IN SLOPED AREA

**NOTES:**

1. FIBER ROLLS ARE TUBES MADE FROM POROUS BIODEGRADABLE FIBER STUFFED IN A PHOTO-DEGRADABLE OPEN WEAVE NETTING. THEY ARE APPROXIMATELY 8" DIAMETER.
2. FIBER ROLL INSTALLATION REQUIRES THE PLACEMENT AND SECURE STAKING OF THE ROLL IN A TRENCH; 2"-4" DEEP, DUG ON CONTOUR. RUNOFF MUST NOT BE ALLOWED TO RUN UNDER OR AROUND ROLL. ROLLS SHOULD BE ABUTTED SECURELY TO PROVIDE A TIGHT JOINT, NOT OVERLAPPED.

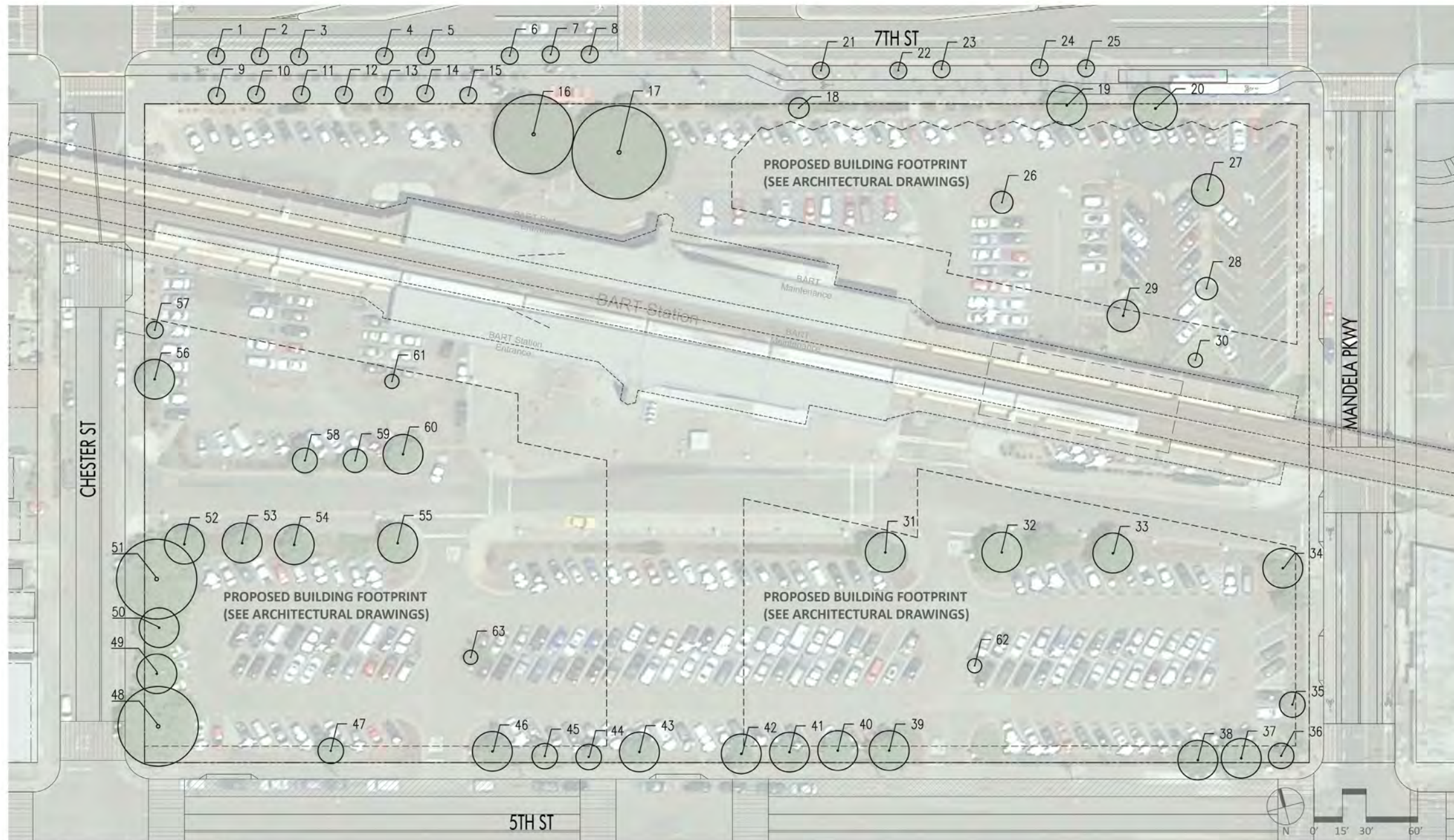
**2 FIBER ROLL**  
NOT TO SCALE



**Notes:**

1. The entrance shall be maintained to prevent sediment tracking or flowing onto public right-of-ways. This may require top dressing, repair and/or cleanout or other measures that trap sediment.
2. When necessary, wheels shall be cleaned prior to entering public right-of-way.
3. When washing is required, it shall be done on an area stabilized with crushed stone that drains into an approved sediment trap or sediment basin.

**3 Site Entrance**



**Trees Proposed for Removal**

Key	Species	DBH (in)
1	Pistacia chinensis	4
2	Pistacia chinensis	4
3	Pistacia chinensis	4
4	Pistacia chinensis	3
5	Pistacia chinensis	5
6	Pistacia chinensis	4
7	Pistacia chinensis	4
8	Pistacia chinensis	4
9	Pistacia chinensis	4
10	Pistacia chinensis	4
11	Pistacia chinensis	4
12	Pistacia chinensis	4
13	Pistacia chinensis	4

Key	Species	DBH (in)
14	Pistacia chinensis	4
15	Pistacia chinensis	4
16	Pinus pinea	36
17	Pinus pinea	32
18	Liriodendron tulipifera	9
19	Liriodendron tulipifera	11
20	Liriodendron tulipifera	13
21	Pistacia chinensis	4
22	Pistacia chinensis	4
23	Pistacia chinensis	4
24	Pistacia chinensis	4
25	Pistacia chinensis	5
26	Maytenus boaria	7

Key	Species	DBH (in)
27	Maytenus boaria	12
28	Maytenus boaria	5
29	Maytenus boaria	16
30	Maytenus boaria	2
31	Liriodendron tulipifera	13
32	Liriodendron tulipifera	11
33	Liriodendron tulipifera	13
34	Liriodendron tulipifera	11
35	Liriodendron tulipifera	9
36	Liriodendron tulipifera	12
37	Liriodendron tulipifera	11
38	Liriodendron tulipifera	10
39	Liriodendron tulipifera	11

Key	Species	DBH (in)
40	Liriodendron tulipifera	9
41	Liriodendron tulipifera	10
42	Liriodendron tulipifera	10
43	Liriodendron tulipifera	9
44	Liriodendron tulipifera	7
45	Liriodendron tulipifera	8
46	Liriodendron tulipifera	8
47	Liriodendron tulipifera	9
48	Platanus X acerifolia	18
49	Platanus X acerifolia	10
50	Platanus X acerifolia	10
51	Platanus X acerifolia	17
52	Liriodendron tulipifera	12

Key	Species	DBH (in)
53	Liriodendron tulipifera	8
54	Liriodendron tulipifera	8
55	Liriodendron tulipifera	9
56	Liriodendron tulipifera	10
57	Liriodendron tulipifera	5
58	Liriodendron tulipifera	8
59	Liriodendron tulipifera	7
60	Liriodendron tulipifera	9
61	Maytenus boaria	5
62	Maytenus boaria	5
63	Maytenus boaria	7

**Bolded trees are protected trees.**

BLUES WALK OF FAME – RELOCATED PLAQUES INTEGRATED INTO SIDEWALK PAVING

EVA ROUTE MARKED W/ COLORED CONCRETE EDGE BANDS

EVENT SPACE BELOW ELEVATED TRACKS W/ ADJUSTABLE LIGHTING

CONCRETE PLANTERS W/ CONTINUOUS INTEGRAL BENCH SEATING TO PROTECT PLANTS

STAGE AREA WITH COLORED CONCRETE AND STRIPED MAZE

COLORED CONCRETE PAVING W/ SAWCUT BANDING

16' HIGH LIGHT POST, TYP.

ACER RUB 'ARMSTRONG' MAPLE, 3'X3' TREE WELLS WITH ADJACENT STRUCTURAL SOIL BELOW SIDEWALK.

CONCRETE PAVING W/ SAWCUT BANDING

QUERCUS COCCINEA, SCARLET OAK, 6'X6' TREE WELLS WITH ADJACENT STRUCTURAL SOIL BELOW SIDEWALK.

THE PLAZA IS DESIGNED TO SUPPORT AND BALANCE THE NEEDS OF FUTURE USERS. THESE PATRONS WILL VARY FROM COMMUTERS TO THOSE COMING TO ATTEND LARGE EVENTS OR SIMPLY TO VISIT THE UNIQUE MARKETS. VARIOUS SIZED GATHERING SPACES ACCOMMODATE DIVERSE USES.

**IRRIGATION METHOD & WATER EFFICIENT LANDSCAPE ORDINANCE**

- IRRIGATION SYSTEM WILL BE DESIGNED TO PROVIDE THE MINIMUM AMOUNT OF WATER NECESSARY TO SUSTAIN GOOD PLANT HEALTH. ALL SELECTED COMPONENTS TO BE COMMERCIAL GRADE, SELECTED FOR DURABILITY, VANDAL RESISTANCE AND MINIMUM MAINTENANCE REQUIREMENT.
- THE SYSTEM WILL BE DRIPLINE IRRIGATION AS APPROPRIATE TO PLANT TYPE, EXPOSURE AND SLOPE CONDITIONS.
- CONTROL OF THE SYSTEM WILL BE VIA A WEATHER-ENABLED CONTROLLER CAPABLE OF DAILY SELF-ADJUSTMENT BASED ON REAL-TIME WEATHER CONDITIONS AS MEASURED BY AN ON-SITE WEATHER SENSOR.
- THE SYSTEM WILL INCLUDE A MASTER CONTROL VALVE AND FLOW SENSING CAPABILITY WHICH WILL SHUT DOWN ALL OR PART OF THE SYSTEM IF LEAKS ARE DETECTED.
- THE LANDSCAPE IS OVER 500 SQUARE FEET OF NEW LANDSCAPING AND WILL THEREFORE MEET THE REQUIREMENTS OF THE WATER EFFICIENCY LANDSCAPE ORDINANCE (WELO): [HTTPS://WATER.CA.GOV/LEGACYFILES/WATERUSEEFFICIENCY/LANDSCAPEORDINANCE/DOCS/TITLE%2023%20EXTRACT%20-%2020OFFICIAL%20CR%20PAGES.PDF](https://water.ca.gov/legacyfiles/wateruseefficiency/landscapeordinance/docs/title%2023%20extract%20-%2020OFFICIAL%20CR%20PAGES.PDF)

**WEST OAKLAND STATION CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN STRATEGIES**

- AREA LIGHTING:**
- ALL LOADING, UNLOADING, KISS AND RIDE, DROP OFF, PICK UP AREAS WILL BE LIT TO 5 CANDLE FOOT. SIDEWALKS AROUND THE SITE WILL BE LIT TO 1.25 CANDLE FOOT (CITY STANDARD), BIKE LOCKER AREAS WILL BE LIT TO 10 CANDLE FOOT, AND EXTERIOR PATRON WAITING AREAS OUTSIDE THE BART FAIR GATES WILL BE LIT TO 10 CANDLE FOOT. SEE SHEET L1.07 FOR MORE DETAIL REGARDING THE LOCATION OF THESE AREAS.
  - POLE LIGHTS ARE KEPT 20' CLEAR OF TREES AND LANDSCAPING. LIGHT POLES ARE LESS THAN 22' HIGH. OTHER LIGHTS WILL BE MOUNTED ON BUILDING, ON BOLLARDS OR ALONG OVERHEAD WIRES (<20' HIGH).
  - THE LIGHTING WILL USE A FULL COLOR INDEX IN ORDER TO BETTER DISTINGUISH FORM AND MOVEMENT.
- AREA PLANTING:**
- PLANTINGS, INCLUDING THE RAISED PLANTERS AND BERMS THEY ARE IN, WILL NOT EXCEED 36" IN HEIGHT. TREES WILL BE LIMBED UP TO 7' HIGH. THE INTENTION IS TO AVOID BLIND SPOTS CAUSED BY HIGH PLANTS AND PLANTERS. TREES WILL BE THE TYPE THAT RETAIN A THIN TRUNK AS THEY GROW AND MATURE. THE TREE CANOPIES WILL NOT BE OVERLY DENSE AND WILL ALLOW LIGHT IN FROM ABOVE.

- AREA SITE FURNISHINGS:**
- SITE FURNISHINGS, EARTH FORMS, AND ARTWORK WILL BE DESIGNED NOT TO OBSCURE VISIBILITY. BENCHES HAVE SEPARATORS/ARMRESTS, SO THEY ARE USED FOR SITTING ONLY. PLANTER BOXES AND LANDSCAPE WALLS WILL HAVE GROOVES OR ELEMENTS TO DETER SKATEBOARDERS. FENCES ARE DESIGNED TO RESIST TEMPERING AND CLIMBING.
- AREA SIGNAGE:**
- DIRECTIONAL AND WAYFINDING SIGNAGE WILL BE PLACED AT APPROPRIATE LOCATIONS TO HELP DIRECT THE VISITOR TO THEIR DESTINATION.
- SURVEILLANCE CAMERAS:**
- A SERIES OF SURVEILLANCE CAMERAS WILL BE PLACED TO COVER THE ENTIRE SITE. THESE CAMERAS WILL COMPLY WITH BART FACILITY STANDARDS.



OWNER  
 HENSEL PHELPS  
 suda  
 BUILDING T1 FDP PACKAGE  
 PROJECT

**WEST OAKLAND BART**  
 1451 7th St Oakland, CA 94607

ARCHITECT  
 jrdv ARCHITECTS  
 PROJECT TEAM

BKF ENGINEERS  
 1646 N CALIFORNIA BLVD, SUITE 400  
 WALNUT CREEK CA 94596  
 (925) 940-2200

PGA DESIGN  
 444 17th Street  
 Oakland CA 94612  
 (510) 465-1284

STOK  
 948 Front Street  
 San Francisco CA 94111  
 (415) 329-7100

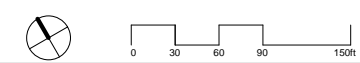
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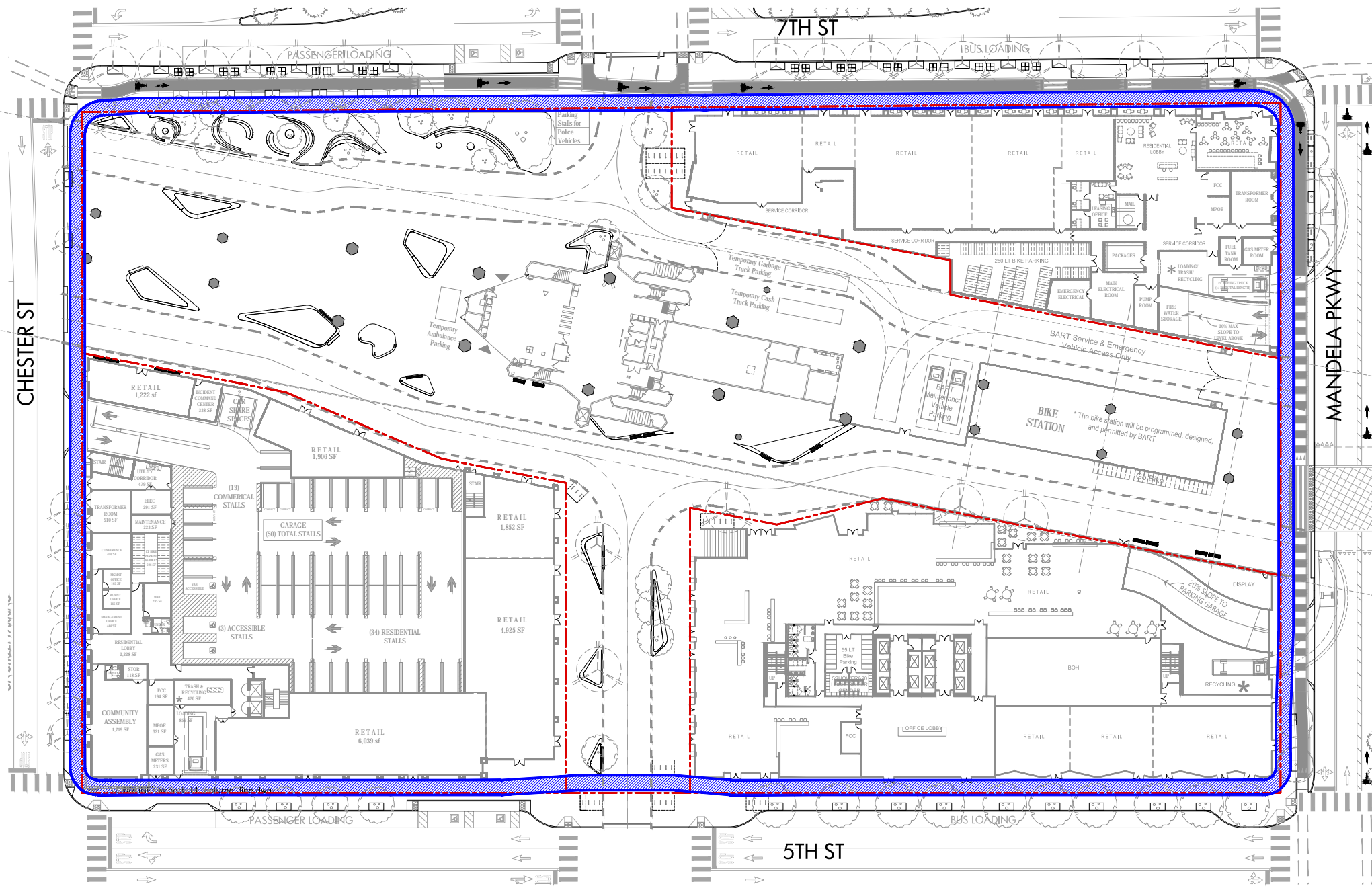
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PROJ # 168-153 WO BART  
 DATE 03/26/20  
 SCALE 1"=60' - 0"

**LAYOUT & MATERIALS PLAN**

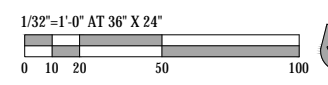
SHEET: **L1.01**



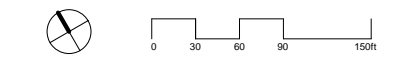
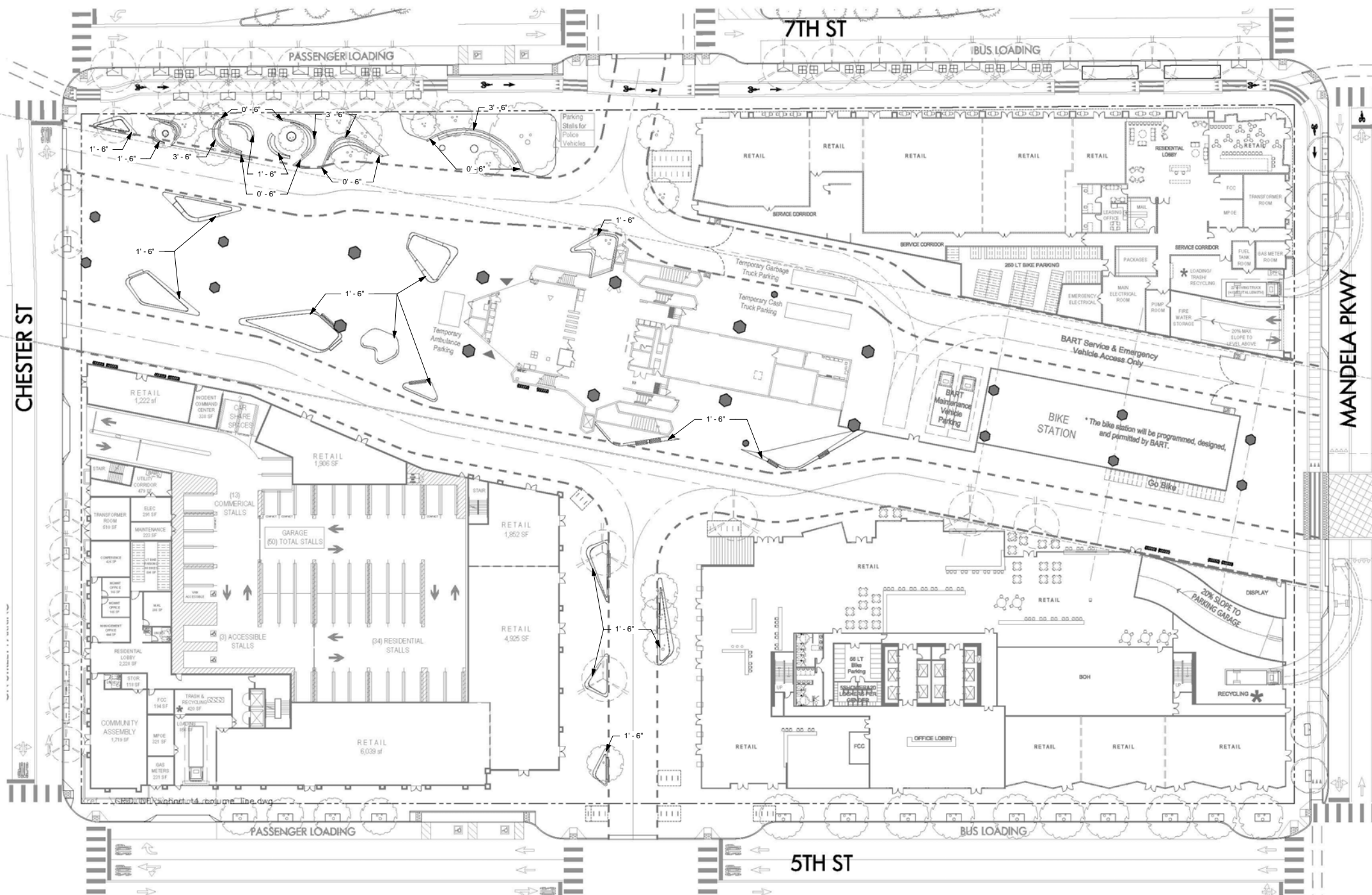


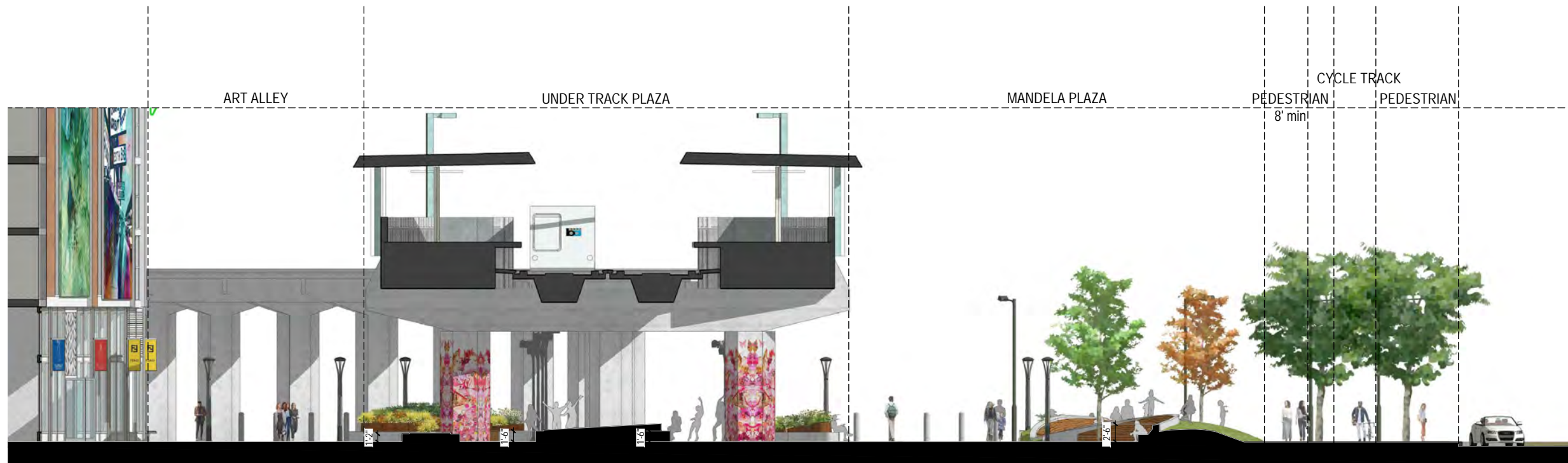
LEGEND

- 8' CLEAR ZONE FOR PEDESTRIAN
- PROPERTY LINE

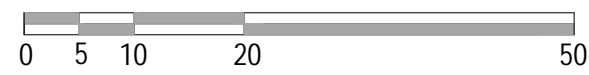






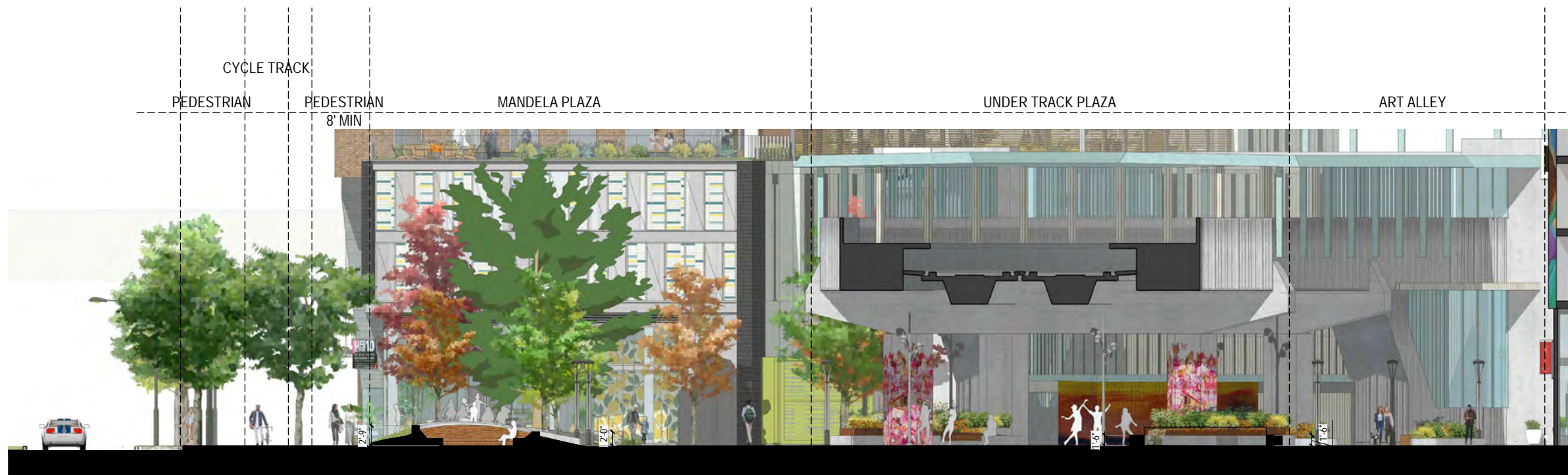


1 SECTION 1  
L1.03 1/16" = 1'-0"

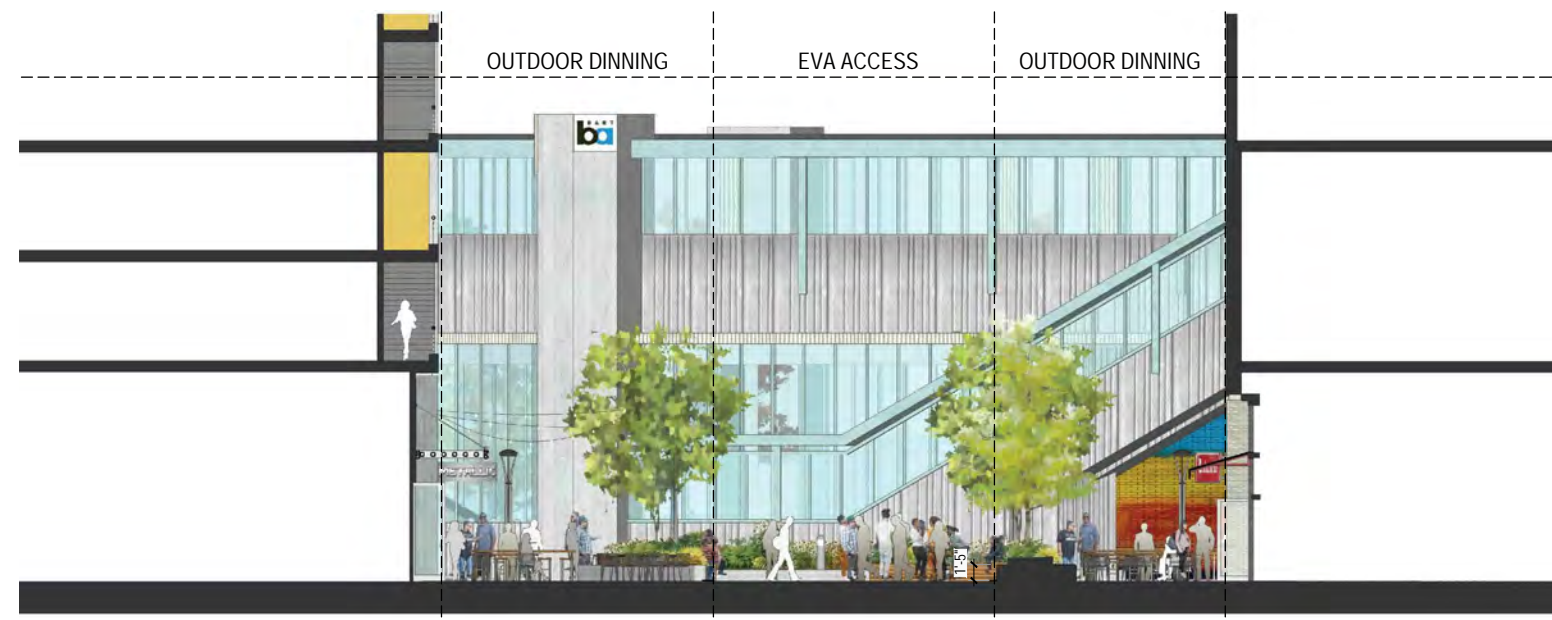
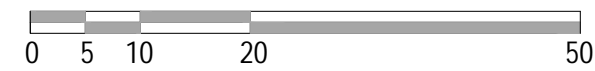


2 SECTION 2  
L1.03 1/16" = 1'-0"

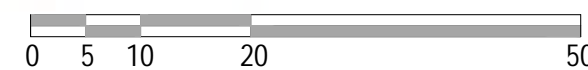


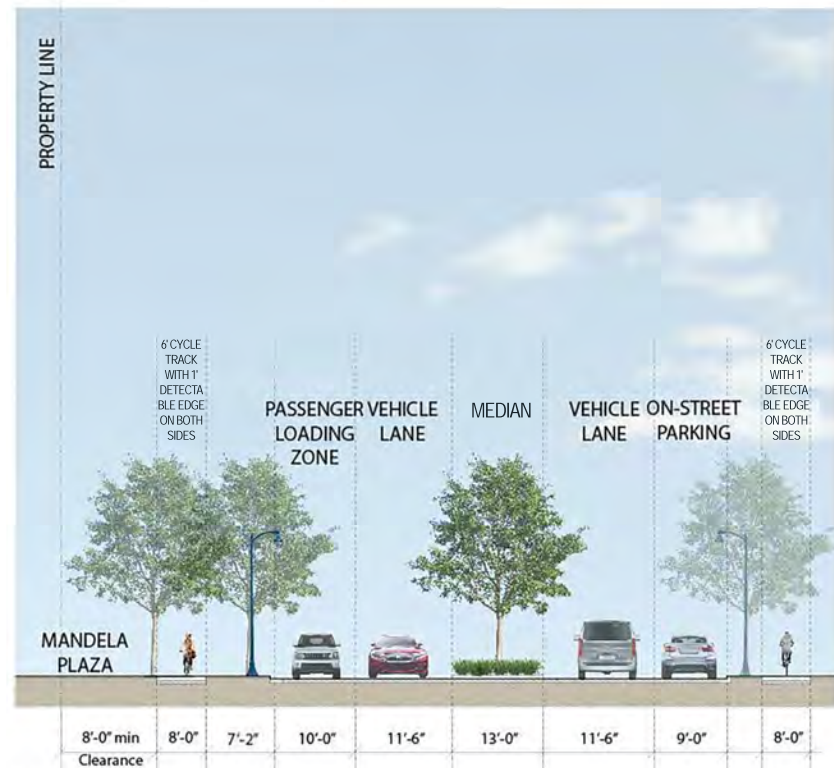


1 SECTION 3  
L1.04 1/16" = 1'-0"

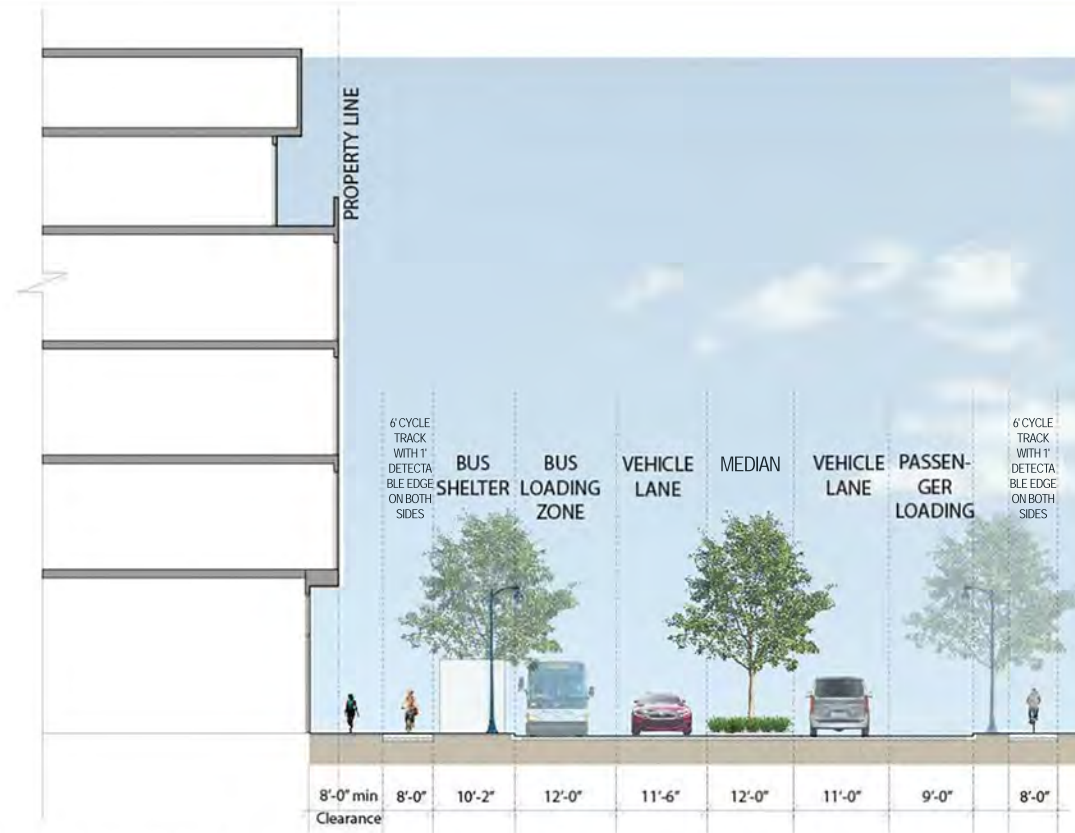


2 SECTION 4  
L1.04 1/16" = 1'-0"

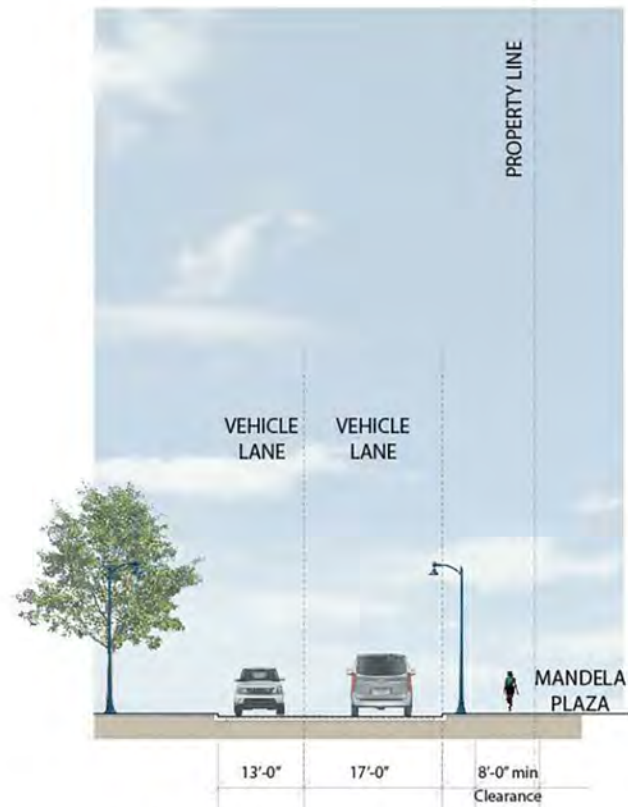




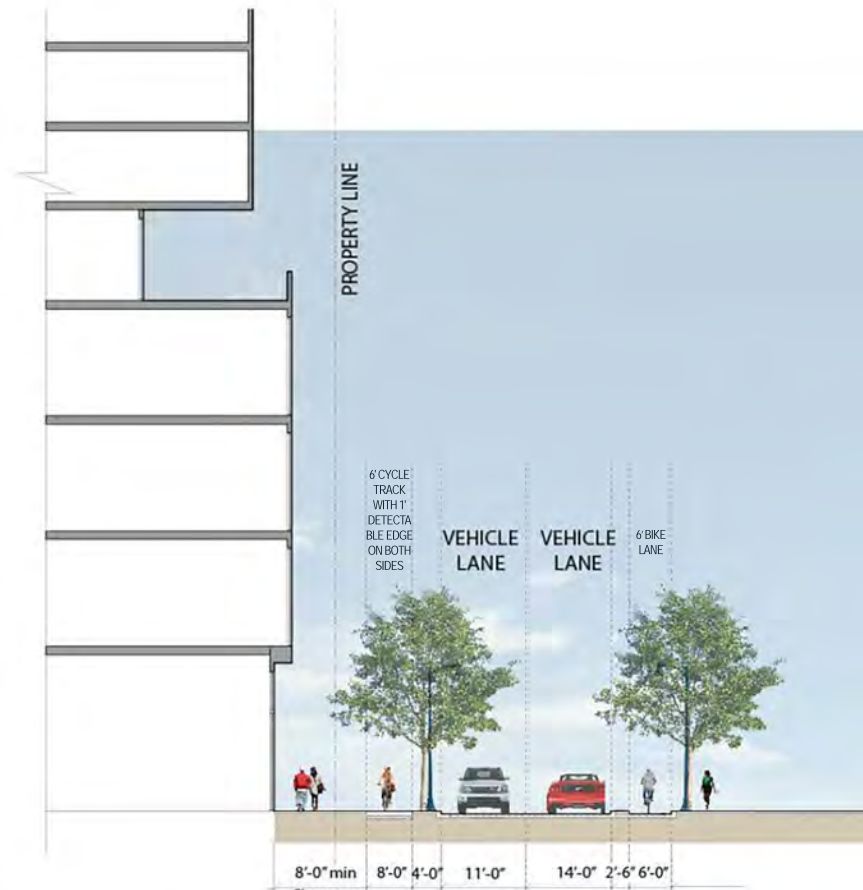
1 7TH STREET WEST SECTION  
1" = 20'-0"



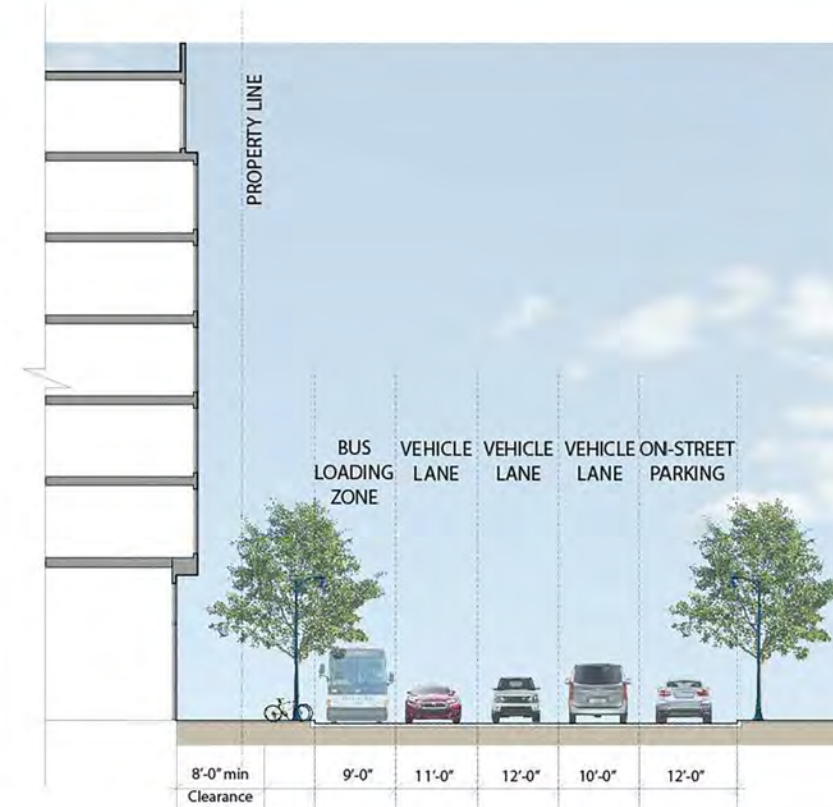
2 7TH STREET EAST SECTION  
1" = 20'-0"



3 CHESTER STREET SECTION  
1" = 20'-0"

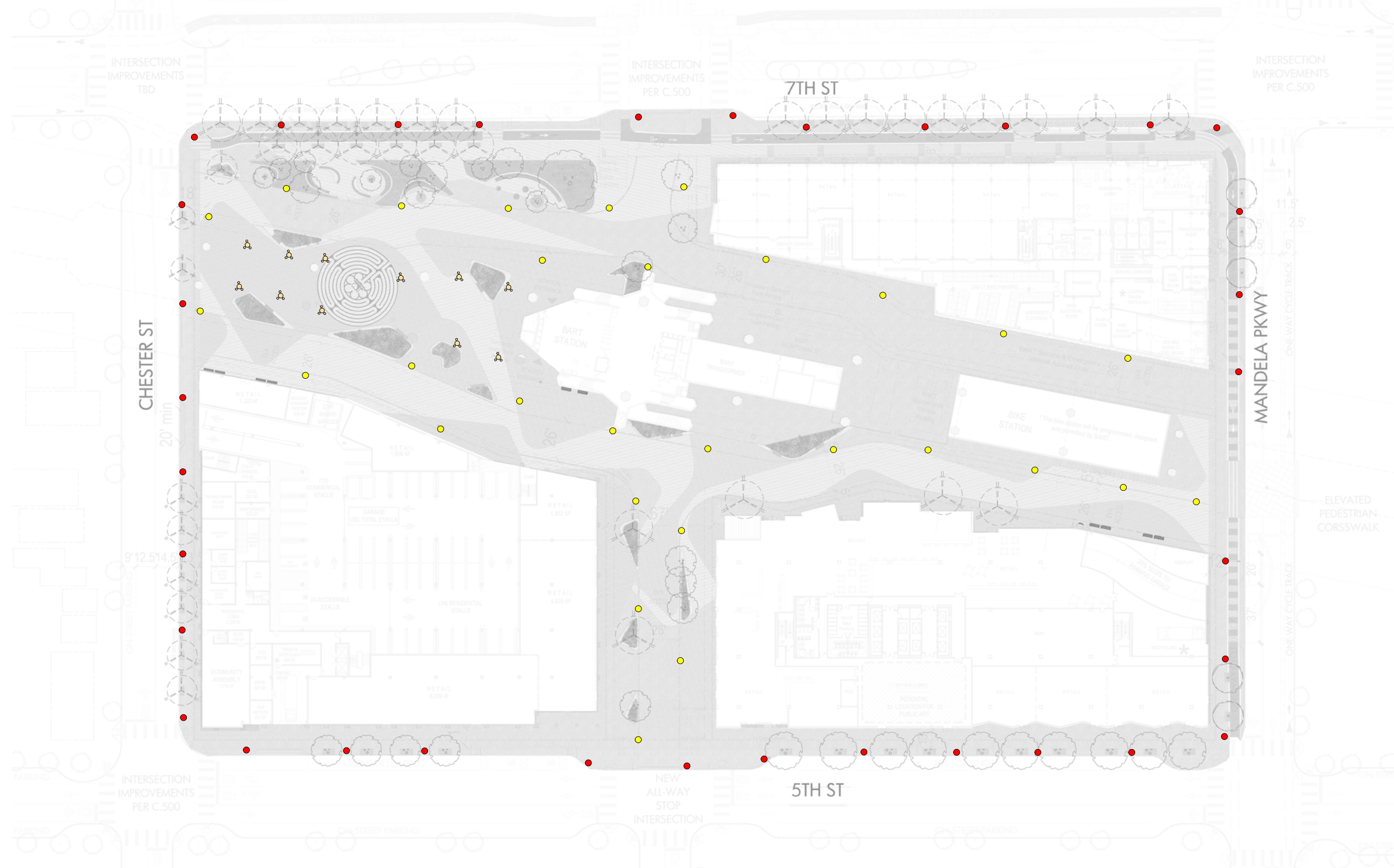


4 MANDELA PKWY SECTION  
1" = 20'-0"



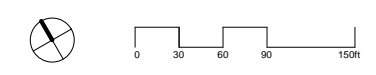
5 5TH STREET SECTION  
1" = 20'-0"

OWNER  
 HENSEL PHELPS  
 suda  
 BUILDING T1 FDP PACKAGE  
 PROJECT  
**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607  
 ARCHITECT  
 jrdv ARCHITECTS  
 PROJECT TEAM  
 BKF100  
 stok  
 ST/HP  
 KEY  
 PROJ # 188-153 W/O BART  
 DATE 06/04/20  
 SCALE 1" = 20'-0"  
 TITLE **STREET SECTIONS**  
 SHEET: **L1.05**



**LEGEND - LIGHTING PLAN**

- AREA LIGHTS FOR PLAZA
- AREA LIGHTS WITH MULTIPLE HEADS FOR EVENT PLAZA
- STREET LIGHTS AS CITY REQUIRED



OWNER  
 HENSEL PHELPS  
 suda  
**BUILDING T1 FDP PACKAGE**  
 PROJECT  
**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607

ARCHITECT  
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 510.300.4300  
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 1646 N CALIFORNIA  
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 CA 94596  
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 444 17th Street  
 Oakland  
 CA 94612  
 (510) 485-1284  
 STOK  
 1458 Front Street  
 San Francisco  
 CA 94111  
 (415) 328-7100

STAMP

KEY

PROJ #: 168-153 WO BART  
 DATE: 03/26/20  
 SCALE: 1"=60' - 0"  
 TITLE: **LIGHTING PLAN**

SHEET: **L1.06**

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**PLANTING LEGEND**

**PLANTING ZONE 1: MANDELA PLAZA ENTRY**

- CAREX DIVULSA
- NANDINA DOMESTICA ALBA 'LEMON-LIME'
- BERBERIS 'ORANGE ROCKET'
- IRIS DOUGLASIANA 'CANYON SNOW'
- MUHLENBERGIA DUBIA 'PINE MUHLY'
- PITOSPORIUM TOBIRA 'MOJO'
- PHORMIUM 'YELLOW WAVE'
- CORDYLINE 'DESIGN-A-LINE'
- DIGIPLEXIS ILLUMINATION 'APRICOT'

**PLANTING ZONE 2: SHADE TOLERANT PLANTS BELOW BART TRACKS**

- MAHONIA 'SOFT CARESS'
- MUHLENBERGIA DUBIA 'PINE MUHLY'
- BLECHNUM SPICANT 'DEER FERN'
- HELLEBORUS ARGUTIFOLIUS
- WOODWARDIA FIMBRIATA 'GIANT CHAIN FERN'
- CORDYLINE 'DESIGN-A-LINE'
- EUPHORBIA CHARACIAS SSP.
- PHORMIUM 'YELLOW WAVE'

**PLANTING ZONE 3: SOUTH FACING ALLEY**

- CAREX DIVULSA
- MUHLENBERGIA DUBIA 'PINE MUHLY'
- IRIS DOUGLASIANA 'CANYON SNOW'
- MONARDELLA VILLOSA 'RUSSIAN RIVER'
- SALVIA SPATHACEA 'HUMMINGBIRD SAGE'
- SALVIA 'DARA'S CHOICE'

PLEASE SEE RELATED IMAGES ON THE NEXT PAGE



**BUILDING T1 FDP PACKAGE**

PROJECT

**WEST OAKLAND BART**  
1451 7th St, Oakland, CA 94607



**PROJECT TEAM**

**BAT ENGINEERS**  
1841 CALIFORNIA BLVD, SUITE 400  
PALM BEACH, FL 33409  
(561) 849-2200

**FOA DESIGN**  
444 17th Street  
Oakland, CA 94612  
(510) 455-1284

**STOK**  
8480 Pine Street  
San Francisco, CA 94111  
(415) 524-1100

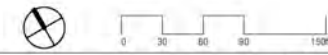
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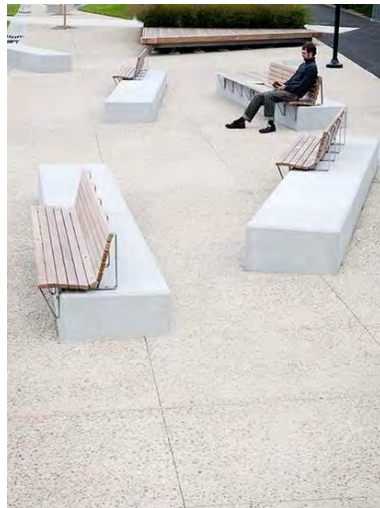
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DATE 05/04/20  
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TITLE **PLANTING ZONE**

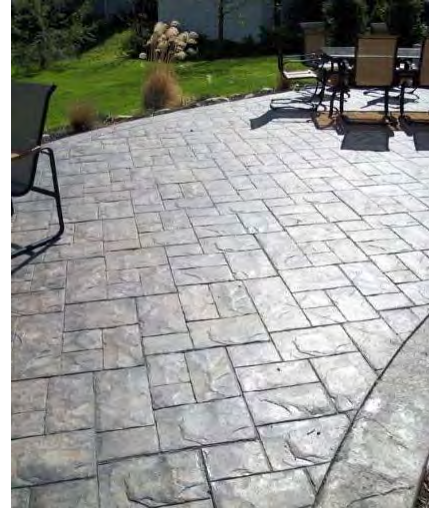
SHEET **L1.07**



**CAST-IN-PLACE CONCRETE PAVING**



COLORED C.I.P w/ SAWCUT BANDING



COLORED C.I.P w/ SAWCUT BANDING



C.I.P w/ SAWCUT BANDING

NOTE: ALL PAVING SHALL BE ADA COMPLIANT AND MINIMIZE UNDULATING AFFECTS WHEN TRAVELING OVER SURFACE

**PLANTER WALL**



POUR-IN-PLACE CONCRETE WITH SACK FINISH

**SITE FEATURES**



SLOPED TURF



EVENT PLAZA

**SITE FURNITURES**



BENCH: MODEL: NEW STANDARD BENCH, MANUFACTURER: STREETLIFE, TEL: 215.247.0148, WEBSITE: <https://www.streetlife.nl/us>



BIKE RACK: MODEL: BOLA BIKE RACK, MANUFACTURER: LANDSCAPEFORMS, TEL: 800.430.6209, WEBSITE: <https://www.landscapeforms.com/en-US>



TRASH BIN: MODEL: CHASE PARK LITTER, MANUFACTURER: LANDSCAPEFORMS, WEBSITE: <https://www.landscapeforms.com/en-US>



PICNIC TABLE SET: MODEL: GRETCHEN PICNIC TABLE, ADA COMPLAINT STYLE, MANUFACTURER: LANDSCAPE FORMS, TELEPHONE: 800.430.6209, WEBSITE: [WWW.LANDSCAPEFORMS.COM](http://WWW.LANDSCAPEFORMS.COM)



DINING TABLE & CHAIR SET: 21 CHAIR WITH ARMS BY LANDSCAPE FORMS, 4 CHAIRS PER TABLE, CATENA TABLE, 48" DIAMETER, NO UMBRELLA HOLE, ADA COMPLAINT QUAD SUPPORT LEGS, ADJUSTABLE GLUDES. FINISH: LANDSCAPE FORMS PROPRIETARY PANGARD II® POLYESTER POWDERCOAT COLOR: TBD FROM MANUFACTURER'S STANDARD PALETTE MANUFACTURER: LANDSCAPE FORMS, TELEPHONE: 800.430.6209, WEBSITE: [WWW.LANDSCAPEFORMS.COM](http://WWW.LANDSCAPEFORMS.COM)



OWNER  
 HENSEL PHELPS  
 suda  
 BUILDING T1  
 FDP PACKAGE  
 PROJECT

**WEST OAKLAND BART**  
 1451 7th St, Oakland, CA 94607

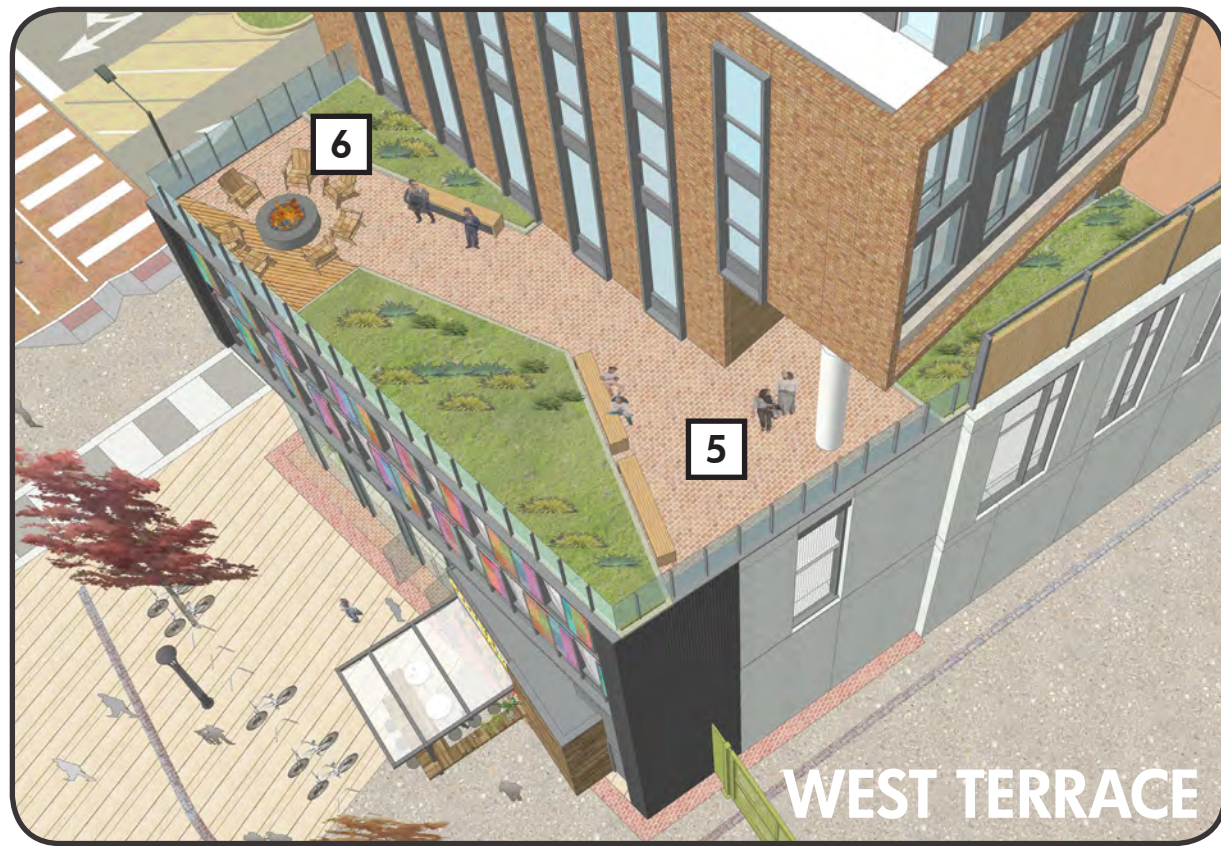
ARCHITECT  
 jrdv ARCHITECTS  
 PROJECT TEAM  
 BKF ENGINEERS  
 1646 N CALIFORNIA  
 BLVD, SUITE 400  
 WALNUT CREEK  
 CA 94596  
 (925) 948-2200  
 PGA DESIGN  
 444 17th Street  
 Oakland  
 CA 94612  
 (510) 485-1284  
 STOK  
 9453 Front Street  
 San Francisco  
 CA 94111  
 (415) 328-7100

STAMP  
 KEY  
 PROJ #: 168-153 WO BART  
 DATE: 06/04/20  
 SCALE:  
 TITLE:  
**LANDSCAPE MATERIALS**  
 SHEET:  
**L1.09**









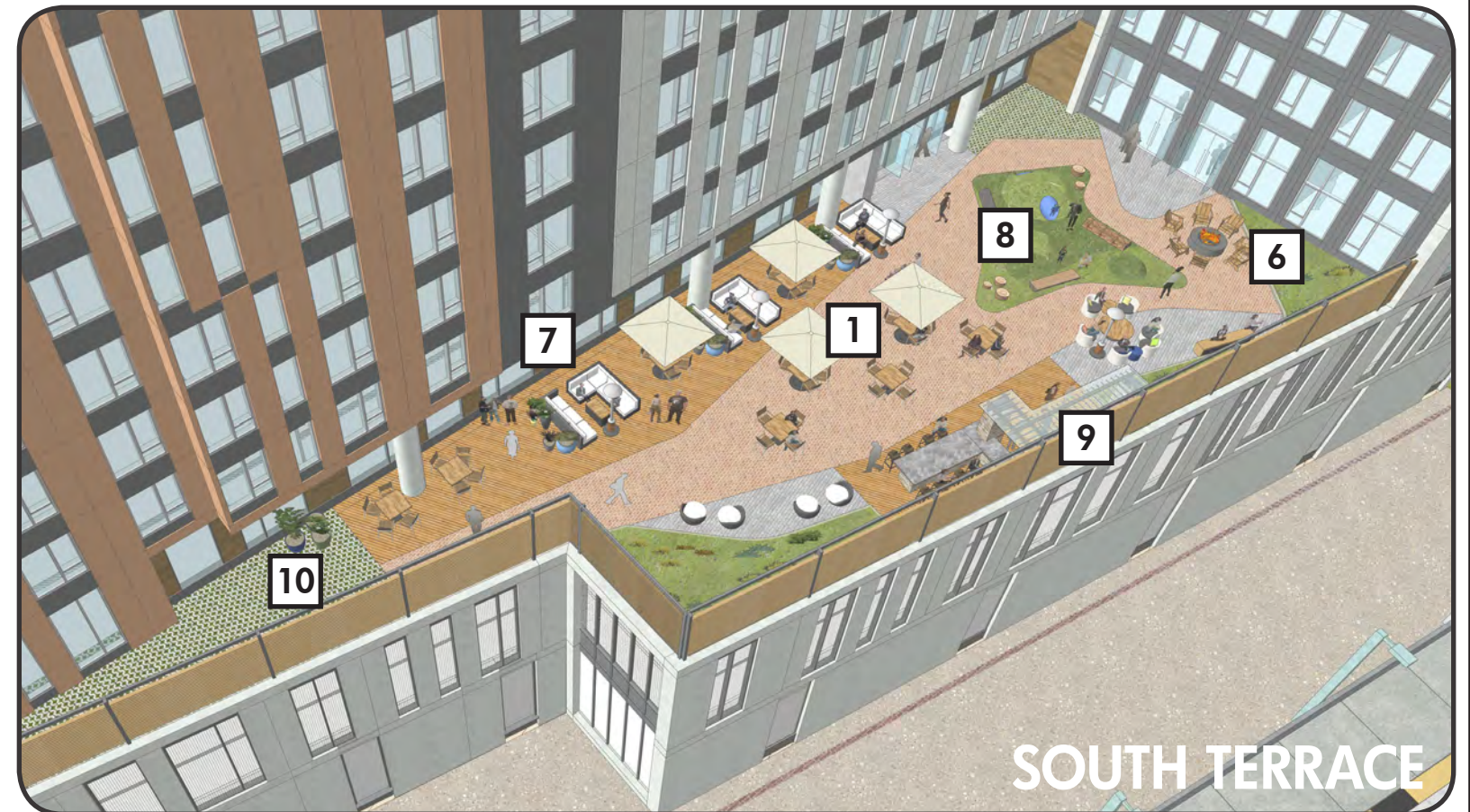
**WEST TERRACE**



**BBQ AND OUTDOOR DINING AREA**



**CHILDRENS PLAY AREA AND LOUNGE SEATING**



**SOUTH TERRACE**

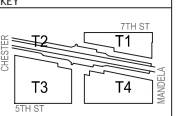
## 4th Floor Roof Terrace Features

- |                                |                             |                              |  |
|--------------------------------|-----------------------------|------------------------------|--|
| <b>1</b> OUTDOOR DINING        | <b>4</b> WIND SCREEN        | <b>7</b> LOUNGE SEATING AREA | <b>10</b> DOG WALK AND WASHING STATION |
| <b>2</b> WOOD LOUNGE PLATFORMS | <b>5</b> BENCH SEATING AREA | <b>8</b> CHILDRENS PLAY AREA |  |
| <b>3</b> DECK OVERLOOKS        | <b>6</b> GAS FIRE PIT       | <b>9</b> BBQ AREA            |  |

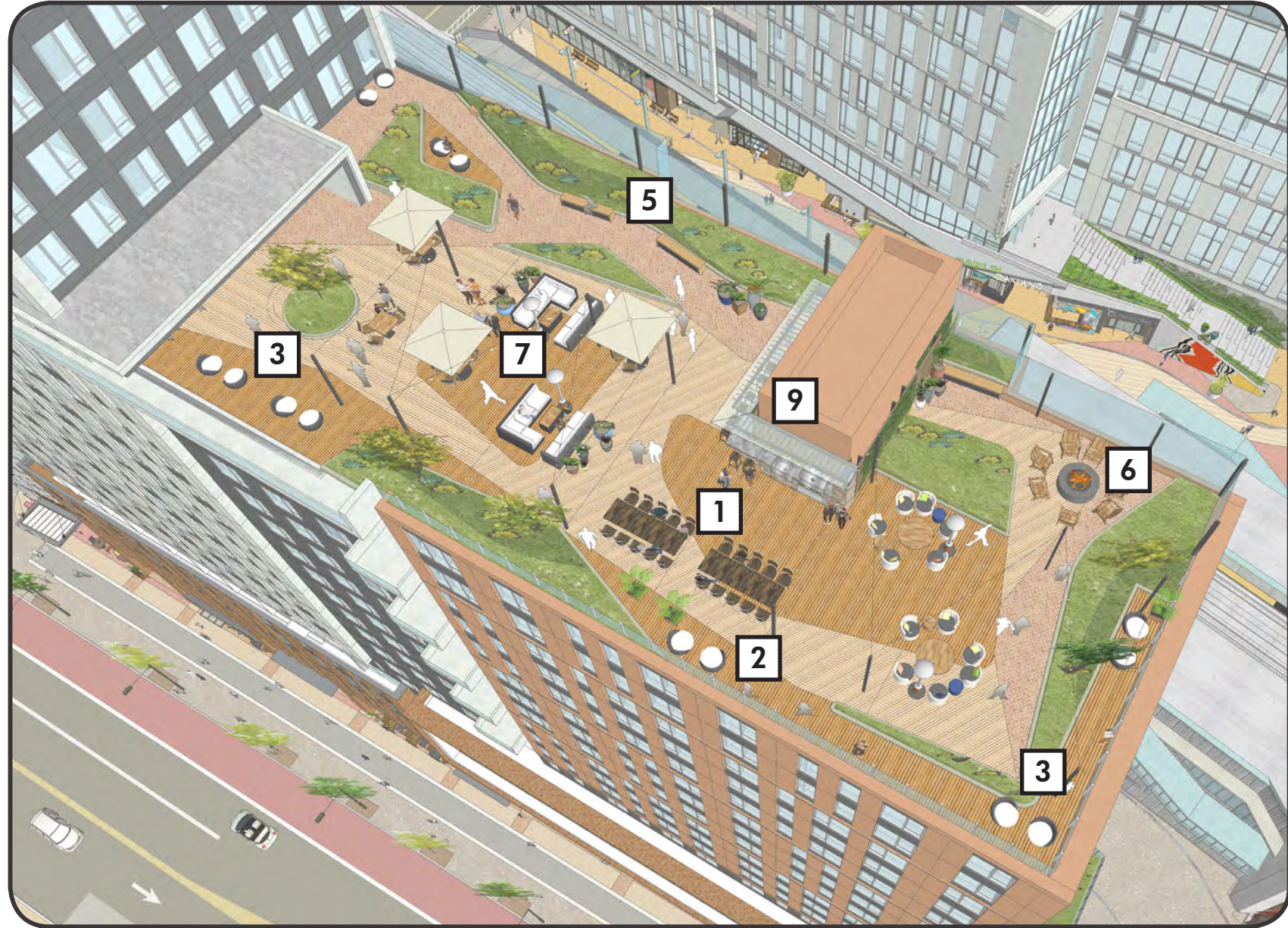
OWNER  
**HEC**  
**HENSEL PHELPS**  
**suda**  
 BUILDING T1  
 FDP PACKAGE  
 PROJECT  
 MANDELA STATION @ WEST OAKLAND BART  
 1451 7th St, Oakland, CA 94607

ARCHITECT  
**jrdrv**  
 ARCHITECTS  
 PROJECT TEAM  
 BKF ENGINEERS  
 1646 N CALIFORNIA  
 BLVD, SUITE 400  
 WALNUT CREEK  
 CA 94612  
 (925) 945-2200  
 PGA DESIGN  
 444 17th Street  
 Oakland  
 CA 94612  
 (510) 455-1284  
 STOK  
 455B Front Street  
 San Francisco  
 CA 94111  
 (415) 325-7100

STAMP



PROJ # 168-153 WO BART  
 DATE June 04, 2020  
 SCALE  
 TITLE **PODIUM LAYOUT PLAN T1**  
 SHEET **L1.13**



BBQ AND OUTDOOR DINING AREA



LOUNGE SEATING, OVERLOOK, AND FIRE PIT

## 19th Floor Roof Terrace Features

**1** OUTDOOR DINING

**4** WIND SCREEN

**7** LOUNGE SEATING AREA

**10** DOG WALK AND WASHING STATION

**2** WOOD LOUNGE PLATFORMS

**5** BENCH SEATING AREA

**8** CHILDRENS PLAY AREA

**3** DECK OVERLOOKS

**6** GAS FIRE PIT

**9** BBQ AREA

**PODIUM PALETTE\_T1**

**AMENITIES**



ADA COMPLIANT OUTDOOR KITCHEN



SEATING WITH FIRE PIT



LOUNGE AREA WITH ADA COMPLIANCE  
COMMUNITY TABLE

**PAVING MATERIALS**



WOOD DECKING



STEPSTONE PAVERS



CHILDREN PLAY AREA WITH ADA ACCESS



**SITE FURNISHINGS**



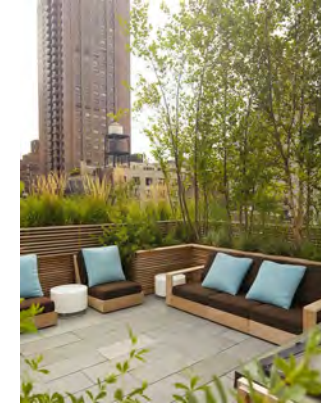
MOVABLE LOUNGE CHAIRS



SEATING ALONG PLANTER



PLANTERS



BUILT-IN FURNISHINGS



DINING AREA

**FENCING AND ARBOR**



GLASS WIND SCREEN

**PLANTING PALETTE**



PHORMIUM 'DARK DELIGHT'



CEANOTHUS X PALLIDUS 'MARIE SIMON'



ANIGOZANTHOS 'BUSH GOLD'



KNIPHOFIA 'NANCY'S RED'



JUNCUS PATENS



VERBENA BONARIENSIS



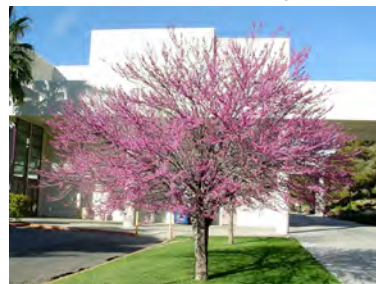
NYALLA LOMANDRA



SENECIO MANDRALISCAE



ACER RUBRUM



CERCIS OCCIDENTALIS



ARBUTUS 'MARINA'



FEIJOA SELLOWIANA

OWNER



**BUILDING T1  
FDP PACKAGE**

PROJECT

**WEST OAKLAND BART**  
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM

BKF ENGINEERS  
1646 N CALIFORNIA  
BLVD, SUITE 400  
WALNUT CREEK  
CA 94596  
(925) 948-2200

PGA DESIGN  
444 17th Street  
Oakland  
CA 94612  
(510) 485-1284

STOK  
1453 Front Street  
San Francisco  
CA 94111  
(415) 328-7100

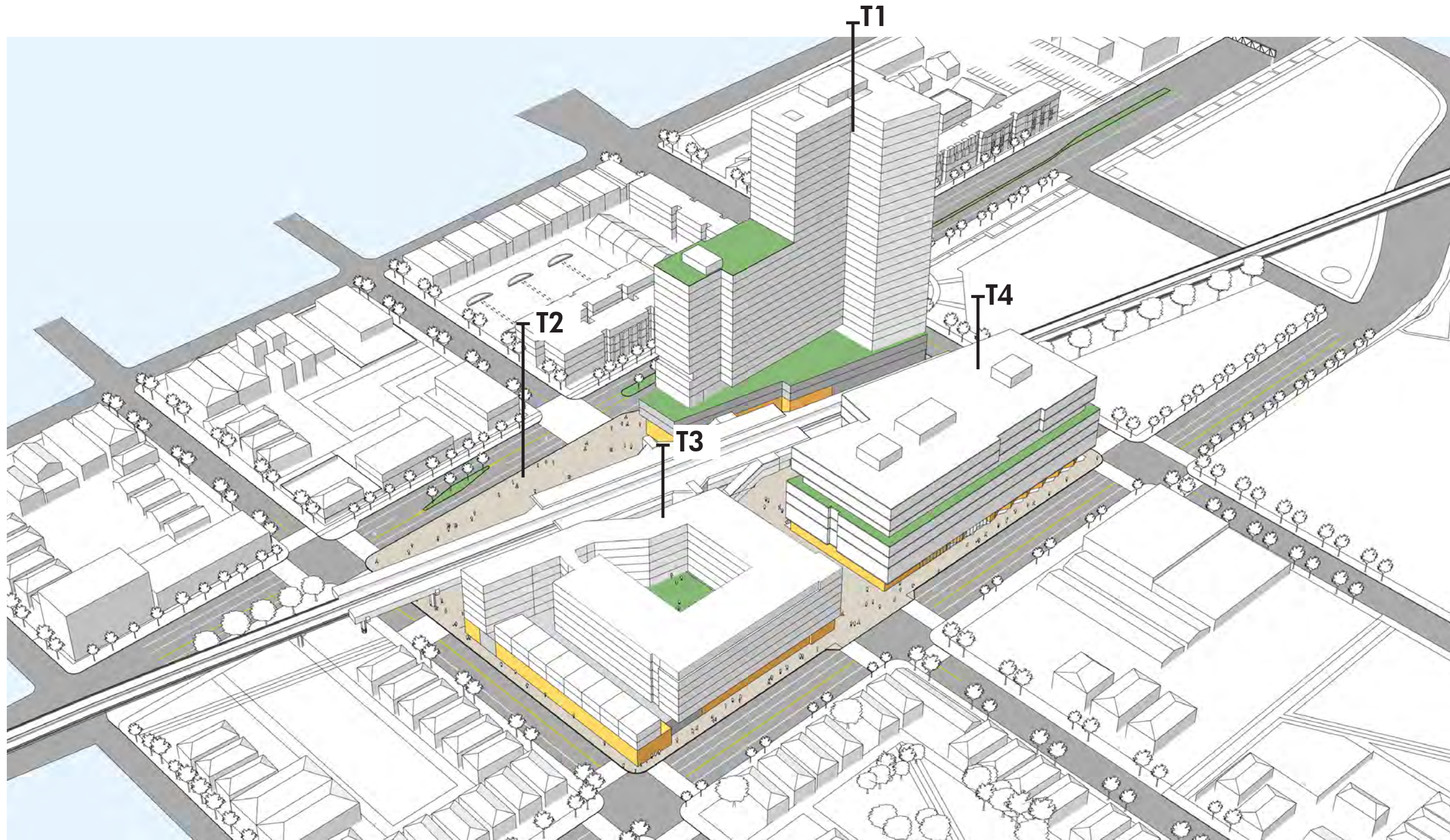
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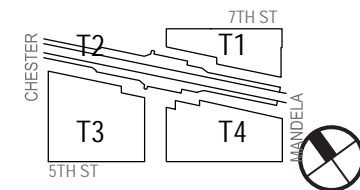
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DATE: 06/04/20  
SCALE:  
TITLE:

**PODIUM  
PALETTE\_T1**

SHEET:  
**L1.15**



**MANDELA STATION  
@WEST OAKLAND  
BART**  
1451 7th St, Oakland,  
CA 94607



JRDV Architects, Inc  
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Broadway and Telegraph  
PO Box 70126  
Oakland, CA 94612 USA  
510 295 4392 T  
www.jrdv.com



PROJ. # 168-153 WO BART  
DATE: July 24, 2020

SHEET: MASSING  
DIAGRAM  
A-02.01



# BUILDING T1 MASSING UPDATE



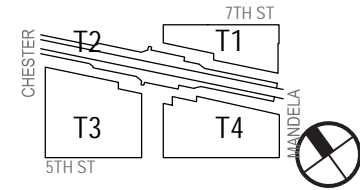
PREVIOUS PDP MASSING



UPDATED FDP MASSING



**MANDELA STATION**  
**@WEST OAKLAND**  
**BART**  
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PROJ. # 168-153 WO BART  
 DATE: July 24, 2020



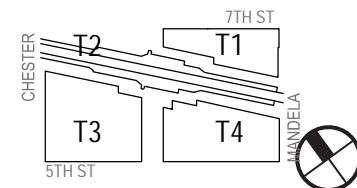
SHEET: MASSING UPDATE  
 A-02.02



**MANDELA STATION  
@WEST OAKLAND**

**BART**

1451 7th St, Oakland,  
CA 94607



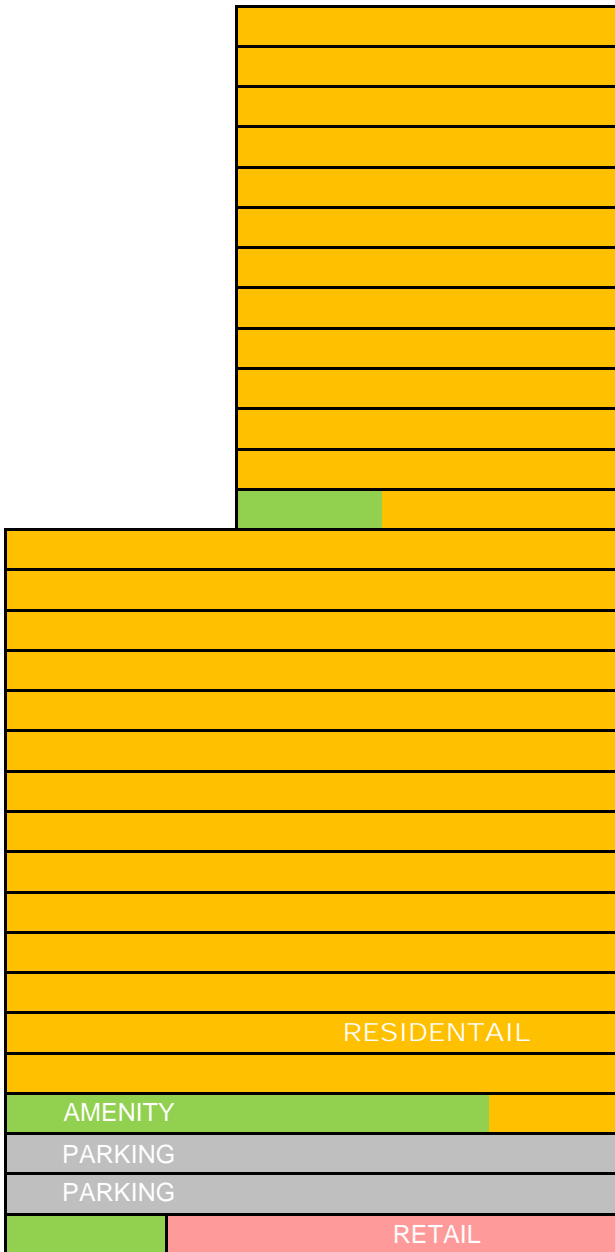
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The Cathedral Building  
Broadway and Telegraph  
PO Box 70126  
Oakland, CA 94612 USA  
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PROJ. # 168-153 WO BART  
DATE: July 24, 2020

SHEET: UNIT MIX  
A-02.03

Fls	FL-FL Hgt	Total Hgt
31	10.50'	320.00'
30	10.50'	303.09'
29	10.50'	292.59'
28	9.67'	282.09'
27	9.67'	272.42'
26	9.67'	262.75'
25	9.67'	253.08'
24	9.67'	243.41'
23	9.67'	233.74'
22	9.67'	224.07'
21	9.67'	214.40'
20	9.67'	204.73'
19	9.67'	195.06'
18	9.67'	185.39'
17	9.67'	175.72'
16	10.67'	166.05'
15	9.67'	155.38'
14	9.67'	145.71'
13	9.67'	136.04'
12	9.67'	126.37'
11	9.67'	116.70'
10	9.67'	107.03'
9	9.67'	97.36'
8	9.67'	87.69'
7	9.67'	78.02'
6	9.67'	68.35'
5	9.67'	58.68'
4	9.67'	49.01'
3	9.67'	39.34'
2	9.67'	29.67'
1	20'	20'



**T1 Retail Summary**

Total Gross Area
14,350 sf

Bldg T1		UNIT MIX				
Gross Area		3Bedroom	2Bedroom	1Bedroom	STUDIO	AVG UNIT AREA
		1,184 sf	937 sf	618 sf	436 sf	
Lvl31	14,787 sf	2	2	6	3	
Lvl30	14,787 sf	3	2	6	3	
Lvl29	14,787 sf	3	2	6	3	
Lvl28	14,787 sf	3	2	6	3	
Lvl27	14,787 sf	3	2	7	3	
Lvl26	14,787 sf	3	2	7	4	
Lvl25	14,787 sf	3	2	7	4	
Lvl24	14,787 sf	3	2	7	4	
Lvl23	14,787 sf	3	2	7	4	
Lvl22	14,787 sf	3	2	7	4	
Lvl21	14,787 sf	3	2	7	4	
Lvl20	14,787 sf	3	2	7	4	
Lvl19	15,146 sf	2	1	6	4	
Lvl18	22,391 sf	3	5	10	5	
Lvl17	22,391 sf	3	5	10	5	
Lvl16	22,391 sf	3	5	10	5	
Lvl15	22,391 sf	3	5	10	5	
Lvl14	22,391 sf	3	5	10	5	
Lvl13	22,391 sf	3	5	10	5	
Lvl12	22,391 sf	3	5	10	5	
Lvl11	22,391 sf	3	5	10	5	
Lvl10	22,391 sf	3	5	10	5	
Lvl9	22,391 sf	3	5	10	5	
Lvl8	22,391 sf	3	5	10	5	
Lvl7	22,391 sf	3	5	10	5	
Lvl6	22,391 sf	3	5	10	5	
Lvl5	22,391 sf	3	5	10	5	
Lvl4	21,714 sf	1	1	2	1	
Lvl3	30,324 sf					
Lvl2	30,324 sf					
Lvl1	29,343 sf					
617,769 sf		80	96	228	118	

**T1 Residential Summary**

Total Gross Area	3BR	2BR	1BR	STUDIO	TOTAL
527,778 sf	80	96	228	118	522

**T1 Parking Summary**

Lvl2	Lvl3	TOTAL
67	70	137

**T1 Area Summary**

Total Gross Area (Planning)
617,769 sf

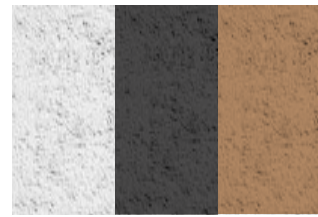
UPPER FLOORS

TOP FLOOR



CHANNEL  
METAL  
MECHANICAL  
SCREEN

ALL ELEVATION



ARCHITECTURAL  
PRECAST PANELS

EAST ELEVATION

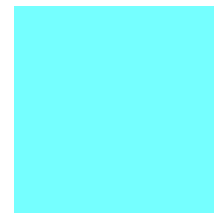


BEIGE  
PLASTER

WINDOW SYSTEM

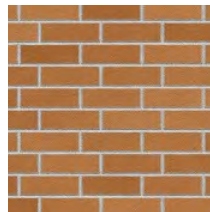


ALUMINUM  
WINDOW  
MULLION  
SYSTEM



HIGH PERFORMANCE  
INSULATED GLAZING  
UNIT - LIGHT GREEN

PODIUM



LIGHT GAUGE  
MTL WALLS  
w/ THIN  
BRICK FACE

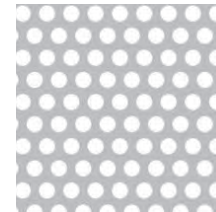


LIGHT GAUGE MTL /  
MASONRY WALLS  
w/ WHITE 3-COAT  
CEMENT PLASTER

NORTH, WEST, SOUTH ELEVATION PODIUM



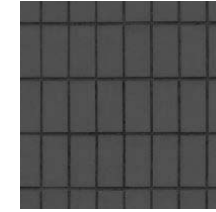
DECORATIVE METAL PANEL



PERFORATED  
METAL PANEL



GREY  
PAINTED  
STEEL PANEL  
FRAME



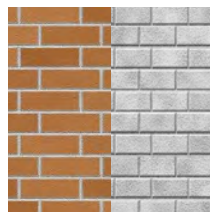
DARK GRAY  
TILE



3"X3"WOVEN  
WIRE MESH IN  
MTL FRAMES

GROUND FLOOR

EAST AND NORTH ELEVATION

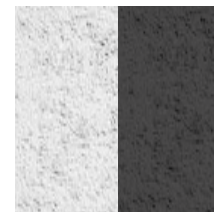


LIGHT GAUGE  
MTL WALLS  
w/ THIN  
BRICK FACE



EXPOSED  
CONCRETE  
COLUMNS

SOUTH ELEVATION



MASONRY  
WALLS w/  
3-COAT  
CEMENT  
PLASTER

STOREFRONT SYSTEM



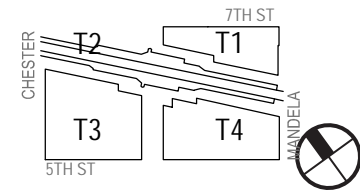
WINDOW  
MULLION  
w/ KYNAR  
FINISH



CLEAR GLASS  
STOREFRONT  
SYSTEM



MANDELA STATION  
@WEST OAKLAND  
BART  
1451 7th St, Oakland,  
CA 94607



JRDV Architects, Inc  
The Cathedral Building  
Broadway and Telegraph  
PO Box 70126  
Oakland, CA 94612 USA  
510 295 4392 T  
www.jrdv.com



PROJ. # 168-153 WO BART  
DATE: July 24, 2020

SHEET: MATERIAL  
PALETTE  
A-02.04





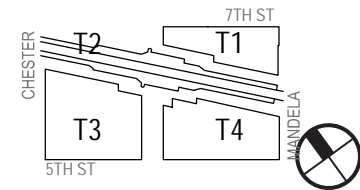
PODIUM  
LIGHT GAUGE MTL WALLS W/ THIN BRICK



UPPER FLOORS  
CLARK COMPOSITE ARCHITECTURAL PRECAST PANELS (C-CAPP)



MANDELA STATION  
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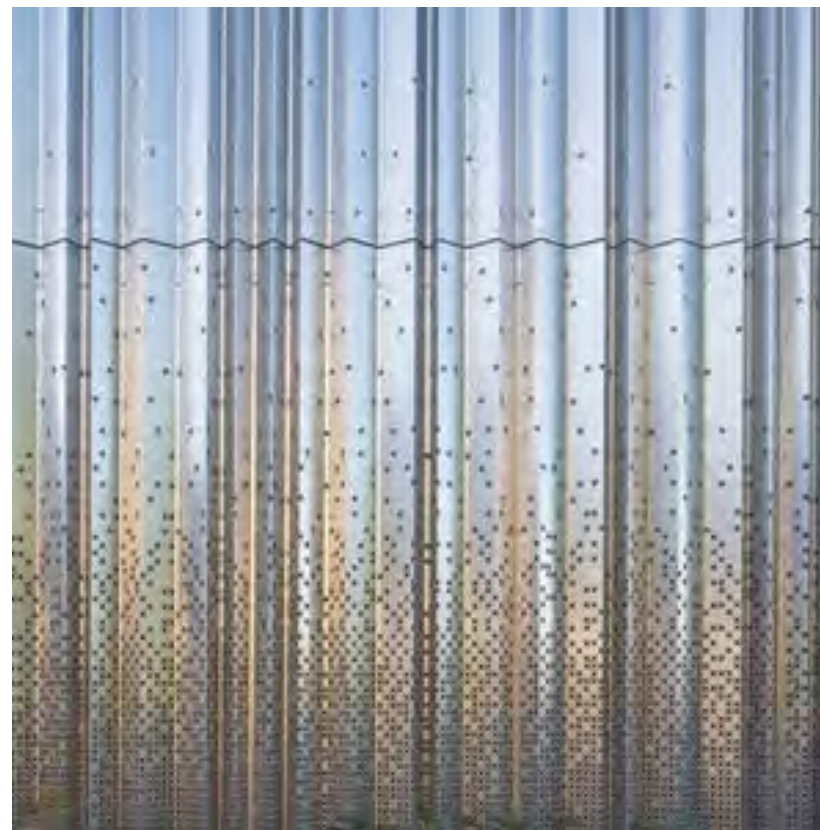


PROJ. # 168-153 WO BART  
DATE: July 24, 2020

SHEET: MATERIAL PALETTE  
A-02.05



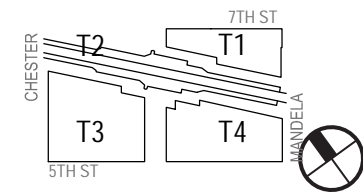
CURTAIN WALL  
HIGH SOLAR PERFORMANCE WINDOW SYSTEM



ROOF MECHANICAL SCREEN  
METAL CLADDING WITH STANDING SEAM



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PALETTE  
A-02.05